

MARDA LOOP

AREA REDEVELOPMENT PLAN

PROPOSED



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THE CITY OF
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LAND USE PLANNING & POLICY

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The Marda Loop Area Redevelopment Plan (ARP or the Plan) sets out the vision and policy framework that will guide future development in Marda Loop. An area with a unique character and an inner-city location, Marda Loop is a popular shopping destination and a desirable place to live. However, the area is under development pressure and there is a need to ensure that future development enhances the area's character and makes a positive contribution to Marda Loop as a whole.

The Marda Loop ARP aligns with city-wide objectives to accommodate a portion of Calgary's growth within the developed areas of the city and along mixed-use corridors. Marda Loop is envisioned to develop into a mixed-use area with a complementary blend of commercial and residential development. The Plan allows for retail space at the ground level of buildings with residential units and/or office space above. It is intended to be flexible such that future development can adapt to changing market conditions. Importantly, the Plan ensures that new development will provide a sensitive transition to the adjacent residential streets and will implement a high standard of urban design for the area, requiring new developments to provide enhancements to the public realm and the pedestrian experience.

1.1 Purpose of the Plan

The purpose of this Plan is to provide a policy framework to guide the long-term redevelopment of this inner-city area. The Plan provides clear policy direction for key aspects such as the vision, scale, urban form and character for Marda Loop's redevelopment.

1.2 Intent of the Plan

The Plan is future-oriented and depicts how the Marda Loop Plan area is to be developed over an extended time period. No specific timeframe is applied to the Plan although the majority of the proposed development is expected within a 25 to 30 year horizon.

The main elements that the Marda Loop ARP addresses are:

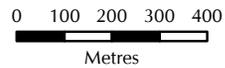
- establishes a coherent and consistent vision for the area to guide its development and improvement
- translates strategic policies from the Municipal Development Plan and Calgary Transportation Plan to the local area level

- establishes a design and land use framework to achieve the vision
- provides a clear design approach for new development, which will guide decision makers including Council, Calgary Planning Commission, Administration and the Subdivision & Development Appeal Board on Outline Plan/Land Use Amendment and Development Permit applications
- provides the basis on which development proposals will be evaluated.

Map 1.1 Plan Area Location and Boundary

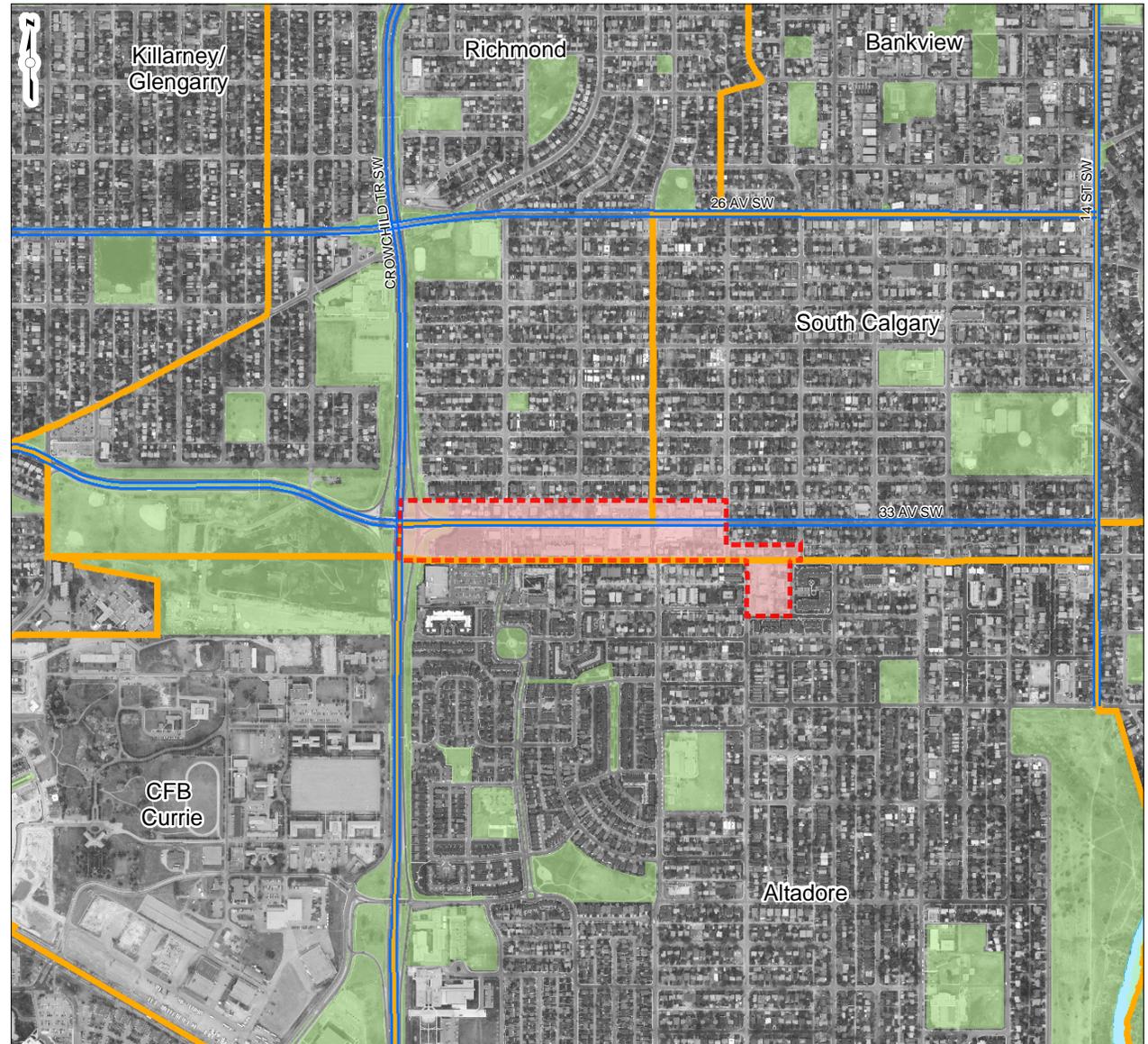
Legend

-  Major Road
-  Plan Area Boundary
-  Community Boundary
-  Parks & Open Space



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1.3 Plan Context

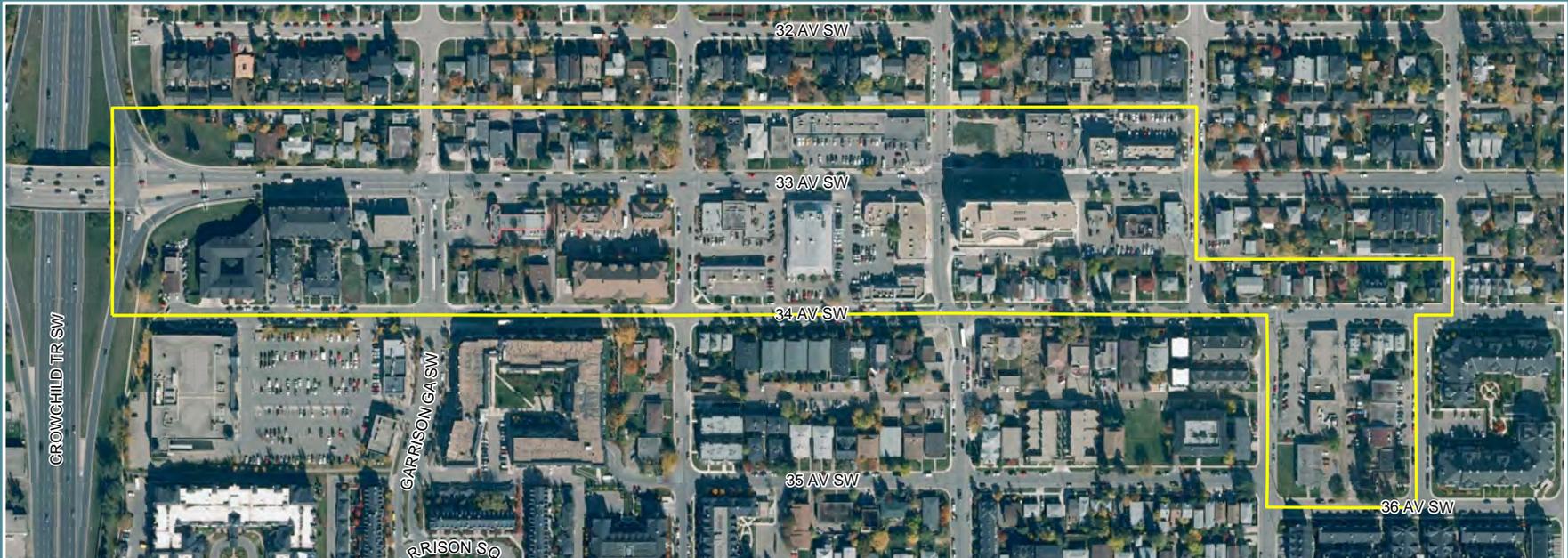
1.3.1 Plan Area Location & Boundaries

Marda Loop is located within the southwestern quadrant of the city and is within close proximity to the downtown core (Map 1.1). The Plan area boundaries are defined by Crowchild Trail SW to the west; the lane north of 33 Avenue SW between Crowchild Trail SW and 19 Street SW and the lane

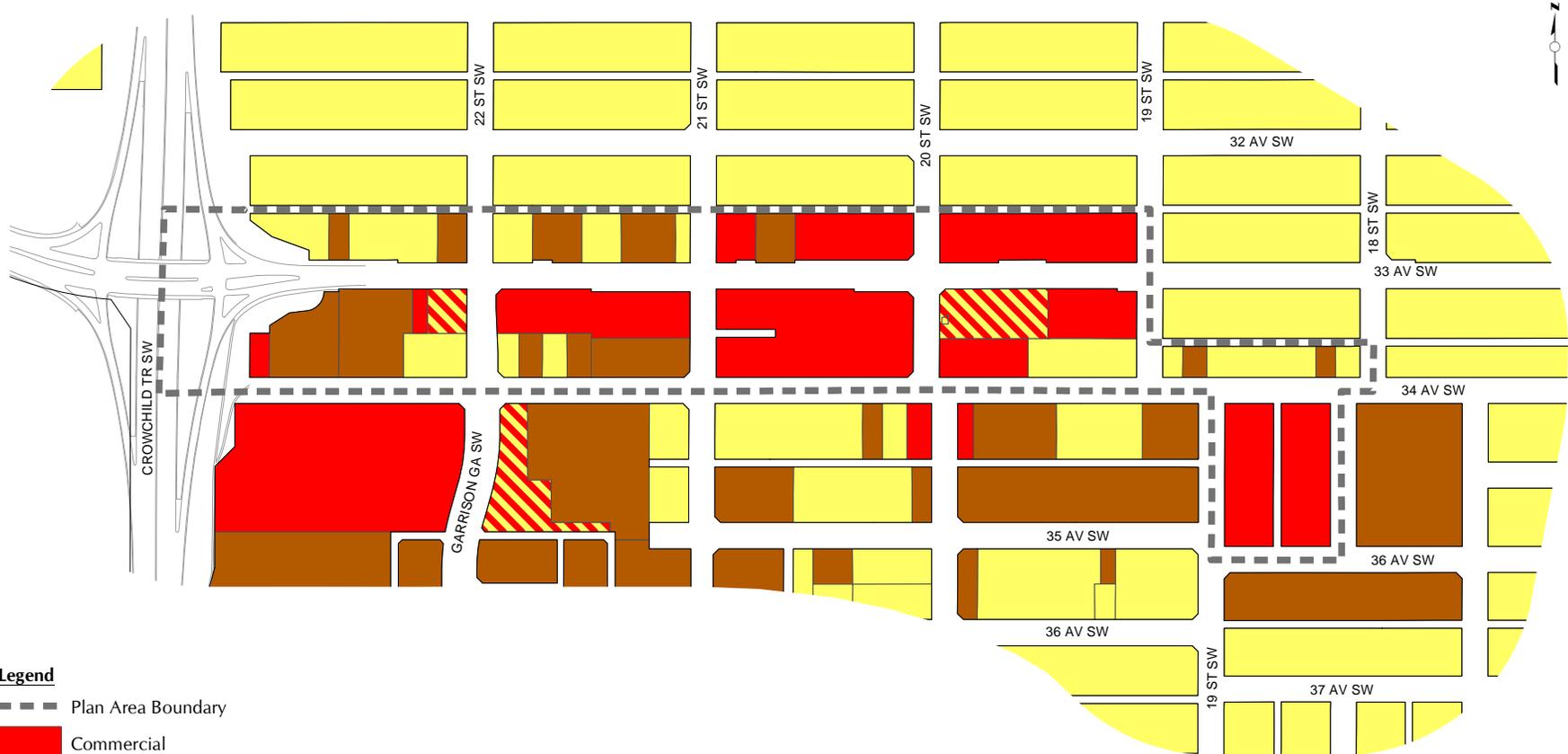
north of 34 Avenue SW between 18 Street SW and 19 Street SW to the north; 18 Street SW to the east; 34 Avenue SW between Crowchild Trail SW and 19 Street SW and 36 Street SW between 19 Street SW and 18 Street SW to the south (Figure 1.1). The Plan area is primarily focused on the corridors of 33 Avenue SW and the north side of 34 Avenue SW as these areas are primarily commercial and would be included within the Neighbourhood Corridor defined by the MDP. The Plan area also includes an existing commercial block that is situated along 18 Street SW and 19 Street SW, between 34 and 36 Avenue

SW due to its proximity to the Neighbourhood Corridor and potential to redevelop in a manner similar to the rest of the Plan area.

Figure 1.1 Air Photo

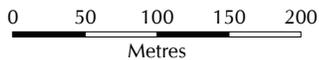


Map 1.2 Existing Land Use



Legend

- Plan Area Boundary
- Commercial
- Mixed-Use Commercial/Multi-Residential
- Residential - Low Density
- Residential - Medium Density



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1.3.2 Existing Land Use and Built Form

Marda Loop is characterized by a mix of commercial, multi-residential, and low-density residential land uses (Map 1.2). Sites within Marda Loop have undergone redevelopment at various points in time, and the area is characterized by various types of development that are typical of the time during which they were constructed. As a result, the current land use and development pattern within Marda Loop is varied and inconsistent.

Buildings are primarily single-use (commercial or residential), although there are some newer mixed-use buildings in the area. Building heights vary from one to six storeys, with most buildings not exceeding two storeys in height. Newer developments are generally built to the full extent allowed under their land use district, whereas older developments are built below the extent allowed by their land use districts at the time.



Six storey mix-use building



Single storey commercial building



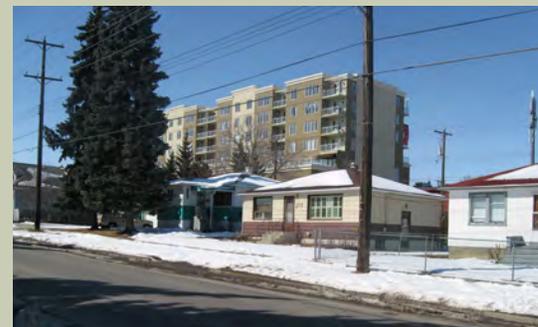
Single-detached dwellings converted to commercial use



Low-scale apartment buildings



Two storey commercial building



Single-detached residential dwelling units

1.3.3 Opportunities and Challenges

● opportunities
● challenges

Rear lane access north of 33 Avenue SW - the existing rear lane north of 33 Avenue SW can provide vehicle access for new development on these sites.

Grade separation near Crowchild Trail - the rise in elevation between the road and sidewalk makes for difficult pedestrian access.

Separation between buildings and the sidewalk - several parcels accommodate parking between the sidewalk and the building frontage.

The main image is an aerial photograph of a residential and commercial area. A large blue arrow points from a yellow circle (challenge) on Crowchild Trail to a yellow circle (challenge) on a residential street. A green circle (opportunity) is located on a street to the east. Three inset photographs provide ground-level views of the challenges mentioned in the text.



Adjacency to low density residential - low density residential dwellings are located across the lane from commercial and mixed-use development.

Vehicle crossing of sidewalks - vehicle crossings are prevalent throughout the Plan area and result in interruptions to the pedestrian realm.

Poor at-grade connections and lack of active uses - few doors, windows and pedestrian connections from the sidewalk to commercial buildings provides little in the way of pedestrian activity and interest.

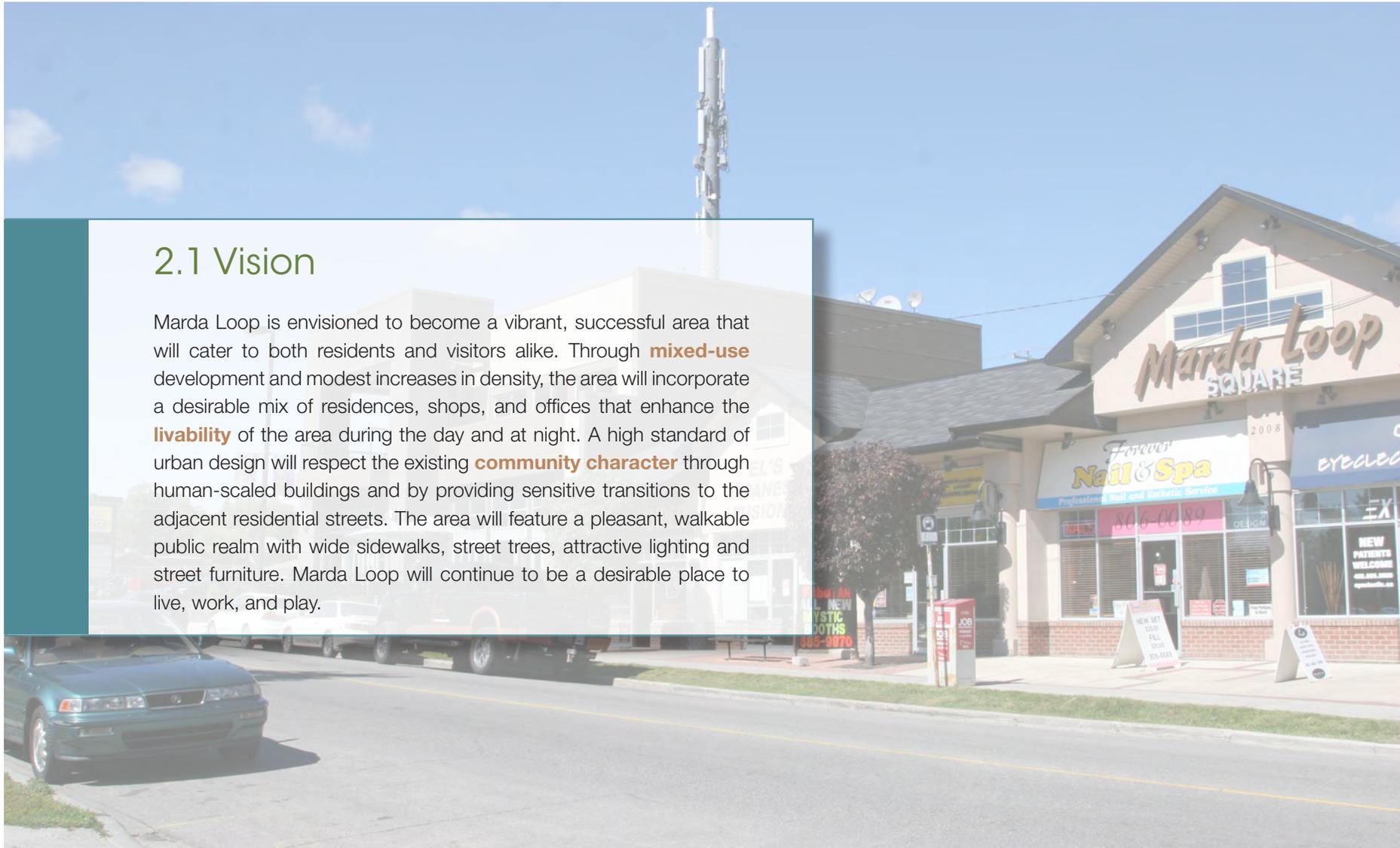
Underdeveloped or vacant sites - these sites provide an opportunity for redevelopment in the near term due to the limited nature of development currently on the site.

2.0

Vision & Guiding Principles

2.1 Vision

Marda Loop is envisioned to become a vibrant, successful area that will cater to both residents and visitors alike. Through **mixed-use** development and modest increases in density, the area will incorporate a desirable mix of residences, shops, and offices that enhance the **livability** of the area during the day and at night. A high standard of urban design will respect the existing **community character** through human-scaled buildings and by providing sensitive transitions to the adjacent residential streets. The area will feature a pleasant, walkable public realm with wide sidewalks, street trees, attractive lighting and street furniture. Marda Loop will continue to be a desirable place to live, work, and play.



2.2 Guiding Principles

The following Guiding Principles represent the key concepts from the Vision as well design intents for future redevelopment in the Plan area. These principles are intended to apply to both an area-wide and site-specific perspective, and will inform planning policy and future development within the Marda Loop area.



1. Community Character

Development should respect the local context, history and character of the area and promote a sense of place through the design of buildings and public spaces. Building height and massing should respect the scale of adjacent neighbourhoods.



2. Livability

The design of buildings, sidewalks and public spaces should provide a sense of both comfort and security for pedestrians by providing natural surveillance, encouraging activity throughout the day and defining and beautifying the public realm.



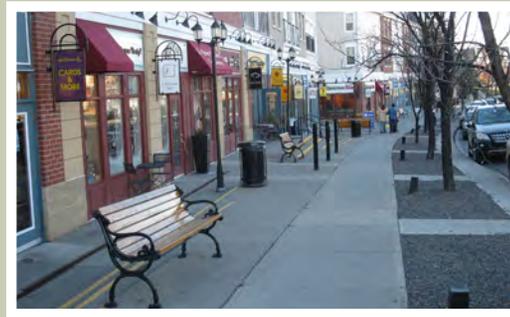
3. Mixed-Use

A mix of compatible land uses, including retail, office, residential, and live/work units, provide vitality and life to the area throughout the day and evening. A variety of services and housing types will allow for the ability to live, work and play without the requirement of an vehicle.



4. Walkability

Pedestrians should be the primary focus. Streets, sidewalks and buildings should be designed to support walking. Sidewalks should be lined with active uses and not be interrupted by vehicle crossings or curb cuts as these disrupt the pedestrian experience.



5. Streetscape Design

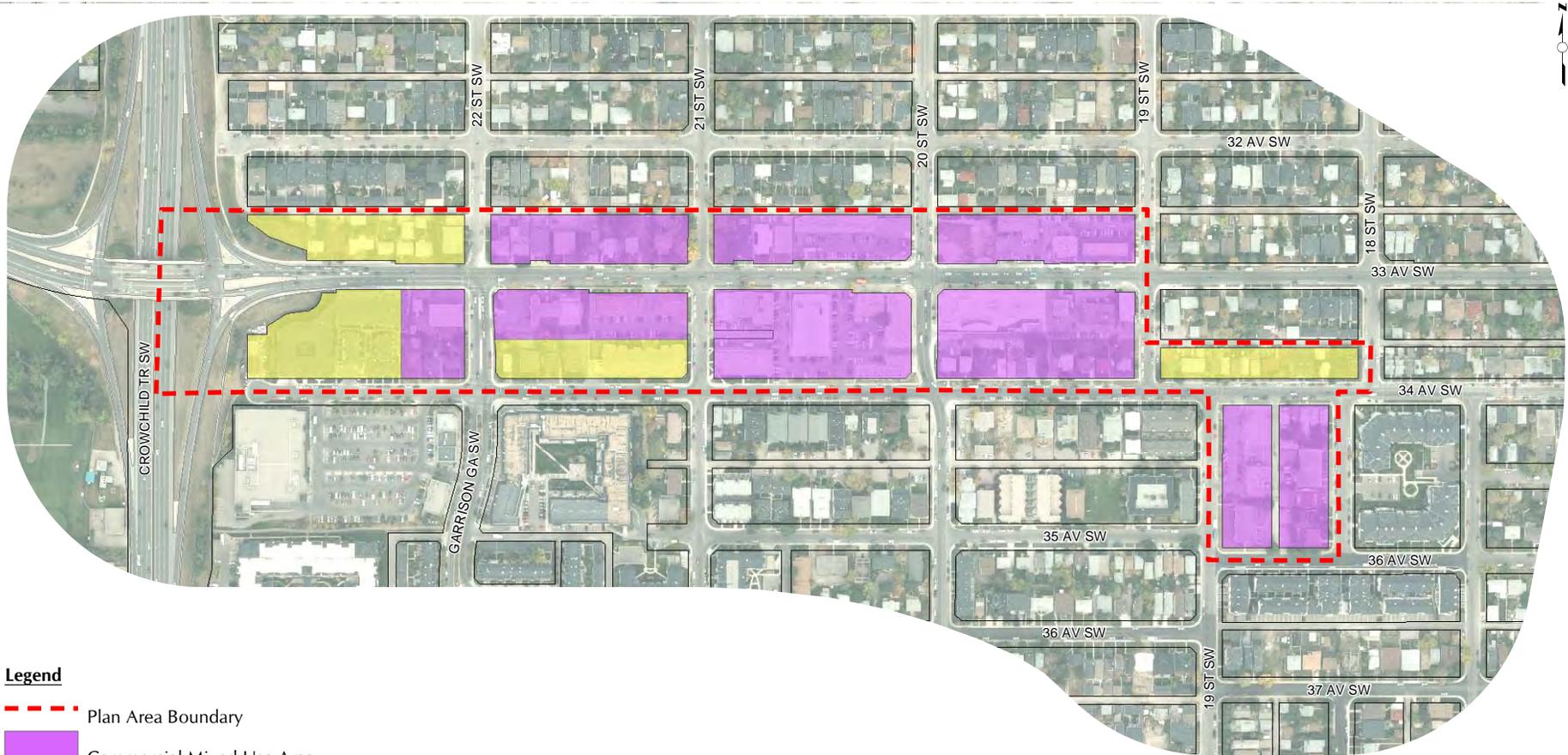
A high standard of streetscape design should help provide for easy connections throughout an area. Streets should be supported by streetscape elements, such as street trees, lighting, street furniture and pavement treatments. These elements enhance the public realm and improve the desirability for walking.



6. Street Fronting Buildings

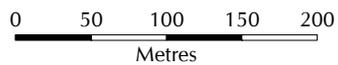
To define the streetscape and avoid disruption of the public realm, buildings should create a continuous street wall. Building façades should provide for doors and windows along the street and be designed to establish a human-scale rhythm that creates visual interest at the street level.

Map 3.1 Land Use Policy Areas



Legend

- Plan Area Boundary
- Commercial Mixed-Use Area
- Residential / Retail Area



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3.0

Land Use & Density

The majority of future development within Marda Loop is intended to be mixed-use, however, certain areas will feature a stronger commercial or residential component in order to take advantage of locational attributes. The Plan area is divided into two land use policy areas which set out clearly defined land use objectives: a Commercial Mixed-Use area and a Residential/Retail area. Both policy areas will provide for development that will encourage pedestrian activity, transit ridership and diversity in the population and users of the area.

Corridors should provide for a broad mix of residential, employment and retail uses.

MDP 3.4.1.a

3.1 General Policies

1. Land use redesignations must be consistent with the general land use classifications identified on Map 3.1 Land Use Policy Areas.
2. New automobile service centres, drive-through businesses and service stations are not allowed within the Plan area.
3. The provision of a broad range of housing types is encouraged for different types of households, income levels, age groups and lifestyles. Provision of larger unit sizes and ground oriented units appropriate for families with children is strongly encouraged.
4. A mix of land uses is encouraged for all development projects, including:
 - Residential uses (e.g., townhouses and apartments)
 - Employment uses such as offices
 - Street-oriented retail/commercial uses
 - Neighbourhood local services including day-cares and clinics
 - Local entertainment uses including recreation and cultural facilities, theatres and eating and drinking establishments
 - Live / work units
5. Stand-alone single-use parking lots and parking structures are strongly discouraged throughout the Plan area.

Auto-oriented uses and designs that generate high volumes of traffic, consume large amounts of land in a low density form, require extensive surface parking, and create negative impacts for pedestrian travel and access should be discouraged.

MDP 3.4.3.g

3.2 Commercial Mixed-Use Area

The Commercial Mixed-Use policy area is located primarily along 33 Avenue SW and extends onto parts of the north side of 34 Avenue SW and to the southeastern edge of the Plan area. These locations receive a high volume of pedestrian and vehicular traffic or already contain ground floor commercial uses, thus making them more suitable for a strong commercial focus. Ideally, buildings will contain ground floor retail/commercial uses with residential or office/commercial uses above.

1. New development should incorporate a mix of land uses within a single building. The mix of uses should include ground floor retail/commercial development and a minimum of one of the following uses: office/commercial and/or multi-residential located above ground floor retail/commercial development.

Develop an active street environment by encouraging retail and service uses at-grade with residential and office uses on upper floors.

MDP 3.4.1.d

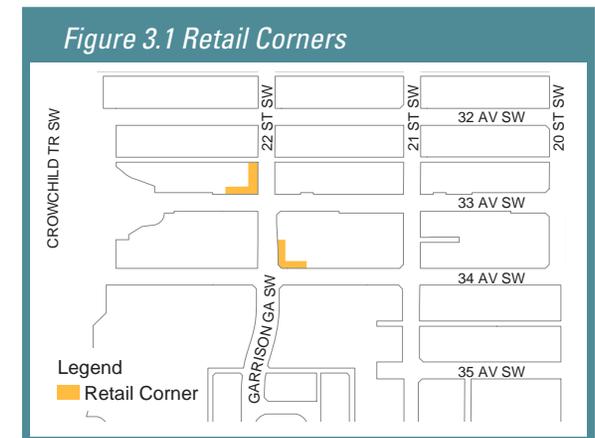
2. Small-scale retail/commercial establishments are encouraged to be located on the ground floor of buildings. Retail/commercial establishments larger than 300 square metres should be discouraged except for uses such as supermarkets, pharmacies and other similar uses which provide various daily goods and services for residents.
3. Commercial uses that do not generate significant pedestrian activity, such as financial institutions, may locate on the ground floor provided the frontage does not exceed 15 metres. The remainder of the commercial area should locate on a second floor or wrap behind adjacent retail units. Lobbies for multi-residential developments may also locate on the ground floor provided the frontage does not exceed 15 metres.
4. The Development Authority may consider appropriate relaxations with respect to the minimum distance between restaurants and adjacent low density residential districts where it can be determined that potential impacts, such as noise, odour, vibration, heat, high illumination levels and waste, can be mitigated.

3.3 Residential/Retail Area

The Residential/Retail policy area is located primarily at the edges of Marda Loop and includes sites that are currently residential. This policy area is intended to be flexible, allowing for single- or multi-use development that can adapt to changes in the market and to the

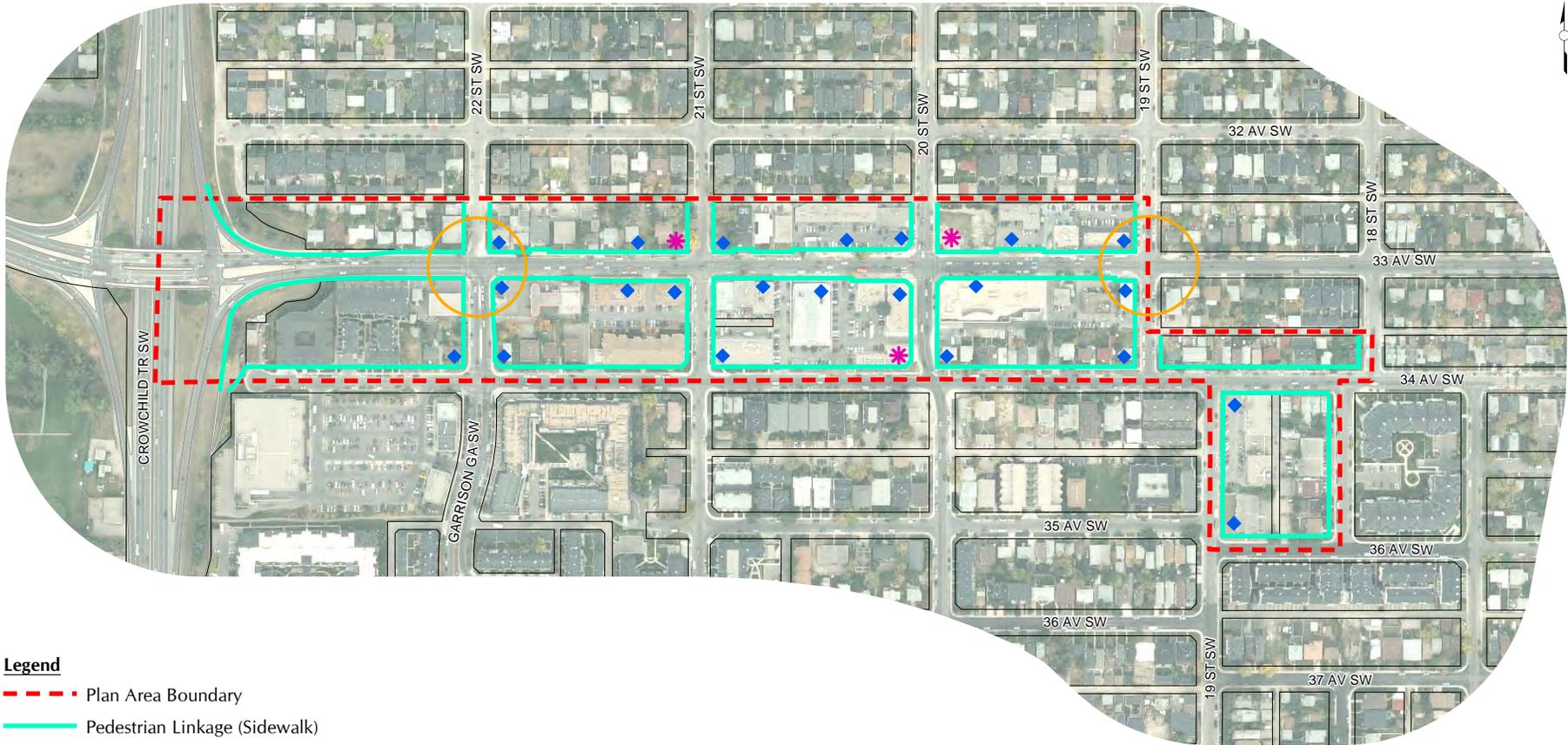
evolving needs of the community. Retail/commercial uses must be located on the ground floor of buildings at key corners within the area.

1. New development should primarily be multi-residential, but may include retail/office commercial uses within the first and second storey.
2. To provide for pedestrian activity along 22 Street SW, residential and office uses are not allowed on the ground floor fronting onto key retail corners as shown in Figure 3.1. Ground floor uses at these locations should be retail/commercial.



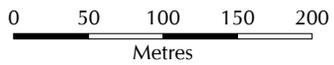
3. Where residential uses are provided on the ground floor, the design of the building should allow for future conversion to retail/commercial uses.
4. New stand-alone single-use retail buildings are strongly discouraged.
5. Live/work units are encouraged throughout the Residential/Retail Area.

Map 4.1 Public Realm



Legend

- - - Plan Area Boundary
- Pedestrian Linkage (Sidewalk)
- * Potential Plaza Location
- ◆ Potential Location for Public Art / Local History Interpretation Feature
- Primary Gateway



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4.0

Built Form & Site Design

Buildings within the Marda Loop area should foster a vital and active pedestrian-oriented street life. They should relate well to the street and to each other, provide opportunities to maintain views and sunlight penetration to streets, create attractive rooflines and minimize shadowing. Buildings should enclose streets and spaces through height and massing, forming inviting 'urban rooms' where the public life of the neighbourhood can thrive. Taller buildings should address the skyline with distinctive architectural and rooftop design.

Promote site and building design that contributes to high quality living environments and attractive, walkable, diverse neighbourhoods and communities.

MDP 2.4.2



Marda Loop development model (concept)

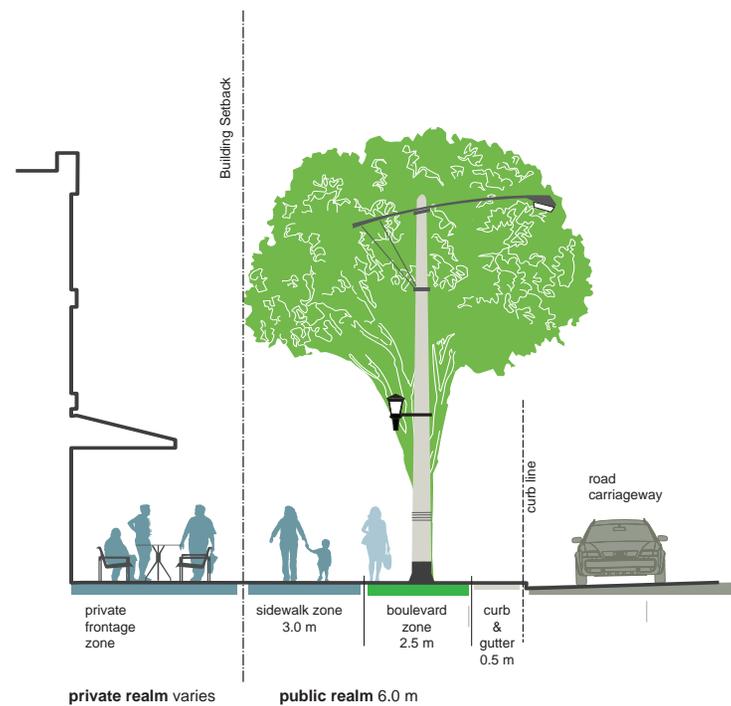
4.1 Site Design

The success of Marda Loop is dependent not only on architectural design and use of buildings but equally on the spaces between them. Planning of individual sites should consider the relationship between streets, sidewalks, landscaping and building edges. It is also important to consider the location and design of parking so that it does not detract from the quality of the urban environment.

Enhance the public realm and promote pedestrian use through the coherent and collaborative design of streets, building interfaces and public spaces.

MDP 2.4.3

Figure 4.1 Public Realm Zones



Private Frontage Zone	Width: Variable
	Uses: Outdoor seating, bicycle racks, building and entrance projections, windows, canopies and signage.
	Surface treatments should be consistent with sidewalk zone.
Sidewalk Zone	Width: 3.0 metres.
	Uses: None; must be kept clear. Can be used for underground utilities.
	Surface treatments: Smooth, non-slip surfaces.
Boulevard Zone	Width: 2.5 metres.
	Uses: Street tree planting, street furniture, bicycle racks and bus stops.
	Surface treatments: Concrete or pebbled surface. Storm water retention areas incorporated.

4.1.1 Public Realm

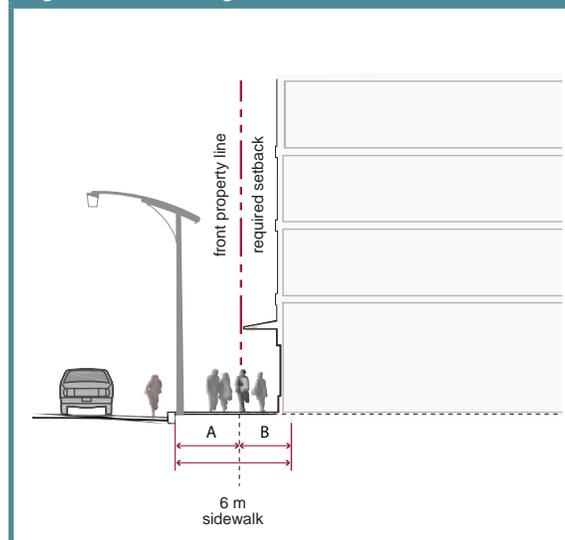
All urban design elements within the Plan area are intended to contribute to a varied yet coherent Marda Loop identity – while providing amenity in the form of interesting and walkable sidewalks, streets designed as outdoor public rooms and usable public outdoor space. The streetscape policies of this Plan are intended to foster and guide the development of a diverse and walkable urban environment. Map 4.1 Public Realm identifies potential locations for plazas and public art based on the development potential of these sites, their location at key intersections or corners and in certain cases their location on the north side of the avenues.

Provide sufficient and uniform sidewalk width to allow for comfortable and safe pedestrian traffic, the planting of trees and additional landscaping and wayfinding elements.
MDP 2.4.3.c

Pedestrian Network

1. All redevelopment sites should provide an improved public realm street edge within the public road rights-of-way between the existing curb and property line. This new pedestrian realm should be consistent with the zones and minimum dimensions shown on Figure 4.1 Public Realm Zones.
2. New development should provide a minimum building setback of 6.0 metres from the back of curb (see Figure 4.2) in order to provide for the Public Realm Zones shown in Figure 4.1.

Figure 4.2 Building Setbacks



3. At the discretion of the Development Authority, the minimum building setback in Policy 4.1.1.2 may be reduced where site constraints, particularly limited lot depth, would impact the ability to develop the site.
4. Where the minimum public realm dimensions shown in Figure 4.1 cannot be accommodated within the existing rights-of-way, additional building setback should be provided and may be secured through the use of easements or public access agreements.
5. Disruptions to the pedestrian network should be minimized (e.g., curbcuts, parking access or above-ground utilities).
6. The appearance of sidewalks (scoring pattern or special paving) should be continued across driveway and alley access points.
7. Small public plazas and open spaces should have clear and legible public access and should consider social interaction and passive recreation in their design.

Public Art

8. Opportunities for public art should be incorporated throughout the pedestrian realm as part of the streetscape design. Unique public art pieces that engage the pedestrian and activate the public realm are encouraged. Potential locations are shown on Map 4.1 Public Realm.



Marda Loop development model (concept)

9. Local history interpretive elements should be incorporated throughout the Plan area through such means as plaques, public art, concrete etchings or other textural effects in public realm improvement projects. Potential locations are shown on Map 4.1 Public Realm.
10. Special public realm treatments should be employed at key entrances to the Plan area, including expanded sidewalks, public art and signage.

Street Furniture

11. A coherent and consistent design theme

specific to the Plan area should be applied to all streetscape elements along all streets.

12. Provision for banners or other signage, which distinguishes the Marda Loop area should be incorporated into the streetscape design and as part of public art.
13. The design of the public realm is encouraged to take into consideration the ability to program the space with markets, festivals or other activities. This could include infrastructure such as electric plug-ins and public washrooms.
14. The type, design, and materials of street furniture should be selected to reflect the

desired character and identity of the area. Potential street furniture types, based on recent development in the Plan area, are shown in Appendix D.

15. Priority locations for street furniture, particularly benches/public seating, include transit stops, major building entries, mixed use locations, and public plazas.
16. The use of movable outdoor furnishing should be explored to allow users the flexibility of creating their own configurations.

Street Trees

17. Where possible, street trees should be planted in continuous planting strips between the curb and sidewalk along 33 Avenue SW, the north side of 34 Avenue SW and intersecting streets. Street trees should be planted in discrete tree and planting openings, or in wells with grates.
18. Tree species and planting techniques should be selected to create a unified image for the street.

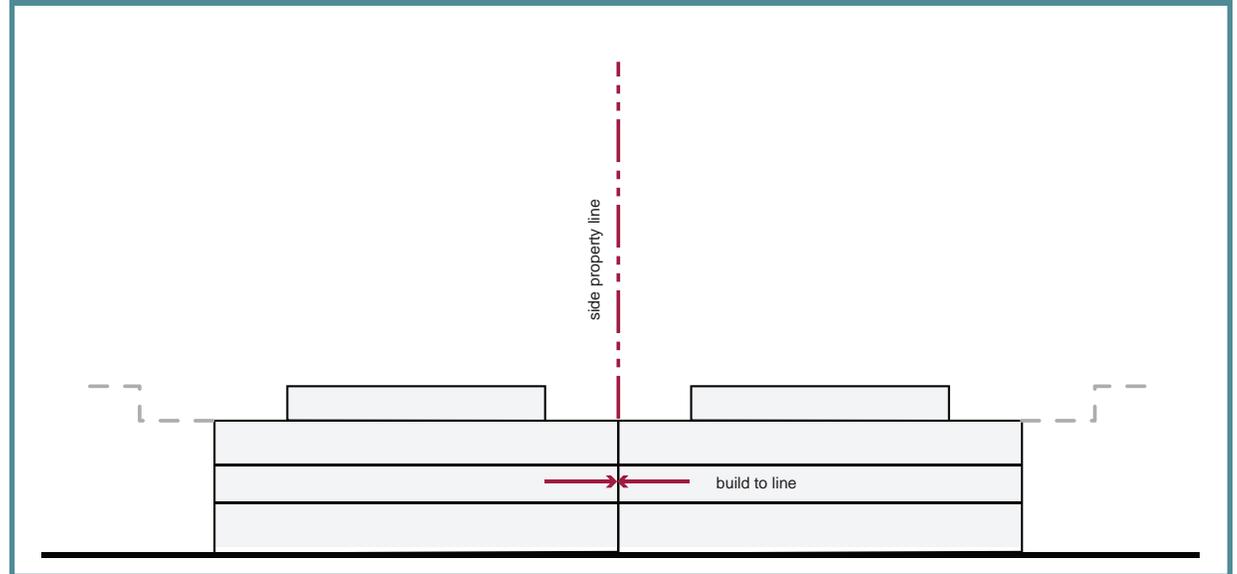
Promote and protect trees in street corridors as a means to support pedestrian and amenity areas in commercial districts, soften industrial developments and enhance the attractiveness of residential communities.

MDP 2.4.1.g

4.1.2 Building Location, Orientation & Use

1. Where possible, buildings should be built to the interior side property lines (no setbacks) to create an uninterrupted façade along the street. Interior side setbacks should not be allowed below the minimum building height of 2 storeys (Figure 4.3). Exceptions may be allowed where a development site abuts a parcel designated as residential.
2. Development of built forms and uses, such as residential units or ground floor commercial uses that wrap the building edge along the
3. Buildings with commercial uses on the ground floor should be built to the front property line or applicable setback line to create a continuous street wall along the street with direct access from the sidewalk to the ground floor use. Variations of up to 5 metres may be allowed for features such as outdoor seating.
4. Residential buildings are encouraged to have limited separation from the sidewalk such as a small landing, elevation change, porch or stoop.

Figure 4.3 Continuous Street Wall



4.1.3 Parking

Parking Access

1. In order to prevent interruptions along pedestrian linkages and conflicts between pedestrians and vehicles, wherever possible, vehicular access to on-site parking and loading facilities should be provided from rear lanes or the flanking streets and not from 33 and 34 Avenues SW.
2. For mid-block sites without rear lane access, driveway access should be shared and designed to minimize the impact on the pedestrian realm.

Driveway access to parcels fronting onto Corridors should be consolidated and new accesses minimized to provide a continuous building façade and safer pedestrian zone.

MDP 3.4.1.o

3. Parking entrances should be integrated into the building and/or landscape, and exposed walls should be architecturally treated. Good visibility should be provided for vehicles at access points to enhance the safety of pedestrians and the security of the building.

Parking Location

4. On-site parking should not be allowed between any new building and a street.
5. The majority of parking requirements are to be accommodated underground. Underground parking shall be secure, well lit and well ventilated.
6. Above grade parking structures should be screened from public streets by active uses at-grade and architectural treatments that make the parking areas indistinguishable from the rest of the building façade. They should also be screened from adjacent developments to the satisfaction of the Development Authority.
7. Where feasible, bicycle parking should be installed close to building entrances, in visible and secure locations that are protected from the elements.

Parking Infrastructure

8. Lighting for all parking areas shall be appropriate in function and scale for both pedestrian and vehicular traffic.

9. Parking infrastructure should provide electric charging to accommodate plug-in electric vehicles.
10. Developments are encouraged to include priority parking for small fuel-efficient, alternative fuel, or electric vehicles and dedication of car pooling or car co-op spaces. Parking Requirements
11. All new developments should provide for common parking and storage of bicycles.
12. Transportation Demand Management (TDM) measures such as transit incentives, parking management, van/car pool programs, car share co-ops and telecommuting are encouraged. Reductions in the required parking stalls may be considered with the adoption of proven and effective TDM measures.
13. Ground floor commercial uses that are less than 300 square meters of use area in size should have no minimum parking requirement when located in buildings that contain dwelling units, hotel or office uses above the ground floor.
14. Reduction of parking requirements will only be considered with the submission of a parking study.

4.1.4 Landscaping

1. All areas of a parcel not required for buildings, vehicular access, loading or garbage enclosures shall be landscaped.
2. Landscaping area requirements may be met through a combination of at grade and above grade landscaping.
3. Landscaping, fencing, retaining walls and the like should be integrated with the site and its architecture. A mixture of plant materials, paving elements and plant accessories should be used to create landscaping treatments unique to each property while complementing the area's aesthetic.
4. Landscaping should provide clear views from the street to pathways, open space or car parking areas.
5. Coniferous trees should be incorporated into landscape design to provide natural colour during the winter and act as a valuable windscreen.
6. Exterior planting areas should be designed to allow stormwater to collect and percolate.

4.2 Building Design

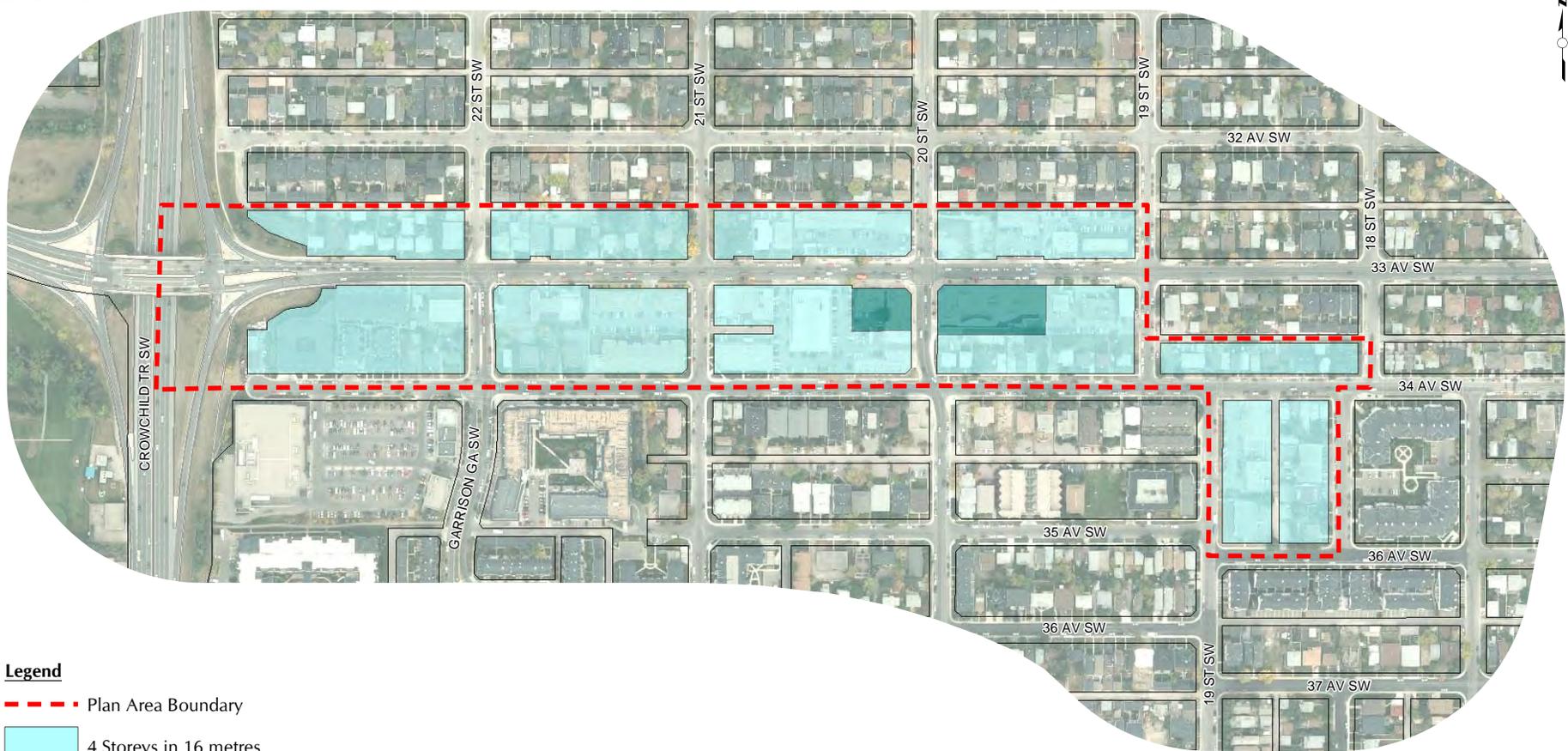
Buildings within the Plan area should help to characterize and define the street, enhance the sense of place and contribute to the high quality expected within the community. Buildings should give consideration to issues of massing, form facade articulation and location of entrances. For taller buildings the upper storeys should be pulled back from the street to minimize shadowing and building mass and to allow for views of open sky.

1. All new buildings are encouraged to achieve the highest applicable environmental standard at the time of development. A list of strategies is outlined in Appendix B.
2. New development should be designed in accordance with the principles of Crime Prevention Through Environmental Design and Emergency Services Safe Community Design.



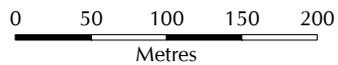
Marda Loop development model (concept)

Map 4.2 Maximum Building Heights



Legend

- Plan Area Boundary
- 4 Storeys in 16 metres
- 6 Storeys in 23 metres



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4.2.1 Building Height

The tallest building heights have been placed central to the Plan area with the lower building heights situated throughout the rest of the area. This creates new opportunities for redevelopment that reinforces the pedestrian oriented nature of these streets while providing appropriate built form transitions to the low-density, low-rise residential community.

1. New development should comply with the maximum building heights indicated in Map 4.2 Maximum Building Heights unless otherwise specified in Policy 4.2.2 and 4.2.3.
2. Several sites have been identified on Map 4.1 Public Realm as potential plaza locations based on their development potential, location at key intersections and location on the north side of the avenues. At the discretion of the Development Authority, buildings on these sites may be allowed to increase the maximum building height to 5 storeys provided a publicly accessible open space is introduced on the site.
3. For sites on the south side of 33 Avenue SW, between 21 Street SW and 19 Street SW, the Development Authority may consider an increase in the maximum building height to a maximum of 6 stories in 23 metres provided the proposed development:
3. For sites on the south side of 33 Avenue SW, and the north side of 34 Avenue SW

For Corridors that run east-west, south facing public open spaces and plazas should be incorporated in the buildings fronting the north side of the Corridor.

MDP 3.4.1.s

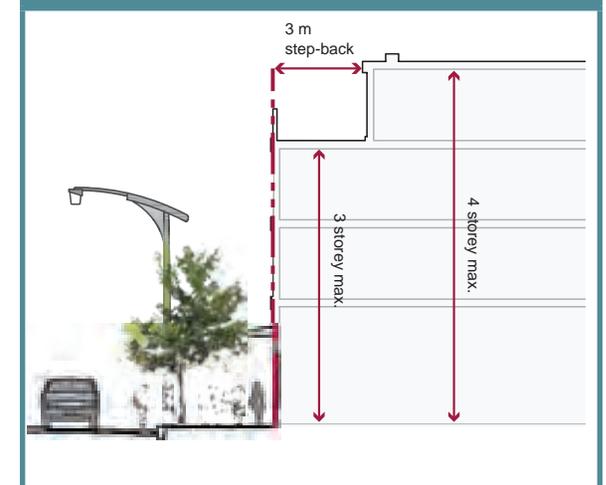
between Crowchild Trail SW and 19 Street SW, the Development Authority may consider an increase in the maximum building height provided the proposed development:

- Does not shadow the opposite sidewalk along 33 Avenue SW for a minimum of 5-hours from March 21st - September 21st.
 - Steps back the upper storeys of the building to mitigate the perception of height and create comfortable pedestrian conditions along 33 Avenue SW.
4. New buildings should have a minimum building height of 2 storeys in order to establish a consistent streetwall and ensure that new development will contribute sufficient activity to the area.

4.2.2 Building Massing & Form

1. Along the street frontages, buildings that are four storeys or greater should be stepped back to minimize massing. This step-back should be a minimum 3 metres and take place at either the first, second or third storey. Other architectural and design techniques intended to minimize the mass of the building and reduce shadowing impacts may be considered.
1. Along the street frontages, buildings that are four storeys or higher should be designed to minimize massing. A step-back or other architectural design techniques intended to minimize the mass of the building and reduce shadowing impacts should be considered.

Figure 4.4 Building Step-back



2. Where new developments share a lane with a low density residential district (Figure 4.5) the building should meet the following guidelines:

- Provide a minimum setback of 5 metres from the rear property line.
- Provide a minimum step-back of 3 metres at either the second or third storey.
- Balconies should not project beyond the building façade.

3. New development located at the intersection of two streets should provide for a minimum 5 metre corner cut setback to provide for additional space on the sidewalk and create waiting areas for pedestrians at intersections. (Figure 4.6).

4. For buildings with a frontage of over 60 metres in length along the street the overall mass of the building should be broken up with changes in width, height and finishing materials along the façade. Building façades should not exceed 15 metres in length without a change in plane or material.

Figure 4.5 Transition to Lower Density Residential

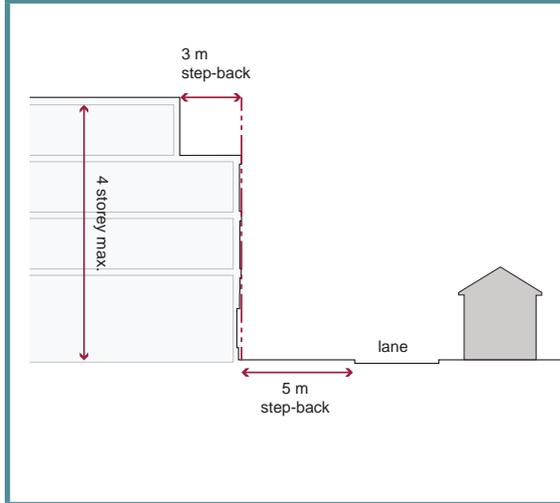
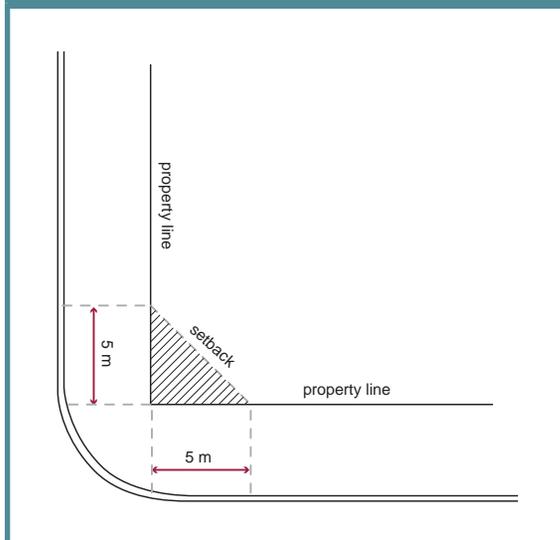


Figure 4.6 Corner Cut Setback



4.2.3 Building Character

1. Buildings located at the intersection of two streets should address both streets with architectural and/or massing elements, including entrances, windows and other façade projections and features.
2. Rear and side building elevations that are visible from public rights-of-way should be designed with details and materials similar to those of the primary elevation. However, the highest amount of articulation should be on the primary, street facing façade.
3. Blank walls facing streets are prohibited.
4. New development should be designed and built with high quality, durable, natural and recycled materials.

The ground and lower levels of developments should demonstrate a strong relationship to the human scale and contribute positively to the public realm and street.

MDP 2.4.2.b

4.2.4 Building Entry & Access

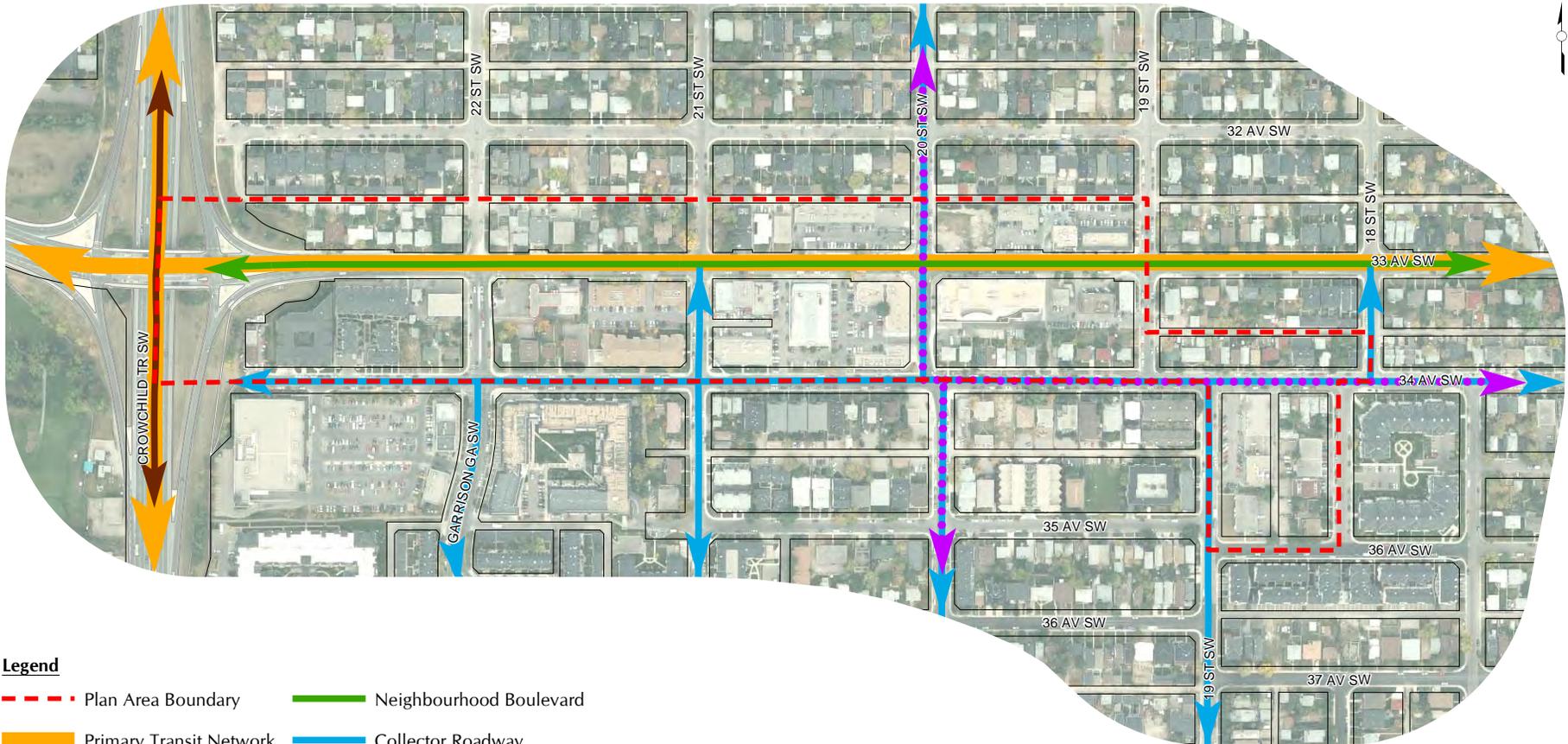
1. New buildings should be designed in accordance with The City's Access Design Standards to ensure universal access for all persons. Where feasible, buildings should be designed to eliminate the need for access ramps. Where this is not feasible, the ramps should be designed to have minimal impact on the sidewalk and should not intrude into the pedestrian throughway.
2. New development should provide entrances that are distinct, visible from the street and that emphasize a pedestrian scale.
3. Individual storefronts that are greater than 15 metres in width should provide multiple entrances at the street level, which may include incorporating separate individual retail units that have entrances oriented to the street.
4. New development should create an open and inviting ground floor by including a high level of glass and detailing to encourage pedestrian activity and bring a human scale to the street level. A minimum of 70 percent of non-residential building façades on the ground floor should have transparent glazing (doors and windows) to provide views into the interior of buildings.
5. Buildings should provide canopies or other forms of shelter for pedestrians and bicycle parking.

6. New development is encouraged to provide integrated weather protection devices or structures over the public sidewalks and transit waiting areas.
7. Buildings with residential units should be designed based on the following:
 - Ground floor residential units that front a public sidewalk or a publicly accessible private sidewalk require individual, primary entrances (e.g., front doors) providing direct access to and from the sidewalk.
 - All ground floor residential units should be designed to provide visual privacy from any public or internal sidewalk without the need for high or non-transparent privacy fences or walls that detract from the active street edge.
 - All new residential units should be provided with private outdoor amenity space, either exclusive to an individual unit or as a common amenity available to all units within a development. Common amenity space may be provided at or above grade, within courtyards, behind façades or on rooftops, terraces or ground level patios that may be completely or partially visible from the street or other buildings.

4.2.5 Signs

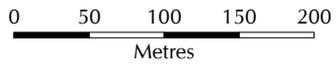
1. Signs and awnings should be designed as part of the overall design and concept of a building. Signs should be oriented to the pedestrian in terms of location and scale.
2. Signs should not obscure key architectural features, door and/or window openings.
3. Signs should be aligned with other signs within the block to maintain a consistent pattern of horizontal and vertical features.
4. Freestanding signs should not be allowed within the sidewalk area.
5. Retail frontages are encouraged to employ blade signage oriented towards pedestrians.
6. If illuminated, signs should be spotlighted, externally lit, or back lit with a diffused lighting source. Back-lighting should illuminate only the letters, characters, or graphics on the sign rather than the background of the sign.
7. Internally illuminated signs and electronic message board signs are specifically discouraged.

Map 5.1 Mobility



Legend

- - - Plan Area Boundary
- Neighbourhood Boulevard
- Primary Transit Network
- Collector Roadway
- Skeletal Road
- On Street Bikeway



Approved: 23P2013
 Amended:
 This map is conceptual only. No measurements of distances or areas should be taken from this map.

The vision of this Plan requires a balanced transportation network, providing convenient and efficient access to all modes of transportation: walking, bicycling, transit and motorized vehicles. To achieve this balance, streets and sidewalks are designed as multi-functional spaces, safe and pleasant for all users. In the Marda Loop area, the street network is a street grid with reasonable block lengths, a variety of routes throughout the area, and easy wayfinding. However, with the intent to create a network of complete streets, opportunities to further integrate streets with the pedestrian, bicycle, and transit infrastructure should be capitalized upon.

The Calgary Transportation Plan (CTP) classifies 33 Avenue SW as a Neighbourhood Boulevard. Neighbourhood Boulevards support retail and medium-density residential corridors. Pedestrians and cyclists have the highest priority. These streets are destinations, but primarily for the local communities surrounding them. A transit hub is to be created at Crowchild Trail and 33 Avenue SW when the bus rapid transit is introduced as part of the West LRT. Enhancements will improve accessibility, passenger comfort, ease of transfer and operations.

5.1 Pedestrian and Bicycle Circulation

Design streets and sidewalks to encourage pedestrian comfort, safety and linkages between neighbourhoods, open spaces and adjacent land uses.

MDP 2.4.3.a

1. Street and sidewalk design should prioritize the comfort and convenience of pedestrians and cyclists, while accommodating the needs of motorized vehicles.
2. Where on-street parking is allowed, street design should include curb bulbs to enhance pedestrian visibility.
3. Crosswalks at signalized intersections should incorporate both visual and audio cues for pedestrians and allow for adequate crossing times.

4. Marked crosswalks should be provided at all signalized intersections for all legs of the intersection.
5. The use of crosswalk materials that are significantly different in color, texture and design is encouraged.
6. 20 Street SW is a designated on-street bikeway. Future improvements to the street network should consider cycling conditions and connectivity through the area.
7. Bicycle detection should be provided at the intersection of 20 Street SW and 33 Avenue SW to ensure the signal operation accommodates cyclists.
8. Wayfinding to transit stops and key pedestrian and bicycle routes should be incorporated into the Plan area.

5.2 Vehicle Circulation

Maintain automobile, commercial goods and emergency vehicle mobility in Calgary while placing increased emphasis on sustainable modes of transportation (walking, cycling and transit).

MDP 2.5.1

1. All street designs should align with the Complete Streets objectives, guidelines and principles.
2. Permanent partial or full street closures by new buildings, utilities, ramps, or transportation improvements should not be allowed. Where a street closure to vehicular traffic may be essential, access for pedestrians and bicycles should be maintained.
3. Rear lanes should be maintained throughout the Plan area. For comprehensive developments that encompass an entire block, rear access may be provided through private internal vehicle access routes.
4. The introduction of rear lanes where they currently do not exist should be considered with new development.
5. On-street parking is encouraged on all local streets. On-street parking should focus on serving visitors and customers to the area who have short-stay parking and loading needs. Long-stay on-street parking is discouraged.
6. Consideration should be given to slowing vehicular traffic in the area by narrowing travel lanes, incorporating curb extensions, providing on-street parking and/or street trees, slower speed limits and could include a variation in paving materials with well lit pedestrian crossings.
7. Transit priority measures should be provided to enhance service reliability, support the primary transit network and minimize delay for transit service in and out of the Plan area. These measures can include:
 - Transit priority at traffic signals
 - Queue jumps and passbys
 - Transit only connections
8. Transit stops should be upgraded when opportunities arise through streetscape redevelopment. This could include improvements such as trees, shelters, seating areas, lighting and passenger information systems.

6.0

Implementation

6.1 Authority of the Plan

6.1.1 Interpretation Of Map Boundaries

The boundaries separating different types of land uses in Map 3.1 and the boundaries separating different maximum building heights in Map 4.2 are intended to be conceptual only. The precise location of these boundaries will be determined by City Administration at the time of a land use or development permit application.

6.1.2 Policy Interpretation

Where “shall” is used in a policy, the policy is considered mandatory. However, where actual quantities or numerical standards are contained within a mandatory policy, the quantities or standards may be deviated from provided that the deviation is necessary to address unique circumstances that will otherwise render compliance impractical or impossible, and the intent of the policy is still achieved.

Where “should” is used in a policy, the intent is that the policy is to be complied with. However, the policy may be deviated from in a specific situation where the deviation is necessary to address

unique circumstances that will otherwise render compliance impractical or impossible or to allow an acceptable alternate means to achieve the general intent of the policy to be introduced.

Where a policy requires compliance at the Outline Plan/Land Use Amendment stage, that requirement may be deferred to the Subdivision Approval or Development Permit Approval stage without requiring an amendment to the Plan.

6.1.3 Interpretation Of Specific Versus General Language

The Plan uses language that is both general and very specific in nature. Where general direction is given, flexibility should be used in the interpretation of the Plan. Where specific language is used, it is meant to give clear and unambiguous direction to both the Development Authority and the development industry.

6.1.4 Non-Statutory Components Of The Plan

All Appendices attached to the Plan are to be used as supporting information only and do not form part of the statutory Area Redevelopment Plan.

Policies from the Municipal Development Plan (MDP) have been provided throughout the Plan. These are intended to support the policies of the Plan and reference should be made to the MDP to determine current MDP policies.

6.1.5 Land Use Approvals And Suitability Of Specific Sites

Area Redevelopment Plans are long-term planning documents by nature. As such, they promote a vision for a community and put in place policies and guidelines that work toward achieving that vision over time. Policies and guidelines in an ARP are not to be interpreted as an approval for a use on a specific site, as the policies do not address the specific situation or condition of each site within the Plan area. In that regard, no representation is made herein that any particular site is suitable for a particular purpose as site conditions or constraints, including environmental contamination, must be assessed on a case-by-case basis as part of an application for Land Use Amendment, Subdivision or Development Permit approval.

6.1.6 Amendment of the Plan

There will always be new concepts and ideas that arise that may be constrained by or contradictory to certain policies that are not anticipated by

this Plan. Where such new concepts and ideas respond to and meet the intent of the Vision and Guiding Principles or offer a creative solution to a particular problem, efforts shall be made to find ways to allow for their implementation, including, where necessary, amendments to the Plan. To make any change to the text or maps within the Plan, an amendment to the Plan that includes a Public Hearing of Council shall be required in accordance with the MGA.

Where an amendment to the Plan is requested, the applicant shall submit the supporting information necessary to evaluate and justify the potential amendment and ensure its consistency with the MDP and other relevant policy documents.

6.2 Future Projects

Approval of this Plan is simply the first step in realizing the potential for Marda Loop. The most important step will be its implementation. A number of initiatives, studies, investments and other actions may be needed as Marda Loop continues its development into a mixed-use, pedestrian friendly area. Careful planning and approval of privately and publicly led development will also be fundamental in the success of Marda Loop. Preliminary assessment of capacity of existing utilities is underway. Potentially, existing water, wastewater and storm infrastructure may need to be upgraded to accommodate re-development.

6.2.1 Public Realm

To meet the retail vitality and pedestrian-friendly design objectives of this Plan a comprehensive streetscape design concept for 33 and 34 Avenues SW and intersecting side streets (18 Street SW, 19 Street SW, 20 Street SW, 21 Street SW and 22 Street SW) should be undertaken. This design concept could include the following:

- Thorough review and assessment of the existing condition for the above mentioned avenues and the intersecting streets
- Transportation analysis of the current condition with regards to pedestrian, vehicular, bicycle and transit movement
- Public realm concepts in consultation with the community and businesses
- Street furniture handbook that will define style, design, colour and character of all the elements of the street furniture for example (benches, litter and recycling bins, pedestrian lights, street lights, trees, public art, surface material)
- Phasing plan to define priority areas for implementation

Detailed design drawings, based on the streetscape design concept, will be required at the implementation stage

6.3 Financing the Public Realm

Creating a high-quality mixed-use corridor requires appropriate development on individual sites and in the public realm. Financing the range and quality of public realm improvements required to implement the vision within the Plan area requires a partnership between The City, private development and local business interests.

In achieving such a partnership, The City should:

- Establish clear expectations with respect to the responsibilities of individual developments for upgrading of adjacent public rights-of-way.
- Establish an equitable method to enable all new development to contribute to public realm improvements that serve the entire area.

6.3.1 Publicly Funded Improvements

The South Calgary and Altadore communities have experienced significant growth and change in recent years. As the main mixed-use and commercial area serving these communities, the redevelopment of the Marda Loop area in the manner proposed in this Plan contributes to the achievement of Council's sustainability principles and approved policies and is serving a greater

public interest. City Council may be requested in future capital budgets to allocate funds for projects such as:

- Funding part of the cost of burying the overhead power lines along 34 Avenue SW. This improvement is considered highly desirable in changing the character of this area into a pedestrian friendly environment.
- The redesign and construction of the improved pedestrian environment of 33 and 34 Avenue SW.
- Improvements to major water, wastewater and storm infrastructure.

6.3.2 Development Funded Improvements

Lands within the Marda Loop area will benefit from enhanced development opportunities and an improved physical environment. Therefore, such development should contribute to the creation of this environment.

Subject to the provisions of the Municipal Government Act, new developments may be required to contribute to the creation of a high-quality public realm in the following ways:

- Dedication of New Public Rights-of-Way: All new development located adjacent to existing public rights-of way, regardless of density, may be required, where the legal authority

exists, to dedicate and construct new public rights-of-way adjacent to the development, incorporating the appropriate sidewalk, landscaping, street furniture and lighting.

- Upgrading of Adjacent Public Rights-of-Way: All new development regardless of density will be required to reconstruct public rights-of-way adjacent to the development, incorporating the appropriate sidewalk, landscaping street furniture and lighting.
- Provision of Land for Public Open Spaces: New development may be required to provide land for public open space in accordance with this Plan. This includes the dedication of Municipal Reserve where the opportunity exists.
- Utility Obligations: All new development will be responsible for all local underground utilities connections necessary to service the development, including any studies required by The City.
- Improvements to local water, wastewater and storm infrastructure.

6.4 Approval Processes

6.4.1 Review of Development Applications

1. All development applications within the Marda Loop ARP boundaries are subject to the provisions of this Plan.
2. In reviewing development permits, the Development Authority should:
 - consider the context of the ultimate development concept as identified in the Plan.
 - have regard to possible impacts on the existing development with respect to its ongoing operation and functioning until such time as it is replaced with new development consistent with the Plan.
3. Design guidelines for signage should be submitted in conjunction with a Development Permit Application for a development that will comprise part of a retail / commercial street or for any development proposing signage on a street that interfaces with residential development.
4. The evaluation of Development Permit Applications should have particular regard to the policies contained in Section 3.0, Section 4.0 and Section 5.0.

5. As a way of ensuring that the requirements of this Plan can be effectively applied, at the discretion of the Development Authority, significant projects may be reviewed by the Calgary Planning Commission and the Urban Design Review Panel.
6. All applications shall be circulated to the Richmond and Marda Loop Communities Associations as well as the Marda Loop Business Revitalization Zone.

6.4.2 Land Use Amendments

1. Land use redesignations are to be in conformance with the Plan. Where feasible, land use districts established in the Land Use Bylaw will be used to implement the policies set out in this Plan.
2. The exact land use district boundaries will be determined at the land use redesignation stage, using the land use policy area boundaries on Map 3.1 Land Use Policy Areas as a guide.

6.5 Affordable Housing

In support of the City's objectives to create more choices for housing – City Departments, including Corporate Properties & Buildings, the Calgary Housing Company and relevant social agencies – should work together to explore the opportunity of providing affordable housing where feasible.

Partnerships between The City and non-profit housing providers and / or the private sector to develop affordable housing are encouraged.

In order to facilitate the delivery of affordable housing within the Plan area, the Development Authority is encouraged to consider the following:

1. Support relaxation of bylaw regulations where it is demonstrated that the relaxation is appropriate for the development and that the development is secured through an agreement to ensure long-term affordability for low-income households.
2. Parking relaxations for proposed affordable housing development may be considered, where it is demonstrated that the proposed development would have a reduced automobile ownership rate and that the development is secured through an agreement to ensure long-term use for low income households.

6.6 Plan Monitoring

The policies of this Plan will be monitored for their effectiveness and adaptation to changing conditions. Amendments arising from the monitoring program will be brought forward for City Council consideration where appropriate.

APPENDICES

A

Appendix A: Planned Development Capacity

The Municipal Development Plan, which classifies Marda Loop as a Neighbourhood Corridor, sets a minimum intensity threshold for the area of 100 jobs and population per hectare. At this intensity, high-frequency transit service and mixed-use developments become cost-effective for landowners, tenants, and The City as a service provider.

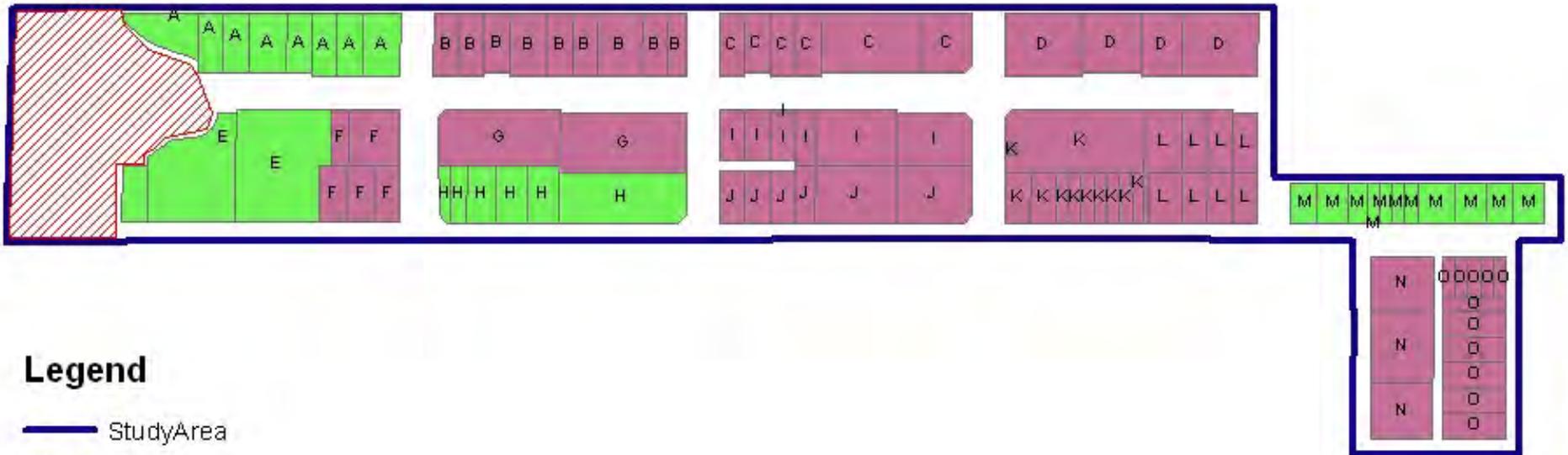
The Land Use Policy Areas and Building Heights maps outline general areas where specific assumptions may be applied. These assumptions are defined in the User Guide to the Municipal Development Plan & Calgary Transportation Plan and are meant to provide consistency in how the targets of the MDP and CTP are interpreted.

Presently, the Marda Loop ARP area is home to approximately 420 residents and an estimated 680 jobs. At 11.3 gross hectares, the Plan area has an intensity of 98 jobs and population per hectare. As this is not significantly different from the MDP intensity target of 100, the policies of this plan are designed to encourage moderate intensification to enhance the area's existing vibrancy.

This appendix contains projections for the area's future population and employment based on a floor area ratio (FAR) assumed through the development of the Plan. **The numbers used are the product of multiple layers of assumptions and should be used for illustrative purposes only.** At FARs between 2.5 and 3, the Marda Loop ARP area can accommodate a total of 1,450 residents and 1,620 jobs. This represents an increase in intensity to a total of 273 jobs and population per gross hectare.

This development scenario assumes a changeover of the area's building stock over the 20-30 year lifetime of the plan. Due to recent building activity, it is unlikely that all of the area will be redeveloped in such a time frame. For this reason, the projections given here can be viewed as the **theoretical** maximum permitted for each block rather than the minimum expected across the entire study area.

Map A.1 – Development Parcels



Legend

— StudyArea

Inventory

■ C/MU

■ R/R

LAND USE POLICY AREA

- Residential/Retail
- Commercial Mixed Use

Table A.1 –MARDA LOOP ARP: PROPOSED BUILD-OUT SCENARIO

Block	Site Area (M2)	FAR	Gross Floor Area	Net Floor Area	Residential Floor Area	Commercial Floor Area	Units	Residents	Jobs	Work at Home	Total Population & Jobs	Net Intensity (P&J/H)	Gross Intensity (P&J/H)
A	5,357	2.5	13,391	11,383	8,537	2,846	85	134	63	5	202	378	N/A
B	5,769	2.5	14,422	12,258	6,129	6,129	62	96	136	4	236	409	N/A
C	5,571	2.5	13,927	11,838	5,919	5,919	59	93	132	4	228	409	N/A
D	5,719	2.5	14,299	12,154	6,077	6,077	61	95	135	4	234	409	N/A
E	7,004	2.5	17,510	14,884	11,163	3,721	112	175	83	7	265	378	N/A
F	3,078	2.5	7,695	6,541	3,270	3,270	33	51	73	2	126	409	N/A
G	5,190	2.5	12,976	11,030	5,515	5,515	55	87	123	3	212	409	N/A
H	4,751	2.5	11,878	10,097	7,572	2,524	76	119	56	5	179	378	N/A
I	4,920	3.0	14,761	12,546	6,273	6,273	63	98	139	4	242	491	N/A
J	5,049	3.0	15,147	12,875	6,438	6,438	64	101	143	4	248	491	N/A
K	5,695	2.5	14,239	6,051	6,051	6,051	61	95	134	4	233	409	N/A
L	4,728	2.5	11,820	5,024	5,024	5,024	50	79	112	3	194	409	N/A
M	3,664	2.5	9,161	7,787	5,840	5,840	58	92	43	3	138	378	N/A
N	4,137	2.5	10,342	8,791	4,395	4,395	44	69	98	3	169	409	N/A
O	4,145	2.5	10,363	8,808	4,404	4,404	44	69	98	3	170	409	N/A
	74, 778		191,930	163,140	92,608	70,533	926	1,454	1,567	55	3,077	411	273

Plan Gross Developable Hectares: 11.25 ha

Assumptions:

- Residential/Retail Area = 25% commercial floor space, 75% residential
- Commercial Mixed Use Area = 50% commercial floor space, 50% residential
- Residential Units = 100 m2 of floor space
- Occupancy Rate = 1.57 persons per unit
- Commercial Jobs = 45 m2 of floor space per employee
- Work at Home = 3.8% of residents estimated to also work at home

	Avg. FAR	Units	Residents	Jobs (est.)	Pop.	Gross Intensity
Current	0.7	350	420	680	1,100	98
Maximum	2.6	930	1,450	1,620	3,070	273

B

Appendix B: Environmental Sustainability Strategies

The Approving Authority should work with developers to incorporate the following sustainable neighbourhood and building practices:

1. Optimizing solar gain and reducing energy demand by considering solar layout of buildings and streets as well as how building heights may permit/block solar penetration.
2. Supplying energy from local sources to minimize distribution losses e.g. by incorporating district energy and combined heat and power.
3. Using renewable energy and low-carbon sources (e.g. ground and air source heat pumps, geothermal, solarthermal, photovoltaic, hydroelectric, wind turbines, biomass, energy from waste).
4. Including long-term energy metering and monitoring plans as part of multi-residential, mixed use or commercial/retail development proposals.
5. Minimizing water demand through the use of efficient water fittings (e.g. low flow, dual flush household goods, conservation landscaping or xeriscaping).

6. Reduce demand for high quality drinking water by matching water quality to use through rainwater harvesting and stormwater re-use to meet irrigation needs, and through the appropriate use of reclaimed water.

7. Incorporation of green roofs and living walls.

8. Reducing impervious surfaces associated with development to improve water quality and reduce runoff volume. This may be accomplished by applying Low Impact Development (LID) stormwater management practices. Stormwater management practices alongside landscaping that uses native vegetation with low-water requirements will make landscaping more resilient to drought while reducing demand on the treated water supply.

9. Integrating indigenous planting and biodiversity of material within landscaping, streetscaping and public spaces.

10. Provide building recycling facilities on-site and space to accommodate composting facilities.

11. Prioritize conservation, re-use, and recycling as well as the use of natural, healthy, and local materials in the construction and operational phases of community redevelopment.

12. Minimizing waste production in both construction and operation through the use of site waste management plans, centralised materials handling, modern methods of construction, the use of re-used and recycled material in construction, production of a building lifecycle strategy and deconstruction plan and the provision of space and facilities for recycling and composting.

13. Encouraging LEED certification or constructing to highest sustainable building standards possible for all new and renovated buildings.



Appendix C:

Planning History & Engagement

C.1 Planning History

Marda Loop is located on land that was annexed to Calgary in 1910 and developed as one of Calgary's first suburbs. Subdivisions have occurred from the time of the annexation to the end of the 1950s when the majority of the residential development occurred in the area. The Marda Loop area is named for two significant historic community landmarks that were established in the area: a trolley route that ran south to 33 Avenue and 20 Street "looping" back to the City Centre and the Marda Theatre started by Mark and Mada Jenkins (Marda is a combination of the founders first names).

The Marda Loop area falls within the boundary of both the South Calgary and Richmond communities. Prior to the adoption of the Marda Loop ARP, the Plan area was previously included within the boundaries of both the South Calgary/Altadore ARP and the Richmond ARP, both of which were approved by Council in 1986. The revitalization and future development of a "commercial centre" in the 33/34 Avenue SW area was encouraged within both of the ARPs. This centre was identified as a commercial focal point for both the Richmond and South Calgary/Altadore communities as well as for the regional traffic on 33 Avenue SW.

C.2 Community Engagement

The development of the Marda Loop ARP was informed by extensive public consultation. An Advisory Group was established and consisted of members of the South Calgary and Richmond Community Associations, as well as the Marda Loop Business Revitalization Zone. The Advisory Group was consulted regularly throughout the development of this Plan and provided considerable input into its direction and policies.

The general public was also consulted through several Open Houses. At the Open Houses, attendees were given the opportunity to review concepts and policies under development and to provide feedback verbally. The public also provided feedback through questionnaires available at the Open Houses and online. The questionnaires were used to further guide the project and to inform specific policies.

D Appendix D: Potential Street Furniture

Street Furniture

Backed Bench with Side and Centre Arms

SPECIFICATIONS

Manufacturer: MAGLIN

Model: MLB700AW

- 70" length (177.8 cm)
- solid cast aluminum ends with ipe wood slats
- centre arm (not shown)
- surface mounted

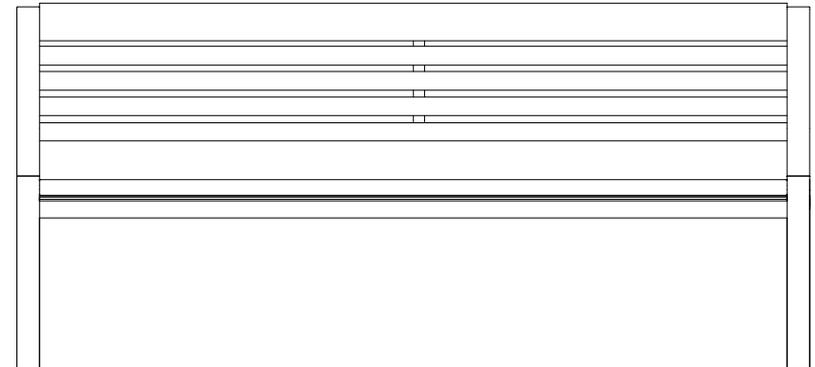
Color: Graphite Powdercoating

Supplier: Maglin

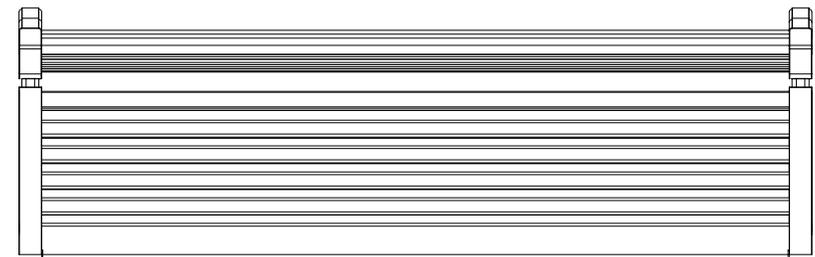
Phone – (888) 271 – 8666



As a follow up to a transportation study undertaken by City of Calgary Roads Department in 2005, The City engaged a consultant to prepare a Handbook of Streetscape Improvements for Marda Loop. Elements from the Handbook are included within this Appendix in order to provide consistency of the street furniture standards to be considered when reviewing redevelopment applications within the Plan area. These furniture standards maybe reviewed and amended from time to time in consultation with the businesses and the South Calgary/Altadore and Richmond communities.



FRONT VIEW



TOP VIEW

Street Furniture

Backless Bench without Arms

SPECIFICATIONS

Manufacturer: MAGLIN

Model: MLB700BW

- 70" length (177.8 cm)
- solid cast aluminum ends with ipe wood slats
- surface mounted

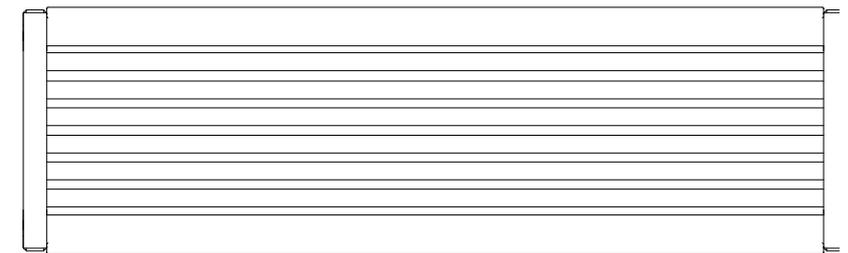
Color: Graphite Powdercoating

Supplier: Maglin

Phone – (888) 271 – 8666



FRONT VIEW



TOP VIEW



IMAGES

Street Furniture

Trash Receptacle

SPECIFICATIONS

Manufacturer: MAGLIN

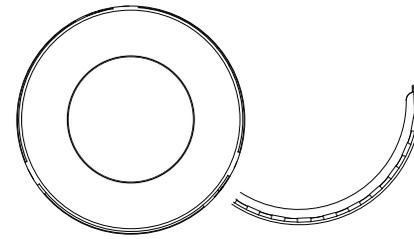
Model: MLWR250-32SO

Color:

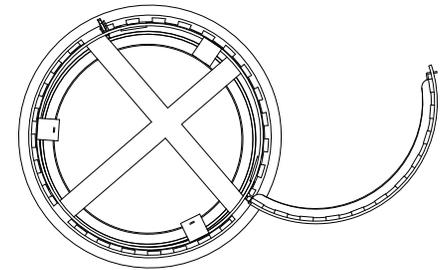
Supplier: Maglin

- OUTSIDE EDGE OF TRASH RECEPTACLE TO BE 500mm FROM OUTSIDE EDGE OF PLANTER CURB (817.50mm FROM CENTRE OF TRASH RECEPTACLE)
- CENTRE BENCH ON WIDTH OF PLANTER CURB

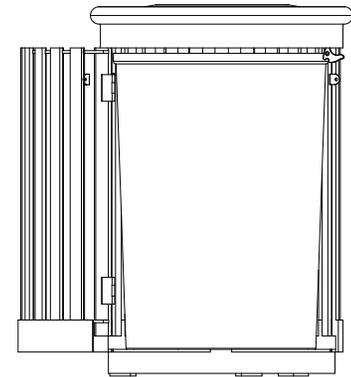
- OUTSIDE EDGE OF TRASH RECEPTACLE TO BE 500mm FROM FACADE WHERE APPROPRIATE (817.50mm FROM CENTRE OF TRASH RECEPTACLE TO FACADE)
- ENSURE MINIMUM CLEARANCE OF 1750mm FOR SIDEWALK CIRCULATION IS MET



TOP VIEW



BOTTOM VIEW



FRONT VIEW



Street Furniture

Bike Rack

SPECIFICATIONS

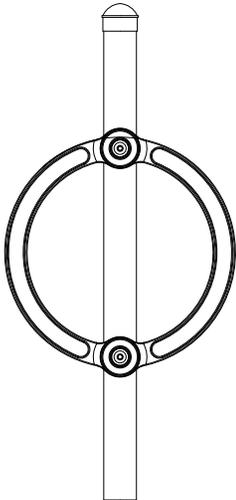
Manufacturer: **MAGLIN**

Model: **MBR200**

- 36" high (91.44 cm)
- 16.75"Ø (42.55 cm) ring
- galvanized H.S. steel tube and aluminum casting
- surface mounted

Color: Graphite Powdercoating

Supplier: **Maglin**
Phone – (888) 271 – 8666



FRONT VIEW



SIDE VIEW



TOP VIEW

LAYOUT OPTIONS



Street light standard to be inserted



THE CITY OF
CALGARY
LAND USE PLANNING & POLICY