

Background and Planning Evaluation

Background and Site Context

The subject site is in the southeast developing community of Belvedere. The site is within the southwestern portion of the *Belvedere Area Structure Plan* (ASP), adjacent to Stoney Trail SE. Residential development is underway to the east across 84 Street SE, and a stormwater management pond system exists to the south. Stoney Trail SE borders the site's western side, and the East Hills shopping centre is located directly north, across 17 Avenue SE.

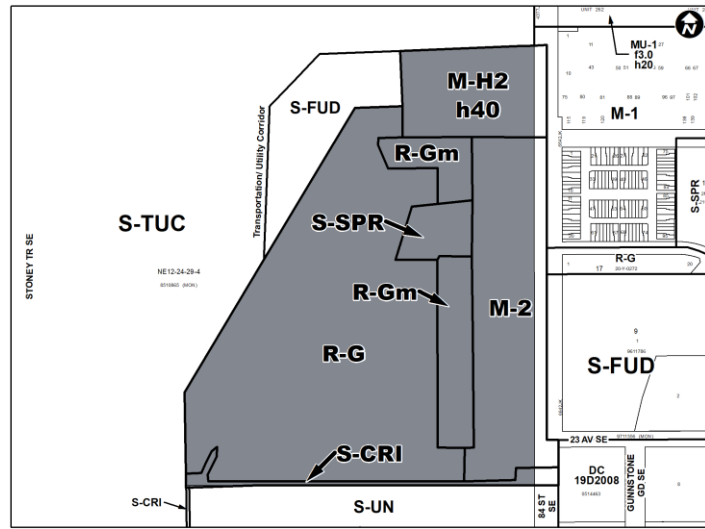
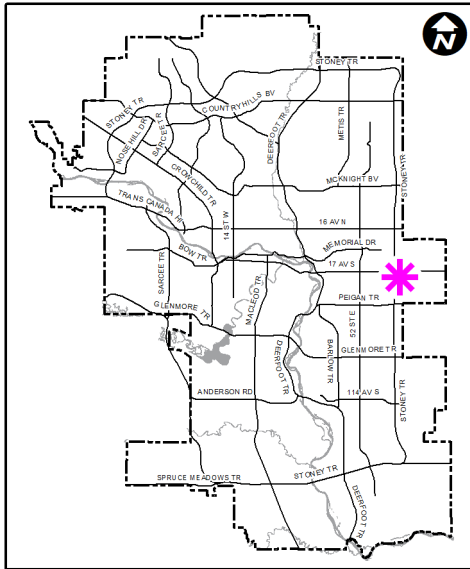
The subject site is approximately 18.53 hectares (45.79 acres) in size with an irregular shape. The site is undeveloped and was historically used for agricultural purposes. The site topography is relatively flat and has been graded in anticipation of future development. Access to this site is only available from 84 Street SE on the eastern boundary of the plan area. The all-turns access to 17 Avenue SE north of the site will eventually be removed and access will primarily be via 19 Avenue SE and 21 Avenue SE to the east.

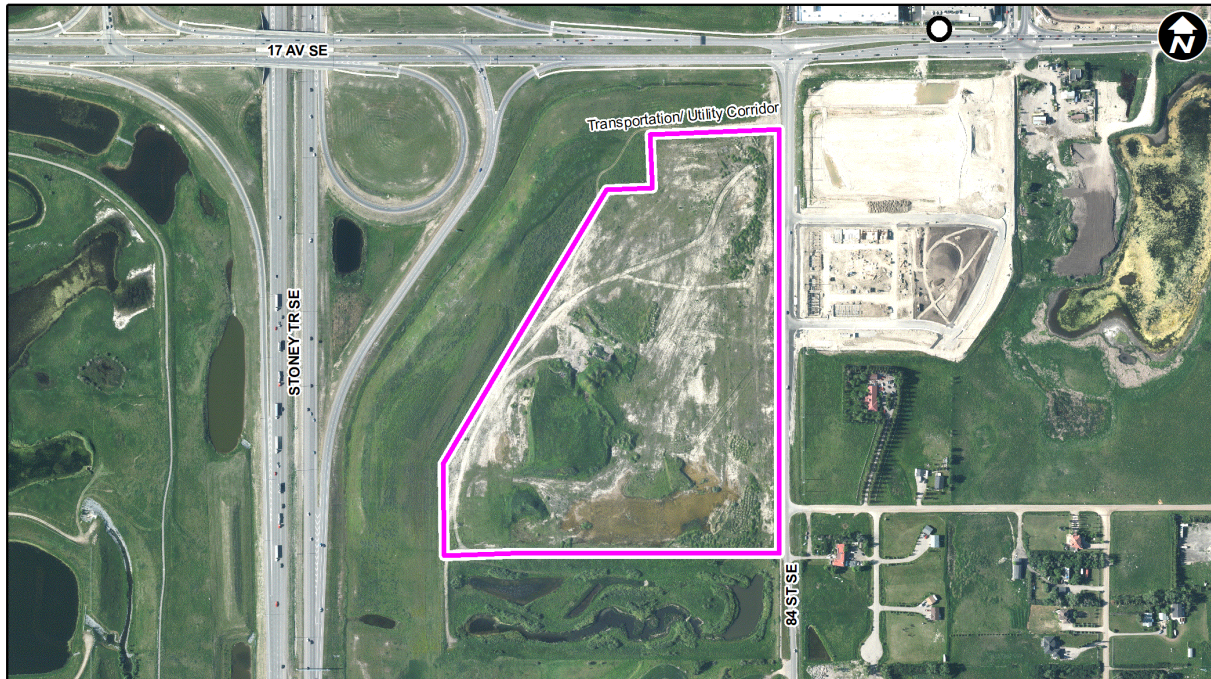
This proposed policy amendments, outline plan and land use amendment provide a logical extension and connection to the street and block pattern planned for in adjacent neighbourhood areas.

Community Peak Population Table

No data is available for Belvedere in the 2019 City of Calgary Civic Census since there was no significant residential development in the area prior to that time. While there has been substantial growth in Belvedere in recent years, no population data is available at this time.

Location





Previous Council Direction

None.

Planning Evaluation

Land Use

The existing land use is a Direct Control (DC) District [24D2010](#) based on the Commercial – Regional 3 (C-R3) District, which is intended to accommodate regionally oriented, large retail commercial developments where uses are typically in multiple buildings, distributed over multiple sites.

This application proposes several residential and special purpose districts:

- Residential – Low Density Mixed Housing (R-G and R-Gm) District;
- Multi-Residential – Medium Profile (M-2) District;
- Multi-Residential – High Density Medium Rise (M-H2h40) District;
- Special Purpose – City and Regional Infrastructure (S-CRI) District; and
- Special Purpose – School, Park, and Community Reserve (S-SPR) District.

The proposed R-G and R-Gm Districts are intended to support a variety of low-density residential building forms including single and semi-detached dwellings, duplex dwellings, and rowhouse buildings, along with secondary suites and backyard suites. The maximum building height in these Districts is 12.0 metres. R-Gm differs from R-G in that R-Gm designated lands are not intended to accommodate single detached dwellings except where subdivision results in remnant single lots. R-G District sites comprise 7.44 hectares \pm (18.38 acres \pm) and R-Gm District sites comprise 1.02 hectares \pm (2.52 acres \pm) of the proposed redesignation area.

The proposed M-2 District allows multi-residential development of medium height and density and it is intended to be near or adjacent to low density residential development. The M-2 District allows for a maximum building height of 16.0 metres (approximately five storeys). The maximum floor area ratio (FAR) is 3.0. The M-2 District has a minimum density of 60 units per hectare and no maximum density (limits on height and massing restrict potential density). The M-2 District sites comprise 2.59 hectares \pm (6.40 acres \pm) of the proposed redesignation area.

The M-H2 District is intended for multi-residential buildings (e.g., apartment buildings) that may have a limited range of commercial storefronts. This application proposes a height modifier of 40 metres, or a maximum height of about 10 to 12 storeys. This application proposes one, 1.74 hectare (4.30 acre) M-H2 site at the northeastern corner of the site, at the intersection of 17 Avenue SE and 84 Street SE.

The proposed S-CRI District is intended to provide for city and regional infrastructure necessary for the proper servicing of the development. This district is proposed for the lands in the southern portion of the plan area that contain public utilities and utility rights-of-way. The sites will be designated as Public Utility Lots (PUL) pursuant to the *Municipal Government Act* (MGA). S-CRI District sites comprise 0.37 hectares \pm (0.91 acres \pm) of the proposed redesignation area.

The proposed S-SPR District is intended to provide for schools, parks, open space and recreational facilities, with parcels of varying sizes and use intensities. This District is only applied to lands that will be dedicated as School Reserve or other forms of Municipal Reserve (MR) pursuant to the MGA. S-SPR District sites comprise 0.40 hectares \pm (1.00 acres \pm).

Development and Site Design

The design of the proposed outline plan responds to the context and characteristics of the site. Given the presence of 17 Avenue SE and the future Bus Rapid Transit (BRT) station directly northeast of the subject site, the highest intensity multi-residential uses, which may have commercial storefronts, have been strategically located at the northeastern corner of the site, with density and intensity decreasing the further the land is from the BRT. To the east, residential development is underway and exists at varying stages of completion. Other features of the proposal include:

- Medium density multi-residential development along old 84 Street SE near the future transit amenities;
- Low density residential in the form of townhouses, rowhouses, semi-detached, and single detached homes with a variety of front drive and laned housing forms;
- A centrally located public park space; and
- A block-based grid street network that fosters strong pedestrian and cycling connectivity.

Open Space

The 10% MR dedication was previously provided as a payment of cash-in-lieu of reserves for these lands with subdivision application SB2010-0462 when the parcel was created. However, a caveat on the parcel's title requires the provision of 0.40 hectares (1.0 acre) of open space with the development of the site. This application proposes one MR parcel that satisfies this requirement, located in the north-central portion of the plan area, close to the largest expected population densities. This park space is expected to contain a playground, walking paths, seating opportunities and passive recreation spaces.

Pathways

An existing regional pathway exists along the west side of 84 Street SE. A new 3.0 metre local pathway is proposed to enter the site at the extension of 19 Avenue SE and continue south, to connect with a proposed local pathway that will run along the southern boundary of the site, adjacent to the stormwater ponds further south.

Density and Intensity

At build-out, the proposed plan area is expected to have an anticipated 1,268 units (see Proposed Outline Plan Data Sheet, Attachment 7). The proposed development is anticipated to achieve a residential density of 68.42 units per hectare (27.69 units per acre). The anticipated population and jobs of the proposed development is 3,265 people and 124 jobs and therefore the anticipated intensity is 182.89 people and jobs per gross developable hectare.

The *Municipal Development Plan* (MDP) sets out minimum density and intensity targets for new communities at a density of 20 units per gross developable hectare (8 units per acre) and an intensity of 60 people and/or jobs per gross developable hectare. The *Belvedere ASP* sets out the same density and intensity targets as the MDP. Based on the anticipated residential density of 68.42 units per hectare and the anticipated intensity of 182.89 people and jobs per gross developable hectare, the proposed development exceeds the targets of both the MDP and the ASP.

Transportation

The subject site is bounded by the Transportation Utility Corridor (TUC), Stoney Trail SE, to the west, 17 Avenue SE to the north and 84 Street SE to the east. Lands to the south are currently developed as a stormwater management facility. Vehicular access to both Stoney Trail SE and Peigan Trail SE is currently available via 84 Street SE, providing excellent regional connectivity for the plan area.

A Local Transportation Impact Assessment (TIA) was submitted to establish internal street classifications and intersection configurations for the plan area as part of the outline plan process. The Local TIA was reviewed and accepted by Administration. The Local TIA identified that the main access to and from the plan area, 84 Street SE, will operate in both an interim condition and an ultimate condition. In the interim scenario, 84 Street SE will operate in its existing condition; a four lane Arterial Street with signalized access to 17 Avenue SE. In the ultimate scenario, the signalized all-turns access at the intersection of 84 Street SE and 17 Avenue SE will be closed and converted to a right-in, right-out only access. When the closure occurs, traffic will be re-routed via a re-aligned 84 Street SE to the east and connections to the plan area will ultimately be provided via 21 Avenue SE, 19 Avenue SE and a new access south of the plan area. The future re-alignment of 84 Street SE is currently partially constructed.

The proposed active transportation network includes regional pathways and local multi-use pathways which allow for excellent bicycle and pedestrian connectivity. Customized street cross-sections have been proposed to replace sidewalks with multi-use pathways, as shown on the outline plan in Attachment 5. Portions of the plan area are located in the Transit Oriented Development area due to the proximity to the future Max Purple BRT Station. The future BRT Station is envisioned on 17 Avenue SE, at the East Hills Shopping Center, approximately 500 metres northeast of the plan area. Future transit service for the community will be provided along the new, re-aligned 84 Street SE connecting transit riders to the future BRT Station.

Utilities & Servicing

The plan area had been previously approved for a commercial land use, therefore, in conjunction with other commercial development in the area, servicing has already been constructed to accommodate development of this site. The Developer will be responsible to construct on-site main extensions to connect to the adjacent public utilities.

Water Infrastructure

Water is available to service the site along the east and south boundaries of the plan area. Multiple connections to the existing public mains will be provided by the Developer to create a looped system throughout the proposed plan area.

Sanitary Infrastructure

Sanitary servicing can be provided through a connection to the existing Sanitary Trunk Sewer located within 84 Street SE. The existing main has been previously sized to accommodate development of this site.

Storm Infrastructure

Storm servicing is available from a pre-serviced connection in the southwest corner which drains into the East Hill constructed stormwater wetland adjacent to the site to the south. The existing pipe and the wetland were previously designed and constructed to accommodate the development of this site. The site is within the Forest Lawn Creek catchment area.

Fire Infrastructure

In order to support additional growth in the area and improve response times, budget has been approved to construct a fire station to the north of the site in the East Hills Shopping Centre that would service the plan area. The new station is likely to be operational in time to support this development, however fire protection can be provided for the interim from existing stations nearby if needed.

Environmental Site Considerations

No significant concerns were identified through the Environmental Site Assessments. This is a greenfield site which has, until recent years, remained in a natural condition with limited agricultural uses in the past. The existing site conditions are suitable for the proposed uses.

Legislation and Policy

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered, and is aligned with, the policy direction of the [South Saskatchewan Regional Plan](#) (SSRP) which directs population growth in the region to cities and towns, and promotes the efficient use of land.

Growth Plan (2022)

The recommendation aligns with the policy direction of the Calgary Metropolitan Region Board's [Growth Plan](#) (GP). The proposed land use amendment builds on the principles of the GP by promoting efficient use of land and regional infrastructure, and establishing strong, sustainable communities.

Rocky View County/City of Calgary Intermunicipal Development Plan (Statutory – 2012)

The site is within the Policy Area on Map 1: Plan Area of the [Rocky View County/City of Calgary Intermunicipal Development Plan](#) (IDP). The application was circulated to Rocky View County

for their review and no concerns were identified. The proposal is consistent with the policies of the IDP.

Municipal Development Plan (Statutory – 2009)

The subject parcel is located within a Developing Planned Greenfield areas with an existing Area Structure Plan as identified on Map 1: Urban Structure in the [Municipal Development Plan](#) (MDP). This application proposes: integrating a mix of dwelling types and land uses; using a grid-based pattern of complete streets in the subdivision design; and meeting minimum intensity and density targets. This application aligns with applicable MDP policies.

Climate Strategy (2022)

This application addresses objectives of the [Calgary Climate Strategy - Pathways to 2050](#). The applicant is providing green mobility by establishing a grid-based street and pathway network that will support pedestrians and transit users, near a future BRT station. Further opportunities to align development with applicable climate strategies will be explored and encouraged at subsequent development approval stages.

Belvedere Area Structure Plan (Statutory – 2013)

The subject site is located within the [Belvedere Area Structure Plan](#) (ASP). The ASP identifies the subject lands as a Super Regional Retail Centre, however, after conducting an analysis of the retail demand in this area, the landowner concluded that there is insufficient demand to support retail development in this area. Additionally, the all-turns access currently existing at the intersection of 17 Avenue SE and 84 Street SE that would provide direct access to this site will be removed in the future, making access to this area less convenient for vehicular traffic and therefore decreasing its viability as a regional commercial development site. This would also result in more non-local traffic traveling through the adjacent residential areas to access this site from the ultimate realignment of 84 Street SE further east.

Administration has evaluated this application and is supportive of the proposal to amend the ASP for this site from Super Regional Retail Centre to Neighbourhood Area and to add this area to Community D, which is consistent with the developing residential neighbourhood directly east of this site. As detailed in Attachment 8, Proposed Amendment to the Belvedere Area Structure Plan, the specific amendments are to Maps 5 and 6, the graphic on page 5 and to Table 3 on page 24, which lists the 'Belvedere ASP Community Size, Number of Neighbourhoods, Anticipated Population and Jobs' to reflect the change in this area from commercial to residential. The proposal meets or exceeds all applicable policies in the ASP for residential development.