

OVERVIEW

SUBJECT LANDS

The subject lands (referred to as the Romeo & Juliet site) are comprised a 2.585 acre (1.046ha) parcel located at 222 Greenbriar Place NW – within the emerging mixed-use community of Greenwich. The site falls within the Greenbriar Special Study Area of the Bowness Area Redevelopment Plan (ARP) and is currently designated as Multi-Residential – Contextual Medium Profile (M-C2f2.5d42) District.

GREENWICH

Greenwich is ushering in the “New Northwest” with 59 acres of multi-family urban living, complete with boutique shopping, restaurants and office space; plus parks and trails for active outdoor living – all within an hour of the Rocky Mountains and 18 minutes from downtown Calgary.

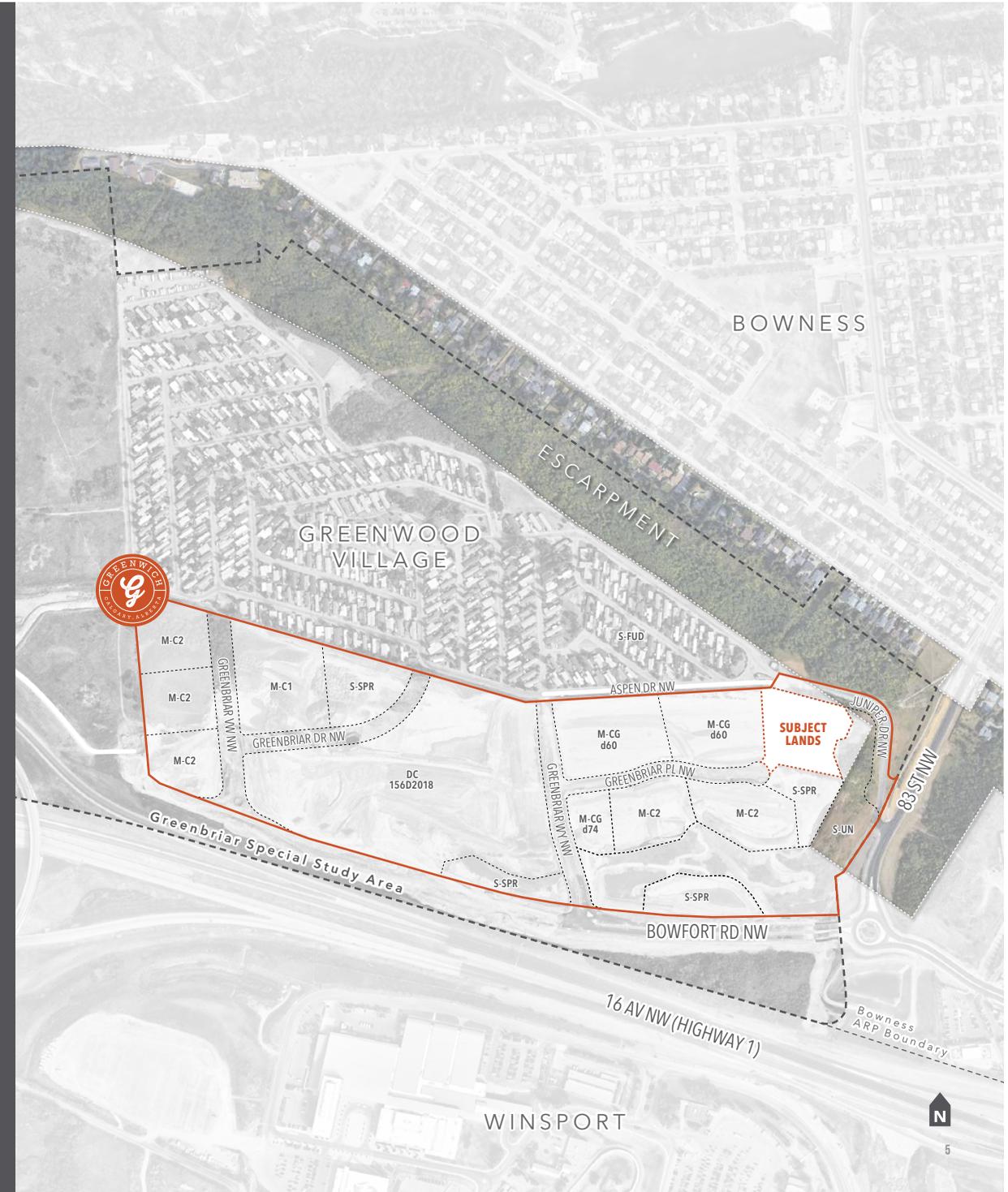
The central design concept of Greenwich will realize a mixed-use urban village that features a diverse range of housing options for Calgarians, underpinned by high standards of architectural design, amenity and day-to-day convenience.

Construction on several residential phases of Greenwich is ongoing, including the first stages of the mixed-use commercial core of the neighbourhood, which will feature the second location of the Calgary Farmer’s Market.

APPLICATION SUMMARY

This Brief provides both the context and planning analysis which has informed an Architectural Design Study undertaken by S2 Architecture for the currently vacant site. This preliminary concept contemplates two multi-residential buildings that transition in height and intensity from north to south. The proposed land use change is aimed at diversifying the range of housing options within the community, while also taking advantage of the site’s size, viewscales, proximity to parks and open space, and strategic location near major transportation corridors that have seen recent major infrastructure investments.

To support the future development vision, a proposed land use redesignation is required to transition the subject lands from Multi-Residential – Contextual Medium Profile (M-C2f2.5d42) to a Direct Control (DC) District (based on M-C2 and M-H1 land use district rules).



SITE CONTEXT

CHARACTERISTICS

The subject site is primarily flat, with a significant downward slope to the north and east of the site. This topography provides a vegetated buffer for a natural transition interface with the adjacent park and low density built-form fabric of Bowness. The site's topography also affords the subject lands with excellent views of Calgary and surrounding area to the north, east and south.

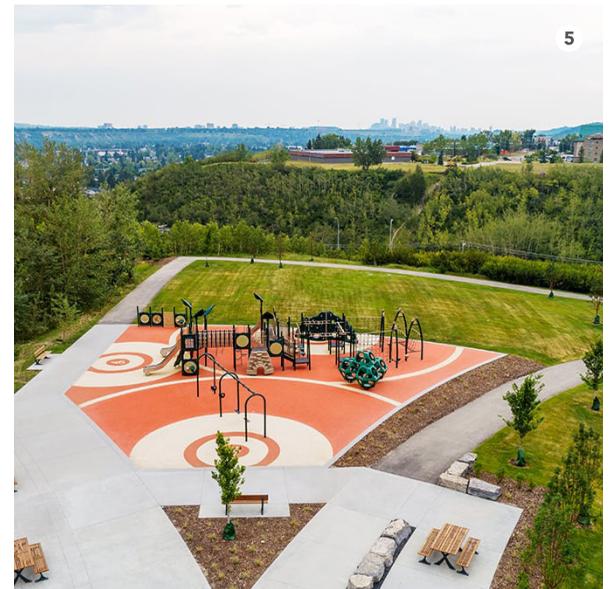
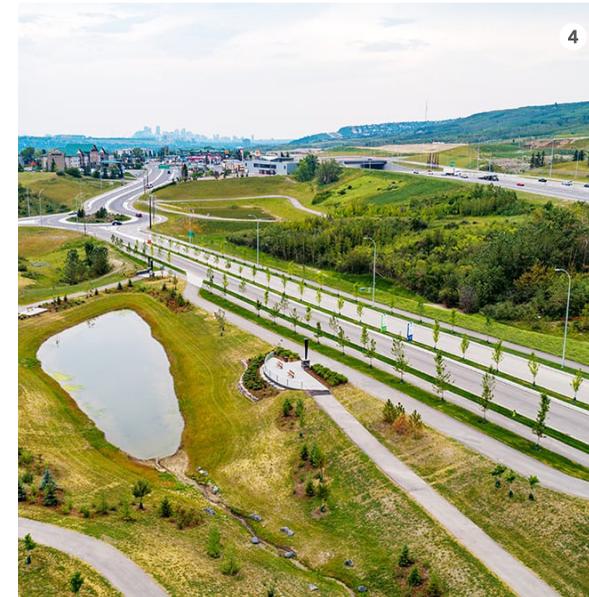
AMENITIES

The subject site is strategically situated to take advantage of the recent investments in local area parks and open spaces, infrastructure, transit connections, and easy access to local and regional destinations via 16 AV NW (Trans Canada Highway). Located in the developing community of Greenwich, the site further benefits from being within short walking distance (300m) to the emerging mixed use village area, future home of the Calgary Farmers' Market's second location.



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- 1 Visualization of future Calgary Farmers' Market
- 2 Subject site within Greenwich and surrounding context
- 3 Recently completed Greenwich pathway system



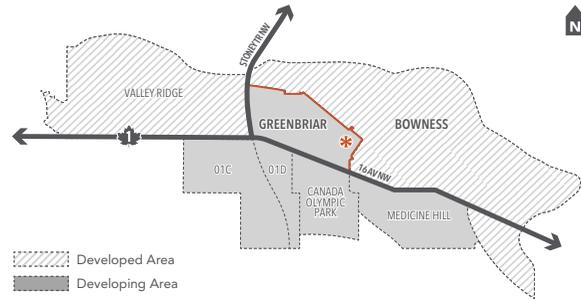
- 4 Recently completed Bowfort Road, pathway network and stormwater infrastructure
- 5 Recently completed playpark, immediately south of the subject site

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POLICY CONTEXT

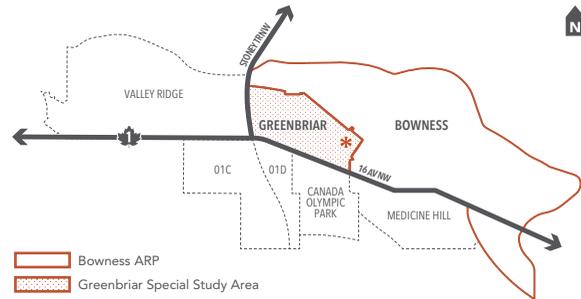
MUNICIPAL DEVELOPMENT PLAN

The subject lands fall within the Developing Areas of the Municipal Development Plan (MDP). The MDP encourages residential density to concentrate in areas adjacent to open spaces, with easy access to transit and community amenities. The Developing Areas are also subject to policies of the New Community Planning Guidebook which further advocate for complete communities with a wide range of housing options.



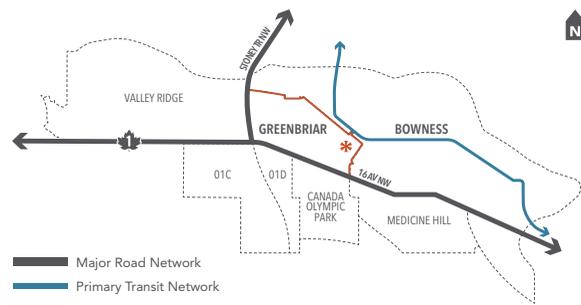
LOCAL AREA PLAN

The Bowness Area Redevelopment Plan (ARP) was originally adopted by the City in 1995 with the most recent update undertaken in 2018. The ARP places the subject site within the Greenbriar Special Study Area and supports a predominantly residential land use with a mix of housing types, including medium-high density apartment style developments. Consistent with the MDP, the ARP promotes higher densities in areas next to parks and within walking distance of local amenities and transit.



PRIMARY TRANSIT NETWORK

The subject lands are within ~400m walking distance of Bowness Road NW, a Primary Transit Network corridor identified in The City's strategic transit plan, RouteAhead. The Bowness Road Primary Transit corridor provides residents with daily high frequency public transit service (Route 1) with a frequency of every 10 minutes or less, 15 hours a day, 7 days a week. The subject lands are also served by local transit service (Route 53) with stops only 30m away.



GREENBRIAR SPECIAL STUDY AREA

The Greenbriar Special Study Area is specifically identified within the Bowness ARP as a special policy area that is distinct in context and history from the rest of the Bowness community. The Greenbriar Special Study area's primary objective is to create a new complete community – a walkable neighbourhood noted for its distinct sense of place, housing choice, and mixed uses intended to meet the basic day-to-day needs of the community. The minimum residential density in Greenbriar is 21 units per hectare (8.5 units per acre), to a maximum of 32 units per hectare (13 units per acre) across the balance of the plan area (based on the gross developable area).

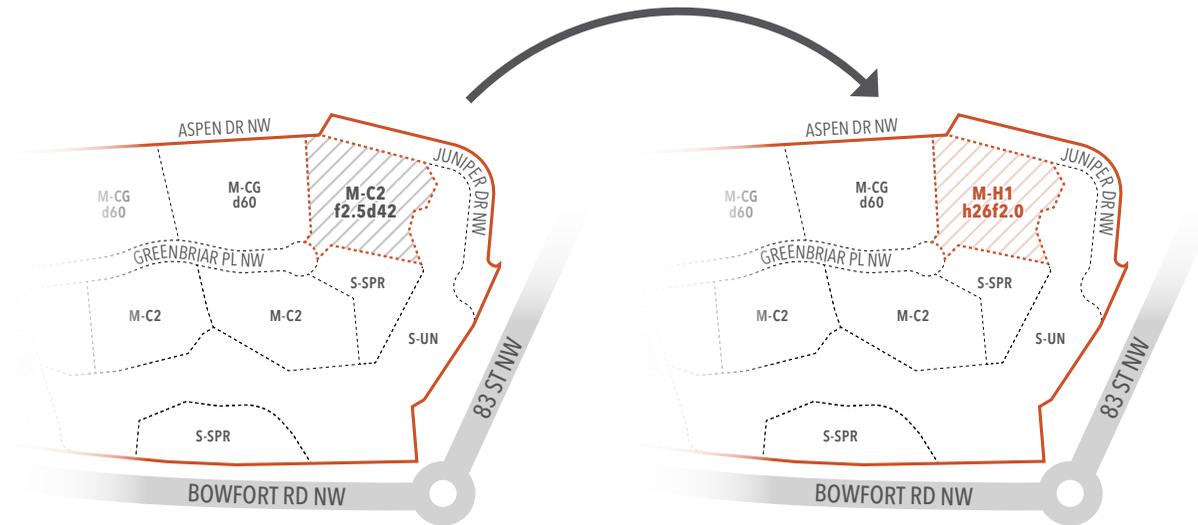


PROPOSED CHANGE JUNE 2019

ORIGINAL LAND USE REDESIGNATION APPLICATION

In June of 2019, the project team submitted a Land Use Redesignation application to transition the subject lands from the current Multi-Residential - Contextual Medium Profile (M-C2f2.5d42) to Multi-Residential - High Density Low Rise Profile (M-H1h26f2.0), with a maximum building height of 26m and a maximum FAR of 2.0.

The proposed Land Use Redesignation aimed to reallocated already approved densities within Greenwich and correct an artificially low site-specific density allocation for the subject site, which was based on the limits of a 2010-era area transportation network. Major transportation infrastructure improvements have since been completed in the area, including the Trans Canada Bowfort Road interchange and 83 ST NW upgrades. The removal of the transportation network constraints that originally informed the site's current land use district have already resulted in changes to the Bowness Area Redevelopment Plan, which now uses a blended minimum and maximum density across the entire Greenbriar Special Study Area.

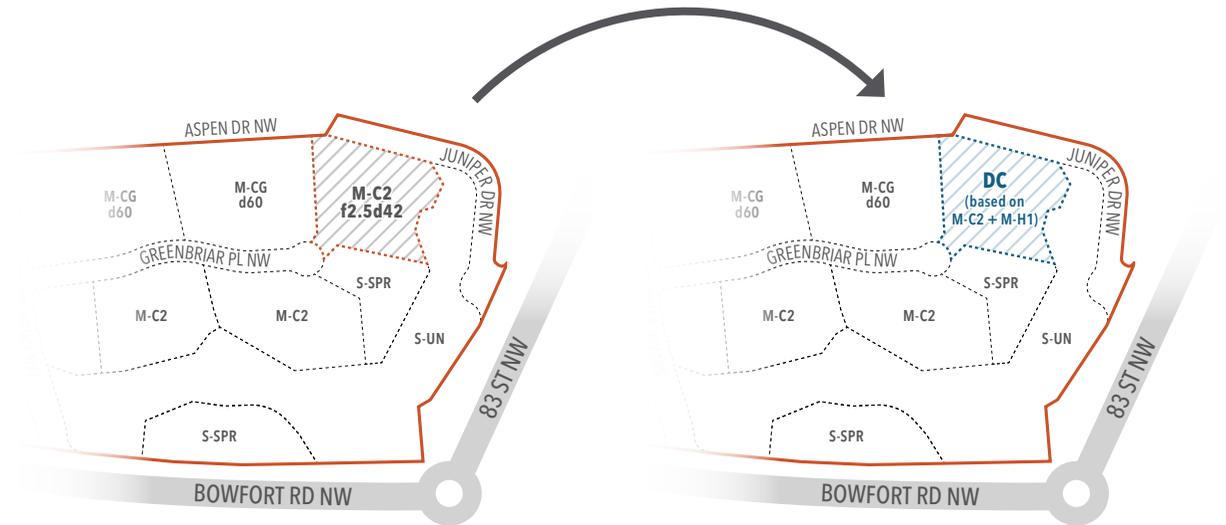


PROPOSED CHANGE FEBRUARY 2020

REVISED LAND USE REDESIGNATION APPLICATION

Based on further planning work, supporting studies and the stakeholder feedback received during the project team's outreach process, a revised Land Use Redesignation is proposed to transition the subject lands from the current Multi-Residential - Contextual Medium Profile (M-C2f2.5d42) to a custom Direct Control (DC) District, based on the rules of both the M-C2 and M-H1 land use districts. The proposed change effectively split the subject site into two discrete building sites, allowing future development to respond to the subject site's unique location and site conditions. The primary purpose of the proposed DC District is:

- To create a built form where building height transitions from high to low from the south of the site towards Juniper Drive NW; and
- To facilitate grade-oriented, multi-residential development that responds to the unique characteristics, constraints, and prominent location of the subject site and adjacent lands.



LAND USE CHANGE DIRECT CONTROL DISTRICT

PROPOSED DIRECT CONTROL (DC) DISTRICT

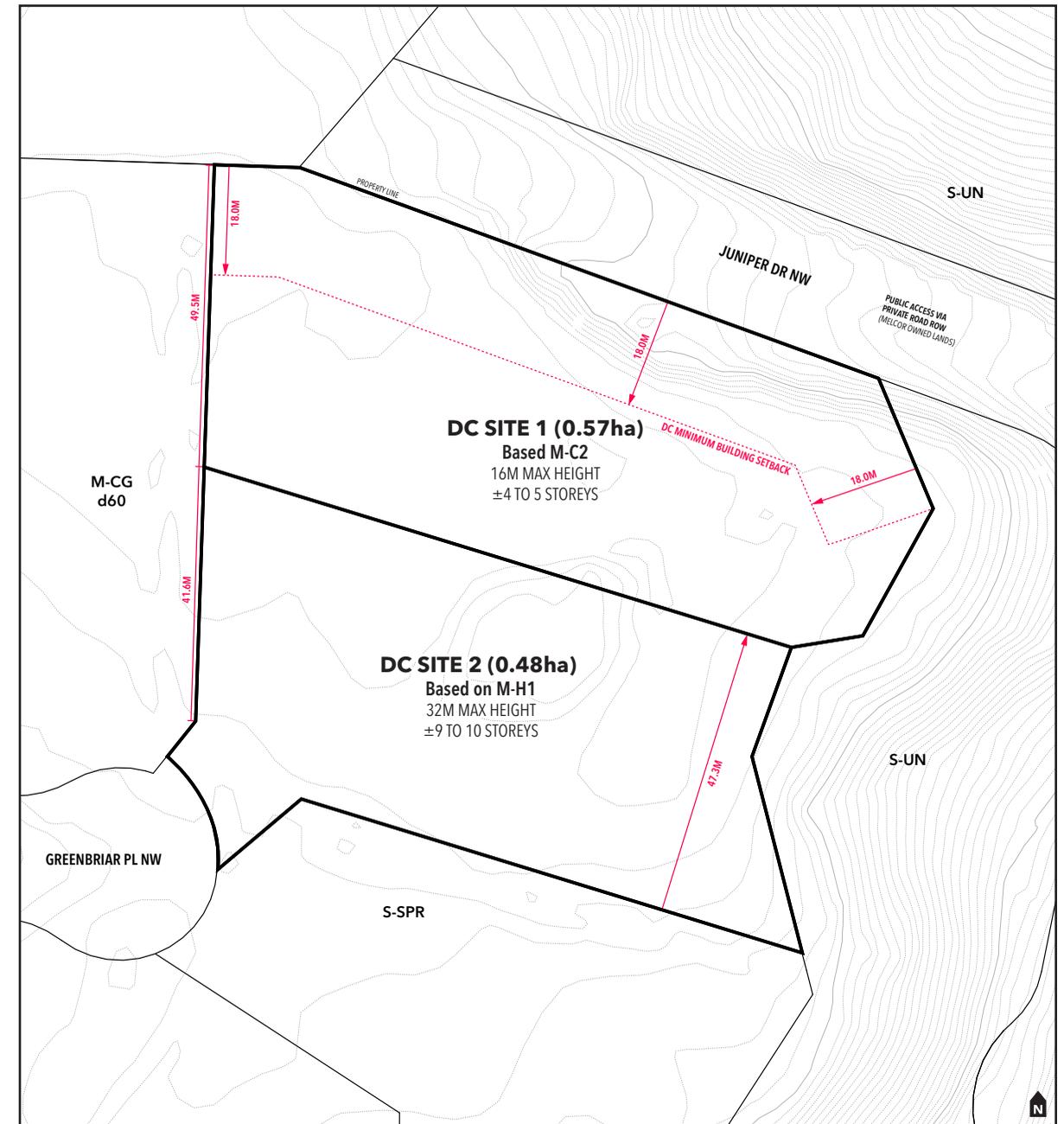
The proposed Direct Control (DC) District is a custom land use district developed by the project team, designed to facilitate the transition of both building height and mass across the subject site, from north to south. The proposed DC District also includes a custom 18m building setback from the northern property line, providing a substantial development buffer from Juniper Drive NW and the Bowness escarpment lands to the north.

The northern portion of the site (DC Site 1), which makes up approximately 54% of the total site area will continue to be governed by the same low rise M-C2 building height and massing rules in place today, with a maximum building height of 16m (4 to 5 storeys) and an additional 18m building setback from the northern property line. The 18m building setback area includes the northern sloped portions of the subject site.

In order to achieve a balance of building height, mass and viable unit densities across the subject site, the Southern portion of the site (DC Site 2), which makes up approximately 46% of the subject site, will be governed by rules found in the M-H1 District, with a revised maximum building height of 32m that will facilitate the development of a new ground-oriented mid-rise building of 9 to 10 storeys.

The proposed land use redesignation is the direct result of further planning work, supporting studies and the stakeholder feedback received during the project team's outreach process. Coupled with a proposed site-specific policy amendment to the Bowness ARP, the project team's approach aims to balance Melcor's development goals with unique site conditions and need to provide all stakeholders with a greater level of certainty for a high quality development outcome on this special site.

DRAFT DIRECT CONTROL SITE PLAN



BOWNESS ARP SITE-SPECIFIC POLICY AMENDMENT

SITE-SPECIFIC POLICY APPROACH

In combination with the proposed Direct Control District, the project team has developed a supporting site-specific amendment to the Bowness ARP that provides additional certainty for all stakeholders. The proposed ARP amendment acknowledges the historical significance and prominent location of the subject site, while providing strong urban design direction for a future Development Permit submission.

PROPOSED BOWNESS ARP POLICY AMENDMENT

Remove the existing *Residential Policy 10* on page 26 of the Bowness ARP, as the Bowfort Road / Trans Canada Highway interchange is now complete and the associated unit allocation and density limits are no longer applicable. Replace with below:



Residential (pg.26)

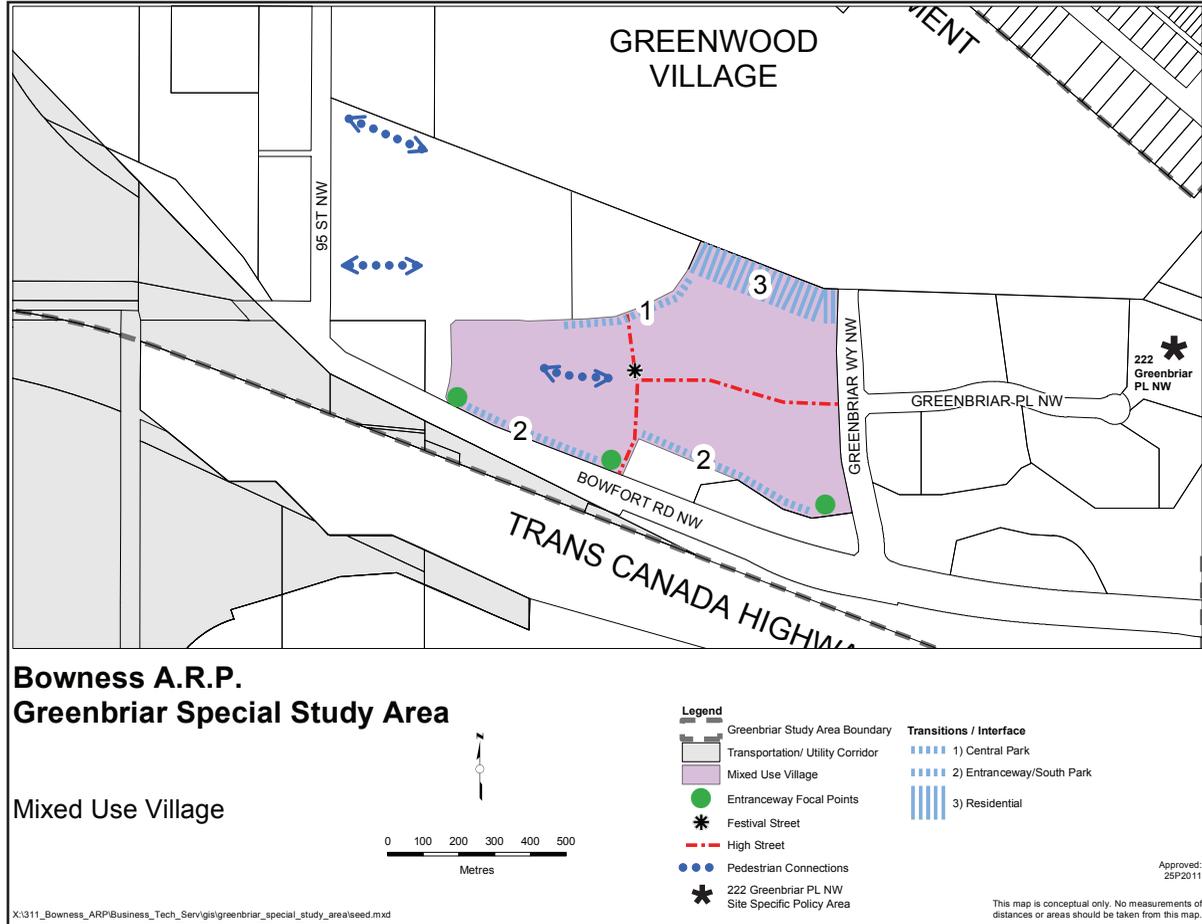
10. Site Specific Policy: 222 Greenbriar Place NW

Also known as the site of Romeo and Juliet Inn and formerly the Bowness Golf and Country Club established in 1912. As identified on Map 6, 222 Greenbriar Place NW is a prominent site and historically a landmark location. The following site-specific policies reflect the unique conditions for approval required to provide an additional layer of guidance in the review of any new development and ensure a high-quality development outcome:

- a. New development should demonstrate a high quality of design appropriate for a prominent landmark site.
- b. New development should consider the integration of interpretive elements, commemoration and/or names that acknowledge the historic significance of the site.
- c. A vehicular connection between Juniper Drive NW and Greenbriar Place NW should be provided to facilitate a second emergency vehicle access point from Juniper Drive NW.
- d. The design of and location of underground parking access should minimize impact on the pedestrian realm.
- e. Servicing, loading functions and building mechanical systems such as air intakes and exhausts, should be strategically located to minimize impact on the pedestrian realm and surrounding development.
- f. The development should provide on-site open spaces that are usable, well-designed, complementary and connected to surrounding natural areas and existing pathway system.
- g. New development should present a high-quality, well-articulated and grade-oriented interface to the adjacent park lands (Environmental Reserve and Municipal Reserve).
- h. The development should minimize shadow impacts on park lands and surrounding residential neighbourhoods through the strategic location of buildings and careful distribution of density and height.
- i. As identified in Map 8, the 18 metre development setback from the top of slope contained within the easterly adjacent environmental reserve must be followed, as set out in Section 3 "Environmental Policies", Policy 4.

- j. Appropriate setback and transition in scale to the North:
 - i. To encourage innovation and high-quality design, the northern 18 metre development setback requirement from the top of slope contained within the subject site, as set out in Section 3 "Environmental Policies", policy 4, may be reduced, where the following requirements are achieved by buildings within the setback area:
 - 1) Grade-orientation of at-grade dwelling units;
 - 2) Well-articulated building facades; and
 - 3) Durable, high quality materials.
 - ii. As part of the new development, the north sloped area within the property should be rehabilitated or reestablished to complement the adjacent natural landscape to the north and east.

DRAFT ARP AMENDMENT MAP 6: Mixed Use Village



DRAFT ARP AMENDMENT MAP 8: Top of Slope Development Setbacks (222 Greenbriar PL NW)



ESCARPMENT SETBACK

MDP + ARP POLICY

Both the MDP and Bowness ARP require that any new development or redevelopment adjacent to an existing escarpment should provide a 60 foot (18 metre) development setback from the top of the escarpment. The Bowness ARP Escarpment policy area boundary is just north and east of the subject site property lines.

PRELIMINARY NATURAL SITE ASSESSMENT

A *Preliminary Natural Site Assessment* undertaken by Westhoff Engineering found the construction of Juniper Drive NW introduced a considerable physical barrier separating the north Bowness escarpment from the subject site, having also changed the physical nature of the slope south of Juniper Drive. Based on field observations, the study found that residual plant communities and sloped lands south of Juniper Drive to be distinctly different from the residual native aspen forest of the Bowness escarpment. The study noted that the lands south of Juniper Drive have relatively low ecological value and are influenced considerably by weed invasion. In particular, the extensive Caragana patch found on the subject site offers low species diversity and likely provides minimal habitat value when compared to the native aspen forest of the escarpment north of Juniper Drive.

SITE-SPECIFIC TOP OF SLOPE SETBACK POLICY

The subject site is bounded by an existing natural area to the east, designated as Special Purpose – Urban Nature (S-UN) District. To maintain natural slope stability, provide a natural buffer and protect public views, a minimum setback of 18m from the top of slope within the S-UN lands is reinforced by the proposed site-specific ARP amendment.

DIRECT CONTROL (DC) DISTRICT BUILDING SETBACK

The proposed Direct Control (DC) District includes an 18m building setback from the northern property lines. The resulting setback area provides a substantial development buffer from Juniper Drive NW and the Bowness escarpment lands to the north. Sloped portions within the subject site may need to be re-engineered as part of a comprehensive site redevelopment strategy in order to prepare the site for development and stabilize the slope, with details to be provided at the Development Permit stage. The proposed site-specific ARP amendment requires that these lands to be rehabilitated or reestablished to complement the adjacent natural landscape to the north and east.

