

Previous CPC Report from November 5, 2020 (CPC2020-1212)

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Item # 7.1.1

Planning & Development Report to
Calgary Planning Commission
2020 November 05

ISC: UNRESTRICTED
CPC2020-1212

Development Permit in Hillhurst (Ward 7) at multiple properties, DP2020-3902

EXECUTIVE SUMMARY

This development permit application was submitted on 2020 June 25 by Riddell Kurczaba Architecture on behalf of the developer, Ocgrow Group of Companies and landowner B-V Automotive Ltd. This application proposes new mixed-use development consisting of:

- one 8-storey mixed-use building with indoor rooftop amenity space;
- retail and consumer service uses at-grade facing 14 Street NW with a floor area of 746.75 square metres;
- 172 dwelling units,
- 88 motor vehicle parking stalls; and
- 233 bicycle parking stalls.

The layout, design and integration within the existing streetscape and community were carefully considered during the review process. The proposal is in alignment with planning policy in the *Municipal Development Plan and Hillhurst/Sunnyside Area Redevelopment Plan (ARP)*. Urban design improvements are proposed to achieve a density bonus as per the ARP.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission **APPROVE** the proposed Development Permit application DP2020-3902 for New: Dwelling Units, Retail and Consumer Service (1 building), Sign – Class B (Fascia Sign) and Sign – Class A (Address Sign) at 211, 217, 219 and 221 - 14 Street NW (Plan 6219L, Block 3, Lots 3 to 10), with conditions (Attachment 1).

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

Following a Public Hearing on 2019 December 16, amendments to the *Hillhurst/Sunnyside Area Redevelopment Plan (ARP)* and land use amendment to a DC Direct Control District ([Bylaw 227D2019](#)) were approved. The amended ARP and DC District allow for approval of mixed-use development at the subject site with a maximum floor area ratio of 5.0 and maximum height of 26 metres with density bonusing.

This application was submitted on 2020 June 25 by Riddell Kurczaba Architecture on behalf of the developer, Ocgrow Group of Companies and landowner B-V Automotive Ltd. The land use of the site is DC Direct Control District ([Bylaw 227D2019](#)). The DC District density bonus provisions allow for a floor area ratio increase from 2.8 to 5.0 subject to a contribution to the Hillhurst/Sunnyside Community Amenity Fund or urban design improvements.

Approval(s): T. Goldstein concurs with this report. Author: M. Rockley

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Site Context

The subject parcel is located on the west side of 14 Street NW in the community of Hillhurst. The walking distance from the subject site to the Sunnyside LRT Station is 1.1 kilometres (approximately 14 minute walk) and the site is 1.3 kilometres (approximately 17 minute walk) to downtown.

The site is currently occupied by a one storey auto-oriented use with an associated vehicle storage yard. The existing building on the site was constructed in 1958.

The site is surrounded by low density residential uses across the lane to the west, two-storey commercial and retail uses immediately north and south of the site, one storey eating establishments, a one storey auto oriented use and an eight storey mixed use building across 14 Street NW to the east. Existing development on the block to the north of the subject site includes a nine storey multi-unit residential building and a seven-storey multi-unit residential building.

As identified in *Figure 1*, the community of Hillhurst has seen population decline over the last several years after reaching its population peak in 2015.

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Figure 1: Community Peak Population

Hillhurst	
Peak Population Year	2015
Peak Population	6,737
2019 Current Population	6,558
Difference in Population (Number)	-179
Difference in Population (Percent)	-2.7%

Source: The City of Calgary 2019 Census

Additional demographic and socio-economic information may be obtained online through the [Hillhurst](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Land Use

The site is regulated by a DC Direct Control District ([Bylaw 227D2019](#)) that is based on the Mixed Use – Active Frontage District (MU-2) District.

Administration highlights the following relaxations to the Land Use Bylaw 1P2007. Administration has reviewed each relaxation and considers them to be acceptable for the reasons outlined in the table below.

Land Use Bylaw Relaxations			
Regulation	Standard	Provided	Administration Rationale Supporting a Relaxation
1385 Setback Areas	(2) Where the parcel shares a property line: (a) with a street or LRT corridor there is no requirement for a setback area.	Plans indicate floors 3-8 are 1.82m into the Road Right of Way setback. Plans indicate the underground parking structure is 1.25m into the Road Right of Way.	Building within the road right of way setback are located with adequate clearance, 7.5 metres above grade and 3.0 metres below grade.
1335 Building Separation	(2) The façade of a building located above 23.0m from grade must provide a minimum horizontal separation of: (b) 5.5m from a property line shared with another parcel	Plans indicate the North and South setback to the 8 th floor is 0m (-5.50m). Plans indicate the rooftop staircase is 0.39m (-5.11m) to the South property line.	Consistent building setbacks at the north and south property lines preferred over setbacks at the eighth storey. Rooftop amenity level includes setbacks.
1338 Street Wall Stepback	Where the height of a building is greater than 23.0m measured from grade, the façade of the building within 6.0m of a property line shared with	Plans do not indicate a portion of the street facing façade stepped back 2.0m or greater.	Well articulated front building elevation facing 14 St NW.

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	a street must have a horizontal separation from the portion of the façade closest to grade where: (a) the horizontal separation has a minimum depth of 2.0m; and (b) the horizontal separation occurs between a minimum of 7.5m and a maximum of 23.0m measured from grade.		
1343 Internal Access to Uses	The uses listed in the Residential Group of Schedule A to this Bylaw, with the exception of a Hotel, may only share an internal hallway with any other use in the Residential Group of Schedule A to this Bylaw, with the exception of a hotel.	Plans indicate the ground floor Retail and Consumer Service Uses share a hallway with the Dwelling Unit Uses.	Allows for improved internal access to the laneway facing residential units. Without hallway access, residents in the laneway housing would need to go outdoors to enter the underground parkade. Could result in laneway residential parking in the surface retail parking stalls.
Parking Stalls (min.)	13 Commercial Motor Vehicle Parking Stalls Required.	Plans indicate 10 (-3) commercial motor vehicle parking stalls are provided on site.	Alternate modes of transportation available.
	109 Residential Motor Vehicle Parking Stalls Required	Plans indicate 88 (-21) residential motor vehicle parking stalls are provided on site.	Alternate modes of transportation available.

Site and Building Design

This application proposes a new mixed-use building with active uses at grade facing 14 Street NW and residential units at grade facing the lane. Seven stories of dwelling units will be located above the ground floor. A rooftop common amenity area including fitness centre and indoor seating lounge will be developed for use and enjoyment by the building residents. The building plans include two levels of underground parking including space for storage lockers, pet grooming area, bike parking and bike repair station.

Surface parking stalls accessed from the lane are included for the commercial uses. Loading stalls, waste and recycling facilities, and ramp to underground parking are accessed from the lane. A parking layby is provided at 14 Street NW adjacent to the at grade active commercial uses.

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New boulevard trees, shrubs and patio furniture are proposed at the redeveloped sidewalk along 14 Street NW. A 2.0 metre clear pedestrian path is provided between tree grate and patio furniture locations.

Additional boulevard trees, street furniture and sidewalk improvements are proposed to the north of the site along 14 Street NW and 2 Avenue NW as urban design improvements to achieve bonus density to a maximum floor area ratio of 5.0. These proposed offsite improvements are shown on the landscape plans (Attachment 2).

City-wide Urban Design

Building and site design revisions have been made by the applicant in response to comments from the City-wide Urban Design Team. The City Wide Urban Design Team is supportive of the application as proposed.

Urban Design Review Panel

The development pre-application was reviewed by the Urban Design Review Panel and design recommendations provided by the UDRP (Attachment 4) have been included in the applicant's formal development permit application.

Transportation

At the land use redesignation stage a Transportation Impact Assessment was provided in support of the proposal; which was reviewed and accepted by Transportation Planning. The subject site is located adjacent to Transit bus service on 14 Street NW, including Route 65 (Market Mall / Downtown West) and Route 414 (14 Street Crosstown). In addition to this, the site is located approximately 1.1 kilometre walking distance from the Sunnyside LRT Station, Route 201 (Somerset / Tuscany).

Vehicular access to the site will be available from the rear lane; providing access to a proposed underground parkade along with several surface parking stalls.

The parcel is subject to the Bylaw setback for public realm enhancements. The CPAG team and the applicant group coordinated a mutually beneficial solution which included a partial underground encroachment of setback area to facilitate portions of the parkade and foundation while maintaining the above-grade setback for the benefit of pedestrians and for public realm enhancements. The above-grade setback is defined through a public access easement for the benefit of public sidewalk users.

Construction drawings were submitted to review proposed developer-initiated changes to 14 Street NW and the rear lane. Generally speaking, these drawings are required to implement a new parking lane (layby) on southbound 14 Street NW adjacent to the site for the benefit of the retail uses. The construction drawings also address the proposed paving of the rear lane; as required by Waste & Recycling services.

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The proposed changes to 14 Street NW were reviewed by the City's Main Streets team; who accepted the current proposal with an acknowledgement that the 14 Street NW cross section is subject to further revisions based on the ultimate streetscape to be established by Main Streets.

Environmental Site Considerations

A Phase 3 Environmental Site Assessment was provided with this application stating that an underground storage tank was removed and the remedial activities were successful.

Utilities and Servicing

Water, sanitary sewer and storm sewer mains are available to service the subject site. A Development Site Servicing Plan (DSSP) is required to be submitted prior to the release of the development permit.

Climate Resilience

The proposal makes an effort to support energy conservation through an automation approach based on ALEXA smart building systems. The approach to building automation has some potential to reduce energy use in the building by establishing a platform where energy consumption information is readily available to users and closer integration with energy providers is possible. It should be noted that only electricity consumed by electrical devices will be measured, and energy consumed for space heating or hot water will not be measured, leaving a significant component of the energy mix unmonitored. Although the effort to deploy future oriented technologies may deliver modest energy savings is commendable, the opportunity to plan for future charging of electric vehicles in this building appears to be a significant missed opportunity.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to stakeholders and a notice was posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Comments were provided by two adjacent residents. Concerns included: waste and recycling operations, loading stall and rear surface parking stall noise, lane traffic, layby safety, rooftop amenity space noise, traffic calming on 15 Street NW and 2 Avenue NW requested and potential of reflective glare from windows. Administration considered the relevant planning issues raised and has determined the proposal to be appropriate.

The applicant held an online public open house for the project on 2020 June 30. A presentation was provided by the applicant, followed by a question and answer session (Attachment 5).

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The Hillhurst Sunnyside Planning Committee (HSPC) was circulated as part of this application and a letter was received in response (Attachment 3). Topics discussed within the letter include: the proposed development setting the tone and precedent for quality future redevelopment on 14 Street NW, high standard of urban design required with density bonusing and offsite improvements as an opportunity for incremental improvement.

Administration considered the relevant planning issues raised by the letters from adjacent residents, the applicant-led engagement and letter from the HSPC. The rooftop amenity area is setback from the edge of the roof to mitigate noise concerns. Surface parking, loading stalls, waste and recycling facilities meet City standards as proposed and the laneway facing residential units will assist in moderating noise from laneway operations. The high quality building design and off-site urban design improvements set a positive tone for further redevelopments along 14 Street NW.

Strategic Alignment

Administration considered the proposal against relevant planning policies listed below and found the proposal to be acceptable.

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the [South Saskatchewan Regional Plan](#), which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the Calgary Metropolitan Region Board's [Interim Growth Plan](#). This development permit builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure and establishing strong sustainable communities.

Municipal Development Plan (Statutory – 2009)

The subject site is identified on [Map 1: Urban Structure Map](#) of the [Municipal Development Plan](#) as being within the Neighbourhood Main Street Area. Neighbourhood Main Streets are typically located along Primary Transit Network within the Inner City and have a strong historical connection to the communities they abut. They are the "main streets" for one or more communities, providing a strong social function and typically support a mix of uses within a pedestrian-friendly environment. The application is in alignment with the main street planning direction as it provides for increased population and jobs near transit.

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Hillhurst/Sunnyside Area Redevelopment Plan (Statutory – 1988)

The subject site falls within the Transit Oriented Development Area of the [Hillhurst/Sunnyside Area Redevelopment Plan](#) (ARP), approved by Council in 2009 February. The subject site is situated in the area identified as Regional Mixed-Use on the Land Use Policy Area Map of the ARP. New mixed-use development in a multi-storey format with residential uses above the street level is strongly encouraged. The proposed development complies with the policies of the ARP.

In 2012 November, Council approved an amendment to the *Hillhurst/Sunnyside Area Redevelopment Plan* to include density bonus provisions, which allow for a density increase up to the maximum floor area ratio specified in the Area Redevelopment Plan. The density increase is subject to a contribution to the community amenity fund or provision of an urban design improvement. At this time the applicant intends to work with Administration to design and potentially construct improvements including street trees, public seating and sidewalk improvements north of the subject site at 14 Street NW and 2 Avenue NW.

Climate Resilience Strategy (2018)

The [Climate Resilience Strategy](#) identifies programs and actions intended to reduce Calgary's greenhouse gas emissions and mitigate climate risks. The efforts to establish a whole building platform where electricity consumption information can be tracked supports Climate Mitigation Program 2 related to 'Energy Consumption Information' and improved energy literacy.

Social, Environmental, Economic (External)

The proposed development allows for greater density, including more housing and job opportunities within a walkable community close to transit, and as such, the proposed change may add to the vibrancy of the Neighbourhood Main Street and active street environment.

Financial Capacity

Current and Future Operating Budget

There are no other known impacts to the current and future operating budgets.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns.

Risk Assessment

There are no significant risks associated with this proposal.

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REASON(S) FOR RECOMMENDATION:

The proposal is in keeping with the applicable policies of the Municipal Development Plan and the vision of the Hillhurst/Sunnyside Area Redevelopment Plan. Given that 14 Street NW is identified as a Neighbourhood Main Street, the proposed development will utilize an under-developed parcel in the inner city for a higher use while still respecting the low density context adjacent to the site.

ATTACHMENTS

1. Conditions of Approval
2. Development Permit Plans
3. Community Association Letter
4. Urban Design Review Panel Comments
5. Open House Summary

Approval(s): T. Goldstein concurs with this report. Author: M. Rockley

Conditions of Approval

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a complete digital set of the amended plans in PDF format and a separate PDF that provides a point-by-point explanation as to how each of the Prior to Release conditions were addressed and/or resolved. The submitted plans must comprehensively address the Prior to Release conditions as specified in this document. Ensure that all plans affected by the revisions are amended accordingly. To arrange the digital submission, please contact your File Manager directly.
2. As per Part II, section 3.1.5 of the Hillhurst/Sunnyside Area Redevelopment Plan, provide a letter, to the satisfaction of the Development Authority, detailing how improvements to the urban realm at 2 Avenue NW and 14 Street NW will be undertaken. The letter must address: what urban realm improvements will be provided, coordination of the work and how any cost overages will be addressed based on discussions with City Staff. The contribution is based upon the contribution formula found in the Direct Control Bylaw for the subject site:

\$18.14 x 4,598.73 square metres (total floor area in m2 above base floor area ratio of 2.8) = **\$83,420.89**

or

Remit payment (certified cheque, bank draft) for contribution to the Hillhurst/Sunnyside Community Amenity Fund as per DC Direct Control District (227D2019).

a) Payment is based on the following:

\$18.14 per square metre x 4,598.73 square metres = **\$83,420.89**

Development Engineering:

3. Amend the plans to:

Waste & Recycling Services - General

- a. Provide an overhead door (min. size of 3.0m wide X 2.1m high) and a separate man door with keypad access for the waste facility.

Waste & Recycling Services - Collection Vehicle Access

- a. Indicate that the adjacent lane will be paved at the developer's expense, as the containers will be rolled into the lane for collection.
- b. Provide (include relevant grades) a level transition between the collection / staging area and the adjacent alley.

Conditions of Approval

4. Consolidate the subject parcels. Submit a copy of the registered plan and certificate of title, confirming the consolidation of subject parcels onto a single titled parcel, to the Development Engineering Generalist.
5. Submit three (3) sets of the Development Site Servicing Plan details to Development Servicing, Inspections and Permits, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact developmentservicing2@calgary.ca for additional details.

For further information, refer to the following:

Design Guidelines for Development Site Servicing Plans

https://www.calgary.ca/PDA/pd/Documents/urban_development/publications/DSSP-Design-Guidelines.pdf

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/pd/Documents/development/development-site-servicing-plan.pdf>

6. Follow the submission requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control (Erosion and Sediment Control Reports and Drawings: Technical Requirements) and either submit the required (2) copies of an Erosion and Sediment Control (ESC) Report and Drawings or the required (2) copies of a Written Notice to Development Engineering, for review and acceptance by Stormwater Pollution Prevention (SPP), Water Resources. The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/esc (under Approvals).

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Guidelines for Erosion and Sediment Control* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Professional Engineer (P. Eng.), Professional Licensee (P.L. Eng), Professional Agrologist (P. Ag.) or Certified Professional in Erosion and Sediment Control (CPESC). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

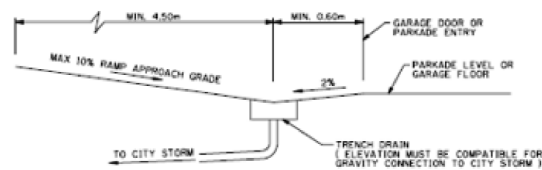
If you have any questions, contact 3-1-1. A Service Request (SR) will be created for the Erosion Control Team in Water Resources.

Transportation:

7. Obtain final approval for the Construction Drawings CD2020-0134 for the proposed offsite improvements. Ensure the plans for release of DP2020-3902 are consistent with the approved Construction Drawings.
8. Amend the plans to indicate the Public Access Easement area (pedestrian) required for the 1.37 meter horizontal encroachment into the Bylaw setback area.

Conditions of Approval

9. Execute and register on title a Public Access Easement Agreement with the City of Calgary over the horizontal encroachment of the Bylaw setback / future road right of way associated with the project lands (Servient Lands) in favour of 14 ST NW (Dominant Lands) for the purpose of pedestrian access. The agreement and registerable access right of way plan shall be to the satisfaction of the Director, Transportation Planning and the City Solicitor. A standard template for the agreement and an Instruction Document can be provided by the Transportation CPAG Generalist. Submit an original copy of the executed agreement and the certificate of title(s), indicating the agreement is registered on title, for all affected parcels.
10. Amend the plans to indicate the following with respect to the 14 ST NW boulevard and sidewalk:
 - Indicate that all adjacent boulevards are graded to the satisfaction of Calgary Roads.
 - Provide two (2) boulevard cross sections. Indicate the existing and proposed dimensions, elevations and slopes at the top of curb, ultimate property line and the existing property line and the main floor. **As agreed to by Roads, a compromised maximum 3% grade is permitted in the boulevard.**
11. Regarding the bicycle parking supply:
 - Amend the plans to provide the Bylaw required supply of Class 2 bicycle parking stalls (10 additional stalls / 5 additional racks).
 - Provide a detail of the Class 2 racks. We recommend inverted, U-shaped racks..
 - Provide a written protocol for Class 1 bicycle parking access. Where will entry be provided for cyclists ? How will safe access be guaranteed at all times ?
12. Amend the plans to provide signage in all Visitor parking stalls indicating the restricted use. Provide a detail of the signage on the plans.
13. Amend the plans to provide wheel stops in parking stalls located off the lane.
14. Amend the plans to include a Trench drain detail per Roads specification 454.1010.003. See below:



DETAIL - MINIMUM REQUIREMENTS FOR RAMPS TO
UNDERGROUND PARKADE OR GARAGE

15. Regarding the adjacent Transit zone:
 - Amend the plans to indicate that the adjacent Transit stop will include a Transit shelter.
 - The Transit shelter shall be provided at cost to the developer.
 - Confirm satisfactory payment for the Transit shelter. Contact Olivia Veltom (Olivia.veltom@calgary.ca) for cost estimate.

Conditions of Approval

16. Amend the plans to remove the indication of patio furniture from the Road right of way + Bylaw setback area for this DP submission. Although such items can be considered, there is a separate approval process (license of occupation: call 311).
17. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Closure and removal of existing driveway crossing
- b. Construction of new sidewalks
- c. Construction of new wheelchair ramps,
- d. Construction of new curb and gutter
- e. Construction of tree trenches to City standards
- f. Construction of lay-by City standards
- g. Lane paving.
- h. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel.

18. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Roads

- a. Street lighting upgrading adjacent to the site and as may be required for the proposed offsite improvement area.

Parks:

No comments.

Permanent Conditions

The following permanent conditions shall apply:

Planning:

19. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
20. No changes to the approved plans shall take place unless authorized by the Development Authority.

Conditions of Approval

21. A Development Completion Permit shall be issued for the development **before the use is commenced or the development occupied**. A Development Completion Permit is independent from the requirements of Building Permit occupancy. Call Development Inspection Services at 403-268-5311 to request a site inspection for the Development Completion Permit.
22. Upon completion of the main floor of the building, proof of the geodetic elevation of the constructed main floor must be submitted to and approved by the Development Authority prior to any further construction proceeding. Email confirmation to geodetic.review@calgary.ca.
23. All electrical servicing for freestanding light standards shall be provided from underground.
24. A lighting system to meet a minimum of 54 LUX with a uniformity ratio of 4:1 on pavement shall be provided.
25. The walls, pillars and ceiling of the underground parkade shall be painted white or a comparable light colour.
26. The light fixtures in the parkade shall be positioned over the parking stalls (not the drive aisles).
27. All stairwell doors and elevator access areas shall be installed with a transparent panel for visibility.
28. Fascia signage shall be placed only in the designated sign area as indicated on the approved plans. Any damage to the building face, as a result of the sign installation or removal, shall be repaired to the satisfaction of the Development Authority.

Development Engineering:

29. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
 - a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
30. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good

Conditions of Approval

housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

For all soil disturbing projects, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

31. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.
32. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
33. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.

Transportation:

34. This section of 14 ST NW is the subject of a future Main Streets initiative.
35. The developer shall be responsible for the cost of public work and any damage during construction in City road right-of-ways, as required by the Manager, Transportation Planning. All work performed on public property shall be done in accordance with City standards.
36. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the applicant's expense, upon completion of the foundation. Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.

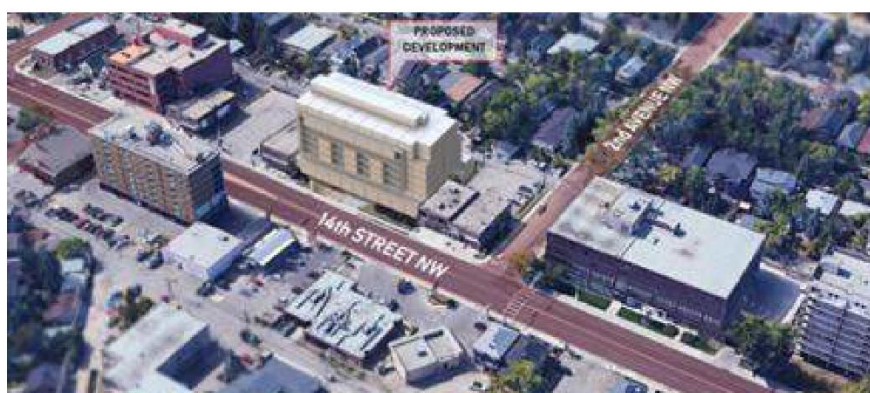
Conditions of Approval

Parks:

37. Any damage to public parks, boulevards or trees resulting from development activity, construction staging or materials storage, or construction access will require restoration at the developer's expense. The disturbed area shall be maintained until planting is established and approved by the Parks Development Inspector. Contact 311 for an inspection.
38. Any tree planting in the City boulevard shall be performed and inspected in accordance with Parks Development Guidelines and Standard Specifications – Landscape Construction (current edition). Applicant is to contact the Parks Development Inspector through 311 to arrange an inspection.

Development Permit Plans

DP APPLICATION SOLA



TRANSPORTATION



STRUCTURAL



MECHANICAL / ELECTRICAL



CIVIL



LANDSCAPE



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- A26 - SHADOW STUDY
- A27 - SHADOW STUDY
- A28 - GARBAGE DETAIL
- L29- LANDSCAPE
- L30- LANDSCAPE

Development Permit Plans

DP APPLICATION NOTES

ZONING DC 227 D2019
PERMITTED HEIGHT 30M - 26M AND 30M ARE MET
SETBACKS - PER BY-LAW- SEE SECTION A19 FOR DETAILED SETBACKS
GROSS SQUARE METER = 10451.6
SITE COVERAGE - 14,582 SF 1354.7 SM SITE COVERAGE ON 2090.3 SM .209 hectares SITE COVERAGE = 64.8%
168 UNITS or 804 UNITS / HECTARE

Easement relaxation - the 14th street road widening easement is respected at grade. We seek relaxation of the intermittent soffit of the 3rd floor of the building which has an important architectural feature of 6 projecting bays. There is still 7.280 meters clear under these bay projections. see A19

Flood - The electrical equipment is predominantly on P1. The foundation will have a pump system to manage any water seepage. It is expected that in the event of a flood that P1 floor can be protected. All electrical equipment on P1 will be raised above the slab floor to allow for drainage down to the lower level. The transformer and generator on the main floor will be at 5030. While caution must be exercised to mitigate flood effects, effort has been made to raise the main FF with 2 internal steps to keep the rear elevation meaningfully above the lane elevation while not compromising the direct at grade access on 14th street to encourage the usage of the retail environment. Construction specs of the main floor will mitigate flood damage risk by raising glass sills to 5030, avoiding electrical and water vulnerable finishes below 5030 as examples of a few strategies. residents using storage in the basement will be informed of potential for flooding.

The **public realm** on 14th has been enhanced with effort to consolidate services to allow for a tight rhythm of tree planting. The lay-by parking stalls provide a sense of protection to pedestrians while enhancing the accessibility to the retail. Two tone paving will provide a rhythm in coordination with the tree spacing. An approximate 2meter space for egress from CRU units will be activated by retail units with opportunities for street furniture street displays and other appropriate functions. The sidewalk proper has been widened to the requested 2 meters. Lighting on the street with recessed strip fixtures will wash the front of the glass, the CRU units and provide spill over light on the sidewalk.

The **offsite enhancements** north of the site have added trees on 14th street and street furniture, provided for the buss shelter and focused on the 2nd street intersection with landscaping wrapping around the north side.

The **lane** will be paved with asphalt from the site north to 2nd avenue. 10 parking stalls will be reserved for retail usage. This parking area will be finished with concrete in textured finishes to help set off the site and add to the functional width of the lane with additional space on the lane behind the stalls. The internal rear circulation hall has been separated in function from the Condo to allow for free flow of all patrons to the retail or condo building. The north entry has been moved out in line with the other entry to reduce the depth of the recess on the building face. The residential units will not have back doors on the hall to access other building amenities but their primary entrance is from the lane. Separations in the parking has been added at the residential entries to provide access to these units. The courtyard spaces have been split for each unit and to formalize the resident entry zone. The brick red color accent finish of the building has been utilized to distinguish these units with their own identity. These units have the advantage of greater ceiling height which helps to provide a sense of town house. Lighting on the lane is at all entries and service areas. The light fixtures will respect dark sky objectives and avoid glare across the lane while providing security to the immediate building. Tree planting was not achieved due to parking under and power over head. Planters will be incorporated in the courtyards for residents to plant according to their tastes, but the location is not suitable for anything but smaller decorative plantings. The courtyard fences will use the same material as the wall but with a different rhythm in the pattern. These fences will be 5ft tall to provide privacy and security with lockable gates. UDRP recommended Live work' as a reasonable alternate provide for these units. It would be beneficial to allow flexibility for such uses on a user demand basis and allow the market place to have this option. In such circumstances application could be made to alter the fence to increase visibility.

Building Architecture and Finishes - The building base on 14th street is finished with gloss white porcelain to offset the texture of the dominant cement board panels above. These will ensure a robust maintenance free finish at grade that will provide a clean contemporary expression. There are 3 colors of panels each with two textures. (White, Charcoal, Brick red) the contrast of gloss vs flat finish will add dimension to each of these fields of color. The building expression counterbalances balconies with projections that are viewed as enclosed balconies. 49 of the units do not as a result have balconies. There is dependence on the roof top for outdoor amenity for these units. The pushing and pulling of projecting forms provides a repeating pattern that breaks down the massing on the east and enhances the stepping effect on the west. The roof top canopy reaches out to the south and 14th street side providing identity to the top while being practical to manage microclimate on the roof. The roof paving creates a walk around path with various outdoor seating environments that include fire pits and a micro climate protected partial glass enclosure facing the mountains. The roof top will be a primary gathering area for residents as well providing a fitness room that features the cities night lights. These amenities disguise the mechanical spaces creating the effect of destination on the roof.

Mechanical, Electrical, Structural, Civil, Transportation, Geotech, Environmental layouts while not submitted in this package are implicit in the plans showing structural grids and accurate columns, stairs elevators and shear wall sizes. Location of mechanical and electrical has been rationalized on all floors with provision for primary ducting. Heating and cooling will provide for individual HRV systems for each unit but with efficient central hot and cold water. This will lead to a healthy building providing isolation between units and obtain the highest energy efficient potential. The building envelop will meet the current high standards of the codes leading to a high performance building. Full energy modeling will take place as part of construction documentation. The building will employ smart technologies for the total building and individual suites allowing full remote control of systems which is projected to save 1/3 of energy costs. Full scale civil are separate from this attachment. Full DSSP documents to follow in two weeks. Note reports for transportation, environmental, geotech are complete and can be submitted if needed.

Garbage SEE DETAIL SHEET #28

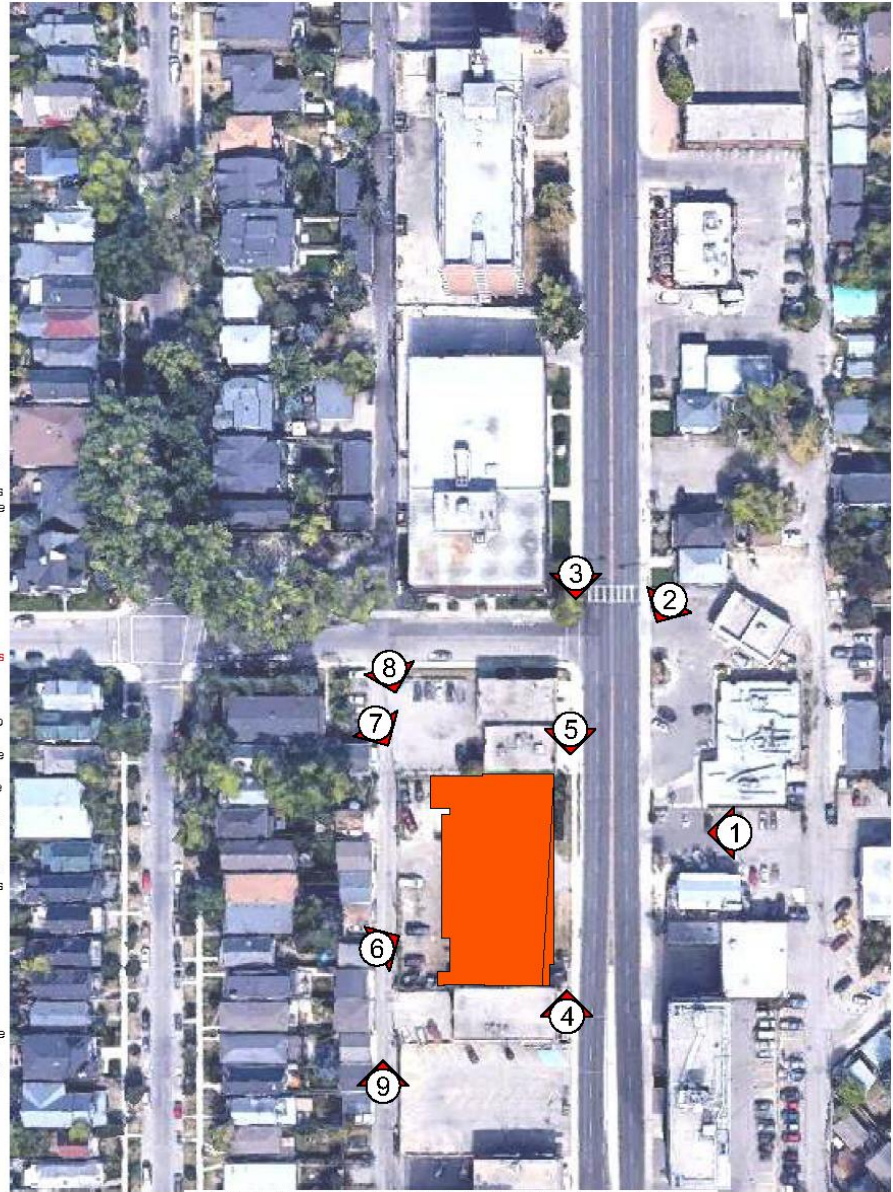
LOADING SWEEP PATHS SEE DETAIL SHEET see sheet detail #10

Loading sweep paths show a 6.4 Meter long truck (TAC 2017 LSU) as worst case. Larger vehicles cannot clear the power poles with .8m clearance. Moving vehicles for small units recommend 17ft trucks, this size can easily use the lane.

BIKE PARKING SEE A7 FOR DETAILS (215 CLASS 1 & 18 CLASS 2)- the objective is to have 1.25 bike stalls per unit on site with most as class 1 stalls so that there is a symbolic correlation between number of units and number of bikes. Bikes will typically enter the building on the car ramp. Stalls are located on P1 to provide convenient access to this entry zone. The ramp clearance meets minimum standards of 2.4 meters should some ride down the ramp. Riding down the ramp is not encouraged but will take place. There are no other bulkhead or clearance issues affecting bike access. Extra space is provided in the p1 bike parking area for a bench and bike mount for bike maintenance and repairs. This will be encouraged rather than taking bikes up to units. Elevators are adequate to handle bikes, though the use of elevators for bike movement is not preferred.

PARKING SEE Bunt calculation below .87 stalls required / .88 stalls provided - 17 designated guest stalls provided

MJ-2 Bylaw Vehicle Parking Calculation									
Project #	221 14 Street NW								
Date:	2020-08-06								
Bicycle (Class 1) Supply:	1.25 per unit								
Land Uses	Density	Bylaw Parking Ratios		Transit Reduction		Bylaw Stalls		After Shared Parking	
		Base Ratios	per	With No Transit Reduction	With Transit Reduction	With No Transit Reduction	With Transit Reduction	With No Transit Reduction	With Transit Reduction
Multi-Family (Resident)	172 units	0.75 stalls per unit	130	25%	97	65	97	65	65
		-0.25 stalls per extra bike							
Multi-Family (Visitor)		0.10 stalls per unit	17	25%	13	13	26	19	19
Retail	749 m2 GUFA	2.00 stalls per 100 m2 GUFA	15	25%	12	9			
		-3.00 stalls ground reduction							
Total					127	87	123	84	
Parking Reductions Applied									
1352 - Reduction For Transit Supportive Development									
The required number of motor vehicle parking stalls in section 1350 is reduced by 25.0 per cent when the use is located in a building located within 600.0 metres of an existing or approved capital funded LRT platform or within 150.0 metres of frequent bus service.									
Note:	Transit Service on 14 Street NW (<25m) does not meet the 'frequent bus service' threshold								
Note:	Transit Service on Kensington Road (145m) does meet the 'frequent bus service' threshold								
1354 - Reduction for Bicycle Supportive Development									
The total number of motor vehicle parking stalls required by section 1350 for all of the units within the development is reduced by 0.25 motor vehicle parking stalls for each additional bicycle parking stalls - class 1 provided in excess of the number of bicycle parking stalls - class 1 required in section 1353 to a maximum of 25 per cent of the total number of motor vehicle parking stalls required by section 1350 for all of the units within the development.									
Note:	To qualify for the maximum 25% reduction requires a Class 1 bicycle supply ratio of 1.25 stalls per unit								

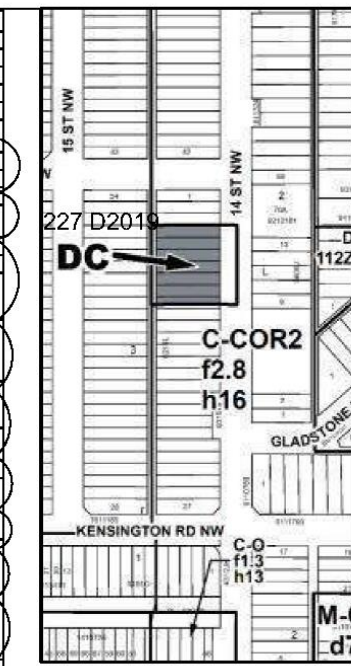


CONTEXT PLAN AND PHOTO LOCATIONS

GROSS AND NET AREAS					
Floor	Units	common area	Gross sm	Net sm	N/G
P2	29 stalls				
P1	49 stalls				
Total garage	78 STALLS				
1 total main floor			1221.49	976.04	
1 main floor retail			0	746.75	
1 Main floor residential			0	221.01	
2	25	1253	1288.75	1163.42	
3	25	1253	1366.79	1241.56	
4	25	1253	1366.79	1241.56	
5	25	1253	1366.79	1241.56	
6	25	1253	1293.30	1167.98	
7	23	1253	1271.56	1146.24	
8	20	1253	1063.83	938.51	
9			212.28		
Total NET residential				8361.83	
Total FAR (FAR 5=22,500sf x 5 =112,500sf max allowed)	172	0	10451.58	9108.58	87.2%
2090.32 x 5 = 10451.6 sm					

AMENITY SPACE		
Floor	area	notes
P2		Doq wash
P1		Bike rep
1	48	Balcony
2	118	Balcony
3	44	Balcony
4	72	Balcony
5	72	Balcony
6	72	Balcony
7	107	Balcony
8	203	Balcony
PENT	129	inside
PENT	597	outside
TOTAL	1462	860sm required

UNIT TYPE SUMMARY						
UNIT TYPE	Width	LENGTH	Bedrooms	AREA of UNIT	# units	
A1 medium	14' 4"	34' 10"	2 bed	556sf 51.65m	2	
A2 medium	14' 4"	44'	2 bed	629sf 58.44m	3	
A3 medium	14' 4"	34'	2 bed	582sf 54.07m	5	
TOTAL					10	
B1 wide	16' 5"	44' 9"	2 bed	618sf 57.14m	2	
				700sf 65.03m	3	
B2 wide	16' 5"	31' 4"	2 bed	632sf 58.71m	2	
B3 wide	16' 5"	36' 5"	2 bed	713sf 66.24m	3	
B4 wide	16' 5"	31' 4"	2 bed	550sf 51.10m	5	
B5 wide	16' 5"	35'		541sf 50.26m	3	
				492sf 45.7m	1	
B6 wide	16' 5"	35'		567sf 52.68m	4	
TOTAL					24	
C1 narrow	12' 3"	35'		445sf 41.34m	2	
				432sf 40.13m	48	
				412sf 38.28m	1	
C2 narrow	12' 3"	35'		455sf 42.27m	2	
				471sf 43.76m	3	
				484sf 44.96m	3	
				490sf 45.52m	2	
C3 narrow	12' 3"	40' 11"		512sf 46.57m	30	
	12' 4"			516sf 47.94m	3	
				520sf 48.31m	7	
C4 narrow	12' 3"	41' 6"	2 bed	550sf 51.10m	17	
			2 bed	580sf 53.88m	8	
TOTAL					126	
Town house	24' 8"	24' 5"	2 bed	595sf 55.28m	4	
				618sf 57.41m		
				503sf 46.73		
				636sf		
E penthouse W. Floor 7-8			2 bed	872sf 81.01m	8	
				865sf 80.36m		
				582sf 54.8m		
				466sf 43.3 m		
				696sf 64.66m		
				708sf 65.77m		
				681sf 63.27m		
				1090sf 101.26sm		
RES TOTAL					172	



riddell kurczaba **PROJECT NOTES**

#19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL10



2020-08-31 **DP - REVISION 1 A-2**

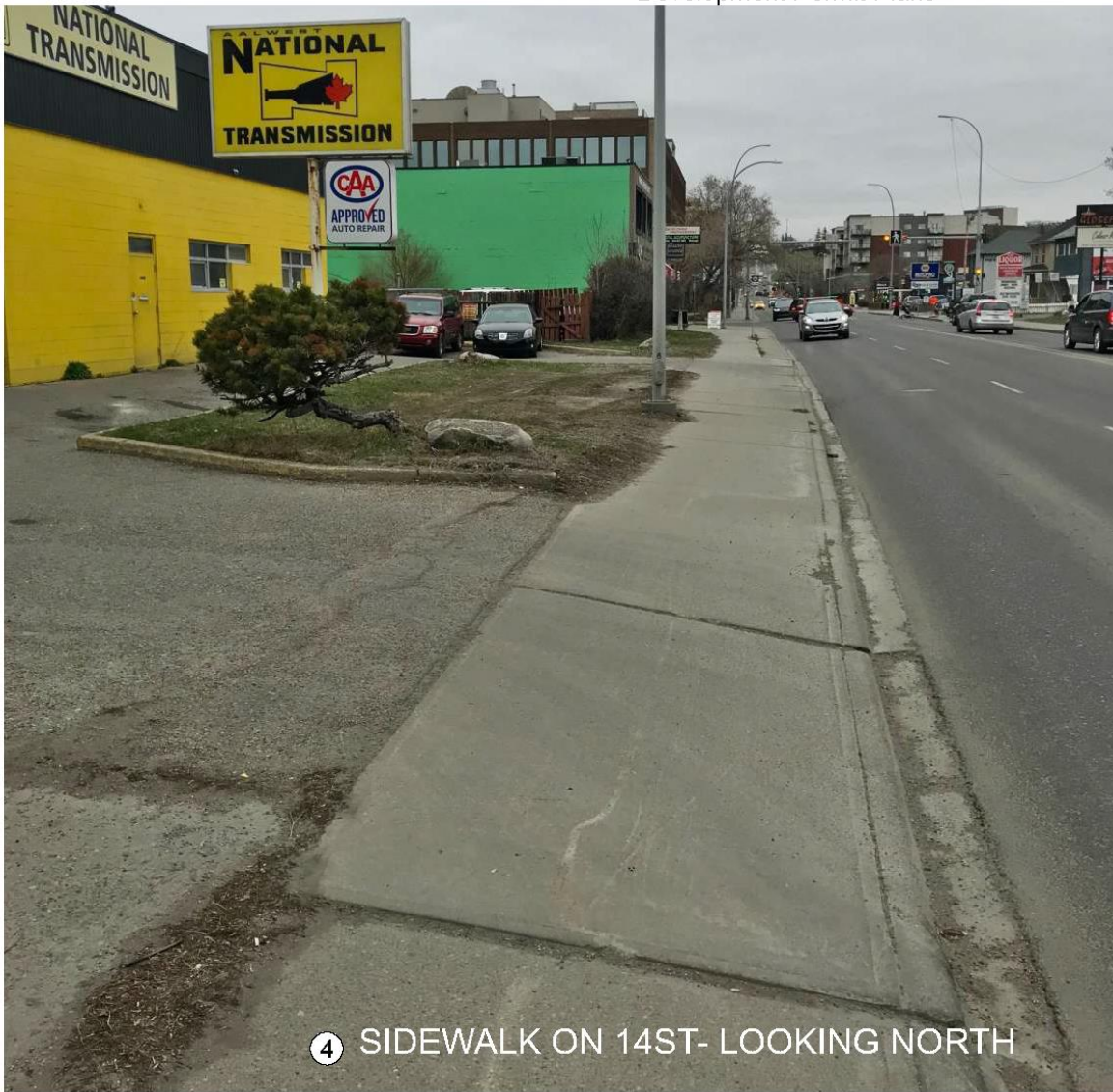
SCALE:

Development Permit Plans



riddell kurczaba **14 ST PHOTOS** SOLA 2020-08-31 DP - REVISION 1 A-3
#19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL10 OCGROW GROUP OF COMPANIES SCALE:

Development Permit Plans



④ SIDEWALK ON 14ST- LOOKING NORTH



⑤ SIDEWALK ON 14ST- LOOKING SOUTH

riddell kurczaba
#19-4592

SIDEWALK PHOTOS
211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL10

SOLA 2020-08-31
OCGROW GROUP OF COMPANIES

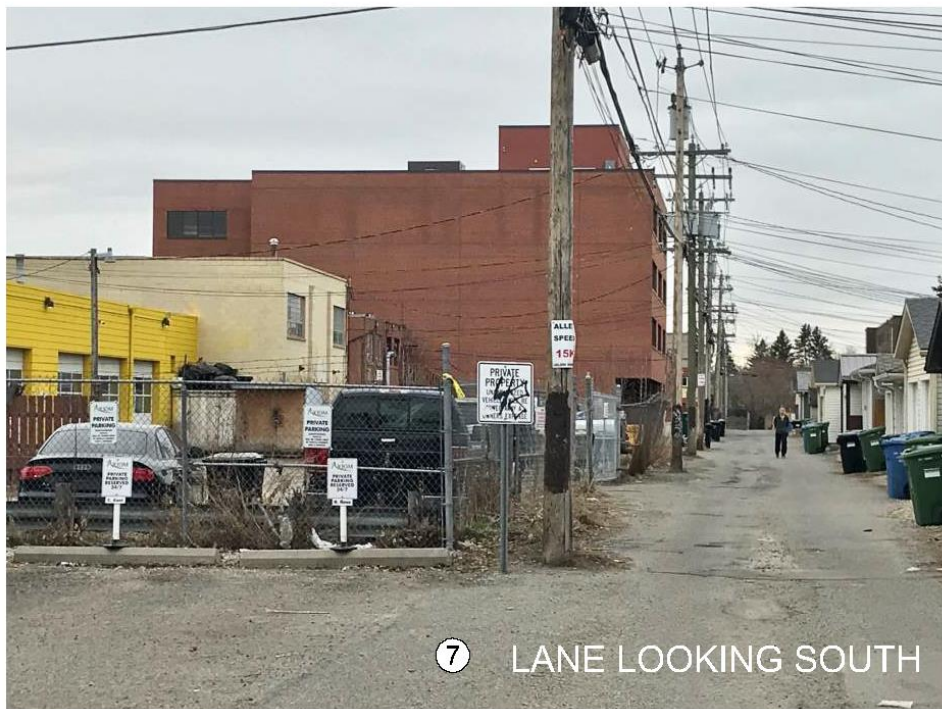
DP - REVISION 1
SCALE:

A-4

Development Permit Plans



⑥ LANE WEST VIEW OF EXISTING BUILDING



⑦ LANE LOOKING SOUTH



⑧ LANE LOOKING SOUTH

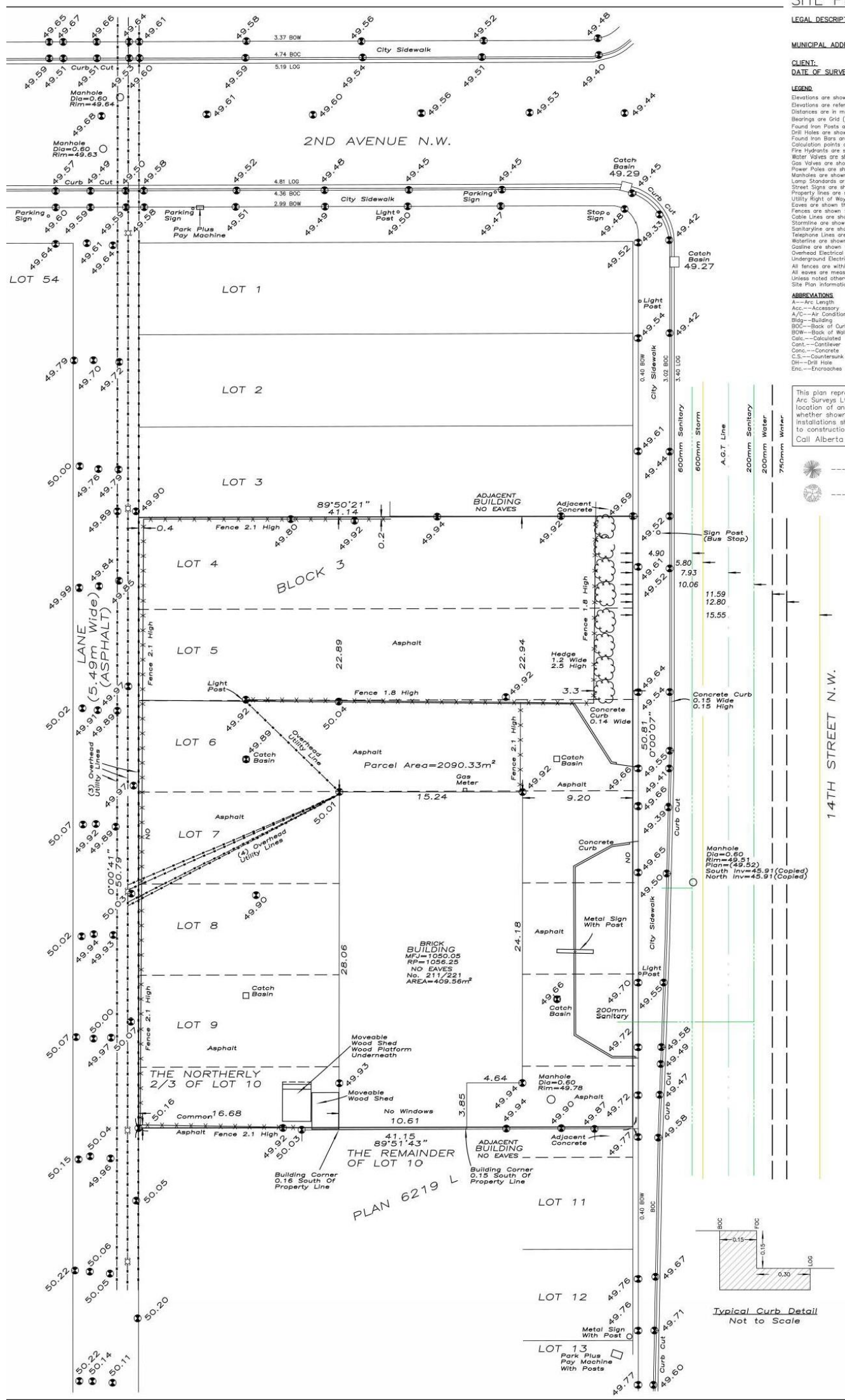


⑨ LANE LOOKING NORTH

riddell kurczaba **LANE PHOTOS** SOLA 2020-08-31 DP - REVISION 1 A-5
#19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL10 O C G R O W SCALE:
GROUP OF COMPANIES

Development Permit Plans

SITE PLAN



LEGAL DESCRIPTION: Lots 4-10
Block 3
Plan 6219L

MUNICIPAL ADDRESS: 211 - 221 14 Street N.W.
Calgary, Alberta

CLIENT: Ocgrow Group of Companies

DATE OF SURVEY: February 25th, 2020

LEGEND:
Elevations are shown thus: \bullet = 1000.00 Metres (Geodetic)
Elevations are referred to ASCM 80861 Elev=1050.47
Distances are in metres and decimal thereof.
Bearings are Grid (3TM-114W) and Derived from GNSS Observations.

Found Iron Posts are shown thus: \bullet
Found Iron Bars are shown thus: \bullet
Calculation points are shown thus: \bullet
Fire Hydrants are shown thus: \bullet
Water Valves are shown thus: \bullet
Gas Valves are shown thus: \bullet
Power Poles are shown thus: \bullet
Manholes are shown thus: \bullet
Street Signs are shown thus: \bullet
Property Lines are shown thus: \bullet
Utility Right of Way are shown thus: \bullet
Eaves are shown thus: \bullet
Fences are shown thus: \bullet
Cable Lines are shown thus: \bullet
Stormlines are shown thus: \bullet
Sanitarylines are shown thus: \bullet
Telephone Lines are shown thus: \bullet
Waterline are shown thus: \bullet
Gasline are shown thus: \bullet
Overhead Electrical lines shown thus: \bullet
Underground Electrical lines shown thus: \bullet

ABBREVIATIONS:
A---Arc Length
Acc---Accessory
A/C---Air Conditioner
Bldg---Building
BOC---Back of Curb
BOW---Back of Walk
Calc---Calculated
Cant---Cantilever
Conc---Concrete
C.C---Counterank
DH---Drill Hole
Enc---Encroaches
F.C---Face of Curb
F.F---Found
I---Iron Post
IB---Iron Bar
LOG---Lip of Gutter
M.A---Maintenance Access
Mx---Mark
R---Radius
O.D---Overland Drainage
Reg---Registration
Rel---Retaining
R/W---Right of Way
W/O---Walkout Statement
W.W---Wetland Wall

This plan represents the best information at the time of survey. Arc Surveys Ltd. and its employees take no responsibility for the location of any underground conduits, pipes or other facilities whether shown on or omitted from this plan. All underground installations should be located by the respective authorities prior to construction.
Call Alberta One-Call: 1-800-242-3447

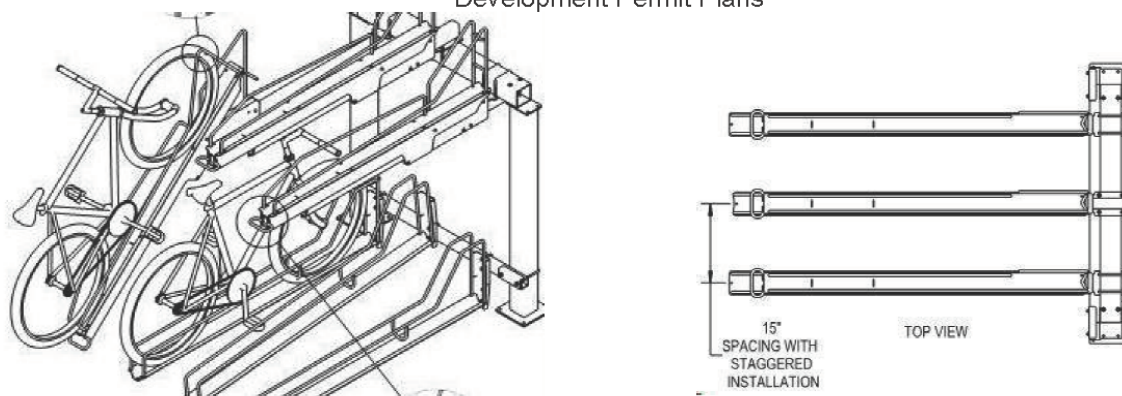
--- denotes Coniferous
--- denotes Deciduous

Demolition- existing on site building, shed, surface paving, catch basins, manholes, fences, signage as identified on this survey drawing will be removed as part of the building permit process.

Surveyed: JY Drawn: JW/JW Checked by: RB Scale: 1:200 File No.: 200386

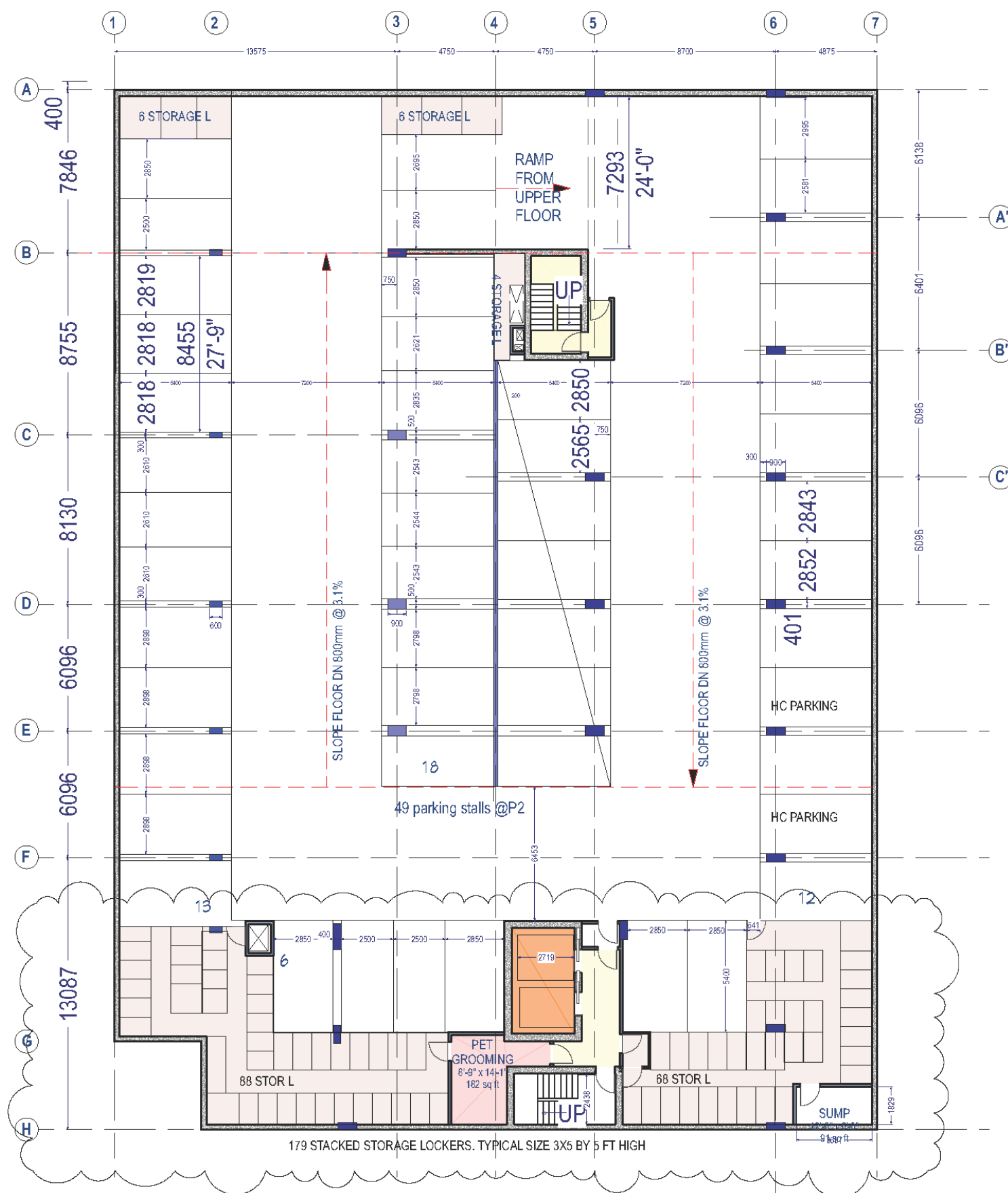
riddell kurczaba **SURVEY** SOLA 2020-08-31 DP - REVISION 1 A-6
#19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL 10 O C G R O W GROUP OF COMPANIES SCALE: NTS

Development Permit Plans

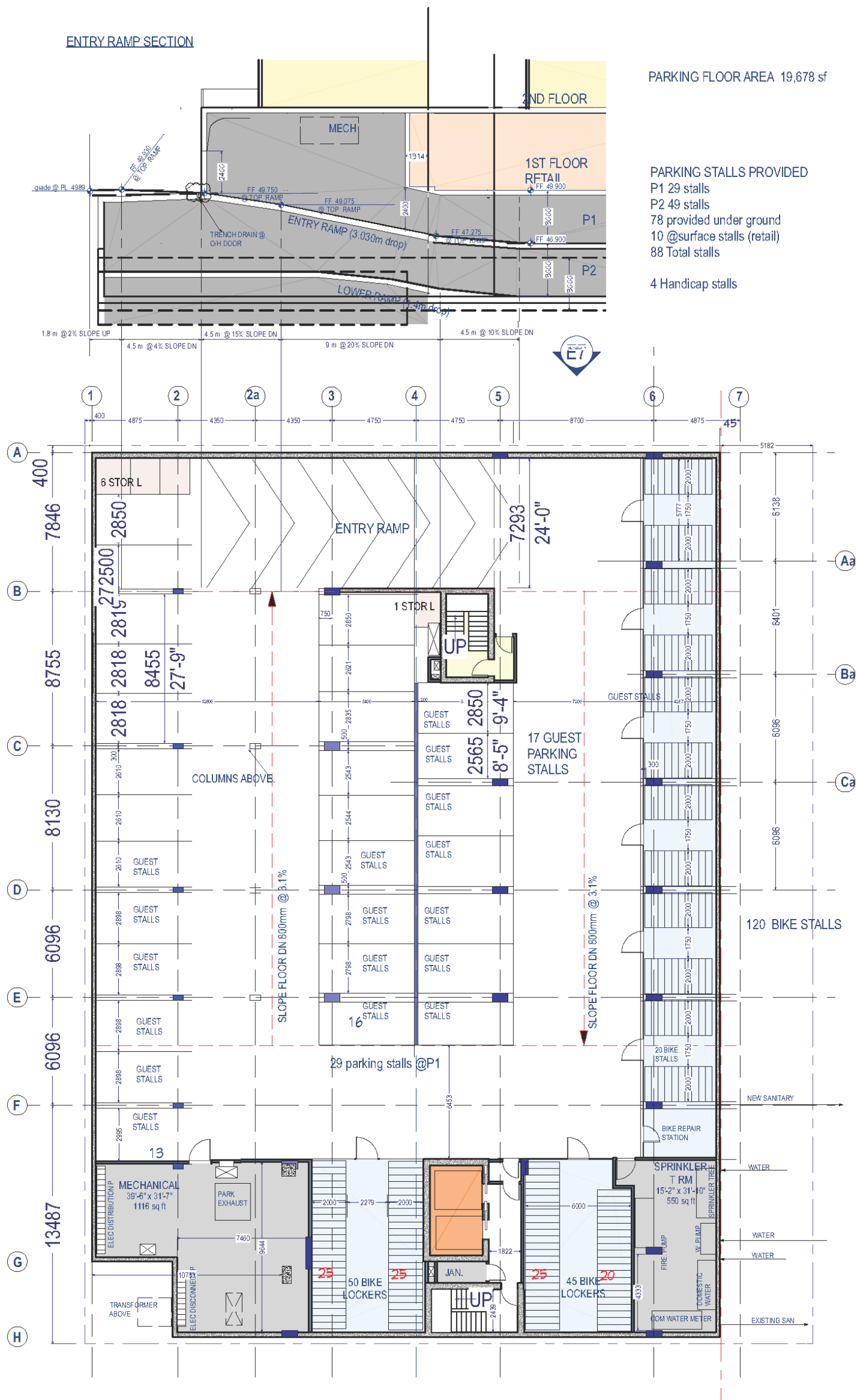


BIKE PARKING CLASS 1 - MADPAX horizontal stack
215 BIKE STALL ON P1 @ RATIO OF 1.25 PER UNIT
CLASS 2 18 surface stalls

the stalls are on P1 provides easy access from the paved lane down the auto ramp.



Development Permit Plans



riddell kurzaba

#19-4592



PARKING 1 PLAN

211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL 10

SOLA

2020-08-31

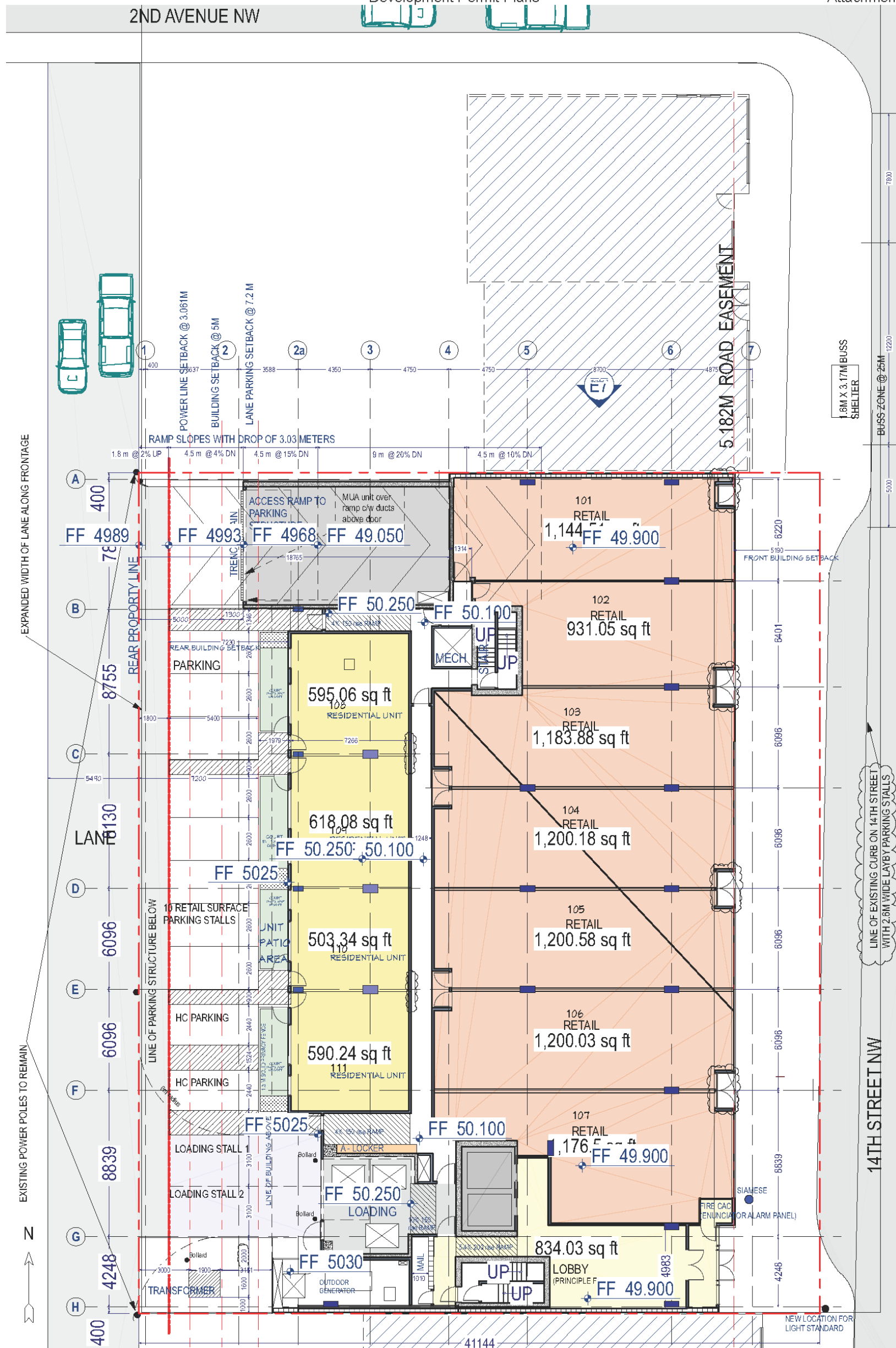


DP - REVISION 1

SCALE: 1:100
(11/17 1:200)

A-8

Development Permit Plans



riddell kurzaba **GROUND FLOOR PLAN** SOLA 2020-08-31 DP - REVISION 1 A-9
 #19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL 10 OCGROW GROUP OF COMPANIES SCALE: 1:100 (11/17 1:200)

CPC2020-1212 - Attach 2
ISC: UNRESTRICTED

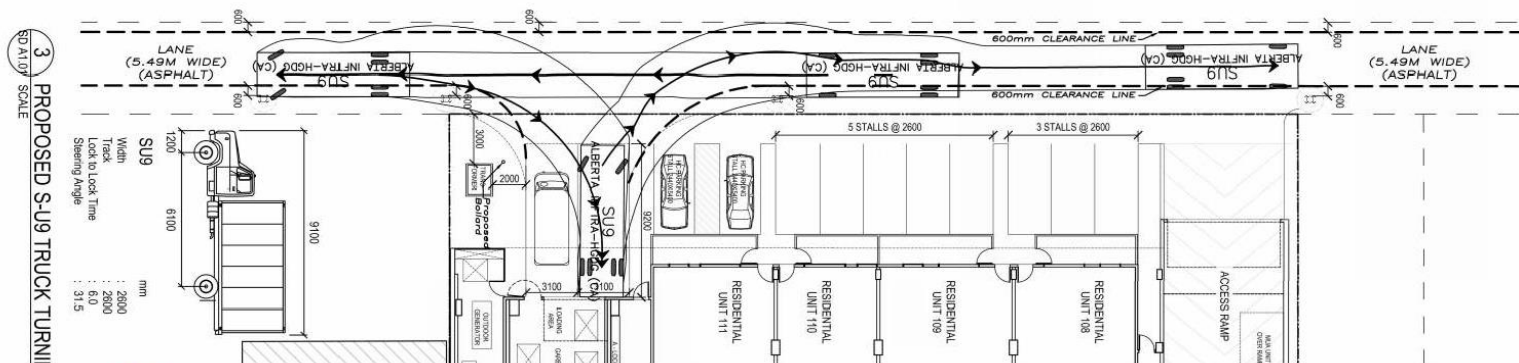
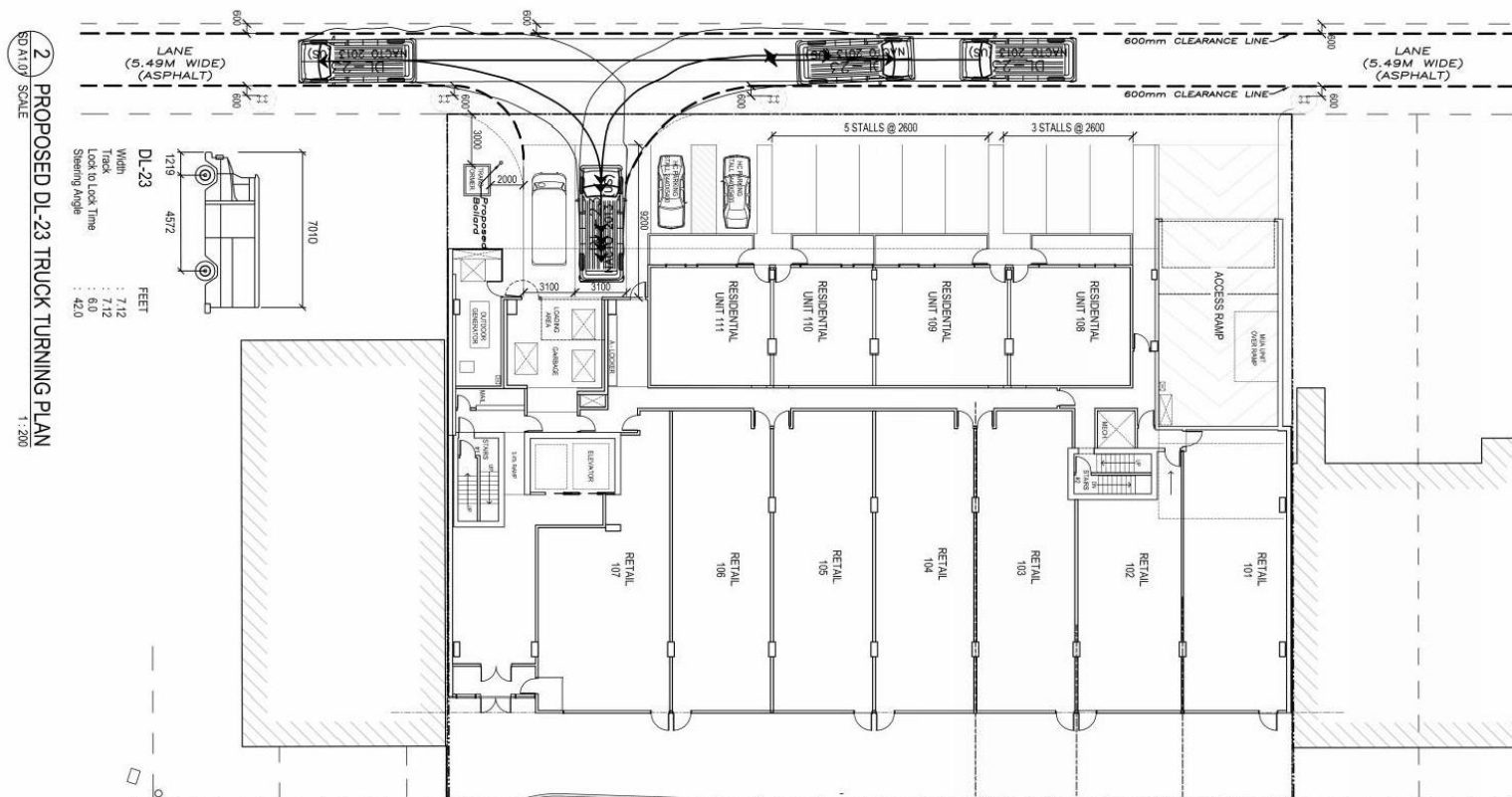
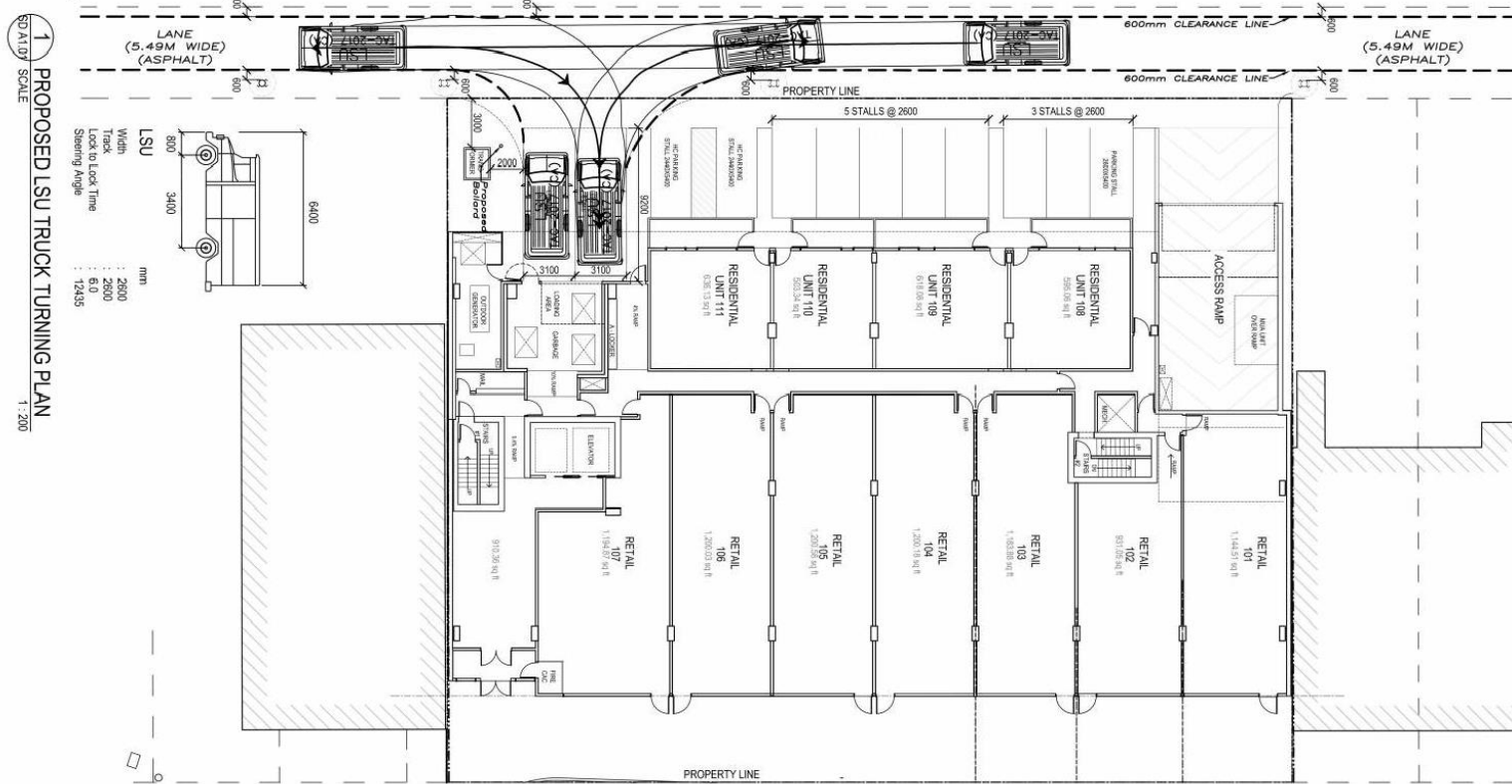
Development Permit Plans

This sweep path analysis reviews 3 truck sizes. From large to small SU9 9.1m, DL23 7.01m and LSU 6.4m. The loading sweep paths for the 6.4 Meter long truck (TAC 2017 LSU) is the smallest of the trucks but represents the practical maximum size that can be accommodated. As the units in the building are very small the recommended moving vehicle size as defined by U-haul is a 17ft truck, The 6.4 meter truck fulfills the loading requirements of the residential units and of the retail which will primarily be loaded with delivery vans.

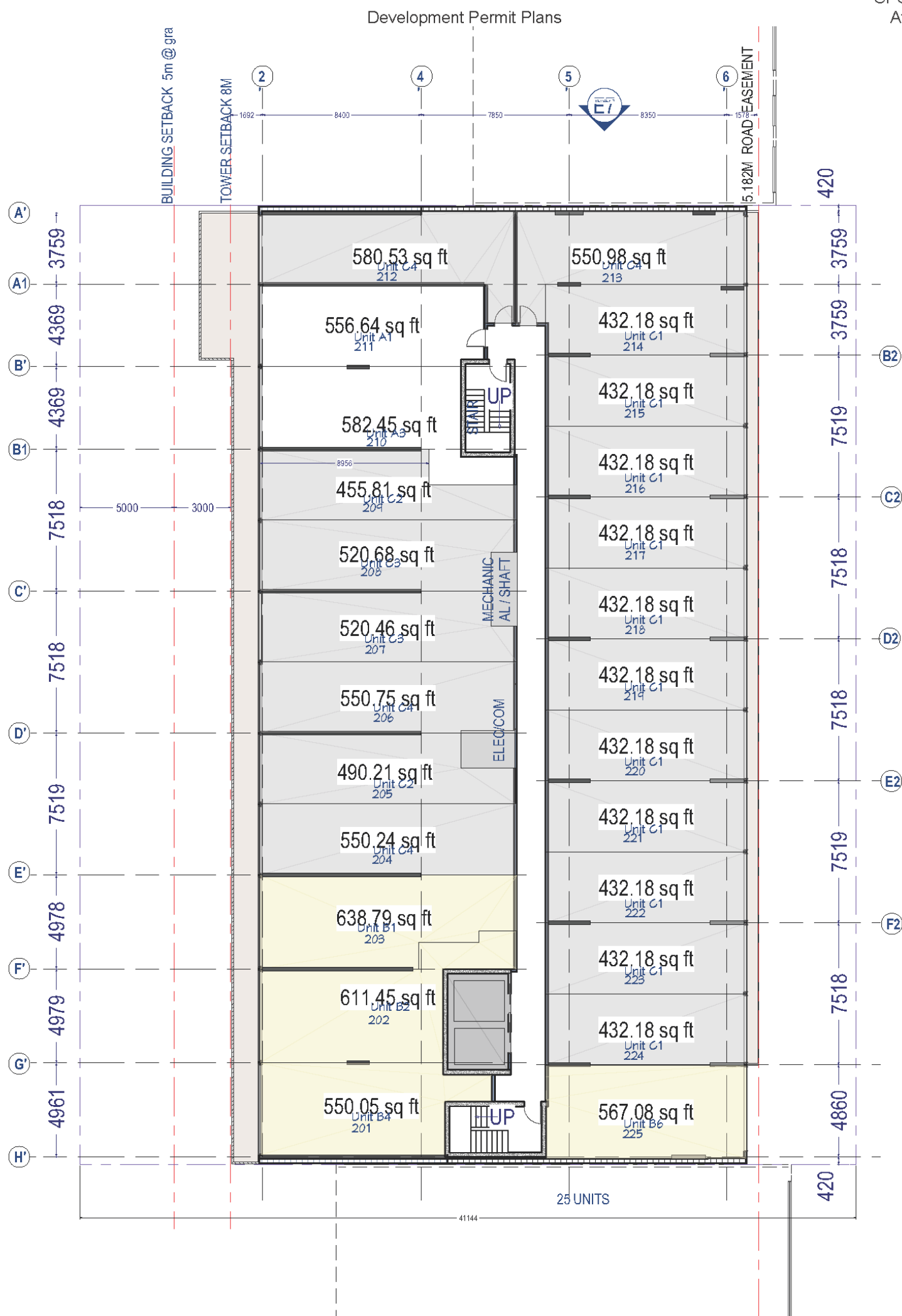
The midsize truck 7.01m also works with modest incursions into the .6m buffer zone. This truck size would have been approved in previous regulations prior to the .6m buffer requirement.

The larger truck does not work under any scenario. Due to the location of the power poles and their distance apart there is no loading sweep path that functions. This version has the truck failing on one of the power poles. We are not assuming that 2 medium or large trucks would load at the same time. We view that one loading stall could have been sought for and accepted as such the 2 stalls provide good flexibility for multi-use such as a staging area for garbage as well as for loading.

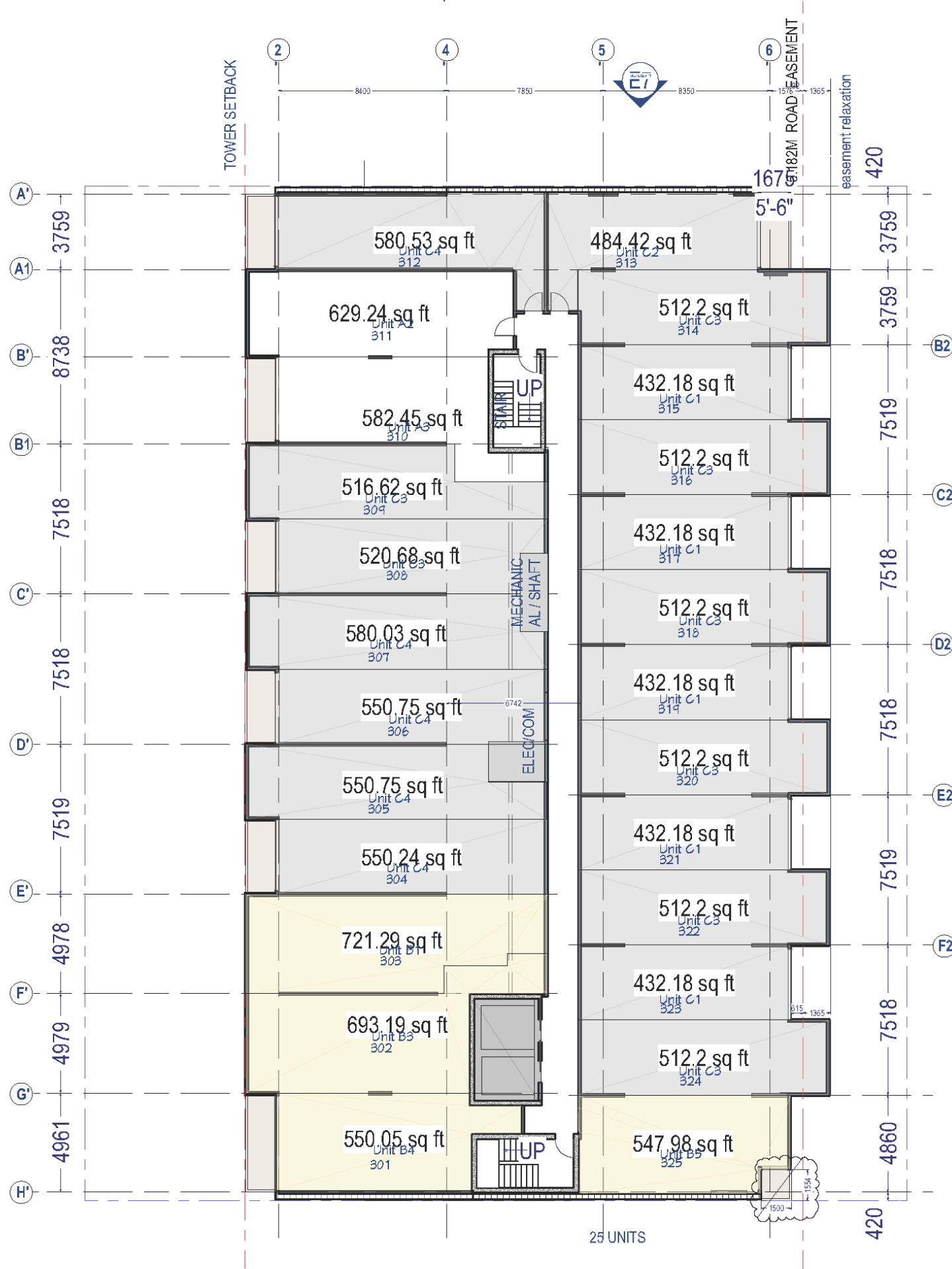
On this site as in all the new developments in Kensington with narrow lanes there are challenges with loading. This proposal is consistent with the loading solutions recently approved in near by developments. The rule to load with the sweep path completely on site is not a reasonable requirement. The implications would eliminate much of the ground floor development foot print. We are asking for a relaxation consistent with others to allow the car parking stalls to back into the lane and to allow trucks using the two loading stalls to back into the lane as part of the normal course of access.

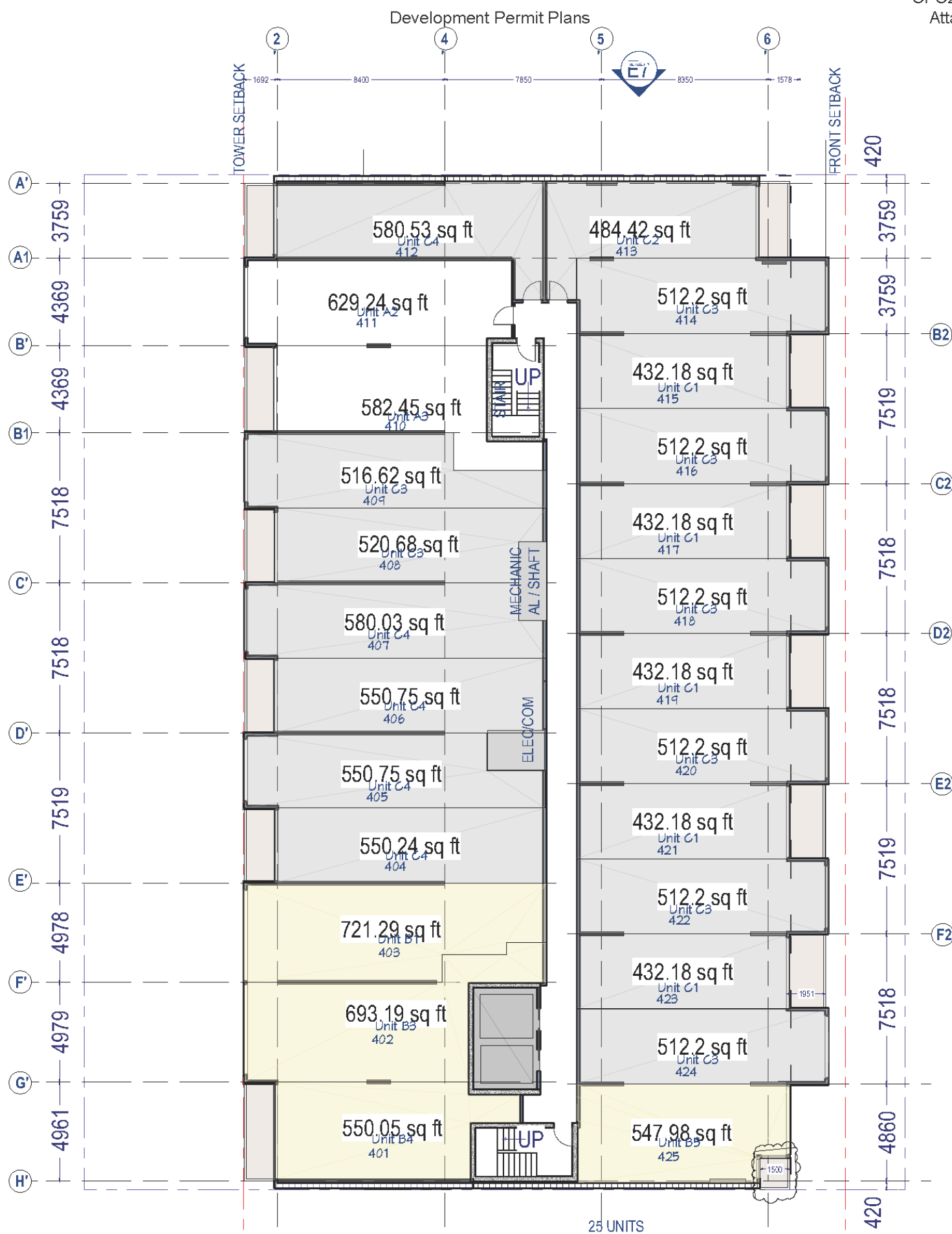


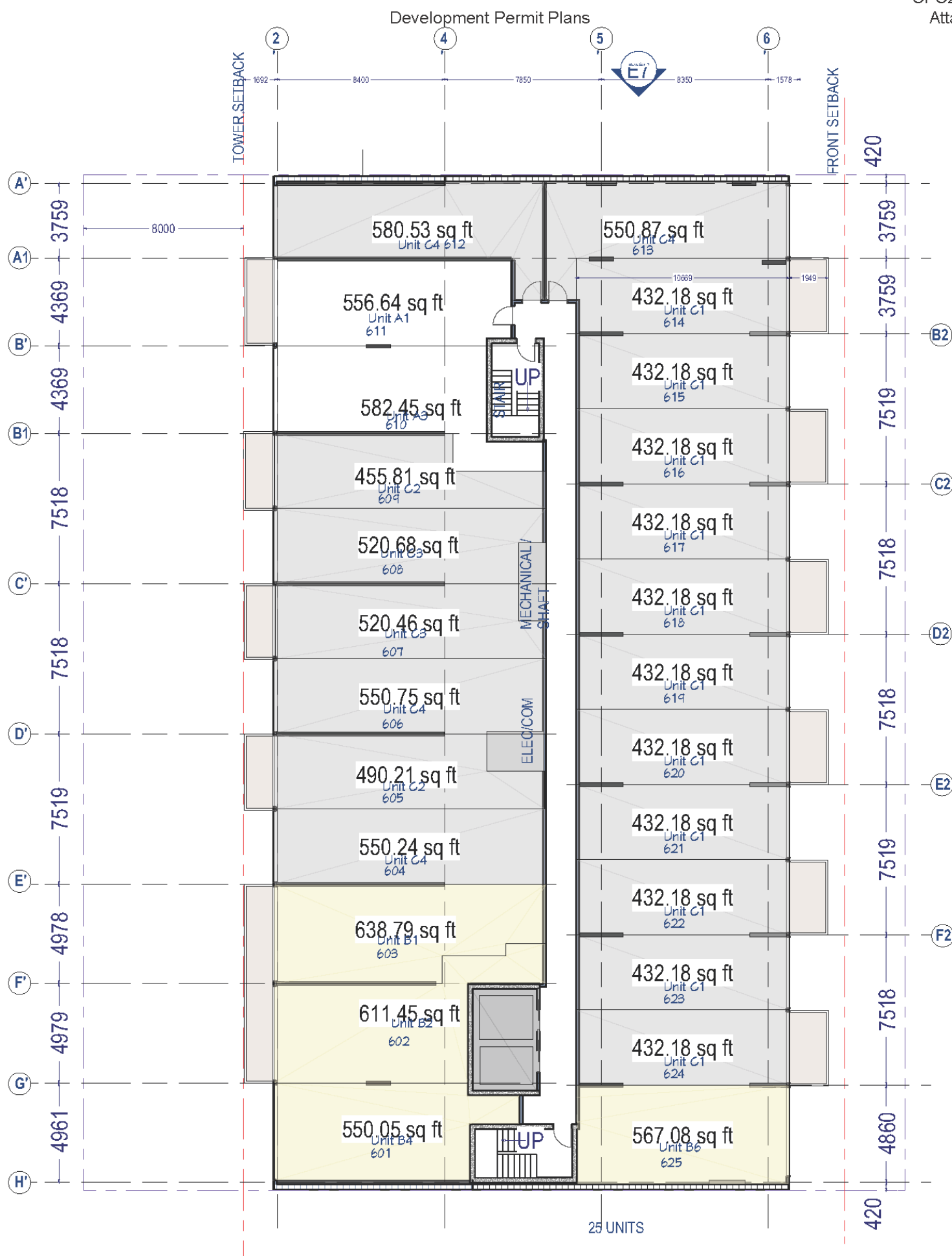
riddell kurzczaba **LOADING SWEEP PATHS SOLA** 2020-08-31 **DP - REVISION 1 A-10**
 #19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL 10 **SCALE: NTS**

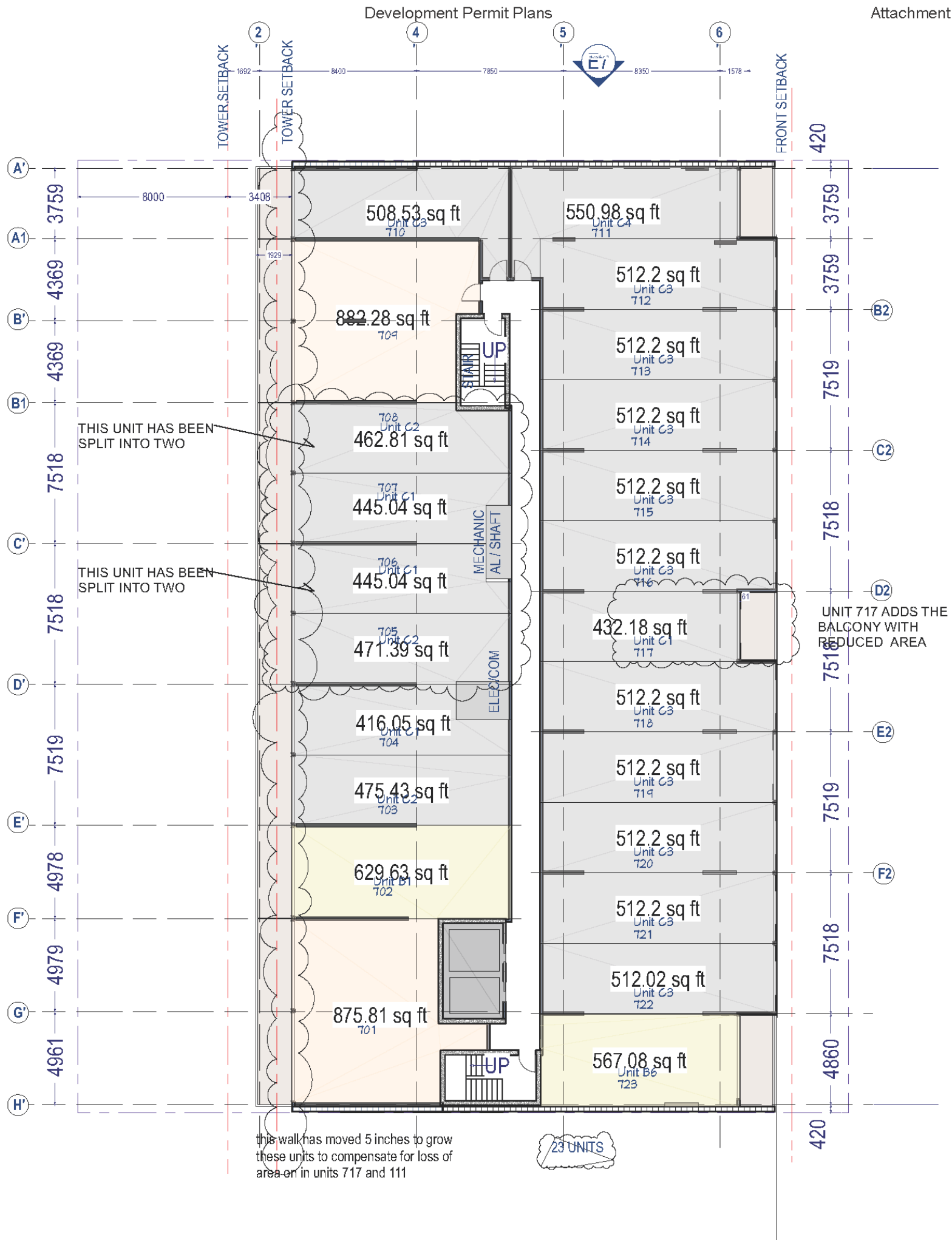


Development Permit Plans

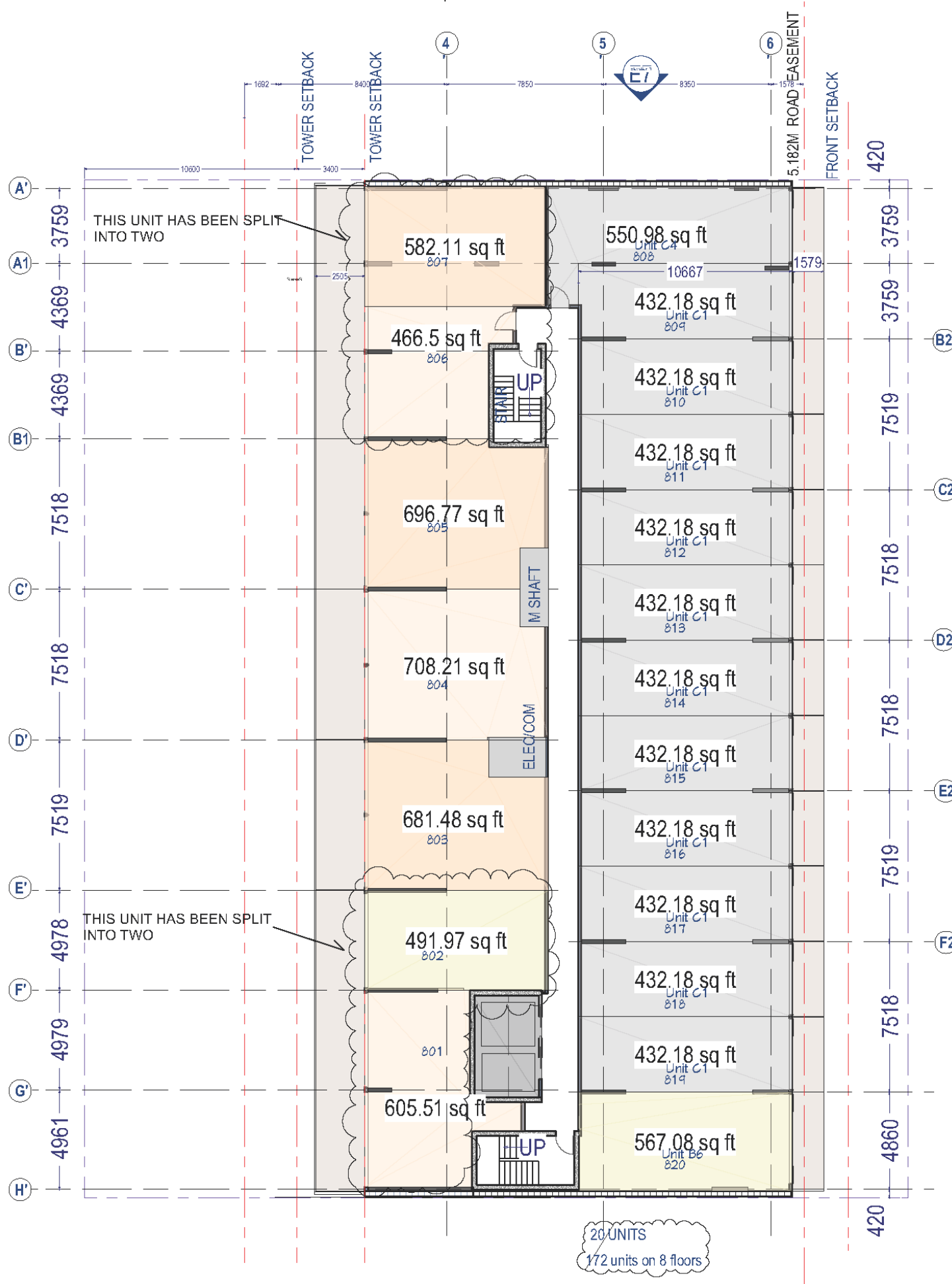


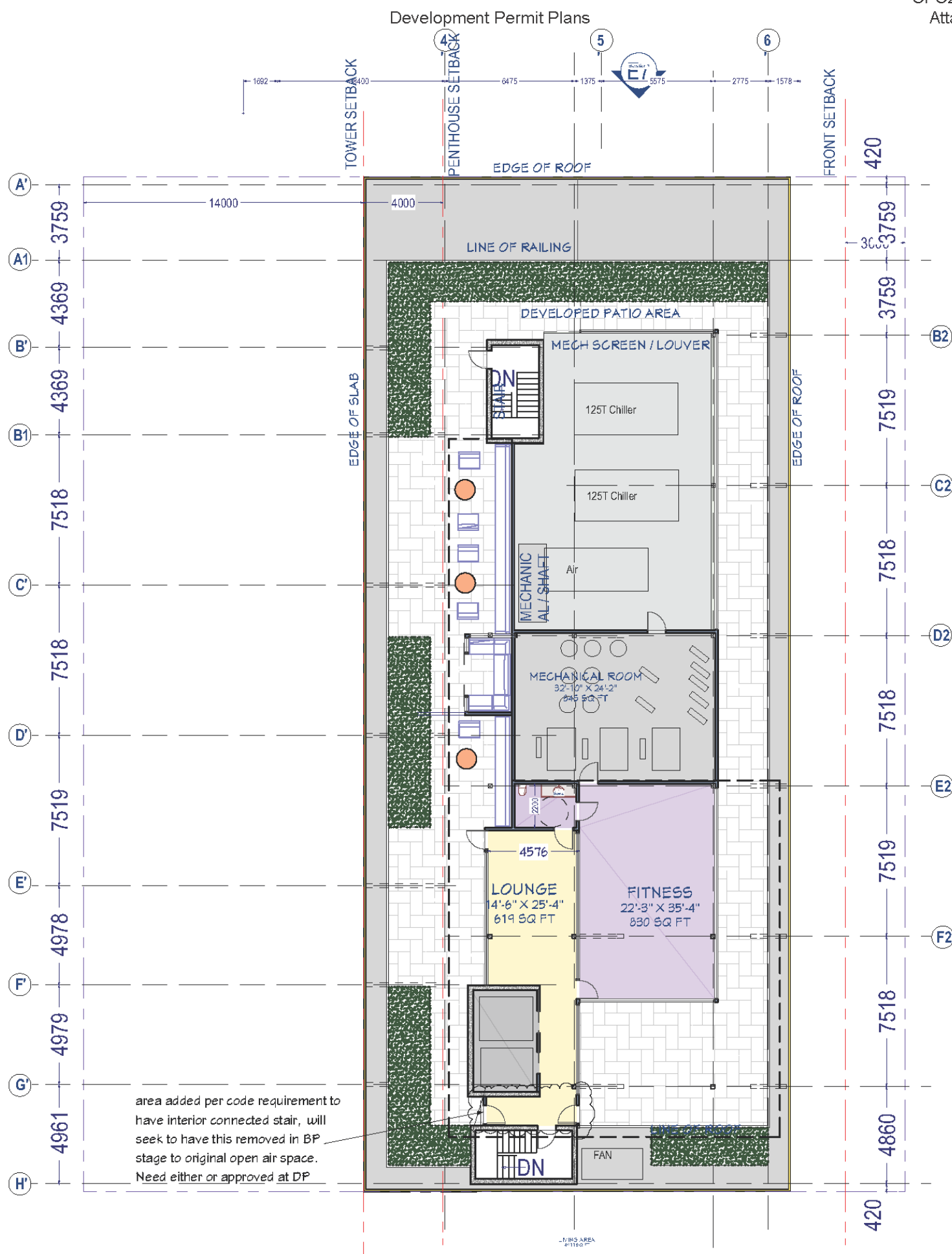




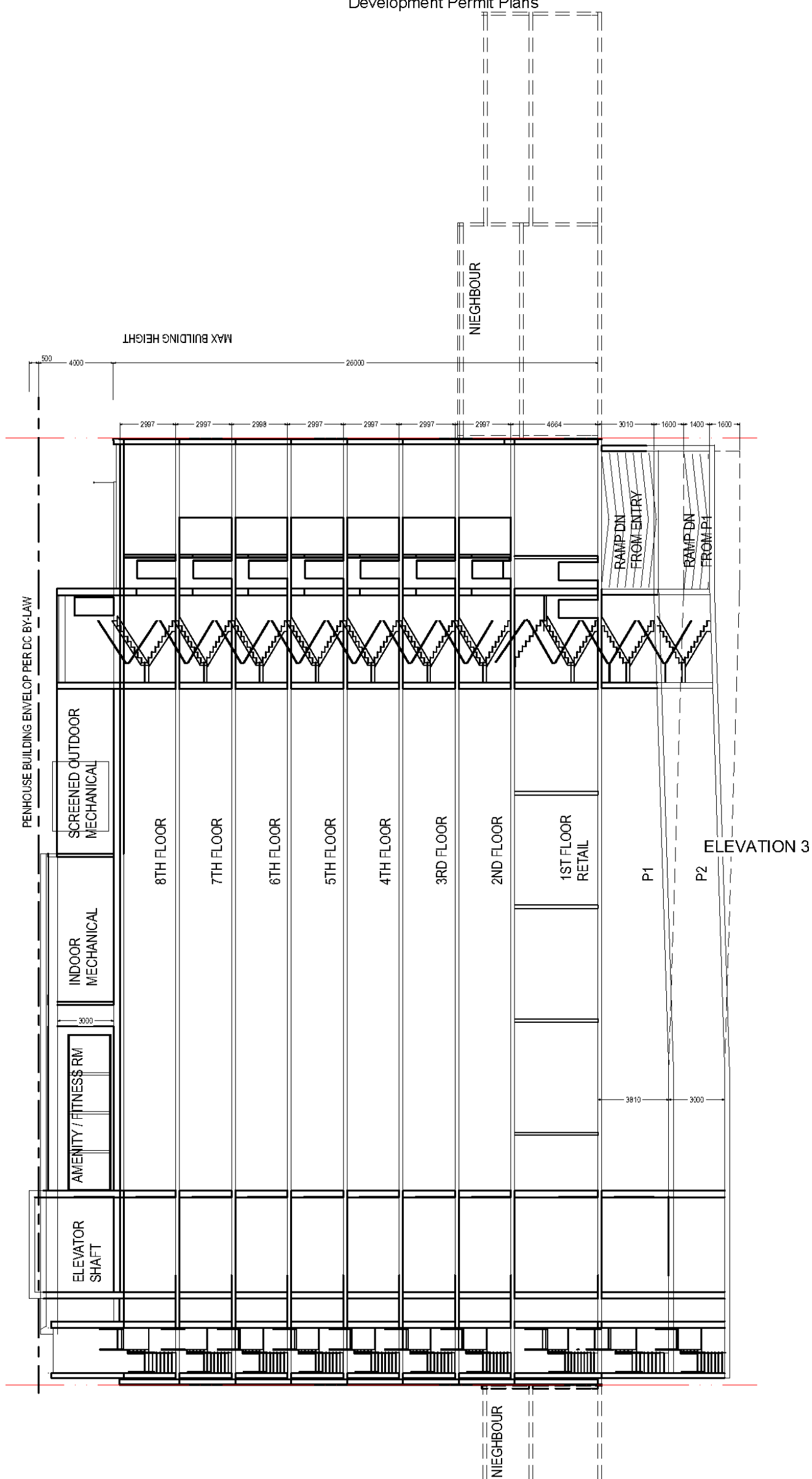


Development Permit Plans





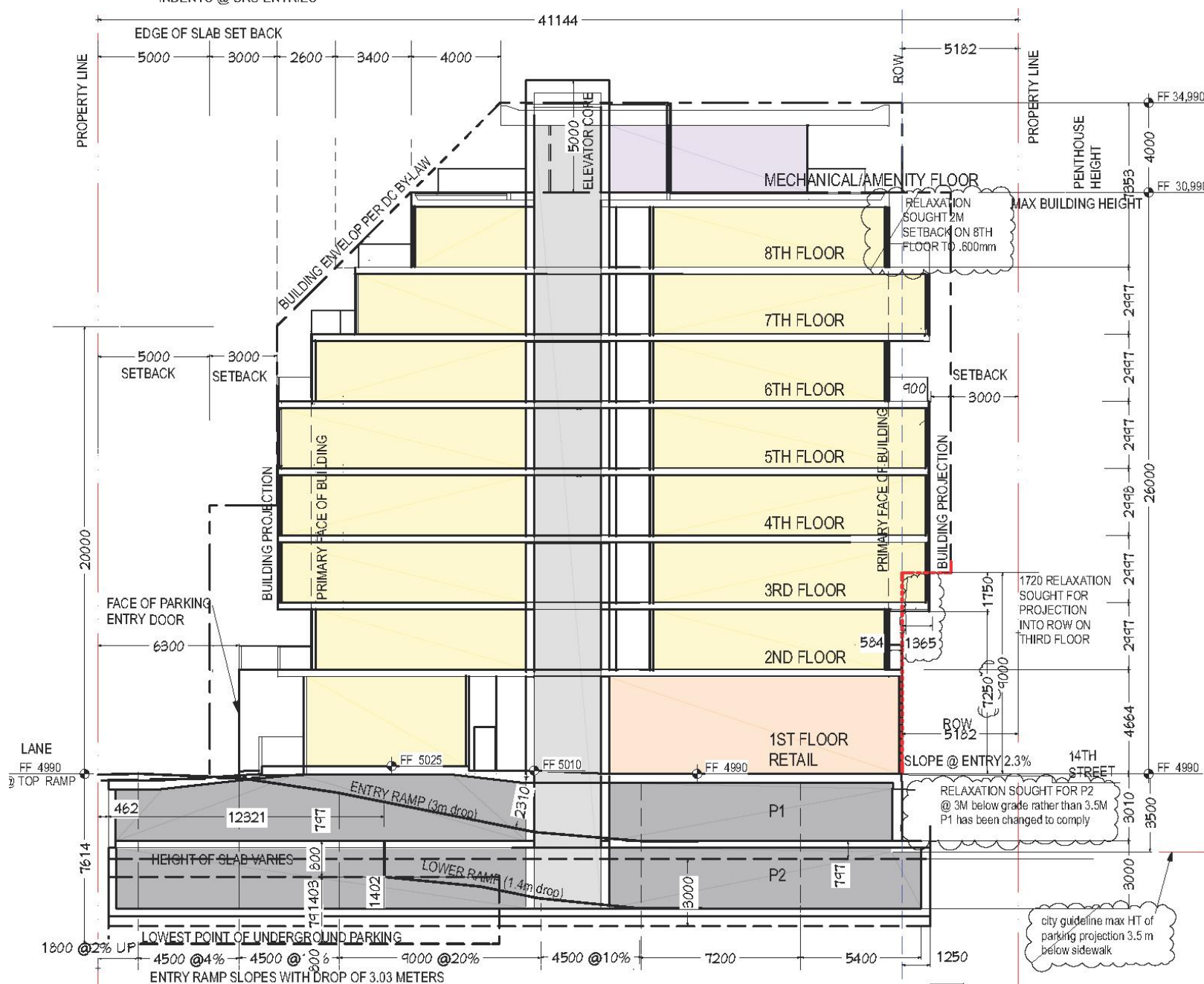
Development Permit Plans



Development Permit Plans



INDENTS @ CRU ENTRIES



riddell kurczaba 2020-08-31 **EAST WEST SECTION** SOLA **DP - REVISION 1** **A-19**
 #19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL10 OCGROW GROUP OF COMPANIES SCALE: 1:100 (11/17 1:200)

CPC2020-1212 - Attach 2
ISC: UNRESTRICTED

Development Permit Plans



Development Permit Plans



EAST ELEVATION SIGNAGE



WEST ELEVATION

FINISHES

- 1 GREY CEMENT PANEL - FLAT FINISH AND GLOSS FINISH
- 2 WHITE CEMENT PANEL - FLAT FINISH AND GLOSS FINISH
- 2A WHITE PORCELAIN PANEL - GLOSS FINISH
- 3 BRICK RED CEMENT PANEL - FLAT FINISH AND GLOSS FINISH
- 4 WHITE OR GREY LOUVRE
- 5 SLAB RAIL - NOT FINISHED
- 6 CONC. 6" SPLIT CASE, 400mm ht
- 7 SLAB NOT FINISHED

SIGNAGE NOTES

CRU SIGNAGE TO BE RAISED LETTERING ON TOP OF MIDDLE WINDOW LIGHT, CONTINUOUS HORIZONTAL GRILL TO BE INSTALLED OVER WINDOW C/W 2 BANDS TO SUPPORT LETTERING. LETTERING TO BE 500mm HIGH

SIGNAGE TO BE ALSO LOCATE ABOVE THE TWO REAR RETAIL ENTRY DOORS

Development Permit Plans



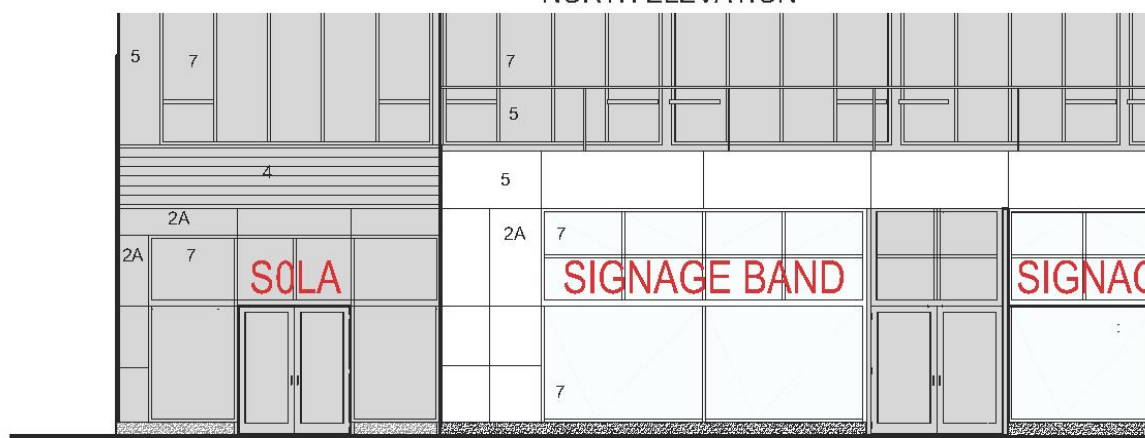
SOUTH ELEVATION



NORTH ELEVATION

FINISHES

- 1 GREY CEVENT PANEL - FLAT FINISH AND SLGSS FINISH
- 2 WHITE CEVENT PANEL - FLAT FINISH AND SLGSS FINISH
- 2A WHITE PORCELAIN PANEL - SLGSS FINISH
- 3 BRICK RED CEVENT PANEL - FLAT FINISH AND SLGSS FINISH
- 4 WHITE OR GREY LOUVRE
- 5 SLASS RAIL - NOT FINISH
- 6 CONC. or Sinterstone 400mm x 400mm
- 7 SLASS NOT FINISH



riddell kurczaba
#19-4592

ELEVATIONS

211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL10

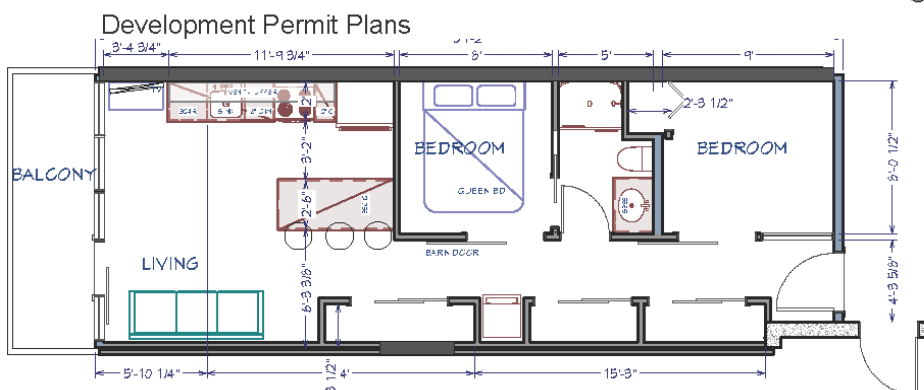
SOLA 2020-08-31
O C G R O W
GROUP OF COMPANIES

DP - REVISION 1

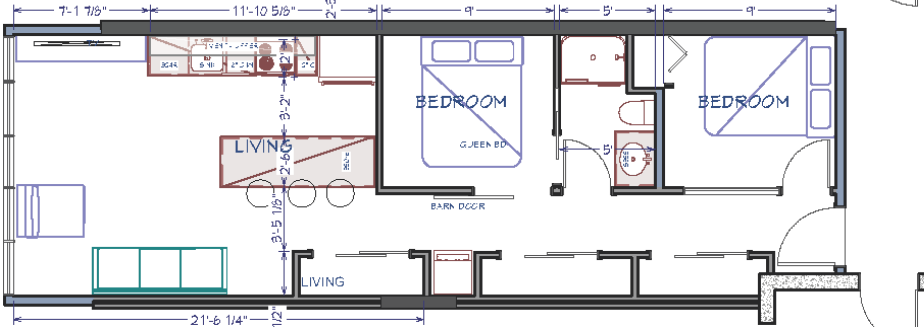
SCALE: 1:100
(11/17 1:200)

A-22

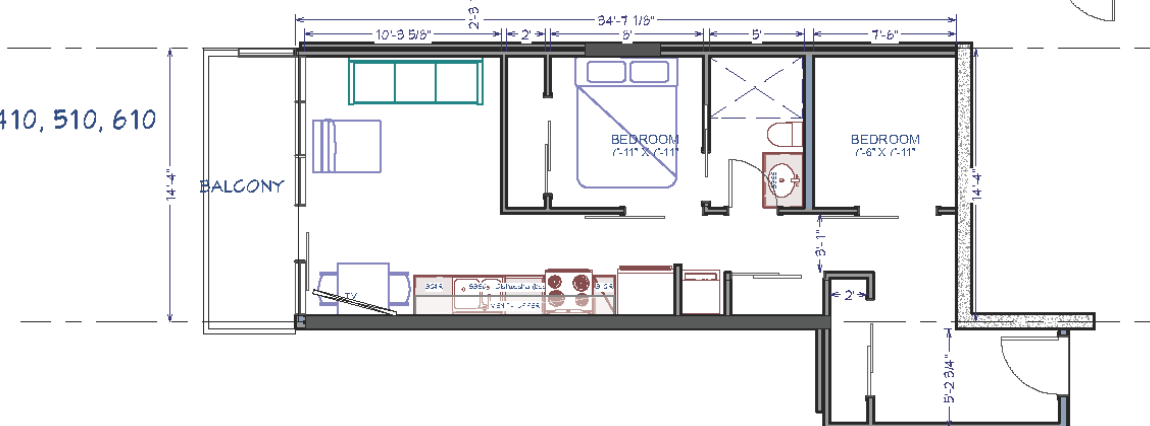
A1 556sf
units - 211, 611



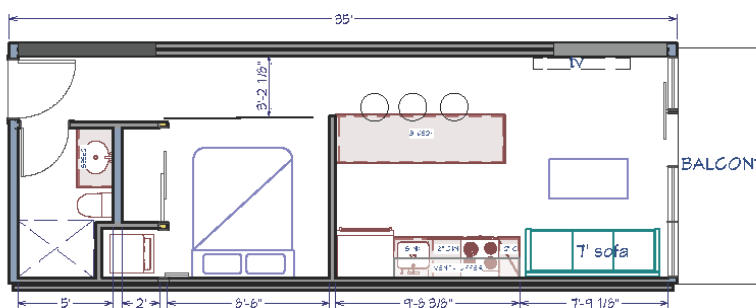
A2 629sf
units 311, 411, 511



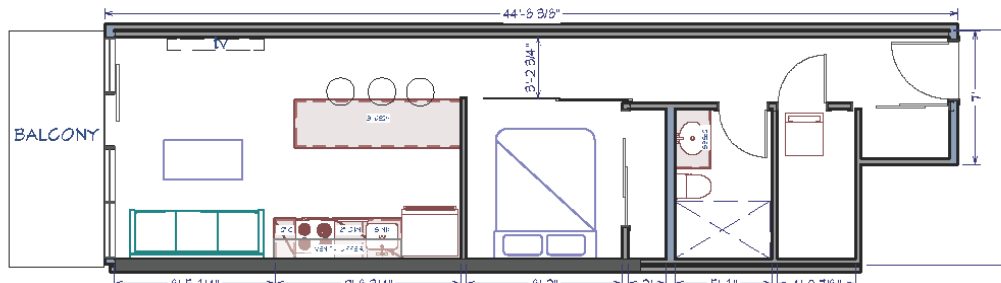
A3 582 sf
units 210, 310, 410, 510, 610



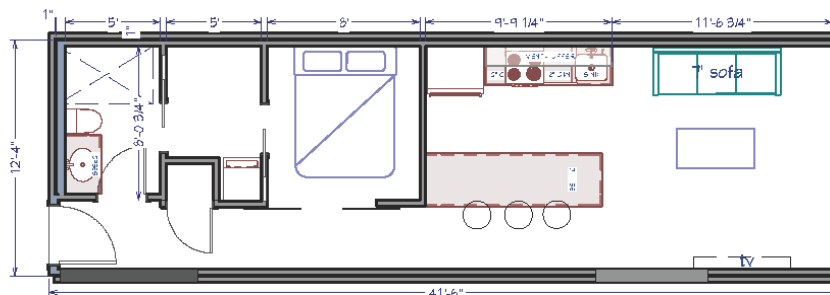
C-1 432sf
49 units



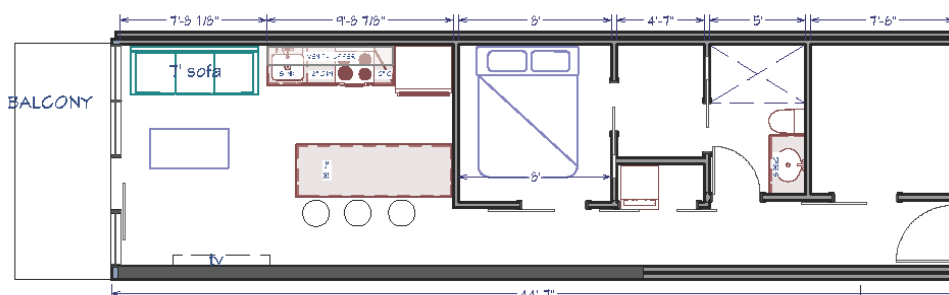
C-2 490sf
8 units



C-3 512sf
40 units



C-4 550sf
25 units
5 units @ 580 sf

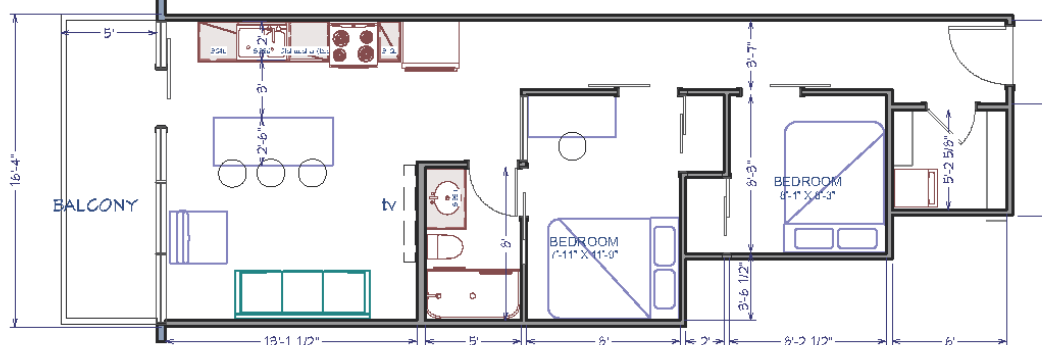


ENTRY OF
ALTERNATE
PLAN

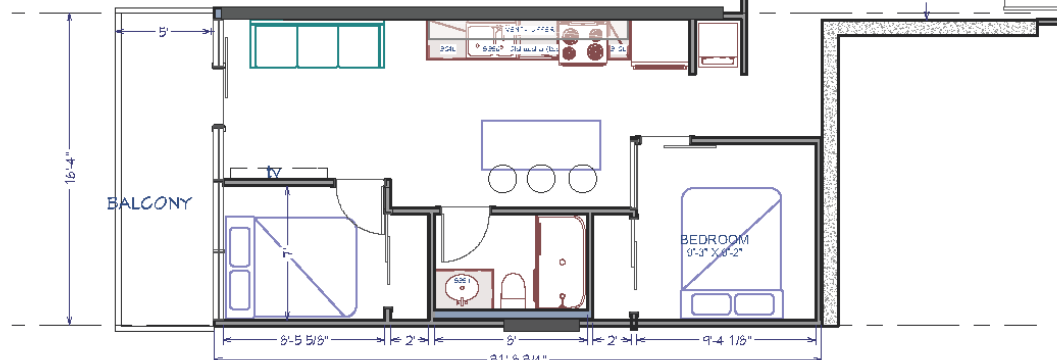
Development Permit Plans

B1 638sf
units 203, 603,

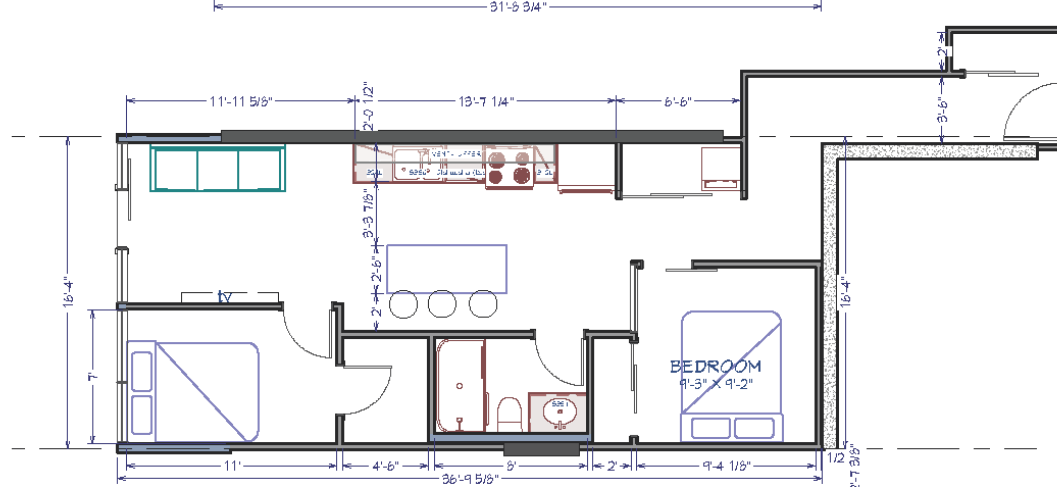
B1 721sf
units 303, 403, 503
this unit adds the balcony space



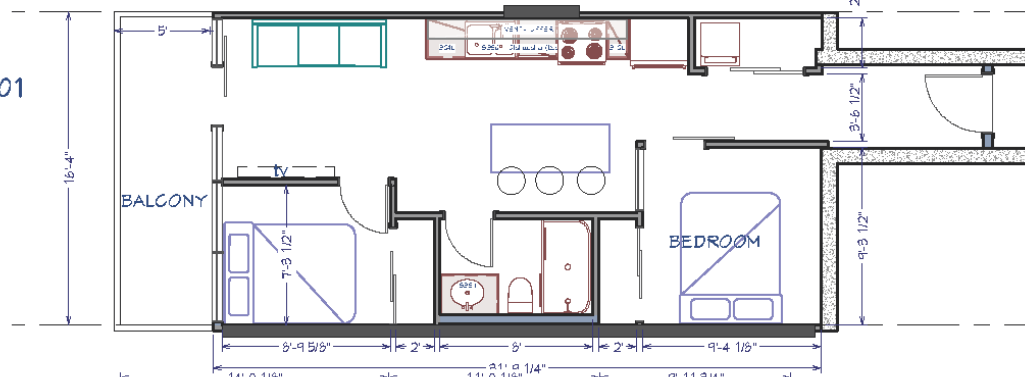
B2 611sf
UNITS 202, 602



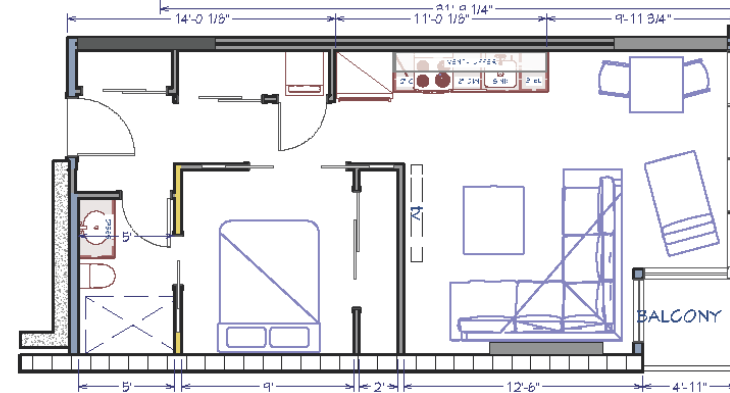
B3 693 sf
UNITS 302, 402, 502



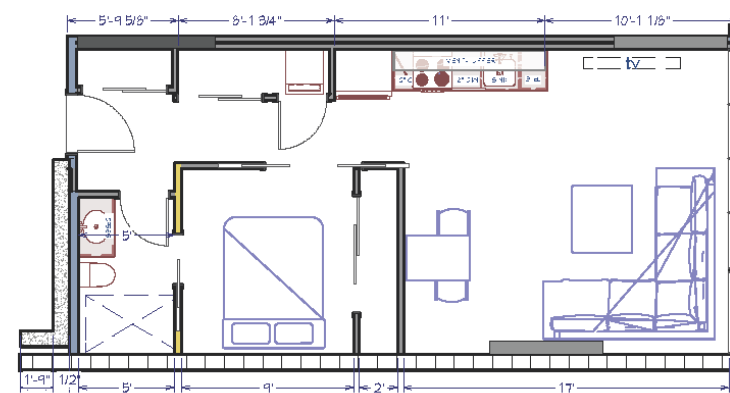
B4 550sf
UNITS 201, 301, 401, 501, 601



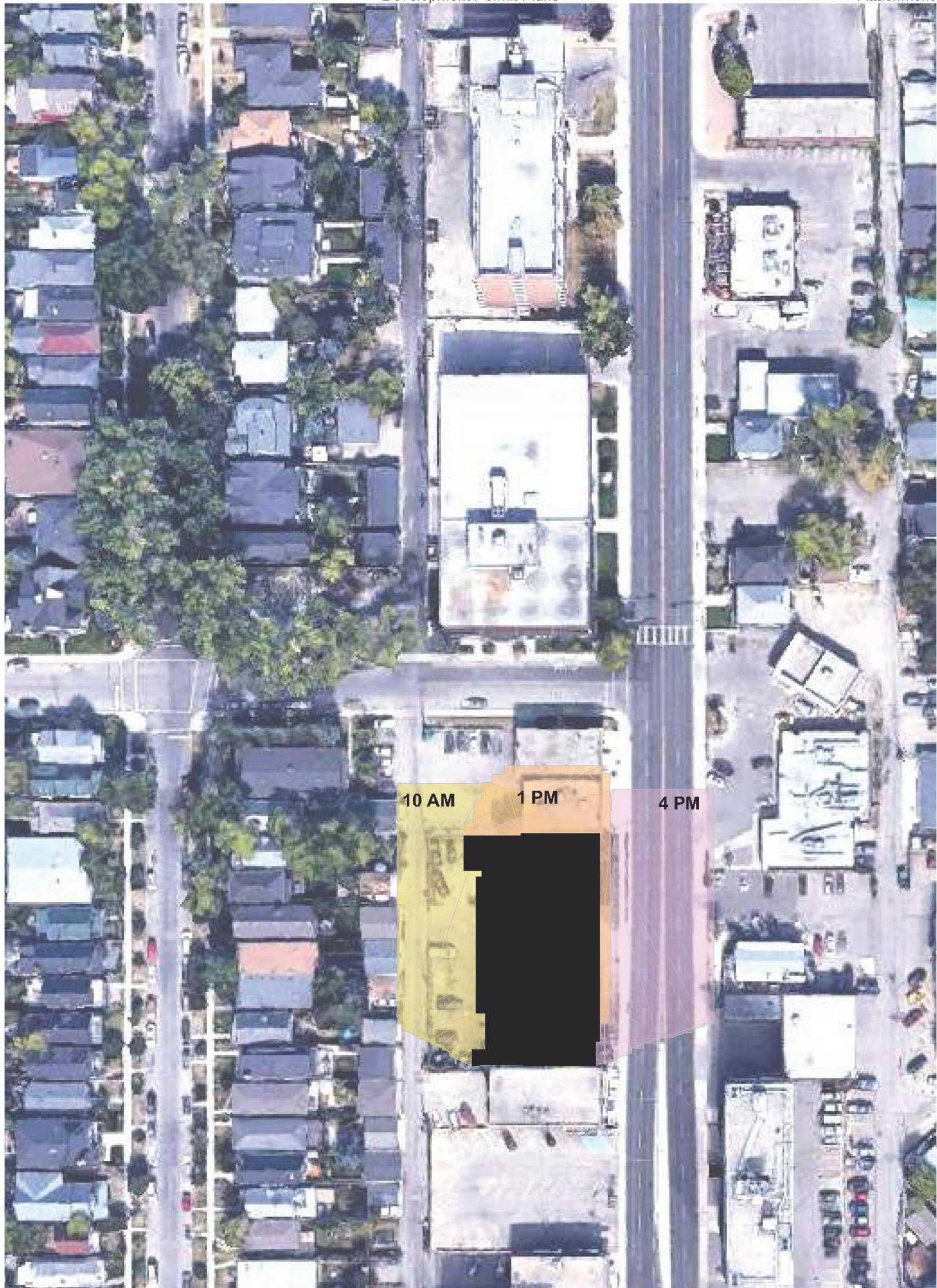
B5 541sf
units 325, 425, 525



B6 567sf
unit 225, 625, 721, 818



Development Permit Plans



The June 21 shadow created by the proposed building on neighbouring residential to the west is marginally less than the shadow the ARP envelope would create. This is achieved by moving the building as close to 14th street as possible and stepping the building.

Development Permit Plans



The March 21 shadow created by the proposed building on neighbouring residential to the west is marginally more than the shadow the ARP envelope would create. This means that for more than half of the year the shadow created by the building is equivalent to the shadow the ARP envelop would create.

Development Permit Plans



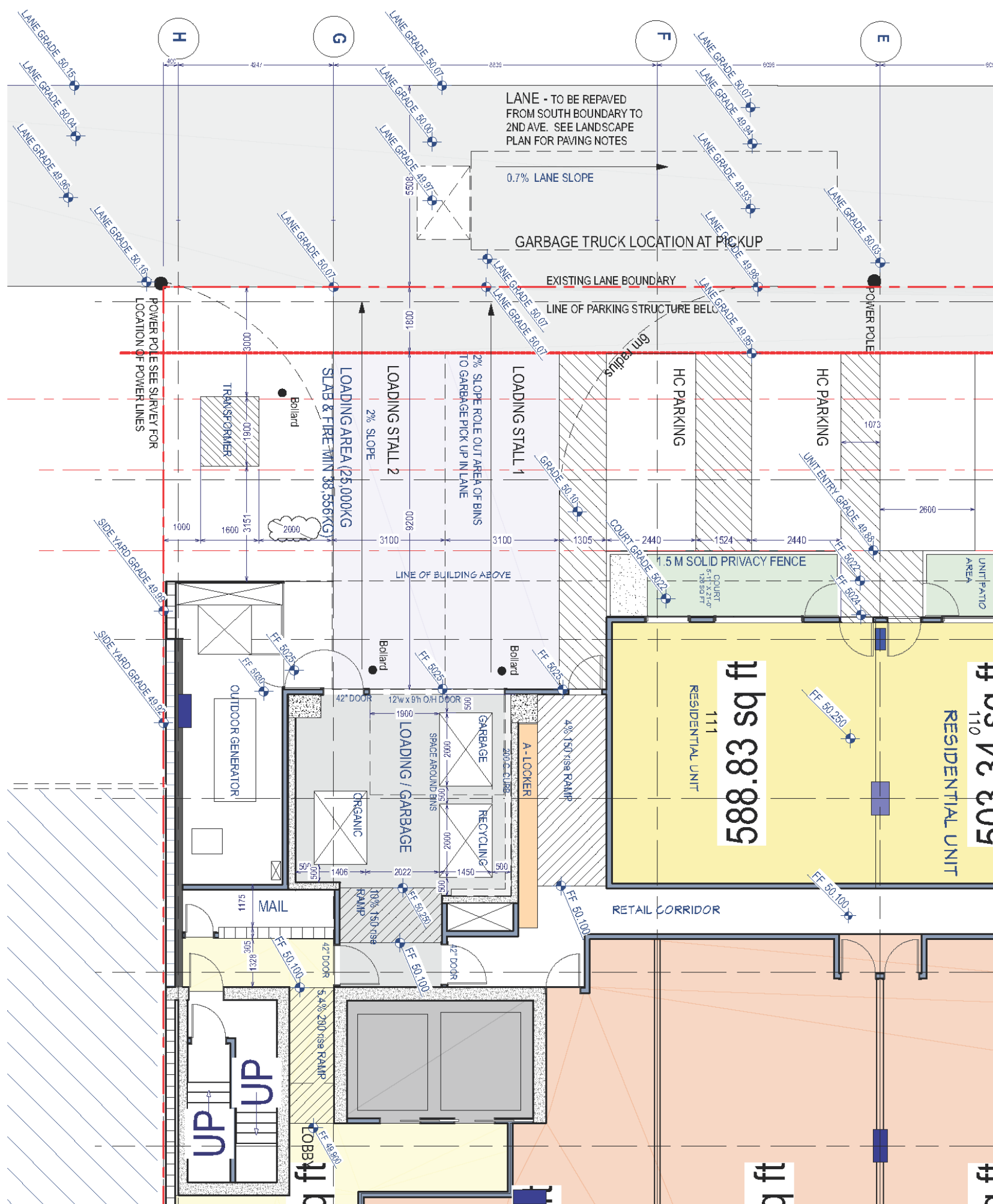
The December 21 shadow created by the proposed building on neighbouring residential to the west is more than the shadow the ARP envelope would create. However for those residences immediately across the lane the impact is still marginally less than the ARP shadow due to the building steps. The shadow impact is greater on lots further away than immediate neighbours. It is meaningful to keep in perspective that the winter sun is predominantly in the south, as such the real impact of shadow created by this building is to the north of the site on 14th street and on other taller buildings where the impact is less of a concern.

riddell kurczaba **DEC 21 SHADOWS** SOLA 2020-08-31 **DP - REVISION 1 A-27**
#19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL10 OCGROW GROUP OF COMPANIES SCALE:

Development Permit Plans

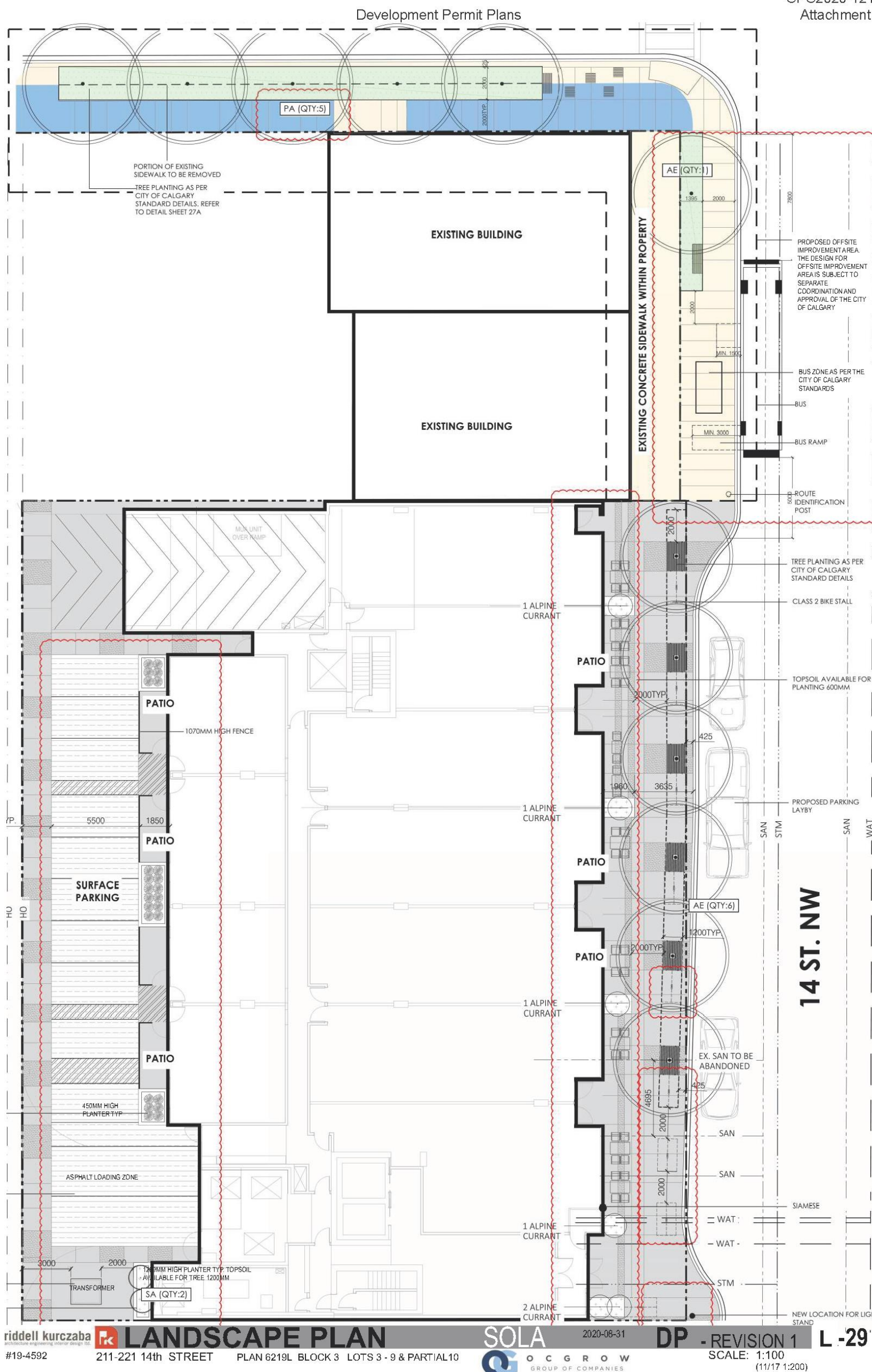
Garbage The assumed amount of garbage of .3 yds/unit x 168 = 50.4 yards per week plus the garbage from 747sm of commercial or 2.5 yards. Provision is made for 3 six yard container for a total of 18 yards of garbage containers. This will require two pick ups per week. With the predominance of small units the .3 yards per unit over estimates the amount generated but given that the service will be procured privately the condo association will be able to monitor the frequency of garbage pick up. The preferred container solution will utilize 4 yard containers which are easier to move and access, this would result in a likely frequency of 3 pickups per week. The garbage room can be fitted with 3 large bins. The service apron outside is essentially level (2%) with the paved lane making access practical. This apron and lane pick up area would be cleaned of snow for year-round accessibility. The overhead power lines make it necessary to load garbage completely from the lane. This loading area will be shared between the retail units and the condominium. Final decisions on size of bin and frequency of pickup will be subject to contractual agreements with a service provider. While the space is tight, .5m space is provided around each large bin with sufficient space to move bins in and out for pick up in the lane and allow for loading to happen through the space.

The overhead door is 12 ft wide by 9 ft tall to allow for direct removal of the bins with minimal sideways movement. A concrete curb is included to prevent bins from hitting the door track and interior walls and bollards have been added at the overhead door to protect the tracks. The interior walls are also of concrete to ensure the space can withstand the abuse of moving the bins.




GARBAGE LOADING

 2020-08-31
 
 DP - REVISION 1
 
 SCALE:



riddell kurczaba **LANDSCAPE PLAN** SOLA 2020-08-31 DP - REVISION 1 L -29
 #19-4592 211-221 14th STREET PLAN 6219L BLOCK 3 LOTS 3 - 9 & PARTIAL10 O C G R O W SCALE: 1:100 (11/17 1:200)
 GROUP OF COMPANIES

Development Permit Plans

LEGEND

- EXISTING CONCRETE TO REMAIN
- PROPOSED REGULAR GREY COLOR CONCRETE
- PROPOSED CONCRETE IN OFFSITE AREA
- ASPHALT SURFACE
- LIGHT GREY SANDBLAST FINISH CONCRETE
- PLANTING BED WITH CEDAR MULCH
- SOD
- DECIDUOUS TREES

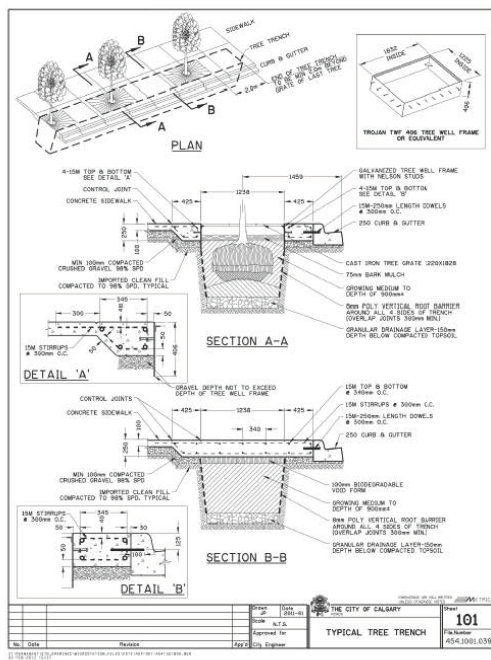
- DECIDUOUS SHRUBS
- PERENNIALS
- MOVABLE FURNITURE
- TREE GRATE
- SEATING
- CLASS 2 BIKE STALL
INVERTED, U-SHAPE BIKE RACK
TOTAL QUANTITY: 18 STALLS
- AREAS REVISED



19141KMIU
SOLA IN KENSINGTON
 OCGROW GROUP OF COMPANIES

TREE PLANTING

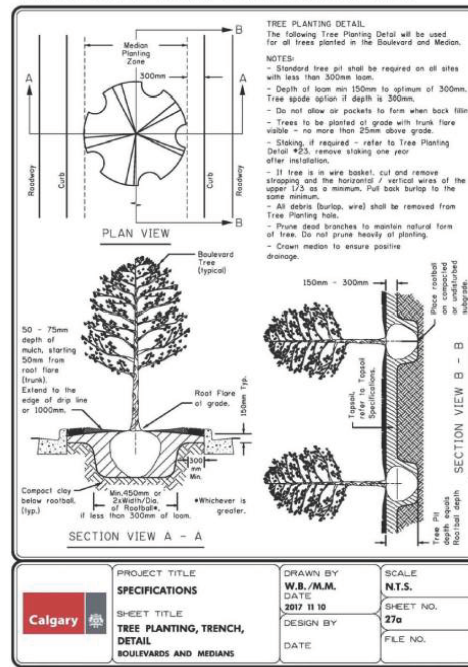
(ALONG 14 STREET NW IN FRONT OF THE SITE)



TREE PLANTING

(FOR OFF-SITE IMPROVEMENT AREA)

Detail Sheet 27a: Tree Planting - Boulevard and Median Trench



Calgary Parks 2019 244

LANDSCAPE ANALYSIS

SITE AREA: 2090.53 SQ.M.

TOTAL TREES PROVIDED = 8
 DECIDUOUS TREES PROVIDED = 8
 DECIDUOUS TREES WITH MIN 75MM CALLIPER PROVIDED = 6
 REMAINING DECIDUOUS TREES TO HAVE MIN. 50MM CALLIPER

TOTAL SHRUBS PROVIDED = 6

PLANTING SCHEDULE

common name	latin name	size/ spread	quantity
deciduous trees			
SWEDISH COLUMNAR ASPEN	POPULUS TREMULA 'ERECTA'	50MM CAL.	02
AMERICAN ELM	ULMUS AMERICANA	75MM CAL.	06
boulevard tree in off-site area (not part of landscape calculation)			
PRAIRIE SPIRE GREEN ASH	FRAXINUS PENNSYLVANICA 'RUGBY'	65MM CAL.	05
AMERICAN ELM	ULMUS AMERICANA	65MM CAL.	01
common name	latin name	size/ spread	quantity
deciduous shrubs			
ALPINE CURRANT	RIBES ALPINUM	600MM HT	06

NOTES

- THIS DRAWING HAS BEEN PREPARED FOR DEVELOPMENT PERMIT PURPOSES ONLY. IT IS NOT TO BE USED FOR CONSTRUCTION PURPOSES.
- ALL PLANT MATERIAL TO CONFIRM TO THE CANADIAN NURSERY TRADES ASSOCIATION STANDARDS.
- ALL PLANT MATERIAL SIZES SHOWN ARE MINIMUM SIZES.
- VERIFY ALL DIMENSIONS, ELEVATIONS, AND DATUM. REPORT ANY ERRORS AND/OR DISCREPANCIES TO THE OWNER PRIOR TO CONSTRUCTION.
- DO NOT SCALE DRAWINGS.
- ALL PLANTING BEDS TO HAVE A MINIMUM OF 75MM DEPTH CEDAR MULCH UNLESS OTHERWISE NOTED.
- ALL SOFT SURFACE LANDSCAPE AREAS TO BE IRRIGATED WITH UNDERGROUND IRRIGATION SYSTEM.
- ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE NOTED.

*** TOPSOIL AVAILABLE FOR PLANTINGS OVER PARKADE:
 TREES 1200MM
 SHRUBS 600MM
 GRASS & PERENNIALS 300MM

SEAL

ISSUED FOR	DATE
CLIENT REVIEW	191115
PRE-APP SUBMISSION	200116
DP SUBMISSION	200624
DTR SUBMISSION	200828

SCALE	1:150
DATE	191114
DRAWN BY	CC
CHECKED BY	YN

DP-L1 LAYOUT PLAN

NOT FOR CONSTRUCTION

THIS DRAWING SUPERSEDES PREVIOUS ISSUES. DO NOT SCALE THE DRAWING.
 ALL LANDSCAPE DRAWINGS REMAIN THE PROPERTY OF NAVAGRAH LANDSCAPE ARCHITECTURE + URBAN DESIGN LTD. THE DRAWINGS MAY NOT BE REPRODUCED WITHOUT THE PERMISSION OF THE LANDSCAPE ARCHITECT.

File Location: Y:\Projects\19141KMIU - Kensington Mixed Use\19141KMIU - Kensington Mixed Use\19141KMIU_Site Landscape Plan_200828.dwg Plotted: August 28, 2020 12:38:14 PM by Chen Chen

CPC2020-1212
Revised Attachment 3

Community Association Letter



July 27, 2020

Emailed to Matt Rockley, City of Calgary File Manager

RE: DP2020-3902 | 211-221 14th Street NW | Proposed Mixed Use Commercial & Residential Development (168 Dwelling Units) | "SOLA" Project

Dear Mr. Matt Rockley,

On behalf of the Hillhurst Sunnyside Community Association and its planning committee, we would like to thank you for the opportunity to provide comments. We have been engaged with this file since the pre-application Land Use Redesignation stage in March 2019 and additionally thank the applicant and neighbours for all their involvement.

We preface this letter in stating that it has been very difficult for us to keep up with City-wide planning policies and individual development applications, in part due to the challenges of volunteer engagement and fatigue during summer months with the added strain to the organization due to the pandemic.

We additionally recognize the developer's intent to proceed with redeveloping the site during this challenging time. We believe that the proposed development will set the tone and precedent for quality future redevelopment on 14th Street NW. The applicant's proposed offsite improvements present an opportunity to gradually evolve one of Calgary's main transportation arterials into a more active, vibrant, and walkable community high street. As such, we trust that a rigorous review process involving City of Calgary subject matter experts and the applicant's team will be applied.

We remind the City Development Authority and Calgary Planning Commission that the proposed development is required to demonstrate a high standard of urban design. This is especially important as this application had received Council approval for greater height (30 metres total) and density (5.0 FAR) over the Hillhurst Sunnyside Area Redevelopment Plan (ARP) maximums of 20m and 4.0 FAR.¹

Due to a lack of resources, we have not had the opportunity to discuss the applicant's itemized Community Amenity Fund (HSCAF) contribution of \$83,420.89 (*Appendix 1*). We however note that many redevelopment projects that have successfully applied for ARP amendments have contributed *both* to the HSCAF and have funded offsite public realm improvements.

¹ The maximum heights and densities defined in the ARP are not guaranteed entitlements: "*In order to achieve these maximums, projects will need to meet high standards of architectural and urban design quality that will ensure projects make positive contributions to the public realm*" (ARP Section 3.1.5; 3.2).

CPC2020-1212
Revised Attachment 3

Community Association Letter

We respectfully request that the Development Authority and Calgary Planning Commission refer to the attached comments from neighbours for their thoughtful review and feedback. We additionally support the requested offsite traffic calming measures and trust that through the review process, sensitive design transitions to the lower density residential areas are incorporated into the final design.

While resources are currently limited, we would still like to be kept informed as stakeholders on the progress of the application. We further extend an invitation to the City and applicant to contact the undersigned, should they wish to request community association input on specific aspects of the application and especially as it pertains to wider conversations around the design of the public realm and the development of a cohesive identity for 14th Street NW as an urban street.

Thank you for your understanding,

Lisa Chong, MPLAN
Community Planning & Engagement Coordinator
Hillhurst Sunnyside Community Association

- cc: Matt Crowley, Chair, Hillhurst Sunnyside Planning Committee (HSPC)
Decker Butzner, Kathleen Kenny, Robert McKercher, Executive Committee, HSPC
Brook Melchin, Applicant's Representative, Riddell Kurczaba Architecture
David Down, Architect & Chief Urban Designer, City of Calgary
Dale Calkins, Senior Policy & Planning Advisor, Ward 7 Councillor's Office
Development Permit Circulation Controller
- encl: Letter from directly affected resident involved in the Hillhurst Sunnyside Planning Committee's development review process "*RE DP2020-3902 - Victor Shui (15-Jul-2020).pdf*"

Community Association Letter

July 15, 2020

Victor Shiu
216 15 Street NW, Calgary

To: **Matt Rockley (City of Calgary)**
Dale Calkins (Ward 7)
Lisa Chong (HSCA)

Re: **DP2020-3902**

Hello Matt, Dale, and Lisa,

Please see below my comments based on the DP information provided by the Applicant. I would like to remind the City review team about the ARP rule on "exemplary design" and the Applicant's approved height/density over the ARP. At land use stage, the Applicant noted that a lot of the details on site improvements and design would not be worked out until DP stage which is now.

I sincerely ask for your help in ensuring the Applicant fulfills their obligations and responsibilities as promised in exchange for the increased height and density obtained at Council. It is very much appreciated.

Site Improvements

1. We are waiting for the Applicant to provide an itemized list of proposed site improvements and visually identified on site plans/renderings. Each item should also be identified as required by standard City bylaw/regulations or as bonus density contribution against the \$83,420 for bonus density obtained on land use.
2. We feel strongly that the offsite improvements should include more traffic calming measures, safety, landscaping in the lane rather than focused primarily wrapped along 2nd Ave. Proposed site improvements would cover both areas overall.
3. We strongly request more traffic calming related improvements for all surrounding affected roadways – lane, 2nd Ave, 15th St.
4. We support having both paved pedestrian crosswalks across 2nd Ave as well as across 14th St. The paved pedestrian crosswalk across 14th St was removed from the DP drawings but shown on previous plans. We would like this to be added back as a site improvement.

Rear At-Grade Residential Units

1. Overall, we are supportive of these units to soften the direct impact to residents behind the lane.
2. We suggest more landscaped rear interface for the residential units on the lane to increase privacy for both the unit residents as well as residents across the lane.
3. We encourage the Applicant to investigate other recent developments in the community (including the Kensington building, Pixel & Ezra) as inspiration for successful laneway facing residential units.

RE: DP2020-3902

Victor Shiu

Page 1/3

CPC2020-1212 - Attach 3
ISC: UNRESTRICTED

Page 3 of 5

CPC2020-1212
Revised Attachment 3

Community Association Letter

Garbage / Loading

1. *"The overhead power lines make it necessary to load garbage completely from the lane."* Does that mean garbage/compost/recycling bins will all be pulled back from the building past the power line setback, or past the rear property line into the lane? Please identify on site plan the intended area where bins would be pulled out to waiting for pick up.
2. We are concerned that the pulled-out bins for pick up with block us and/or our neighbours from backing out of our garages onto the lane. Will this be a problem?
3. *"The preferred container solution will utilize 4 yard containers which are easier to move and access, this would result in a likely frequency of 3 pickups per week."* Given the regular and high frequency of pickups, will there be noise (loading trucks, bins being pulled) and smell (compost, garbage) concerns from the bins being pulled out waiting for truck pick up? How are these mitigated?
4. *"This apron and lane pick up area would be cleaned of snow for year-round accessibility."* Like above, we are concerned that snow would be cleared on to the lane blocking traffic. Will this be a problem?
5. It appears the overhead powerlines imposes constraints on truck size and required bins to be pulled out for pick up. Has the Applicant/City Administration looked at putting this underground to remove the constraints? This could be a good potential site improvement item for bonus density.

Traffic Calming / Layby

1. The layby spaces along 14th St have the potential to cause danger and traffic issues on this busy thoroughfare. For example, if there were three cars parked and the middle car left, the incoming car would be required to parallel park to access this space which seems like an unreasonable expectation on this type of road.
2. I feel these items are important as the proposed layby parking spaces along 14th St can be dangerous for vehicles parking and driving through, people getting in and out of the vehicles, as well as pedestrians. However, I am supportive of layby parking in general but only if these factors and traffic impact (e.g. detours through lane, 2nd Ave, 15th St) are fully addressed.
3. We encourage the Applicant team to get in touch with the City of Calgary Established Areas Growth and Change Strategy team. As a longer-term project for the City to support increased population in our communities, traffic calming has been proposed on 14th Street in the form of raised crosswalks and curb extensions to "indicate arrival into residential setting[s]. Potential locations include 2nd Ave, 7th Ave, 8th Ave (both legs), 10th Ave, 11th Ave NW."

Lane

1. "Bikes will typically enter the building on the car ramp." The design intends that up to 168 bicycles would access the underground stalls through the car ramps same as vehicles. This will create significant bicycle traffic in the lane coming from both 2nd Ave and Kensington Road, and cause huge issues for the lane. Primary access should be through the front lobby of the building facing 14th St which will be pedestrian friendly instead so that bicycle traffic is NOT encouraged in the lane for safety.
2. It looks like there are 2 exterior doors accessible from the lane into the building corridor in addition to the front lobby entrance. Are these 2 entrances to the public as well as residents? The addition of 168 residential units will inevitably generate significant non-vehicle traffic: foot traffic, bicycles, scooters, and other non-conventional methods of transportation. This significant increase to existing traffic renders the lane unsafe, how will this be mitigated?

RE: DP2020-3902

Victor Shiu

Page 2/3

CPC2020-1212 - Attach 3
ISC: UNRESTRICTED

Page 4 of 5

CPC2020-1212
Revised Attachment 3

Community Association Letter

3. Does the TIA provided to City Administration for review address all the following items?
 - Pedestrian traffic – access via lane 2 exterior entrances
 - 168 bike stalls – access via lane car ramp
 - 84 underground parking stalls – access via lane
 - 10 surface retail stalls – access via lane
 - 4 to 5 layby stalls – access via 14th St
4. Aside from the initially proposed speed bump, the Applicant has not shown or identified any additional traffic calming measures for the lane. The Applicant has consistently noted that they are open to suggestions on how the lane can be improved from us. The problem is that we are not experts and do not know what would work based on best practices. The Applicant should work with the City to identify traffic calming measures suitable for the lane and provide to the Community for review and discussion. It is the Applicant's responsibility to identify and provide options for traffic calming/improvement for the Community's review. We as directly affected residents rely on the City to ensure that the Applicant is completing this work, as we do not have the expertise in rear lane design or traffic calming in general.
5. We are supportive of speed bumps in the lane and would support having 2 of them to bookend the north and south boundary of the lane behind the site. What are the reasons that City does not support speed bumps? What are the benefits/disadvantages? I believe there is already one speed bump near the south end of the lane behind the Telus building.
6. We request the City investigate the feasibility of designating/enforcement of the lane for 1-way traffic only, as the lane is already one-way only as it exists. Per survey the width of the lane is only 5.49 m, and for two-way traffic ~6.5 to 7 m is needed. If vehicles were coming from both ends, one of the vehicles has to pull over against a garage just so the other vehicle can pass. Making the lane one-way could potentially eliminate that problem, but we would like to hear more about the City's position and the benefits/negatives. Please also take consider the significant new traffic that will be created from the underground parkade, bicycles, and other untraditional forms of access (e.g. scooters).

Noise / Privacy / Reflection / Lighting

1. We are concerned about early morning/late night noise problems with the rear surface stalls for retail. How will this be mitigated for residents across the lane?
2. Will the glazing (and/or other finishing materials) on the rear of the building facing the lane cause significant glare for residents across the lane for our rooms?
3. Will the rooftop amenities cause noise and light pollution issues for us in early mornings/evenings? I am especially concerned about the open patio area with potential late gatherings of residents/visitors.

Sincerely,



Victor Shiu, 216 15th Street NW, Calgary

RE: DP2020-3902

Victor Shiu

Page 3/3

CPC2020-1212 - Attach 3
ISC: UNRESTRICTED

Page 5 of 5

Urban Design Review Panel Comments

Date	February 5, 2020	
Time	1:00	
Panel Members	Present Chris Hardwicke (Co-Chair) Ryan Agrey Michael Sydenham	Distribution Chad Russill (Chair) Terry Klassen Ben Bailey Colin Friesen Beverly Sandalack Glen Pardoe Jack Vanstone Gary Mundy
Advisor	David Down, Chief Urban Designer	
Application number	PE2020-00130	
Municipal address	221 14 St NW	
Community	Hillhurst	
Project description	Commercial Development	
Review	first	
File Manager	Matt Rockley	
City Wide Urban Design	Lothar Wiwjorra	
Applicant	Riddell Kurczaba Architecture	

Summary

After presentation and review of this application, the Panel finds the design of this project to be in general alignment with the community and City's urban design principles. The Panel understands this is a preliminary design and appreciates the effort undertaken to respond to the site restrictions and community reaction. It was identified that the Area Redevelopment Plan is under revision and the Panel recommends that the applicant continues coordination efforts with the community and City to best understand and align with these concurrent changes.

The Panel is generally supportive of the project application as the size and mixed-use will provide a positive precedent for future development. The street side offers positive urban design elements with a wide sidewalk and enhanced landscaping that continues across the adjacent property and wraps the 2nd Avenue corner. With below-grade parking, the project prioritizes the walkable realm and the Panel supports this approach. The quality of the laneway and how it interfaces with the at-grade residential units is of primary concern and further development is recommended to ensure a safe and comfortable environment for the residents.

The panel supports the proposed encroachments within the 5.182m bylawed setback on 14th Street, as it enhances the public realm, achieving the vision of the Urban Boulevard classification, while still enabling below-grade parking and an at grade floor height, allowing for greater accessibility into the retail units.

Applicant Response

2/7/2020

PE2020-00130
UDRP Comments

02/05/2020

CPC2020-1212 - Attach 4
ISC: UNRESTRICTED

Page 1 of 4

Urban Design Review Panel Comments

Urban Design Element	
<p>Creativity <i>Encourage innovation; model best practices</i></p> <ul style="list-style-type: none"> Overall project approach as it relates to original ideas or innovation 	
UDRP Commentary	In general, the project directly responds to the site restrictions with a few unique features including a rooftop amenity space and laneway residential units. The panel recommends further refinement of the massing and these unique features to ensure they are contextually successful.
Applicant Response	<ul style="list-style-type: none"> The material color at residences on the lane uses a brick red panel expression to emphasize the distinctive nature of these unit from other uses. The street side is mostly glass but is accented with semi-gloss white porcelain and white marble to create a contemporary clean look Lane residential units have defined entries with individual lighting
<p>Context <i>Optimize built form with respect to mass and spacing of buildings, placement on site, response to adjacent uses, heights and densities</i></p> <ul style="list-style-type: none"> Massing relationship to context, distribution on site, and orientation to street edges Shade impact on public realm and adjacent sites 	
UDRP Commentary	In general, the massing responds well with the context stepping back and setting an acceptable height. The horizontal 'box' across the top appears out of place with the vernacular of Kensington, further refinement is recommended. Shade studies were not presented and should be developed for further reviews.
Applicant Response	<ul style="list-style-type: none"> The horizontal box on the 7th floor was punctuated with a depression mid-way Shadow studies were updated to reflect actual design
<p>Human Scale <i>Defines street edges, ensures height and mass respect context; pay attention to scale</i></p> <ul style="list-style-type: none"> Massing contribution to public realm at grade 	
UDRP Commentary	The widened sidewalk and stepped massing present a street-friendly façade and sets a positive precedent for 14 th Street. A double-storey 'townhouse' was recommended for the rear residential units to better separate them from the lane.
Applicant Response	<ul style="list-style-type: none"> The rear units have greater ceiling height of 4.5 meters but this cannot achieve two floors or a mezzanine due to the 26 meter height constraint
<p>Integration <i>The conjunction of land-use, built form, landscaping and public realm design</i></p> <ul style="list-style-type: none"> Parking entrances and at-grade parking areas are concealed Weather protection at entrances and solar exposure for outdoor public areas Winter city response 	
UDRP Commentary	Setting the precedent for a residential 'mews' in the laneway is an interesting idea but the Panel is uncomfortable with the current design. Further development of the residential units at grade and how they interface with the laneway is required to address the safety and comfort concerns.
Applicant Response	<ul style="list-style-type: none"> Lane way units added courtyards with a planter in each. The courtyard wall is solid to 5ft height to provide visual and functional privacy.
<p>Connectivity <i>Achieve visual and functional connections between buildings and places; ensure connection to existing and future networks.</i></p> <ul style="list-style-type: none"> Pedestrian first design, walkability, pathways through site Connections to LRT stations, regional pathways and cycle paths Pedestrian pathway materials extend across driveways and lanes 	
UDRP Commentary	The panel notes that the street landscaping in front of the building will make a positive contribution to the street. It is recommended to continue the tree line and landscaping to the north site with a consistent quality wrapping the corner down 2 nd Ave. Accessible approaches are recommended including tactile and visual cues. The primary concern is with the laneway residential units and how residents connect through the building on the ground floor. Further refinement is recommended to provide clear and safe connections.
Applicant Response	<ul style="list-style-type: none"> An additional tree was added north of the site as part of offsite improvements as well as 5 trees on 2nd avenue. The door of residents on the main floor was removed at the interior corridor to avoid conflict with the retail usage of this corridor per bylaw requirements. The applicant has asked for flexibility to do both as needed to allow the residents to connect to internal amenities.
<p>Animation <i>Incorporate active uses; pay attention to details; add colour, wit and fun</i></p>	

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	<ul style="list-style-type: none"> • Building form contributes to an active pedestrian realm • Residential units provided at-grade • Elevations are interesting and enhance the streetscape
UDRP Commentary	The design animates the street with a stepped façade, residential and retail at-grade and enhanced streetscape solutions. Understanding that the design is in the preliminary stages, the Panel is hopeful that further refinement of the materials and massing aligns with other exemplary Kensington buildings.
Applicant Response	<ul style="list-style-type: none"> • The design incorporates significant push and pull of the façade with varying solutions to balconies and the expressive roof top amenity will complete the expression resulting in a strong positive presence on 14th street and adding to the redevelopment character of the area.
Accessibility <i>Ensure clear and simple access for all types of users</i> <ul style="list-style-type: none"> • Barrier free design • Entry definition, legibility, and natural wayfinding 	
UDRP Commentary	The widened sidewalk and at-grade units provides opportunity for accessible solutions. Visual and tactile cues are recommended. The landscaped strips in front of the retail units may impede walkability and not recommended. Further entry definition is recommended for the at-grade retail and residential units applying canopy and lighting solutions.
Applicant Response	<ul style="list-style-type: none"> • At grade retail entries were depressed to emphasize rhythm at the pedestrian interface. • Soft landscaping was reduced as suggested to enhance pedestrian flow while still defining space for seating areas as needed by the retail tenants. • Varying textures and colors of cement paving are used to enhance the tactile expression • Downward strip lighting has been added to the bulkhead above the retail store windows to wash the sidewalk and interior displays with light.
Diversity <i>Promote designs accommodating a broad range of users and uses</i> <ul style="list-style-type: none"> • Retail street variety, at-grade areas, transparency into spaces • Corner treatments and project porosity 	
UDRP Commentary	The mixed-use nature of the project and opportunities for smaller retail units is appreciated. The mix of residential units appear to be limited to 1 and 2-bedrooms. The panel recommends live-work units at-grade from the lane to provide further possibilities for destination-type workspaces.
Applicant Response	<ul style="list-style-type: none"> • The applicant agrees that live work should be endorsed as acceptable and desirable for these units, so that the path to approval for individual applicants is straightforward as they seek to maximize the use of this location.
Flexibility <i>Develop planning and building concepts which allow adaptation to future uses, new technologies</i> <ul style="list-style-type: none"> • Project approach relating to market and/or context changes 	
UDRP Commentary	The panel appreciates the smaller ground floor retail units with individual entrances to promote future space flexibility. Further thought is recommended in regard to the laneway residential units to ensure they are resilient to market and contextual changes. See comment above about potential live-work units.
Applicant Response	<ul style="list-style-type: none"> • Improvements have been made in this design is to formalize the entry to the units from the lane. This will activate the lane space.
Safety <i>Achieve a sense of comfort and create places that provide security at all times</i> <ul style="list-style-type: none"> • Safety and security • Night time design 	
UDRP Commentary	The panel identified several safety issues regarding the laneway residential units. It was recommended to push the rear north entrance to the building edge to prevent hidden exterior space. The exterior lighting design in the alley will be critical to provide a safe and comfortable environment for the laneway residential units. Fencing and landscape solutions need to be improved to provide a comfortable buffer between the at-grade residential units and laneway.
Applicant Response	<ul style="list-style-type: none"> • The two entries to the retail from the lane have been adjusted to minimize hidden alcoves while providing overhead protection. They are connected directly to the lane with dedicated pedestrian areas. • The retail entries at the lane will be well lit with direction down lighting that does not spill over to residents across the lane. The activation of the lane by multiple types of users will mitigate loitering challenges.

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Urban Design Review Panel Comments

Orientation <i>Provide clear and consistent directional clues for urban navigation</i>	
<ul style="list-style-type: none"> Enhance natural views and vistas 	
UDRP Commentary	The rooftop amenity space will provide excellent views for the residents. The panel recommends a consistent landscape approach wrapping the block to provide direction clues towards the laneway.
Applicant Response	<ul style="list-style-type: none"> A primary objective of the offsite improvements is to enhance the sidewalk frontage north of the site. The rear entries provide public access to retail from the 10 guest / retail parking stalls in the lane
Sustainability <i>Be aware of lifecycle costs; incorporate sustainable practices and materials</i>	
<ul style="list-style-type: none"> Site/solar orientation and passive heating/cooling Material selection and sustainable products 	
UDRP Commentary	The Panel anticipates the project to develop a sustainable approach through further design development
Applicant Response	<ul style="list-style-type: none"> The project will meet current stringent energy codes that require full energy modeling and rationalizing a high performance envelop. Each unit will have it's own ERV promoting as needed usage of energy with independent fresh air to respond to covid concerns. Alexa integrated building wide systems will save 25% - 30% in energy savings by each units systems to respond intelligently to daily occupancy cycles.
Durability <i>Incorporate long-lasting materials and details that will provide a legacy rather than a liability</i>	
<ul style="list-style-type: none"> Use of low maintenance materials and/or sustainable products Project detailed to avoid maintenance issues 	
UDRP Commentary	The Panel understands this is a preliminary application and anticipates the project to incorporate durable materials and maintenance solutions through further design development
Applicant Response	<ul style="list-style-type: none"> Durable materials are integrated at the buildingbase. Windowsills are raised with precast concrete base board separating the glass, porcelain and marble above from abuse from snow removal etc Architectural panels, above grade will have a long-life finish. The application identifies 3 colors of cement board or other equivalent panel system. The commitment is to provide a panel system with long life expectation that achieves a clean contemporary expression

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Open House Summary

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Attachment 5

SOLA – ONLINE OPEN HOUSE – June 30, 2020 - 7pm
Facilitated by Riddell Kurczaba Architecture and Ocgrow

This online presentation and question and answers is for purposes of informing the public on the nature of the DP submission for the SOLA development project on 14th street north west.
The event was advertised by the community association on their web page and newsletter. Fifty flyers were delivered to the homes on both sides of 15th street between Kensington and 2nd ave. During the presentation 9 separate participants were reported as participating by the ZOOM mtg system. The zoom webinar was open to anyone having the access code.

Attached is a copy of the flyer distributed, the slides shared by Brook Melchin Architect with Riddell Kurczaba for the first 15 minutes and the questions submitted during the zoom webinar with a short version of the verbal response given. The comments in red below represent the answers given.

Questions and comments submitted during the online open house:

From Terry Chau to All Panelists: 07:09 PM

I've been waiting to comment for some time, as I live within block of this site on West side & was away at earlier open houses. I'm so pleased with this project & finally great to see some positive new developments. I fully support this project and it will bring great positive benefits to 14th Street NW, which is long overdue. Best regards, Terry

From Victor and Liz to All Panelists: 07:11 PM

Are those rear surface stalls only for at grade residents use? **They are for the retail, residents and guests use the parking under.**

Are the 2 rear door entrances for residential or commercial access? **Both**

From Jasmine to Everyone: 07:11 PM

A 2 metre sidewalk seems quite narrow for the frontage. **This is based on city standards but there is also the area of sidewalk for the tree wells and about 2 meters adjacent to the building that will be activated by retail tenants.**

From Matt Crowley to All Panelists: 07:12 PM

are there multifamily units in the rear of the property? **Yes 4 units** So retail visitor parking is in front of their units? **The privacy fence is a solid 5 ft high fence designed to provide security and privacy.**

From Jasmine to Everyone: 07:13 PM

What about the lane south of the site? **This will not be upgraded** Presumably the congestion an access for the building would occur from both the north and the south. **Yes, the lane will be repaved north and behind the site. The lane is effectively widened at the rear of the site with 1.8 meters behind the parking stalls**

From Rick Karema to Everyone: 07:14 PM

I have lived in Kensington for over 12 years, and absolutely love this project...will we a great addition to the community! Best Wishes,

Looking forward to the sales center hopefully opening soon, Rick

From Victor and Liz to All Panelists: 07:15 PM

Without landscaping and site areas provided, it is difficult to assess the current design. Please consider adding additional plantings on the lane side, especially in an effort to enhance the lane condition for the at-grade residential units.

Applicant needs to provide more detailed street level renderings and elevations to illustrate the streetscape experience as it impacts surrounding residents - both the front of the site and rear facing the lane. In particular, the renderings/elevations for the rear facing the lane should illustrate in detail and include visual examples the following:

- a. Pedestrian presence/activity in the patio area
- b. Vehicular activity/parked in rear surface parking stalls

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- c. Vehicular activity accessing underground parkade entry
- d. Vehicular activity parked/accessing loading/garbage area
- e. Visual elevation/impact on sloping in relation to the lane. **The two renderings from the rear from the north and south along with plan notations were shared again to participants to help explain the lane condition.**

From Peter Salasar to Everyone: 07:17 PM

Our family is in total support of this SOLA project, we need more new developments like this which are such first class, to revitalize all of 14th St....great job and can't wait to see the ugly yellow building, currently there finally gone., with regards, Peter Salasar

From Terry Chau to Everyone: 07:20 PM

Kudos to the developer & team, we fully support this development and renderings look excellent! Terry

From Victor and Liz to All Panelists: 07:21 PM

We request more detail on the street-level renderings:

- a. The design of the ground level patio area for residential units – for example, how will those look from a street level?

- b. We request rendering incorporating the proposed layby, sidewalks, landscaping, and surrounding building massing to understand the overall impacts of the proposed development

Aside from the proposed speed bump, the Applicant has not shown or identified any additional traffic calming measures for the lane. The Applicant should work with City Administration to identify traffic calming measures suitable for the lane and provide to the Community for review and discussion. **It was noted that the proposed speed bump was deleted in the DP submission and that residents who feel this is important should make known to the city the need to calm traffic.**

From Matt Crowley to All Panelists: 07:21 PM

Can you provide a breakdown of the current proposed bonus density improvements and budget for where that money is proposed to be spent? Thank- **the bonus plan is updated in the current proposed landscape plan with changes as a result of city guidance. The DP package does not break this down. This can be updated from the July 2019 version.**

From Terry Chau to Everyone: 07:22 PM

any idea when the project sales office shall be open? **Hopefully in the fall**

From Victor and Liz to Everyone: 07:24 PM

Are the loading stalls as shown long enough for loading trucks required by the retailers? Is there potential for loading trucks to stick out onto the lane setback area and reduce the lane width? **The 2 loading stalls are city standard. The narrow lane also constricted by powerpoles does not allow large trucks to park in these stalls. Van and small u-hall truck sizes will be able to park in these stalls. U-haul publishes a guide based on unit sizes for truck sizes needed. Because of the small units there will not be need for larger trucks for those moving in and out.**

From Matt Crowley to All Panelists: 07:26 PM

has reconfiguring this building as a MF rental been considered? There is no condo market in Calgary right now, projects in the area are struggling to get sales. Anthem flipped from condo to rental just recently in a good location. **With most product focused on rental we believe there is a niche for condominium product of the appropriate price range.**

From Victor and Liz to Everyone: 07:26 PM

More information is requested on the design of the loading/garbage area. From the plans, it appears that the area is contained within the building envelope in a segregated area, but it is not completely clear. We request that the Applicant provide details on the design, and how it will ensure noise/smell is mitigated for adjacent residents. **The garbage is completely stored inside the building mitigating the visual and smell issues, the grades in the lane are at 2% to allow bins to be wheeled out into the lane for**

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direct pick up this will require the condo to clean the snow for this to happen. With the power lines there is no other option.

From Jasmine to Everyone: 07:26 PM

Would encourage you to consider more public realm improvements to the public frontage of the building. The sidewalk to the north is quite a bit wider. This development is being sold to the community as bringing improvements to 14th street the inclusion of a layby coupled with a narrow sidewalk makes this building very car oriented rather than improving the pedestrian realm. The addition of laybys actually creates a sense of safety for pedestrians plus providing the necessary association with the retail to enhance its viability. The building respects the road widening easement and uses this 5.1 meters for layby, tree planting, sidewalk, and activated area beside retail.

From Lisa to All Panelists: 07:26 PM

Hi Brook - I think the original concept showed some texture on 14 St at the pedestrian crossing to enhance pedestrian visibility - will this still be integrated? The current landscape plan does not show these crossings. We will include them in an updated options listing.

From Victor and Liz to Everyone: 07:29 PM

We have questions surrounding the type of retailers/uses for the proposed development. How will the Applicant ensure adjacent residents are not disturbed by visitors accessing the retail businesses parking/going at the rear surface stalls in the evenings/early mornings? Ocgrow is confident in a good mix of retail but we cannot confirm at this stage who would be there. This will be subject to applications for each user.

From Jasmine to Everyone: 07:31 PM

Agreed that a row of parked cars provide a buffer, but laybys are not the same. In this case we would be taking away the width from the sidewalk in front of the building only (for only a few metres) which does little to improve the pedestrian environment overall but does make walking much more difficult by requiring pedestrians move out of the way for cars. The 5.1 meter easement plus existing sidewalk area can to achieve the items just mentioned. The city has been specific in their guidance that this easement will not be relaxed at grade but must be justified as part of the public realm.

From Victor and Liz to Everyone: 07:33 PM

Based on the pre-application site plan, the CRUs appear smaller - however, that is strictly a demising matter and easily changed and not set in stone. Our rationale for inquiring is that the type of retailers and use operating in those spaces would heavily determine the extent of traffic impact on the lane (rear surface stalls), traffic impact along 14th St NW (front layby parking), noise/smell impact (rear loading/garbage area), among others.

We request clarification on the Applicant's rationale for the proposed placement of speed bump in the lane as shown. We request the placement of 2 speed bumps to book-end the north and south boundary of the lane behind the proposed development. We would support the addition of speedbumps subject to guidance from the city.

From Victor and Liz to Everyone: 07:39 PM

Aside from the proposed speed bump, the Applicant has not shown or identified any additional traffic calming measures for the lane. The Applicant should work with City Administration to identify traffic calming measures suitable for the lane and provide to the Community for review and discussion.

We encourage the Applicant team to get in touch with the City of Calgary Established Areas Growth and Change Strategy team. As a longer-term project for the City to support increased population in our communities, traffic calming has been proposed on 14 Street in the form of raised crosswalks and curb extensions to "indicate arrival into residential setting[s]. Potential locations include 2 Ave, 7 Ave, 8 Ave (both legs), 10 Ave, 11 Ave NW." It is believed that the enhancements in the lane will naturally calm traffic.

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From Matt Crowley to All Panelists: 07:39 PM

has reconfiguring this building as a MF rental been considered? There is no condo market in Calgary right now, projects in the area are struggling to get sales. Anthem flipped from condo to rental just recently in a good location. **We are confident in the for-sale model proposed.**

From Lisa to All Panelists: 07:42 PM

Like the mix of traditional and modern building materials. Maybe consider art at the sides? The Kensington Bucci building has local artwork. **This could be considered.**

From Jasmine to Everyone: 07:48 PM

There are already speed bumps at the south end of the lane due to speeding problems.]
We would support speedbumps in the lane

From Victor and Liz to Everyone: 07:49 PM

We would support speedbumps in the lane too

From Matt Crowley to All Panelists: 07:50 PM

can you show the bonus density improvements and line item budget **this can be updated**

From Lisa to All Panelists: 07:50 PM

For neighbours - once the DP application is in, the sign will be up, or it will be advertised on developmentmap.calgary.ca **the normal process will be followed with a new sign and advertisements.**

We expect that to take place in July.

From Matt Crowley to All Panelists: 07:50 PM

\$86k roughly

From Lisa to All Panelists: 07:50 PM

the community association is not able to share plans, but you will need to call 311 or contact City Property Research (propertyresearch@calgary.ca)

From Jasmine to Everyone: 07:51 PM

Is it the building over the maximum height as set out in the ARP? **It is not over height with respect to the new DC bylaw. And steps to reflect the intent of the original ARP**

From Matt Crowley to All Panelists: 07:51 PM

the previous plan was on 3 sides of the street, the new plan is only contiguous to this site looks like the money is not going as far any more **the crosswalk on 2nd and 14th can be considered as an option in the enhancements.**

From Victor and Liz to Everyone: 07:56 PM

Can you provide an itemized list of proposed site improvements and visually identified on site plans/renderings? Each item should also be identified as required by standard City bylaw/regulations or as bonus density contribution against the \$83,420 for bonus density obtained on land use. That will help us understand and review the Applicant's selected proposed improvement items. **Yes updated listing of enhancements can be made.**

the Zoom format of this meeting is far better for being able to hear what you're saying, Brook

From Matt Crowley to All Panelists: 07:56 PM

yes that is right. The first bonus density plan was on either side of 2nd street and across 14th street. the current plan looks to be only contiguous to 14th street

*contiguous to the site only

gotcha thanks

From Lisa to All Panelists: 07:58 PM

Thanks Brook

From Victor and Liz to Everyone: 08:01 PM

If you don't get approval to do the layby, could you possibly shift the building footprint more towards 14th st? **the city will not relax the 14th street easement at grade so the building could not be moved.**

thank you Brook

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[From Matt Crowley to All Panelists: 08:02 PM](#)
thanks Brook

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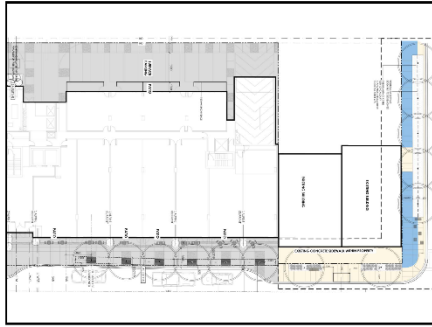
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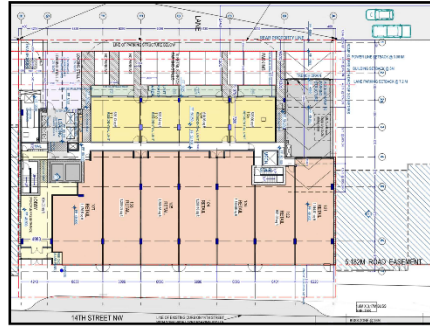
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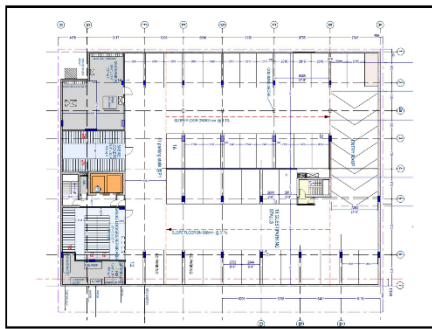
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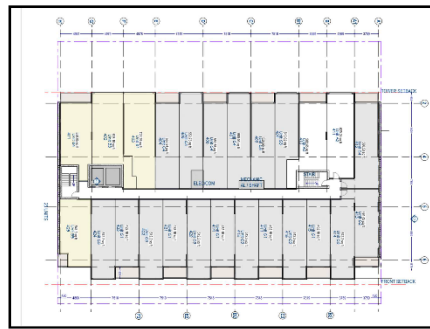
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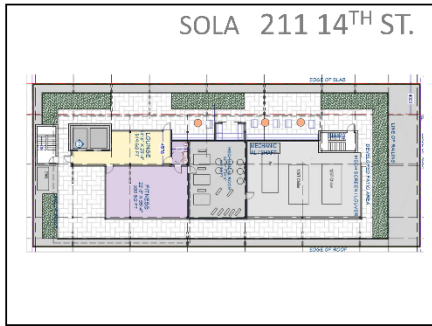
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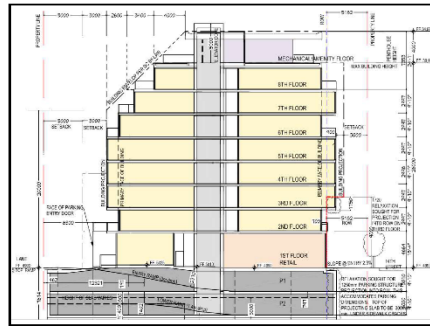
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SOLA

Virtual open House Invitation

(For Development Permit)

Condominium Development Proposal by Ocgrow at:
211 – 221 14th Street NW
(replacing the National Transmission Building)



In preparation for a Development Permit submission to the City, the development team seeks input on the design proposal. Renderings and plans will be shared at this virtual open house with feedback sought on public realm initiatives along 14th street and the Lane.

Tuesday June 30, 2020 7 pm – 8 pm

Virtual webinar is hosted on zoom at the following address:

<https://us02web.zoom.us/j/85831620288>

Facilitated by Riddell Kurczaba Architecture

Should attendance be a concern please email brookm@riddell.ca for information