



AGENDA

CALGARY PLANNING COMMISSION

April 18, 2019, 1:00 PM
IN THE COUNCIL CHAMBER
Members Present

Director M.Tita, Chair
Director R. Vanderputten, Vice-Chair
Councillor G. Chahal
Councillor E. Woolley
Commissioner M. Foht
Commissioner P. Gedye
Commissioner L. Juan
Commissioner A. Palmiere
Commissioner K. Schmalz
Commissioner J. Scott
Mayor N. Nenshi

1. CALL TO ORDER
2. OPENING REMARKS
3. CONFIRMATION OF AGENDA
4. CONFIRMATION OF MINUTES
 - 4.1 Minutes of the Regular Meeting of the Calgary Planning Commission, 2019 April 04
5. CONSENT AGENDA
 - 5.1 Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 – 32 Avenue SW, LOC2018-0232, CPC2019-0431
 - 5.2 Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40 Avenue SW, LOC2018-0207, CPC2019-0425
 - 5.3 Land Use Amendment in West Hillhurst (Ward 7) at 2339 – 5 Avenue NW, LOC2019-0013, CPC2019-0434

5.4 Road Closure and Land Use Amendment in Bridgeland/Riverside (Ward 9) adjacent to 23 McDougall Court NE, LOC2018-0252, CPC2019-0377

5.5 Community Name in Residual Sub-Area 02K (Ward 2), SN2019-0004, CPC2019-0474

6. POSTPONED REPORTS

(including related/ supplemental reports)

None

7. ITEMS FROM OFFICER, ADMINISTRATION AND COMMITTEES

7.1 DEVELOPMENT ITEMS

7.1.1 Development Permit in Hillhurst (Ward 7) at 417 – 10 Street NW, DP2018-4579, CPC2019-0494

7.1.2 Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075, CPC2018-1012

7.2 PLANNING ITEMS

7.2.1 Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370, CPC2019-0404

7.2.2 Land Use Amendment in West Hillhurst (Ward 7) at 2402 Westmount Road NW, LOC2019-0014, CPC2019-0454

7.2.3 Land Use Amendment in Greenview Industrial Park (Ward 4) at 4140 – 6 Street NE, LOC2019-0010, CPC2019-0433

7.2.4 Land Use Amendment in Arbour Lake (Ward 2) at 800 Crowfoot Crescent NW, LOC2018-0245, CPC2019-0461

7.2.5 Land Use Amendment in Residual Sub-Area 02L (Ward 2) at 15454 Symons Valley Road NW, LOC2018-0140, CPC2019-0468

7.2.6 Land Use Amendment in Saddle Ridge (Ward 5) at 9320 and 9325 - 52 Street NE, LOC2018-0170, CPC2019-0317

7.2.7 Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP), CPC2019-0318

7.3 MISCELLANEOUS ITEMS

None

8. URGENT BUSINESS

9. CONFIDENTIAL ITEMS

9.1 Ricardo Ranch Area Structure Plan (Verbal), POL2017-0010 - CPC2019-0389

10. ADJOURNMENT



INDEX FOR THE 2019 APRIL 18 REGULAR MEETING OF CALGARY PLANNING COMMISSION

NOTE:
PLANS SUBMITTED TO THE COMMISSION, AS PART OF THE DEPARTMENTAL REPORTS,
ARE INCLUDED FOR THE SOLE PURPOSE OF ASSISTING THE COMMISSION IN MAKING
A DECISION AND ARE NOT TO BE USED FOR ANY OTHER PURPOSE.

ISC: Unrestricted



CONSENT AGENDA

ITEM NO.: 5.1

Kait Bahl

COMMUNITY:

South Calgary (Ward 8)

FILE NUMBER:

LOC2018-0232 (CPC2019-0431)

PROPOSED POLICY AMENDMENT:

Amendment to the South Calgary/Altadore Area
Redevelopment Plan

PROPOSED REDESIGNATION:

From: Residential – Contextual One / Two Dwelling
(R-C2) District

To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS:

2040 - 32 Avenue SW

APPLICANT:

Perry Poropat

OWNER:

2138426 Alberta Ltd (James David)

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 5.2

Tom Schlodder

COMMUNITY:

Altadore (Ward 8)

FILE NUMBER:

LOC2018-0207 (CPC2019-0425)

PROPOSED POLICY AMENDMENT:

Amendment to the South Calgary/Altadore Area
Redevelopment Plan

PROPOSED REDESIGNATION:

From: Residential – Contextual One / Two Dwelling
(R-C2) District

To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS:

1915 - 40 Avenue SW

APPLICANT:

PLP Design

OWNER:

Aliya Nurani
Imran Nurani

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 5.3

Courtney Stengel

COMMUNITY:

West Hillhurst (Ward 7)

FILE NUMBER:

LOC2019-0013 (CPC2019-0434)

PROPOSED REDESIGNATION:

From: Residential – Contextual One / Two Dwelling
(R-C2) District

To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS:

2339 – 5 Avenue NW

APPLICANT:

Hunter Tristan Design

OWNER:

Muhammad Ali Zain

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 5.4

Stephanie Loria

COMMUNITY:

Bridgeland/Riverside (Ward 9)

FILE NUMBER:

LOC2018-0252 (CPC2019-0377)

PROPOSED CLOSURE:

0.10 hectares \pm (0.25 acres \pm) of road adjacent to 23
McDougall Court NE

PROPOSED REDESIGNATION:

From: Undesignated Road Right-of-Way

To: Multi-Residential – Contextual Low Profile
(M-C1) District

MUNICIPAL ADDRESS:

Adjacent to 23 McDougall Court NE

APPLICANT:

Max Tayefi Architect

OWNER:

The City of Calgary

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 5.5

Jeff Nielsen

COMMUNITY:

Residual Sub-Area 02K (Ward 2)

FILE NUMBER:

SN2019-0004 (CPC2019-0474)

PROPOSED COMMUNITY NAME:

Symons Valley Ranch

APPLICANT:

IBI Group

OWNER:

Symons Valley Ranch GP Inc

ADMINISTRATION RECOMMENDATION:

APPROVAL

DEVELOPMENT ITEMS

ITEM NO.: 7.1.1 Steve Jones

COMMUNITY: Hillhurst (Ward 7)

FILE NUMBER: DP2018-4579 (CPC2019-0494)

PROPOSED DEVELOPMENT: New: Dwelling Units, Financial Institution, Outdoor Café, Restaurant: Licensed – Small, Retail and Consumer Service, Sign – Class B, Sign – Class D

MUNICIPAL ADDRESS: 417 – 10 Street NW

APPLICANT: IBI Group

OWNER: Greywood 10G GP Inc

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.1.2 Brendyn Seymour

COMMUNITY: Beltline (Ward 11)

FILE NUMBER: DP2017-4075 (CPC2018-1012)

PROPOSED DEVELOPMENT: New: Multi-Residential Development, Retail and Consumer Service

MUNICIPAL ADDRESS: 1818 – 1 Street SE, 1825 Park Road SE and 1919 MacLeod Trail SE

APPLICANT: NORR Architects Engineers Planners Ltd

OWNER: Albari Holdings Ltd
Cidex (Elbow River) Developments Ltd

ADMINISTRATION RECOMMENDATION: **APPROVAL**

PLANNING ITEMS

ITEM NO.: 7.2.1

Desmond Bliek

COMMUNITY:

Richmond Knob Hill, South Calgary, and Altadore

FILE NUMBER:

LOC2017-0370 (CPC2019-0404)

PROPOSED POLICY AMENDMENTS:

Amendments to the Marda Loop Area Redevelopment Plan

PROPOSED REDESIGNATION:

From: Commercial – Neighbourhood 1 (C-N1), Commercial – Neighbourhood 2 (C-N2), Multi-Residential-Contextual Low Profile (M-C1) District, Commercial – Corridor 1 (C-COR1f1.0h10) District, Commercial – Corridor 1 (C-COR1f3.0h16) District, Commercial – Corridor 1 (C-COR1f3.0h23) District, Commercial – Corridor 2 (C-COR1f1.0h10) District, Commercial – Corridor 2 (C-COR1f2.0h12) District, Commercial – Corridor 2 (C-COR1f2.5h16) District

To: Mixed Use –General (MU-1f3.0h16), Mixed Use –Active Frontage (MU-2f3.0h16), Mixed Use – Active Frontage (MU-2f3.0h23)

MUNICIPAL ADDRESS:

Various Addresses

APPLICANT:

The City of Calgary

OWNER:

Various Owners

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.2

Josh Ross

COMMUNITY:

West Hillhurst (Ward 7)

FILE NUMBER:

LOC2019-0014 (CPC2019-0454)

PROPOSED REDESIGNATION:

From: Residential - Contextual One/Two Dwelling (R-C2) District

To: Multi-Residential - Contextual Grade-Oriented (M-CG) District

MUNICIPAL ADDRESS:

2402 Westmount Road NW

APPLICANT:

Elizabeth Barry

OWNER:

Nicholas Ryan
Lorraine Ryan

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.3

Courtney Stengel

COMMUNITY:

Greenview Industrial Park (Ward 4)

FILE NUMBER:

LOC2019-0010 (CPC2019-0433)

PROPOSED REDESIGNATION:

From: Industrial – General (I-G) District

To: DC Direct Control District accommodate a
greater variety of commercial uses and allow the
additional discretionary use of Place of Worship
– Large
4140 – 6 Street NE

MUNICIPAL ADDRESS:

APPLICANT:

Se7en DeSIGN

OWNER:

879076 Alberta Ltd (Samir Omar)

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.4

Steve Jones

COMMUNITY:

Arbour Lake (Ward 2)

FILE NUMBER:

LOC2018-0245 (CPC2019-0461)

PROPOSED REDESIGNATION:

From: DC Direct Control District

To: DC Direct Control District to accommodate
mixed-use development

MUNICIPAL ADDRESS:

800 Crowfoot Crescent NW

APPLICANT:

Quantumplace Developments

OWNER:

1240725 Alberta Ltd. (Landstar Development
Corporation)

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.5

Peter Schryvers

COMMUNITY:

Residual Sub-Area 02L (Ward 2)

FILE NUMBER:

LOC2018-0140 (CPC2019-0468)

PROPOSED REDESIGNATION:

From: Special Purpose – Future Urban Development (S-FUD) District

To: Special Purpose – City and Regional Infrastructure (S-CRI) District and Special Purpose – Urban Nature (S-UN) District

MUNICIPAL ADDRESS:

15454 Symons Valley Road NW

APPLICANT:

Stantec Consulting

OWNER:

Cheryl Rowlandson
Wayne Rowlandson
Cheryl Elaine Rowlandson
Nancy Faye Whatmore

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.6

Shane Gagnon/ Mike Davis
(related to Item 7.2.7)

COMMUNITY:

Saddle Ridge (Ward 5)

FILE NUMBER:

LOC2018-0170 (CPC2019-0317)

PROPOSED REDESIGNATION:

From: Residential – Narrow Parcel One Dwelling (R-1N) District, Residential – One Dwelling (R-1s) District, Residential – One / Two Dwelling (R-2) District, Residential – Low Density Multiple Dwelling (R-2M) District, Multi-Residential – Low Profile (M-1d62) District, Multi-Residential – Medium Profile Support Commercial (M-X2d185) District, Multi-Residential – Medium Profile (M-2d124) District and Special Purpose – City and Regional Infrastructure (S-CRI) District

To: Residential – Low Density Mixed Housing (R-G, R-Gm) District, Multi-Residential – Medium Profile (M-2) District, DC Direct Control District to accommodate lower-density ground-oriented residential uses and DC Direct Control District to accommodate transit infrastructure and future mixed-use development

MUNICIPAL ADDRESS:

9320 and 9325 - 52 Street NE

APPLICANT:

B&A Planning Group

OWNER:

Genstar Titleco Limited

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.7

Shane Gagnon/Mike Davis
(related to Item 7.2.6)

COMMUNITY:

Saddle Ridge (Ward 5)

FILE NUMBER:

LOC2018-0170(OP) (CPC2019-0318)

PROPOSED OUTLINE PLAN:

Subdivision of 7.73 hectares \pm (19.10 acres \pm)

MUNICIPAL ADDRESS:

9320 - 52 Street NE

APPLICANT:

B&A Planning Group

OWNER:

Genstar Titleco Limited

ADMINISTRATION RECOMMENDATION:

APPROVAL

**CONFIDENTIAL ITEMS
(CLOSED MEETING)**

ITEM NO.: 9.1

Jeff Quigley/Mona Ha

COMMUNITY:

Ward 12

FILE NUMBER:

POL2017-0010 (CPC2019-0389)

PROPOSED:

Ricardo Ranch ASP draft policy (Verbal Report)

ADMINISTRATION RECOMMENDATION:

RECEIVE FOR INFORMATION



MINUTES
CALGARY PLANNING COMMISSION

April 4, 2019, 1:00 PM
IN THE COUNCIL CHAMBER

PRESENT: Director M. Tita, Chair
Director R. Vanderputten, Vice-Chair
Councillor G. Chahal
Councillor E. Woolley
Commissioner M. Foht
Commissioner L. Juan
Commissioner A. Palmiere
Commissioner K. Schmalz
Commissioner J. Scott

ALSO PRESENT: Acting Principal Planner K. Wishlow
Acting CPC Secretary J. Dubetz
Legislative Advisor G. Chaudhary

1. **CALL TO ORDER**

Director Tita called the meeting to order at 1:00 p.m.

2. **OPENING REMARKS**

No opening remarks were provided at today's meeting.

3. **CONFIRMATION OF AGENDA**

Moved by Commissioner Foht

That the Agenda for the 2019 April 04 Regular Meeting of the Calgary Planning Commission be confirmed.

MOTION CARRIED

4. **CONFIRMATION OF MINUTES**

Moved by Councillor Chahal

That the Minutes of the following meetings be confirmed in an omnibus motion:

- 4.1 Minutes of the Regular Meeting of the Calgary Planning Commission, 2019 March 07
- 4.2 Minutes of the Regular Meeting of the Calgary Planning Commission, 2019 March 21

MOTION CARRIED5. CONSENT AGENDA

With respect to Consent Item 5.1, Report CPC2019-0376, a clerical correction was noted in the header of Attachment 2, by deleting the report number "CPC2019-0367" and replacing with the report number "CPC2019-0376".

Moved by Commissioner Scott

That the Administration Recommendations contained in the following Reports be approved in an omnibus motion:

- 5.1 Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1703 and 1707 – 31 Avenue SW, LOC2018-0218, CPC2019-0376
- 5.2 Road Closure and Land Use Amendment in Section 23 (Ward 12) portion of 86 Avenue SE, LOC2018-0228, CPC2019-0367

MOTION CARRIED6. POSTPONED REPORTS

None

7. ITEMS FROM OFFICER, ADMINISTRATION AND COMMITTEES

7.1 DEVELOPMENT ITEMS

- 7.1.1 Development Permit in Medicine Hill (Ward 6) at 1879 Na'a Drive SW, DP2018-4439, CPC2019-0373

The following items were distributed with respect to Report CPC2019-0373:

- A package of documents titled "The Village - Cell E, at Medicine Hill", dated 2019 April 04,
- A rendering from NORR, titled Townhome Plans.

Moved by Commissioner Scott

That with respect to Report CPC2019-0373, the following be approved:

That the Calgary Planning Commission APPROVE the proposed development permit application DP2018-4439 for a New: Multi-Residential Development (14 buildings with 71 townhouse units) at 1879 Na'a Drive SW (Plan 1612946, Block 2, Lot 13), with conditions (Attachment 1).

Against: Commissioner Palmiere

MOTION CARRIED

7.2 PLANNING ITEMS

7.2.1 Policy Amendment and Land Use Amendment in Bankview (Ward 8) at multiple addresses, LOC2018-0069, CPC2019-0380

Speaker Ajith Karunasena addressed the Commission with respect to Report CPC2019-0380.

Moved by Commissioner Palmiere

That with respect to Report CPC2019-0380, Administration Recommendation 3 be amended in the Mixed Use height modifier by deleting the figure "(MU-1f3.8h20.8)" and replacing with the figure "(MU-1f3.8h21)".

MOTION CARRIED

Moved by Commissioner Scott

That with respect to Report CPC2019-0380, the following be approved, **after amendment as follows:**

- In the report on page 3 of 7, **Site Context**, first sentence, by deleting the width of "20.46 metres" and replacing with the width "28.31 metres"; and
- In Attachment 3, in the document title, by deleting the word "Objection".

That the Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendment to the Bankview Area Redevelopment Plan (Attachment 2);
2. Give three readings to the proposed bylaw;
3. ADOPT, by bylaw, the proposed redesignation of 0.10 hectares ± (0.25 acres ±) located at 1437 – 19 Avenue SW and 2103 and 2107 – 14 Street SW (Plan 261L, Block 13, Lots 1 to 4) from Multi-Residential – Contextual Medium Profile (M-C2) District to Mixed Use – General (MU-1f3.8h21) District; and
4. Give three readings to the proposed bylaw.

MOTION CARRIED

7.2.2 Land Use Amendment in Lower Mount Royal (Ward 8) at 823 - 17 Avenue SW, LOC2018-0188, CPC2019-0379

A letter from Calgary Heritage Initiative, dated 2019 April 03 ,was distributed with respect to Report CPC2019-0379.

Vern Hart addressed the Commission with respect to Report CPC2019-0379.

The Calgary Planning Commission recessed at 2:08 p.m. and reconvened at 2:15 p.m. with Director Tita in the Chair.

Moved by Commissioner Juan

That the distributed letter be received for information.

MOTION CARRIED

Moved by Councillor Woolley

That with respect to Report CPC2019-0379, the following be approved:

That the Calgary Planning Commission:

1. Direct this report (CPC2019-0379) to the 2019 April 29 Combined Meeting of Council to the Public Hearing portion of the Agenda;
2. Recommend that Council hold a Public Hearing; and
 - a. ADOPT, by bylaw, the proposed redesignation of 0.14 hectares \pm (0.35 acres \pm) located at 823 – 17 Avenue SW (Plan 4453L, Block 11, Lots 4 and 5, and a portion of Lot 3) from Commercial – Corridor 1 f3.0h23 (C-COR1f3.0h23) District to DC Direct Control District to accommodate mixed-use development, with guidelines (Attachment 2); and
 - b. Give three readings to the proposed bylaw.

MOTION CARRIED

- 7.2.3 Land Use Amendment in Manchester Industrial (Ward 9) at 429 – 58 Avenue SE, LOC2019-0016, CPC2019-0362

Moved by Commissioner Juan

That with respect to Report CPC2019-03762 the following be approved:

That the Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed redesignation of 0.25 hectares \pm (0.06 acres \pm) located at 429 – 58 Avenue SE (Plan 1546LK, Block 3, Lot 1) from Industrial – General (I-G) District to Industrial – Commercial (I-C) District; and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

- 7.2.4 Land Use Amendment in East Shepard Industrial (Ward 12) at 12725 - 52 Street SE, LOC2018-0267, CPC2019-0417

Moved by Director Vanderputten

That with respect to Report CPC2019-0417, the following be approved:

That the Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed redesignation of 14.59 hectares \pm (36.07 acres \pm) located at 12725 – 52 Street SE (Portion of SE1/4 section 10-23-29-4) from DC Direct Control District to Special Purpose – City and Regional Infrastructure (S-CRI) District; and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

7.3 MISCELLANEOUS ITEMS

7.3.1 Land Use Bylaw Amendment - Enabling Pop-Up and Interim Uses, CPC2019-0281

The following documents were distributed with respect to Report CPC2019-0281:

- A revised page 2 of 6 of Attachment 1;
- A letter from Calgary Economic Development, dated 2019 April 2;
- A letter from the Federation of Calgary Communities, 2019 dated April 4.

Moved by Councillor Woolley

That with respect to Report CPC2019-0281, the following be approved:

That the Calgary Planning Commission receive this report and **revised Attachment 1** for information.

MOTION CARRIED

8. URGENT BUSINESS

None

9. CONFIDENTIAL ITEMS

Moved by Commissioner Juan

That pursuant to Section 24 (advice from officials) of the *Freedom of Information and Protection of Privacy Act*, the Calgary Planning Commission move into Closed Meeting at 3:13 p.m., in the Council Boardroom, to discuss confidential matters with respect to Item 9.1, Enabling Successful Infill Development - Options for Changes (Verbal), CPC2019-0418.

MOTION CARRIED

The Calgary Planning Commission reconvened in Public Meeting at 4:18 p.m. with Director Tita in the Chair.

Moved by Commissioner Palmiere

That the Calgary Planning Commission rise and report.

MOTION CARRIED

9.1 Enabling Successful Infill Development - Options for Changes (Verbal),
CPC2019-0418

Administration in attendance during the Closed Meeting discussions with respect to Report CPC2019-0418:

Clerk: S. Muscoby, J. Dubetz, G. Chaudhary; Advice: L. Kahn, T. Henry, J. Furness, C. Ferguson; Observer: K. Wishlow, M. Beck, J. Silot, K. Holberton.

The following materials were received for the Corporate Record with respect to Report CPC2019-0418, which are to remain confidential pursuant to Section 24 (advice from officials) of the *Freedom of Information and Protection of Privacy Act*:

- A draft document
- A presentation.

Moved by Commissioner Palmiere

That with respect to Report CPC2019-0418, the following be approved:

That the Calgary Planning Commission:

Direct that the closed session discussions, presentation and distribution remain confidential pursuant to Section 24 (advice from officials) of the *Freedom of Information and Protection of Privacy (FOIP) Act*, to be reviewed by 2019 July 31.

MOTION CARRIED

10. ADJOURNMENT

Moved by Commissioner Juan

That this Meeting adjourn at 4:20 p.m.

MOTION CARRIED

**THE FOLLOWING ITEM HAS BEEN FORWARDED TO THE 2019 APRIL 29
COMBINED MEETING OF COUNCIL:**

PLANNING MATTERS FOR PUBLIC HEARING:

- Land Use Amendment in Lower Mount Royal (Ward 8) at 823 - 17 Avenue SW, LOC2018-0188, CPC2019-0379

**THE FOLLOWING ITEMS HAVE BEEN FORWARDED TO THE 2019 MAY 27
COMBINED MEETING OF COUNCIL:**

PLANNING MATTERS FOR PUBLIC HEARING:

- Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1703 and 1707 – 31 Avenue SW, LOC2018-0218, CPC2019-0376

- Road Closure and Land Use Amendment in Section 23 (Ward 12) portion of 86 Avenue SE, LOC2018-0228, CPC2019-0367
- Policy Amendment and Land Use Amendment in Bankview (Ward 8) at multiple addresses, LOC2018-0069, CPC2019-0380
- Land Use Amendment in Manchester Industrial (Ward 9) at 429 – 58 Avenue SE, LOC2019-0016, CPC2019-0362
- Land Use Amendment in East Shepard Industrial (Ward 12) at 12725 - 52 Street SE, LOC2018-0267, CPC2019-0417

The next Regular Meeting of the Calgary Planning Commission is scheduled to be held on 2019 April 18.

CONFIRMED BY COMMISSION ON

CHAIR

ACTING CPC SECRETARY

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0431

Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 – 32 Avenue SW, LOC2018-0232

EXECUTIVE SUMMARY

This land use redesignation application was submitted on 2018 October 23 by Perry Poropat, on behalf of 2138426 Alberta Ltd (James David). This application proposes to change the designation of this property from Residential – Contextual One / Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District to allow for:

- rowhouses, in addition to building types already allowed (e.g. single detached dwellings, semi-detached dwellings, duplex homes, and secondary suites);
- a maximum building height of 11 metres (an increase from the current maximum of 10 metres);
- a maximum of four dwelling units (an increase from the current maximum of two dwelling units); and
- the uses listed in the Residential – Grade-Oriented Infill (R-CG) District.

A minor map amendment to the *South Calgary/Altadore Area Redevelopment Plan* (ARP) is required to accommodate the proposed land use redesignation. The proposal conforms to the objectives of the ARP and is in keeping with applicable policies of the *Municipal Development Plan*.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendment to the South Calgary/Altadore Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares ± (0.15 acres ±) located at 2040 – 32 Avenue SW (Plan 4479P, Block 51, Lots 19 and 20) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Residential – Grade-Oriented Infill (R-CG) District; and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0431

**Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 -
32 Avenue SW, LOC2018-0232**

BACKGROUND

This land use redesignation application was submitted to The City by Perry Poropat on behalf of 2138426 Alberta Ltd (James David) on 2018 October 23.

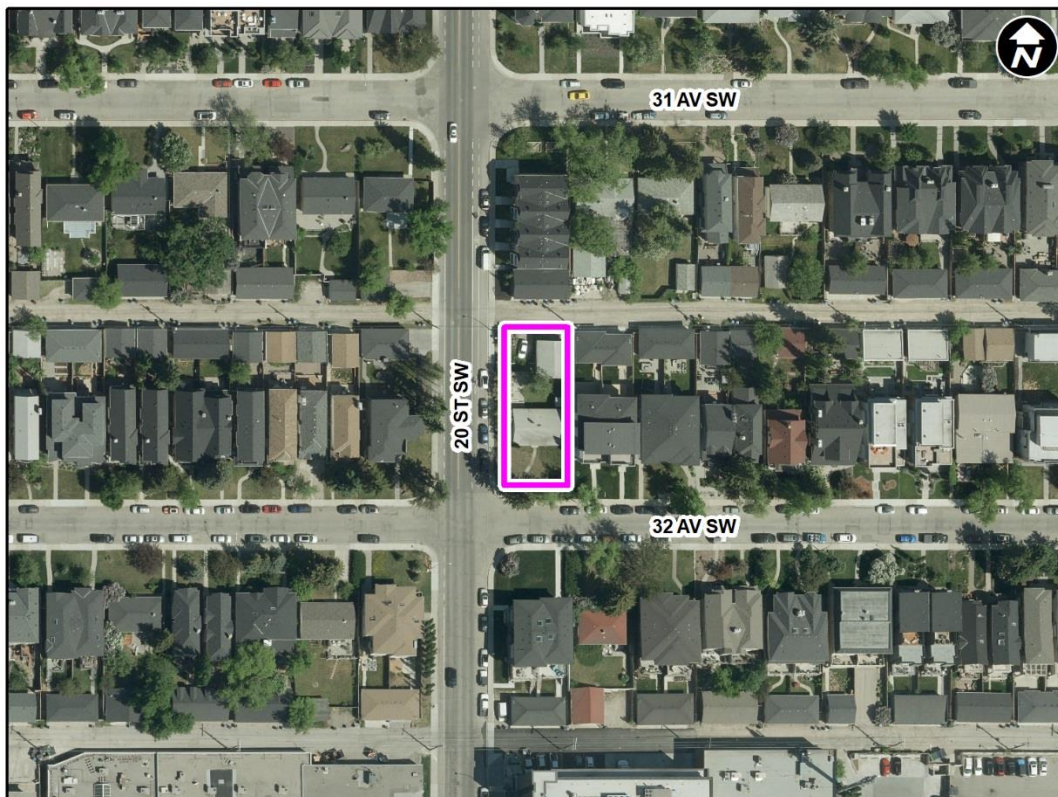
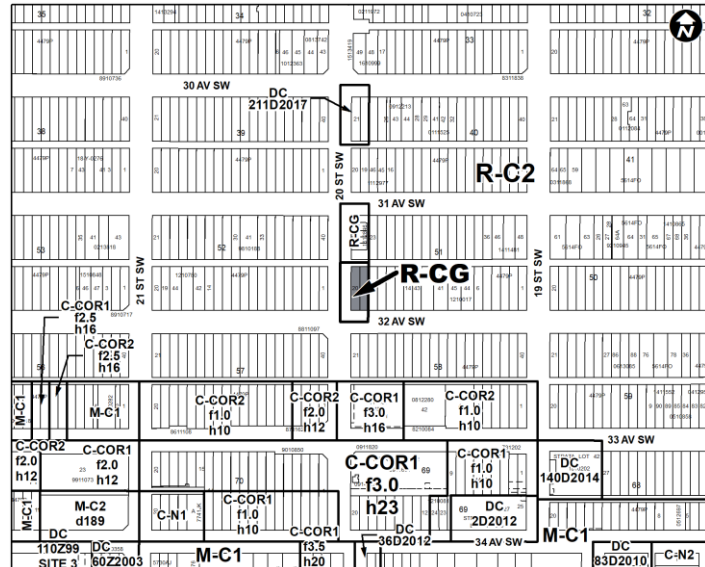
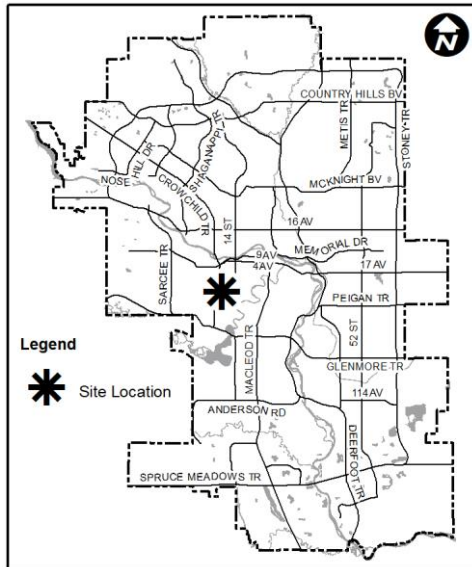
No development permit application has been submitted at this time, however, as indicated in the Applicant's Submission (Attachment 1), their intent is to pursue a development permit application for a four-unit rowhouse, subject to Council's decision on this application.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0431

Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 -
32 Avenue SW, LOC2018-0232

Location Maps



Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 - 32 Avenue SW, LOC2018-0232

Site Context

The subject site is located on the northeast corner of the intersection of 32 Avenue SW and 20 Street SW in the community of South Calgary. Surrounding development is characterized by single detached and semi-detached dwellings. A four-unit rowhouse exists across the rear lane to the north of the subject parcel. The subject parcel is one block north of 33 Avenue SW, a Neighbourhood Main Street.

The subject property is approximately 0.06 hectares in area with dimensions of approximately 15 metres by 39 metres. It is currently developed with a one-storey single detached dwelling, an accessory residential building and a detached garage. This parcel has lane access.

As seen in Figure 1 below, the community of South Calgary has experienced population growth. Peak population year was 2018.

Figure 1: Community Peak Population

South Calgary	
Peak Population Year	2018
Peak Population	4154
2018 Current Population	4154
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [South Calgary](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal represents a modest increase in density for a corner parcel of land in an inner city area and provides for a development form that will be compatible with the low density residential character of the existing neighbourhood as discussed in the Strategic Alignment section of this report.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 - 32 Avenue SW, LOC2018-0232

Land Use

The existing Residential – Contextual One / Two Dwelling (R-C2) District is a residential designation in developed areas that is primarily for single detached dwellings, semi-detached dwellings and duplex dwellings. Single detached dwellings may include a secondary suite. The R-C2 District allows for a maximum building height of 10 metres and a maximum of two dwelling units.

The proposed Residential – Grade-Oriented Infill (R-CG) District allows for two to three-storey (11 metres maximum height) rowhouse developments where one façade of each dwelling unit must directly face a public street. The district provides for a maximum density of 75 units per hectare which would enable up to four dwelling units on the subject site.

The R-CG District also allows for a range of other low density housing forms such as single detached, semi-detached, and duplex dwellings. Secondary suites (one backyard suite or secondary suite per unit) are also allowed in the R-CG District. Secondary suites do not count against allowable density and do not require motor vehicle parking stalls in the R-CG District, provided they are less than 45 square metres in size, are located within 150 metres of a frequent bus service, and provides specific indoor space for mobility alternatives such as bicycles and strollers.

Development and Site Design

A development permit has not been submitted for this parcel. The rules of the proposed Residential – Grade-Oriented Infill (R-CG) District will provide basic guidance for the site development including height and building massing, landscaping and parking. Given the specific context of this corner site, additional items that will be considered through the development permit process include:

- ensuring an engaging and activated built interface along both 20 Street SW and 32 Avenue SW;
- emphasizing individual at-grade entrances;
- provision of parking for the rowhouse development; and
- locations and screening of amenity spaces.

Environmental

There are no environmental concerns associated with the site or this proposal.

Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 - 32 Avenue SW, LOC2018-0232

Transportation

Pedestrian access to the site is available from 20 Street SW and 32 Avenue SW while vehicular access will be provided off the rear lane. The area is served by Calgary Transit, with a bus connection located within 150 metres as part of the Primary Transit Network along 33 Avenue SW. On-street parking adjacent to the site is not subject to any specific regulation. A Transportation Impact Assessment was not required as part of this application.

Utilities and Servicing

Water and sanitary sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. Storm sewer is not immediately available for connection, but appropriate stormwater management solutions as well as site servicing will be considered and reviewed at development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

A letter in support was received from the Marda Loop Community Association. The letter is included in Attachment 3 of this report, it discusses the merits of the proposed redesignation. The letter also expressed concern about continuing adhoc redesignation in the community. Administration received four letters from adjacent landowners expressing concerns regarding congested on-street parking, and loss of privacy.

Administration considered the relevant planning issues specific to the proposed redesignation and determined the proposal to be appropriate. The design compatibility of discretionary uses with respect to the surrounding neighbourhood and parking requirements will be reviewed at the development permit stage.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)* which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 - 32 Avenue SW, LOC2018-0232

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment and policy amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The subject parcel is located within the Residential – Developed – Inner City area as identified on Map 1: Urban Structure in the *Municipal Development Plan* (MDP). The applicable MDP policies encourage redevelopment and modest intensification in inner-city communities to make more efficient use of existing infrastructure, public amenities and transit. Such redevelopment is intended to occur in a form and nature that respects the scale and character of the neighbourhood context. The proposal is in keeping with relevant MDP policies as the rules of the R-CG District provide for a modest increase in density in a form that is sensitive to existing residential development in terms of height, built-form, and density.

South Calgary/Altadore Area Redevelopment Plan (Statutory – 1986)

The subject site is identified as a Residential Conservation typology, which restricts infill development to one unit per parcel. If this application is to be approved, a minor amendment to the Land Use Policy (Map 2) will be required to change the site from Residential Conservation to the Residential Low Density typology (see Attachment 2) which would allow for a greater variety of ground-oriented development. This ARP amendment is consistent with MDP policies, as described in the previous section.

Social, Environmental, Economic (External)

The recommended land use allows for a wider range of housing types than the existing R-C2 District and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics. Further, the ability to develop up to four rowhouse units will make more efficient use of existing infrastructure and services.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed land use amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0431

**Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 2040 -
32 Avenue SW, LOC2018-0232**

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the *Municipal Development Plan*. The proposed R-CG District was designed to be implemented in proximity to or directly adjacent to low density residential development. The proposal allows for a range of building types that have the ability to be compatible with the established building forms that exist in the neighbourhood and can better accommodate the housing needs of different age groups, lifestyles and demographics.

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed Amendment to the South Calgary/Altadore Area Redevelopment Plan
3. Community Association Letter

Applicant's Submission

April 4th 2019
LOC2018-0232

To Whom it may concern,

This application proposes the redesignation of the subject property from R-C2 to R-CG. There is currently a single family bungalow on the property (2040 32 Avenue SW). Redevelopment of the property is intended to take the form of a four unit rowhouse. Our intent is to provide well designed, high quality rowhouse units. We intend to fill the need in the neighbourhood for family ground-oriented units. The intent is to meet all of the Land Use Bylaw rules for Rowhouse in R-CG.

The subject site is located on a lane and parking would be provided onsite to comply with the bylaw required parking.

There is good transit access to the site along 33rd avenue one block to the south of the subject property.

The subject site is included in the Residential Developed – Inner City of the Municipal Development Plan. The Inner city Area comprises of “older housing stock in the form of moderate housing densities and a finer mix of land uses along many of the edge streets” (3.5.2).

This proposal is in line with other overarching policies of the MDP including:

2.2.5 a. Encourage growth and change in low-density neighbourhoods through development and redevelopment that is similar in scale and built form and increases the mix of housing types such as accessory suites, semi-detached, townhouses, cottage housing, row or other ground oriented housing.

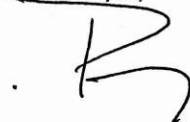
2.3.1 a. provide for a wide range of housing types tenures (rental and ownership) and densities to create diverse neighbourhoods.

The subject parcel is located in the Conservation/Infill area of the South Calgary Altadore ARP. The intent of this area is to “improve existing neighbourhood quality and character while permitting low profile infill development that is compatible with surrounding dwellings. Although the proposed land use triggers a policy amendment to align the listed land uses, our proposal still fits with the intent of the conservation/infill policy.

There is an existing four unit rowhouse directly north of the subject property, and many others in the neighbourhood, mostly on corner parcels. This application is consistent with the redevelopment in South Calgary.

I have reached out to the Community Association and adjacent neighbours about the application. The community association is in support of the applications. I plan to address many of the neighbours' concerns during the development permit stage, and intent to comply with the land use bylaw.

Thank you,



Perry Poropat

Proposed Amendment to the South Calgary/Altadore Area Redevelopment Plan

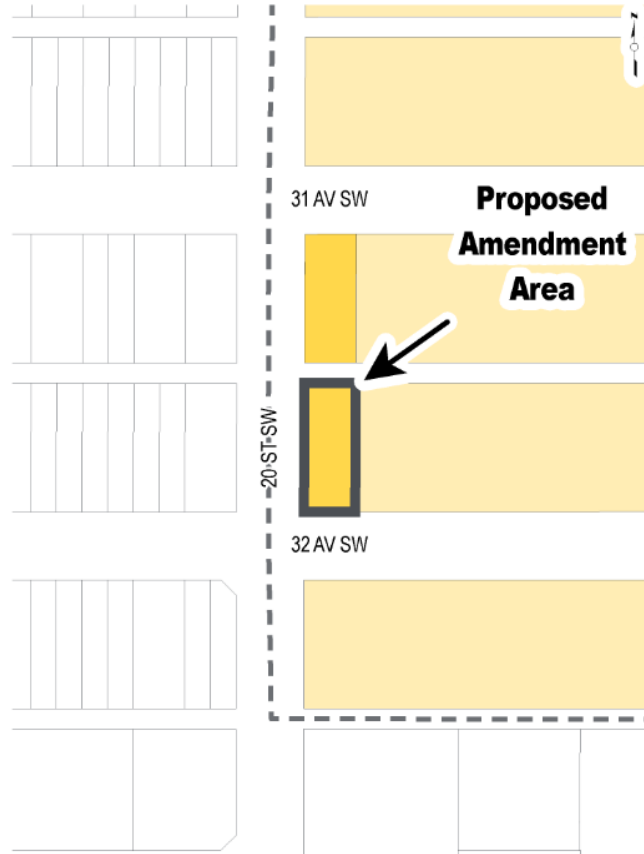
1. The South Calgary/Altadore Area Redevelopment Plan attached to and forming part of Bylaw 13P86, as amended, is hereby further amended as follows:
 - (a) Amend Map 2 entitled 'Land Use Policy', by changing 0.06 hectares \pm (0.15 acres \pm) located at 2040 – 32 Avenue SW (Plan 4479P, Block 51, Lots 19 and 20) from 'Residential Conservation' to 'Residential Low Density' as generally illustrated in the sketch below:

Map 2

Land Use Policy

Legend

- Study Area Boundary
- Residential Conservation
- Residential Low Density



This map is conceptual only. No measurements of distances or areas should be taken from this map.

Community Association Letter



3130 16 Street SW
Calgary, AB, T2T
4G7

November 26,

Circulation Control
Planning & Development #8201
PO Box 2100 Station M
Calgary, AB, T2P 2M5
Email: CPAG.Circ@calgary.ca

Attn: Kaitlin Bahl, kaitlin.bahl@calgary.ca

SENT BY EMAIL

Dear Ms. Bahl;

RE: Community Association Feedback for LOC2018-0232

Thank you for providing us the opportunity to offer feedback on this application that reflects the vision of the Marda Loop Communities Association (MLCA). The following comments have been written with consideration towards what is best for our communities.

This application proposes a change from R-C2 to R-CG. The parcel is located in an area of the community currently undergoing the later stages of the Main Streets initiative, where this portion was recommended to be rezoned for R-CG. Should the zoning proposed in the most recent Main Streets plan be approved, this application would be appropriate within those proposed land-uses.

Absent of the formalization of these higher-level changes at the time of this application, the committee reviews these applications based on The City's Guideline Criteria for Multi-residential Infills.

Upon review, this location meets the following criteria for support:

- (1) Corner parcel;
- (2) Within 400 metres of a transit stop;
- (4) On a collector or higher-standard roadway on at least one frontage;

Community Association Letter

- (7) Along or in close proximity to an existing or planned corridor or activity centre;
- (8) Direct lane access.

The MLCA Planning & Development Committee continues to encourage applicants and developers to engage with us and neighbours of proposed developments proactively, prior to application. Proactive communication allows time for facilitating constructive dialogue. The applicant has contacted the Community Association regarding this application and to date, the MLCA has received no comments from residents. Should this application be approved, the Committee would like to see efforts taken by the applicant to meaningfully engage with affected neighbours and community members.

However, the current pattern of implementing land use changes on an ad hoc basis continues to cause extreme frustration and uncertainty in the community. Debating and engaging on the land use for each and every application requires valuable time and resources for all parties involved, including community members, MLCA volunteers, applicants, City Administration, Calgary Planning Commission, and City Council. The MLCA would request The City be forthcoming of their delivery of the Main Streets/Streetscapes initiatives in order to achieve continuity and stability in our community.

If you have any questions regarding these comments, please contact me at your convenience.
Thank you in advance for considering these comments when assessing the merits of this application.

Regards,



MacKenzie Kroeger
Director, Planning & Development Committee
Marda Loop Communities Association
development@mardaloop.com



Doug Fraser
President
Marda Loop Communities Association
president@mardaloop.com

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0425

Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40 Avenue SW, LOC2018-0207

EXECUTIVE SUMMARY

This land use amendment application was submitted by PLP Design on 2018 September 14 on behalf of PE Comfort Homes and the landowners, Aliya and Imran Nurani. This application proposes to change the designation of these parcels to allow for:

- rowhouses in addition to the building types already allowed (e.g. single-detached, semi-detached, and duplex homes and secondary suites);
- a maximum building height of 11 metres (an increase from the current maximum of 10 metres);
- a maximum of four dwelling units (an increase from the current maximum of two dwelling units); and
- the uses listed in the proposed Residential – Grade-Oriented Infill (R-CG) District.

This application is compatible with the applicable policies identified in the *Municipal Development Plan* (MDP). To accommodate this proposal, a minor amendment to the *South Calgary/Altadore Area Redevelopment Plan* (ARP) is required.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to the South Calgary/Altadore Area Redevelopment Plan (Attachment 2);
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares ± (0.12 acres ±) located at 1915 – 40 Avenue SW (Plan 5641S, Block B, Lots 1 and 2) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Residential – Grade-Oriented Infill (R-CG) District; and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0425

**Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40
Avenue SW, LOC2018-0207**

BACKGROUND

This application was submitted by PLP Design on 2018 September 14 on behalf of PE Comfort Homes and the landowners, Aliya and Imran Nurani. The subject site is located in the community of Altadore, on a corner parcel, that is south of 40 Avenue SW and east of 19 Street SW.

No development permit application has been submitted at this time.

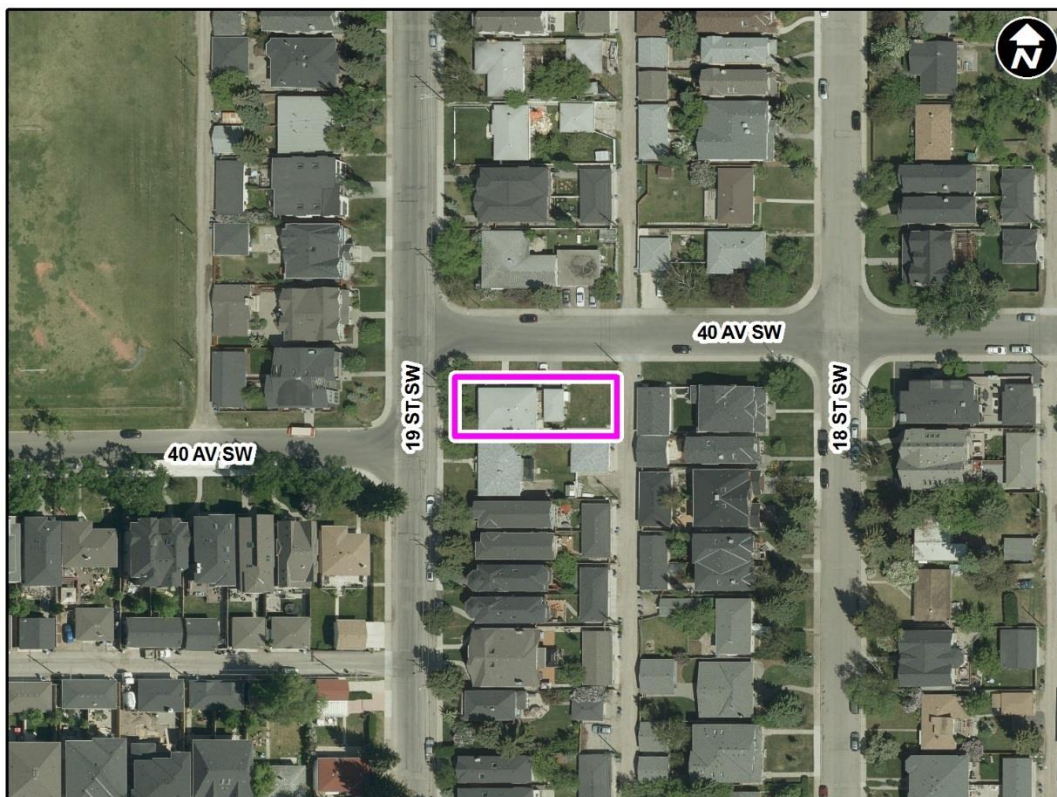
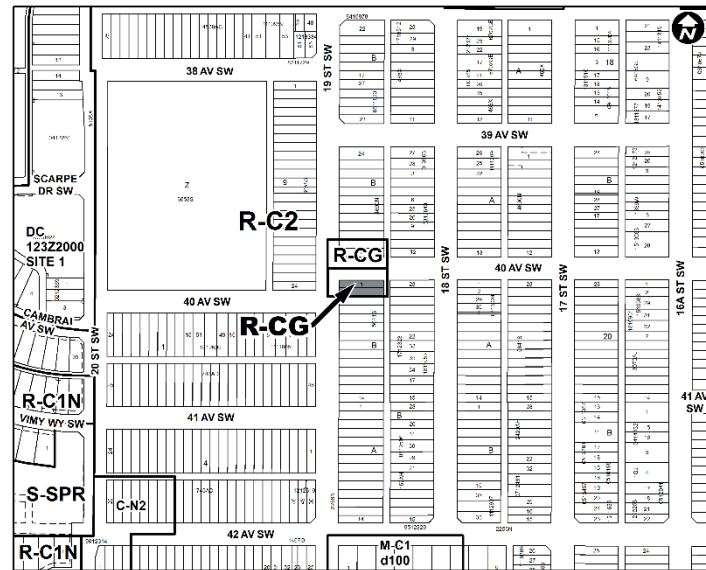
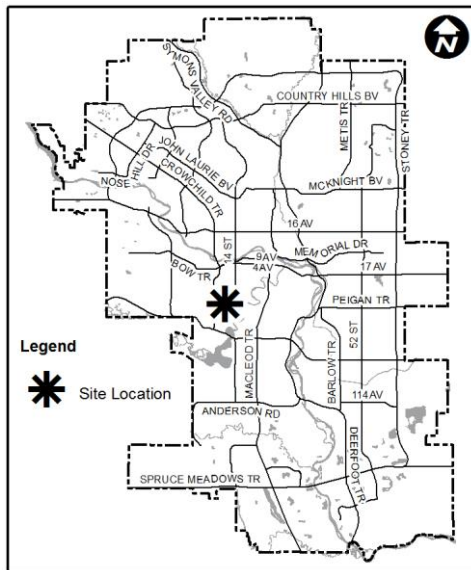
As noted in the Applicant's Submission and conceptual site plan (Attachment 1), they intend to build a three-storey grade-oriented building containing four residential units. This development would include a four-vehicle garage that accesses the lane.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0425

Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40
Avenue SW, LOC2018-0207

Location Maps



Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40 Avenue SW, LOC2018-0207

Site Context

The subject site, which is approximately 15 metres by 40 metres, is located in the community of Altadore, south of 40 Avenue SW and east of 19 Street SW. One-storey single detached dwellings are located to the north and south. A two-storey single detached dwelling is located to the east and a three-storey single detached dwelling to the west. All surrounding parcels are Residential – Contextual One / Two Dwelling (R-C2) District except for north parcel which is designated R-CG District. Doctor Oakley School is located one block west of the subject site.

The subject site is located on a corner parcel within a low density residential area. The site is within 800 metres of 33 Avenue SW, which is a Neighborhood Main Street and part of the Primary Transit Network.

The subject site is relatively flat and is currently developed with a 1950s single detached dwelling.

As identified in Figure 1, the community of Altadore has experienced a population decline from its peak in 2015.

Figure 1: Community Peak Population

	Altadore	Garrison Woods	COMBINED*
Peak Population year	2015	2016	2018
Peak Population	9867	3188	
2018 Current Population	6,831	3,184	10,015
Difference in Population (Number)	-3036	-4	0
Difference in Population (Percent)	-30%	-0.1%	0%

Source: The City of Calgary 2018 Civic Census

* Note that the boundary for the Community of Altadore was redrawn in 2016 to exclude the Community of Garrison Woods. As such, Altadore's population appears artificially lower in the years following. To resolve this, the above table combines the population of Altadore and Garrison Woods accordingly.

Additional demographic and socio-economic information may be obtained online through the [Altadore](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

On 2018 September 14, a land use amendment application was submitted that proposed to change the designation of a single residential parcel to R-CG District. After review, Administration determined that this application aligned with the *Municipal Development Plan* (MDP). The existing local area policy, the *South Calgary/Altadore Area Redevelopment Plan* (ARP), identifies this parcel as part of the Residential Conservation land use area, which encourages the preservation of existing low profile structures. To enable this application, a minor amendment to the land use map contained within the ARP is required to identify the parcel as part of the Residential Low Density area.

Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40 Avenue SW, LOC2018-0207

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

The existing R-C2 District allows for contextually sensitive redevelopment in the form of duplex dwellings, semi-detached dwellings and single detached dwellings. This application proposes to change the designation of the site to R-CG District to allow for rowhouses in addition to the uses already allowed (e.g. single-detached, semi-detached, and duplex homes and secondary suites), a maximum building height of 11 metres (an increase from the current maximum of 10 metres), a maximum of 4 dwelling units (an increase from the current maximum of 2 dwelling units) and the uses listed in the proposed R-CG District.

The proposed district is appropriate for this area as it is contextually appropriate with the surrounding low density residential area in that it would encourage development that respects the scale and character of the neighborhood.

Development and Site Design

The rules of the proposed R-CG District will provide basic guidance for the future site development including appropriate uses, height and building massing, landscaping and parking. Given the specific context of this corner site, additional items that are currently being considered through the development permit process include, but are not limited to:

- ensuring an engaging built interface along both the 19 Avenue NW and 40 Street NW frontages;
- improving pedestrian connections along 40 Street NW by ensuring vehicle access to the site is off the lane; and
- mitigating concerns about overlooking and privacy.

Environmental

No environmental issues have been identified at this time.

Transportation

The subject site is located on a corner lot with lane access, is not in a Residential Parking zone and there are no on-street parking restrictions. There is an existing curb cut on 40 Avenue SW that will have to be closed and rehabilitated at the time of redevelopment, and access will be from the lane. Parking shall be provided at the time of development permit, and shall meet all the required specifications and bylaw requirements for parking stall size, amount, etc.

Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40 Avenue SW, LOC2018-0207

The subject site is located approximately 250 metres from a northbound Route 7 Marda Loop bus stop, which provides service to the Downtown Core, and LRT routes. It's also approximately 250 metres from a southbound Route 7 Marda Loop bus stop, which ends up at a bus loop on 54 Avenue SW just east of Crowchild Trail SW which provides access to Routes 18, 20, 63, 182, and 306 BRT which is a primary bus route, approximately 2.15 kilometres away.

Utilities and Servicing

Public water, sanitary and storm exist within the adjacent public right-of-way. Development servicing will be determined at the development permit and development site servicing plan (DSSP) stage.

Stakeholder Engagement, Research and Communication

Communications

In keeping with Administration's standard practices, this application was circulated to relevant citizens and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised on-line.

The Marda Loop Community Association was circulated as part of Administration's standard practice. No response was received prior to the CPC report deadline.

Administration received 14 objection letters from citizens and are summarized as follows:

- ARP does not allow multi-residential in this area.
- This application does not comply with *Location Criteria for Multi-Residential Infill*.
- There's been no public engagement by the applicant.
- The proposed change will have a perceived negative impact on:
 - neighborhood character;
 - surrounding neighbors privacy (overlooking backyard) and enjoyment (noise & traffic);
 - availability of "green space" (reference to landscaping within front setback area);
 - availability of on-street parking;
 - water/wastewater infrastructure; and
 - surrounding property value.

Administration considered the concerns expressed by the in the circulation comments received as part of the review of this proposed land use amendment application:

- The proposed minor map amendment to the ARP will bring policy into alignment with this application;

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Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0425

**Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40
Avenue SW, LOC2018-0207**

- The *Location Criteria for Multi-Residential Infill* was not evaluated as part of Administrations' review because the proposed land use district is classified as a low density residential district and not multi-residential.
- The applicant was advised by Administration to undertake public engagement for the land use application, but has advised that they intend to do so at the future development permit stage.
- The MDP provides policy direction so as to "Respect the existing character of low-density residential areas, while still allowing for innovative and creative designs that foster distinctiveness" (MDP 2.3.2(a)). The R-CG District aligns with this policy as it is a low density residential district that is intended to accommodate existing residential development while also providing flexible rules that facilitate integration of a diversity of grade-oriented housing over time.
- Building and site development concerns including privacy of the surrounding neighbors, parking requirements and landscaping are addressed as part of the review of a future development permit application.
- As part of the Corporate Planning Applications Group (CPAG) review process, both Transportation and Development Engineering have confirmed that the existing utility infrastructure in the area can accommodate the proposed land use servicing issues. Servicing connections will be further investigated as part of a future development permit application.
- Property value is not a planning consideration.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Applicant-Led Engagement

The applicant has stated that they do not plan on undertaking any public engagement at part of this application and will instead do so at the future development permit stage. They have also stated that they do not wish to submit a concurrent development permit application.

No City-led engagement was undertaken as part of this application.

Planning & Development Report to
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ISC: UNRESTRICTED
CPC2019-0425

Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40 Avenue SW, LOC2018-0207

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The site is located within the City, Town area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The subject site is located within the Residential – Developed – Inner City area as identified on Map 1: Urban Structure in the MDP. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies regarding respecting and enhancing neighborhood character, general developed residential areas and established areas land use.

South Calgary/Altadore Area Redevelopment Plan (Statutory – 1986)

The subject site is identified as a Residential Conservation typology, which restricts infill development to one unit per parcel. If this application is to be approved, a minor amendment to the Land Use Policy (Map 2) will be required to change the site from Residential Conservation to the Residential Low Density typology (see Attachment 2) which would allow for a greater variety of ground-oriented development. This ARP amendment is consistent with MDP policies, as described in the previous section.

Social, Environmental, Economic (External)

The recommended land use amendment will provide for a moderate increase in residential density, allowing for a more efficient use of existing public infrastructure. In addition, this proposal would encourage an increase in socio-economic diversity within the area by providing a variety of housing types and forms.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0425

**Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1915 - 40
Avenue SW, LOC2018-0207**

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is consistent with the applicable policies of the *Municipal Development Plan*. To accommodate this proposal, a minor amendment to the *South Calgary/Altadore Area Redevelopment Plan* (ARP) is required.

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed Amendment to the South Calgary/Altadore Area Redevelopment Plan

Applicant's Submission

To whom it may concern;

Our basis for applying for a Land Use Redesignation is as follows.

- We are looking to increase the density on the lot to provide a more affordable housing option for this area. This is a very popular area in the city which has seen an abundance of redevelopment. A large majority of that redevelopment to date is in the form of single family and semi-detached homes which usually comes with a large price tag and is unattainable for many.
- The increase in density would be in line with other new development applications in the neighborhood which have been approved.
- For a lot of people Single Family and Semi-Detached properties are simply out of reach due to cost. Bringing in a development that maintains the quality of building that is seen throughout the neighborhood at a lower price point would be beneficial for the community.
- We believe this location would be great for higher density as it is in close proximity to a school. As such a development like this would be attractive to young families which would likely stay in the community for years to come.
- The location of this property is in a quiet area at 19th Street and 40th Avenue. There is ample access to this property and the increase in density would not negatively affect the traffic or parking.
- The initial comments we have received have been in regards to property value and parking concerns. Having higher density is not always a negative to property values as it encourages people to move into an area and stay. This generally leads to people buying into larger homes as they get older as they can afford larger properties. Having these long time owners helps to create a sense of community and belonging which over time increase the appeal to an area. It also brings in more tax dollars to maintain adequate services throughout the city. In regards to parking we propose to provide on site parking for all units. This will limit the need for residents to park on the street.

Thank you for taking the time to review this application.

Applicant's Submission



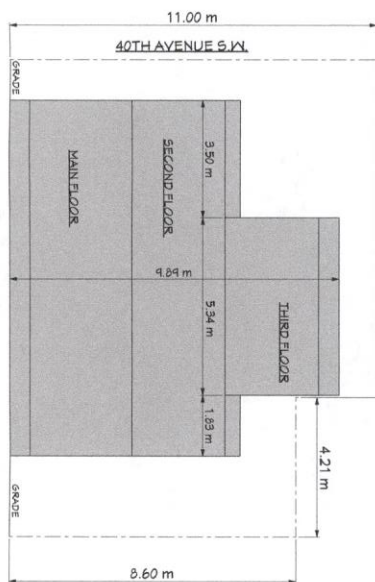
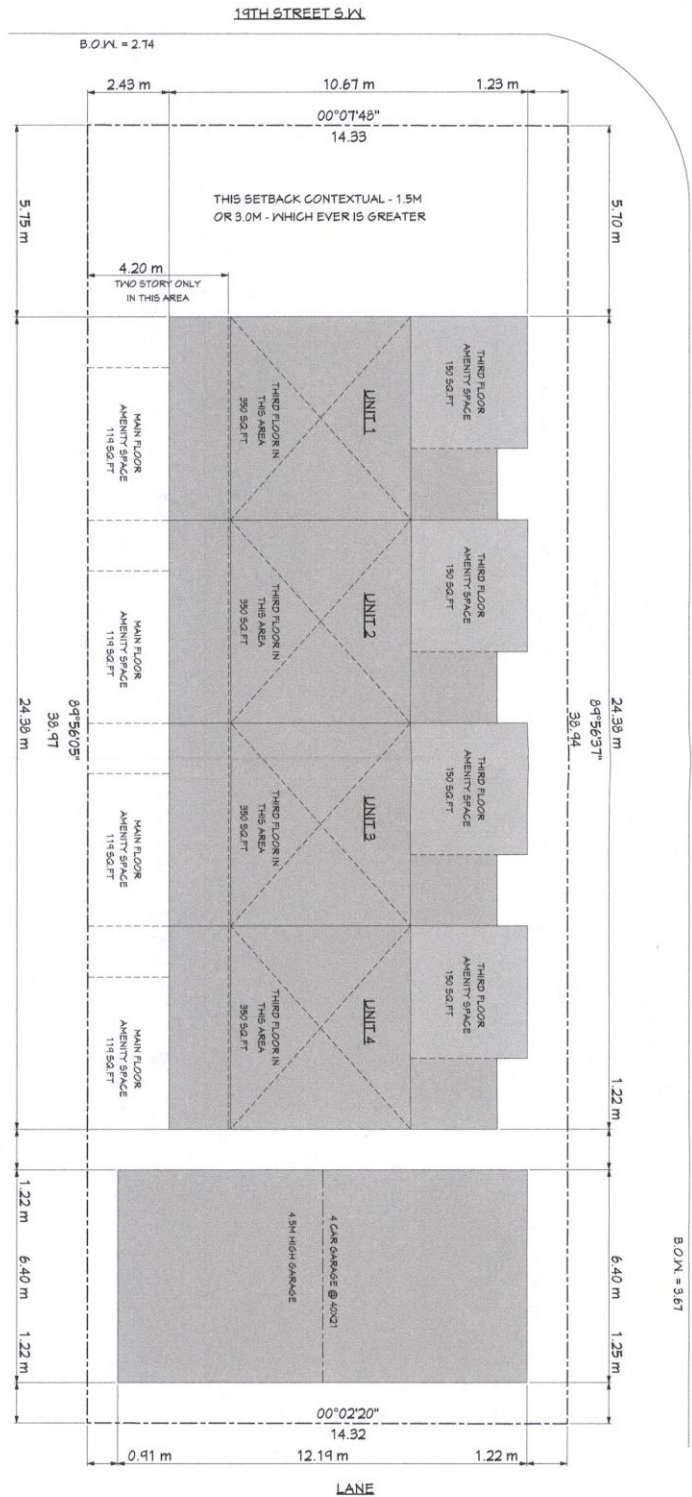
1
A1
SITE PLAN
SCALE = 1:150

UNITS = 2716 SQ FT (252.32 SQ M)
TOTAL COVERAGE = 3986 SQ FT (369.36 SQ M)
= 54.2%

UPH @ 4 UNITS = 66.61 UPH
GFA = 2716 SQ FT (252.32 SQ M)
FAR = 0.45

4 UNITS @ 679 SQ FT MAIN FLOOR
@ 679 SQ FT SECOND FLOOR
@ 360 SQ FT THIRD FLOOR
THESE NUMBERS ARE APPROXIMATE AND
MAY CHANGE BASED ON FINAL DESIGN

264 SQ FT (24.49 SQ M) AVENITY SPACE
EACH UNIT



2
A1
CROSS SECTION
SCALE = 1:150

Proposed Amendment to the South Calgary/Altadore Area Redevelopment Plan

1. The South Calgary/Altadore Area Redevelopment Plan attached to and forming part of Bylaw 13P81, as amended, is hereby further amended as follows:
 - (a) Amend Map 2 entitled 'Land Use Policy', by changing 0.05 hectares \pm (0.12 acres \pm) located at 1915 - 40 Avenue SW (Plan 5641S, Block B, Lots 1 and 2) from 'Residential Conservation' to 'Residential Low Density' as generally shown in the sketch below:

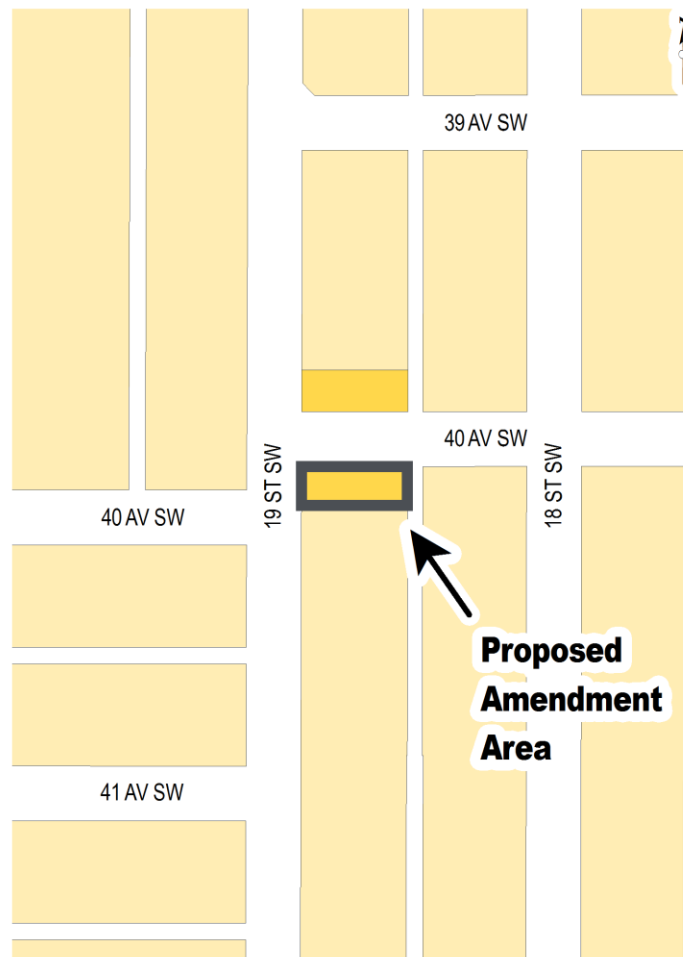
Map 2

Land Use Policy

Legend

- ■ — Study Area Boundary
- King Edward School Site
- Residential Conservation
- Residential Low Density
- Residential Medium Density
- Community Mid-Rise
- Local Commercial
- General Commercial
- Open Space

This map is conceptual only. No measurements of distances or areas should be taken from this map.



Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0434

**Land Use Amendment in West Hillhurst (Ward 7) at 2339 – 5 Avenue NW,
LOC2019-0013**

EXECUTIVE SUMMARY

This land use amendment application was submitted by Les March from Hunter Tristan Design on behalf of the landowner Muhammad Ali Zain, on 2019 January 30. This application proposes to change the designation of this property from Residential – Contextual One / Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District to allow for:

- rowhouses in addition to building types already allowed (e.g. single detached homes, semi-detached, and duplex homes and secondary suites);
- a maximum building height of 11 metres (an increase from the current maximum of 10 metres);
- a maximum of three dwelling units (an increase from the current maximum of two dwelling units); and
- the uses listed in the proposed R-CG District.

The proposal conforms to the relevant policies of the *Municipal Development Plan*. A development permit application for a three-unit rowhouse is currently under review.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares \pm (0.12 acres \pm) located at 2339 – 5 Avenue NW (Plan 710N, Block 19, Lots 1 and 2) from Residential – Contextual One / Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

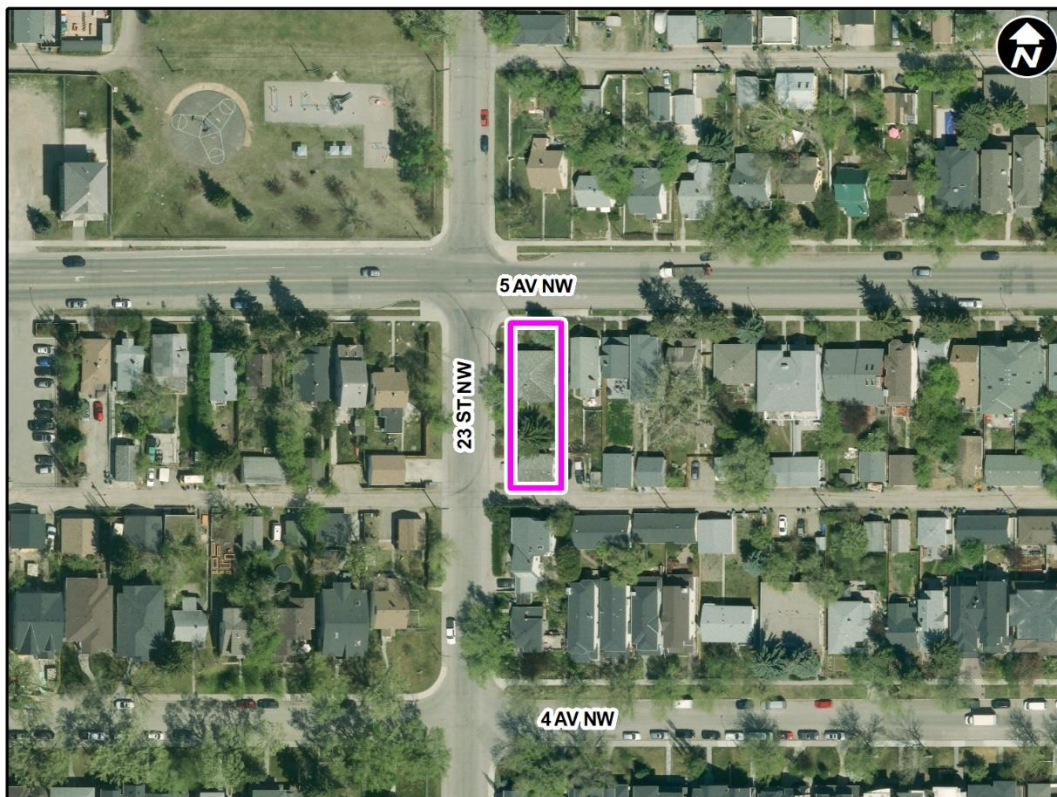
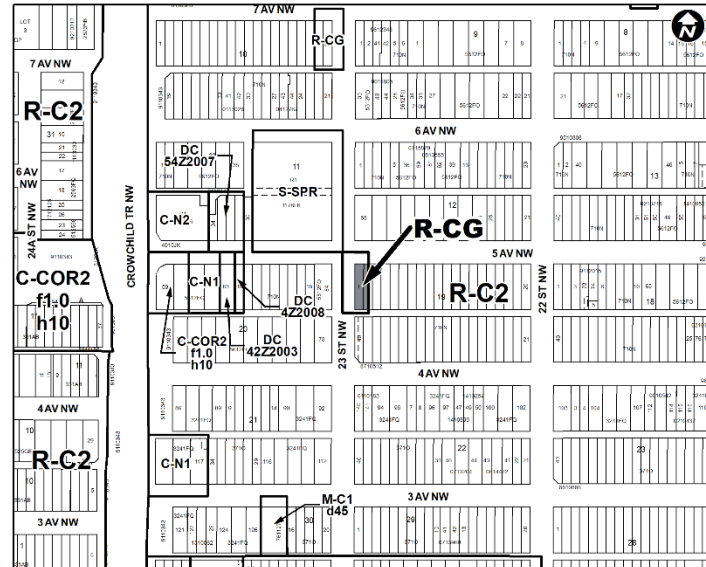
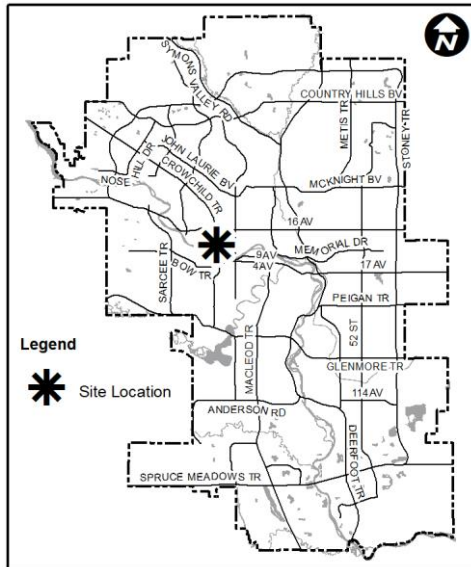
This land use amendment application was submitted by Hunter Tristan Design on behalf of the landowner Muhammad Ali Zain on 2019 January 30 (Attachment 1). A development permit application for a three-unit rowhouse development (DP2019-0470) was submitted by Hunter Tristan Design on 2019 January 30 and is currently under review (Attachment 2).

Planning & Development Report to
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ISC: UNRESTRICTED
CPC2019-0434

Land Use Amendment in West Hillhurst (Ward 7) at 2339 - 5 Avenue NW,
LOC2019-0013

Location Maps



Land Use Amendment in West Hillhurst (Ward 7) at 2339 - 5 Avenue NW, LOC2019-0013

Site Context

The subject site is located in the community of West Hillhurst at the southeast corner of 5 Avenue NW and 23 Street NW. The site is approximately 0.05 hectares in size with 12.19 metres of frontage along 5 Avenue NW and a depth of 39.66 metres. There is a rear lane along the south end of the site. Currently the property is developed with a one-storey single detached dwelling and a detached double-car garage accessed from 23 Street NW.

Surrounding development is characterized by a mix of single and semi-detached dwellings. Grand Trunk Park is located directly across from the site at the northwest corner of 5 Avenue NW and 23 Street NW and Crowchild Trail NW is approximately one block to the west. A large proportion of the lands within close proximity of the site are guided by the Residential – Contextual One / Two Dwelling (R-C2) District. A collection of parcels located adjacent to Crowchild Trail NW (one block west of the site) are subject to a mix of Commercial – Neighbourhood 1 (C-N1) District, Commercial – Neighbourhood 2 (C-N2) District, and Commercial – Corridor 2 (C-COR2) District.

As identified in *Figure 1*, the community of West Hillhurst reached peak population in 1968.

Figure 1: Community Peak Population

West Hillhurst	
Peak Population Year	1968
Peak Population	6,871
2018 Population	6,507
Difference in Population (Number)	-364
Difference in Population (Percent)	-5%

Source: *The City of Calgary 2018 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [West Hillhurst](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal represents a modest increase in density for a corner parcel of land in an inner city area and allows for a range of building forms that respect the scale and character of the existing neighbourhood. This site is not currently subject to a local area plan.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

**Land Use Amendment in West Hillhurst (Ward 7) at 2339 - 5 Avenue NW,
LOC2019-0013**

The existing Residential – Contextual One / Two Dwelling (R-C2) District is a residential designation applied to developed areas that is primarily for single detached, semi-detached and duplex homes. Single detached homes may include a secondary suite, but semi-detached homes may not. The R-C2 District allows for a maximum building height of 10 metres and a maximum of two dwelling units.

The proposed Residential – Grade-Oriented Infill (R-CG) District allows for two to three-storey (11 metres maximum height) rowhouse developments where one façade of each dwelling unit must directly face a public street. The district provides for a maximum density of 75 units per hectare which would enable up to three dwelling units on the subject site. The R-CG District also allows for a range of other low density housing forms such as single detached, semi-detached, duplex dwellings and secondary suites. Administration does not consider secondary suites as an additional dwelling unit.

Development and Site Design

The rules of the proposed Residential – Grade-Oriented Infill (R-CG) District will provide basic guidance for the future site development including appropriate uses, building massing and height, landscaping and parking. Given the specific context of this corner site, additional items that are currently being considered through the development permit process include, but are not limited to:

- ensuring an engaging built interface along both the 5 Avenue NW and 23 Street NW frontages;
- improving pedestrian connections along 23 Street NW by ensuring vehicle access to the site is off the lane; and
- mitigation of overlooking and privacy concerns.

Environmental

There are no environmental concerns associated with the site or this proposal.

Transportation

Pedestrian access to the site is available from existing sidewalks along 5 Avenue NW and 23 Street NW. Vehicular access is currently provided from an existing driveway on 23 Street NW but will be provided from the rear lane in a redevelopment scenario. Parking is available on both 5 Avenue NW and 23 Street NW. Both streets are snow routes and may experience parking bans at times over the winter.

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Land Use Amendment in West Hillhurst (Ward 7) at 2339 - 5 Avenue NW, LOC2019-0013

The site is serviced by Calgary Transit with local standard transit bus stops located approximately 110 metres (one-minute walking distance) west of the site along 5 Avenue NW. Further, the site is within 600 metres of a BRT stop for the 305 Bowness-City Centre route located south along Kensington Road. The closest LRT station (Lion's Park) is approximately 1.6 kilometres (approximately 20 min walking distance) northeast.

A Transportation Impact Assessment was not required as part of this application.

Utilities and Servicing

Water and sanitary mains are available and can accommodate potential redevelopment of the subject site without the need for off-site improvements at this time. Individual servicing connections as well as appropriate stormwater management is being considered and reviewed as part of the development permit.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to external stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

Administration received an email from the West Hillhurst Community Association on 2019 February 28 indicating they have no comments on the land use amendment.

Administration received one citizen response noting concerns related to the potential height and shadowing impacts of a new building in the R-CG District. In this regard, the R-CG District has a height chamfer rule to decrease building height/massing in proximity to a shared property line with a low density residential district. Additional design measures to limit potential massing and shadowing concerns can also be addressed as part of the development permit review process.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The site is located within the City, Town area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

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**Land Use Amendment in West Hillhurst (Ward 7) at 2339 - 5 Avenue NW,
LOC2019-0013**

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The subject parcel is located within the Residential - Developed - Inner City area as identified on Map 1: Urban Structure in the *Municipal Development Plan* (MDP). The applicable MDP policies encourage redevelopment and modest intensification of inner-city communities to make more efficient use of existing infrastructure, public amenities and transit. Such redevelopment is intended to occur in a form and nature that respects the scale and character of the neighbourhood context.

The proposal is in keeping with relevant MDP policies as the R-CG District provides for a modest increase in density in a form that is sensitive to existing residential development in terms of height, scale and massing.

Social, Environmental, Economic (External)

The recommended land use allows for a wider range of housing types than the existing R-C2 District and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics. Further, the ability to develop up to three rowhouse units will make more efficient use of existing infrastructure and services.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

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CPC2019-0434

**Land Use Amendment in West Hillhurst (Ward 7) at 2339 - 5 Avenue NW,
LOC2019-0013**

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the *Municipal Development Plan*. The proposed R-CG District was designed to be implemented in proximity to or directly adjacent to low density residential development. The proposal represents a modest density increase of an inner-city parcel of land and allows for development that has the ability to be compatible with the character of the existing neighbourhood.

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed Development (DP2019-0470) Summary

Applicant's Submission

Jerry Homes is submitting a land use redesignation on the address 2339 5 Ave NW. The community of West Hillhurst already sees a significant amount of gentrification and redevelopment, lending this project to not only meet current community expectations, as far as design and quality, but to exceed these standards and help raise future levels of home designs.

Increasing density is also important for cities and communities as people wanting to live closer to their places of work and maintain higher levels of health (cutting down on commute times).

This project, although on a typically more shallow lot, sees the negative and turns it in to a positive by giving the home owners a more 'square' footprint rather than the more common 'long and narrow' design.

In a nod to the history of the community, we have decided not to go with the more trendy and fad look of Modern, but to rather go with the timeless look of Farmhouse (also a nod to the western heritage of Calgary/Alberta).

Proposed Development (DP2019-0470) Summary

A development permit application (DP2019-0470) was submitted by Hunter Tristan Design on 2019 January 30. The development permit proposes a three-storey three-unit rowhouse with a detached garage to accommodate parking accessed off of the lane. The following images (Figures 1, 2, and 3) are included for information purposes only, and provide a general overview of the proposed development.

The development permit is under review, which will determine the ultimate building design, number of units, and site layout details such as site access and parking, landscaping and amenity space. No decision will be made on the development permit application until council has made a decision on this land use redesignation.

Figure 1: Site Plan

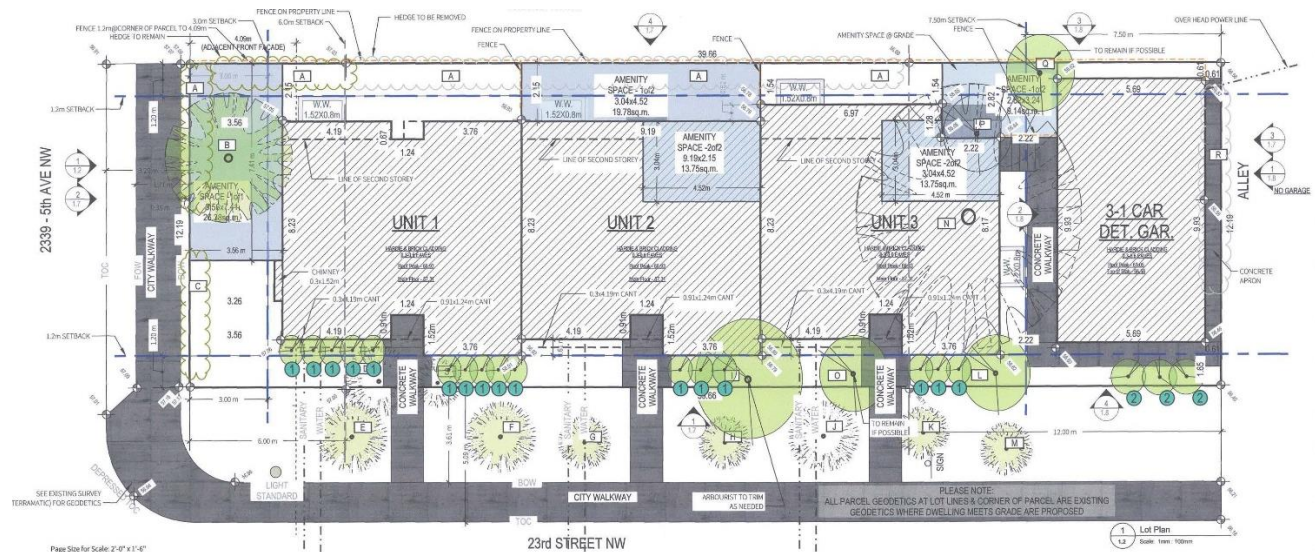
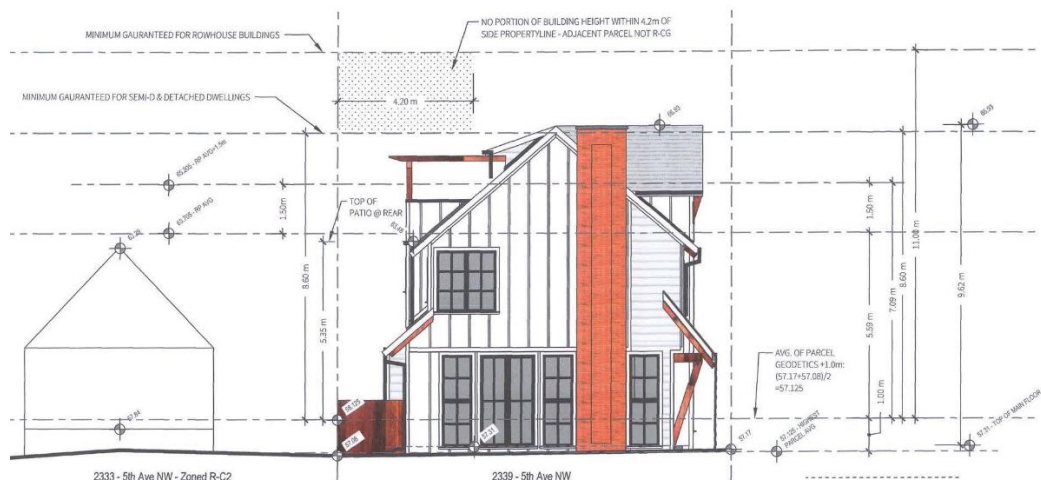
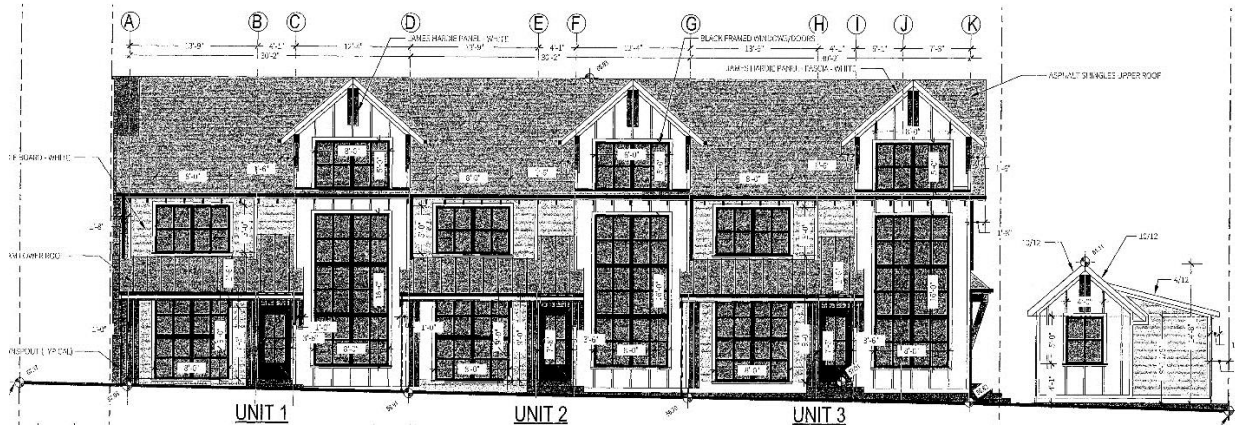


Figure 2: North Elevation – Facing 5 Avenue NW



Proposed Development (DP2019-0470) Summary

Figure 3: West Elevation – Facing 23 Street NW



Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0377

Road Closure and Land Use Amendment in Bridgeland/Riverside (Ward 9) adjacent to 23 McDougall Court NE, LOC2018-0252

EXECUTIVE SUMMARY

This road closure and land use redesignation application was submitted by Max Tayefi Architect on 2018 November 16, with authorization from the landowner, The City of Calgary. The application proposes to close a portion of undesignated road right-of-way that exists adjacent to 23 McDougall Court NE and redesignate the area of closed road to Multi-Residential – Contextual Low Profile (M-C1) District to allow for:

- extension of the existing Assisted Living use on the adjacent site, including administrative offices;
- a maximum density of 148 units per hectare on the site;
- a maximum building height of 14 metres; and
- the uses listed under the M-C1 District.

This proposal is in compliance with the applicable policies of the *Municipal Development Plan* and the *Bridgeland-Riverside Area Redevelopment Plan*. No development permit has been submitted at this time.

ADMINISTRATION'S RECOMMENDATION:

That Calgary Planning Commission recommends that Council hold a Public Meeting, and

1. **ADOPT**, by bylaw, the proposed closure of 0.10 hectares \pm (0.25 acres \pm) of road (Plan 1910503, Area A, Area B, Area C, Area D and Area E) adjacent to 23 McDougall Court NE, with conditions (Attachment 3); and
2. Give three readings to the proposed closure bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 0.10 hectares \pm (0.25 acres \pm) of closed road (Plan 1910503, Area A, Area B, Area C, Area D and Area E) adjacent to 23 McDougall Court NE from Undesignated Road Right-of-Way **to** Multi-Residential – Contextual Low Profile (M-C1) District; and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

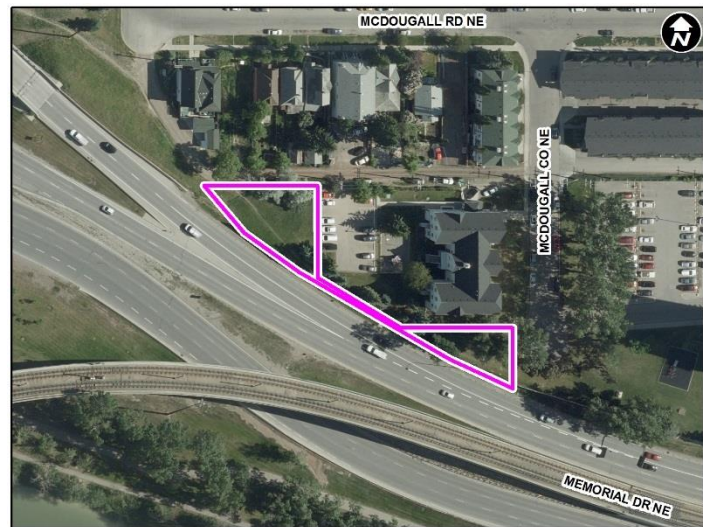
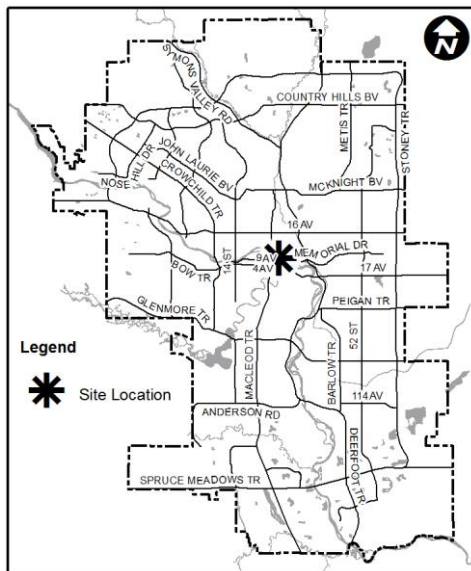
This application was submitted by Max Tayefi Architect on 2018 November 16, on behalf of the land owner The City of Calgary. While no development permit has been submitted at this time, the applicant indicated their intent to extend the existing use in the future. In order to accommodate this, the area of undesignated road right-of-way is proposed to be closed and redesignated to M-C1 District, in accordance with the Applicant's Submission (Attachment 1).

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0377

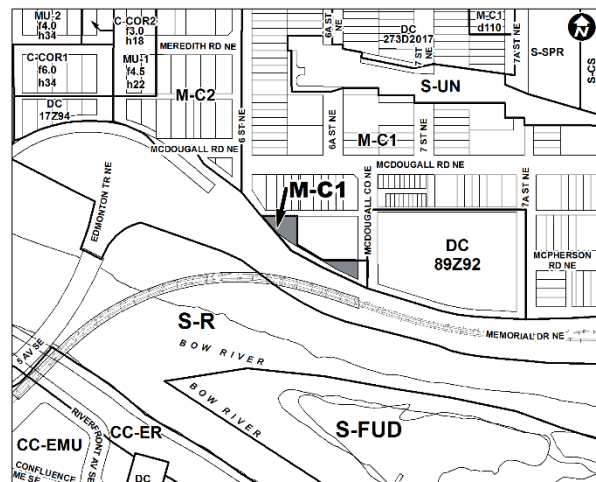
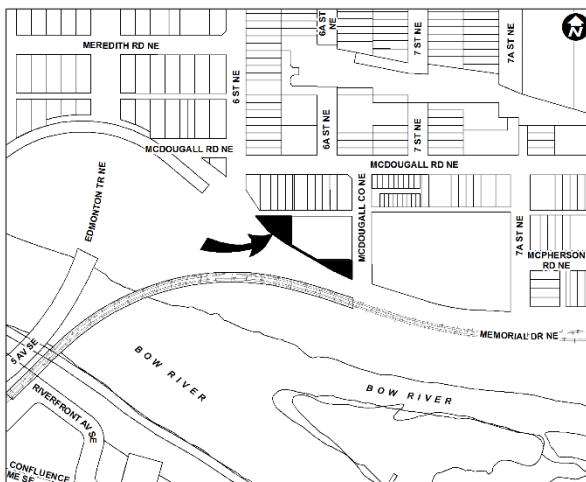
Road Closure and Land Use Amendment in Bridgeland/Riverside (Ward 9)
adjacent to 23 McDougall Court NE, LOC2018-0252

Location Maps



Road Closure Map

Proposed Land Use Map



Road Closure and Land Use Amendment in Bridgeland/Riverside (Ward 9) adjacent to 23 McDougall Court NE, LOC2018-0252

Site Context

The five portions of undesignated road right-of way are located in the northeast community of Bridgeland/Riverside, remnant from Memorial Drive and adjacent to the 23 McDougall Court NE, which is owned by the Calgary Catholic Immigrant's Society, operating as the Margaret Chisholm Resettlement Centre. The centre provides temporary accommodation and assistance to newcomers as they begin their resettlement process in Canada. The three-storey building was constructed in 1993, and can accommodate up to 80 people.

The Resettlement Centre is bound by M-C1 designated land to the north, and the Women In Need Society of Alberta to the east, under DC Direct Control District 89Z92. The subject undesignated road right-of-way is bounded to the south and southwest by Memorial Drive SE.

As seen in the table below, the community of Bridgeland/Riverside's peak population was in 2018, with a population of 6,529 people.

Bridgeland/Riverside	
Peak Population Year	2018
Peak Population	6,529
2018 Current Population	6,529
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: *The City of Calgary 2018 Civic Census*

Additional demographic and social-economic information may be obtained online through the [Bridgeland/Riverside](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Planning Consideration

As part of the review of this application, several key factors were considered by Administration including the proposal's alignment with relevant policies, and the appropriateness of the land use district. The following sections highlight the scope of technical planning analysis conducted by Administration.

Road Closure

The application proposes to close the undeveloped road rights-of-way surrounding the existing Margaret Chisholm Resettlement Centre, consolidating them with the adjacent parcel and redesignating them to the M-C1 District. The attached Conditions of Approval (Attachment 3) provide the guiding conditions of the road closure.

Road Closure and Land Use Amendment in Bridgeland/Riverside (Ward 9) adjacent to 23 McDougall Court NE, LOC2018-0252

Land Use

This land use amendment application seeks to redesignate five areas of undeveloped and undesignated road right-of-way to Multi-Residential – Contextual Low Profile (M-C1) District. The proposed M-C1 District is a designation that provides for multi-residential development of low height and medium density and is intended to be in close proximity or adjacent to low density residential development. The proposed M-C1 District allows for a range of multi-residential housing forms such as townhouses, apartment buildings and development to accommodate assisted living.

Development and Site Design

The proposed redesignation is intended to accommodate expansion of the existing Assisted Living use into a second building. Future redevelopment of the site will be guided by the rules of the proposed M-C1 District. At the development permit stage, Administration will collaborate with the applicant to address the following considerations:

- parking;
- building placement;
- contextual sensitivity; and
- transition and connection to the existing building.

Environmental

An Environmental Site Assessment was not required as part of this application.

Transportation

Site accesses are available at McDougall Court NE or the existing rear lane. Off-site sidewalk and lane improvements will be required at the development permit stage.

Existing transit services are available approximately 500 metres north along 1 Avenue NE. The Bridgeland Memorial LRT Station is located approximately 650 metres east along Memorial Drive SE.

Utilities and Servicing

Once consolidation of the parcels occurs, water, sanitary, and storm sewer mains will be available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

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ISC: UNRESTRICTED
CPC2019-0377

Road Closure and Land Use Amendment in Bridgeland/Riverside (Ward 9) adjacent to 23 McDougall Court NE, LOC2018-0252

Administration received one letter in opposition to the application citing parking as their concern. Parking will be an essential consideration at the development permit stage, although the size of the parcel under request offers many different options to accommodate required parking on the site.

The Bridgeland/Riverside Community Association indicated support for the proposal. No public meetings were held for this application.

Following Calgary Planning Commission, notification for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory - 2009)

The subject parcel is located within the Residential – Developed Established area of the *Municipal Development Plan* (MDP). The applicable MDP policies encourage moderate intensification in a form and nature that respects the scale and character of the neighbourhood.

Bridgeland-Riverside Area Redevelopment Plan (Statutory - 1980)

The *Bridgeland-Riverside Area Redevelopment Plan* (ARP) identifies the site as Non-Family Oriented Development, which should be residential in nature, and kept below RM-5 (Bylaw 2P80), which is the equivalent of Multi-Residential – Contextual Medium Profile (M-C2) District of Land Use Bylaw 1P2007. The M-C2 District allows greater height and density than the proposed M-C1 District. As such, the proposed road closure and land use redesignation complies with the policy.

There is presently work underway to update the ARP. At this time, the proposal complies with the draft policy.

**Road Closure and Land Use Amendment in Bridgeland/Riverside (Ward 9)
adjacent to 23 McDougall Court NE, LOC2018-0252**

Social, Environmental, Economic (External)

The proposal has the potential to allow for intensification of the existing Assisted Living facility that provides refuge for newcomers to Canada, at a location that is served by transit and existing local infrastructure. The proposed M-C1 District facilitates the potential for a more efficient use of land within the city boundary, thereby keeping with the City's goal of accommodating 33 percent growth within our established area by the year 2039.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASONS FOR RECOMMENDATIONS:

The proposal is in keeping with applicable policies of the *Municipal Development Plan* and the *Bridgeland-Riverside ARP*. The proposed M-C1 District is intended for parcels in proximity to low density residential development and is suitable for the context of the area. The proposed undesignated road right-of-way closure proposes a better use for lands that have not been developed or utilized as road, allowing for expansion of the Margaret Chisholm Centre, which provides essential services that assist in resettling and providing care for newcomers to Canada.

ATTACHMENTS

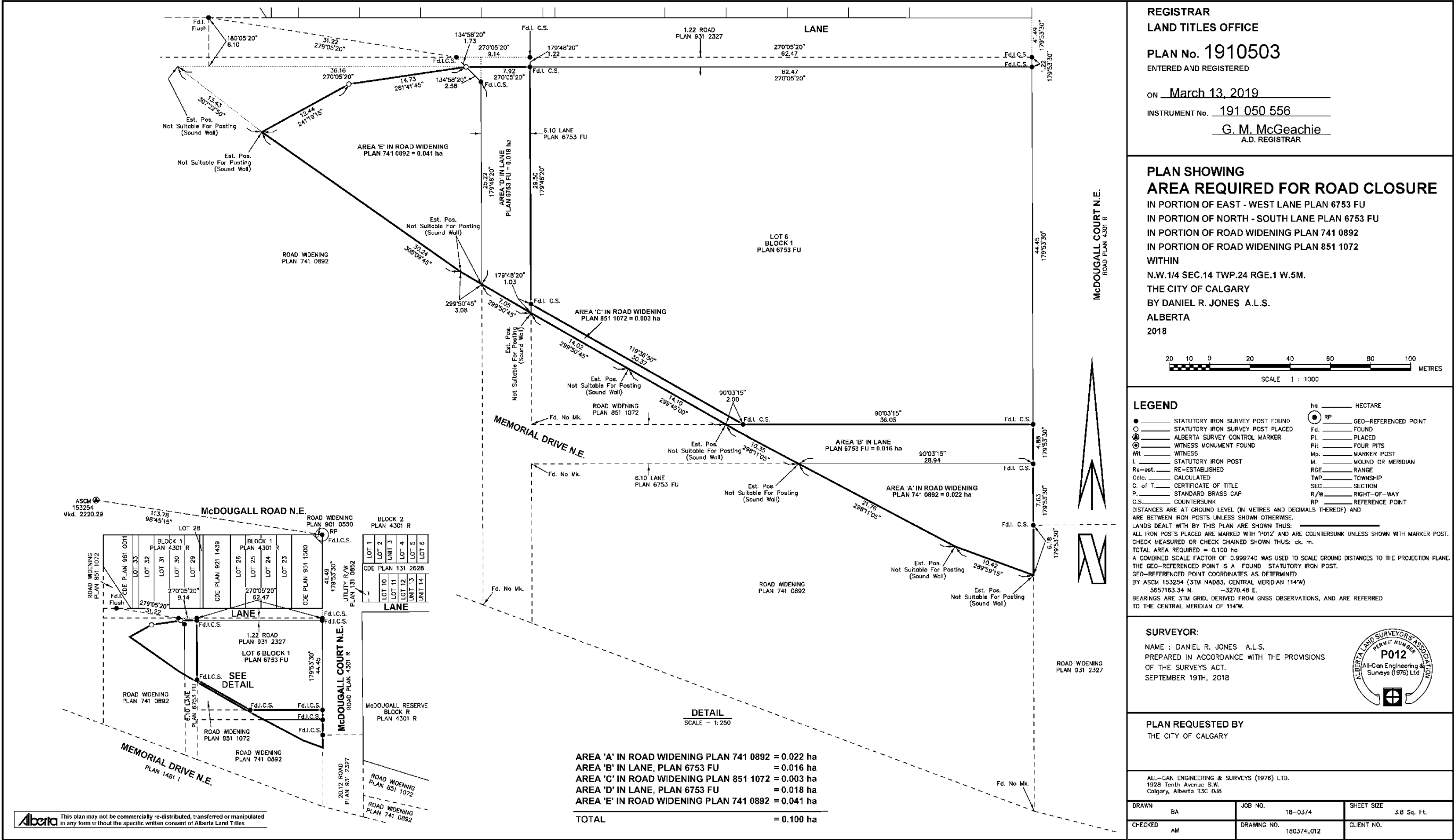
1. Applicant's Submission
2. Registered Road Closure Plan
3. Proposed Road Closure Conditions

Applicant's Submission

The subject lands which are part of public land owned by The City of Calgary are being offered to purchase by Calgary Catholic Immigration Society (CCIS) and being zoned same as the existing zoning of the adjacent parcel by CCIS (ie. MC1).

The subject parcels then will be consolidated with adjacent MC1 land owned by CCIS to create one parcel with the same zoning.

Registered Road Closure Plan



Proposed Road Closure Conditions

1. All existing utilities within the road closure area shall be protected by easement or relocated at the developer's expense.
2. The applicant is responsible for all costs associated with the closure including all necessary physical construction, removal, rehabilitation, utility relocation, etc.
3. The closed road right-of-way is to be consolidated with the adjacent lands.

Planning & Development Report to
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2019 April 18

ISC: UNRESTRICTED
CPC2019-0474

Community Name in Residual Sub-Area 02K (Ward 2), SN2019-0004

EXECUTIVE SUMMARY

This community naming application was submitted on 2019 March 11 by IBI Group Ltd on behalf of the landowners Symons Valley Ranch GP Inc, in conjunction with outline plan, LOC2017-0232. This application proposes a community name for the future subdivision and development of 12.44 hectares \pm (30.74 acres \pm) to create a new Community Activity Centre in the heart of the *Glacier Ridge Area Structure Plan*.

The landowners seek Calgary Planning Commission and City Council support in approving Symons Valley Ranch as a community name for the area identified in the above-mentioned outline plan. The proposed community name will capture the sites historical legacy, acknowledge the sites agricultural and ranching roots and further affirm the Farmers market sense of community and hospitality.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council:

1. **ADOPT**, by Resolution, of the proposed community name of Symons Valley Ranch.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

IBI Group Ltd, on behalf of Symons Valley Ranch GP Inc, submitted the subject application on 2019 March 11 and have provided a summary of their proposal in the Applicant's Submission (Attachment 1). Currently the area is identified as Residual Ward 2 – Sub Area 02K. The associated outline plan LOC2017-0232 was heard by Calgary Planning Commission on 2019 March 21 and was recommended to Council for approval.

The subject site was part of land annexed to the City of Calgary from Rocky View County in 2007. At the time, various uses were being operated on site including a rodeo ground, RV parking and storage, and a restaurant and bar. In 2015, this parcel was identified as a Community Activity Centre within the *Glacier Ridge Area Structure Plan*. This Community Activity Centre is intended to develop as a mixed-use village that cultivates a farm-to-table hub centred by the rebuilt Symons Valley Farmers' Market.

The Symons Valley Farmers' Market had been operating on the subject site for several years. In 2017, a fire destroyed the building that housed it. The desire to rebuild the Symons Valley Farmers' Market resulted in a land use redesignation and outline plan application (LOC2017-0232). The outline application includes several additional elements and an increased intensity from what previously existed on the site and is well illustrated through a Master Concept Plan. (Attachment 2).

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ISC: UNRESTRICTED
CPC2019-0474

Community Name in Residual Sub-Area 02K (Ward 2), SN2019-0004

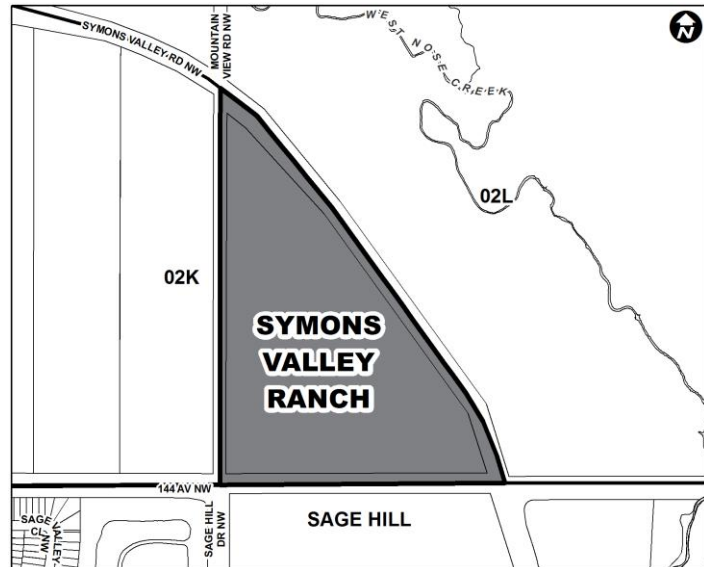
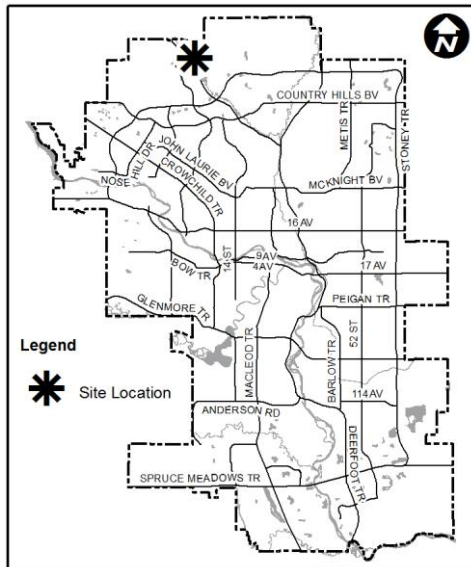
The legacy of the site traces back over 50 years and has since become a unique multi-purpose facility that continues to evolve. The name Symons Valley Ranch is a unique community name but encapsulates the area's agricultural and ranching roots. The proposed community name will pay tribute to the site's historical legacy and affirms the farmers' market sense of community and hospitality within the area.

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Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0474

Community Name in Residual Sub-Area 02K (Ward 2), SN2019-0004

Location Maps



Planning & Development Report to
Calgary Planning Commission
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ISC: UNRESTRICTED
CPC2019-0474

Community Name in Residual Sub-Area 02K (Ward 2), SN2019-0004

Site Context

Situated west of West Nose Creek and north of the community of Sage Hill, the site is a triangular parcel comprising approximately 12.44 hectares (30.74 acres) of land located at the northwest corner of 144 Avenue NW and Symons Valley Road NW. Currently the site contains several buildings including a site office and restaurant. There are portions of the site that are also currently being used for the Symons Valley Farmers' Market and vehicle storage.

Surrounding parcels to the north, west and east of the site remain in an agricultural and/or natural state but are identified for future development. South of 144 Avenue NW is a City of Calgary park with a Park's Depot and a storm pond. Further to the south is the community of Sage Hill.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Stakeholder Engagement, Research and Communication

The subject application was circulated to various City of Calgary business units and other agencies for review. No objections to the proposed community name were received.

Strategic Alignment

The proposal aligns with the *Municipal Naming, Sponsorship and Naming Rights Policy*.

Social, Environmental, Economic (External)

Not applicable.

Financial Capacity

Current and Future Operating Budget

Not applicable.

Current and Future Capital Budget

Not applicable.

Risk Assessment

There are no significant risks with the proposed name.

REASON(S) FOR RECOMMENDATION(S):

Administration recommends approval as the proposed community name complies with the *Municipal Naming, Sponsorship and Naming Rights Policy* and there were no objections received during circulation of the proposed community name of Symons Valley Ranch.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0474

Community Name in Residual Sub-Area 02K (Ward 2), SN2019-0004

ATTACHMENT(S)

1. Applicant's Submission
2. Master Concept Plan

Applicant's Submission



IBI GROUP
500 – Meredith Block, 611 Meredith Road NE
Calgary AB T2E 2W5 Canada
tel 403 270 5600 fax 403 270 5610
ibigroup.com

March 11, 2019

Ms. Vivian Barr
Senior Planning Technician, Community Planning
City of Calgary
5th Floor, Municipal Bldg
800 Macleod Trail SE
PO Box 2100, Station M
Calgary, AB T2P 2M5

Dear Ms. Barr:

COMMUNITY NAMING REQUEST IN THE GLACIER RIDGE ASP AREA - RELATED TO LOC2017-0232

As per our discussion related to the LU/OP for LOC 2017-0232, we are requesting the approval of the following community name for the area identified in the attached exhibit:

- ***Symons Valley Ranch***

As you may be aware, the subject site, which is approximately 12.4 hectares/30.7 acres in area is home to the historic Symons Valley Ranch Farmer's Market, which was unfortunately lost to a fire in early 2017. Through LOC 2017-0232, this entire plan area is being master planned to become a new Community Activity Centre in the heart of the Glacier Ridge Area Structure Plan.



The land use composition and urban framework for this new CAC is not only unique and distinct from the immediate surrounding areas, but will be unique to the City of Calgary. Altogether, this clearly supports a differentiated community name which is well anchored to the site's agricultural and ranching roots.



The site's legacy traces back over 50 years when the Jones family purchased the 30.7 acre lot in what was then the outskirts of Calgary. It has since become known as a unique multipurpose facility that has continued to evolve and grow along with the communities surrounding it. The previous facility had housed multiple banquet halls, a family restaurant and lounge and a thriving, sought after Farmer's Market. All this will be revived as the heart and soul of this new Community Activity Centre.



Perched on the west side of Nose Creek and the Symons Valley itself with the welcoming atmosphere

IBI Group Professional Services (Canada) Inc. is a member of the IBI Group of companies

Applicant's Submission

IBI GROUP

2

Ms. Vivian Barr – March 11, 2019

of the "Ranch" functioning as an epicentre of social and commercial activity in this part of the region for over 50 years, we believe this land is well befitting the community moniker of ***Symons Valley Ranch***.

The proposed community name will capture the site's historical legacy and further affirm the Farmer's Market sense of inclusiveness, community and hospitality.

On behalf of our team, we respectfully request the approval of the above noted community name for this new LU/OP.



Sincerely,
IBI Group

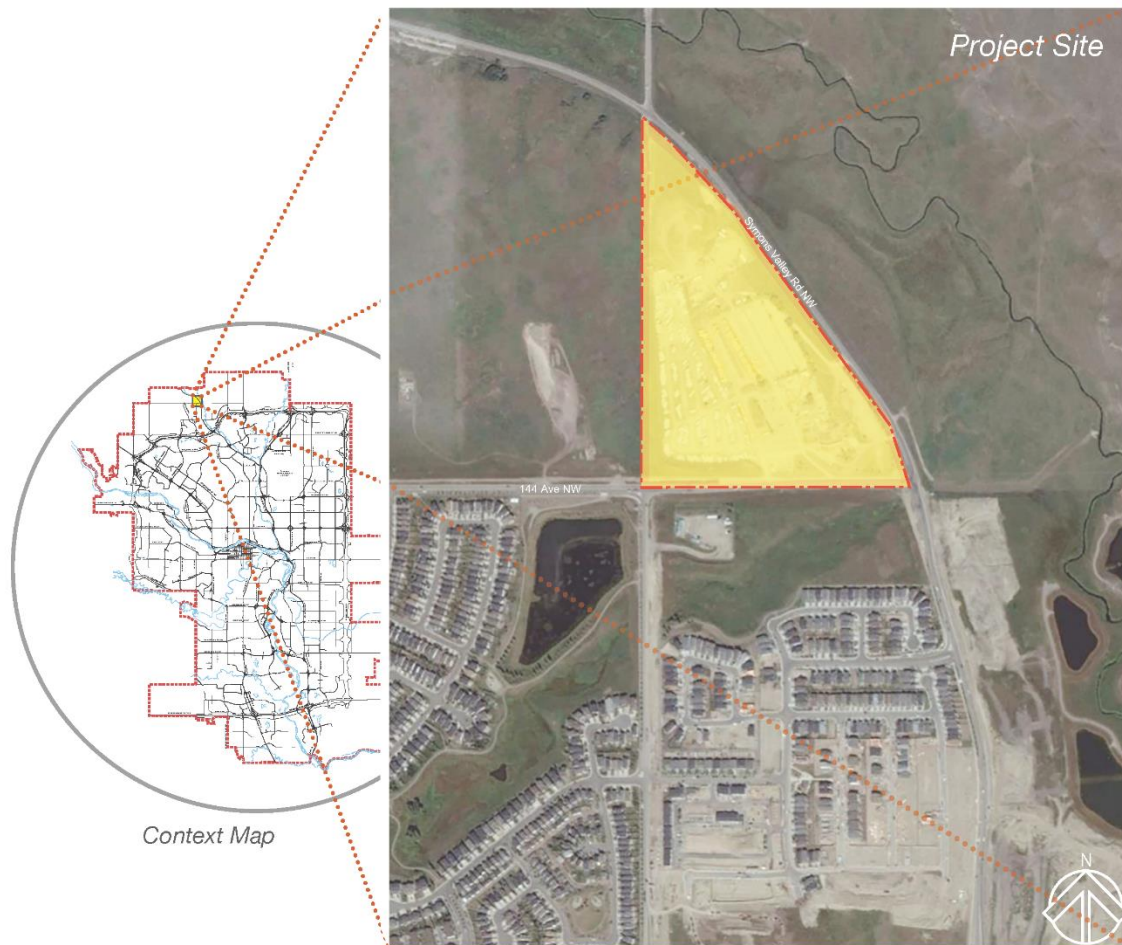

Benjamin Lee,
Associate Director


Amanda Polini
Planner

cc: IBI Group
Capexco

/Attchmt

Applicant's Submission



capexco

FRANK

Boundary Area of Proposed Community
Name: Symons Valley Ranch



Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0494

Development Permit in Hillhurst (Ward 7) at 417 – 10 Street NW, DP2018-4579

EXECUTIVE SUMMARY

This development permit application was submitted by IBI Group on 2018 September 25 on behalf of Greywood 10G GP Inc. This application proposes a new mixed-used development including:

- at-grade commercial uses along 10 Street NW;
- residential units along the lane;
- nine storeys of residential above the ground floor;
- 115 residential units in total;
- Class B (fascia) and Class D (canopy) signs; and
- a development consistent with the intent of the DC Direct Control District (Bylaw 152D2018).

The proposed development aligns with policies in the *Municipal Development Plan* and *Hillhurst/Sunnyside Area Redevelopment Plan* and will provide a mixed-use development with local retail, employment and housing opportunities in close proximity to and the Sunnyside LRT Station and the downtown.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission **APPROVE** the proposed development permit application DP2018-4579 for a New: Dwelling Units, Financial Institution, Outdoor Café, Restaurant: Licensed – Small, Retail and Consumer Service, Sign – Class B, Sign – Class D at 417 – 10 Street NW (Plan 1811460, Block 1, Lot 10), with conditions (Attachment 1).

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

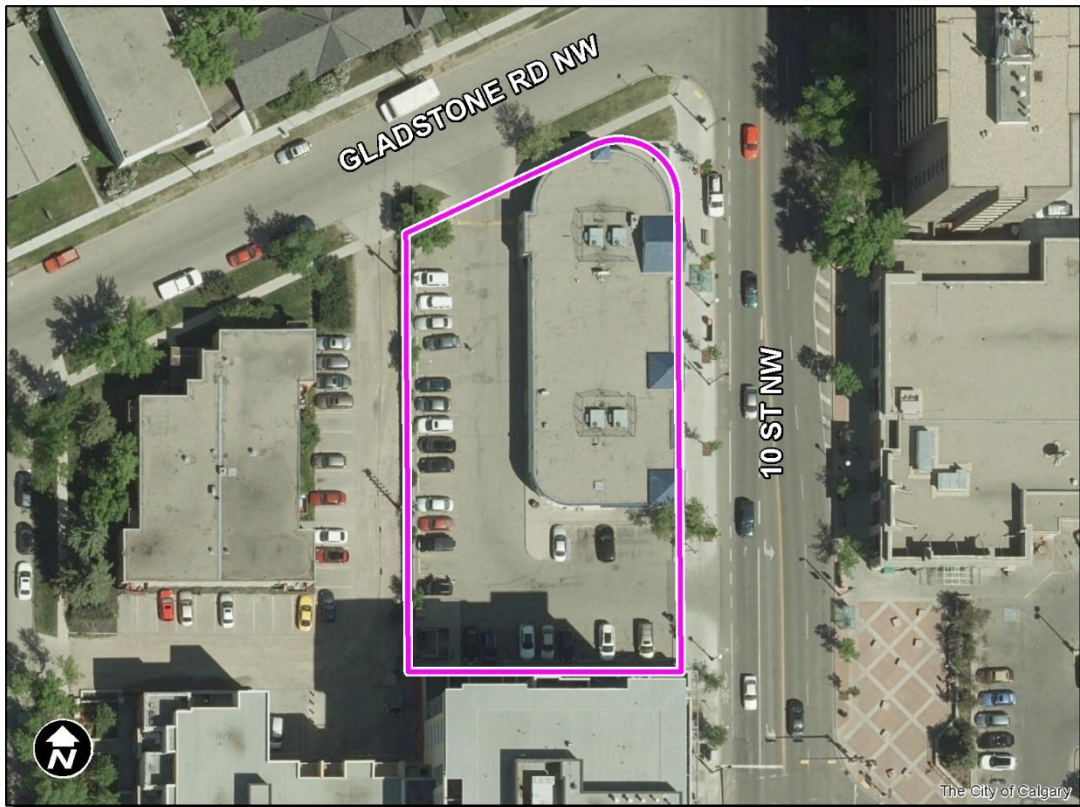
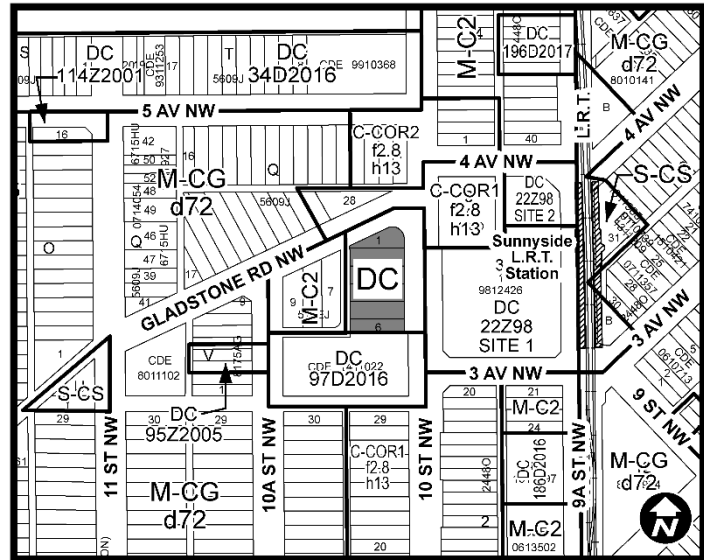
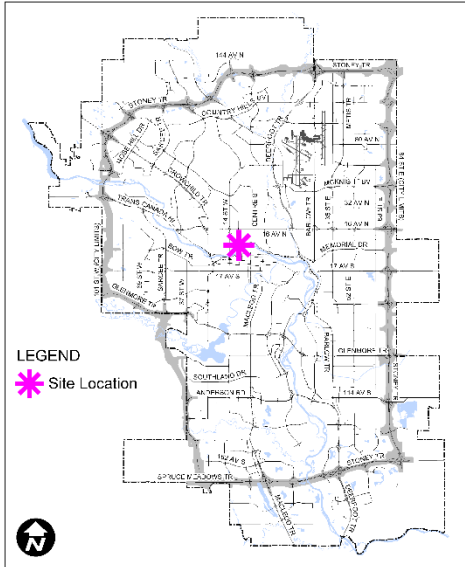
The development permit application (Attachment 2) was submitted by IBI Group on 2018 September 25 on behalf of Greywood 10G GP Inc. to develop the site with a mix of multi-residential and at-grade commercial uses.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0494

Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

Location Maps



Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

Site Context

The subject site is located along 10 Street NW, at the north end of the commercial area of Kensington in the community of Hillhurst. The primary frontage for the site is along 10 Street NW, while it also features frontage along Gladstone Road NW. The site is approximately 240 metres from the Sunnyside LRT platform and within walking distance of downtown. Currently, the subject site is occupied by an older single storey commercial building.

Surrounding the subject site are mixed-use commercial/residential buildings to the south and east; a multi-residential building to the west; and smaller commercial developments to the north across Gladstone Road NW. A land use amendment was recently approved for the parcels immediately to the north, adjacent to the historic church, for development of a scale similar to that allowed on this site.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Land Use

On 2018 May 07 Council approved the redesignation of this site to a DC Direct Control District based on the Mixed Use - Active Frontage (MU-2) District to allow for a range of multi-residential, commercial and office uses.

The DC Direct Control District (Bylaw 152D2018) allows for a maximum height of 32.0 metres and maximum density of 5.0 FAR. The DC District also includes a rule for the implementation of the density bonus provisions in the *Hillhurst/Sunnyside Area Redevelopment Plan*.

Application Review

The initial development concept proposed by the applicant was taken to the Urban Design Review Panel (UDRP) meeting on 2018 June 27. Following submission of the Development Permit on 2018 September 25, a Detailed Team Review (DTR) was provided to IBI Group on 2019 January 02 and required a response to address the following:

- City Wide Urban Design and UDRP comments;
- provision of additional architectural;
- detail, particularly at the northeast corner of the building;
- removal of a masonry wall along the south edge of the site adjacent to the existing development;
- submission of a soil and groundwater management plan and sanitary servicing study;
- additional commercial parking and bicycle parking; and
- Land Use Bylaw discrepancies.

Amended plans were submitted on 2019 February 25. The application addressed Administration's concerns through the amended plan submissions. Outstanding issues, that are minor in nature, are to be addressed through the prior to release conditions of the Development Permit Conditions of Approval (Attachment 1).

Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

City Wide Urban Design

The following comments were raised by City Wide Urban Design during the review period:

- Given the landmark/gateway location of the site, ensure that the massing and overall design celebrates the community entry at 10 Street NW and Gladstone Road NW through distinctive architecture, use of materials, well detailed public spaces, and pedestrian scaled signage and lighting which animates the public realm.
- Consider landscape, furniture, lighting and public art at the corner of 10 Street NW and Gladstone Road NW to emphasise the gateway character and animate the public realm.
- Provide a vertical street/sidewalk interface along 10 Street NW which animates the public realm for example with floor-to-ceiling transparent glazing, pedestrian scaled canopies which provide weather protection, recessed entries or set-back areas to allow for outdoor café space or retail spill-out.

Through the amended plans submitted on 2018 September 25 and 2019 February 25, the applicant was able to address the comments raised by City Wide Urban Design through updates of materials and the provision of illustrative materials and material boards. The design of the public realm and the corner elements have been enhanced through creation of a plaza space and outdoor café at the corner, creation of a clear glass feature extending up the building as a corner element with the addition of unique coloured glass panels, and provision of second floor balconies which allow for canopies for weather protection, break up the massing along the ground floor and bring additional activity to the street.

Urban Design Review Panel

The application was brought before the Urban Design Review Panel (UDRP) on 2018 June 27. UDRP comments, together with the applicant's response, are provided in Attachment 4. In summary, UDRP's comments focused on the significance of the location and the creation of a signature building that invites people to the 10 Street NW corridor.

Key comments from the UDRP include:

- Add seating and other complementary furnishings that focus on the mixed-use retail frontage on 10 Street NW at street level and consider increased articulation/animation of the retail frontage to provide more textural depth to the building interface.
- More definition at street level, including various entry conditions, would assist in breaking up the long volume and adding interest and rhythm.
- Gateway art and a more obvious and celebrated entrance to the residential component of the site would be beneficial and would add to the stature of the project in its gateway role.
- Enhance the interface along Gladstone Road NW adjacent to the sidewalk, perhaps reviewing opportunities to narrow the carriageway of Gladstone Road if possible.

Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

Changes were incorporated into the initial plans submitted on 2018 September 25 as well as the amended plans submitted 2019 February 25. These include providing two-sided public seating, enhancing the retail frontage through increased windows, masonry entry accents, and different signage opportunities, development of a distinct realm for the residential entry and development of the plaza area at the north east corner of the site. The amended plans satisfied the concerns raised by the UDRP and City Wide Urban Design and it was determined that a second UDRP review was not required.

Site Building and Design

The application proposes a ten-storey mixed-use development with a total of 115 units. This includes 53 one-bedroom units, 50 two-bedroom units, and 12 three-bedroom units. In addition to the common entrance lobby fronting Gladstone Road NW there are two individual unit entries proposed at-grade along the lane. Commercial/retail units face 10 Street NW with a proposed restaurant and outdoor café wrapping the corner at the 10 Street NW and Gladstone Road NW.

Although the proposed building is ten storeys, the applicant has attempted to be respectful of adjacent building to the south by providing a setback beginning at the third floor and continuing to the top of the building. A stepback has also been provided after the second floor along the 10 Street façade to assist in reducing the perceived mass and height of the building from the pedestrian scale. The building has been also provided a generous setback along 10 Street NW in order to allow for a wider pedestrian path and offer areas for retailers to display goods or create small seating areas.

The building utilizes a combination of materials. The proposed two-storey base of the building is primarily made of a light masonry material with storefront glazing. The upper levels are designed with metal panels as the primary material along with masonry elements providing a major vertical element on the north elevation. The applicant has also added accent spandrel panels in the corner units using dichroic glass. This material has properties which change colour when viewed from different locations.

Landscaping and Public Realm

The public realm for the development includes a public plaza provided at the north east corner of development. This space is active with curving rows of concrete and wood seating benches. Landscaping includes ash and linden trees with permeable foliage that will provide shade over the seating as well as separation from the outdoor café.

Along 10 Street NW the development has incorporated a similar approach to what has been previously used along the 13 Avenue Greenway. Planting has been provided along the public realm adjacent to the street. This includes the elm street trees as well as a strip of Blue Lyme Grass that will allow for a continuous soil trench and better tree establishment as well as protection for pedestrians from the street. Two-sided benches have been provided within this area to allow users experience both the retail and the street.

Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

A roof top patio has also been proposed as part of the development. This provides for amenity space for the residents and will have the ability to host larger groups or building functions. This amenity space will include several planters as well as a number of seating options and outdoor fireplace table.

Site Access and Parking

Vehicular access is provided from the lane to underground parking. Residential parking stalls have been provided in excess of Land Use Bylaw requirements and includes the required number of visitor parking stalls. The applicant is asking for a relaxation for the commercial parking requirements supported by a parking study. Administration is supportive of the relaxation. The applicant is also requesting a relaxation for the bicycle parking stall as a number of bicycle stalls have been provided along 10 Street NW, within the public realm, but outside of the subject property.

Environmental

A Phase II Environmental Site Assessment was submitted with this application as well as a soil and groundwater management plan, which were accepted by Administration.

Transportation

Vehicular access to the development site is available through the existing rear lane. The site is located adjacent to transit stops for several bus routes on 10 Street NW as well as 240 metres from the Sunnyside LRT Station. A Transportation Impact Assessment was not required for this application.

Utilities and Servicing

Water, sanitary and storm water mains are available and can accommodate the proposed development of the application. A development site servicing plan will be required prior to release of this development permit.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

In 2018 February, when the land use amendment review was underway, the applicant held an open house. Feedback was generally positive from the community. The applicant also met with the Hillhurst/Sunnyside Community Association (HSCA) in 2018 September and 2019 February to discuss the proposal. The HSCA was circulated and provided a letter with comments on the application. That applicant addressed many of these comments through the amended plans and the HSCA have indicated they are mostly in support of the proposed development.

Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

No letters of support or objection were received from the public in regard to the development permit application.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)* which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed development builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory - 2009)

The subject site is identified on the Urban Structure Map of the Municipal Development Plan (Map 1) as being within the Neighbourhood Main Street Area. Neighbourhood Main Streets typically are located along Primary Transit Network within the Inner City and have a strong historical connection to the communities they abut. They are the “main streets” for one or more communities, providing a strong social function and typically support a mix of uses within a pedestrian-friendly environment.

This proposal is in keeping with relevant MDP policies as it provides opportunities for mixed-use development including contributing to a range of housing options and the provision of jobs in an area well-served by transit.

Hillhurst/Sunnyside Area Redevelopment Plan (Statutory - 1988)

The subject site falls within the Transit Oriented Development Area of the *Hillhurst/Sunnyside Area Redevelopment Plan (ARP)*, approved by Council in 2009 February. The subject site is situated in the area identified as Urban Mixed-Use on the Land Use Policy Area Map of the ARP. This area has the potential to accommodate the highest densities and building heights in the area with minimal impact on the character or quality of the nearby residential districts. The proposed development meets the intent and policies of the Area Redevelopment Plan.

In 2012 November, Council approved an amendment to the *Hillhurst/Sunnyside Area Redevelopment Plan* to include density bonus provisions, which allow for a density increase up to the maximum floor area ratio specified in the Area Redevelopment Plan. The density increase is subject to a contribution to the community amenity fund or provision of an urban design improvement. At this time the applicant intends to work with Administration to design and potentially construct a realignment and curb extension at Gladstone Road NW and 10 Street NW in order to improve the pedestrian cross walk in this area.

Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

Land Use Bylaw and DC Direct Control District

The proposed development generally complies with the Land Use Bylaw with the exception of the below discrepancies. These Bylaw discrepancies, identified in the table below, are supported by Administration as the development still meets the intent of the rules, and relaxations of the rules has minimal adverse impact on adjacent developments.

The subject parcel is governed by DC Direct Control District (Bylaw 152D2019) (Attachment 5). The purpose of the DC Bylaw is intended to provide for medium density mid-rise mixed-use development as well as to implement the density bonus provisions of the Area Redevelopment Plan. It should be noted that the DC Bylaw provides the Development Authority permission to relax the building height rule contained within this DC Direct Control District if it can be demonstrated that the relaxation does not unduly interfere with the use, value and enjoyment of the adjacent properties.

Bylaw Relaxations		
Regulation	Standard	Provided
247 Outdoor Café	(f) has a maximum area of 25.0 square metres in the C-N1, C-N2, C-C1, C-C2, C-COR1, C-COR2, C-O, C-R1, I-B, CC-MHX, CC-X, CC-COR, CC-ER, CC-ERR, CC-EMU, CC-EIR, CC-EPR, CC-ET, MU-1, MU-2, M-H1, M-H2, M-H3, M-X1 and M-X2 districts.	Plans indicate the area of the outdoor café is 55.02m ² (+30.02m ²). <i>Administration supportive of relaxation.</i>
282 Restaurant: Licensed - Medium	(d.1) must not be within 45.0m of a residential district when the use is located within the C-C1, C-C2, C-COR1, C-COR2, CC-COR, CC-X, MU-1, MU-2 and S-R Districts, which must be measure from the building containing the use to the nearest property line of a parcel designated as a residential district.	Plans indicate the Restaurant: Licensed – Medium located within CRU 6 is 24.77m (-20.23m) from the residential district to the West of the development site. <i>Administration supportive of relaxation</i>
1385 Setback Areas (min.)	(3) For the storey closest to grade, the maximum building setback from a property line shared with a street is 4.5m for 60% of the length building façade that faces the street.	Plans indicate 11.20% (-48.80%) of the North façade of the building is located within 4.5m of the property line shared with the street. <i>Administration supportive of relaxation</i>

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Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

1339 Building Orientation	Units and individual uses located at grade with an exterior wall facing a street must provide: (b) an entrance that is visible from the street; and	Plans indicate the entrance to unit A1 is not visible from Gladstone Ave NW. <i>Administration supportive of relaxation</i>
1337 Ground Floor Height	(1) Unless otherwise referenced in subsection (2), the minimum height of the ground floor of a building is 4.0m as measured vertically from the floor to the ceiling.	Plans indicate the ground floor height for commercial units is 3.66m (-0.34m). <i>Administration supportive of relaxation</i>
DC 152D2018 9 Building Height	The maximum building height is 32.0 metres.	Plans indicate the building height from grade on the North elevation is 32.33m (+0.33m). Plans indicate the building height from grade on the West elevations is 32.75m (+0.75m). <i>Administration supportive of relaxation</i>
1342 Rules for Commercial Uses Facing a Street	(1) Unless otherwise referenced in subsection (2), the façade of a building located on the floor closest to grade and facing a street must provide windows with unobscured glass that: (a) occupy a minimum of 65.0% of the façade between a height of 0.6m and 2.4m; and	Plans indicate 41.71% (-23.29%) of the North façade is finished with unobscured glass between 0.6m and 2.4m. <i>Administration supportive of relaxation</i>
1346 Planting Requirements	(4) Shrubs must be min height or spread of 0.60m.	Plans indicate the proposed Blue Danube Juniper has a height of 0.40m. <i>Administration supportive of relaxation</i>
Parking Stalls (min.)	Requires 38 motor vehicle parking stalls for commercial uses.	Plans indicate 5 (-33) commercial parking stalls provided. <i>Administration supportive of relaxation</i>
Loading Stalls (min.)	Requires 2 loading stalls.	Plans indicate 1 (-1) loading stalls provided. <i>Administration supportive of relaxation</i>

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CPC2019-0494

Development Permit in Hillhurst (Ward 7) at 417 - 10 Street NW, DP2018-4579

Bicycle Parking Stalls	Requires 14 bicycle stalls class 2.	Plans indicate 0 (-14) bicycle stalls class 2 provided. <i>Administration supportive of relaxation (28 stalls have been provided off-site adjacent to 10 Street NW)</i>
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Social, Environmental, Economic (External)

The proposed development contributes to housing diversity, encourages social interaction through an enhanced public realm, and will allow for more efficient use of existing infrastructure.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed development does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this application.

REASON(S) FOR RECOMMENDATION(S):

Administration is supportive of the proposed mixed-use development as the application meets the over-arching objectives set forth in the *Municipal Development Plan* and *Calgary Transportation Plan*. The proposal complies with the *Hillhurst/Sunnyside Area Redevelopment Plan* and meets the intent of the DC Direct Control District with minor bylaw relaxations.

ATTACHMENT(S)

1. Conditions of Approval
2. Development Permit Drawings
3. Applicants Submission
4. Applicant Response to UDRP
5. Direct Control Bylaw 152D2018

Conditions of Approval

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a total of seven (7) complete sets of Amended Plans (file folded and collated) to the Planning Generalist that comprehensively address the Prior To Release conditions of all Departments as specified below.

In order to expedite the review of the Amended Plans, please include the following in your submission:

- a. Four (4) of the plan set(s) shall highlight all of the amendments.
- b. Four (4) detailed written response(s) to the Conditions of Approval document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.

Please ensure that all plans affected by the revisions are amended accordingly.

2. Amend A-001 to show the correct number of residential units (115) and revise the parking counts accordingly.
3. Amend A-100 and A-201 to show the exact bench locations and design (as shown on L1.0) as well as the design of the corner of 10 Street and Gladstone Road NW.
4. Amend A-401 and A-404 to show the exact bench locations and design (as shown on L1.0).
5. Amend L1.0 by labelling and including in the Legend the area to be sod along Gladstone Road NW. Add note to Legend to confirm "Plain Concrete with 1.0 X 1.0m Sawcut Joints" will match existing sidewalk treatment along 10 Street NW.
6. Amend L1.1 to indicate the caliper size of the proposed Clump Paper Birch.
7. Amend the plans to indicate the soil depth for trees and shrubs planted over the parkade to comply with the Land Use Bylaw requirements.
8. Amend the plans to reflect the changes to floors 2 to 10 required to meet the maximum 5.0 FAR.
9. Submit a letter from Enmax Corporation and indicate on the amended plans that the following issues have been addressed.
 - a. Existing transformer (21C-375) conflicts with the proposed building.
 - b. Space for new pad-mounted transformer may not be sufficient - must be approved by EPC
10. As per Part II, section 3.1.5 of the Hillhurst/Sunnyside Area Redevelopment Plan, provide a letter, to the satisfaction of the Development Authority, detailing how

Conditions of Approval

improvements to the urban realm at Gladstone Road NW and 10 Street NW will be undertaken. The letter must address: what urban realm improvements will be provided, coordination of the work and how any cost overages will be addressed based on discussions with City Staff. The contribution is based upon the contribution formula found in the Direct Control Bylaw for the subject site:

$\$18.14 \times 4934.95\text{m}^2$ (total floor area in m^2 above base floor area ratio of 2.8) =
\$89,520.00

or

Remit payment (certified cheque, bank draft) for contribution to the Hillhurst/Sunnyside Community Amenity Fund as per DC Direct Control District (152D2018).

a) Payment is based on the following:

$\$18.14$ per square metre \times 4934.95 square metres = **\$89,520.00**

Development Engineering:

11. Waste & Recycling Services - General

- a. Indicate a location for the grease waste storage area with dimensions (1.5m x 2.0m minimum).
- b. Relocate the protection bollards placed 2.2 m in front of the two waste collection doors. - NEW

Waste & Recycling Services – Collection Vehicle Access

- a. Realign the vehicle sweep path as the proposed collection vehicle sweep path comes in contact with the building.
- b. Provide (include relevant grades) a level transition between the staging area and the adjacent alley.

Waste & Recycling Services – Industrial, Commercial and Institutional

- a. Ensure each waste stream can be maneuvered independently of the other two.

Waste & Recycling Services – Multi-Family

- a. Ensure each waste stream can be maneuvered independently of the other two.

12. Submit three (3) sets of the Development Site Servicing Plan details to Development Servicing, Inspections and Permits, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact developmentservicing2@calgary.ca for additional details.

For further information, refer to the following:

Design Guidelines for Development Site Servicing Plans

http://www.calgary.ca/PDA/pd/Documents/urban_development/publications/DSSP2015.pdf

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/pd/Documents/development/development-site-servicing-plan.pdf>

Conditions of Approval

13. After the Development Permit is approved but prior to its release, the landowner shall execute an Off-Site Levy Agreement for the payment of off-site levies pursuant to Bylaw 2M2016.

As per the current application (67 multi-residential (1 bedroom), 70 multi-residential (2 bedroom) and 8 commercial units) and based on 2018 rates, the preliminary estimate for this application is \$137,958.24.

Should payment be made prior to release of the development permit, an Off-Site Levy Agreement will not be required.

To obtain the off-site levy agreement, contact David Boettcher, Infrastructure Strategist, City of Calgary Approvals Coordination at David.Boettcher@Calgary.ca or (403) 268-2720.

Transportation:

14. Amend the parkade plans and sheet A-401 by providing a detailed cross-section of the proposed parkade ramp between levels P1 and P2 that indicates all grades, transitions, and elevations in accordance with current City of Calgary standards. For details, please refer to [Roads Construction 2015 Standard Specification](#) sheet 116. Note: ensure trench drain detail is included to City standards.
15. To the satisfaction of Transportation Planning, provide a Perpetual Maintenance Agreement to be registered on the development site's land title(s) for the applicant requested non-standard surface element(s) located in the road right-of-way concurrently with the execution of the Indemnification Agreement. Contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.
16. Submit Construction Drawings for review and approval of:
- The realignment of Gladstone Road at 10 Street NW.
 - The curb extension for the pedestrian crossing of Gladstone Road NW along 10 Street NW.
 - Associated utilities and subsurface drainage changes.
17. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Closure of exiting private driveway on 10 Street NW;
- b. Closure of exiting private driveway on Gladstone Road NW;
- c. Construction of new concrete/asphalt lane paving and
- d. Improvements to Gladstone Road and 10 Street NW intersection should it be approved as the Developer's urban realm improvement per Condition (10).

Conditions of Approval

- e. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel.
- 18. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Roads

- a. Street lighting upgrading adjacent to the site.

Parks:

No comments

Permanent Conditions

The following permanent conditions shall apply:

Planning:

- 19. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
- 20. No changes to the approved plans shall take place unless authorized by the Development Authority.
- 21. A Development Completion Permit shall be issued for the development before the use is commenced or the development occupied. A Development Completion Permit is independent from the requirements of Building Permit occupancy. Call Development Inspection Services at 403-268-5311 to request a site inspection for the Development Completion Permit.
- 22. The maximum seating area of the outdoor café shall not exceed the area indicated on the plans.
- 23. Upon completion of the main floor of the building, proof of the geodetic elevation of the constructed main floor must be submitted to and approved by the Development Authority prior to any further construction proceeding. Email confirmation to geodetic.review@calgary.ca.
- 24. All electrical servicing for freestanding light standards shall be provided from underground.
- 25. A lighting system to meet a minimum of 54 LUX with a uniformity ratio of 4:1 on pavement shall be provided.
- 26. The walls, pillars and ceiling of the underground parkade shall be painted white or a comparable light colour.

Conditions of Approval

27. The light fixtures in the parkade shall be positioned over the parking stalls (not the drive aisles).
28. All stairwell doors and elevator access areas shall be installed with a transparent panel for visibility.
29. Fascia signage shall be placed only in the designated sign area as indicated on the approved plans. Any damage to the building face, as a result of the sign installation or removal, shall be repaired to the satisfaction of the Development Authority.

Development Engineering:

30. Prior to issuance of a Development Completion Permit or any occupancy of the building, payment shall be made for off-site levies pursuant to *Bylaw 2M2016*.
31. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
 - a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
32. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control. The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

Conditions of Approval

33. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.
34. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
35. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.

Transportation:

36. The developer is responsible for the cost of public work and any damage during construction in City of Calgary road rights-of-way, as required by the Manager of City of Calgary Transportation Planning. All work performed on public property shall be done in accordance with City of Calgary standards.
37. Indemnification Agreements are required for any work to be undertaken adjacent to or within City of Calgary rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City of Calgary rights-of-way, bylawed setbacks, and corner cut areas must be removed to the satisfaction of the Manager of City of Calgary Transportation Planning, at the developer's expense, upon completion of the foundation. Prior to permission to construct, contact Paul Cardiff, Indemnification Agreement Coordinator, City of Calgary Roads at Paul.Cardiff@Calgary.ca or (403) 268-3505.
38. Vehicular access to the site is only permitted via the adjacent lane.
39. A Perpetual Maintenance Agreement is to be registered on the development site's land title(s) for the applicant requested non-standard surface element(s) located in the road right-of-way concurrently with the execution of the Indemnification Agreement. Contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.

Parks:

40. Any damage to public parks, boulevards or trees resulting from development activity, construction staging or materials storage, or construction access will require restoration at the developer's expense. The disturbed area shall be maintained until planting is established and approved by the Parks Development Inspector. Contact 311 for an inspection.
41. Public trees located on the boulevard adjacent to the development site shall be retained and protected unless otherwise authorized by City of Calgary Urban Forestry. Prior to construction, install a temporary fence around the extent of the branches ("drip line") and ensure no construction materials are stored inside this fence.

Conditions of Approval

42. Any tree planting in the City of Calgary boulevard shall be performed and inspected in accordance with Parks Development Guidelines and Standard Specifications – Landscape Construction (current edition). Applicant is to contact the City of Calgary Parks Development Inspector through 311 to arrange an inspection.
43. Tree protection information given as per the approved development permit does not constitute Tree Protection Plan approval. Tree Protection Plan approval must be obtained separately through Urban Forestry. Visit www.calgary.ca or call 311 for more information.
44. The submitted plans indicate that the removal of existing public trees along property frontage is necessary. As per the City of Calgary Tree Protection By-law, a letter of authorization to remove public trees is required from Parks Urban Forestry. The applicant is to contact Urban Forestry at 311 to make arrangements for the letter and compensation.

Development Permit Drawings



Sheet List - Development Permit	
Sheet Number	Sheet Name
A-000	COVER SHEET
A-001	VICINITY PLAN & PROJECT DATA
A-100	SITE PLAN
A-201	MAIN FLOOR PLAN
A-202	2ND FLOOR PLAN
A-203	3rd FLOOR PLAN
A-204	TYPICAL - 4th & 7th FLOOR PLAN
A-205	TYPICAL - 5th & 6th FLOOR PLAN
A-206	TYPICAL - 8th & 9th FLOOR PLAN
A-207	10th FLOOR PLAN
A-208	ANCILLARY SERVICES
A-209	ROOF PLAN
A-211	PARKADE LEVEL 1 PLAN
A-212	PARKADE LEVEL 2 PLAN
A-213	PARKADE LEVEL 3 PLAN



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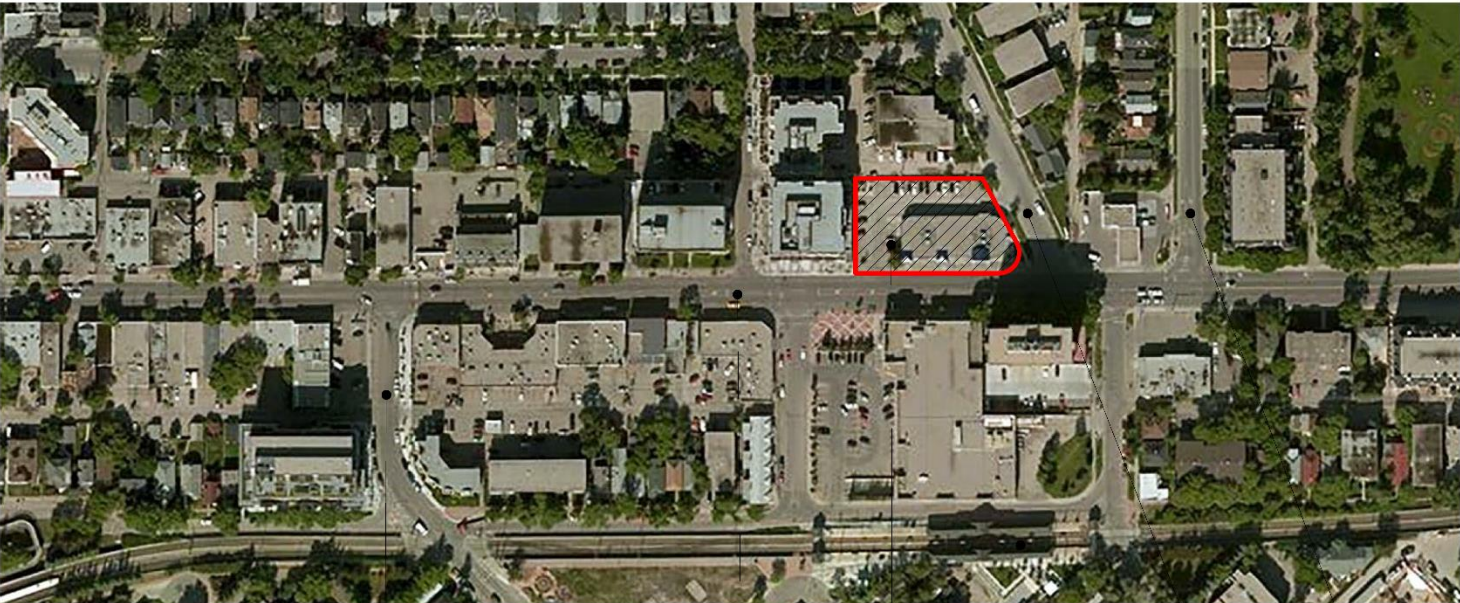


COVER SHEET
The Theodore
417 Tenth Street N.W., Calgary, Alberta

Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: NTS

A-000

Development Permit Drawings



2 AVENUE NW		10TH STREET NW	PROPOSED SITE	SUNNYSIDE LRT	GLADSTONE ROAD NW	5 AVENUE NW
1 Municipal Address - 417 10th Street NW., Calgary, Alberta						
3 Legal Address - Lots 10, Block 1, Descriptive Plan 1811460						
3 Zoning - Direct Control District DC152D2018 Proposed Uses Include - Dwelling Units; Outdoor Café; Restaurant; Licensed - Medium, Retail and Consumer Service; Financial Institution						
4 Site Area 2243.16m ² (24145.19 sq. ft.) 0.224ha (0.554 acres)						
5 Gross Floor Area Main Floor 1,373,407m ² (14,783,215 sq. ft.) Second Floor 1,464,369m ² (15,762,321 sq. ft.) Typical Floor (Third - Ninth) 1,043,070m ² (11,227,501 sq. ft.) Tenth Floor 1,082,132m ² (11,647,990 sq. ft.) Ancillary Services 155.18 m ² (1,670.34 sq. ft.) (Not Included in FAR)						
Total Floor Area Above Grade 11,221,396m ² (120,786.00 sq. ft.) Total Floor Area Below Grade 4,004.86m ² (43,108 sq. ft.)						
6 Floor Area Ratio F.A.R. Permitted 5.00 F.A.R. Proposed 5.00						
7 Area Summary of Each Use						
a Main Floor: C.R.U. 1 - Retail and Consumer Service 186m ² (2002.08ft ²) C.R.U. 2 - Financial Institution 112m ² (1205.59ft ²) C.R.U. 3 - Retail and Consumer Service 88.0m ² (947.22ft ²) C.R.U. 4 - Retail and Consumer Service 96.0m ² (1033.33ft ²) C.R.U. 5 - Retail and Consumer Service 96.0m ² (1033.33ft ²) C.R.U. 6 - Restaurant (Licensed - Medium) 213m ² (2292.71ft ²) Outdoor Café 63m ² (678.13ft ²) Dwelling Units Type A 48.0m ² (515.69ft ²) Dwelling Unit Type A-1 49.0m ² (527.43ft ²) Lobby Second Floor: Dwelling Units Unit Type B1 (X13) 47.0m ² (505.90ft ²) Unit Type B1a 53.0m ² (570.48ft ²) Unit Type B2 66.0m ² (710.41ft ²) Unit Type B3 53.0m ² (570.48ft ²) Unit Type C1 67.0m ² (721.18ft ²) Unit Type C2(X2) 56.0m ² (602.77ft ²) Unit Type C3 61.0m ² (656.59ft ²) Unit Type C4 62.0m ² (667.36ft ²) Unit Type C5 57.0m ² (613.54ft ²) Unit Type C6 62.0m ² (667.36ft ²) Unit Type C7 30.0m ² (322.91ft ²) Typical Floor - 3rd to 6th Floor: Dwelling Units Unit Type D1 (X2) 62.0m ² (667.36ft ²) Unit Type D2 65.0m ² (698.85ft ²) Unit Type D2a 60.0m ² (646.83ft ²) Unit Type E (X4) 68.0m ² (731.95ft ²) Unit Type F1 87.0m ² (938.45ft ²) Unit Type F2 93.0m ² (1001.04ft ²) Unit Type F3 110.0m ² (1184.02ft ²) Unit Type G 118.0m ² (1270.14ft ²) Typical Floor - 7th to 9th Floor: Dwelling Units Unit Type D3 80.0m ² (861.11ft ²) Unit Type D4 (X2) 82m ² (882.63ft ²) Unit Type E (X4) 68.0m ² (731.94ft ²) Unit Type F1 83.0m ² (893.40ft ²) Unit Type F2 89.0m ² (957.98ft ²) Unit Type F3 110.0m ² (1184.02ft ²) Unit Type G 118.0m ² (1270.14ft ²) 10th Floor: Dwelling Units Type F1 67.0m ² (721.18ft ²) Unit Type F2 93.0m ² (1001.04ft ²) Unit Type F3 110.0m ² (1184.02ft ²) Unit Type G 118.0m ² (1270.14ft ²) Unit Type H (X2) 132.0m ² (1420.83ft ²) Unit Type I (X2) 140.0m ² (1506.94ft ²) 8 Height of Proposed Building - 31.82m (104ft 4.8in)						
9. Parking Requirement a. Dwelling Units: 0.75 resident vehicle stalls/unit. 115 Proposed units x 0.75 stalls/unit = 86.25 (25% reduction as per section 1352) 0.1 visitor stalls/unit. 115 Proposed units x 0.1 stalls/unit = 11.5 (25% reduction as per section 1352) b. Financial Institution : 3.0stalls/100m ² . 112m ² x 3.0stalls/100m ² = 3.36 (25% reduction as per section 1352) c. Retail and Consumer Service : 2.0stalls/100m ² . 466m ² x 2.0stalls/100m ² = 9.32 (25% reduction as per section 1352 and Max. reduction of 3 stalls as per section 1350) d. Restaurant: Licensed - Medium : 2.85 Stalls/10m ² of public area. 45% of 213.0m ² 95.8m ² x 2.85stalls/10m ² = 27.30 (25% reduction as per section 1352) e. Outdoor Café: 55.02m ² Total 102 stalls required 111 stalls provided * (Relaxation sought for 29 stalls as per parking study)						
10. Bicycle Parking Requirement a. Dwelling Units: 0.5 class I stalls/unit. 115 proposed units x 0.5 stalls/unit =57.5 0.1 class II stalls/unit. 115 proposed units x 0.1 stalls/unit =11.5 b. Financial Institution: No class I and II stalls required c. Retail and Consumer Service : No class I stalls required 1.0 class II stalls/250m ² . 481m ² x 1.0stalls /250m ² = 1.9 stalls d. Restaurant: Licensed - Medium: No class I stalls required 1.0 class II stalls/250m ² . 213.0m ² x 1.0stalls /250m ² = 0.85 stalls e. Outdoor Café: 63m ² No class I and II stalls required Total Bicycle Parking - Class I Stalls 57 stalls required 57 stalls provided Total Bicycle Parking - Class II Stalls 15 stalls required 15 stalls provided 11. Waste and Recycling Area Residential area requirements: Waste Produced per week = 115X0.3yd ³ = 34.5yd ³ 35 yd ³ of waste/6yd ³ Container of waste/bin = 5.83 or Area required = 8.12m ² / bin x 6 bins = 48.72m ² Commercial area requirements: Waste produce per week for CRU, Financial Institute and Restaurant = 807X3/1000(Every 1000 m ² of the development produce 3m ³ waste/ week) = 2.421m ³ or 3.166yd ³ of waste, 4yd ³ of waste Area required = 807m ² x 9m ² /1000m ² = 7.263m ² (9m ² of storage area/1000m ² of Gross floor area) 7.26m ² area required 47m ² area provided * (Waste & recycling collection shall be provided by private collection company)						

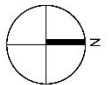


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VICINITY PLAN & PROJECT DATA

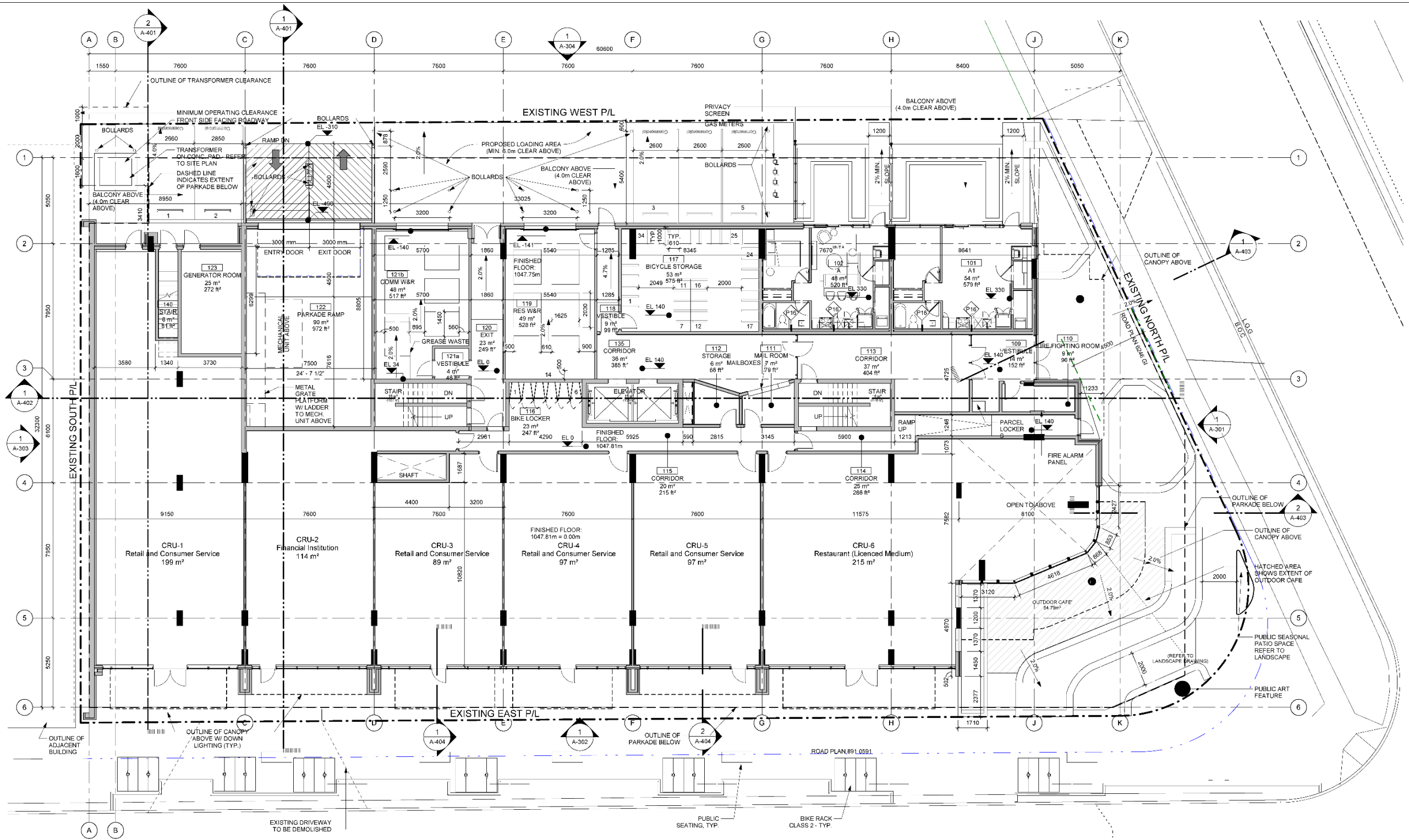
The Theodore
417 Tenth Street N.W., Calgary, Alberta



Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: NTS

A-001

Development Permit Drawings

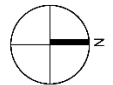


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MAIN FLOOR PLAN

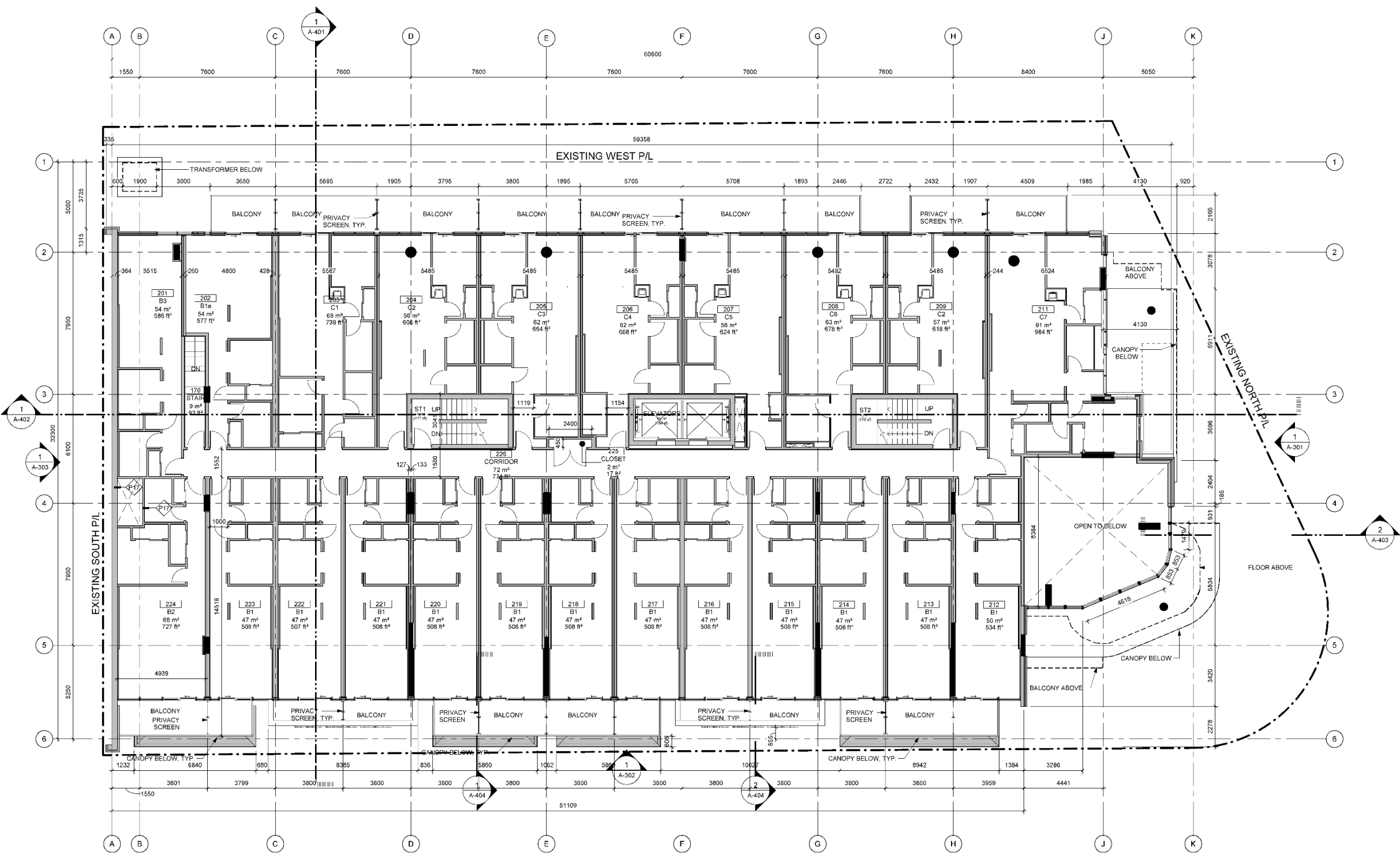
The Theodore
417 Tenth Street N.W., Calgary, Alberta



Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-201

Development Permit Drawings

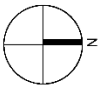


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2ND FLOOR PLAN

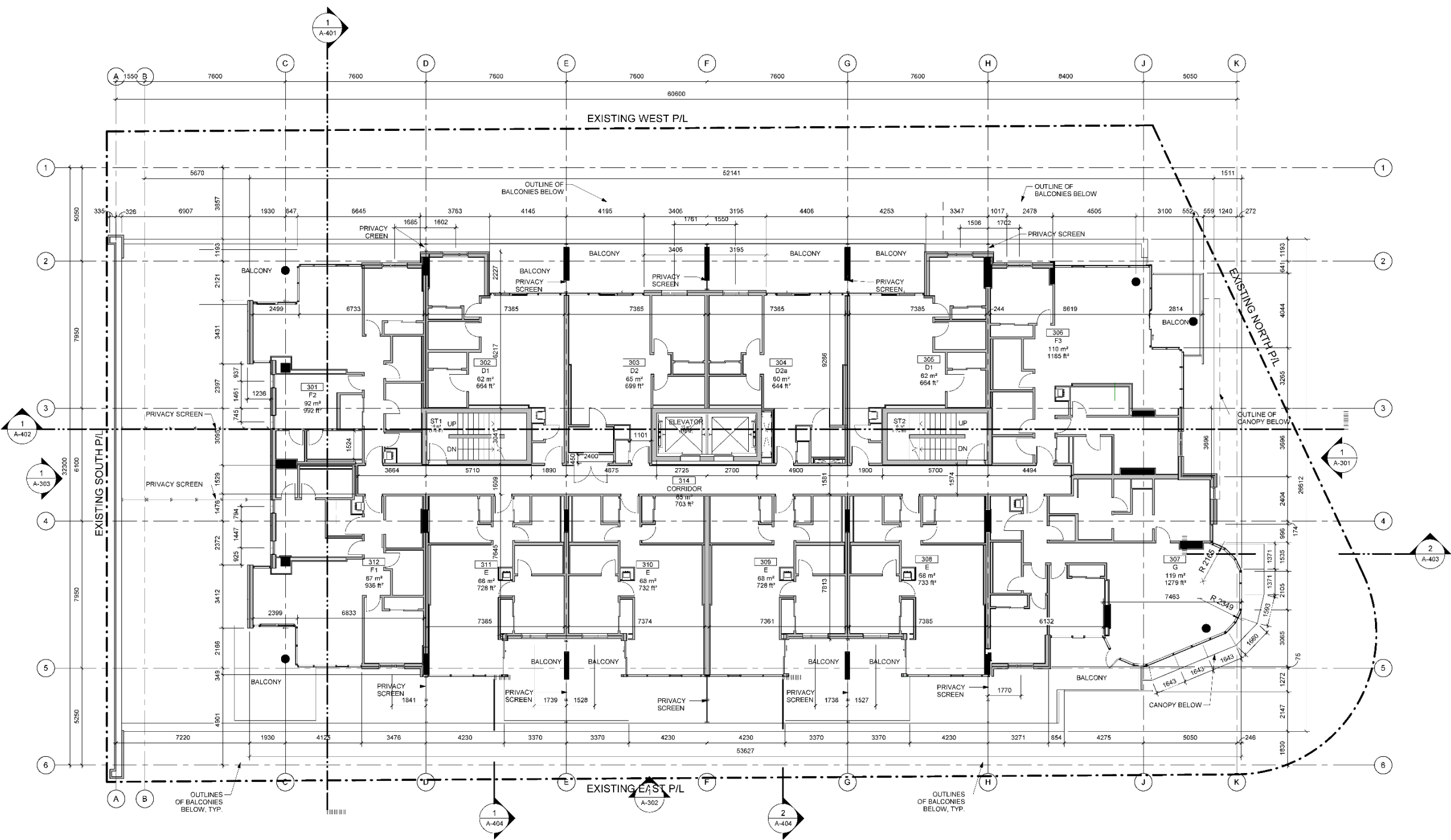
The Theodore
417 Tenth Street N.W., Calgary, Alberta



Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-202

Development Permit Drawings

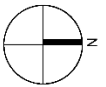


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3rd FLOOR PLAN

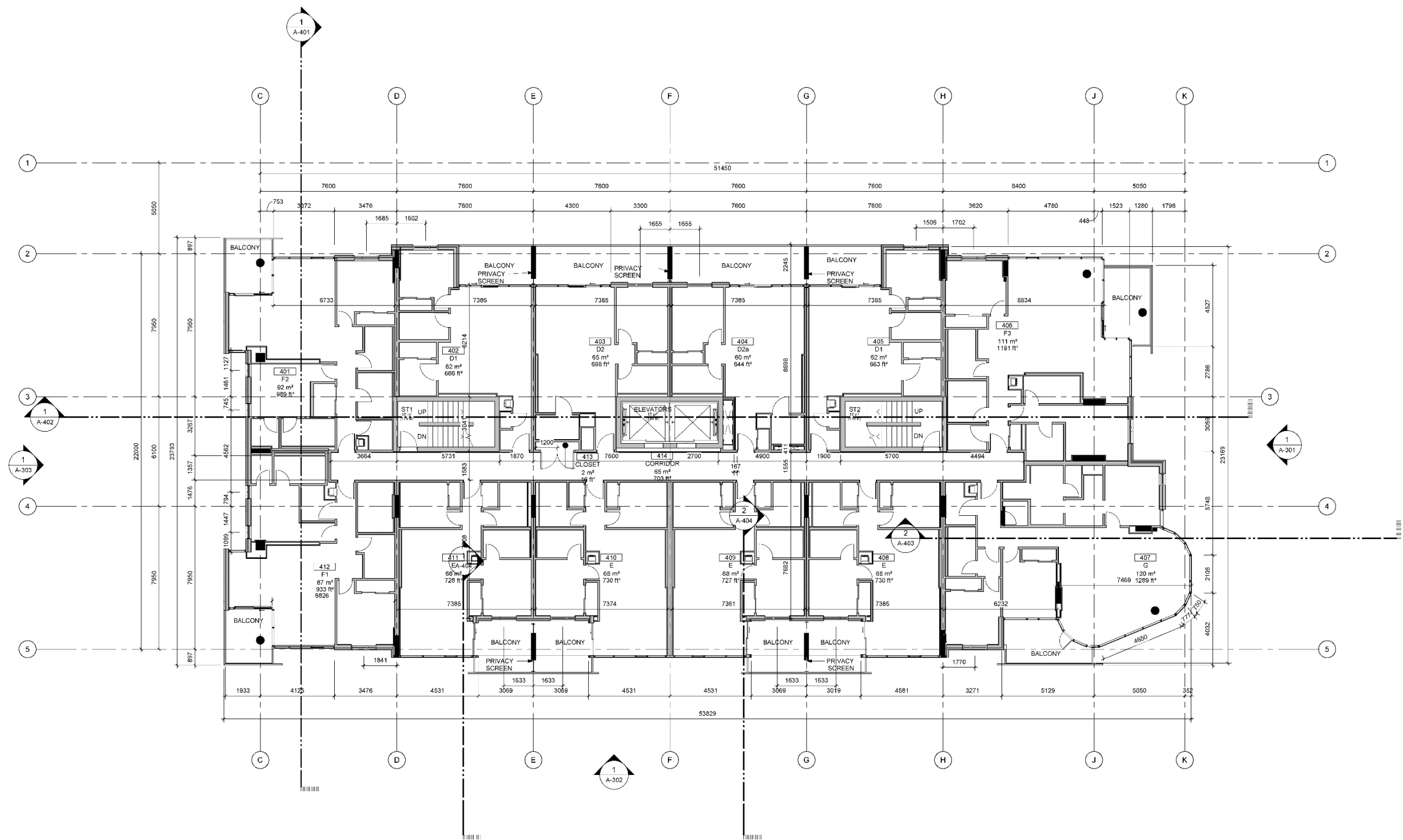
The Theodore
417 Tenth Street N.W., Calgary, Alberta



Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-203

Development Permit Drawings

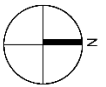


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TYPICAL - 4th & 7th FLOOR PLAN

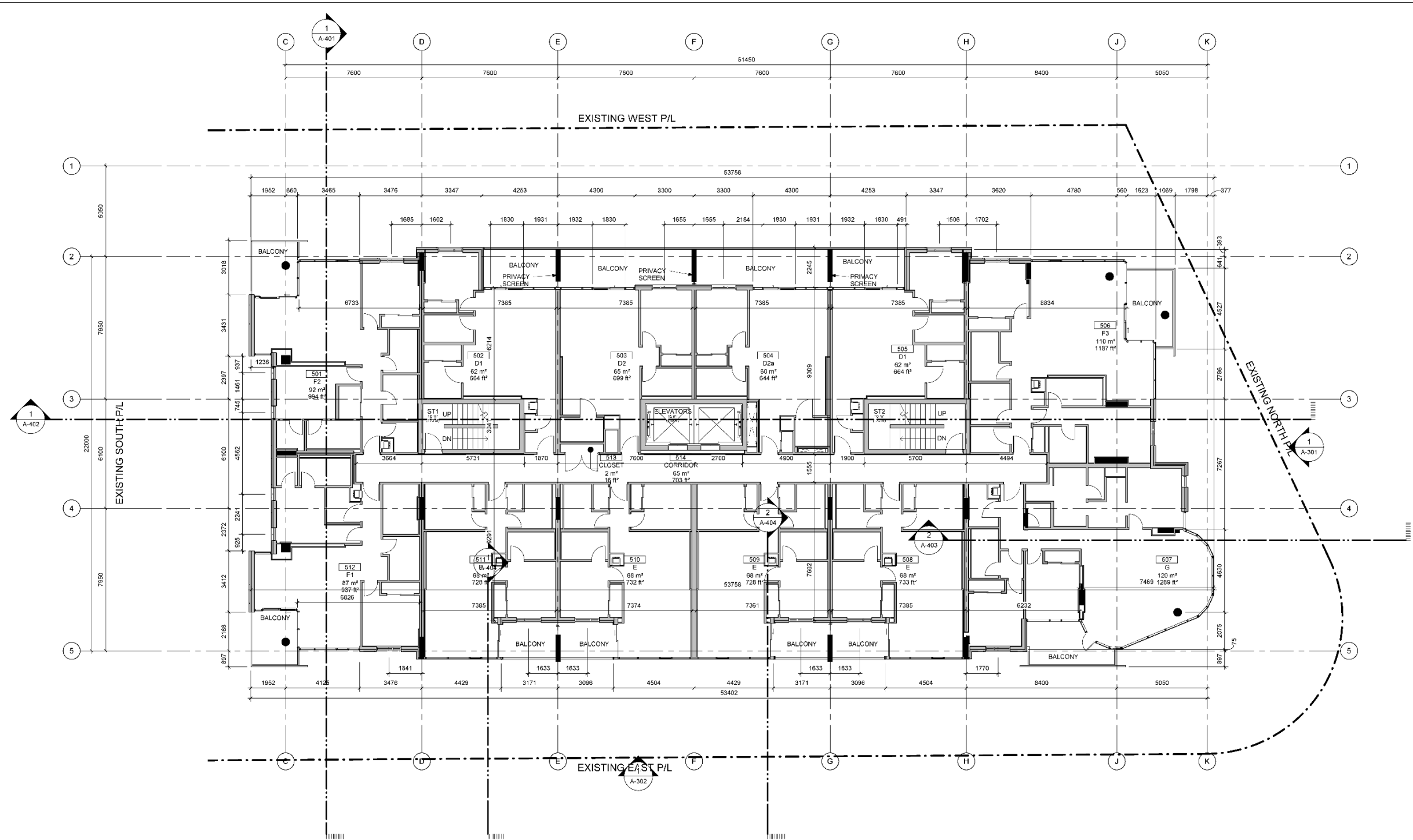
The Theodore
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Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-204

Development Permit Drawings

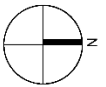


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TYPICAL - 5th & 6th FLOOR PLAN

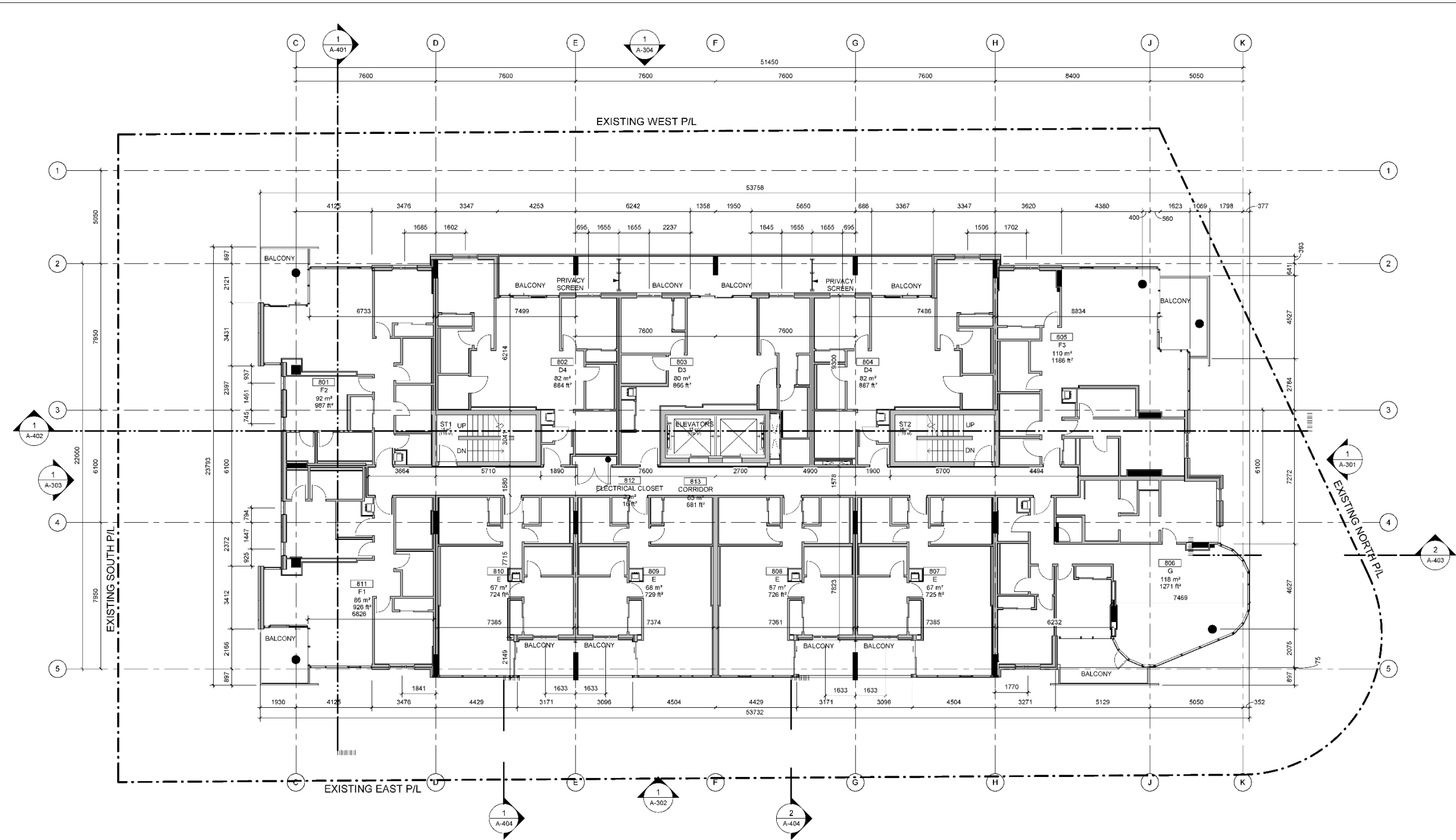
The Theodore
417 Tenth Street N.W., Calgary, Alberta



Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-205

Development Permit Drawings

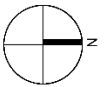


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TYPICAL - 8th & 9th FLOOR PLAN

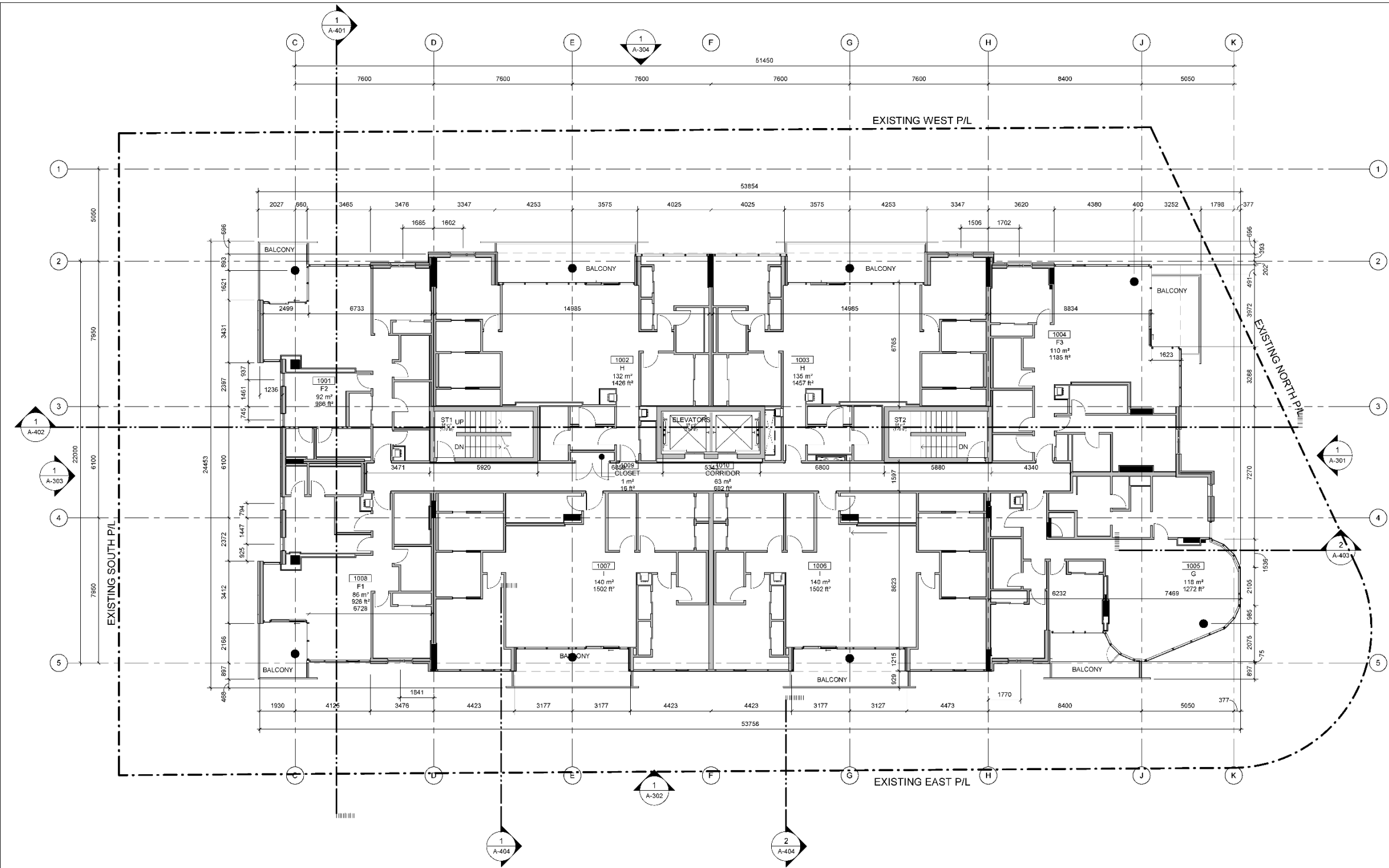
The Theodore
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Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-206

Development Permit Drawings

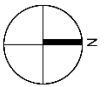


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10th FLOOR PLAN

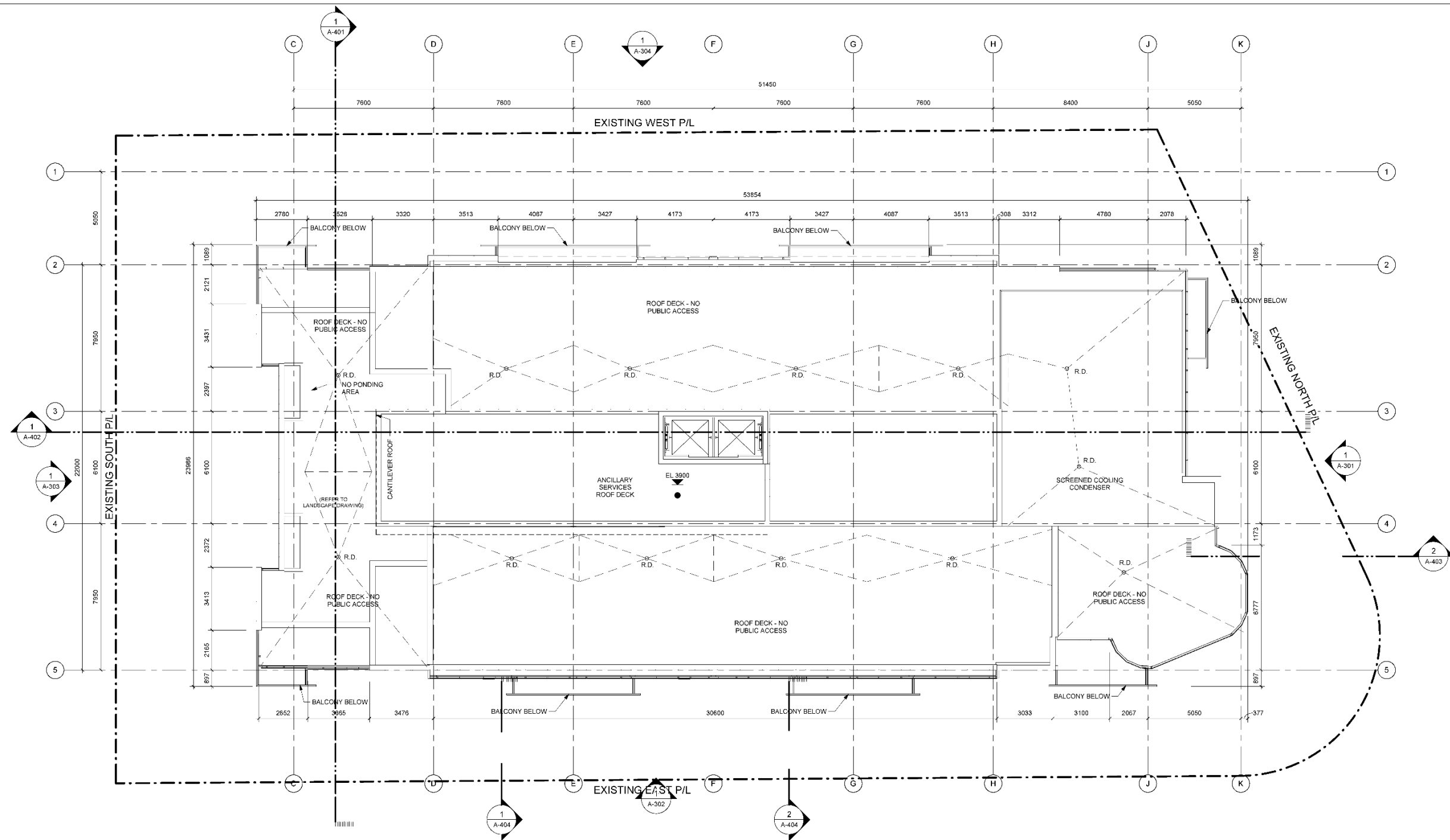
The Theodore
417 Tenth Street N.W., Calgary, Alberta



Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-207

Development Permit Drawings

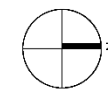


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ROOF PLAN

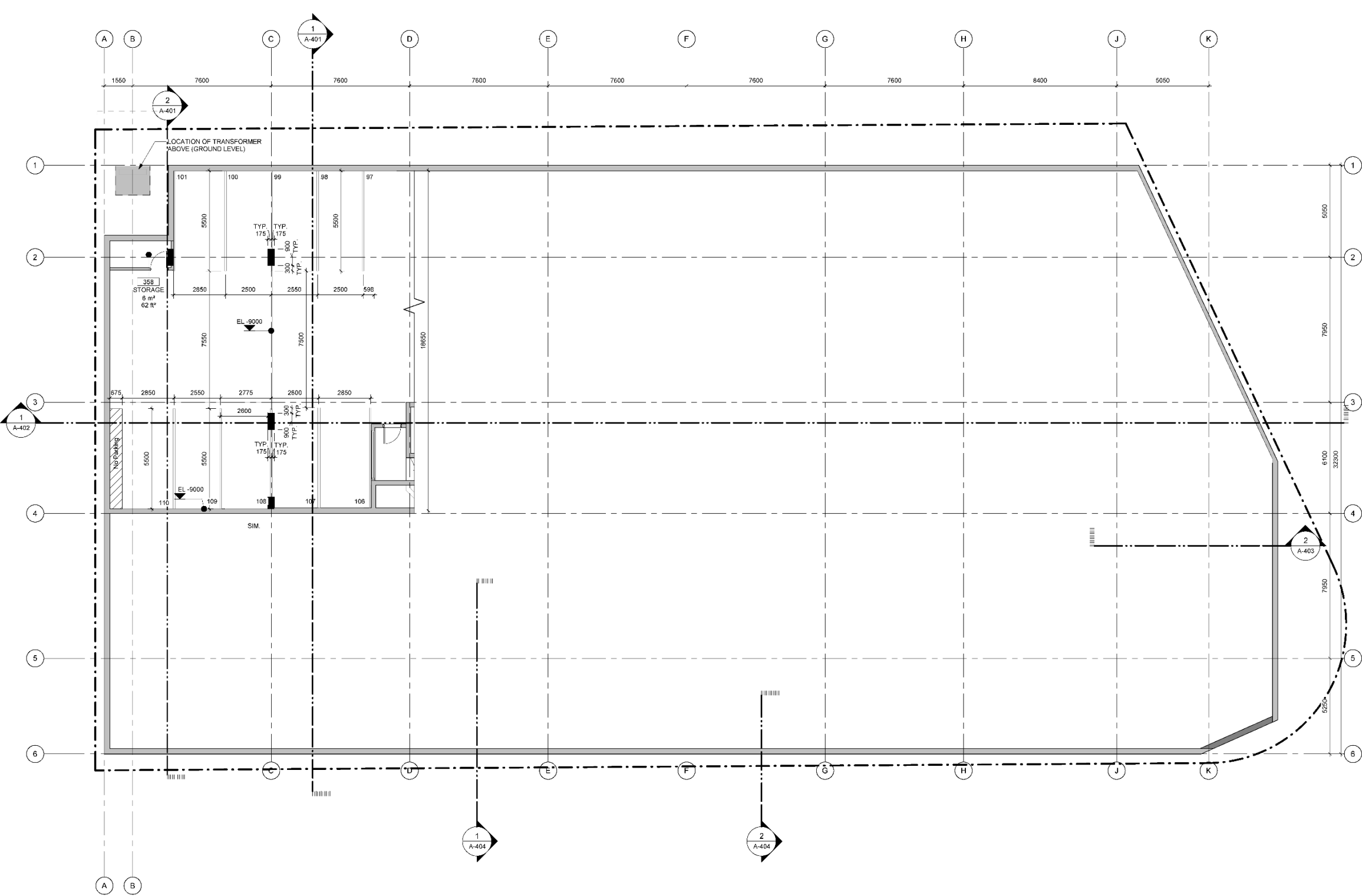
The Theodore
417 Tenth Street N.W., Calgary, Alberta



Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-209

Development Permit Drawings

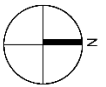


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PARKADE LEVEL 3 PLAN

The Theodore
417 Tenth Street N.W., Calgary, Alberta



Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-213

Development Permit Drawings

KEYNOTE LEGEND

- 1 BRICK - ARCTIC WHITE (400X200 & 400X100)
- 2 ALUMINUM CURTAIN WALL - TRANSPARENT VISION PANELS C/W 'WOOD' GRAIN MULLIONS
- 3 ALUMINUM WINDOW WALL - TRANSPARENT VISION PANELS C/W PEARL WHITE MULLIONS (OPERABLE WHERE INDICATED)
- 4 ALUMINUM WINDOW WALL - TRANSPARENT VISION PANELS C/W SSG (CAPLESS MULLION)
- 5 ALUMINUM WINDOW WALL - COLOURED DICHROIC LAMINATED GLASS
- 6 ALUMINUM WINDOWS - TRANSPARENT VISION PANELS C/W PEARL WHITE MULLIONS (OPERABLE WHERE INDICATED)
- 7 ALUMINUM GLAZED DOORS - TRANSPARENT VISION PANELS C/W PEARL WHITE MULLIONS
- 8 COMPOSITE METAL PANEL - PEARL WHITE
- 9 INSULATED METAL PANEL (MECH. PENTHOUSE & MECH. SCREENING - PEARL WHITE)
- 10 METAL DOOR (INSULATED HOLLOW METAL) - PEARL WHITE
- 11 OVERHEAD DOOR - PEARL WHITE
- 12 EIFS - WHITE
- 13 CANOPY - COMPOSITE METAL PANEL (PEARL WHITE C/W 'WOOD' GRAIN SOFFIT)
- 14 CONCRETE CURB - GRAY
- 15 CONCRETE PLANTER - GRAY C/W 'WOOD' SEATING
- 16 CONCRETE COLUMNS - GRAY
- 17 ALUMINUM RAILINGS - PEARL WHITE & CLEAR VISION PANELS
- 18 ALUMINUM RAILINGS - PEARL WHITE & ETCHED TRANSLUCENT GLASS PANELS
- 19 ALUMINUM PRIVACY SCREENS - PEARL WHITE & ETCHED TRANSLUCENT GLASS PANELS
- 20 MECHANICAL LOUVERS - PEARL WHITE
- 21 MECHANICAL SCREEN - OPAQUE PERFORATED METAL SPANDREL PANEL
- 22 SIGN TYPE CLASS B
- 23 SIGN TYPE CLASS D
- 24 SIGN BANNER TYPE CLASS A
- 25 BOLLARDS - PAINTED YELLOW
- 26 TRANSFORMER
- 27 METAL FLASHING - PEARL WHITE
- 28 CONCRETE BLOCK - FIREWALL



NORTH ELEVATION

The Theodore
417 Tenth Street N.W., Calgary, Alberta



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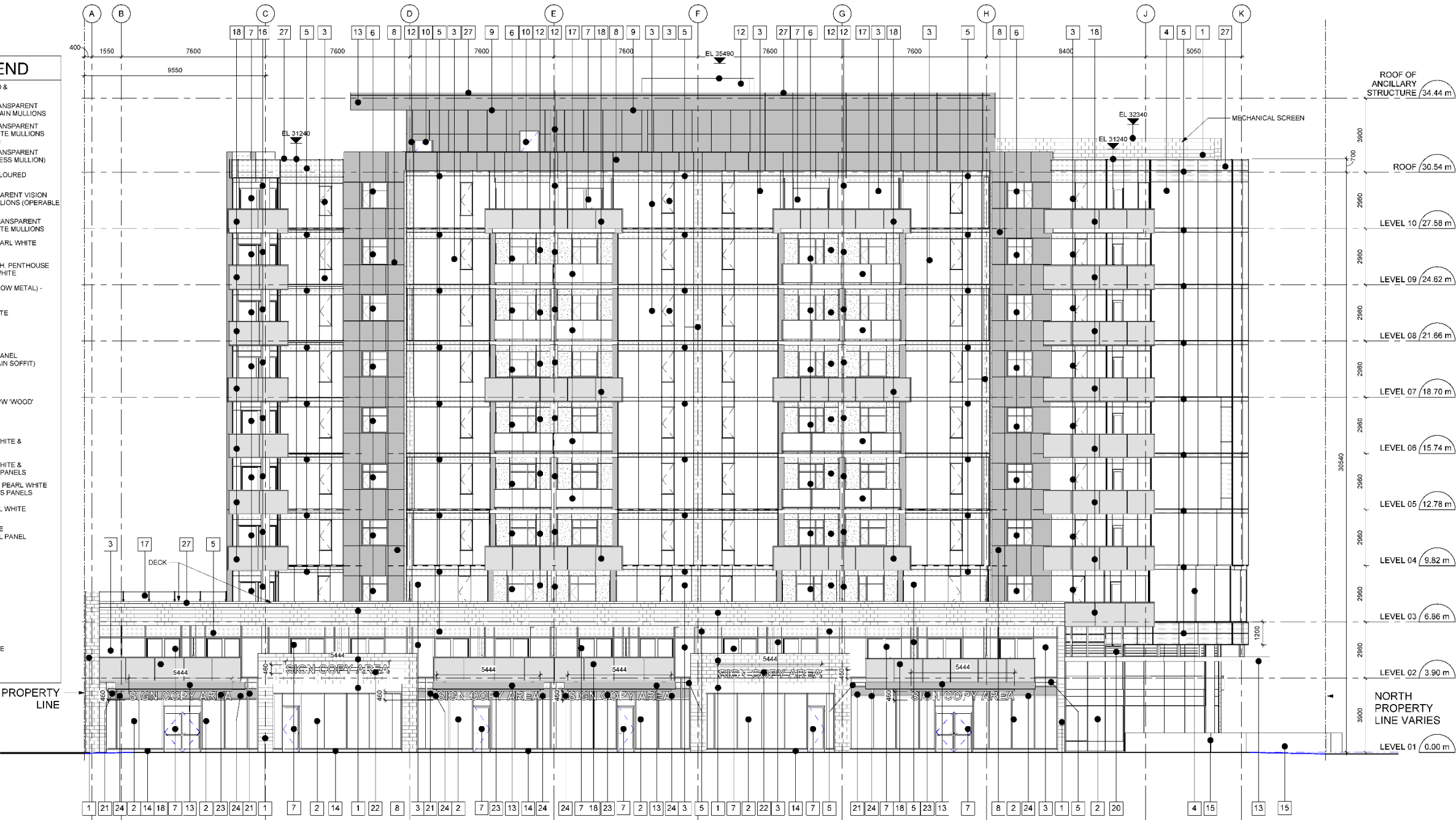
Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-301

Development Permit Drawings

KEYNOTE LEGEND

- 1 BRICK - ARCTIC WHITE (400X200 & 400X100)
- 2 ALUMINUM CURTAIN WALL - TRANSPARENT VISION PANELS C/W 'WOOD' GRAIN MULLIONS
- 3 ALUMINUM WINDOW WALL - TRANSPARENT VISION PANELS C/W PEARL WHITE MULLIONS (OPERABLE WHERE INDICATED)
- 4 ALUMINUM WINDOW WALL - TRANSPARENT VISION PANELS C/W SSG (CAPLESS MULLION)
- 5 ALUMINUM WINDOW WALL - COLOURED DICHROIC LAMINATED GLASS
- 6 ALUMINUM WINDOWS - TRANSPARENT VISION PANELS C/W PEARL WHITE MULLIONS (OPERABLE WHERE INDICATED)
- 7 ALUMINUM GLAZED DOORS - TRANSPARENT VISION PANELS C/W PEARL WHITE MULLIONS
- 8 COMPOSITE METAL PANEL - PEARL WHITE
- 9 INSULATED METAL PANEL (MECH. PENTHOUSE & MECH. SCREENING - PEARL WHITE)
- 10 METAL DOOR (INSULATED HOLLOW METAL) - PEARL WHITE
- 11 OVERHEAD DOOR - PEARL WHITE
- 12 EIFS - WHITE
- 13 CANOPY - COMPOSITE METAL PANEL (PEARL WHITE C/W 'WOOD' GRAIN SOFFIT)
- 14 CONCRETE CURB - GRAY
- 15 CONCRETE PLANTER - GRAY C/W 'WOOD' SEATING
- 16 CONCRETE COLUMNS - GRAY
- 17 ALUMINUM RAILINGS - PEARL WHITE & CLEAR VISION PANELS
- 18 ALUMINUM RAILINGS - PEARL WHITE & ETCHED TRANSLUCENT GLASS PANELS
- 19 ALUMINUM PRIVACY SCREENS - PEARL WHITE & ETCHED TRANSLUCENT GLASS PANELS
- 20 MECHANICAL LOUVERS - PEARL WHITE
- 21 MECHANICAL SCREEN - OPAQUE PERFORATED METAL SPANDREL PANEL
- 22 SIGN TYPE CLASS B
- 23 SIGN TYPE CLASS D
- 24 SIGN BANNER TYPE CLASS A
- 25 BOLLARDS - PAINTED YELLOW
- 26 TRANSFORMER
- 27 METAL FLASHING - PEARL WHITE
- 28 CONCRETE BLOCK - FIREWALL



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EAST ELEVATION

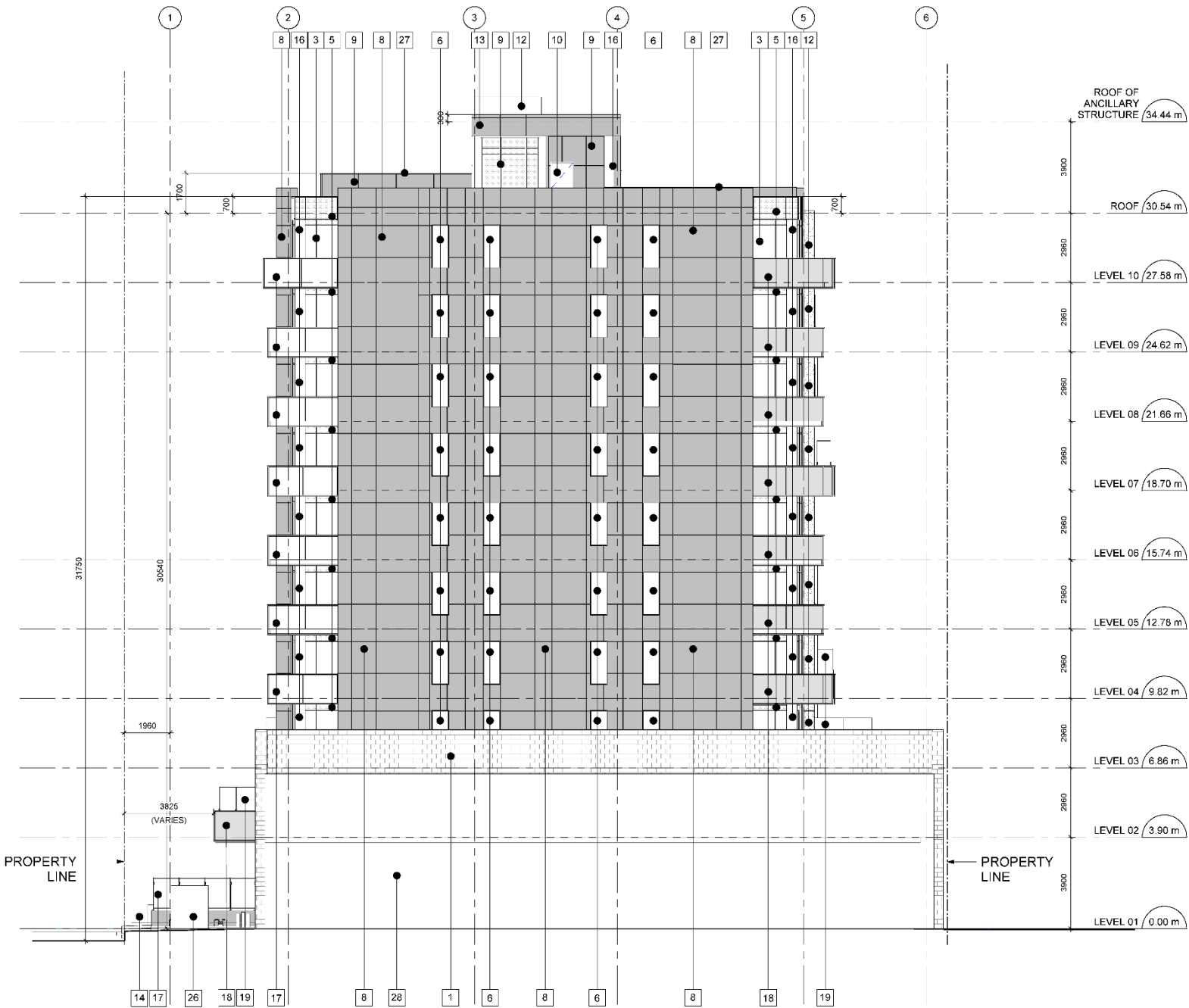
The Theodore
417 Tenth Street N.W., Calgary, Alberta

Issue: DTR-2 Submission
Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-302

Development Permit Drawings

KEYNOTE LEGEND	
1	BRICK - ARCTIC WHITE (400X200 & 400X100)
2	ALUMINUM CURTAIN WALL - TRANSPARENT VISION PANELS C/W 'WOOD' GRAIN MULLIONS
3	ALUMINUM WINDOW WALL - TRANSPARENT VISION PANELS C/W PEARL WHITE MULLIONS (OPERABLE WHERE INDICATED)
4	ALUMINUM WINDOW WALL - TRANSPARENT VISION PANELS C/W SSG (CAPLESS MULLION)
5	ALUMINUM WINDOW WALL - COLOURED DICHROIC LAMINATED GLASS
6	ALUMINUM WINDOWS - TRANSPARENT VISION PANELS C/W PEARL WHITE MULLIONS (OPERABLE WHERE INDICATED)
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14	CONCRETE CURB - GRAY
15	CONCRETE PLANTER - GRAY C/W 'WOOD' SEATING
16	CONCRETE COLUMNS - GRAY
17	ALUMINUM RAILINGS - PEARL WHITE & CLEAR VISION PANELS
18	ALUMINUM RAILINGS - PEARL WHITE & ETCHED TRANSLUCENT GLASS PANELS
19	ALUMINUM PRIVACY SCREENS - PEARL WHITE & ETCHED TRANSLUCENT GLASS PANELS
20	MECHANICAL LOUVERS - PEARL WHITE
21	MECHANICAL SCREEN - OPAQUE PERFORATED METAL SPANDREL PANEL
22	SIGN TYPE CLASS B
23	SIGN TYPE CLASS D
24	SIGN BANNER TYPE CLASS A
25	BOLLARDS - PAINTED YELLOW
26	TRANSFORMER
27	METAL FLASHING - PEARL WHITE
28	CONCRETE BLOCK - FIREWALL



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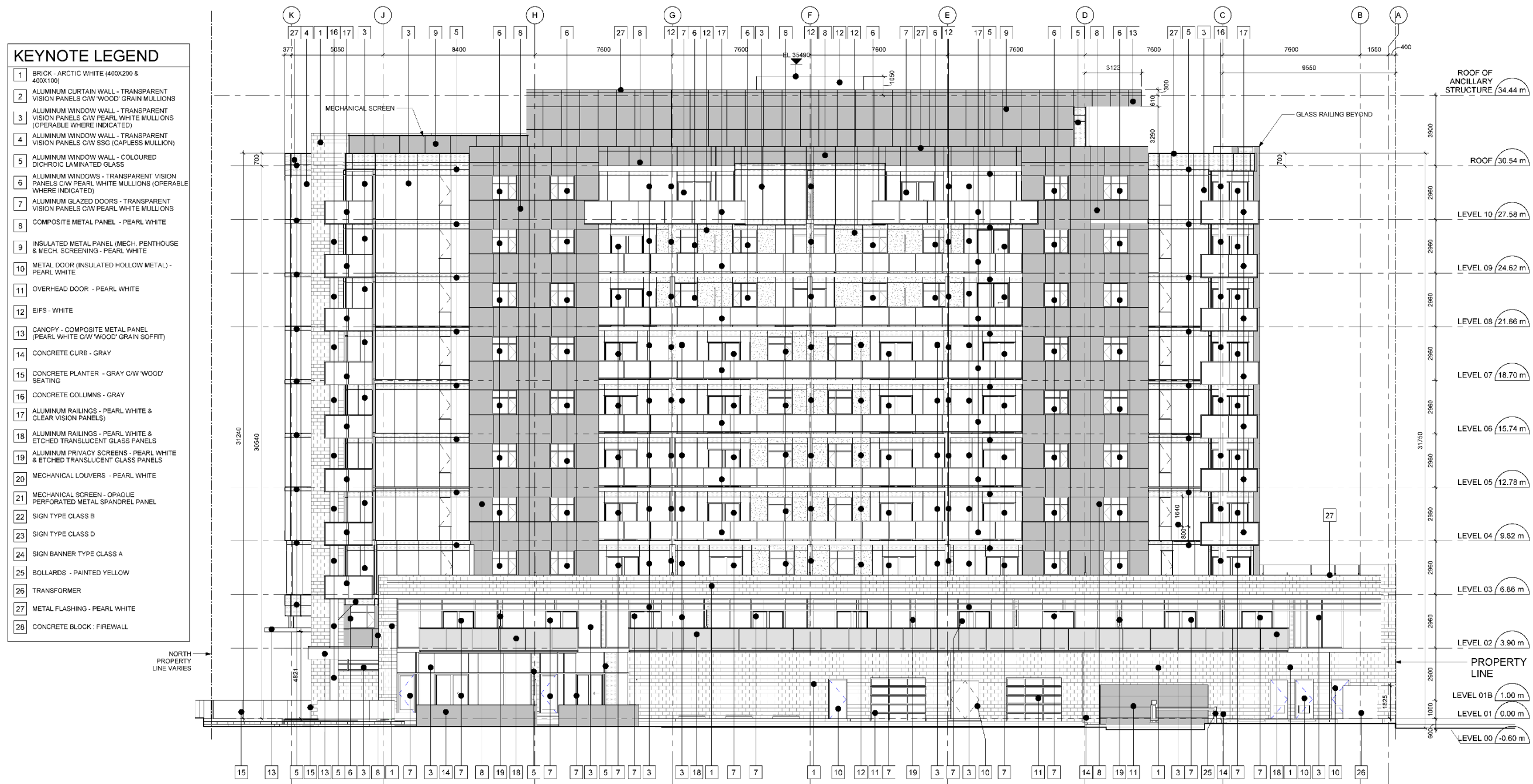
SOUTH ELEVATION

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A-303

Development Permit Drawings



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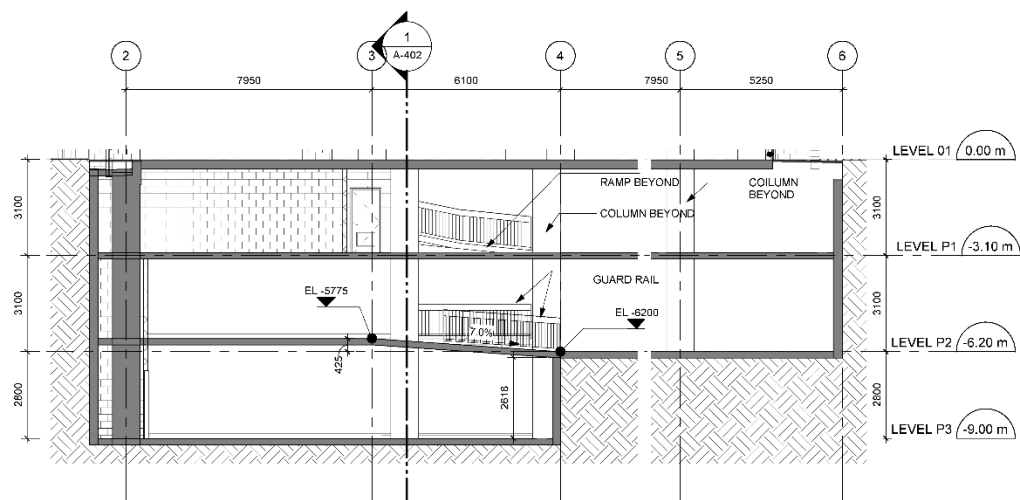
WEST ELEVATION

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Project No: 114782
Scale: 1:200

A-304

Development Permit Drawings

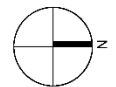


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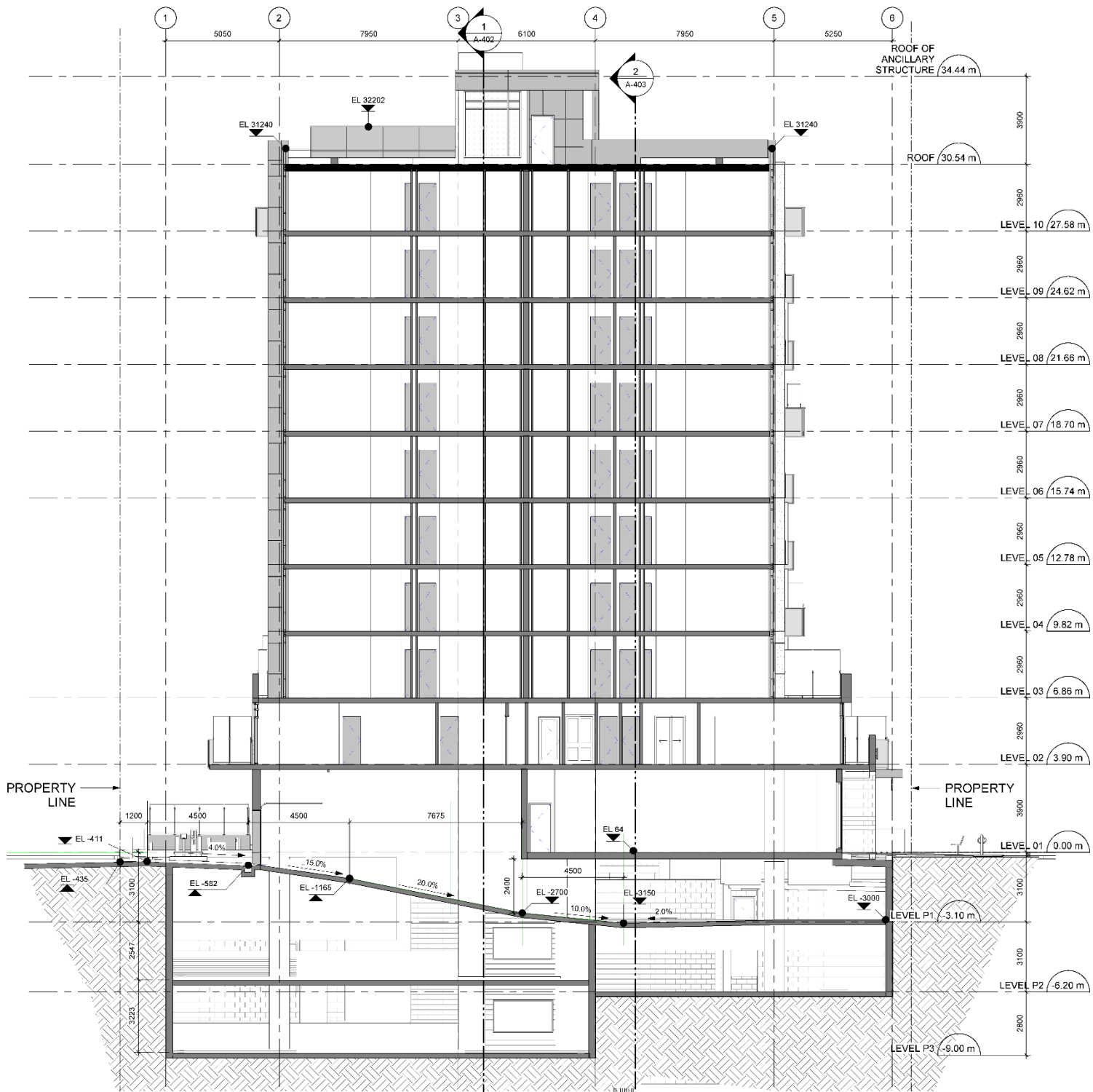
CROSS SECTION

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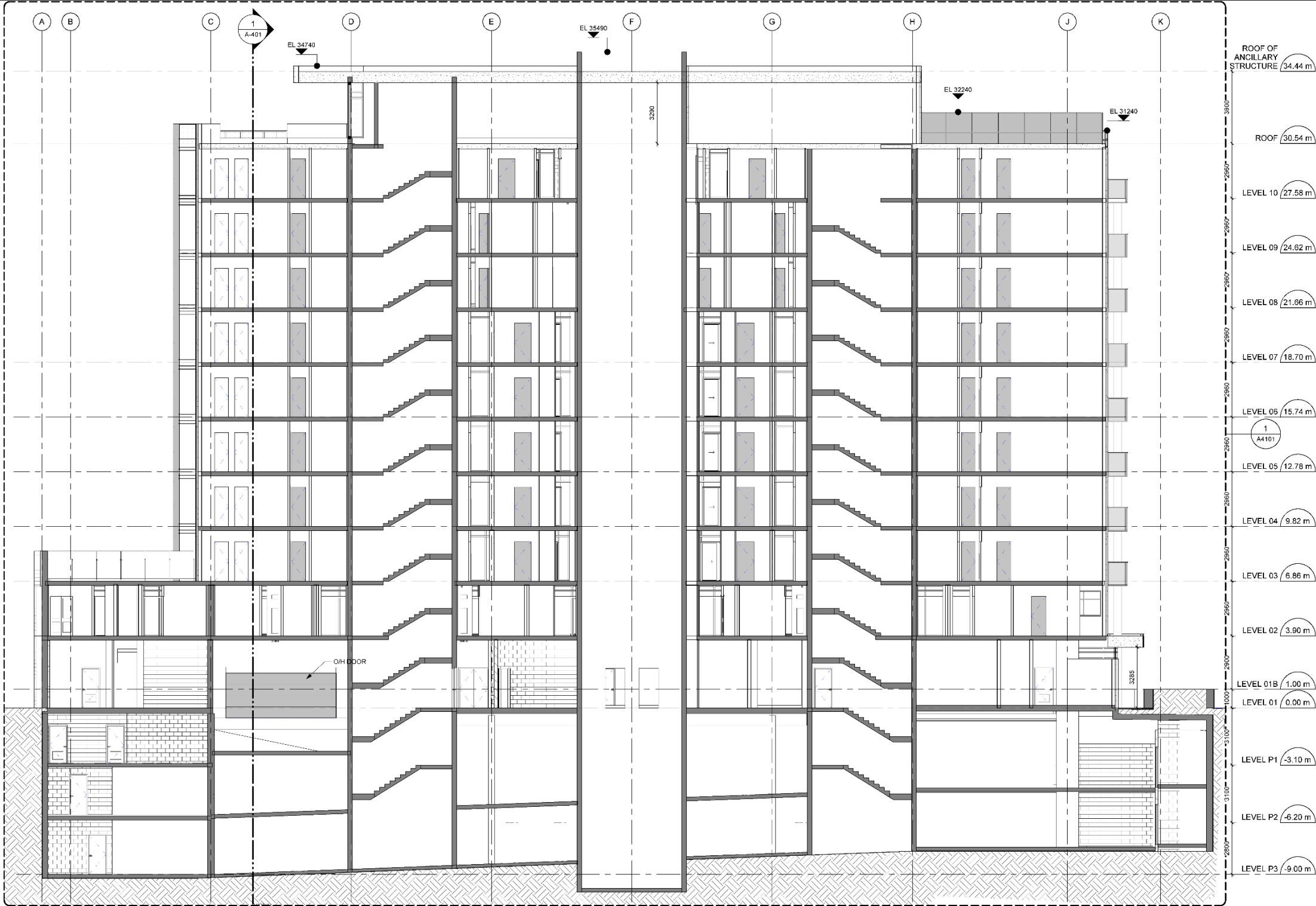


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A-401



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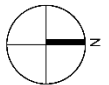


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LONGITUDINAL SECTION

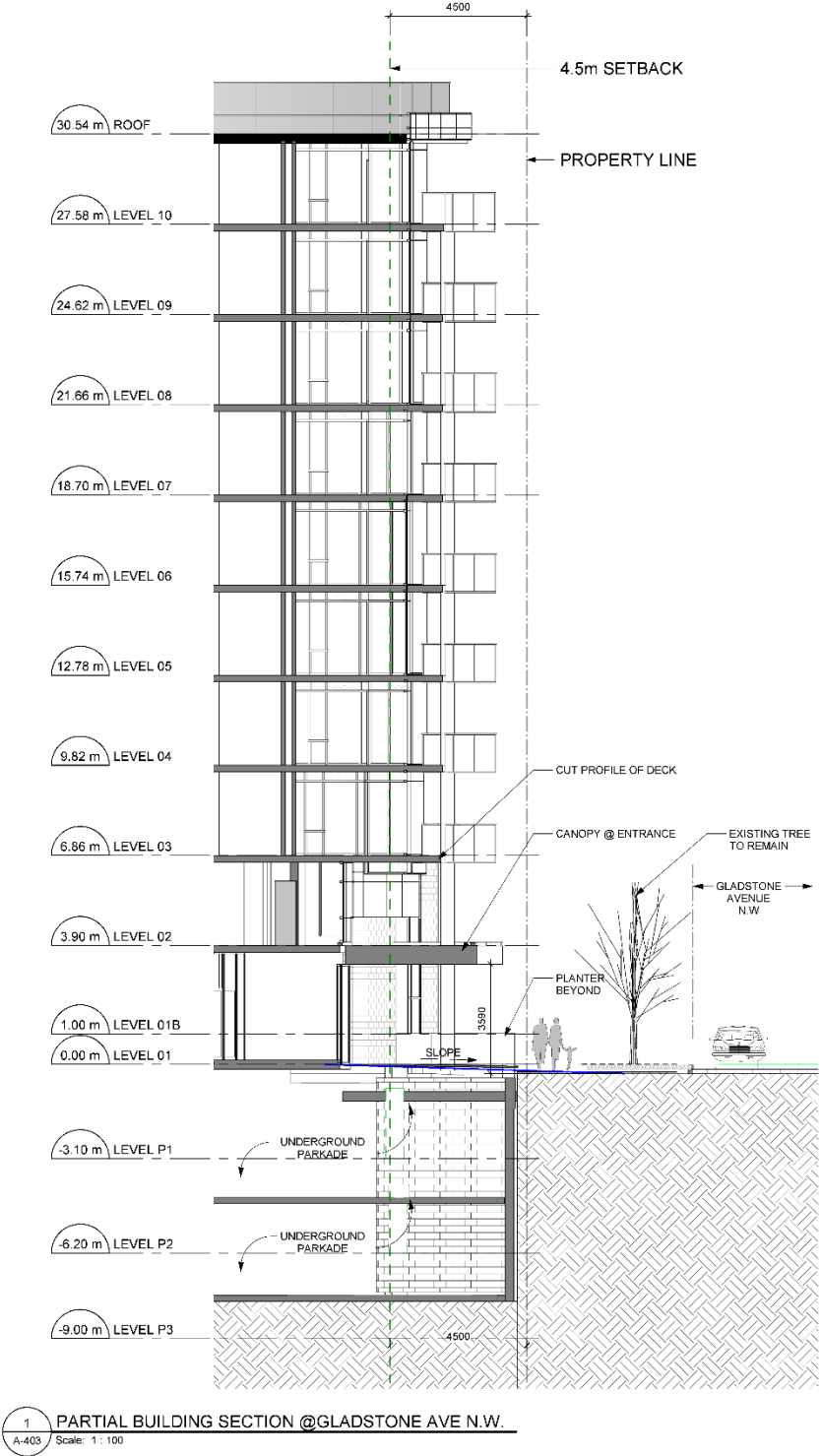
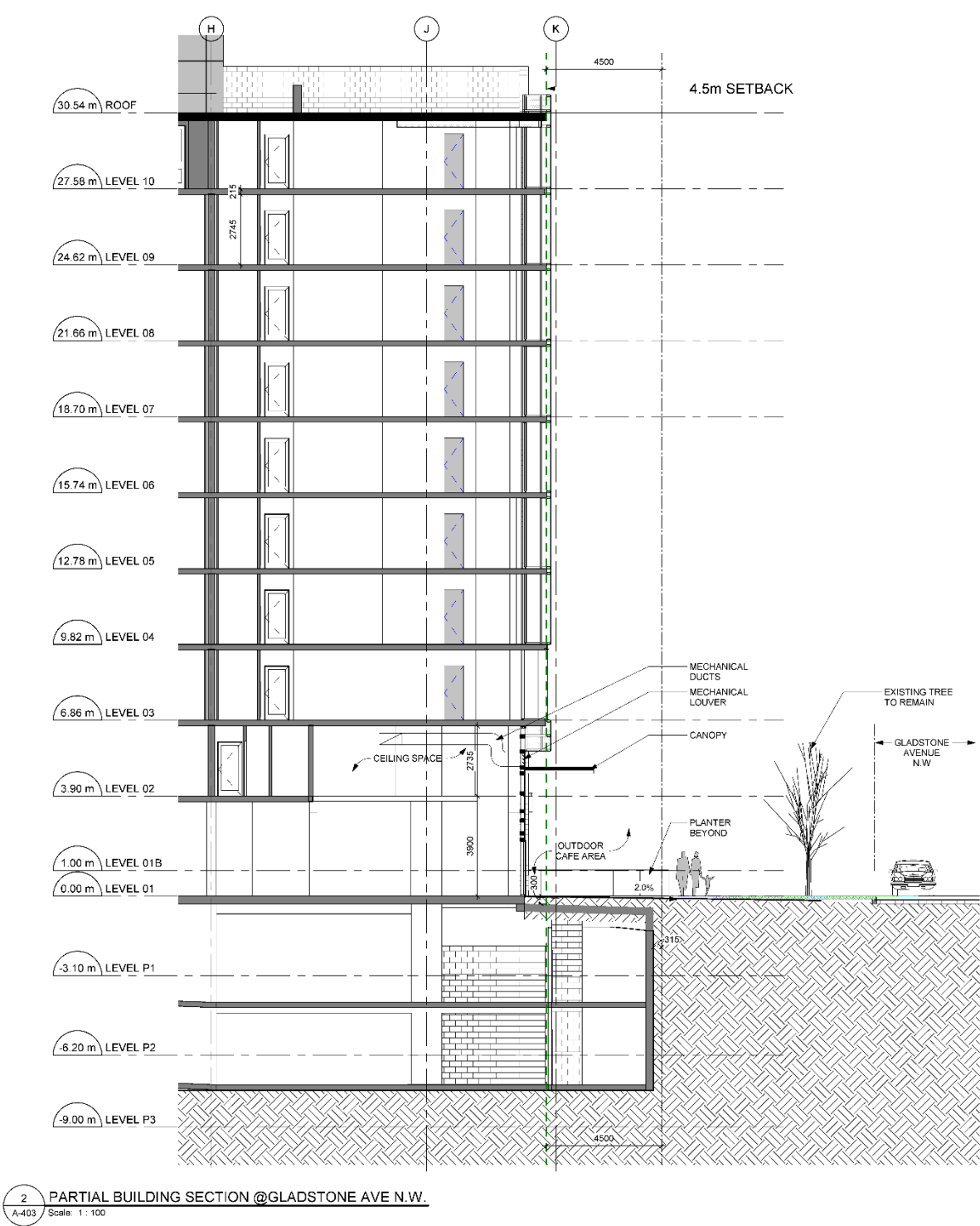
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Date: 2019-04-09
Project No: 114782
Scale: 1:200

A-402

Development Permit Drawings

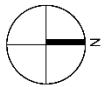


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PARTIAL BUILDING SECTIONS

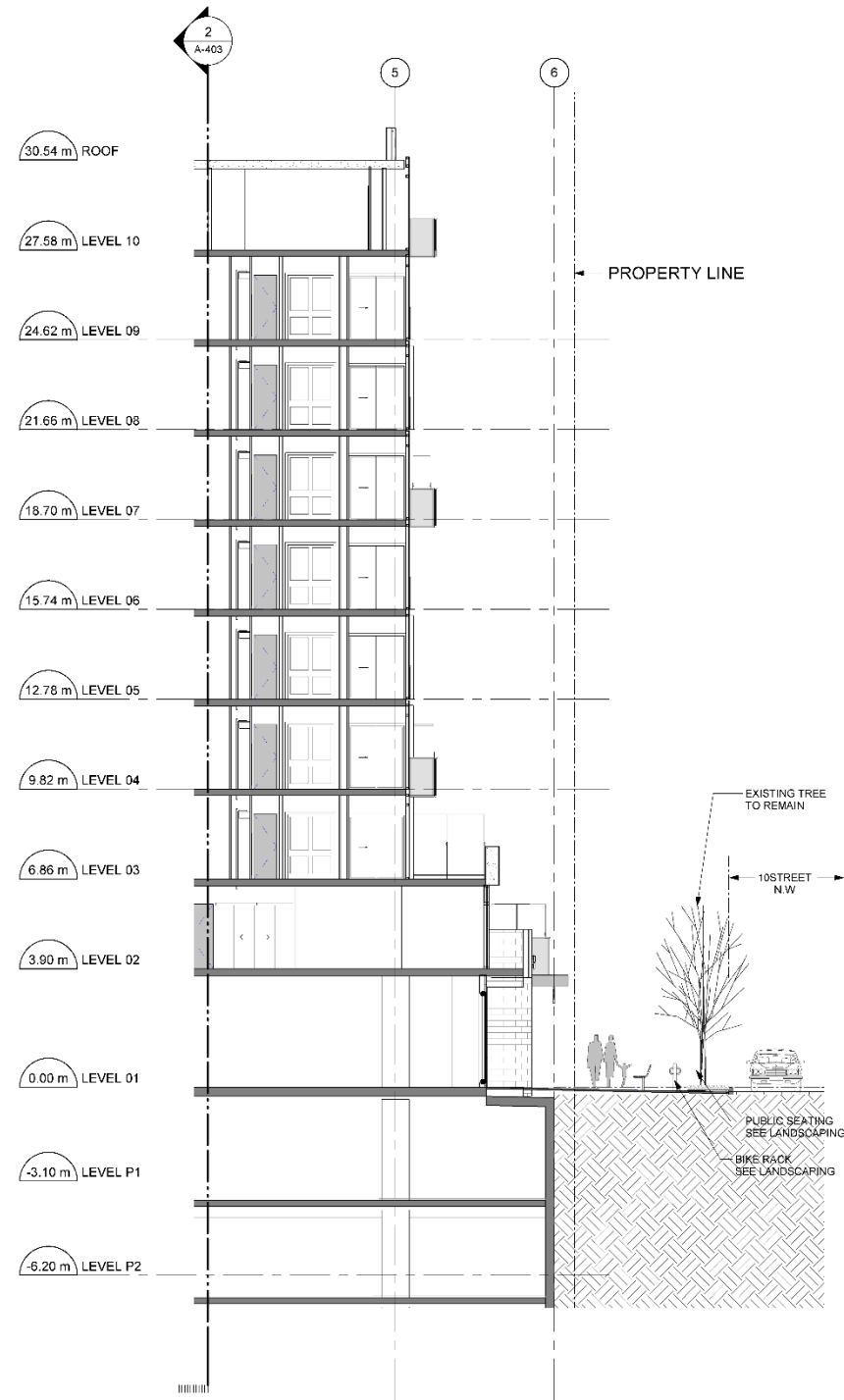
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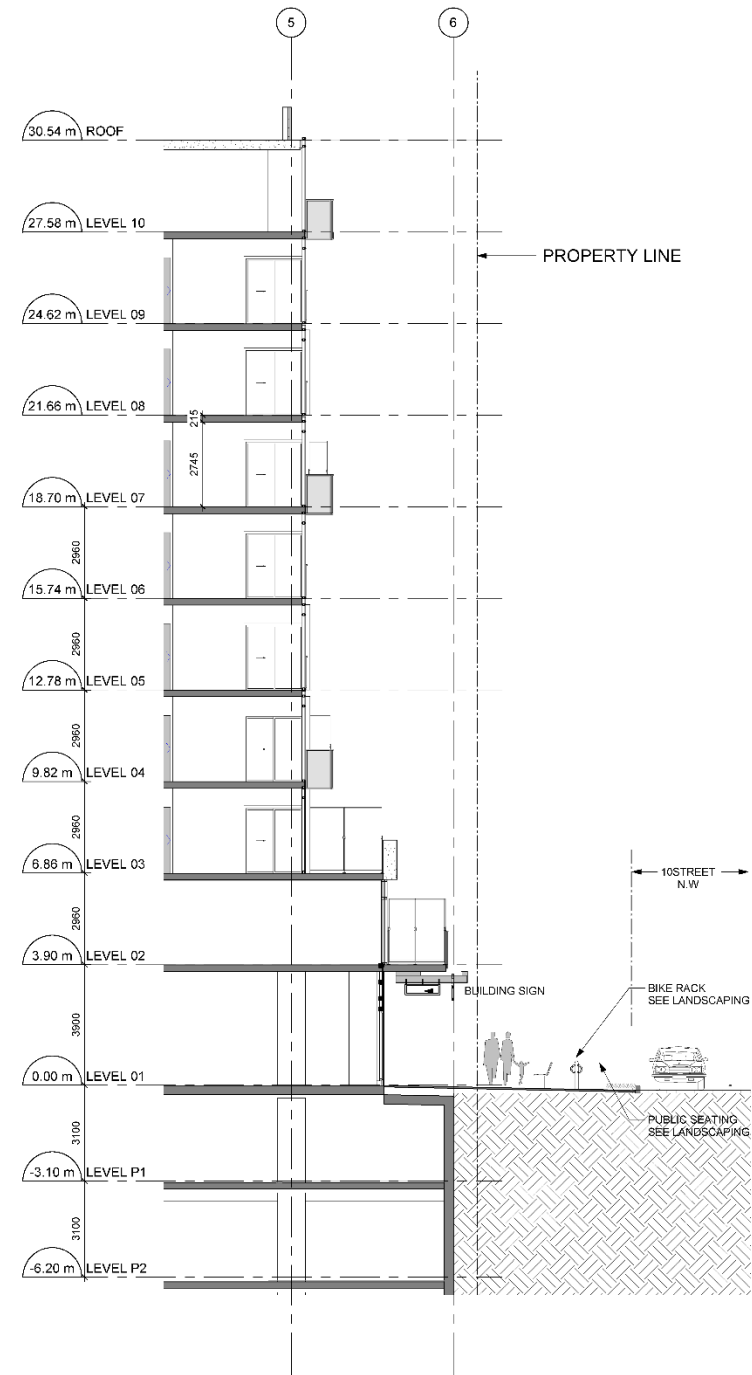
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Scale: 1:200

A-403

Development Permit Drawings



2 PARTIAL BUILDING SECTION @10 ST. NW
A-404 Scale: 1:100



1 PARTIAL BUILDING SECTION @10 ST. NW
A-404 Scale: 1:100

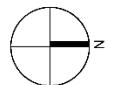


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PARTIAL BUILDING SECTIONS

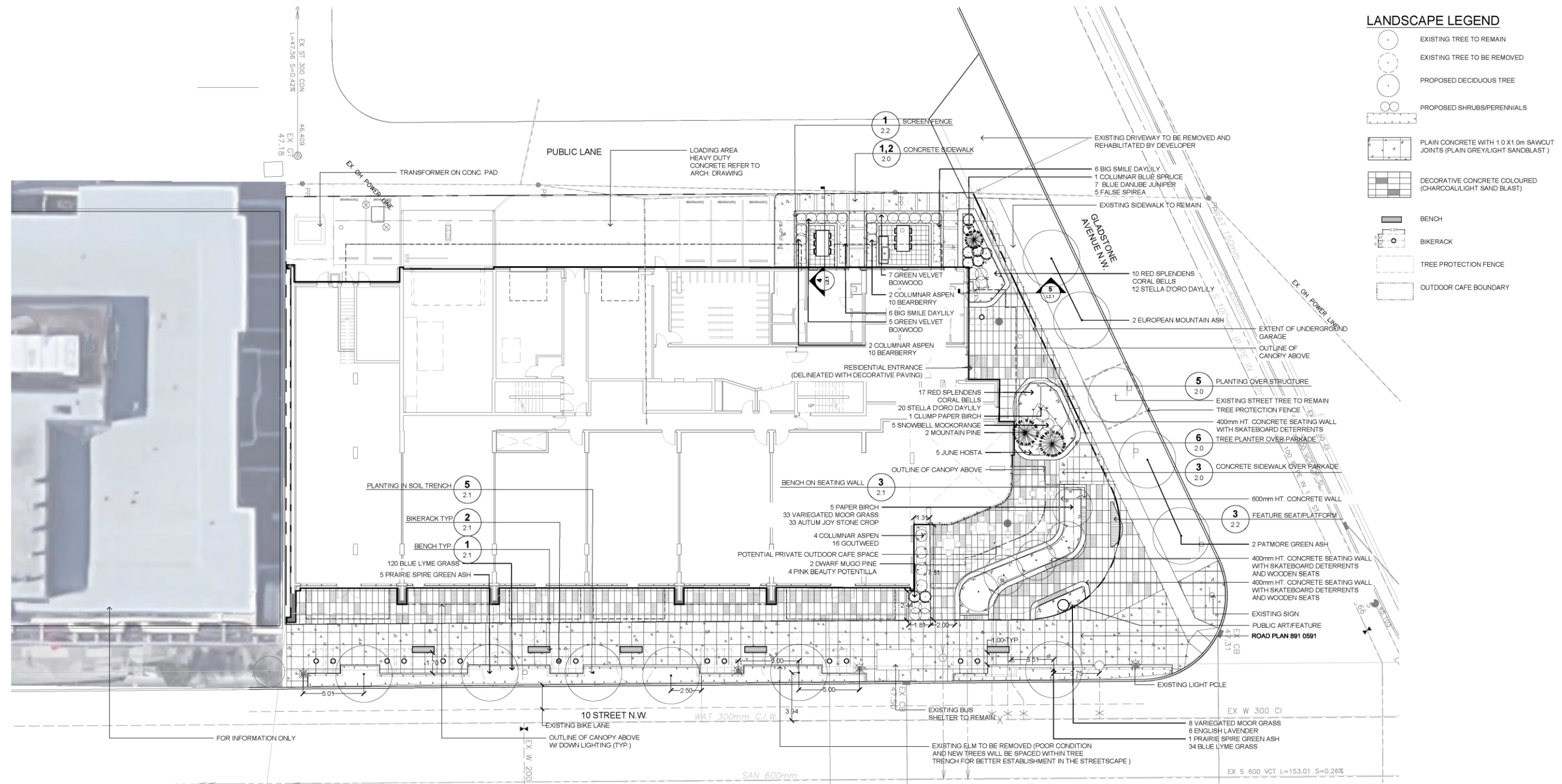
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Project No: 114782
Scale: 1:200

A-404

Development Permit Drawings



NOTE

ALL LANDSCAPE CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF CALGARY PARKS DEVELOPMENT GUIDELINES AND STANDARDS FOR LANDSCAPE CONSTRUCTION (CURRENT VERSION).

ALL SHRUB BED AND TREES TO BE IRRIGATED BY A LOW WATER IRRIGATION SYSTEM.

FOR LANDSCAPED AREAS WITH A BUILDING BELOW, PLANTING AREAS MUST HAVE THE FOLLOWING MINIMUM SOIL DEPTHS:

(a) 1.2 METRES FOR TREES;
(b) 0.6 METRES FOR SHRUBS; AND 0.3 METRES FOR ALL OTHER PLANTING AREAS.

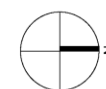


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LANDSCAPE PLAN

The Theodore
417 Tenth Street N.W., Calgary, Alberta



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Scale: 1 : 300

L1.0

Development Permit Drawings

PLANTING SCHEDULE				
QTY.	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
DECIDUOUS TREES				
6	FRAXINUS PENNSYLVANICA 'RUGBY'	PRAIRIE SPIRE GREEN ASH	80mm CAL	B&B
2	FRAXINUS PENNSYLVANICA 'PATMORE'	PATMORE GREEN ASH	80mm CAL	B&B
8	POPULUS TREMULA 'ERECTA'	COLUMNAR ASPEN	80mm CAL	B&B
1	BETULA PAPYRIFERA	CLUMP PAPER BIRCH	3 CLUMP -75mm CAL	B&B
5	BETULA PAPYRIFERA	PAPER BIRCH	60mm CAL	B&B
2	SORBUS AUCUPARIA	EUROPEAN MOUNTAIN ASH	80mm CAL	B&B
CONIFEROUS TREES				
2	PINUS UNICINATA	MOUNTAIN PINE	3000mm HT.	B&B
1	PICEA PLUNIGENS 'FASTIGIATA'	COLUMNAR BLUE SPRUCE	3000mm HT.	B&B
DECIDUOUS SHRUBS				
12	BUXUS 'GREEN VELVET'	GREEN VELVET BOXWOOD	600mm HT.	#2 CONT.
5	PHILADELPHUS 'SNOWBELLE'	SNOWBELL MOCKORANGE	600mm HT.	#2 CONT.
5	SORBARIA SORBIFOLIA	FALSE SPIREA	600mm HT.	#2 CONT.
4	POTENTILLA FRUTICOSA 'PINK BEAUTY'	PINK BEAUTY POTENTILLA	600mm HT.	#2 CONT.
CONIFEROUS SHRUBS				
7	JUNIPERUS SABINA 'BLUE DANUBE'	BLUE DANUBE JUNIPER	800mm SPREAD	#2 CONT.
2	PINUS MUGO 'VAR. PUMILIO'	DWARF MUGO PINE	800mm HT.	#2 CONT.
PERENNIAL AND GRASSES				
12	HEMEROCALLIS 'BIG SMILE'	BIG SMILE DAYLILY		15cm CONT.
8	LAVANDULA ANGUSTIFOLIA	ENGLISH LAVENDER		15cm CONT.
20	ARCTOSTAPHYLOS UVA-URSI	BEARBERRY		15cm CONT.
32	HEMEROCALLIS 'STELLA D'ORO'	STELLA D'ORO DAYLILY		15cm CONT.
5	HOSTA 'JUNE'	JUNE HOSTA		15cm CONT.
41	MOLINIA CAERULEA 'VARIEGATA'	VARIEGATED MOOR GRASS		15cm CONT.
154	ELYMUS ARENARIUS 'BLUE DUNE'	BLUE LYME GRASS		15cm CONT.
33	SEDUM 'AUTUMN JOY'	AUTUMN JOY STONE CROP		15cm CONT.
16	AEGOPODIUM PODAGRARIA 'VARIEGATUM'	GOUTWEED		15cm CONT.
27	HEUCHERA SANGUINEA 'SPLENDENS'	RED SPLENDENS CORAL BELLS		15cm CONT.

TREE PROTECTION PLAN

1. INSTALL TREE PROTECTION FENCING AS REQUIRED FOR TREES WITHIN 6m OF PROPERTY LINE.
2. FOR TREES CLOSER THAN 4m FROM DEVELOPMENT, PLACE FENCING AS CLOSE TO SIDE PROPERTY LINE AS POSSIBLE
3. FOR TREES OVER 4m FROM DEVELOPMENT, PLACE FENCING AT MIN. OF 4m FROM TRUNK CENTER
4. REHABILITATION MEASURES TO REPAIR ADJACENT CITY LAND SHOULD BE PERFORMED AS REQUIRED:
 - AERATION (IF SOIL IS SEVERELY COMPACTED)
 - ADDITIONAL WATERING (IF TREES ARE STRESSED DUE TO CONSTRUCTION ACTIVITY)
 - RE-SOD AS NECESSARY

IRRIGATION NOTES

1. DRIP IRRIGATION SYSTEM TO BE DESIGN BUILD BY CONTRACTOR
2. THE IRRIGATION CONTRACTOR SHALL VERIFY THE EXISTENCE AND LOCATION OF ALL UNDERGROUND UTILITIES AND SERVICES PRIOR TO CONSTRUCTION.
3. ENSURE A PROPER BACKFLOW PREVENTER IS INSTALLED AS PER THE NATIONAL PLUMBING CODE OF CANADA 2010
4. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE PROPER SPACING AND COVERAGE. LOCATION OF DRIP IRRIGATION LINES ARE TO COORDINATED WITH TREE AND SHRUB PLANTING. NO GRASS AREAS TO BE IRRIGATED.
6. IRRIGATION LINES CROSSING UNDER WALKWAYS SHALL BE ENCASED IN SCH 40 PVC SLEEVES WITH MINIMUM 300mm COVER. DIAMETER OF SLEEVES SHALL BE 2 X THE SUM OF THE DIAMETERS OF ENCASED IRRIGATION LINES. INSTALL ADJACENT 50mm WIRE CONDUIT WITH MAIN LINE CROSSING.
7. WIRE SPLICES SHALL BE MADE WITH 3M DBY CONNECTORS AND HOUSED IN CARSON-BROOKS LOCKING VALVE BOX. PROVIDE 1.0m MINIMUM LENGTH WIRE SLACK COILED INSIDE SPLICE BOX AND AT CHANGES IN MAINLINE DIRECTION.
8. INSTALL 1 SPARE # 14 WIRE FROM CONTROLLER CONTINUOUSLY FROM VALVE TO VALVE THROUGHOUT SYSTEM. COIL 600mm LENGTH OF SPARE WIRE NEATLY INTO EACH VALVE BOX.

PLANTING NOTES

1. PLANT LOCATIONS TO BE APPROVED BY LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
2. CONTRACTOR TO ENSURE THE LOCATION OF ALL UNDERGROUND UTILITIES ARE MARKED PRIOR TO CONSTRUCTION.
3. A DRIP IRRIGATION SYSTEM TO BE PROVIDED FOR ALL TREES AND SHRUBS.

MAINTENANCE, WARRANTY NOTES

1. THE MAINTENANCE & WARRANTY PERIOD TO BE ONE FULL GROWING SEASON (MAY-OCT) FROM SUBSTANTIAL COMPLETION OF THE LANDSCAPE.
2. WATER ALL PLANT MATERIAL AS REQUIRED FOR THE DURATION OF THE MAINTENANCE PERIOD.
3. WITHIN THE WARRANTY PERIOD, ANY DEAD TREES AND SHRUBS OR PLANT MATERIAL IN POOR CONDITION AS NOTED BY THE LANDSCAPE ARCHITECT SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.

SITE STATISTICS

LAND USE ZONING	DIRECT CONTROL DISTRICT DC152D2018
SITE AREA	2243.18m²
BUILDING	1333.62m²
GARBAGE & REAR PARKING	408.54m²
LANDSCAPE AREA	501.00m²

LANDSCAPE SITE AREA REQUIREMENTS

LAND USE	MIXED USE
LANDSCAPE AREA	501.00m²
HARD SURFACE LANDSCAPE	360m² (71%, MAX 50% ALLOWABLE)
SOFT SURFACE LANDSCAPE	121m² (24%)

OUTDOOR AMENITY SPACE REQ.	50m²
OUTDOOR AMENITY SPACE PROVIDED	79m²

PLANTING REQUIREMENTS

TREES REQUIRED (1/45m²)	11
TREES PROVIDED	17
SHRUBS REQUIRED (2/45m²)	22
SHRUBS PROVIDED	38

CON. TREES, 2m HT. REQ.	1
CON. TREES, 2m HT. PROVIDED	0
CON. TREES, 3M HT. REQ.	2
CON. TREES, 3m HT. PROVIDED	3
DEC. TREES, 50mm CAL. REQ.	4
DEC. TREES, 50mm CAL. PROVIDED	7
DEC. TREES, 75mm CAL. REQ.	4
DEC. TREES, 75mm CAL. PROVIDED	7

TOTAL TREES PROVIDED	17
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IRRIGATION
DRIP IRRIGATION TO BE PROVIDED FOR ALL LANDSCAPE AREAS.

BIKE RACK REQUIREMENTS

CLASS 2 BIKE RACKS PROVIDED	15
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LANDSCAPE STATISTICS

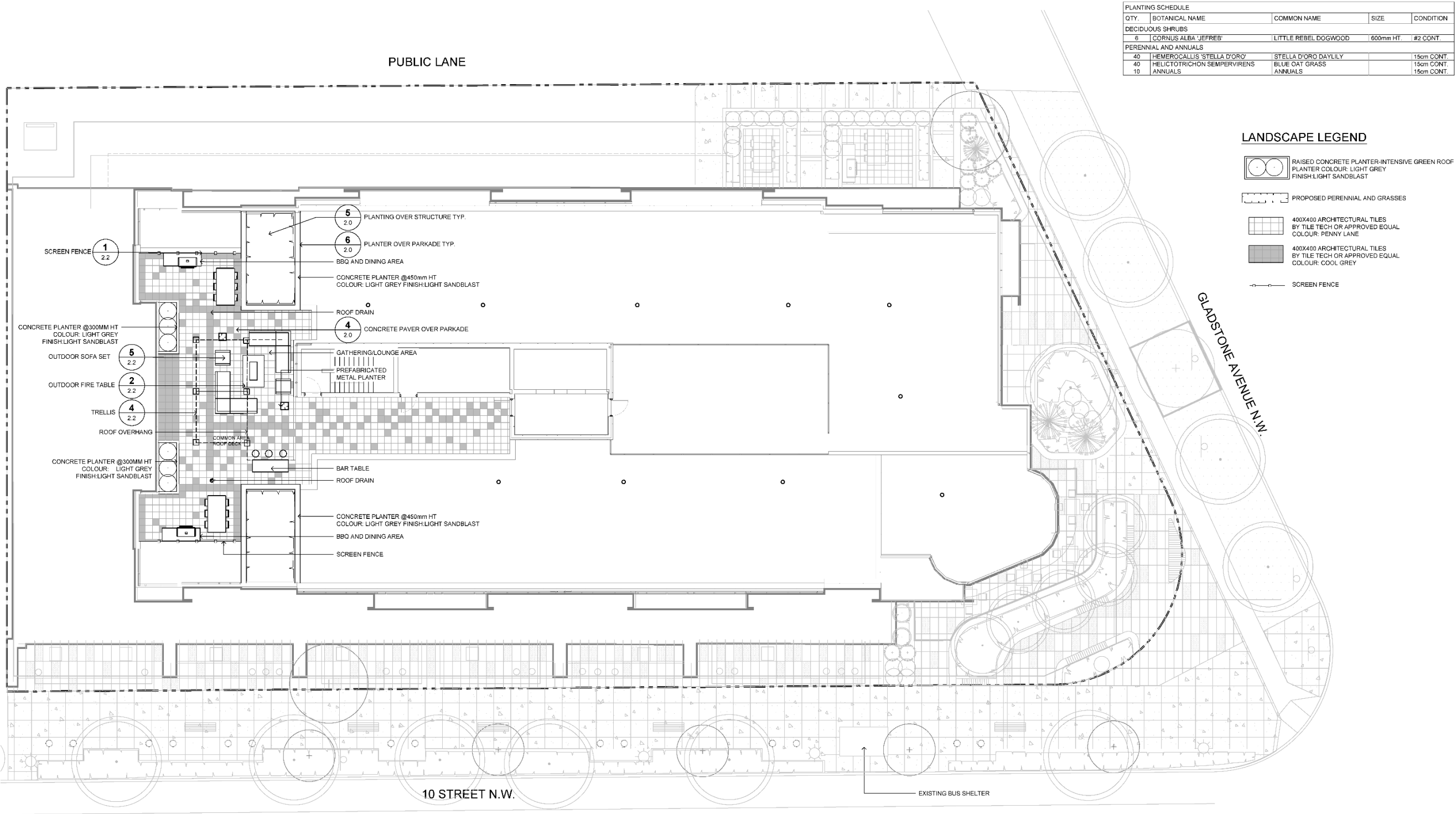
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L1.1

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GRAYWOOD
DEVELOPMENTS



ROOF LANDSCAPE PLAN

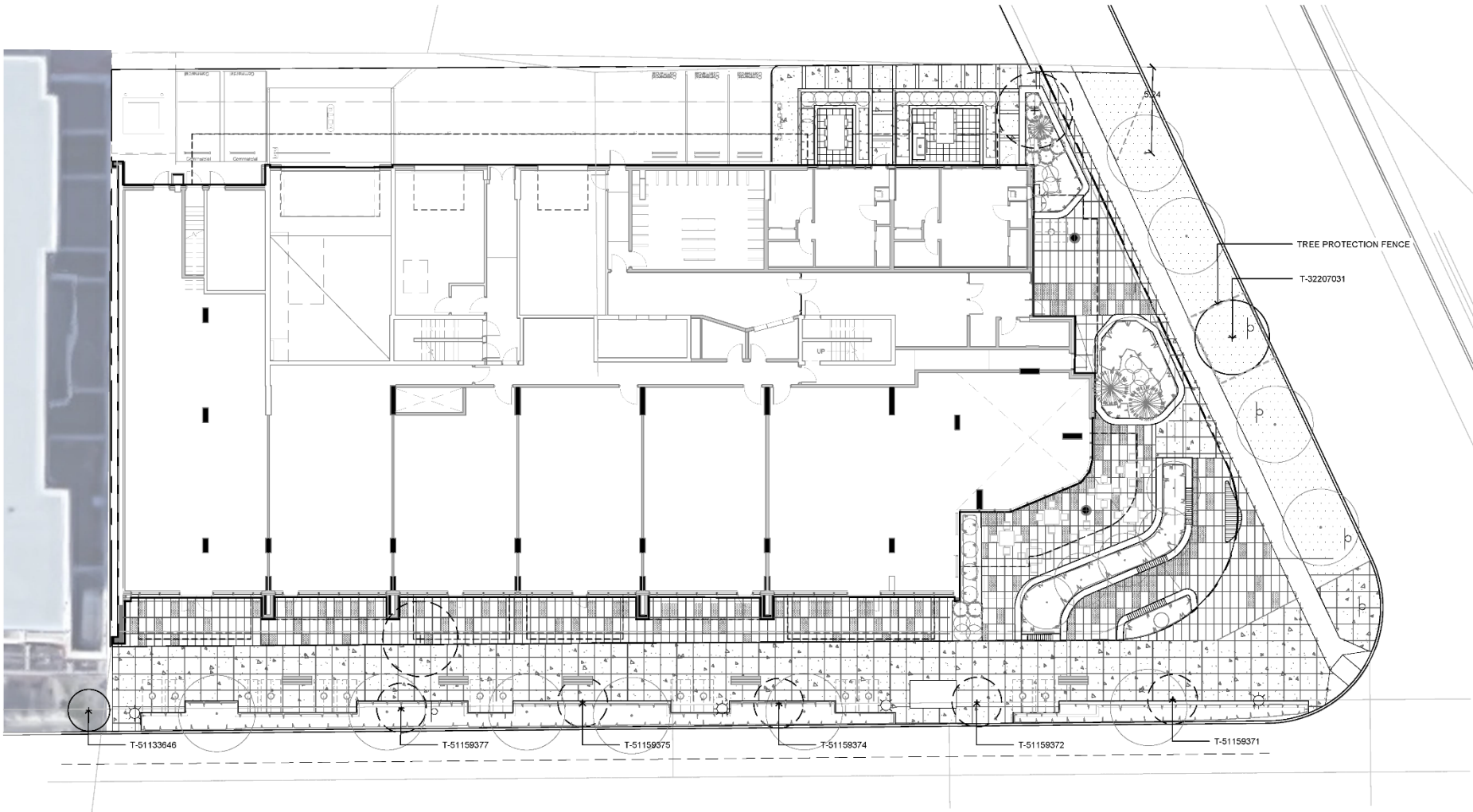
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Date: 2019-04-09
Project No: 116477
Scale: 1 : 200

L1.2

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LANDSCAPE LEGEND

- EXISTING TREE TO REMAIN
- EXISTING TREE TO BE REMOVED
- PROPOSED DECIDUOUS TREE
- PROPOSED SHRUBS/PERENNIALS
- PLAIN CONCRETE WITH 1.0 X1.0m SAWCUT JOINTS (PLAIN GREY/LIGHT SANDBLAST)
- DECORATIVE CONCRETE COLOURED (CHARCOAL/LIGHT SAND BLAST)
- BENCH(RELOCATED EXISTING MAGLIN BENCH TO MATCH KENSINGTON STREET FURNITURE STYLE)
- BOLLARD/BIKERACK
- TREE PROTECTION FENCE

NOTES:

TREE PROTECTION MEASURES AS PER CITY OF CALGARY TREE PLAN GUIDELINES. (SEE PAGE 62 CURRENT EDITION OF THE CITY OF CALGARY PARKS DEVELOPMENT GUIDELINES AND STANDARD SPECIFICATIONS LANDSCAPE CONSTRUCTION

TEMPORARY TREE PROTECTION FENCING OR BARRIER MATERIAL SHALL BE A BRIGHT OR CONTRASTING COLOUR AND BE DURABLE. FENCE POSTS SHALL BE COMPARABLE TO METAL T-POSTS OR HEAVIER. FENCE WILL BE MAINTAINED IN AN UPRIGHT POSITION THROUGHOUT THE DURATION OF THE PROJECT, AS PER CITY OF CALGARY PARKS TREE PROTECTION PLAN SPECIFICATIONS.

TREE PROTECTION FENCING TO REMAIN IN PLACE FOR DURATION OF SITE CONSTRUCTION

ALL TREES WITHIN 6.0m OF THE PROJECT BOUNDARY SHALL BE PROTECTED BY FENCING DESCRIBED IN THE NOTES ABOVE.

PRIOR TO CONSTRUCTION, AND AFTER TEMPORARY FENCE INSTALLATION, THE CONTRACTOR SHALL CONTACT CITY OF CALGARY URBAN FORESTRY AND ARRANGE AN ON SITE MEETING PRIOR TO COMMENCING CONSTRUCTION.

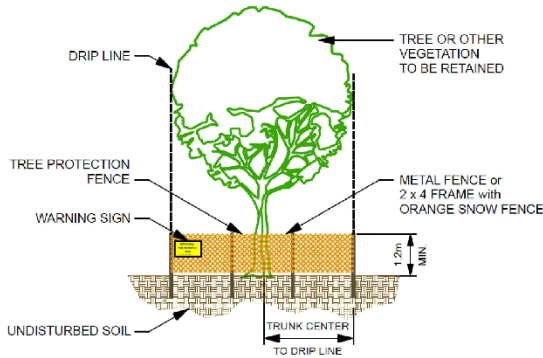
THE CONTRACTOR SHALL ADHERE TO ALL CITY OF CALGARY URBAN FORESTRY REQUIREMENTS AND APPROVALS PRIOR TO COMMENCING CONSTRUCTION. (SEE CITY OF CALGARY "TREE PROTECTION PLANS FOR CAPITAL PROJECTS" BROCHURE.)

NO STORAGE OF CONSTRUCTION MATERIALS SHALL OCCUR WITHIN FENCED TREE PROTECTION ZONES.

ALL PUBLIC TREES SHOULD BE PROTECTED AT THE TIME OF CONSTRUCTION AND TREE PROTECTION RULES WILL APPLY (IN A CASE IF NOT ALL THE BOULEVARD TREES ARE FAC'D AT THE TIME OF TPP APPROVAL)

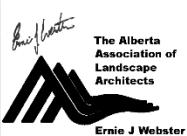
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T-51159371	American Elm	9	Remove
T-51159372	American Elm	8	Remove
T-51159374	American Elm	8	Remove
T-51159375	American Elm	8	Remove
T-51159377	American Elm	8	Remove
T-51133646	American Elm	8	Retain

1 TREE INVENTORY
L1.2 NTS



2 TREE PROTECTION FENCE DETAIL
L1.1 NTS

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TREE PROTECTION PLAN

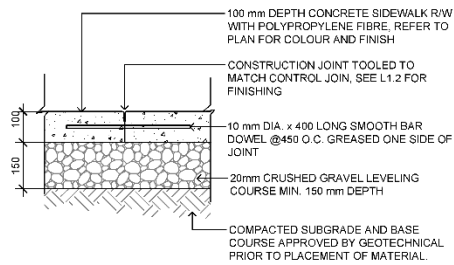
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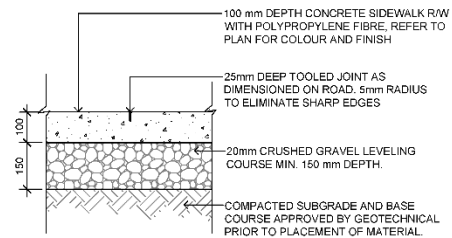
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Project No: 116477
Scale: 1 : 300

L1.3

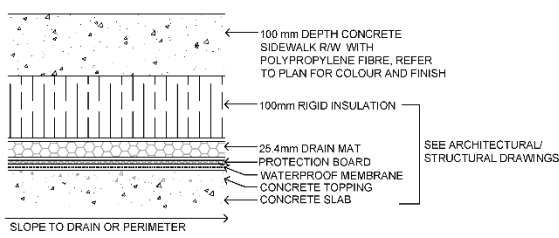
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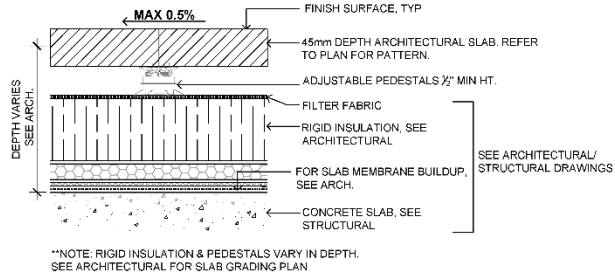
1 CONCRETE SIDEWALK CONSTRUCTION JOINT
L2.0 1:10



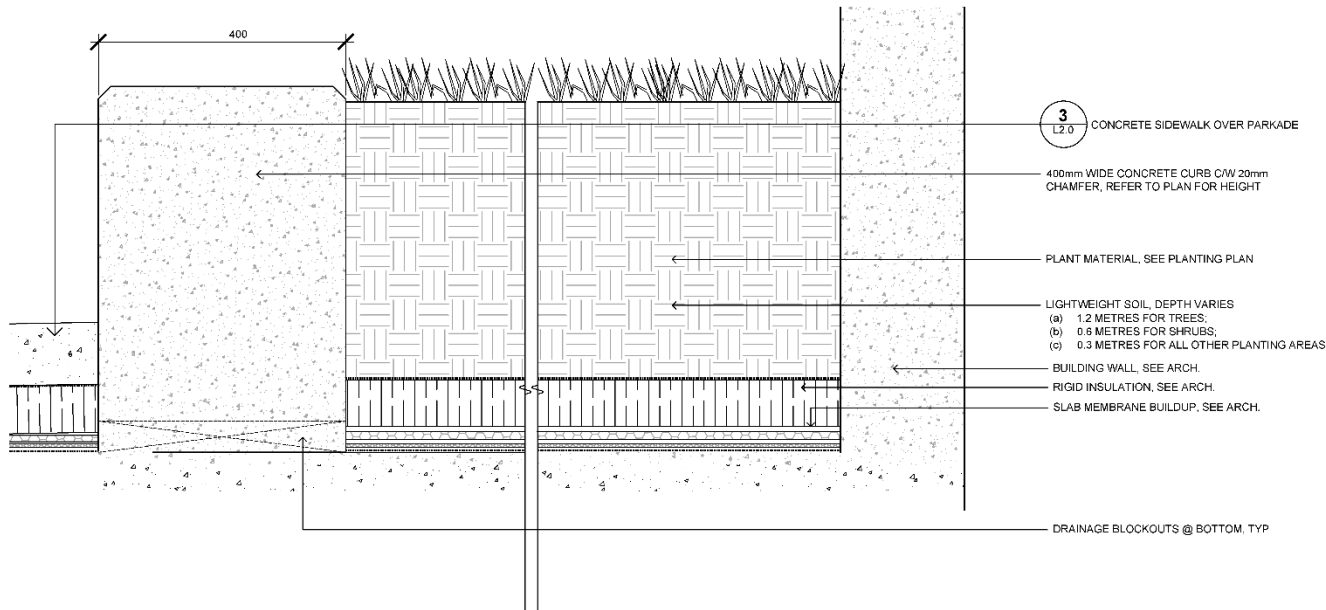
2 CONCRETE SIDEWALK CONTROL JOINT
L2.0 1:10



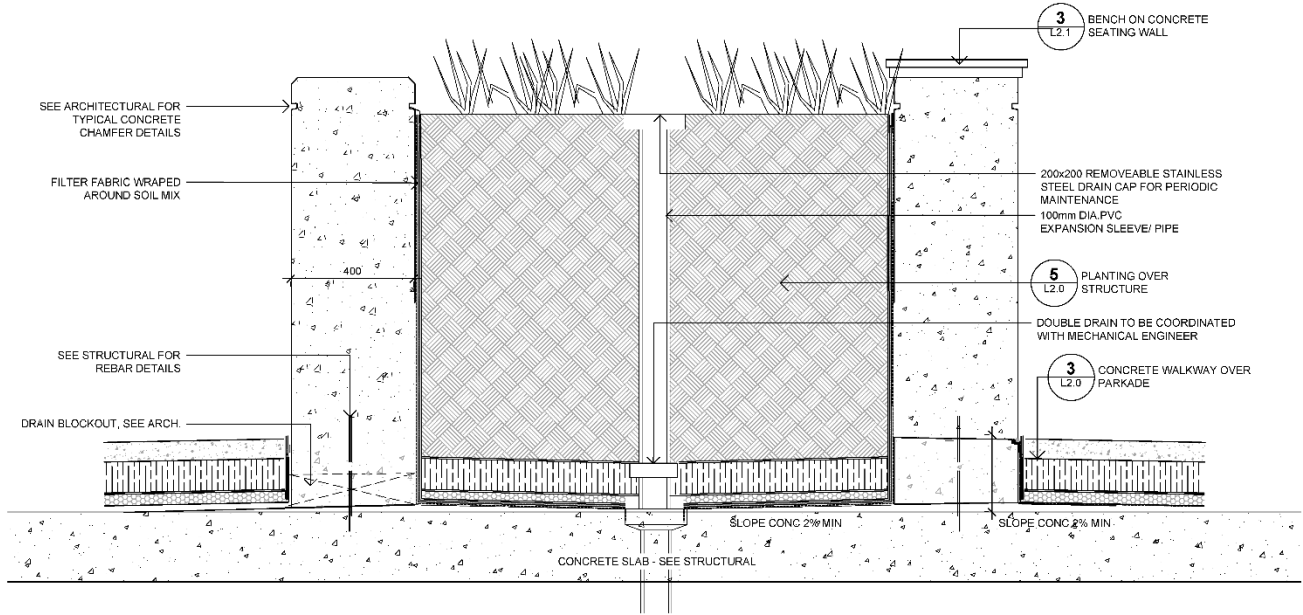
3 CONCRETE SIDEWALK OVER PARKADE
L2.0 1:5



4 CONCRETE PAVER OVER PARKADE
L2.0 1:5



5 PLANTING OVER STRUCTURE
L2.0 1:5



6 TREE PLANTER OVER PARKADE
L2.0 1:10

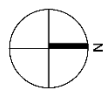


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LANDSCAPE DETAILS

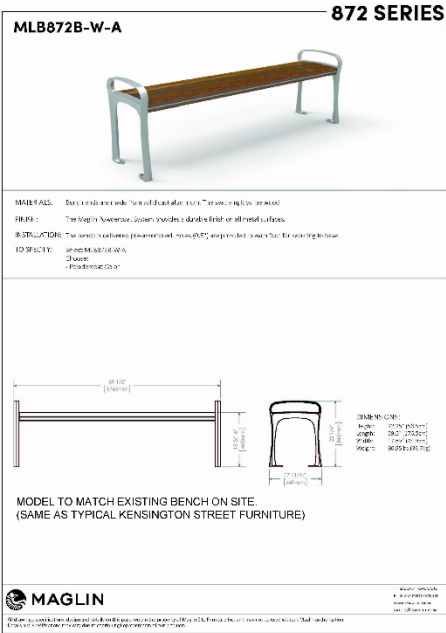
The Theodore
417 Tenth Street N.W., Calgary, Alberta



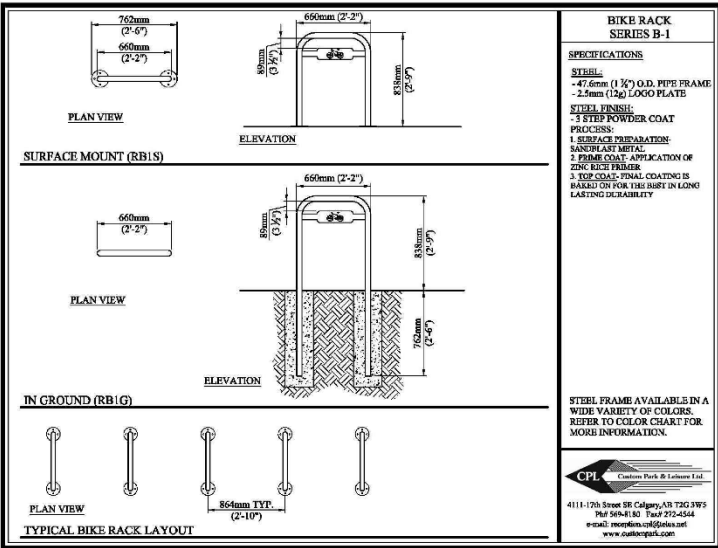
Issue: DTR2 SUBMISSION
Date: 2019-04-09
Project No: 116477
Scale: AS NOTED

L2.0

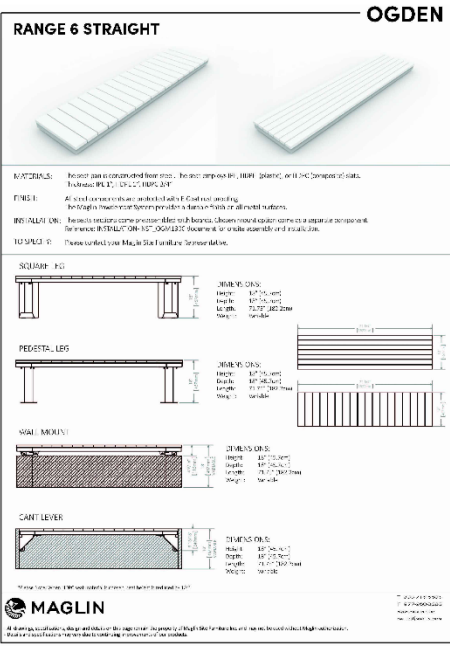
Development Permit Drawings



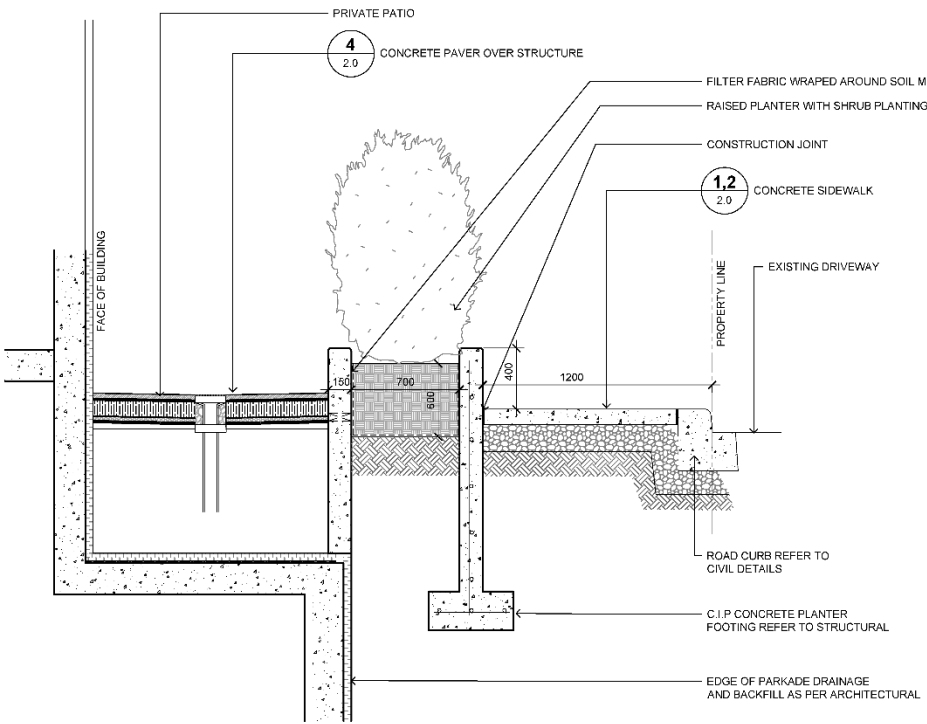
1 BENCH
L2.1 NTS



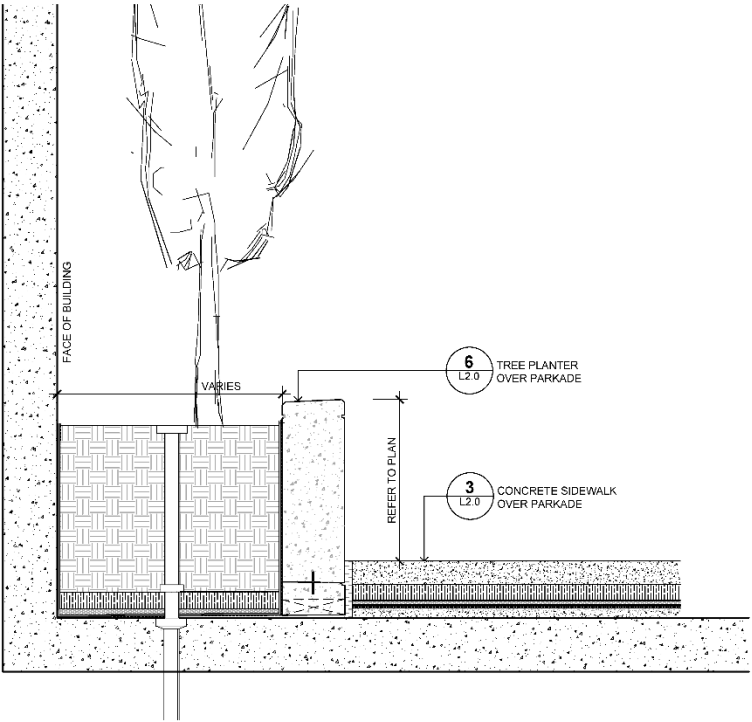
2 BIKERACK
L2.1 NTS



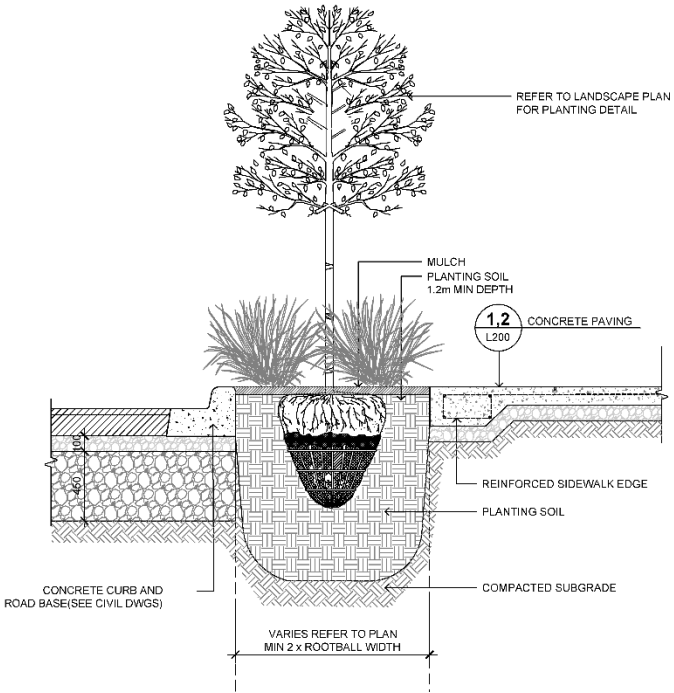
3 BENCH ON SEATING WALL
L2.1 NTS



4 PLANTER BY PATIO AND SIDEWALK
L2.1 1:20

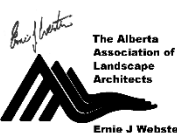


5 PLANTER OVER PARKADE AT BUILDING
L2.1 1:20



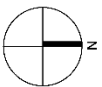
5 PLANTING IN SOIL TRENCH
L2.1 1:20

IBI GROUP
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Calgary, AB T2E 2W5 Canada
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LANDSCAPE DETAILS

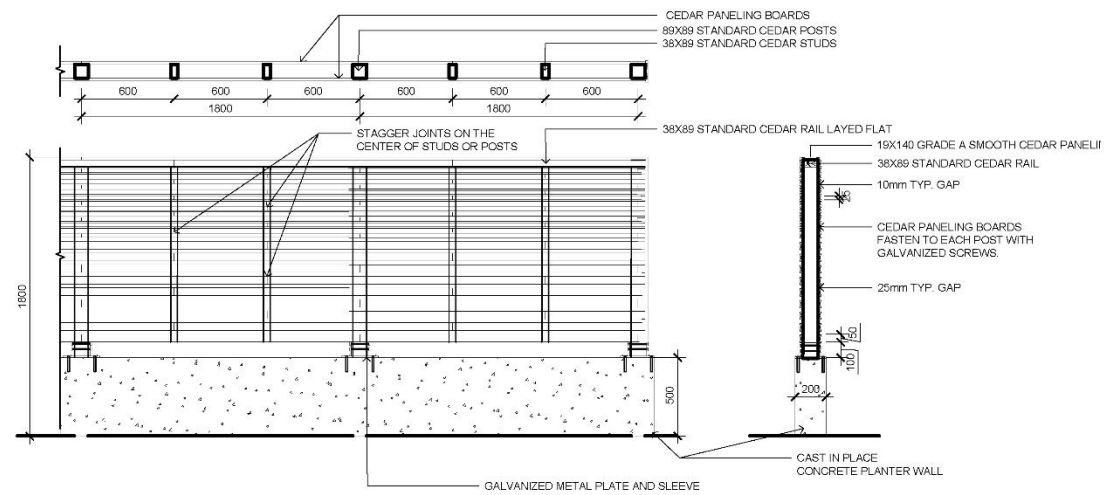
The Theodore
417 Tenth Street N.W., Calgary, Alberta



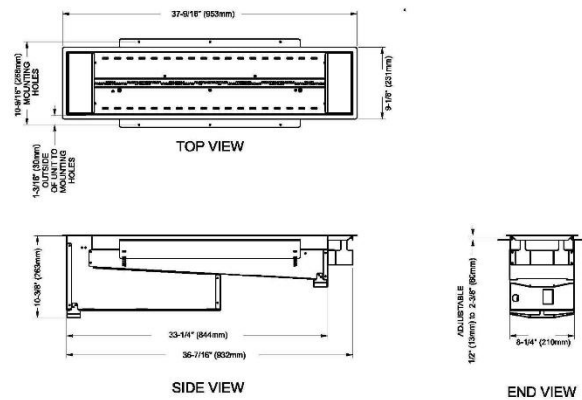
Issue: DTR2 SUBMISSION
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L2.1

Development Permit Drawings



1 SCREEN FENCE
L2.2 NTS



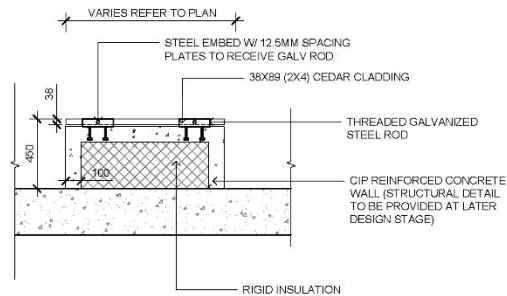
2 OUTDOOR FIRE TABLE
L2.2 NTS

MODEL PT030 OUTDOOR DECORATIVE FIREPLACE
by REGENCY FIREPLACE PRODUCTS

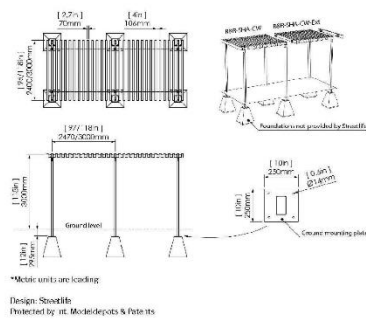


PLATFORM SEAT TO BE CUSTOMIZED TO FIT SITE.
(IMAGE IS REPRESENTATION OF THE CONCEPT)

3 FEATURE SEAT/PLATFORM
L2.2 NTS



4 TRELLIS
L2.2 NTS



MODEL LOUVRE by CABANA COAST
DETAILED SHOP DRAWING TO BE APPROVED BY LANDSCAPE ARCHITECT PRIOR TO INSTALLATION

5 OUTDOOR SOFA SET
L2.2 NTS



Applicants Submission

Situated on the corner of 10th Street NW and Gladstone Road, the Theodore is a new 10 storey development that looks to infuse the traditions of the Kensington vernacular through a modern translation of material and form to create a gateway building and plaza from the north.

Starting from the North, the public is immediately presented with an animated, prominent public "gateway" plaza. This is anchored by a focal point feature of a semi-public art or theme-works piece. The nature and character of the focal point will be visually striking and reflect the eclectic and creative character of the Kensington neighborhoods. This public space is activated with curving rows of concrete and wood seating benches for the public, visitors to the buildings, residents and employees to mingle, rest and interact. This place presents pleasant and ambient spaces to stop and view art, interact or find a quiet contemplative space to sit.

A spine of vegetation and medium sized Ash or Linden trees with permeable foliage provides shade over the seats as well as green space. This spine creates a subtle separation for the public should an outdoor café be included to public plaza. The plaza provides many opportunities for people in the community just passing or stopping near the street. A unique customized wooden seat/lounger provides visual interest and an ad hoc "stage" for any small gatherings that might occur.

Plaza paving materials articulate the public and private space and extend into the public sidewalk to highlight seating areas and entrances, making the development feel integral to the overall public realm. Paving will be concrete - a mix between a grey saw cut (NO tooled joints) sidewalk and decorative paving areas with smaller joint spacing and light sand blasting. This creates a variety of colour and texture while still providing a cohesive and accessible overall treatment of the plaza's hard surfaces. The café/retail area at front of the building under the canopy will also be paved with a surface that differentiates this area from the pathway to the building entrances.

Winter interest will include twinkle/festive season lighting on each of the 5 trees in the plaza (similar to other trees in the Kensington neighborhoods along with up lighting of the trees adjacent to concrete seating walls. These walls can be LED/coloured and add warmth and seasonal interest. Mass plantings of Feather Reed Grass and Lyme Grass also creates winter interest as the pedestrian travels south along 10th Street.

The massing for the podium and tower of the building are based on the two and three storey vernacular of Kensington combined with a light feeling modern tower. Lines are taken from a series of buildings like 19 Carscallen Block II and the adjoining 4 retail Blocks to the south and 120 10th Street (Trend Fashions) beside it to the north. The idea of a masonry portal taken from these buildings forms the precedent for the façades for CRU 2 and CRU 5. The cornice line and floor lines are expressed in the detailing of the 4 inch brick over and under a course of 8 inch brick. The size of brick is also a reflection of the Lido coursing. The use of large symmetrical plate glass storefront is also incorporated into the facades of CRU 1 3 4 and 6 as a reflection of the goal that these Edwardian commercial style buildings wished to achieve more than 100 years ago.

We have incorporated a similar approach used on the 13th Ave SW Greenway - where Blue Lyme Grass is planted to soften the edge. Too many buildings in Kensington with frontage onto 10th St do not have soft edges and space for trees and planting. The generous building setback and the adjacent bike lane allows the frontage here to be different. We are able to include planting in the public realm rather than squeeze planting into the building frontage where it's difficult to grow under a building canopy. Elm Street tree planting has been reworked with better

Applicants Submission

spacing to better align with design elements. The strip of Blue Lyme Grass will allow for a continuous soil trench and better tree establishment, better than the existing condition. The existing Kensington "standard" Maglin benches can be recycled and moved from their existing positions facing out towards a busy 10th Street and positioned within the Blue Lyme Grass strip facing inwards along with the other furniture thus framing urban spaces.

Paving materials define public and private space and extend into the public sidewalk to highlight seating areas and entrances making the development feel integral to the overall public realm.

Cantilevered canopies are hung below the decks above and are clad with a wood veneer soffit to warm the streetscape and provide a unique space from which to hang tenant signage. These canopies form a semi-public space that can be used by retailers to display merchandise or product or be used as limited seating areas for café or coffee shops to activate the public realm.

The second level decks of the residences are expressed in relationship to the CRU units below in brick or frosted glass. The added 1.8m set back to 10th Street and the landscape boulevard allow a connection to the pedestrian level below while being guarded against street traffic with the landscaped buffer.

The corner commercial unit is designed to relate and provide spatial transition to a semi-public plaza to the corner of 10th Street and Gladstone Road. The glass at the CRU level is a storey and a half tall in height and provides for ceiling that flows out in the same plane as the exterior canopy allowing for inside and outside spaces to blend together. Following the line of canopy above are landscape planters that define the exterior edge of the semi-public space thought of as an outdoor café.

Above the canopy is the jewel of the project where highly detailed glass and white spandrel flow up from the third floor to the top of level 10, This free curved glass element expresses a strong vertical element on the corner as it directly addresses the corner of 10th Street and Gladstone Road. It is flanked by a masonry vertical element to the west and by the typical white metal panel and glass façade to the south.

The residential floors from 3 to 10 are stepped back from the podium level on 10th Street and on the lane. The tower itself is comprised of durable and long lasting metal panel for the bedroom areas and window wall for the living areas. The west and east façade incorporates this metal to create portals at the higher levels to reflect the ideas at the podium level and encapsulate the window wall living areas. Decks are recessed in between the living areas to create a depth in the tower breaking up the mass with four "postage stamps" inserts. These set back exterior areas are large enough to create conversation areas for residences. The larger white metal panel from the 3rd to 10 floors creates a single frame element facing 10th Street. The window wall uses clear glass and aluminium mullions and metal panel are white to allow the building to pick up the colour of the light from Calgary's blue skies or the flashes of red from our spectacular sunrises and sunsets.

The raised planter on the NE corner of the building creates a subtle barrier between public realm and the residential entrance. The entrance to the residences is located off Gladstone Road on the north side of the building. A strong horizontal metal panel canopy addresses the street and provides protection from the elements. Two masonry walls flank the residential entry with the west wall providing space for the Theodore name and address branding. Planters also align both sides of the walk from Gladstone to the front doors. The design is asymmetrical but remains a well-balanced composition scaled to the residential users. A change in paving

Applicants Submission

texture/material also reinforces the start of the private residential entry off Gladstone.

Two laneway flats are provided to the north west of the site elevated slightly above grade. These two units are visually connected to the second level residences with the same floor to ceiling glass system that bypasses the second floor slab on each side of the cantilevered decks. This area will not feel like a back lane/typical servicing driveway but rather a residential frontage that animates the lane. Privacy screens are provided with larger planters that enclose a private patio for each unit, these screens can also serve as trellises for planting to create a "green wall". This will mitigate views of the servicing/waste and recycling and loading areas. The patios are scaled to allow for conversation seating. Additional Elm trees will be planted along the existing grass boulevard in order to increase tree cover along the edge of the street.

Lastly, atop the south end floor is a roof top patio. A modest sized area the roof top patio provides amenity space for the residents and provides outdoor space for the 2nd floor suites. A sectional type seating area around a natural gas table top fire place is provided along with places around a high top counter and two dining areas. The roof top patio is setback from the building edges and will maintain views to the city centre for all residences.

Applicant Response to UDRP



THE THEODORE

UDRP REPORT

#114782 | MARCH 2019



Applicant Response to UDRP



Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL



The design of the Theodore has been revised and clarified to resolve issues and observations raised by the Urban Design Review Panel. The following items address each item raised by the UDRP.

The design of the urban realm on 10th Street is intentionally turned inward toward the site vs to the street. Within this realm we have incorporated several elements that will enhance the pedestrian and retail experience and create a sense of place from street to building face.

These include:

i. Increased Retail Window Frontage.



In a pre-application meeting held with the City Representatives, we reviewed an overall vision for the project. One element noted was to a preference to develop a more modern look to the retail frontage vs a copy of the typical historic masonry elements. Our approach reinterpreted the historic approach of the older facades in the area and applies more open, visible storefronts broken into 7m +/- modules as outlined in the ARP.



We developed a 2 storey frontage consistent with the Kensington podium line, broken up by masonry and vertical spandrel elements. The 2 storey scheme incorporates a 4.2m high retail frontage at grade and a 3.0m residential unit frontage above included in a single plane window wall element. The façade is broken up with fully glazed projecting balconies serving as amenity for the individual residential units as well as weather protection for the storefronts below. There are 2 masonry clad retail entry elements providing a texture storefront alignment. The entire 2 storey element is framed by a masonry border. In essence we looked to maximize storefront glazing within a masonry portal similar to "The Bridges" and Carscallen Block 2 buildings down 10th street.



The resultant architecture provides a transparent, active, single plane frontage that is broken up with individual projecting elements.

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Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL

ii. Masonry Entry Accents

To keep with the vernacular of the Kensington area we have included a masonry element to the urban frontage. As noted above, we developed an open architecture using increased glazing with less masonry and punched windows. We have had input from others that suggested a deep, pronounced masonry bay similar to others in East Village was found to be successful. We have introduced 2 - 1.0m deep portals which provide to bracket 2 retail units providing an animated frontage, opportunities to allow for retail display in exterior realm or opportunities for small seating areas associated with the retail user.

iii. Balcony / People on the Street

We have provided balconies with separations for the residential units located on the 2nd floor which project onto the street. In pleasant times this will give the opportunity to have people, activity on the street on a second level increasing the human interaction in the area.

iv. Differing Signage Opportunities

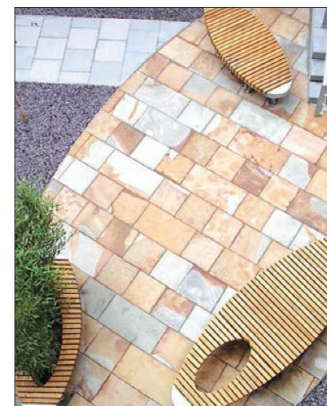
We have provided opportunities for various signage to be applied to the frontage. We have provided sign bars from which signs can be hung from the balcony soffits in a 3-dimensional form vs flat panels. We have provided opportunities for either word based or graphic form signage to the masonry portals. The retailer also has the opportunity to provide window signage within the ample window areas provided.

v. Sidewalk Material Changes

The retail frontage is set back from the property line by approximately 1.8 - 2.0m. Within this space we are proposing to provide an interesting coloured patchwork of sidewalk finish to animate the urban area in front of the individual units. The idea is to create public spaces where retailer have the opportunity to engage the public realm with in "pocket areas".

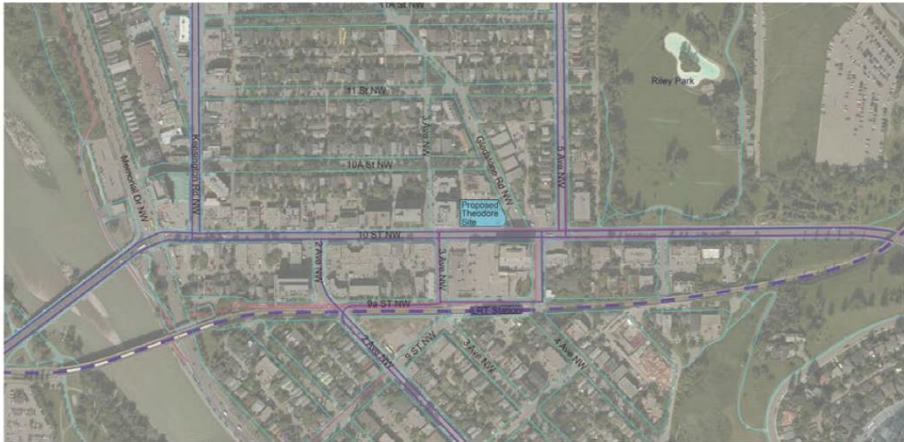
vi. Weather Protection

We have a variety of continuous weather protection elements designed within the retail frontage. These include the storefronts under the residential balconies and recessed storefronts within the masonry entry elements. The soffits of these elements will be finished in warm wood like materials.



Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL



TRANSPORTATION NETWORK STUDY



ZONING LAND USES

Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL



We have developed and improved the interface of the development and Gladstone. We have added 4 street trees within the boulevard. It has been recommended by the City to provide simple grass in the boulevard.

Within the site we have developed a porous environment where the public may travel across the corner.

We have developed a distinct realm for the Residential Entry ¹ and Public Space. ²

We have provided an extension of the developed area to include the exposed lane corner. ³ Here we have provided 2 living units at grade which include well landscaped patio areas taking advantage of the street and lot geometries.

In the development of the laneway units ⁴ we had considered the potential future development of the adjacent property. Given the geometry of the lots and the required building setbacks we have found that the 1st unit off Gladstone will be fully exposed and dependant on the final design of the adjacent lot, the 2nd units will have a minimum of 50% exposure to view. The units are set back 6m off the lane to provide separation both from vehicular traffic and also the adjacent building development and to provide a generous outdoor area for the residences to use.

The corner of Gladstone ⁵ and 10th is special in that our proposed plaza development and the plaza development associated with the Hillhurst Baptist Church create a lively public corridor. We are limited in how the two can relate other than being spaces which provide opportunity for pedestrian enjoyment. The two cannot be linked with mid-block crossings. Further development consideration should be applied to the angled south corner of the gas/convenience site to the north to create a comprehensive open space development at the confluence of these 3 corners.

It was suggested that modifications to Gladstone Road be made such as narrowing the carriageway. The City may review this internally and discuss these offsite changes as a general improvement for the site built we do not find these directly applicable to the development proposed.



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Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL



i. West Elevation

Similar to the retail frontage we have integrated a single plane glazed façade to the lane units facing west. This provides fully glazed frontages for both the ground and second floor residential units. The 2 storey glazed element is punctuated by projecting glazed balconies. These elements bring animation to the lane frontage.

We have anchored the 2 storey base element through the use of a masonry frame. Together with the fenestration of the vision panels the two floors are liked as one expression of the masonry residential units for the NW corner.

We have also introduced a slot window extending from grade to the 3rd floor integrating the lane and second floor units to Gladstone road.



ii. Patios

The two patios are well landscaped with low conifers to maintain year round greening and colourful hardy annuals maintaining a strong natural environment throughout the seasons. There are taller trees located on the south unit patio to provide a natural screening to the adjacent lane services. Screening, similar to the wood fence shown on the left, will also assist in separating the lane patios from the service areas.

The sidewalk is extended to provide an integrated access feel to the units, versus entering directly off the lane paving. This provides these units with an address.



iii. Relationship to Building

The treatment of the lane architecture is similar to that provided an 10th Street. We consciously did not provide a toned down lane architecture. We have integrated the 2 storey glazing and projecting balconies.

We have integrated the two end units to Gladstone by providing a glazed window slot providing views from the Living Room areas. This integration ties the elevational treatment around the corner.

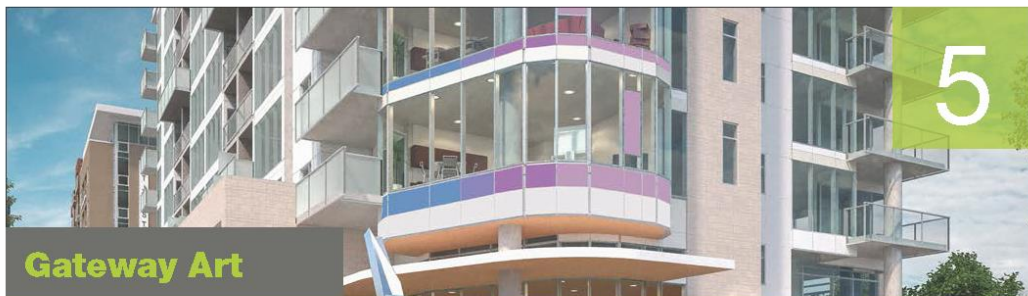
iv. Rooftop Patio

A patio is also proposed for the roof to provide common area amenity space to host larger groups or community functions for the building.



Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL



We are providing an opportunity for public / semi-public art in the plaza development at the north end of the site. This is currently proposed as part of the project for a portion of the bonusing strategy.



*Oval with Points by Henry Moore
Photo credit: Jynto, at the Henry Moore Foundation*



*Sculpture at Borden Park by Peter Hide
Photo credit: Edmonton Journal*

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Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL

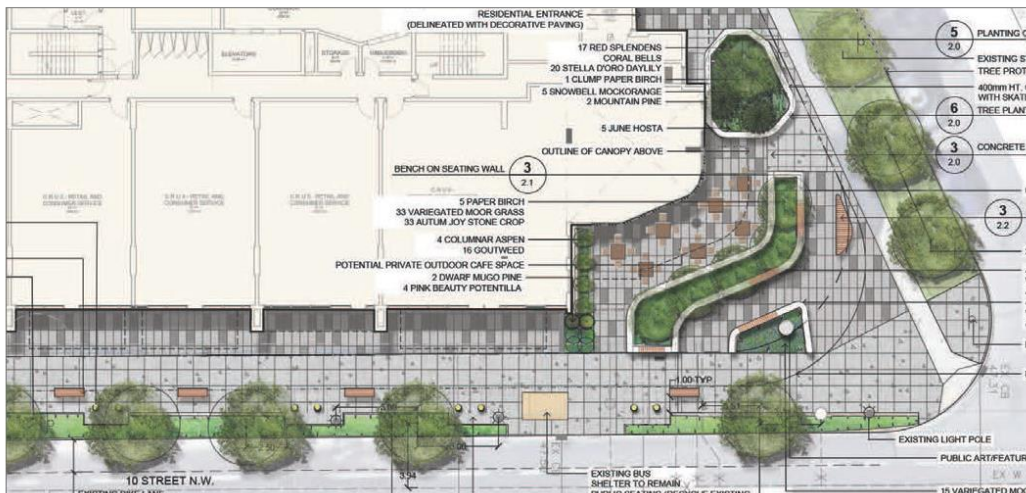


We have re-developed the north Plaza area with varied animation and opportunities. Learning from other areas within Kensington we developed smaller gathering and seating opportunities vs an open square. The landscape design provides a meandering path connecting Gladstone to 10th Street. Along the path we have provided individual seat walls connected to the vegetation. We have a larger bench seat/platform. We have added street benches looking into the plaza area from the street edge and a fun lounge type bench for a unique seating arrangement.

Adjacent to the building we have developed two schemes, one allowing for a seated patio development associated with a restaurant, should that spaced be leased for that use, the second with a development allowing public access for a standard commercial unit. Both provide proper usage and animation, neither diminish the public use of the plaza.

We have continued the varied paving patterns used along the 10th Street frontage throughout the plaza area including the public realm.

We have provided an opportunity for the inclusion of public art in the most northern planting bed. This planting bed includes seating and a bleeding of the natural landscape into the sidewalk without a raised curb.



Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL



Within the overall project we will be collecting the majority of the stormwater from the roofs and balconies. This is collected and released as required. We are however proposing to drain the 10th Street runoff into the street edge planting, providing natural irrigation. It has been found in other areas of the city that the proposed Blue Lyme Grass planting thrives in this type of condition.

We are proposing to add 4 new Patmore Ash, a large canopied deciduous tree to Gladstone and replacing the 5 poor condition Elm trees on 10th with 7 Prairie Spire Green Ash, a large canopied conical shaped deciduous tree. These new trees, the Blue Lyme Grass and new plaza landscape will create an enhanced park like environment as the entry to Kensington.



With the revised plaza design and proposed gate way art piece the development looks to anchor the corner of 10th Street and Gladstone road as the welcoming pedestrian space for 10th street north.

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Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL

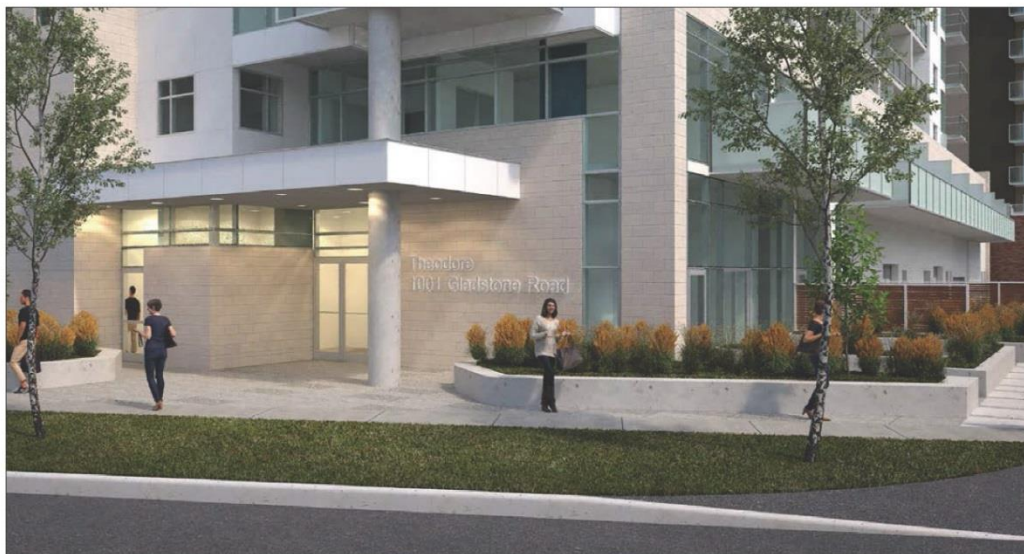


We are providing a distinct break between the Residential Entry and the Plaza to provide separation and security to the residents through the use of well landscaped planting bed including coniferous and deciduous trees. We have intentionally located the entry to the residence off Gladstone, a residential street vs 10th which is primarily commercial.

The front door is protected by a large canopy area. The canopy provides a distinct horizontal element in contrast to the vertical aspects of the north elevation.

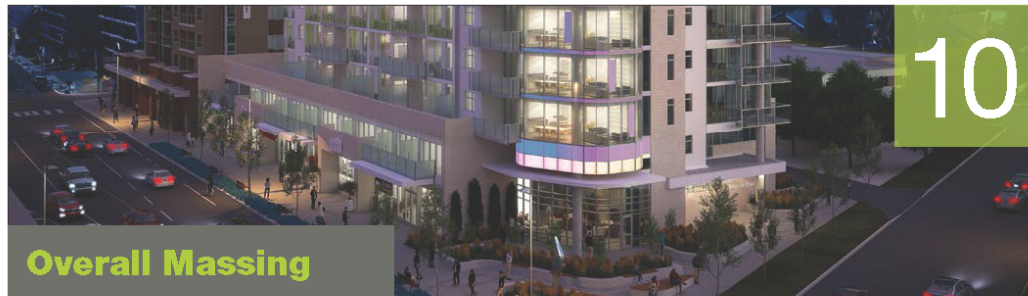
The canopy will be lit on the underside indicating its location. There is an opportunity for a distinctive addressing element on the wall adjacent to the entry doors.

We have incorporated textured and coloured paving to indicate the front door and access.



Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL



We have developed the design of the project to reflect the many critical aspects of the development.

For the project to be viable it needs to be efficiently designed in terms of planning, shape, construction cost and maximizing of the FAR allowed. This is also coupled with the "gateway" aspect envisioned by the neighbourhood and Planning, the odd site with the triangle corner and lane access and how the project relates to the neighbouring site.

i. Urban Streetfront

We have developed a typical 2 storey massing to reflect the Kensington vernacular. This continues as an extended base on the 10th Street and Lane aspects. This is reinforced by the 2 storey glass elements framed by a masonry columns and bands. The massing is animated by projecting elements of balconies and storefront portals.

ii. Gateway Element

We have developed a projecting jewel element on the north corner of the project to accentuate the prominence of the site and Kensington. This element was designed to be somewhat atypical. We refrained from a simple circular column with a "hat" as in the Kensington Pointe and developed an offset ovate plan. This is distinct from any other in the city.

The gem is suspended above the grade at the 3rd floor respecting and providing an antithesis to the extended 2 storey massing on the rest of the building. The jewel is fully glazed to provide a shine, animation and light on the corner. Care has been taken to detail the jewel piece with floor to ceiling vision panel enhanced by and minimal mullion system. The glazing within this element will be without mullion caps to reinforce the smooth surface of the jewel.

We have included a canopy element at the 3rd floor level that both provides weather protection to the uses below but also provides a strong termination and to the design element. This canopy will be lit to provide more strength to the element in the evenings.

We are providing accent glazing panels of dichoric glass. This is a glass type which has a film providing a colour change as one moves around the panel. The glazing changes colour from hues of amber to rose. This glazing will be located on the spandrel panels covering the floor levels and randomly up the verticals of the jewel. This will create an ever changing colour accent as one moves around the site.

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Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL



iii. North Elevation

We have developed this elevation with strong slender vertical elements of masonry with punched windows, balconies and a single round column to emphasize the additional height allowed for this site, to emphasize the thinner aspects of the massing and to provide an animated face to the north.

iv. East Elevation

We have developed a simple typical plan for the tower which maximizes the potential for development. We have broken the elevation down in a graphic form to minimize the layer cake aspect of repetitive floors. We have broken the massing of this elevation (and similarly the west) into vertical end elements securing an 8 storey metal panel frame. Within the frame we have inset balconies to provide simple shadowed elements.



These portal elements are detailed to extend past the wall line to produce a shadow line but still reinforces the "portal form that we see on the retail units at grade. We have broken the shadowed elements by projecting 3 levels of balconies creating "postage stamps" within the frame.

v. Colour and Massing

Current styles of architecture utilize dark shades, patchwork colour patterns, faux historic big city massing and colour for colour sake. It is our belief that a simple presentation of elements is a much more successful approach to architecture. We have provided a simple organization of massing with a few specific accent elements. We have presented a lighter colour selection to brighten the project and not darken the environment. We use natural shadowing to accentuate recesses and forms.



We have chosen materials which will use natural / changing elements in development of colour. The metal panes proposed are iridescent. These will pick up shades of colour from the bright sky, from sun rises and sets and changing cloud patterns. The façade is designed with Calgary in mind where the sky touches our feet. We are proposing a relatively light tint in the window glazing to allow for interior colours and lighting to animate the façade.

Applicant Response to UDRP

IBI GROUP REPORT TO URBAN DESIGN REVIEW PANEL

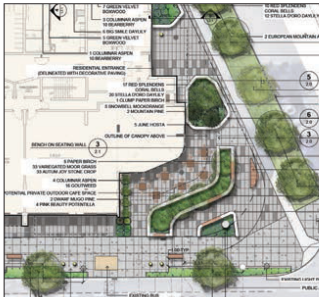


vi. Roof Top

We have reduced the roof top mechanical penthouse and have it set back as far as possible to reduce its appearance and influence on the massing.

vii. Additional Plaza / Building Development

It was suggested that we explore further building development into the north plaza corner. As the projects stands today, we are requesting a reduction in the required landscape area for the site. We cannot extend the footprint of the building any further. We have reserved the majority of the open space to the well landscaped public realms off 10th and Gladstone.



In all we feel we have developed a fine architecture presenting simple elements, massing and colour not relying on unrelated colour for colour, overly detailed or inconsequential elements for form or bold statements for statement sake.

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Direct Control Bylaw 152D2018

BYLAW NUMBER 152D2018
BEING A BYLAW OF THE CITY OF CALGARY
TO AMEND THE LAND USE BYLAW 1P2007
(LAND USE AMENDMENT
LOC2017-0393/CPC2018-0297)

WHEREAS it is desirable to amend the Land Use Bylaw Number 1P2007 to change the land use designation of certain lands within the City of Calgary;

AND WHEREAS Council has held a public hearing as required by Section 692 of the Municipal Government Act, R.S.A. 2000, c.M-26 as amended;

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".
2. This Bylaw comes into force on the date it is passed.

READ A FIRST TIME ON

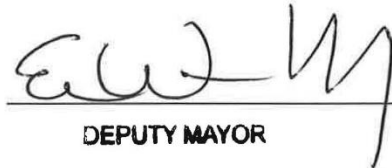
MAY 07 2018

READ A SECOND TIME ON

MAY 07 2018

READ A THIRD TIME ON

MAY 07 2018


DEPUTY MAYOR

SIGNED ON MAY 07 2018


ACTING CITY CLERK

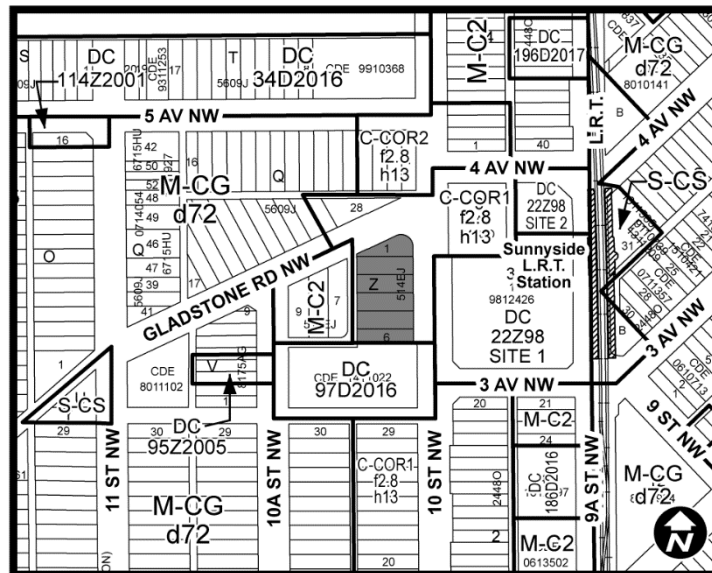
MAY 07 2018

SIGNED ON

Direct Control Bylaw 152D2018

AMENDMENT LOC2017-0393/CPC2018-0297
BYLAW NUMBER 152D2018

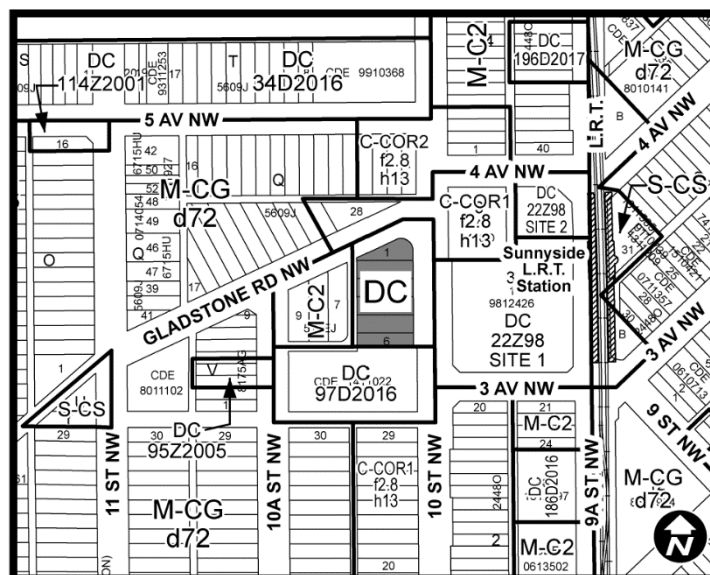
SCHEDULE A



Direct Control Bylaw 152D2018

AMENDMENT LOC2017-0393/CPC2018-0297
BYLAW NUMBER 152D2018

SCHEDULE B



DC DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control District is intended to:

- (a) provide for medium **density** mid-rise mixed-use **development** in compliance with the policies of the applicable local area redevelopment plan; and
- (b) implement the **density** bonus provisions of the Hillhurst/Sunnyside Area Redevelopment Plan.

Compliance with Bylaw 1P2007

2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Direct Control Bylaw 152D2018

AMENDMENT LOC2017-0393/CPC2018-0297 BYLAW NUMBER 152D2018

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The **permitted uses** of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Discretionary Uses

- 5 The **discretionary uses** of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District.

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 apply in this Direct Control District.

Floor Area Ratio

- 7 (1) Unless otherwise referenced in subsection (2), the maximum **floor area ratio** is 2.8.
- (2) The maximum **floor area ratio** may be increased to 5.0 in accordance with the **density** bonus provisions contained in section 8 of this Direct Control District.

Density Bonus

- 8 (1) For the purposes of this section: "Cash Contribution Rate" means: \$17.85 per square metre for the year 2018. The Cash Contribution Rate will be adjusted annually on January 1 by the **Development Authority**, based on the Statistics Canada Consumer Price Index for Calgary.

- (2) A **density** bonus may be earned by a contribution to the Hillhurst/Sunnyside Community Amenity Fund, such that:

Cash Contribution Amount = Cash Contribution Rate x Total floor area in square metres above the **floor area ratio** of 2.8.

- (3) A **density** bonus may be earned by the provision of an off-site improvement in accordance with Section 3.1.5.4 of the Hillhurst/Sunnyside Area Redevelopment Plan, where the allowable bonus floor area in square metres is equal to the cost of construction of the off-site improvement divided by the Cash Contribution Rate, such that:

Allowable bonus floor area = Total construction cost of the off-site improvement / Cash Contribution Rate.

Total construction cost will not include any construction costs necessary to fulfill the infrastructure requirements of a **development permit** for a **development** equal to or less than a **floor area ratio** of 2.8. Details of the construction cost will be determined through the **development permit** process.

Direct Control Bylaw 152D2018

AMENDMENT LOC2017-0393/CPC2018-0297 BYLAW NUMBER 152D2018

Building Height

9 The maximum *building height* is 32.0 metres.

Location of Uses Within Buildings

10 The following *uses* must not be located on the ground floor of a *building* facing the commercial *street*:

- (a) **Addiction Treatment;**
- (b) **Assisted Living;**
- (c) **Catering Service – Minor;**
- (d) **Counselling Service;**
- (e) **Custodial Care;**
- (f) **Dwelling Unit;**
- (g) **Office;**
- (h) **Medical Clinic;**
- (i) **Medical Marihuana Counselling;**
- (j) **Payday Loan;**
- (k) **Place of Worship – Medium;**
- (l) **Place of Worship – Small; and**
- (m) **Residential Care.**

Development Authority – Powers and Duties

11 The *Development Authority* may relax the rule of section 9 of this Direct Control District provided the test for relaxation as set out in Part 2 of Bylaw 1P2007 is met.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2018-1012

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

EXECUTIVE SUMMARY

This development permit application was submitted by NORR Architects Engineers Planners Ltd on 2017 September 06, on behalf of Cidex (Elbow River) Developments Ltd and Albari Holdings Ltd. The application proposes a multi-residential development with a total of 1,252 dwelling units, including a podium and three towers with retail, consumer service and restaurant uses on the main level. The development includes bonus density earning items consisting of publicly accessible private open space adjacent to the Elbow River, as well as 22 affordable housing units. This development will be the first in Calgary to implement a bonus earning item for the provision of affordable housing units.

The development is also notable for:

- having one of the largest dwelling unit counts for a multi-residential development under a single development permit in Calgary;
- a unique podium design of nine levels and 36 metres in height, to accommodate parkade levels above grade to mitigate for flood protection;
- significant improvements to the riverbank for the protection from erosion and infrastructure including the proposed amenity pathway;
- a south facing plaza that takes advantage of the solar gain, Elbow River frontage and regional pathway access, providing excellent activation of the river and pathway frontage; and
- providing extensive areas of outdoor amenity space on the rooftop levels of the podium including a running track, dog park, outdoor seating, outdoor exercise and yoga corner and a barbeque area.

The proposal is consistent with the applicable policies including those of the *Municipal Development Plan*, the *Beltline Area Redevelopment Plan* and the *Centre City Plan*. The proposal aligns with the intent of Land Use Bylaw 1P2007, including the rules under the Centre City Mixed Use District (CC-X), and Administration supports the relaxations noted within the report.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission **APPROVE** the proposed Development Permit application DP2017-4075 of a New: Multi-Residential Development, Retail and Consumer Service at 1818 – 1 Street SE, 1825 Park Road SE and 1919 MacLeod Trail SE (Plan 8311892, Block 2, Lot 26; Plan 8311893, Block 2, Lot 27; Plan 8210888, Block 2, Lot 24), with conditions (Attachment 2).

PREVIOUS COUNCIL DIRECTION / POLICY

None.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2018-1012

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

BACKGROUND

The subject site was the original location of the old Elbow River Casino until the casino was demolished and relocated to a new building one block to the north in 2005-2006. The site has operated as a surface parking lot since that time.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Site Context

The subject site is a gateway to the Centre City and is located in the Beltline community, extending an entire block from 1 Street SE to MacLeod Trail SE. Designated as Centre City Mixed Use District (CC-X), the site is bordered on the south side by the Elbow River and on the north side by a surface parking lot at the northwest corner and Park Road SE along the remainder of the northern boundary.

Nearby and adjacent amenities include:

- a City-owned public park as well as St. Mary's High School across 1 Street SE to the west;
- an EMS station within the old Fire Station No.2 (heritage building) north of Park Road SE;
- the Elbow River Casino one block to the north on the north side of 18 Avenue SE;
- the Victoria Park / Stampede LRT Station which is located 250 metres to the north along MacLeod Trail SE;
- the Calgary Stampede grounds across MacLeod Trail SE to the east;
- the Repsol Sports Centre across the Elbow River to the southwest; and
- the Elbow River pathway system across the river to the south and southeast.

The northern two-thirds of the site is flat and is currently a surface parking lot, operated by West Park that is available for public parking. Approximately 30 percent of the southern portion of the parcel is within the Elbow River Floodway and 6.0 metre Floodway setback. On average, there is an approximate two metre drop in elevation from the higher northern portion of the site to the Elbow River edge. The riverbank within the floodway consists of sloped grassy areas, shrubs and vegetation, mature poplar trees that are mostly in poor health, and very old retaining walls from the 1950s and 60s. There is also an existing closed pathway that extends from the site near MacLeod Trail SE under the MacLeod Trail bridge and over to the Stampede grounds. The pathway has been closed for many years due to safety and security reasons.

As identified in Table 1 below, the Beltline community continues to see steady growth in population, as 2018 is the peak population year.

Table 1. Community Peak Population

Beltline	
Peak Population Year	2018
Peak Population	24,887
2018 Current Population	24,887
Difference in Population (Number)	0
Difference in Population (Percent)	0

Source: *The City of Calgary 2018 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Beltline](#) community profile.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Land Use

The site is designated as Centre City Mixed Use District (CC-X) and allows for high density mixed commercial and residential uses with the option to achieve density bonusing.

Application Review

On 2017 September 13, 2018 January 29 and 2018 June 20, Administration sent initial and detailed team reviews to the applicant that identified concerns with the proposed development including the following key items:

- there was a significant lack of active uses at grade that interface with the public realm (as required in section 4.3.2 of the ARP);
- the design of the parking podium façade did not achieve the ARP policy 4.3.3 for above grade parking which should either be “screened with active uses” or have “architectural treatments that make the parking levels indistinguishable from the rest of the building façade”;
- the podium exceeded the maximum allowable height of 36 metres identified in the ARP and Land Use Bylaw 1P2007;
- there was a large oversupply of parking stalls proposed, particularly with the site being 250 metres from the Stampede LRT Station;
- there was a lack of bonus amenity items provided to achieve the proposed 3.5 bonus Floor Area Ratio (FAR);
- there were proposed structures and extensive infilling along the river bank within the floodway; and
- there was an insufficient quality of landscaping between the building and the Elbow River.

Administration and the applicant collaborated extensively following the initial reviews. The original and subsequent submissions have been revised to address comments as a result of Administration’s review including, but not limited to, the following key changes where there was:

- addition of a substantial amount of retail space on the ground level to create a better active interface with the public realm;
- substantial enhancements to the design and articulation of the façade across the podium at the above grade parking levels two through six as well as the entire façade of the triangle portion of the podium in the southeast corner (gateway feature);
- a reduction in the height of the podium from 39.5 metres to 36.0 metres;
- a reduction in the number of parking stalls from 1,265 (0.92 residential stalls per unit) to 1,180 (0.84 residential stalls per unit) for a total reduction of 85 stalls;

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

- substantial enhancements to the design of the riverfront plaza landscaping to meet both bonusing requirements for publicly accessible private open space and also to satisfy river engineering requirements;
- provision of 22 affordable housing units as a bonus earning item;
- shifting of the building out of the 5.182 metre 1 Street SE right-of-way setback; and
- enhancement of the mechanical penthouse (tower top) articulation that includes the addition of a night lighting architectural enhancement feature on each of the three towers.

The amendments to this application noted above address Administration's comments, align with relevant policy documents and the development permit is now supported.

City Wide Urban Design

The proposed development was reviewed by the City Wide Urban Design team throughout the Corporate Planning Applications Group (CPAG) review process. Due to its prominent location, the development will act as a landmark gateway to the Centre City. Considering that this location presents an opportunity to create distinct architecture and contribute to the place-making of Calgary's Beltline and Centre City, the applicant was asked to explore and consider many changes, including those outlined above. The Urban Design team deems the development to now satisfy urban design expectations for buildings of this scale and context with regard to street edge, frontage, massing, and materiality.

Urban Design Review Panel

The Urban Design Review Panel (UDRP) reviewed the application on 2017 November 01 and 2018 May 02. At the initial meeting, the panel recommended further review based on but not limited to the following reasons:

- improvements to the architectural design, language and massing of the podium required;
- improvements to the grade level interface by introducing more active uses on the ground level required; and
- better north-south pedestrian connectivity through the site was encouraged.

The applicant provided design updates to address the panel's commentary, which resulted in an endorsement, with comments at the UDRP meeting on 2018 May 02. Administration has determined that the application has addressed the panel's comments. The panel's comments along with the applicant's response to UDRP's comments can be found in Attachment 3 and Attachment 4.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Site and Building Design

The development permit proposes 1,252 residential units including a nine-storey podium and three residential towers. The project will be developed over three phases.

Podium and Towers

The main floor of the podium has a height of 9.0 metres and includes two mezzanine parking levels and Retail and Consumer Services and Restaurant uses that line the public realm along the entire riverfront plaza edge and at least half of the 1 Street SE and MacLeod Trail SE edges. Residential lobby areas, vehicular access and bicycle storage areas are also proposed on the main floor. In addition to two levels of underground parking, there are five levels (levels 2 to 6) and two mezzanine levels of above grade parking. Levels 7 to 9 contain residential units.

The composition of the three residential towers atop the podium are outlined in Table 2 below.

Table 2. Tower Details

	Tower A	Tower B	Tower C
Number of Units	425	302	365
Levels (containing units)	10 to 55	10 to 43	10 to 49
Height (including podium but excluding 2 mechanical penthouse levels)	170 metres	135 metres	153 metres
Phase	1	2	3

A combination of one and two bedroom units are proposed throughout the podium and towers and a breakdown is shown in Table 3 below.

Table 3. Unit Details

	One Bedroom Units	Two Bedroom Units
Average size	~55 square metres (592 sq.ft.)	~75 square metres (807 sq.ft.)
Number of Units	780	472
% Unit mix	~62%	~38%

Although not required to be shown on the plans, the applicant has designed the units in the towers in such a way as to allow for the merging of one and two bedroom units to create three bedroom units at some point in the future depending on market demand.

Materials

The exterior of the towers is composed of light and dark blue tinted glazing and vision glass. The podium is clad in clear storefront glazing, light and dark blue tinted glazing and vision glass and a charcoal/gray metal panel system. Refer to Attachment 1, Attachment 5 and Table 4 below for tower and podium exterior materiality.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Table 4. Materials

Tower	Podium	Triangle portion of Podium
	Storefront glazing (clear glass) - c/w dark gray aluminum mullions	Storefront glazing (clear glass) - c/w dark gray aluminum mullions
Window wall system - c/w dark gray aluminum mullions <ul style="list-style-type: none"> Light & dark blue tinted glazing/glass (vision glass / spandrel) Dark gray metal spandrel 	Window wall system - c/w dark gray aluminum mullions <ul style="list-style-type: none"> Light & dark blue tinted glazing/glass (vision glass / spandrel) Dark gray metal spandrel 	Wood composite panel system - loft brown satin finish - c/w light blue tinted glass inserts
Metal panel system (mid to dark gray top mech. ph screening)	Metal panel system (charcoal & mid-dark gray parkade screening, exterior columns, frames & beams)	
Balcony slab - sandblasted painted light gray	Balcony slab - sandblasted painted light gray	
Balcony glass railings - aluminum light gray with clear glass panels	Balcony glass railings - aluminum light gray with clear glass panels	
	Entry canopies - fluoropolymer painted aluminum light gray	

Affordable Housing Units

The applicant has chosen to bonus for 2.45 FAR (32,716 square metres of bonus floor area) through the provision of 22 affordable housing units that have an aggregate gross floor area of 1,931 square metres. This project will be the first in Calgary to provide bonus density using this method. The affordable housing units will be located entirely within Tower A, interspersed throughout the building with a similar mix of unit types as the rest of the development. The tenants of the affordable units will have full access to all of the amenities.

Community Planning and Calgary Housing have been involved in the discussions and parameters of the affordable housing units as a bonus density item and are in support of Cidex being an affordable housing provider, subject to the execution of a housing agreement with Calgary Housing prior to release of the development permit. See Attachment 7 for a letter from the Director of Calgary Housing indicating the mutual understanding between Calgary Housing and Cidex regarding the provision of affordable housing units as part of this development.

Indoor Amenity Space

The development proposes a significant amount of indoor amenity space with amenities such as a meeting and gathering spaces, lounge, gym spaces, game areas and a potential indoor pool.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Landscaping

Publicly Accessible Private Open Space

The applicant has chosen to bonus for 1.2 FAR (16,239 square metres of bonus floor area) through the provision of Publicly Accessible Private Open Space along the riverfront between the building and the river's edge, as shown on Plans DP-109-PAPOS and DP-L1-00 of Attachment 1. The open space proposes:

- a walkway promenade and plaza with decorative pavers and LED in-ground strip lighting along the building's edge;
- an art mural along the MacLeod Trail SE frontage;
- a 4.0 metre wide regional pathway with decorative pavers across the site that connects to the Stampede grounds via the existing MacLeod Trail bridge underpass;
- extensive amounts of native shrubs and trees between the pathway and riparian area;
- vegetated rock riprap along the river's edge and riparian area; and
- native rock seating and steps that lead down to three 'touch-the-water' features.

The signature open space provides an opportunity for a new, high quality public realm destination along the south facing Elbow River riverfront area. The design and value of the proposed public space was reviewed by Administration and found to meet the design and financial value requirements outlined in the Land Use Bylaw 1P2007 and *Beltline ARP*.

It is noted that the legal title of the property extends into the bed and shore of the river and that public access to the river edge will be provided through a public access easement as a condition of approval.

Riverbank Improvements

The development site is within the "Floodway" flood risk area (as per Council approved Land Use Bylaw flood maps). Notwithstanding the minor relaxations which are supported by The City as outlined in the Land Use Bylaw section of this report, the development permit proposal is compliant with Land Use Bylaw 1P2007, Part 3, Division 3 Floodway Regulations. The developer was not required to build or pay for the proposed flood protection improvements on their property, however, has chosen to do so for erosion and flood protection purposes and in order to enhance the quality of the open space for public benefit along the riverbank.

Outdoor Amenity Space

On levels 7 and 10, which are rooftop levels to the podium, extensive areas of outdoor amenity have been proposed, including a running track, dog park, outdoor seating, outdoor exercise and yoga corner, and a BBQ area.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Public Realm Improvement

To enhance the streetscape, seven proposed boulevard trees will be planted along Macleod Trail SE within the public sidewalk and three boulevard trees will be planted along 1 Street SE within the development's property. In addition, multiple planter boxes that include shrubs and flower beds are proposed within the site adjacent to the 1 Street SE and Park Road SE public right-of-way. However, due to utility conflicts, no new boulevard trees can be planted along Park Road SE.

Transportation

Site Access

Vehicular access to the parkade is located on Park Road SE. Loading and waste and recycling access are also taken from Park Road SE.

A Traffic Impact Assessment was required, reviewed and accepted by Administration, and has resulted in the requirement to signalize the intersections of 1 Street SE and 18 Avenue SE and Macleod Trail SE and Park Road SE, at the developer's cost, as a condition of approval.

Utilities and Servicing

Water, storm sewer and sanitary mains are available to service the site. As a condition of approval, existing onsite sanitary and water mains will be relocated at the developer's expense. Development servicing shall be to the satisfaction of Water Resources at the development site servicing plan stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

The decision made by Calgary Planning Commission will be advertised in accordance with the *Municipal Government Act*. As this development permit is for a discretionary use, an appeal may be filed based on the decision on the entire permit, the decision to grant a relaxation, or any of the conditions placed on an approval.

The Beltline Community Association reviewed the application and provided comments on 2018 September 03 (see Attachment 5). In the letter the Association indicated their general support for the development, including remarks of how the project has improved since the initial submission. They did however express concerns with the mass and treatment of the podium.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Engagement

The applicant met with representatives from the Beltline Community Association on 2018 January 30. Administration did not attend the meeting, however the applicant indicated the feedback was generally positive. There were comments from the Association related to the form and design of the podium which were in line with Administration's review comments.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed development builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The subject site is located within the Centre City area as identified on Map 1: Urban Structure in the MDP. The proposed development supports key objectives of the MDP and CTP by fostering distinctive, mixed use development with a strong sense of place, and promoting site and building design that contributes to high quality living environments.

Beltline Area Redevelopment Plan (Statutory – 2006)

The site is located within the Urban Mixed-Use Area of the *Beltline Area Redevelopment Plan* (ARP) and the intent of this policy area is to allow for a wide range and mix of uses in many possible configurations, both within buildings and within the local context resulting in vibrant, pedestrian-friendly streets. Administration finds the proposed development meets the objectives and policies for Urban Mixed-Use Areas, in particular the following:

- creation of vibrant pedestrian-friendly streets and a public realm that provide activity throughout the daytime and evening hours;
- creation of a streetscape that responds to the context of the particular area, has buildings that are built to and frame the sidewalk, and that have a high degree of permeability between interior and exterior space through the use of transparent windows and doors; and
- significant portion of the parking structure above grade level is screened from public streets with architectural treatments that make the parking levels indistinguishable from the rest of the building façade.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

According to the ARP, the maximum floor plate size for floors above 36 metres from grade is 750 square metres. In addition, the minimum building separation for floors above 36 metres from grade is 24 metres.

The typical tower floor plate size proposed in the development is 798 square metres. Further, the separation proposed between Towers A and B at the narrowest point is 17.6 m and the separation between Towers B and C at the narrowest point is 14.0 m. Although these specific requirements of the policy have not been met, Administration finds these variances acceptable given that:

- Tower B is positioned on a 45 degree angle which means that the majority of the separation between the buildings (building mid-point to mid-point) exceeds 24 metres;
- at the point of narrowest separation between Towers B and C (14.0 metres), Tower B has an uninterrupted horizontal view plane angle of 63 degrees for an indefinite distance, which exceeds the ARP minimum requirement of 50 degrees; and
- policy 6.3.7 allows the Development Authority to relax the requirements in order to exercise greater design control in the case of developments with multiple towers as part of a comprehensive development, provided the intent of the section is addressed, which Administration believes is the case.

The tower tops contribute to the skyline profile by having an identifiable, iconic architectural design that skillfully incorporates all elevator cores and mechanical rooms within the tower tops.

The proposed development incorporates an extension of the Elbow River Regional Pathway through the site as shown on Map 6: Transportation Concept.

Centre City Plan (Non-statutory - 2007)

Section 7.2 Centre City Design – Skyline seeks to ensure towers are sited and designed to have a positive contribution to the Calgary skyline. Administration believes that the placement, massing, shape and materials of the towers contribute positively to Calgary's skyline character.

The proposed development incorporates an extension of the Elbow River Regional Pathway through the site as shown on Concept 20: Riverfront and Concept 21: Pedestrian Network.

The development meets policy 7.7.1.5 Linkages regarding the public realm as it complements the street wall with streetscape elements such as a single row of trees, a denser rhythm of pedestrian scaled street lighting (including in-ground LED strip lighting), as well as other special features that enhance the sense of human scale including a fine articulation of the building base at street level with attractive canopies, overhangs and transparent openings into the retail and restaurant uses.

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Land Use Bylaw 1P2007

The subject parcel is designated Centre-City Mixed-Use District (CC-X) and the proposed development permit application aligns with the overall purpose of the district, as the development provides:

- a mix of residential and commercial uses;
- a building form that is street oriented at grade; and
- acceptable public benefit and amenities on-site in exchange for bonus density.

The proposed FAR for the development is 11.5. In accordance with Part 11, Division 7 of the Land Use Bylaw, bonusing is required for density above 8.0 FAR. As such, 3.5 FAR is proposed to be bonused through the provision of Publicly Accessible Private Open Space (achieves 1.2 FAR) and 22 affordable housing units (achieves 2.3 FAR). The financial value of the open space was substantiated through a third-party quantity surveyor and deemed to be in-line with current market rates.

Table 6. Floor Area Ratio Summary

Land Use Bylaw Section	Land Use Bylaw Provisions	Proposed Floor Area Ratio
1166(1)(c)(i)	5.0 FAR maximum	5.0 FAR
1166(1)(c)(ii)	Additional 3.0 FAR maximum for Multi-Residential Development	3.0 FAR
1166(3) & Part 11, Division 7 (1200(b))	Bonus above 8.0 FAR up to maximum of 12.0 FAR	3.5 FAR
Total Proposed FAR:		11.5 FAR

Administration identified the following relaxations to the Land Use Bylaw 1P2007 and after review Administration considers each relaxation acceptable. A response to each relaxation is contained in Table 7 below.

Table 7. Bylaw Relaxations

Regulation	Standard	Provided
58 Alterations to the Floodway and Riverbanks	On those areas of land within the floodway that are subject to municipal jurisdiction, no alterations shall be made to a floodway and no structures including, but not limited to, berms, decks, docks, fences, gates, patios, rip-rap or walls shall be constructed on, in or under a floodway unless those structures are being constructed by, or on behalf of, the City for	Plans indicate proposed vegetated rock rip-rap and alterations to the grade within the floodway. RELAXATION SUPPORTED by River Engineering given that the alteration of the bank by placement of vegetated riprap is for the purpose of erosion protection, and will protect infrastructure including the amenity pathway. It is understood that this work will be completed in accordance with The City's Design Guidelines for Erosion and Flood

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Regulation	Standard	Provided
	the purpose of erosion control, where the primary purpose is to protect public infrastructure.	Control: Streambank and Riparian Stability Restoration . In addition, the relaxation is supported given that the grade alterations are required to allow for the construction of an amenity pathway (required by the Beltline ARP) to minimum City pathway standards, and the alterations are minimal enough that they are anticipated to meet the intent of the bylaw and not pose any significant incremental water level change in the Elbow River.
61 Overland Flow	(1) All buildings in the overland flow area must be designed in the following manner: (d) a sewer back up valve must be installed in every building.	Plans do not indicate a sewer back-up valve is provided. RELAXATION SUPPORTED as this will be resolved through the Development Site Servicing Plan (DSSP)
1169 Front Setback Area	Where the parcel shares a property line with a street, the front setback area must have minimum and maximum depth as noted. (2) The front setback area must have a minimum depth of 1.5 metres and a maximum depth of 6.0 metres for parcels located on the following streets: (a) 1 Street SE; (d) Macleod Trail SE.	Plans indicate the minimum setback provided from the property line shared with 1 Street SE is 0.04m (-1.46m). RELAXATION SUPPORTED as the 5.182 m road right of way setback is provided which gives sufficient depth.
		Plans indicate the maximum setback provided from the property line shared with 1 Street SE is 6.76m (+0.76m). RELAXATION SUPPORTED as the building is predominantly set back from the PL at 5.18 m
1168 Building Orientation	The main public entrance to a building must face the property line shared with a commercial street.	Plans indicate the main public entrance is provided off Park Road SE. RELAXATION SUPPORTED There is no other logical place for the main entrance along 1 Street SE or Macleod Trail SE.
1172 Floor Plate Restrictions	(2) In all other locations, each floor of a building located partially or wholly above 36.0 metres above grade, and containing Dwelling Units, Hotel suites or Live Work Units, has a maximum:	Plans indicate the area of Tower A 36.0m above grade is 799.49m ² (+149.49m ²). RELAXATION SUPPORTED See comments under Beltline ARP for rationale

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Regulation	Standard	Provided
	(a) floor plate area of 650.0 square metres; and	Plans indicate the area of Tower B 36.0m above grade is 666.48m ² (+16.48m ²). RELAXATION SUPPORTED See comments under Beltline ARP for rationale
		Plans indicate the area of Tower C 36.0m above grade is 795.55m ² (+145.55m ²). RELAXATION SUPPORTED See comments under Beltline ARP for rationale
1151 Amenity Space	(7) Private amenity space must: (b) have no min. dimensions of less than 2.0 m..	Plans indicate multiple private amenity spaces provided have a dimension less than 2.0m. RELAXATION SUPPORTED given the extensive amount of proposed outdoor amenity space on the podium roof and riverfront plaza area.
1150 Additional Landscaping Requirements	(1) Unless otherwise referenced in a District, all setback areas on a parcel, not including those portions specifically required for motor vehicle access, sidewalks, or any other purpose allowed by the Development Authority, must be a landscaped area.	Plans indicate the building is located within the East, North and West setback area. RELAXATION SUPPORTED as it is only a minimal amount of building area that is located within the setback areas.
1174 Landscaping In Setback Areas	(1) Where a setback area shares a property line with a street, the setback area must be a landscaped area.	

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

Social, Environmental, Economic (External)

Crime Prevention Through Environmental Design (CPTED)

This development permit aligns with CPTED principles by:

- providing new public access to the riverfront area which has had a history of social disorder in this location;
- activating the public realm along the riverfront plaza by providing transparent glazing for the windows and doors that lead to the lobby spaces and multiple stores; and
- providing significant in ground lighting in the riverfront plaza area as well as adequate fascia and soffit lighting for residents and visitors around the building's edge at grade.

There are no contamination related concerns associated with the subject lands and/or proposed development scope.

The proposed development will increase the density of the site considerably, providing a larger base of residents to use surrounding services, amenities and surrounding retail locations.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed development does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

A risk associated with this application is that should the required housing agreement for the affordable housing units not be entered into, there will be no affordable units provided within the development but the increased density based on the provision of those units will have been approved. This risk will be mitigated by requiring the applicant under such a scenario to make a significant financial contribution to the Beltline Community Investment Fund (BCIF) instead. Details of these requirements are outlined in Attachment 2.

There are no other significant risks associated with this proposal.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2018-1012

Development Permit in Beltline (Ward 11) at Multiple Properties, DP2017-4075

REASON(S) FOR RECOMMENDATION(S):

Subject to the conditions listed, and considering the relaxations noted, the application is in alignment with relevant statutory and non-statutory planning policy contained in the *Municipal Development Plan, Beltline Area Redevelopment Plan, Centre City Plan* and Land Use Bylaw 1P2007. The application is on a major gateway site to the Centre City and proposes exciting enhancements that will benefit the surrounding community, including a riverfront plaza, river-facing restaurant and retail uses, and 22 affordable housing units.

This development permit is recommended for approval.

ATTACHMENTS

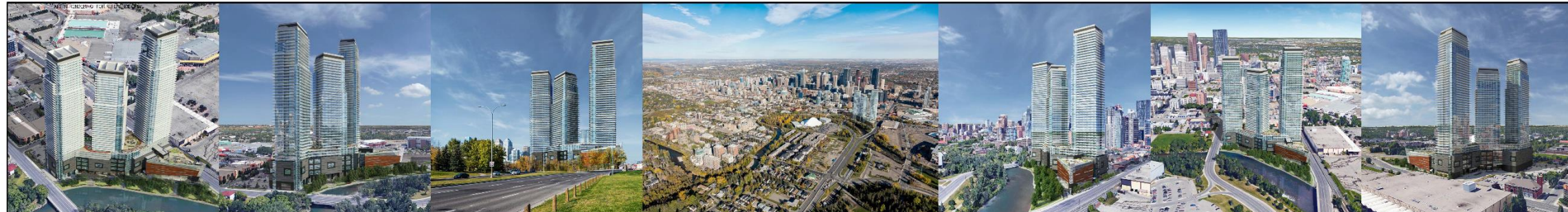
1. Development Permit Plans
2. Conditions of Approval
3. Urban Design Review Panel Comments & Applicant Response (Review #1)
4. Urban Design Review Panel Comments & Applicant Response (Review #2)
5. Beltline Neighbourhood Association Comments
6. Hat @ Elbow River CPC Presentation
7. Letter of Understanding – Affordable Housing

Development Permit Plans

Hat @ Elbow River

1919 MacLeod Trail SE, Beltline
Calgary, Alberta

NORR JOB NO: NCCA140240



DP RESUBMISSION - DTR#3 RESPONSE - 2018 -AUGUST- 27

ISSUED FOR ITR RESPONSE - 2017 - DECEMBER - 18
ISSUED FOR DEVELOPMENT PERMIT - 2017 - SEPTEMBER - 05
ISSUED FOR DTR1 - 2018 - MAY - 15
ISSUED FOR DTR2 - 2018 - JULY - 10
DP2017-4075

ARCHITECTURE	LANDSCAPE	MECHANICAL / ELECTRICAL	CIVIL	SURVEY	STRUCTURAL
NORR Architects Engineers Planners 411-1ST STREET SE, SUITE 2300 CALGARY, AB T2G 4Y5 PHONE: 403-264-4000 FAX: 403-269-7215	NORR Architects Engineers Planners 411-1ST STREET SE, SUITE 2300 CALGARY, AB T2G 4Y5 PHONE: 403-264-4000 FAX: 403-269-7215	TLJ ENGINEERING CONSULTANTS SUITE 301, 301 14TH STREET NW CALGARY, AB T2H 2A1 PHONE: 403-289-8852	RICHMEW ENGINEERING INC. 203 38 AVE NE, SUITE 201 CALGARY, AB T2E 2B3 PHONE: 403-230-3218	KELLAM BERG ENGINEERING & SURVEYS LTD. 5800-1A ST. SW CALGARY, AB T2H 0G1 PHONE: 403-640-0900 FAX: 403-640-0678	
DP SHEET LIST	LANDSCAPE SHEET LIST	CONSULTANT SHEET LIST			
DP-101 SITE PLAN	DP-10-00 LANDSCAPE STATS PLAN	DP-01 SITE SURVEY			
DP-102 SITE STATISTICS					
DP-103 PHASING PLAN	DP-11-00 GROUND FLOOR LANDSCAPE PLAN	DP-02 SITE GRADING PLAN			
DP-104 WASTE & RECYCLING PLANS / SITE DETAILS	DP-11-01 GROUND FLOOR LANDSCAPE PLAN	DP-03 SITE SERVICING PLAN			
DP-105 SWEEP PATHS & LOADING STALLS	DP-11-02 LANDSCAPE PLAN - LEVEL 07				
DP-106 SHADOW STUDY	DP-11-03 LANDSCAPE PLAN - LEVEL 10	DP-04 SITE LIGHTING LAYOUT			
DP-107 3D RENDERINGS	DP-12-00 LANDSCAPE DETAILS	DP-05 LEVEL 6.5 LIGHTING LAYOUT			
DP-108 LEVEL P1 SIGNAGE PLAN	DP-12-01 LANDSCAPE DETAILS				
DP-201 FLOORPLAN - LEVEL P2 - PARKADE	DP-12-02 LANDSCAPE DETAILS				
DP-202 FLOORPLAN - LEVEL P1 - PARKADE	DP-12-03 LANDSCAPE SECTIONS				
DP-203 FLOORPLAN - LEVEL 01 - MAIN FLOOR	DP-12-04 LANDSCAPE DETAILS				
DP-204 FLOORPLAN - LEVEL 01 - MEZZANINE 1					
DP-205 FLOORPLAN - LEVEL 01 - MEZZANINE 2					
DP-206 FLOORPLAN - LEVEL 02 - UPPER PARKADE					
DP-207 FLOORPLAN - LEVEL 03 - UPPER PARKADE					
DP-208 FLOORPLAN - LEVEL 04 - UPPER PARKADE					
DP-209 FLOORPLAN - LEVEL 05 - UPPER PARKADE					
DP-210 FLOORPLAN - LEVEL 06 - UPPER PARKADE					
DP-211 FLOORPLAN - LEVEL 06 - U/S TRANSFER SLAB					
DP-212 FLOORPLAN - LEVEL 07 - PODIUM RESIDENTIAL UNITS					
DP-213 FLOORPLAN - LEVEL 08 - PODIUM RESIDENTIAL UNITS					
DP-214 FLOORPLAN - LEVEL 09 - PODIUM RESIDENTIAL UNITS					
DP-215 FLOORPLAN - LEVEL 10 - PODIUM OUTDOOR AMENITY					
DP-216 FLOORPLAN - LEVEL 11 - TYPICAL TOWER LEVEL					
DP-TA-201 TOWER 'A' TYPICAL FLOORPLANS					
DP-TA-202 TOWER 'A' MECHANICAL PENTHOUSE + ROOF PLAN					
DP-TB-201 TOWER 'B' TYPICAL FLOORPLANS					
DP-TB-202 TOWER 'B' MECHANICAL PENTHOUSE + ROOF PLAN					
DP-TC-201 TOWER 'C' TYPICAL FLOORPLANS					
DP-TC-202 TOWER 'C' MECHANICAL PENTHOUSE + ROOF PLAN					
DP-301 WEST ELEVATION					
DP-302 SOUTH ELEVATION					
DP-303 NORTH ELEVATION					
DP-304 EAST ELEVATION					
DP-305 WINDOW WALL, TYPICAL ELEVATION MODULE DETAIL					
DP-306 ACCENT ELEVATIONS					
DP-401 BUILDING SECTIONS					
DP-402 BUILDING SECTIONS					
DP-403 BOULEVARD & ENTRANCE RAMP SECTIONS					
DP-501 TYPICAL PODIUM UNIT PLANS					
DP-502 TYPICAL PODIUM UNIT PLANS					
DP-503 TYPICAL TOWER UNIT PLANS					
DP-504 TYPICAL TOWER 3 BEDROOM UNIT PLANS					
DP-505 TYPICAL TOWER 3 BEDROOM UNIT PLANS					

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NORR ARCHITECTS ENGINEERS PLANNERS
A Partnership of Limited Companies
Over 100 years of experience in building and design. 1000+ employees. 100+ offices.
NORR is a leading international design and construction firm. For more information, contact:

Victor Smith, President, AAA, DARD, MAJC
Drew McKelvie, Architect, AIA, 2 Arch, MAJC
A. Silvio Raccaniga, Architect, AAA, 4 Arch, MBRC
Arthur Ladda, Planning, AM-Cor
Chris P.W. Peng, AFPCA

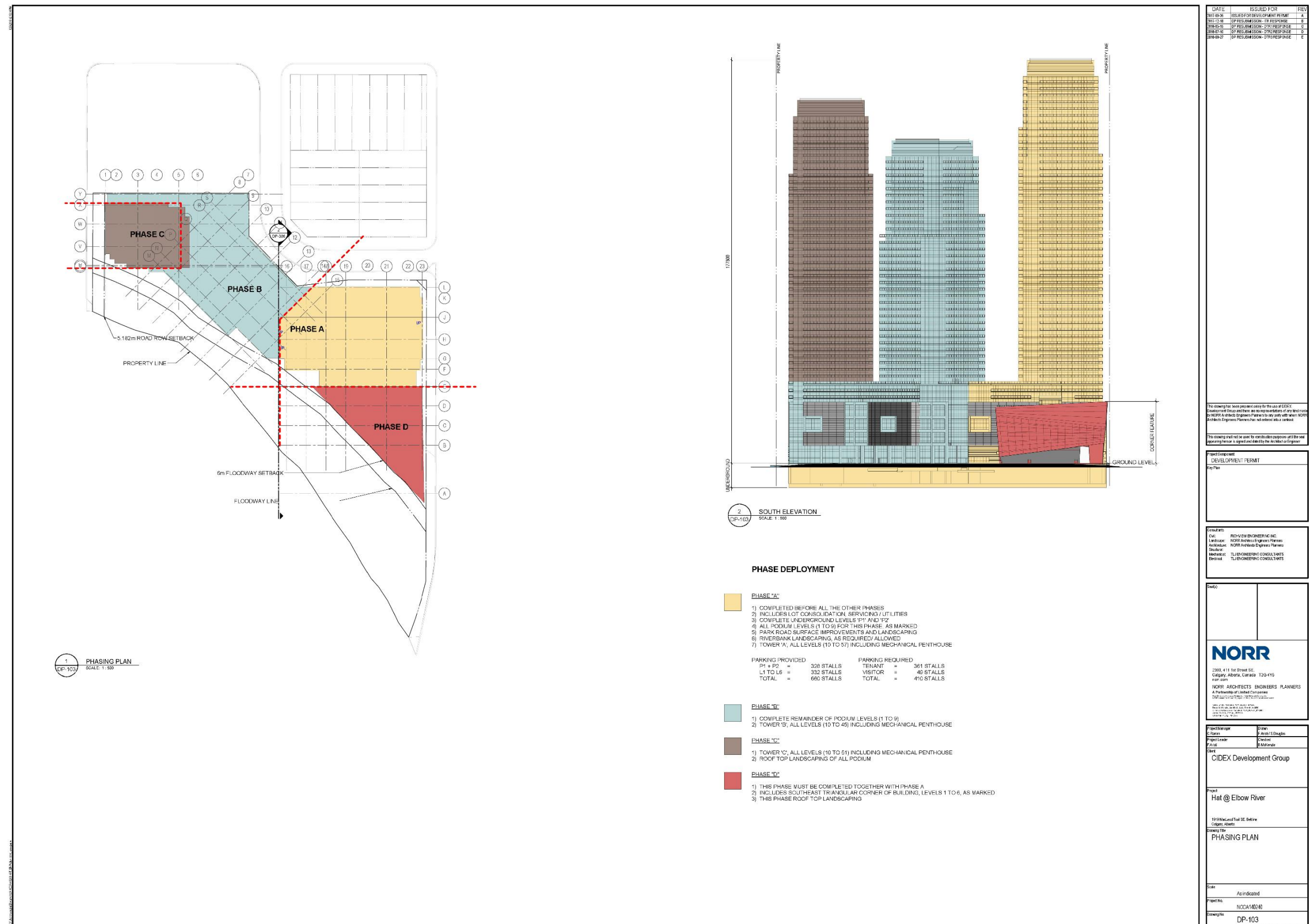
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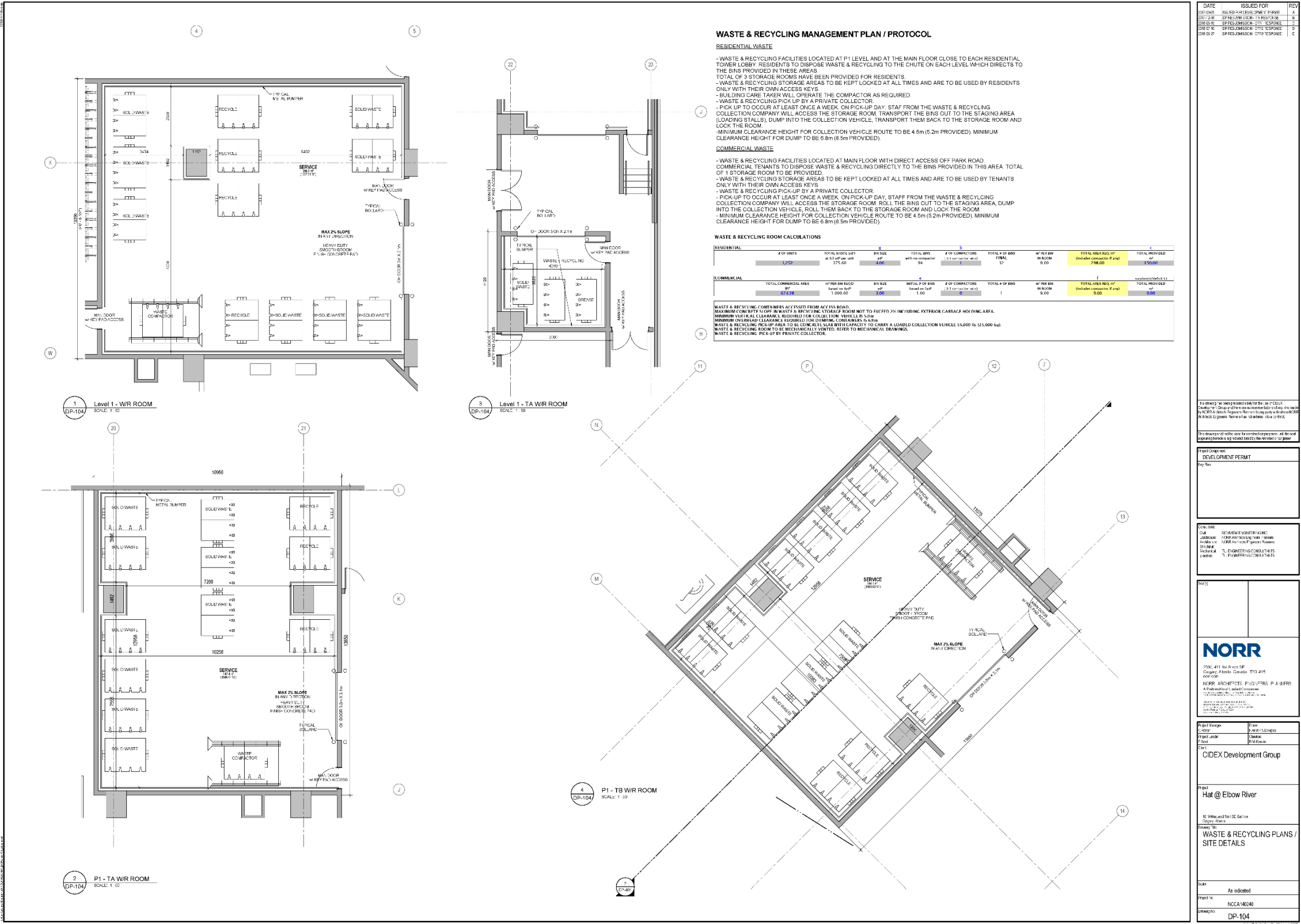
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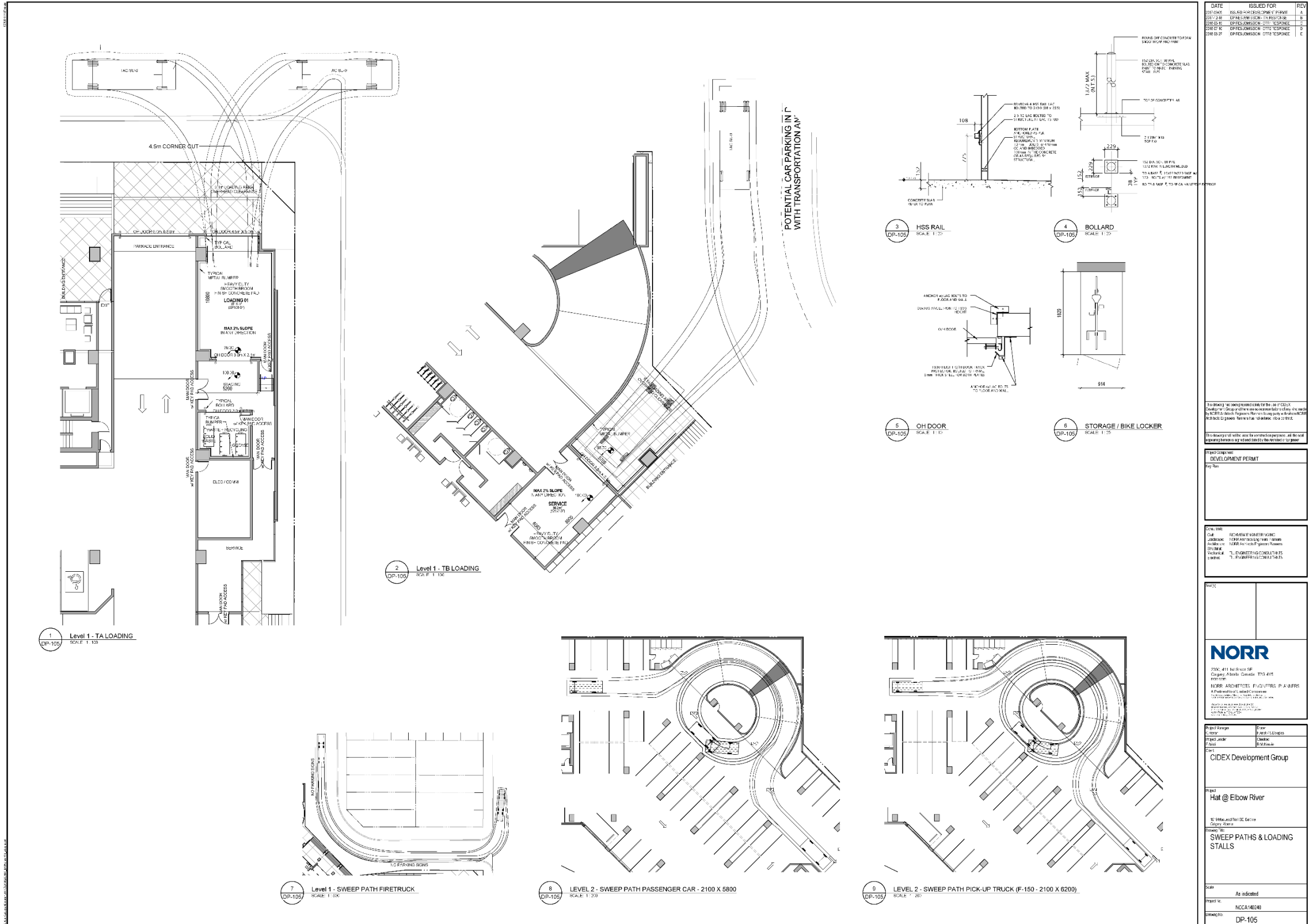
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Development Permit Plans



Development Permit Plans



Development Permit Plans



MARCH 21 - 10A



MARCH 21 - 12P



MARCH 21 - 2PM



MARCH 21 - 4P



SEPTMBER 21 - 10A



SEPTMBER 21 - 12P



SEPTEMBER 21 - 2P



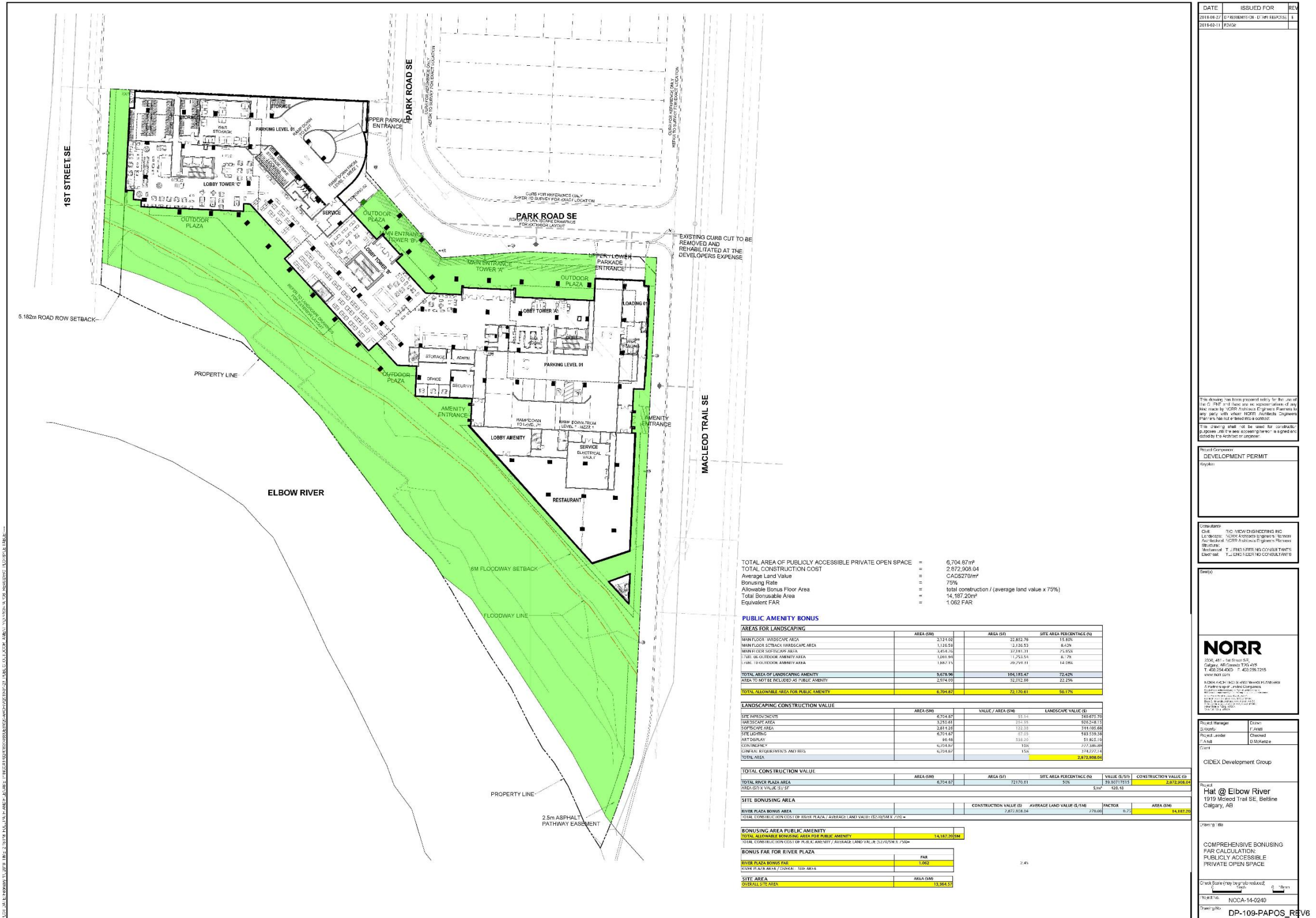
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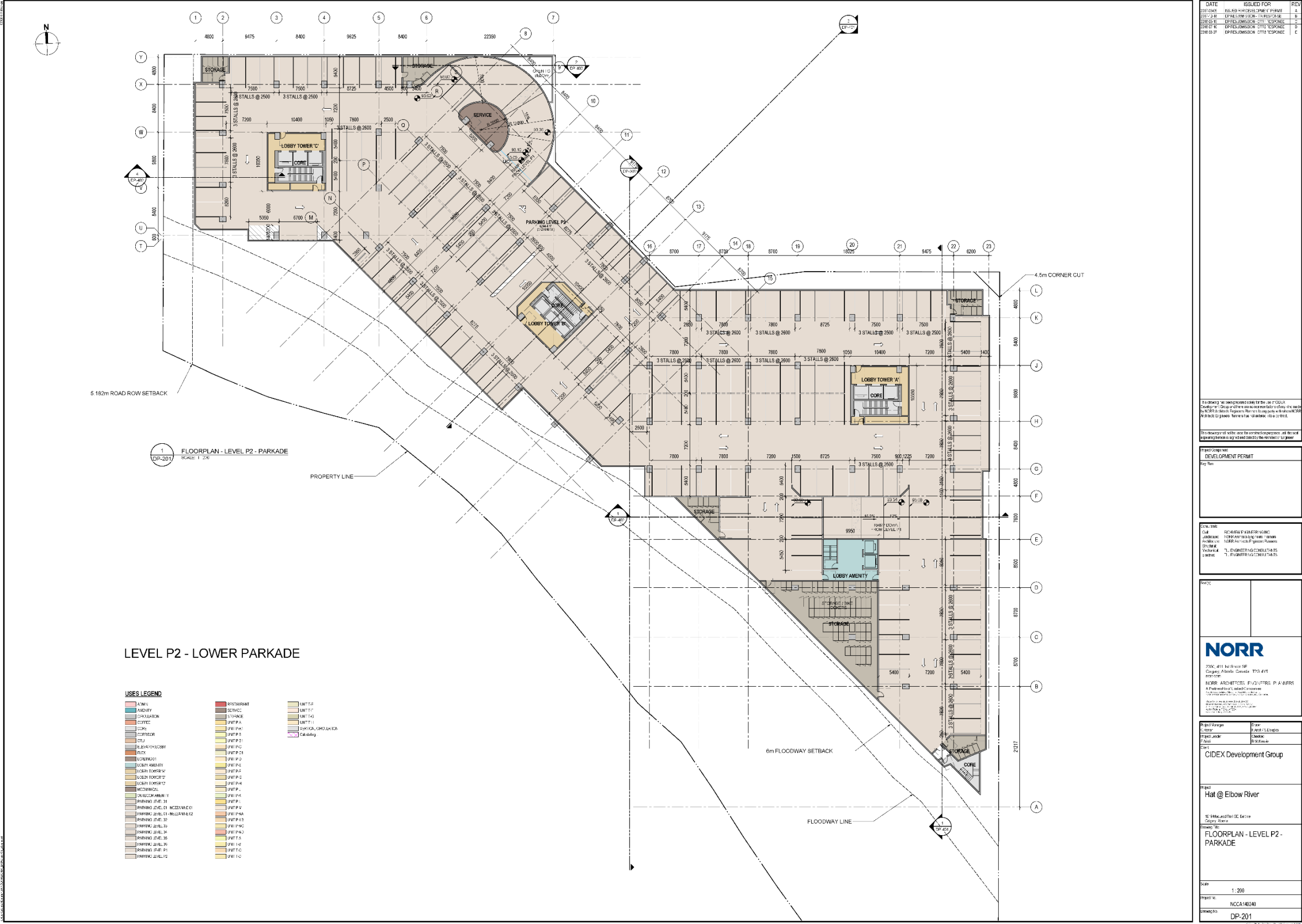
Development Permit Plans



Development Permit Plans



Development Permit Plans



DATE	ISSUED FOR	REV
2018-04-16	ISSUED FOR DEVELOPMENT PERMIT	A
2018-04-16	ISSUED FOR DEVELOPMENT PERMIT	B
2018-04-16	ISSUED FOR DEVELOPMENT PERMIT	C
2018-04-16	ISSUED FOR DEVELOPMENT PERMIT	D
2018-04-16	ISSUED FOR DEVELOPMENT PERMIT	E

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PROJECT NO.	DP-201

OWNER
CIC Development Group

DESIGNER
NORR ARCHITECTS

DATE
2018-04-16

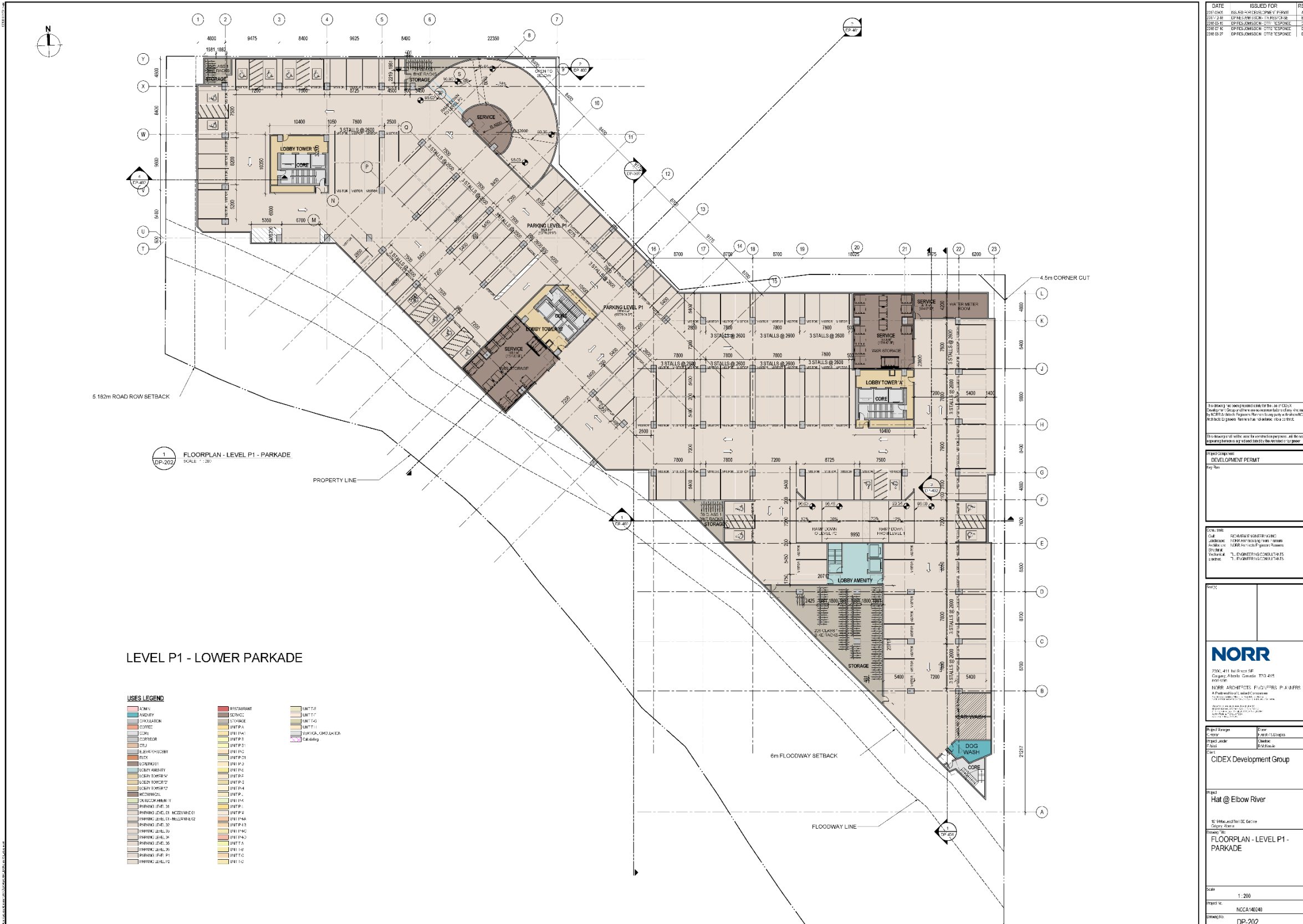
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PROJECT NO.	DP-201

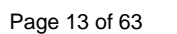
PROJECT
Hat @ Elbow River

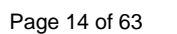
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NCCA-140240

PROJECT NO.
DP-201

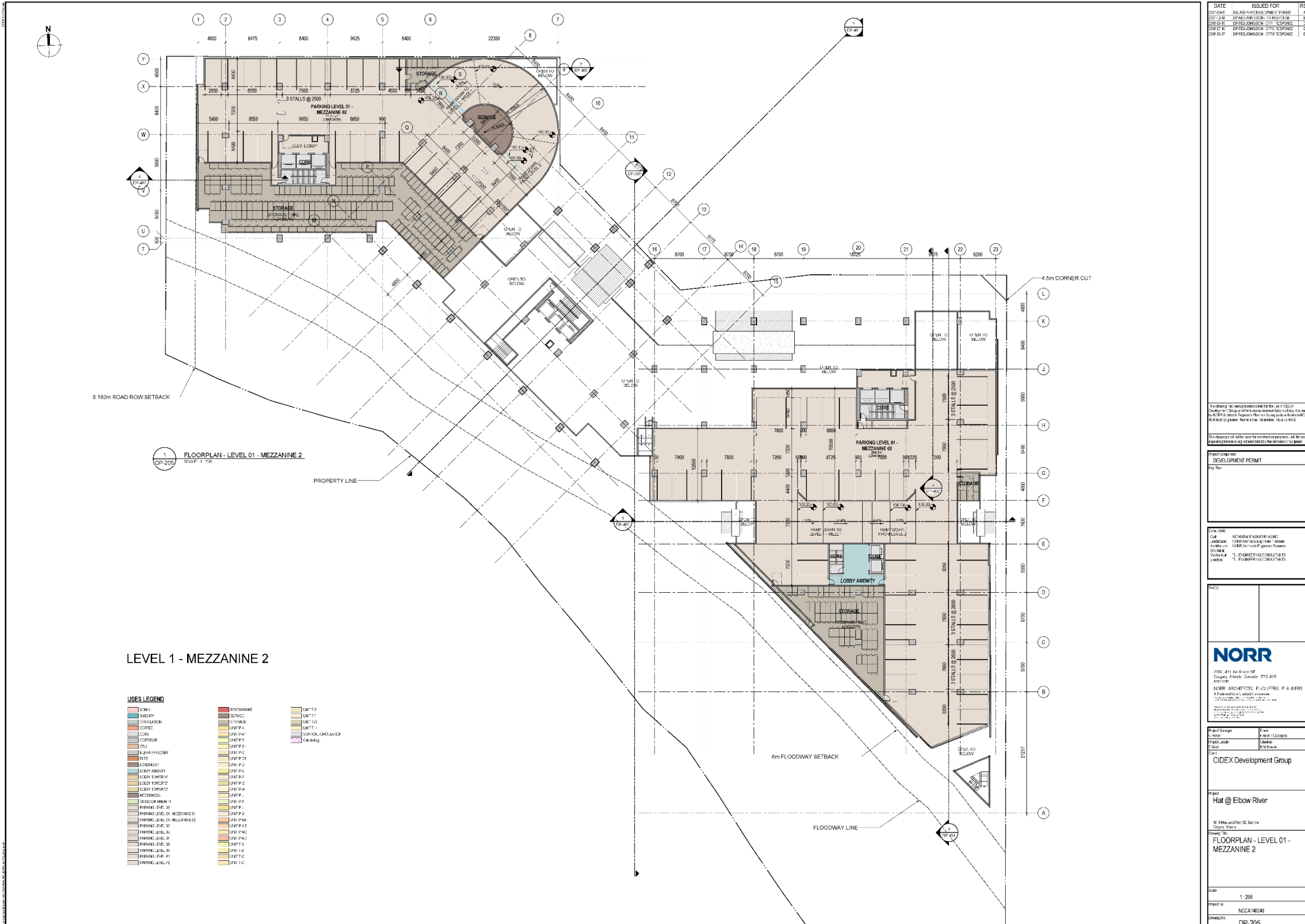
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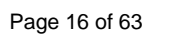


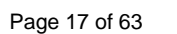


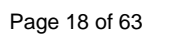


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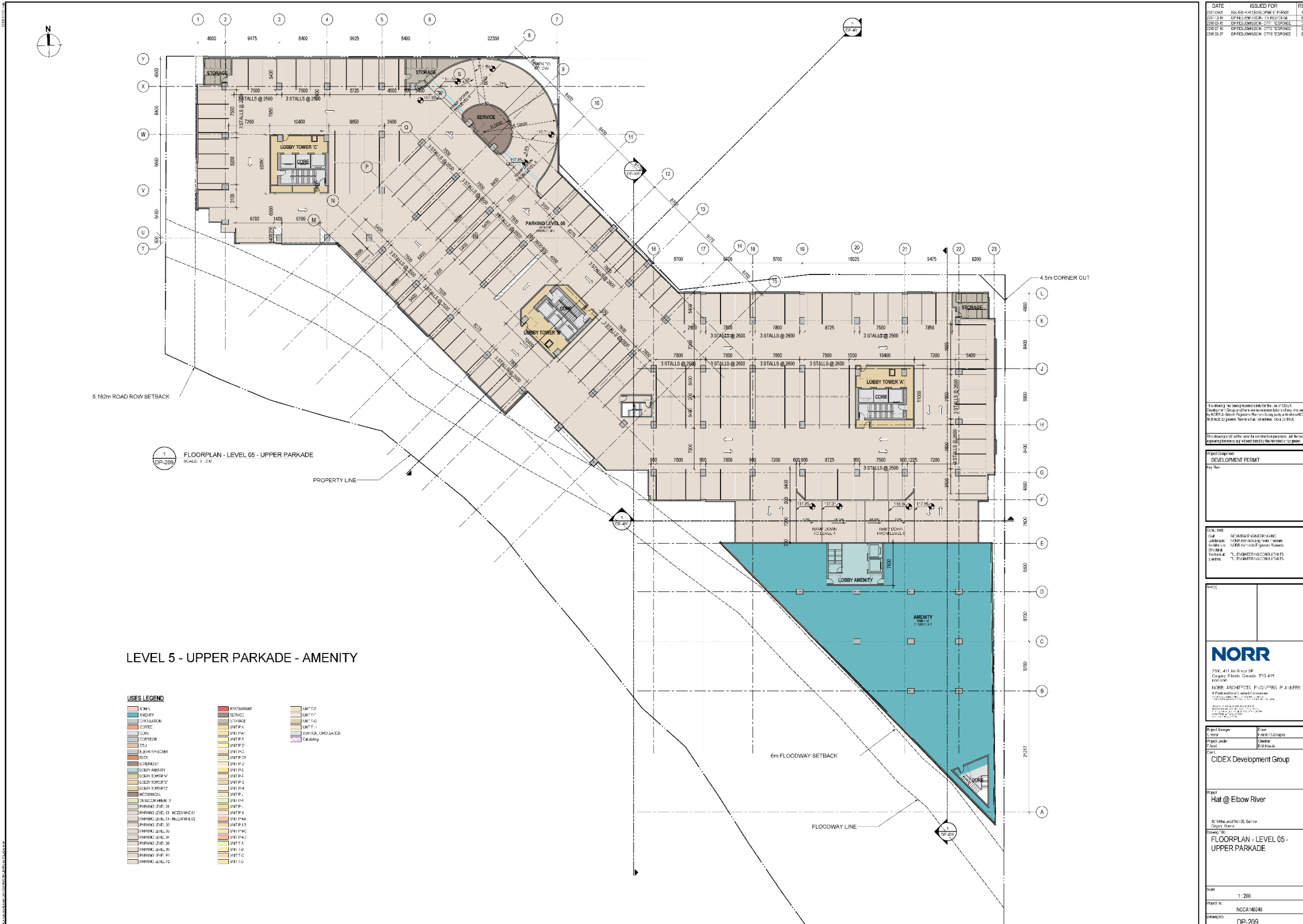


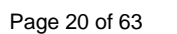


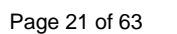




Development Permit Plans







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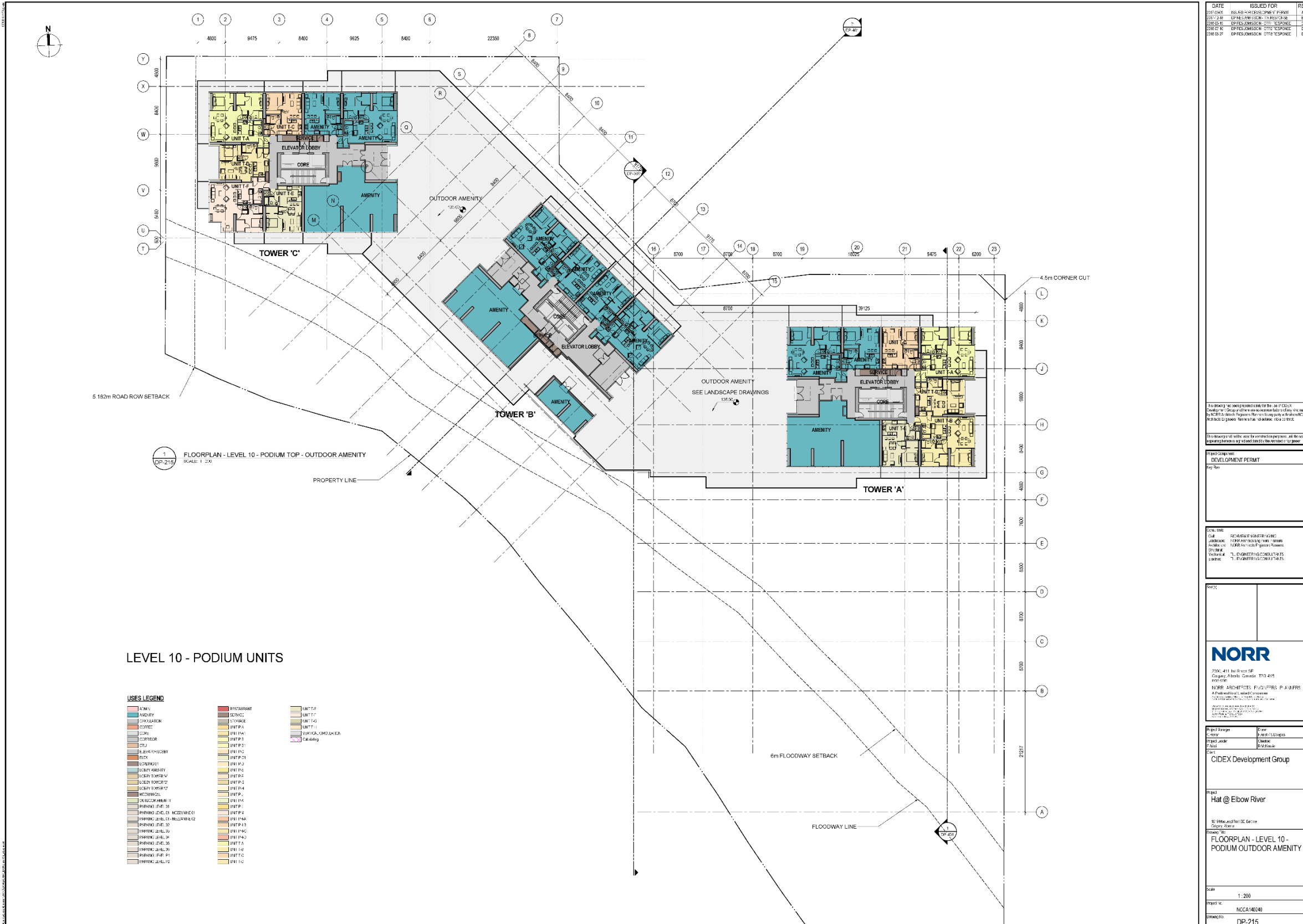
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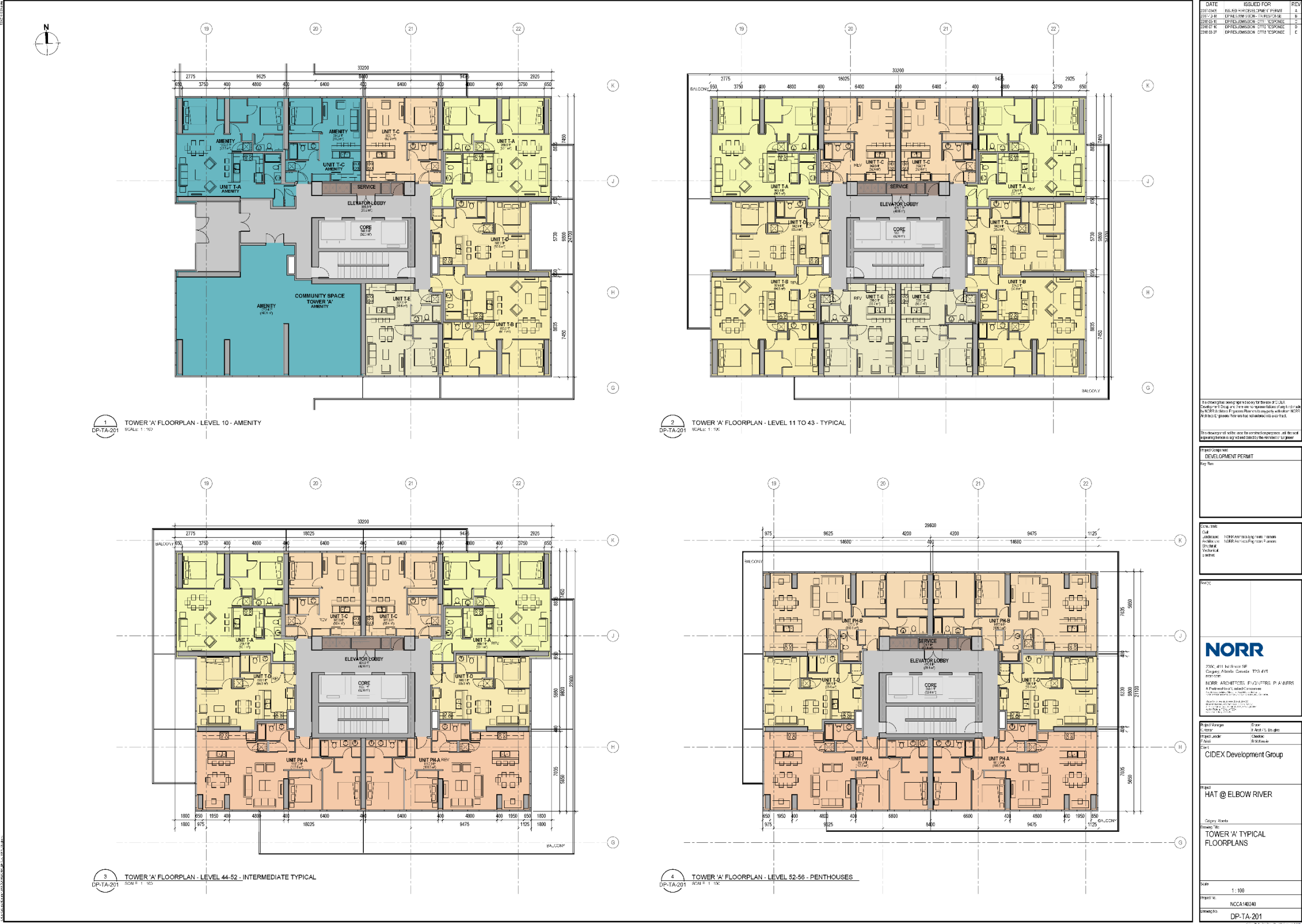
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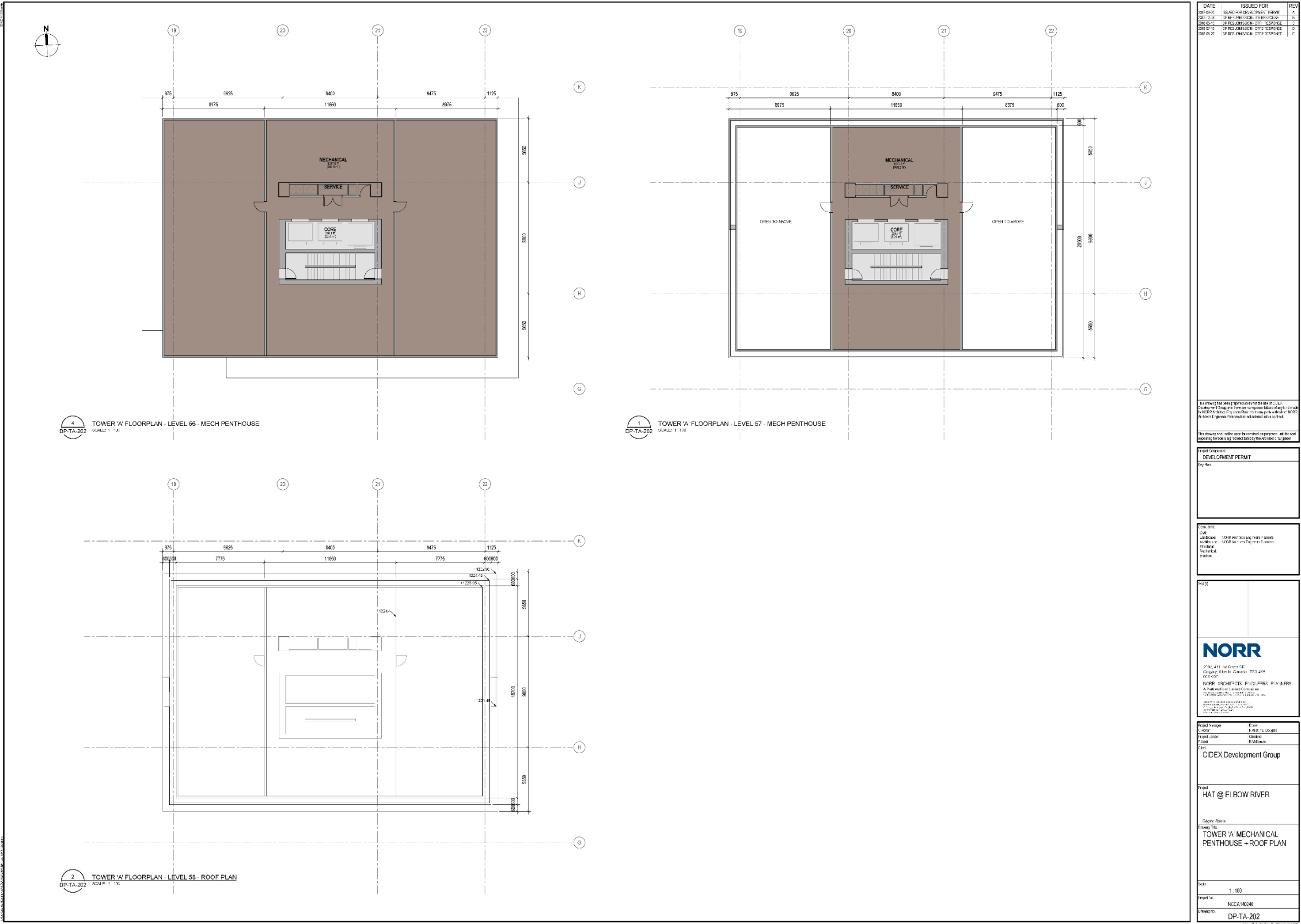
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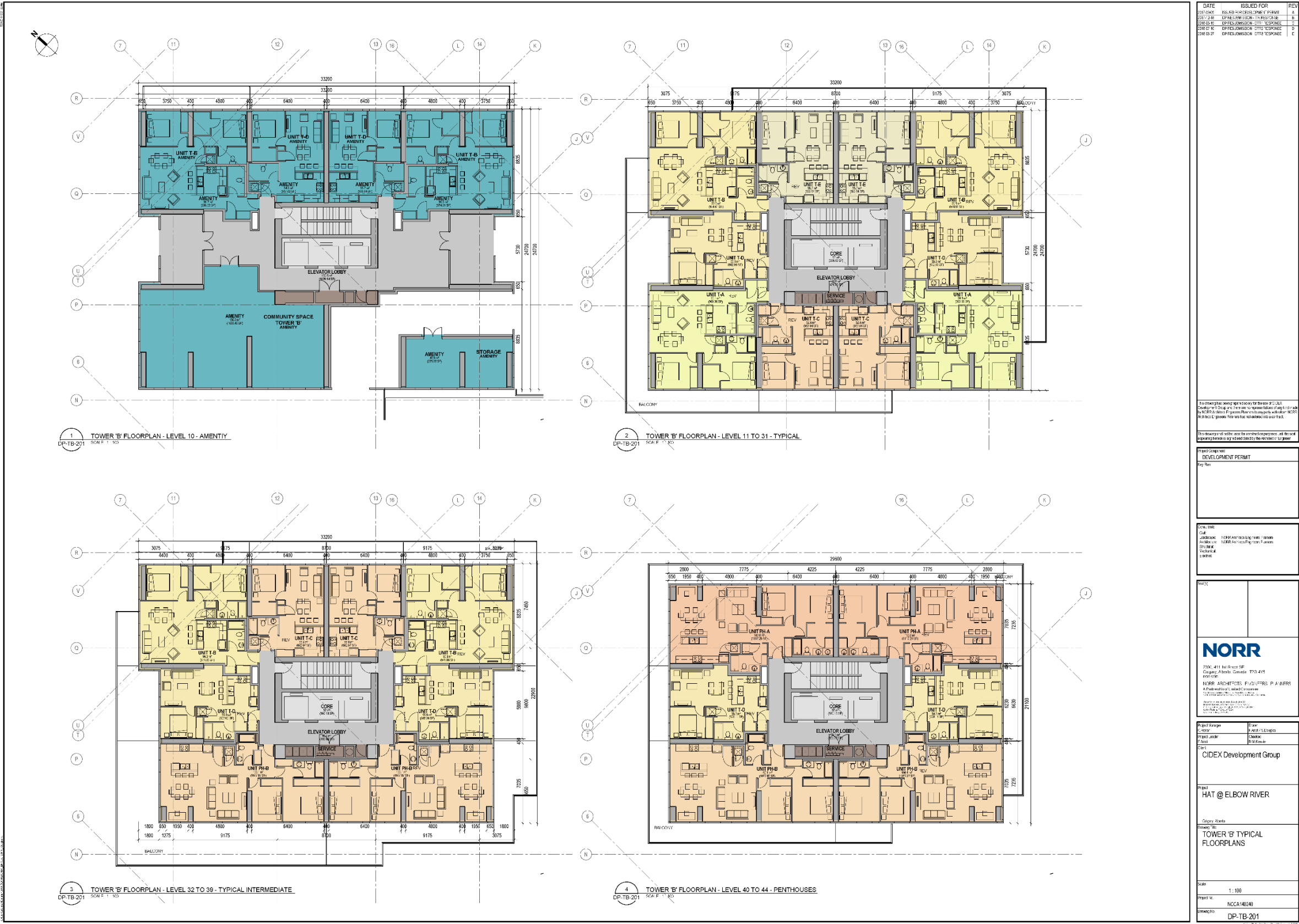
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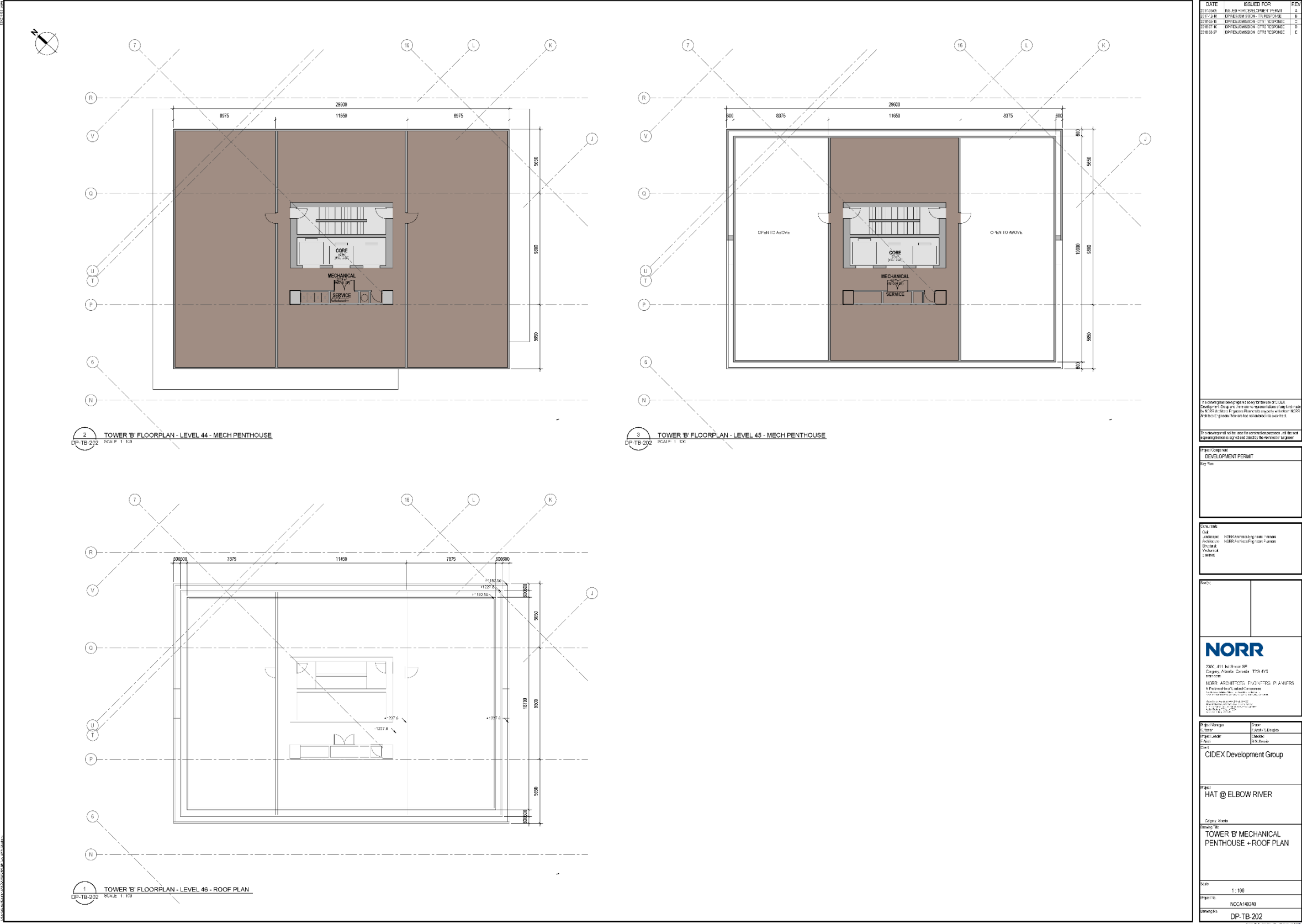
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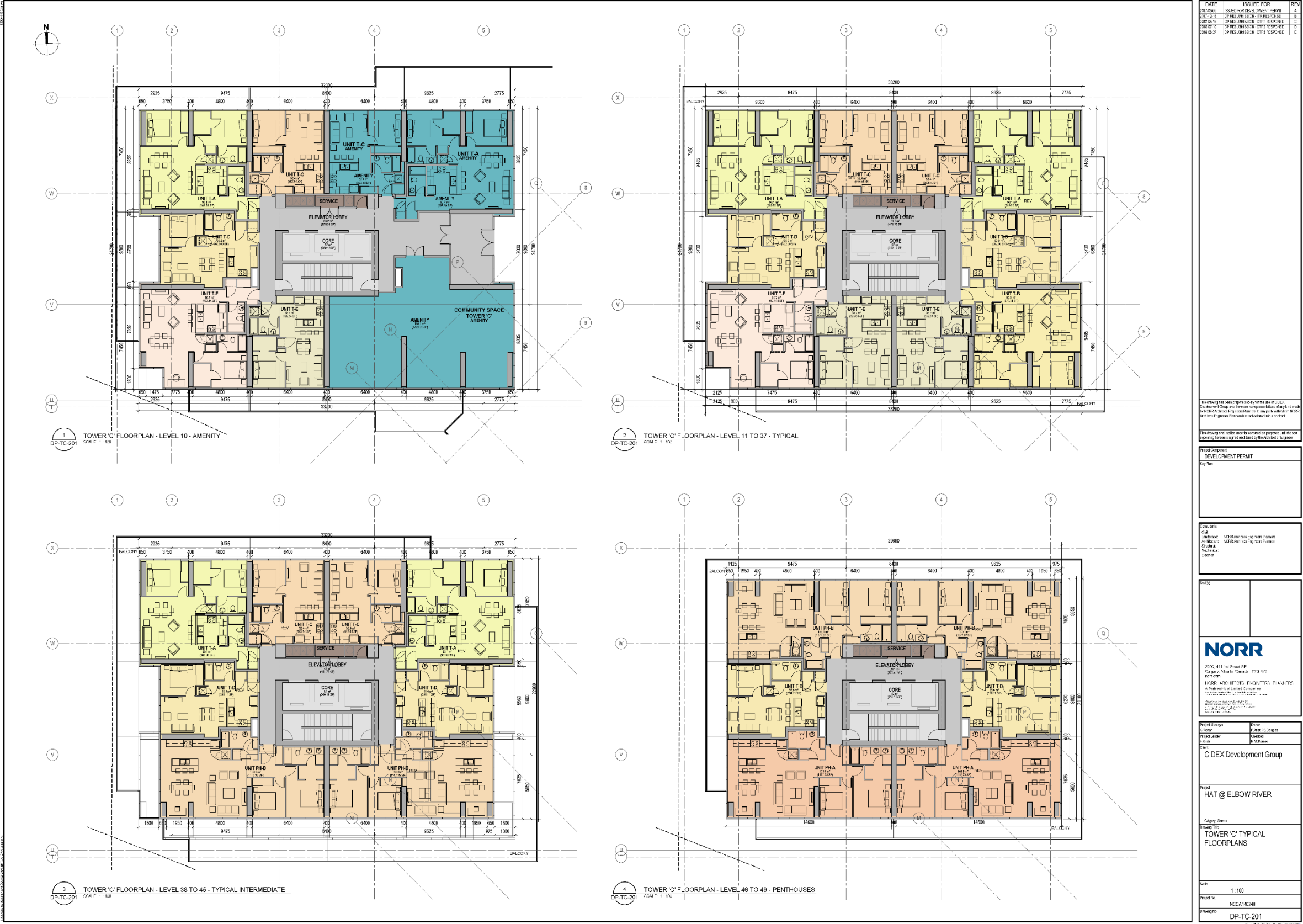
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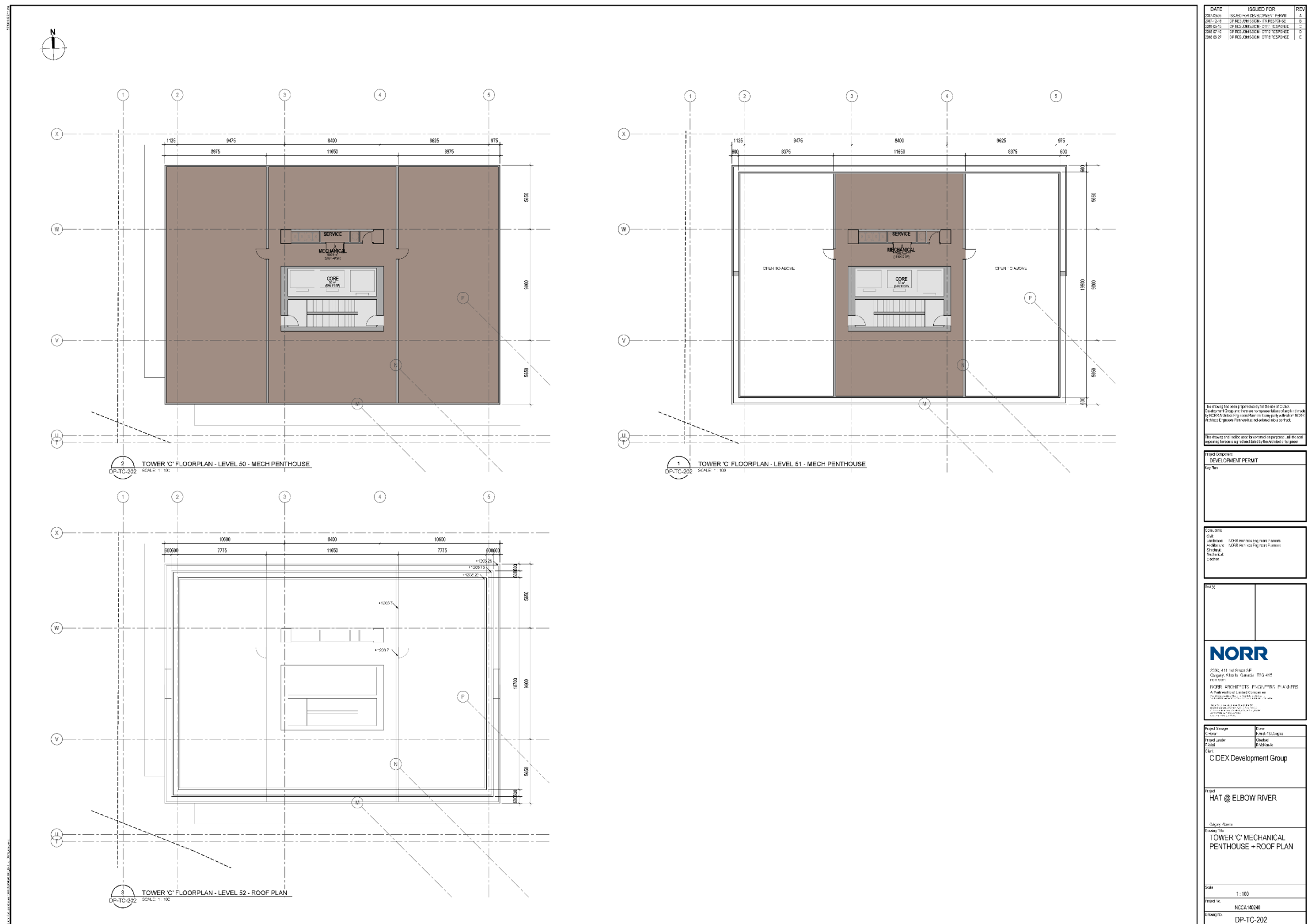
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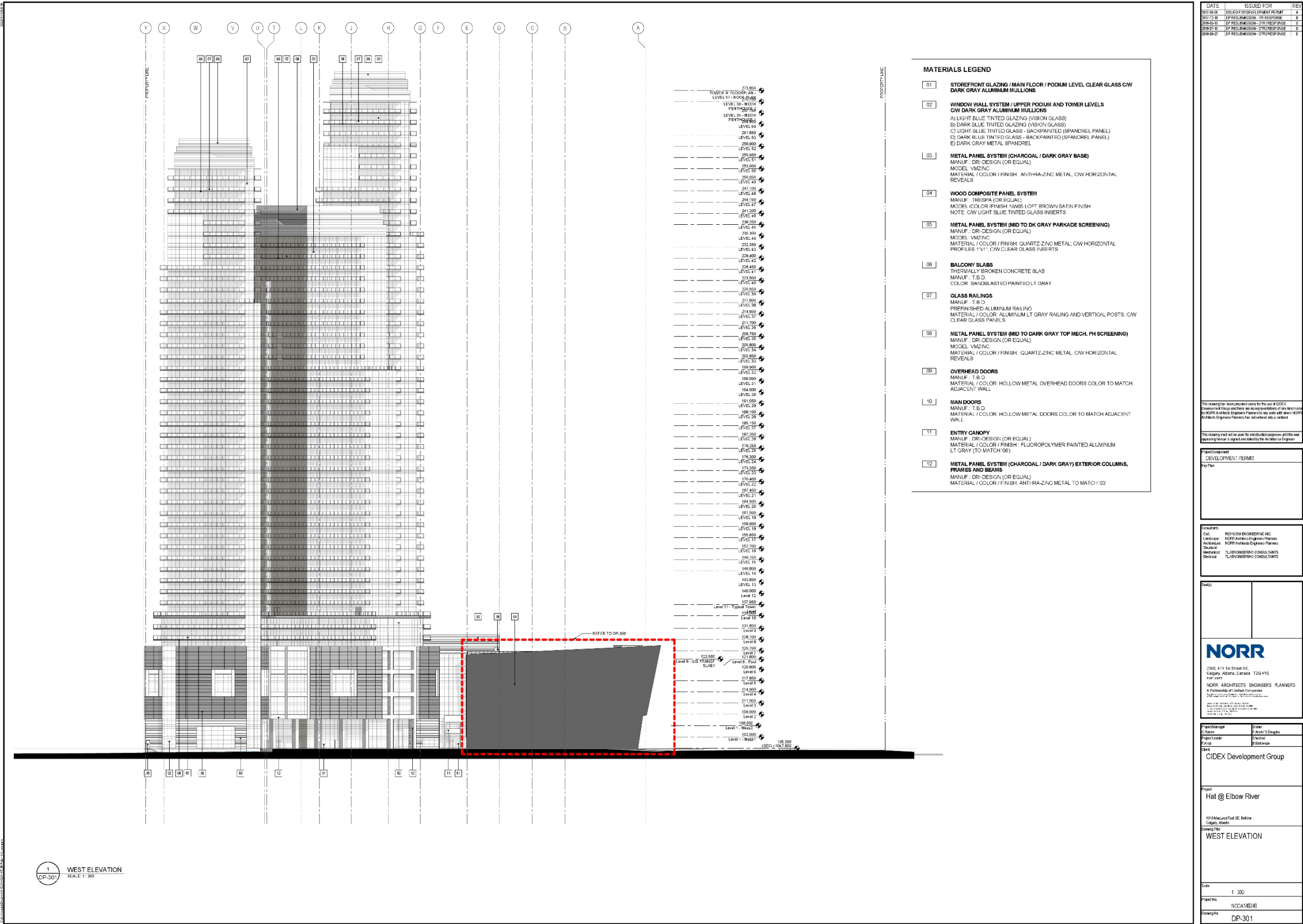
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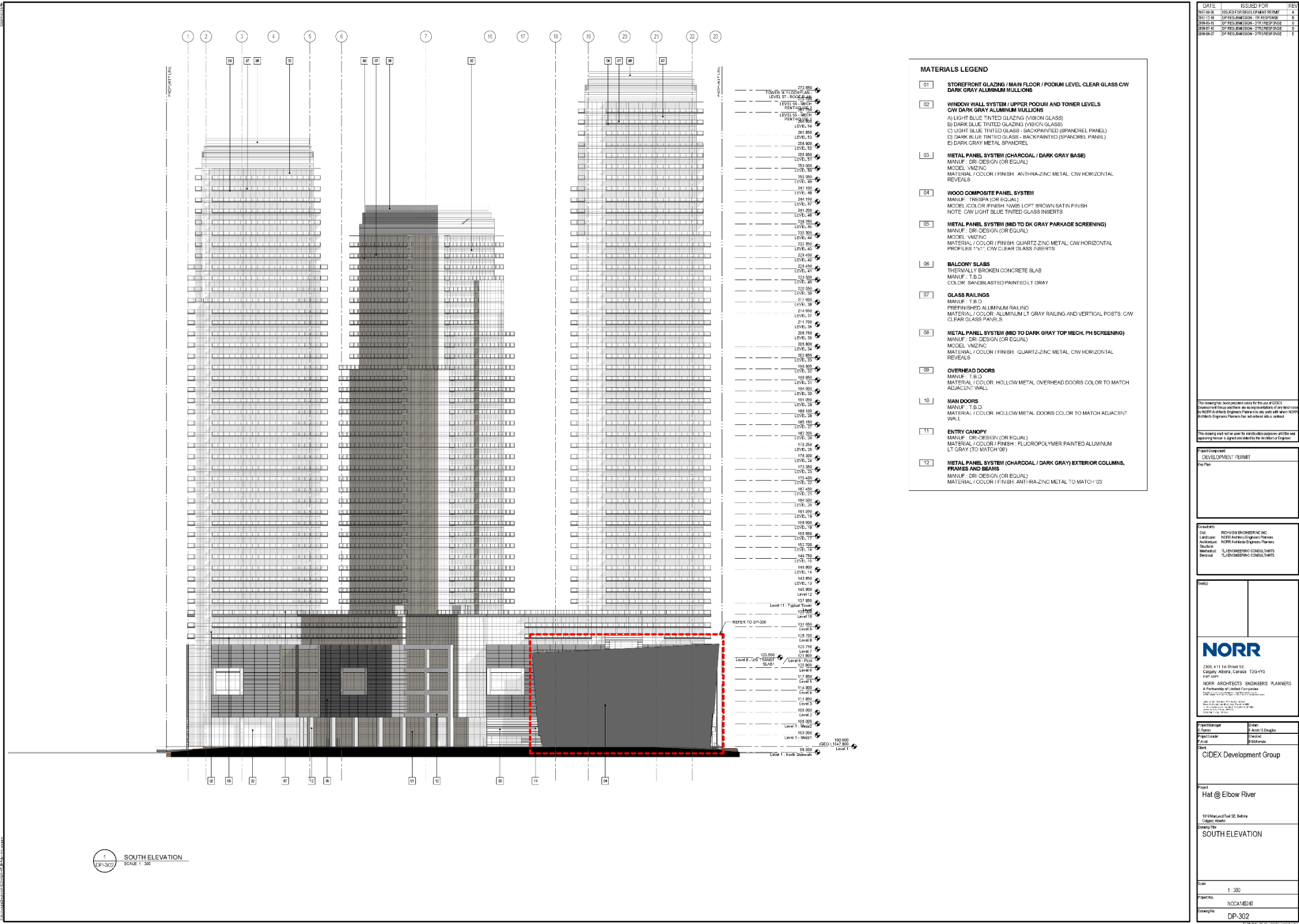
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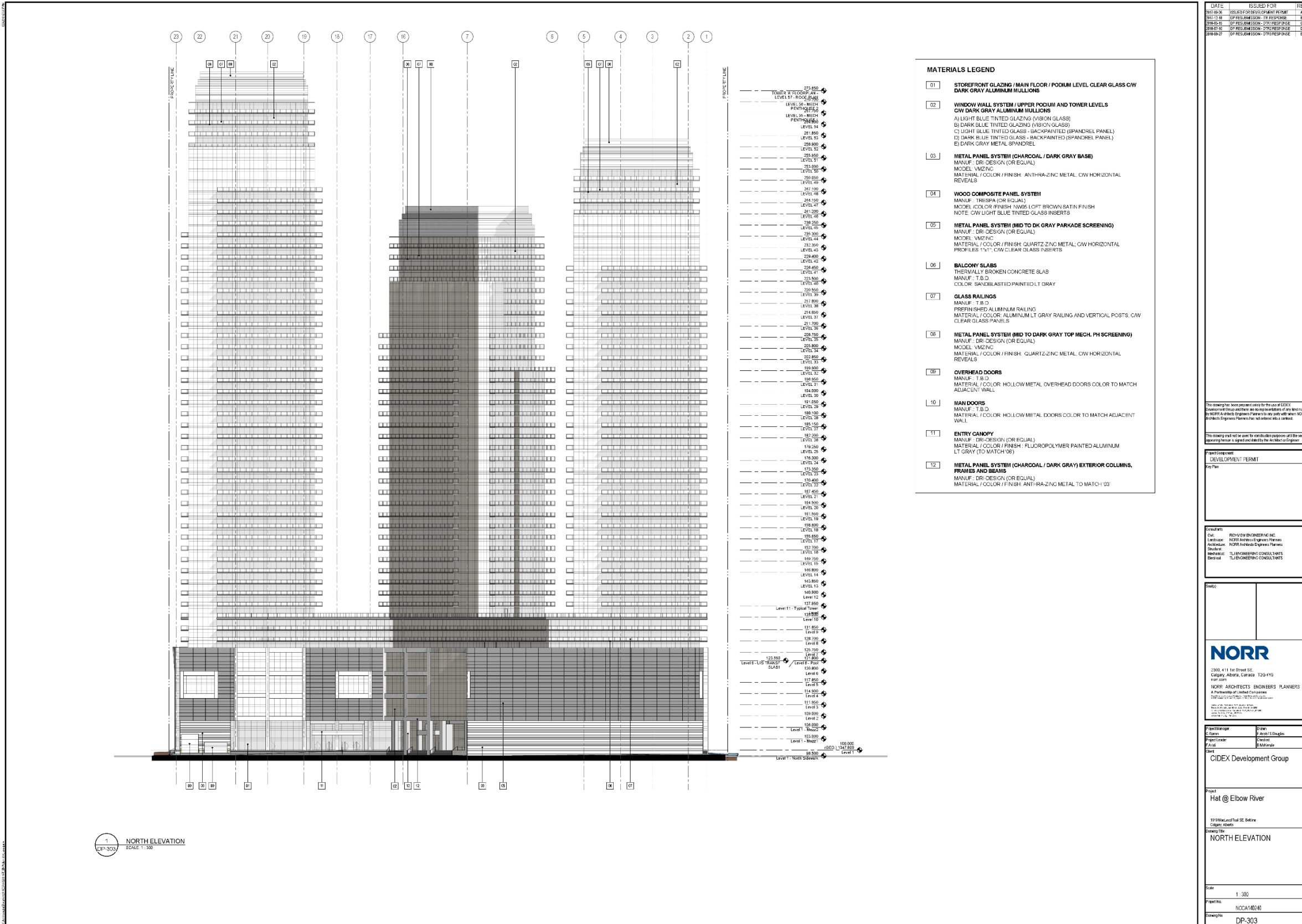
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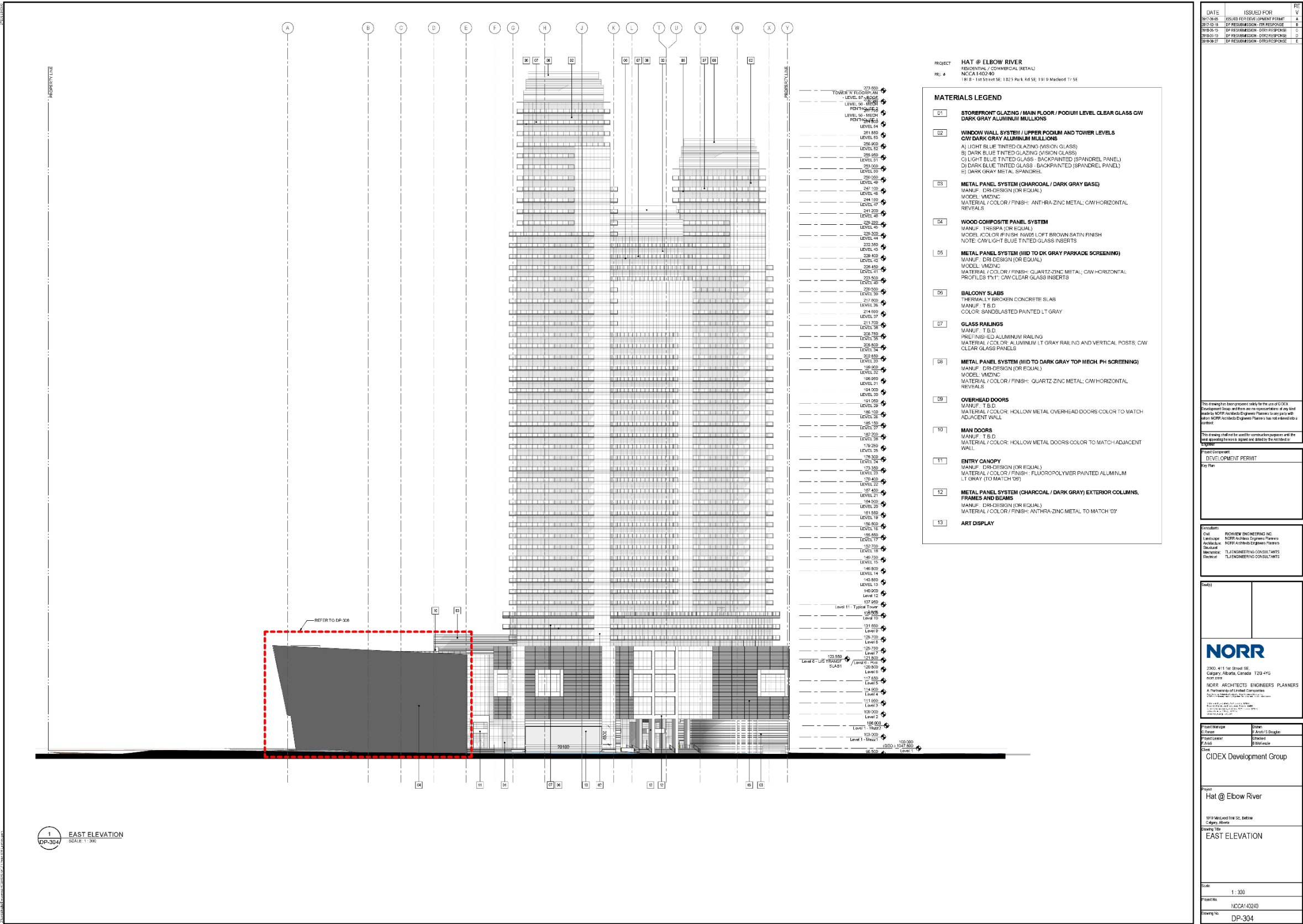
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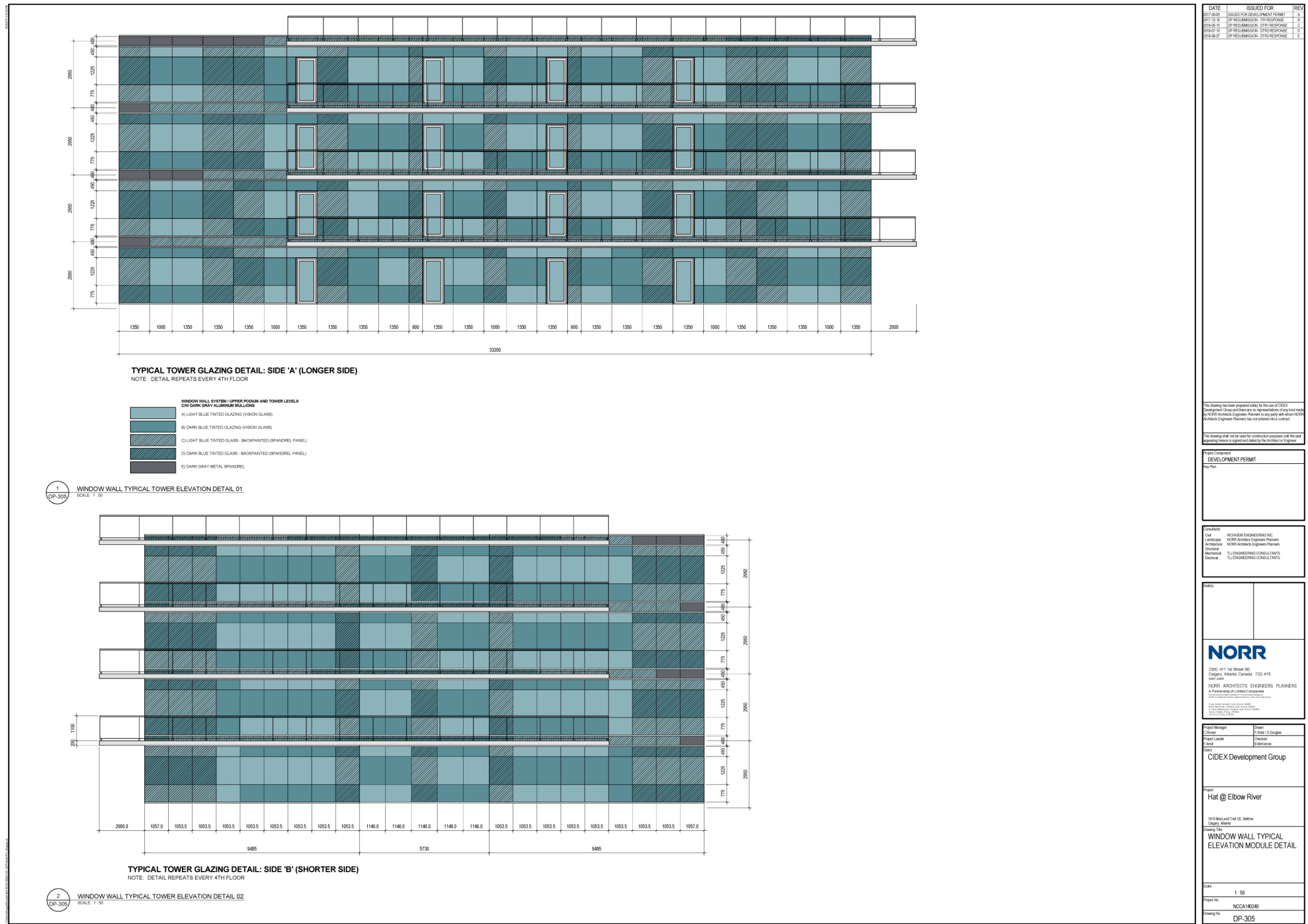
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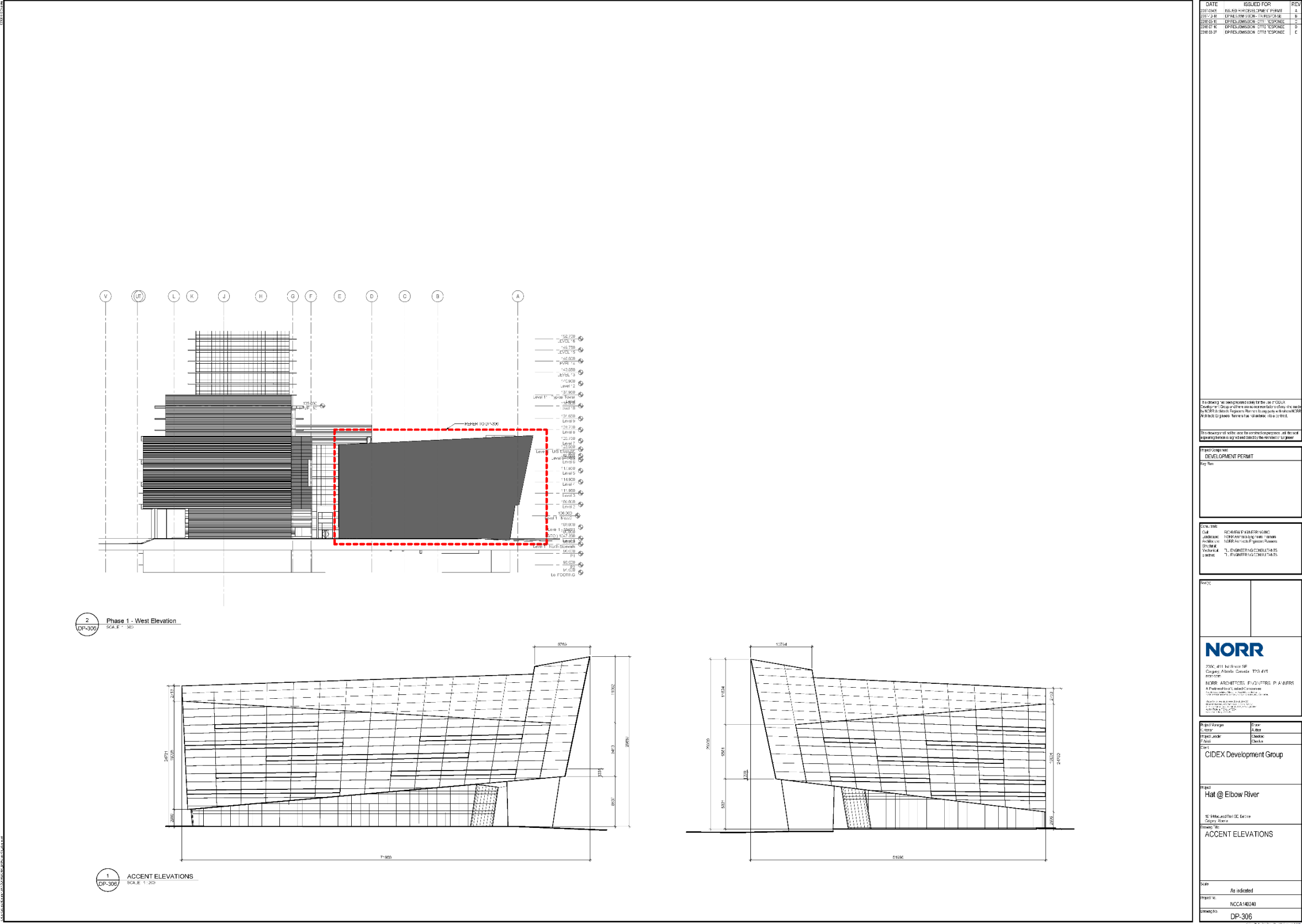
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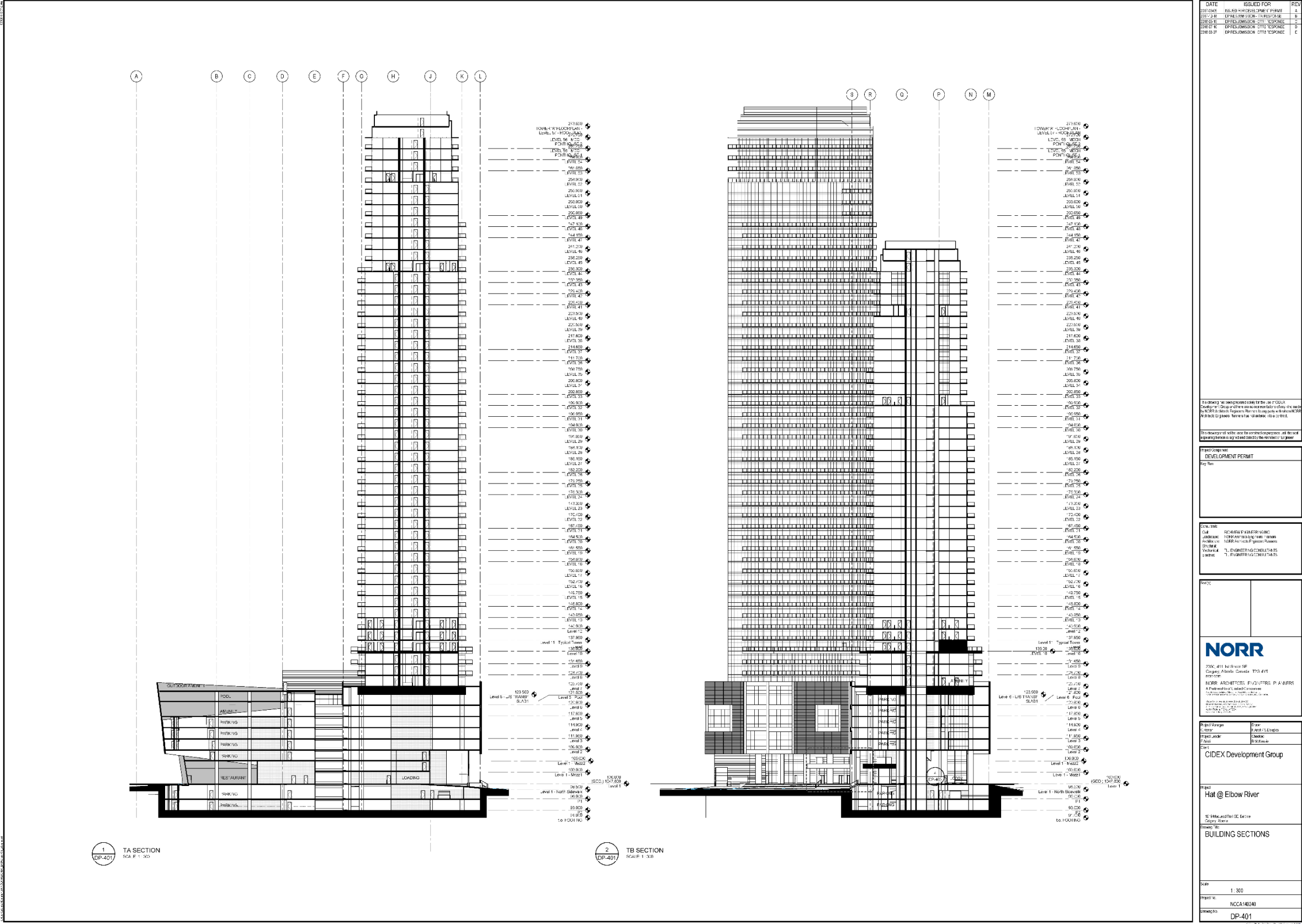
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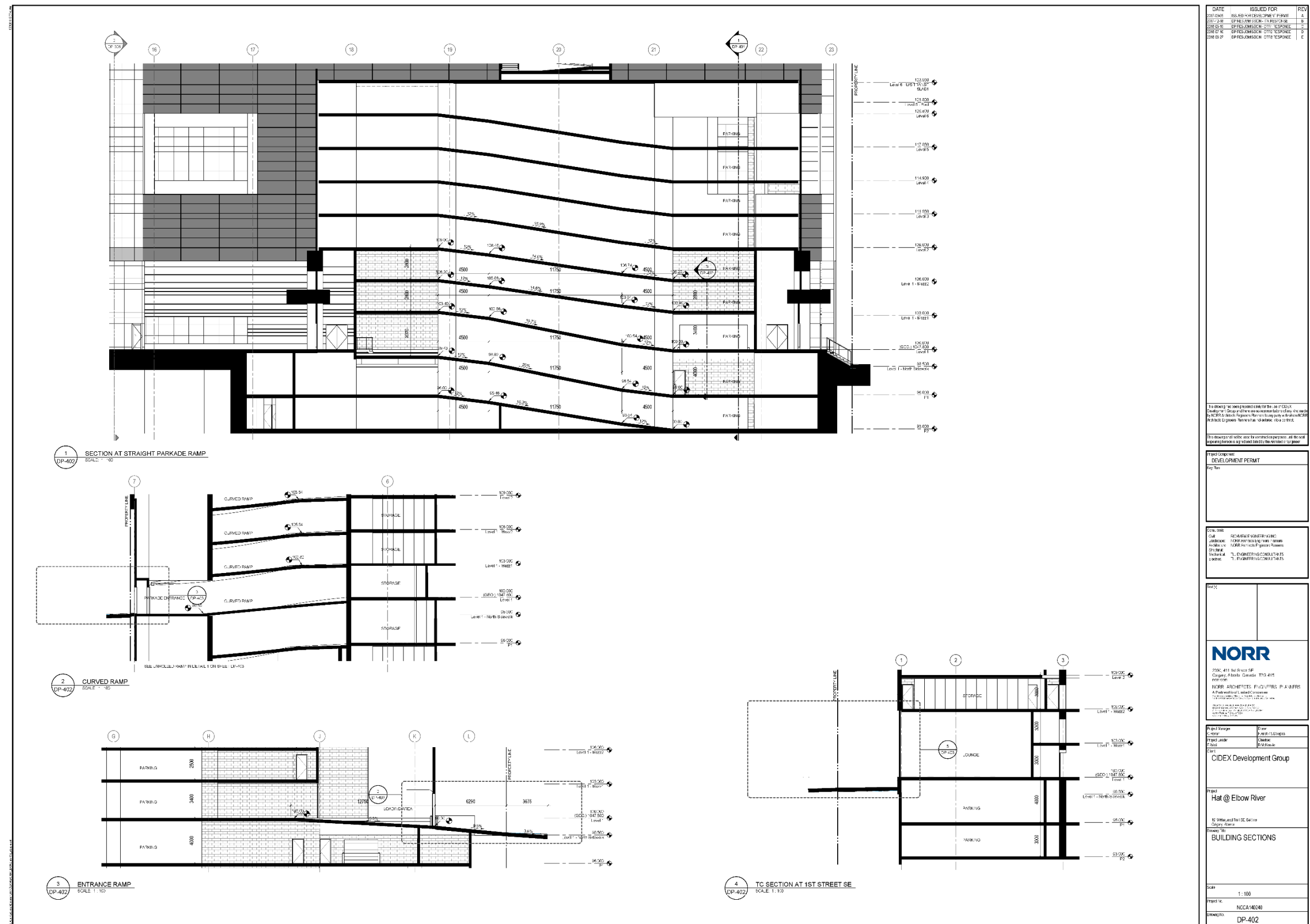
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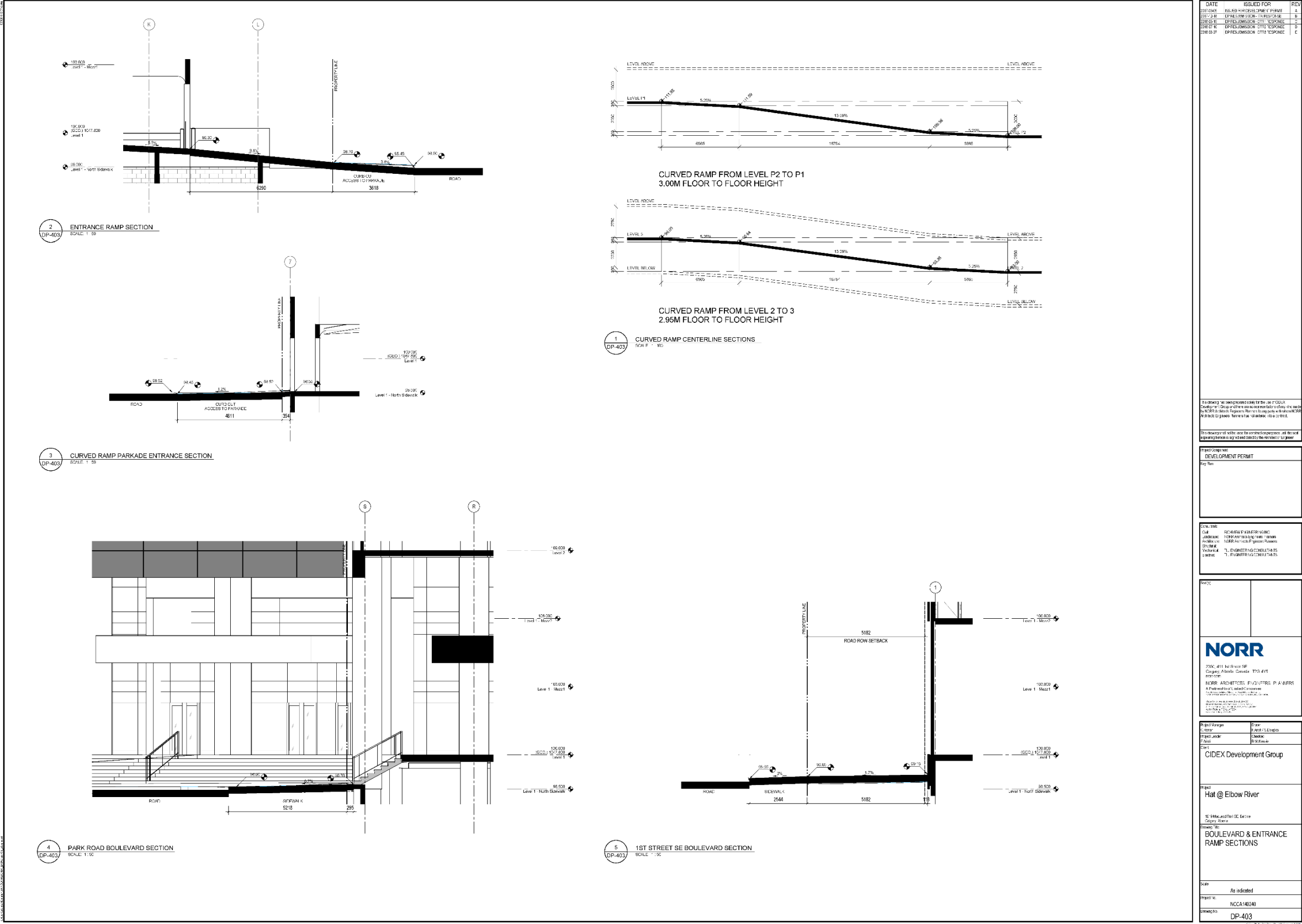
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Development Permit Plans

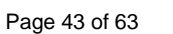


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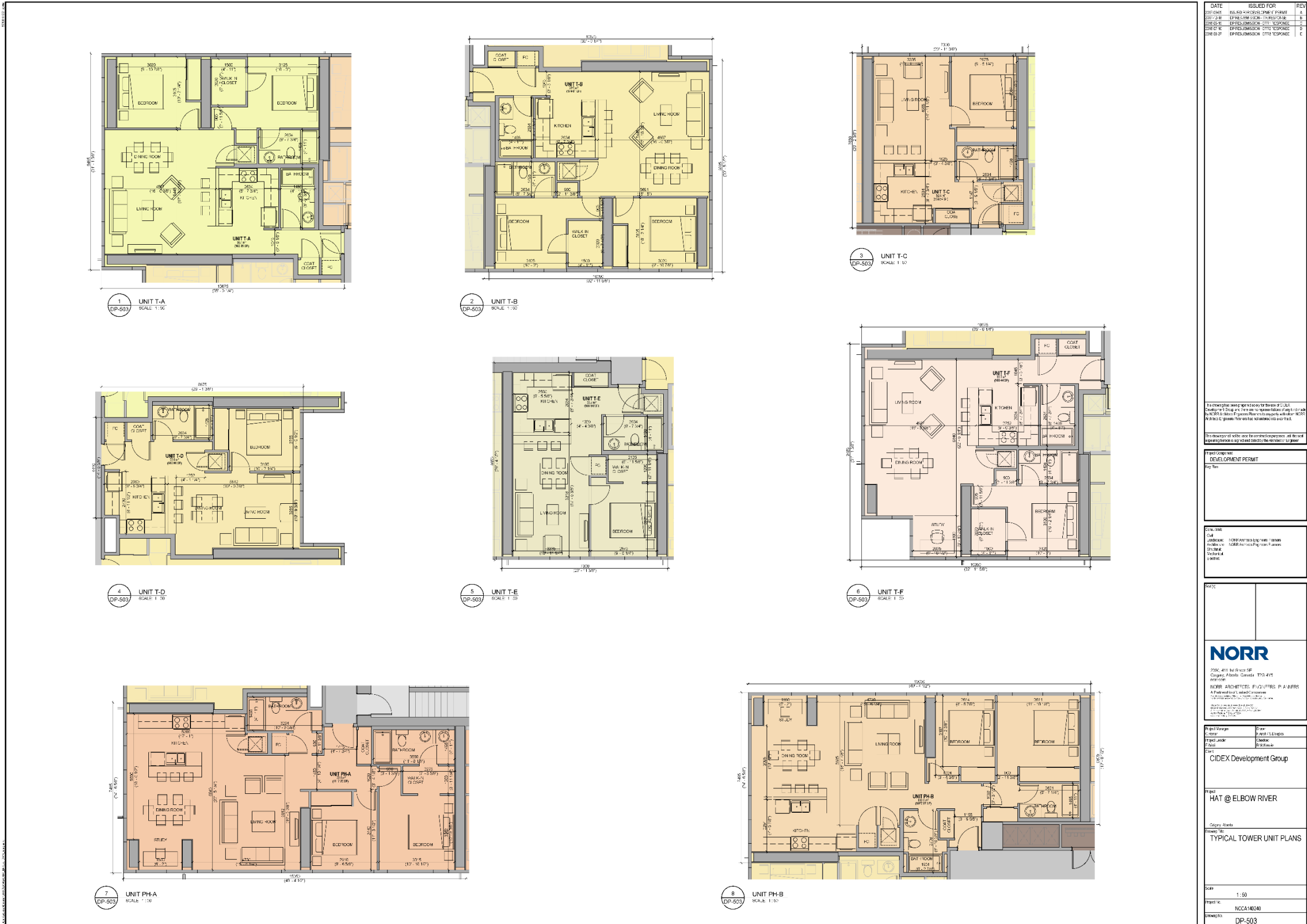


Development Permit Plans





Development Permit Plans



Development Permit Plans



DATE	ISSUED FOR	REV
2017-04-15	ISSUED FOR DEVELOPMENT PERMIT	A
2017-04-15	UPDATES TO DEVELOPMENT PERMIT	B
2017-04-15	UPDATES TO DEVELOPMENT PERMIT	C
2017-04-15	UPDATES TO DEVELOPMENT PERMIT	D
2017-04-15	UPDATES TO DEVELOPMENT PERMIT	E

This drawing is being issued for the purpose of obtaining a Development Permit. It is not to be used for any other purpose without the written consent of the City of Vancouver. The City of Vancouver is not responsible for the accuracy of the information provided in this drawing.

DEVELOPMENT PERMIT
Key: See

OWNER
CIC
ADDRESS: 1000 West 10th Avenue, Vancouver, BC V6H 1A5
CONTACT: 604-681-1111

DESIGNER
NORR
2500-4111 West 10th Avenue
Vancouver, BC V6H 1A5
NORR ARCHITECTS INC. (2017) P.A. 1000
NORR ARCHITECTS INC. (2017) P.A. 1000
NORR ARCHITECTS INC. (2017) P.A. 1000

PROJECT NAME
HAT @ ELBOW RIVER

PROJECT TYPE
TYPICAL TOWER 3 BEDROOM UNIT PLANS

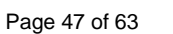
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As indicated

PROJECT NO.
NCCA-160240

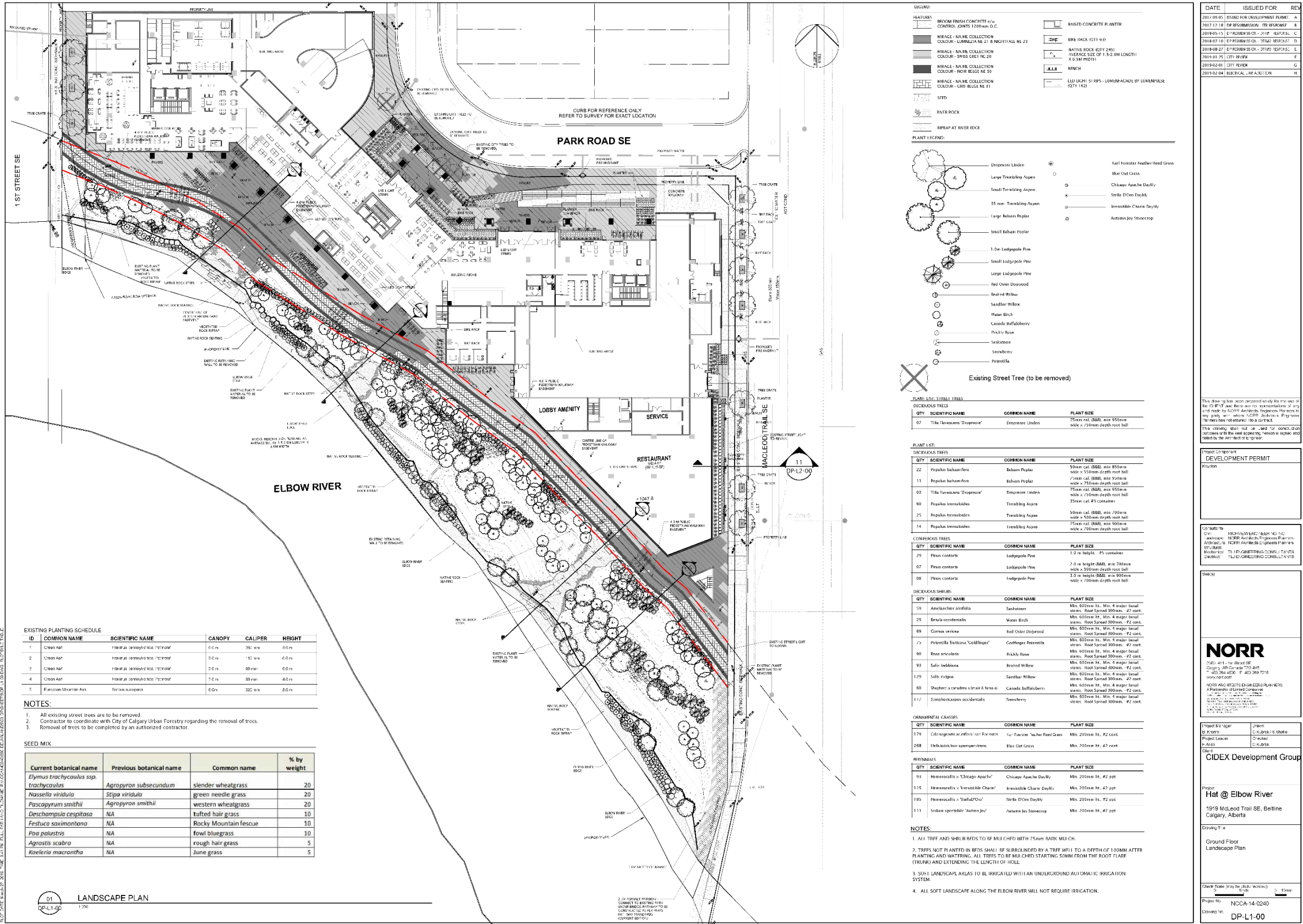
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DP-504

Development Permit Plans

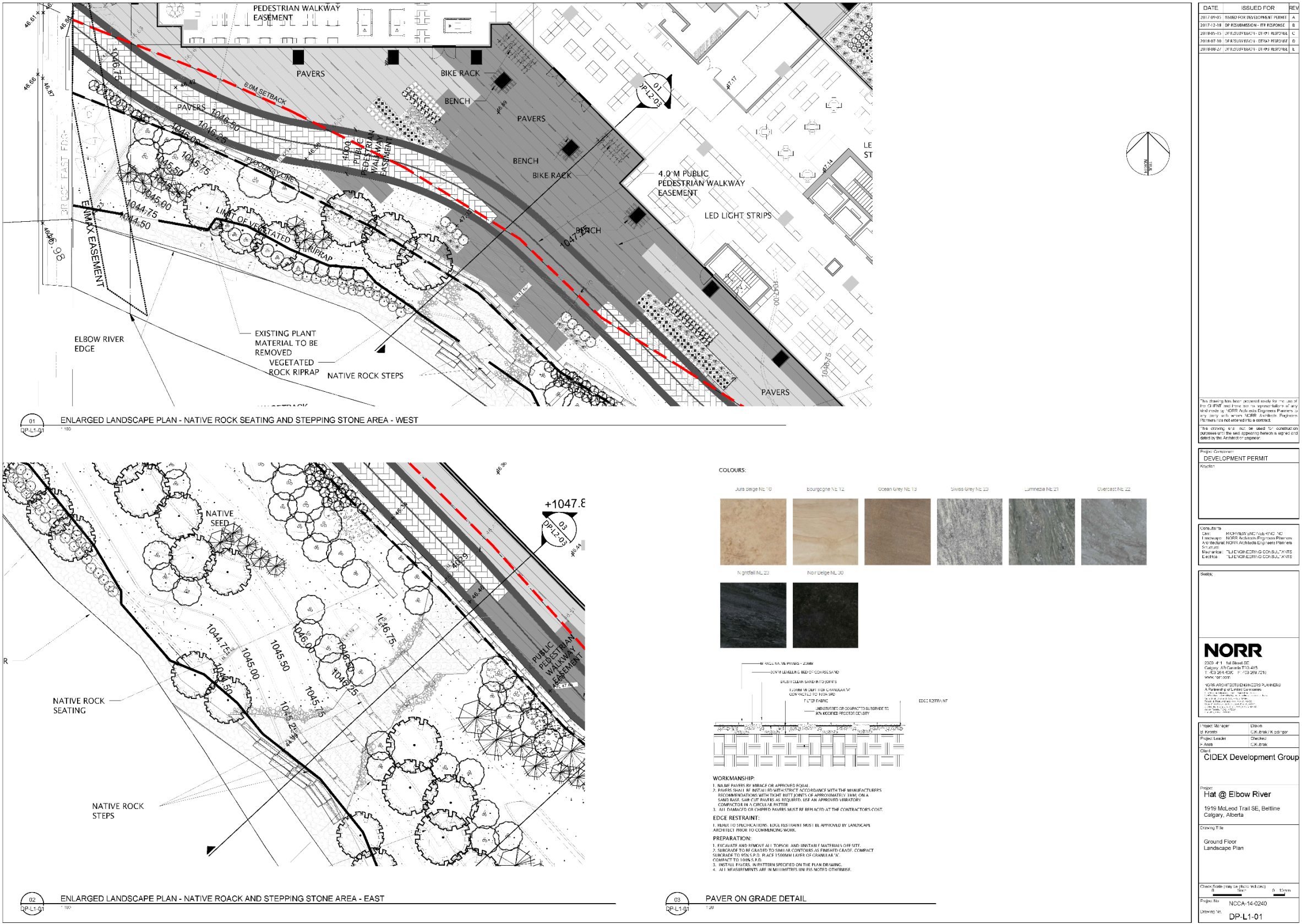




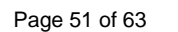
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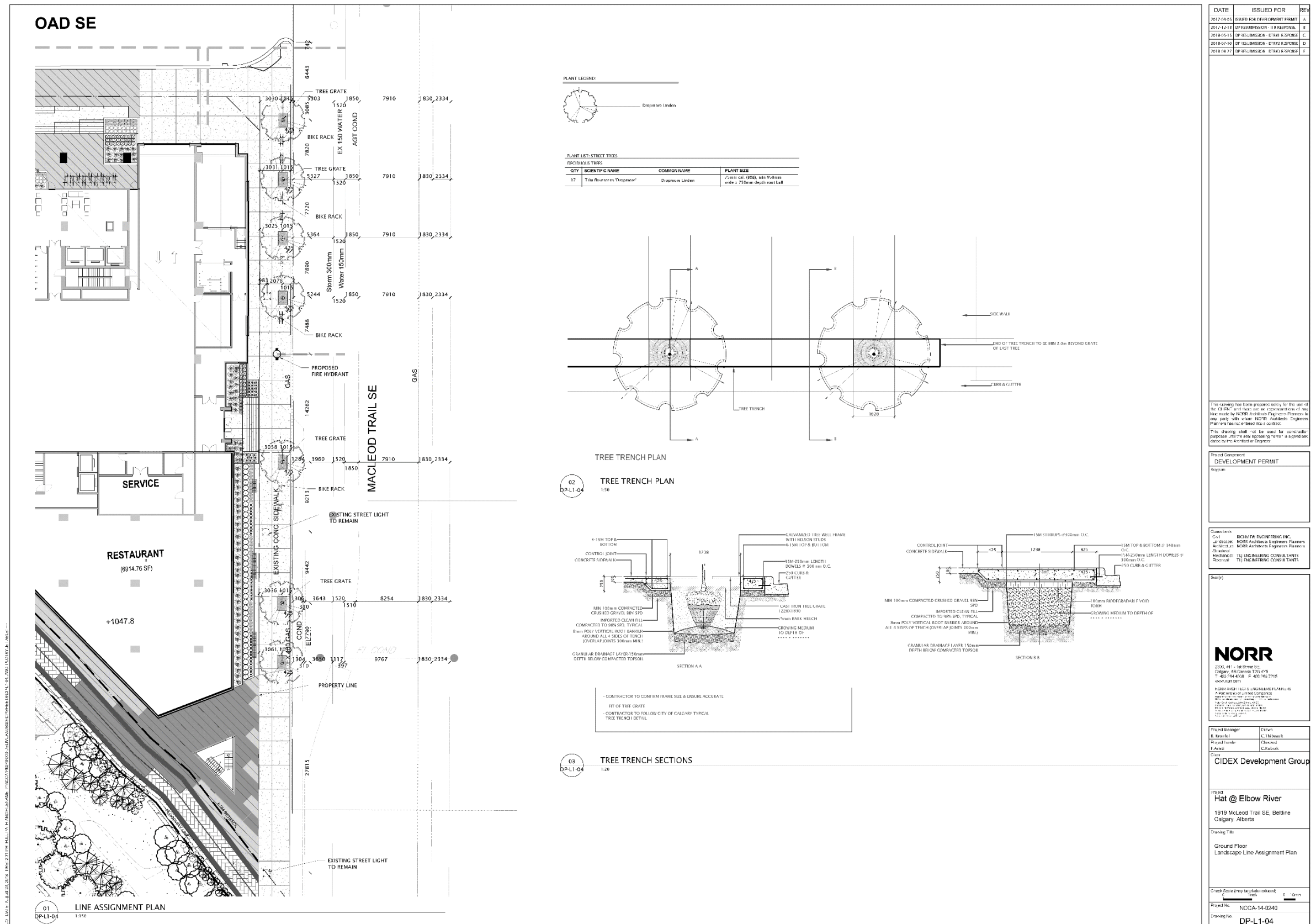
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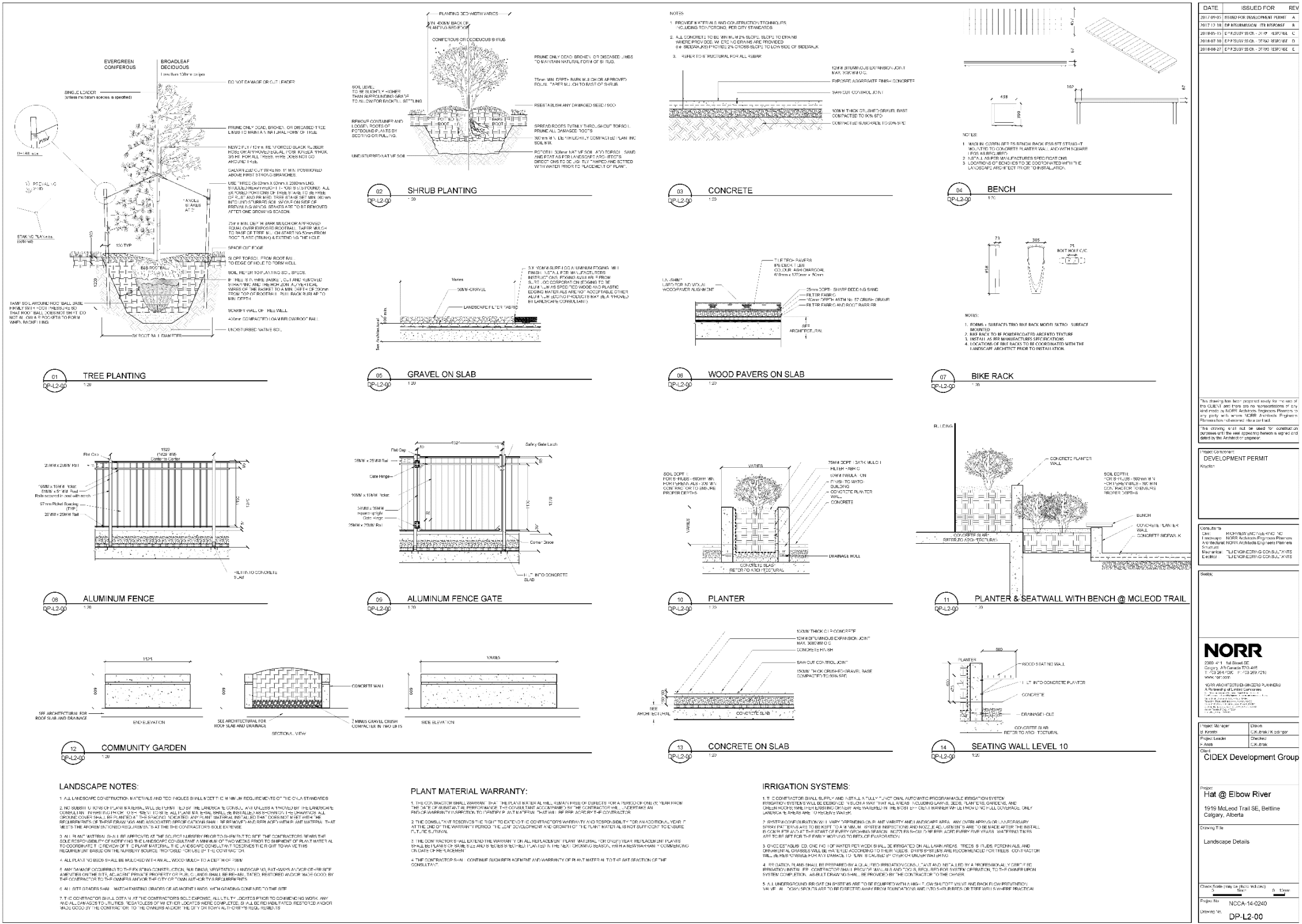
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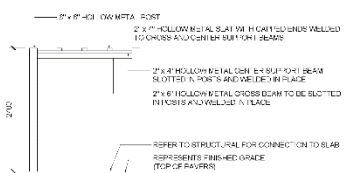
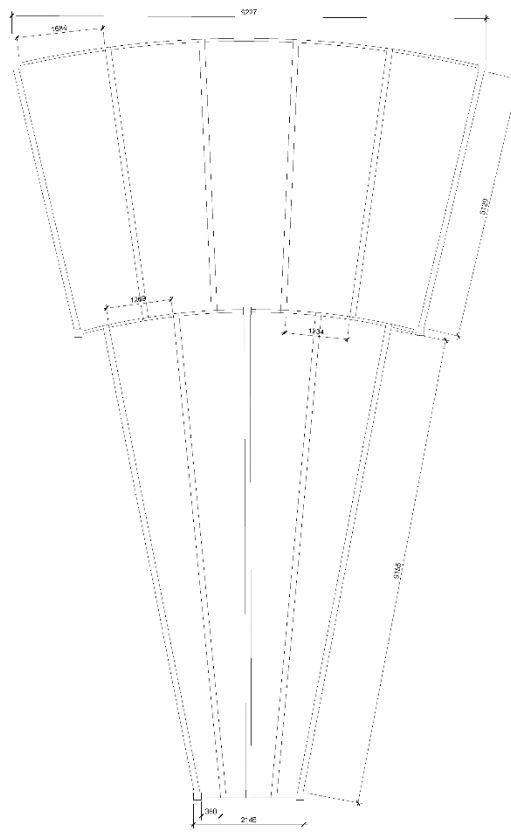
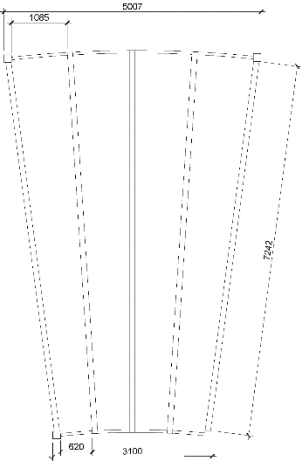
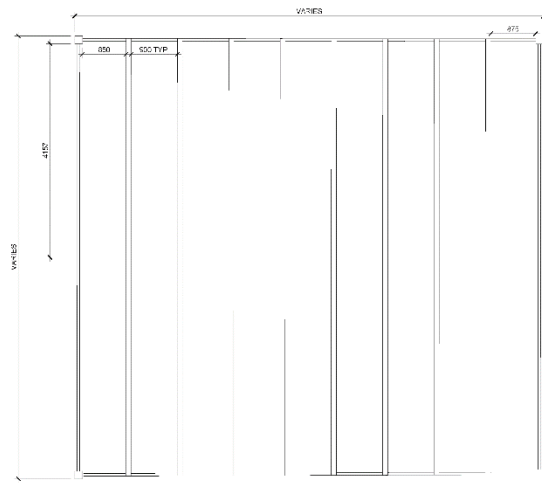
Development Permit Plans



Development Permit Plans

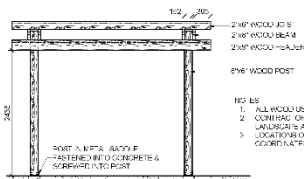
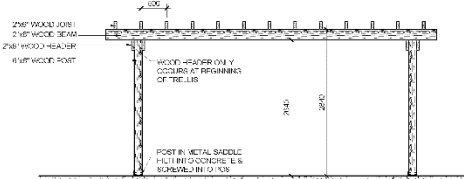
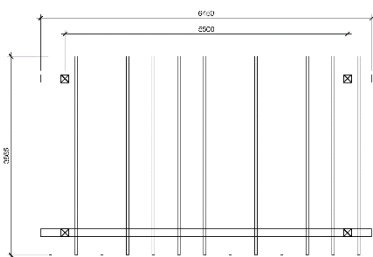


Development Permit Plans



01 METAL PERGOLA - LEVEL 11
DP.L2.02 1/0

02 METAL PERGOLA - LEVE 07
QP-L2-02 1/42



04 PERGOLA - YOGA ROOM - LEVEL 11
DP-L2-02 1.40

DATE	ISSUED FOR	REV
2017-09-05	ISSUED FOR DEVELOPMENT PERMIT	A
2017-12-18	DP RESUBMISSION - ITR RESPONSE	B
2018-05-15	DP RESUBMISSION - CITRER RESPONSE	C
2018-07-10	DP RESUBMISSION - CITRER RESPONSE	D
2018-08-27	DP RESUBMISSION - CITRER RESPONSE	E

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Project Component	DEVELOPMENT PERMIT
Keyplan	

Consultants	
Civil:	RICHVIEW ENGINEERING INC.
Landscape:	NORR Architects Engineers Planners
Architectural:	NORR Architects Engineers Planners
Structural:	
Mechanical:	TLJ ENGINEERING CONSULTANTS
Electrical:	TLJ ENGINEERING CONSULTANTS

Steps:

NORR

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Calgary, AB Canada T2G 4Y6
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www.torcon.com

NORR ARCHITECTS ENGINEERS PLANNERS
A Partnership of Linked Communities
NORR Architects Engineers Planners, Inc.
10000 Wilshire Blvd., Suite 1000, Los Angeles, CA 90024
Tel: 310/206-1000, Fax: 310/206-1001
www.norr.com

Project Manager R. Krenfel	Drawn C. Kutzback / K. Edinger
Project Leader F. Aried	Checked C. Kutzback
Client CIDEX Development Group	

Hat @ Elbow River
1919 McLeod Trail SE, Beltli
Calgary, Alberta

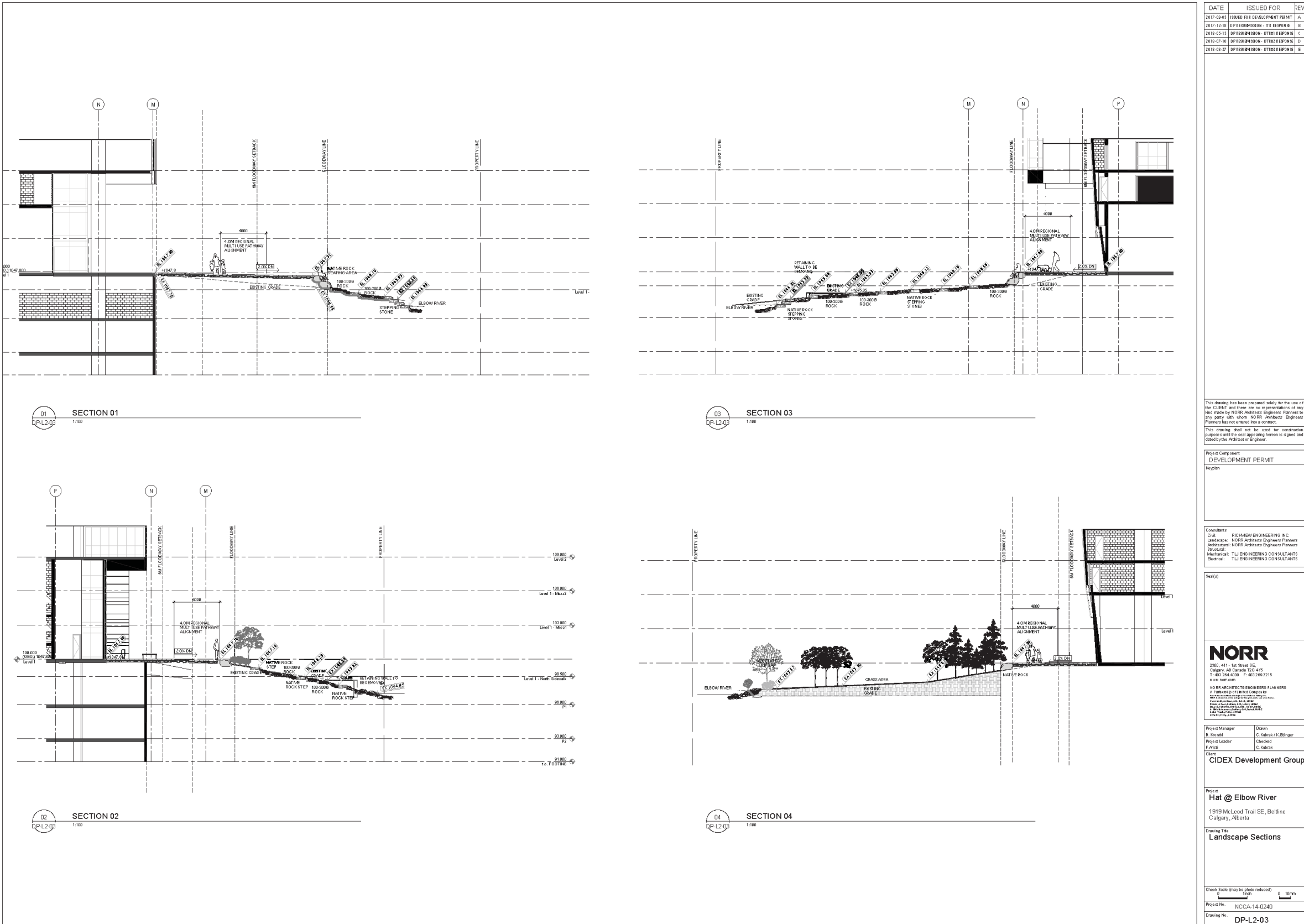
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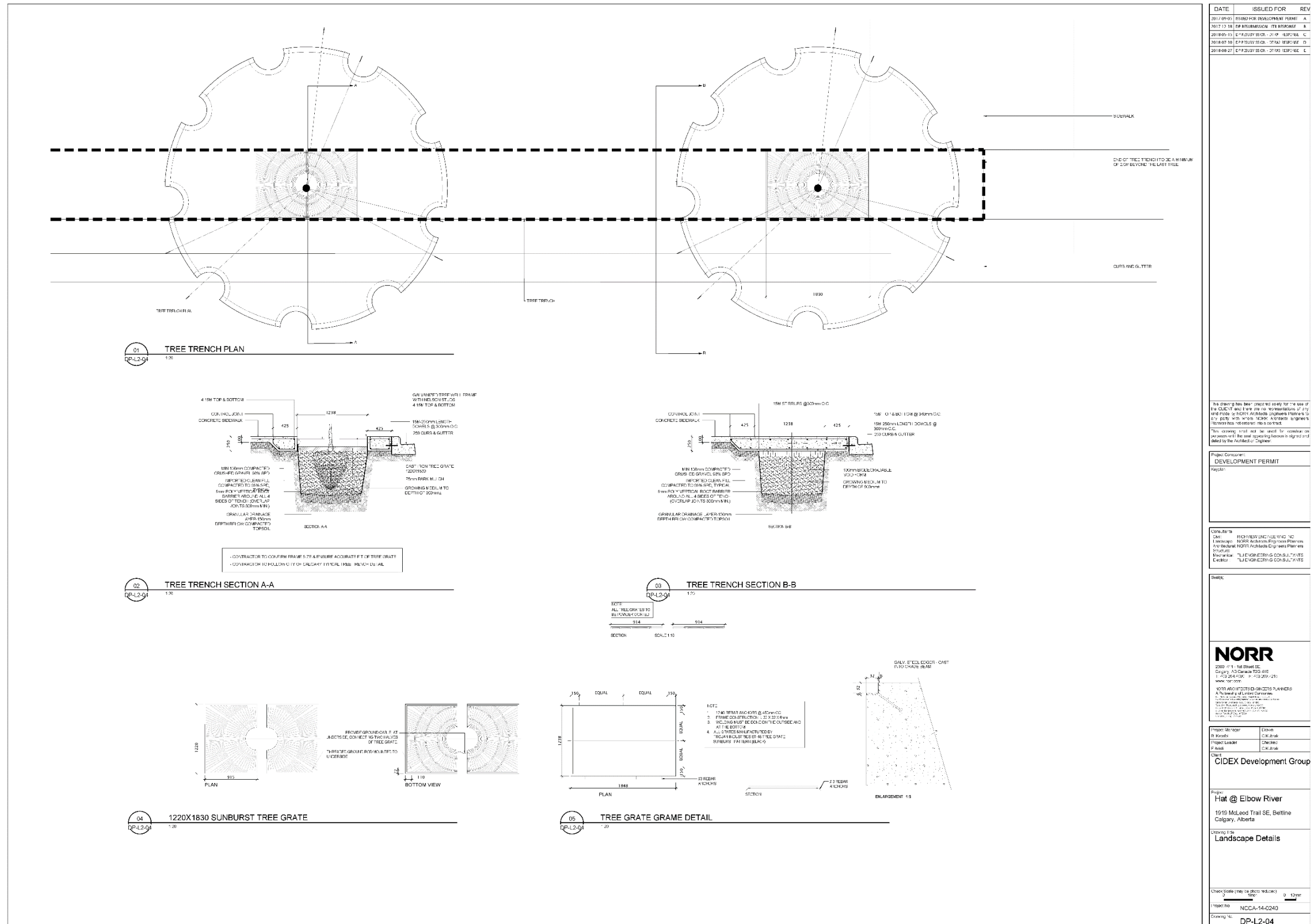
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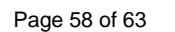
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Development Permit Plans

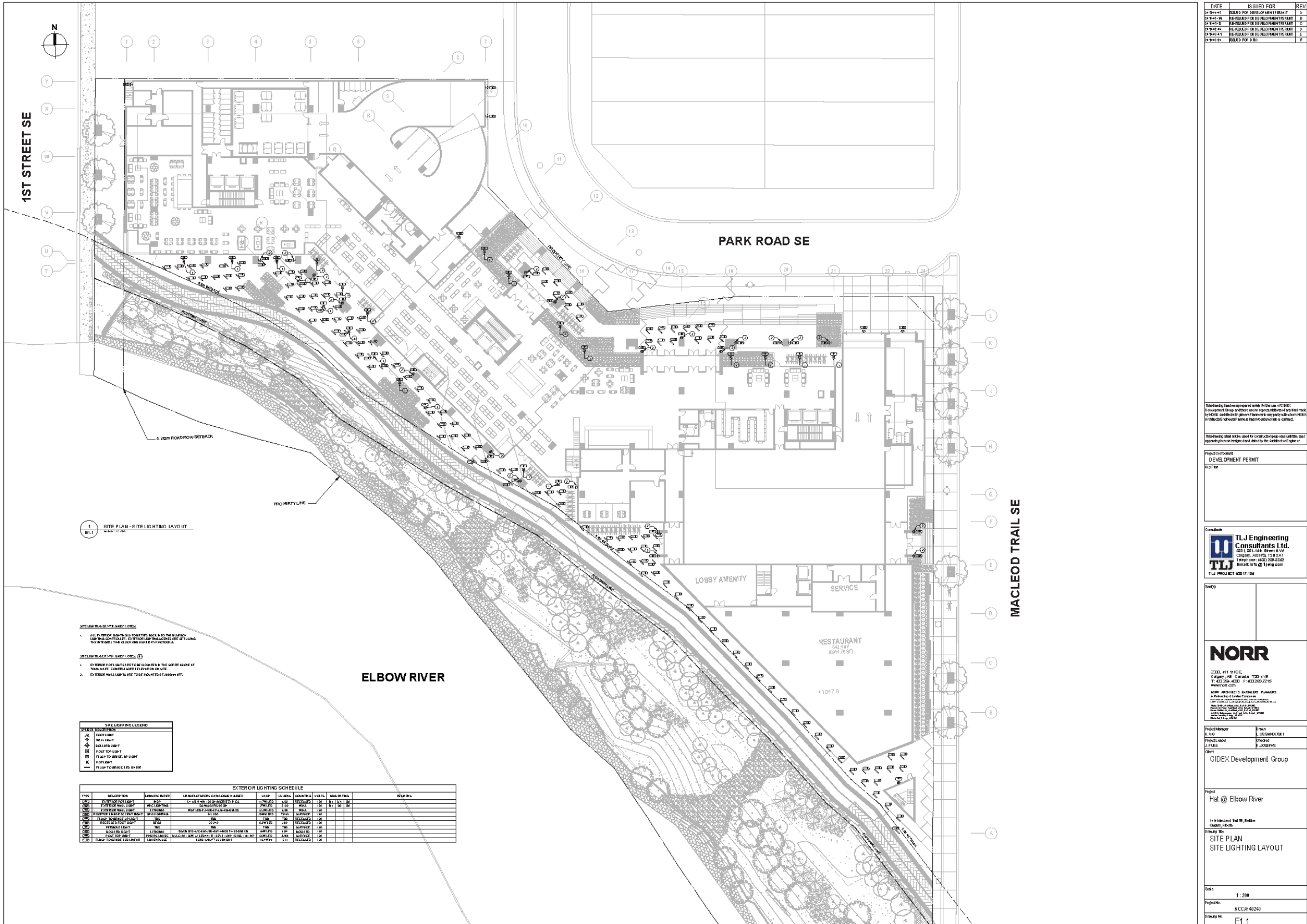


Development Permit Plans

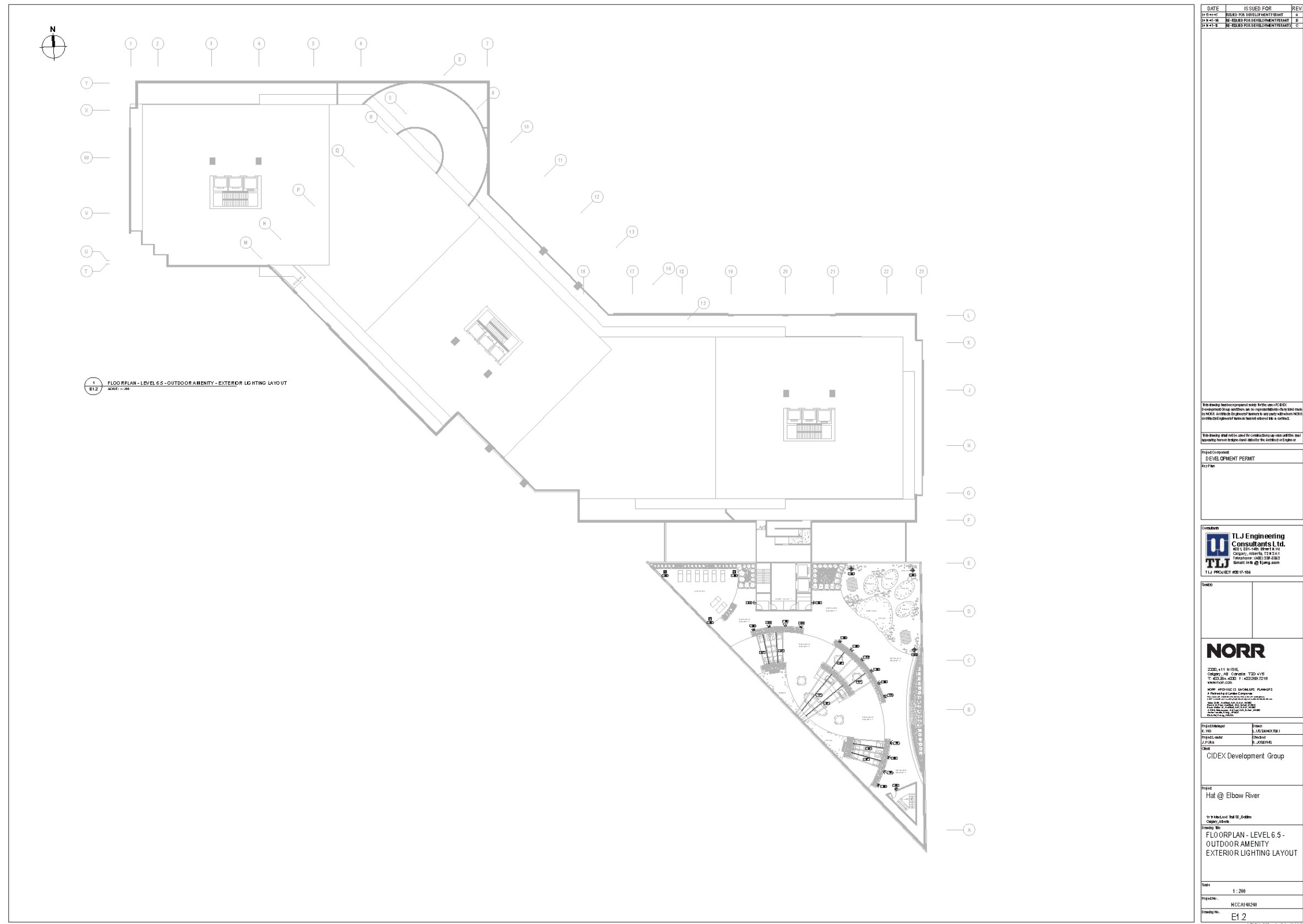




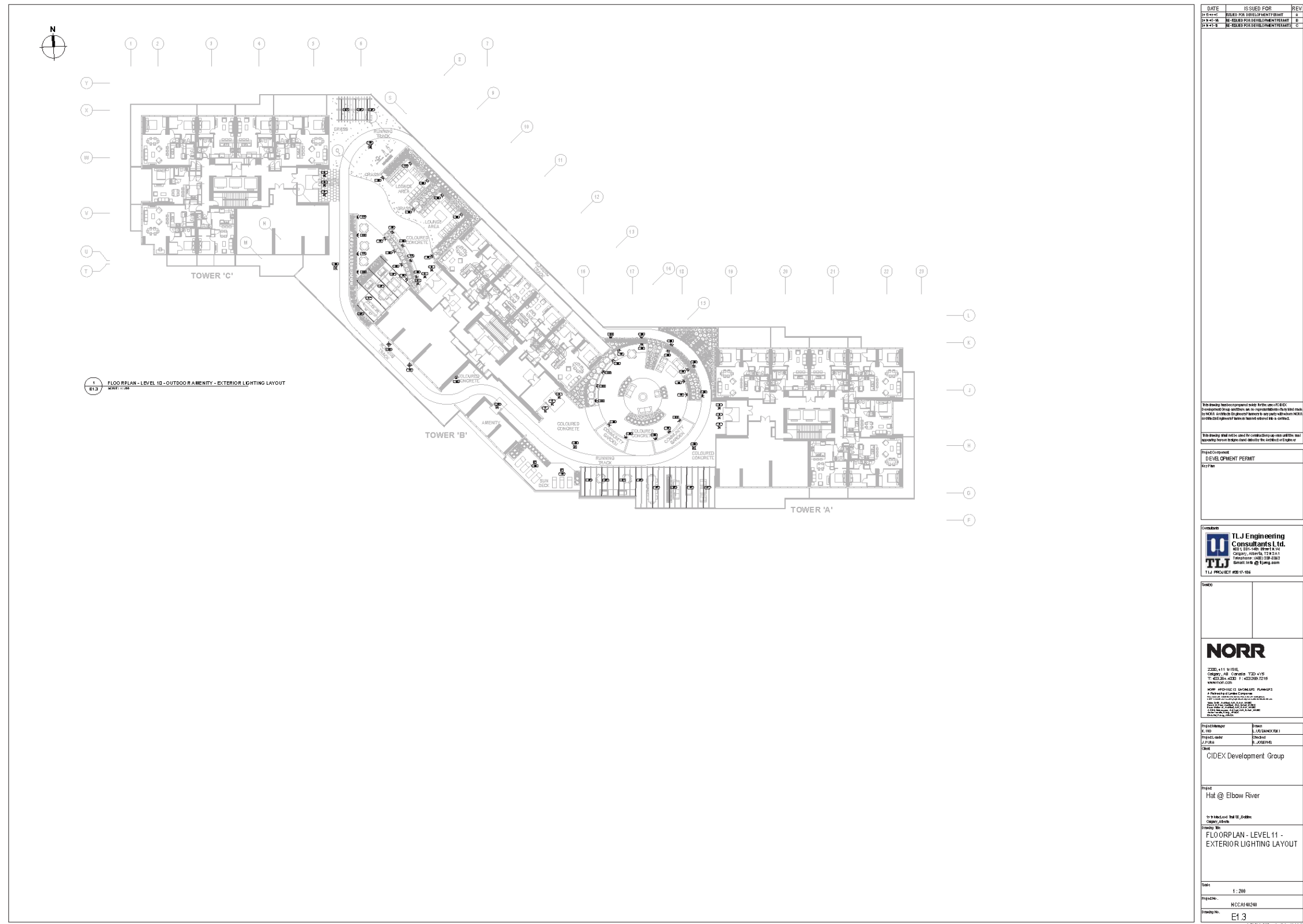
Development Permit Plans



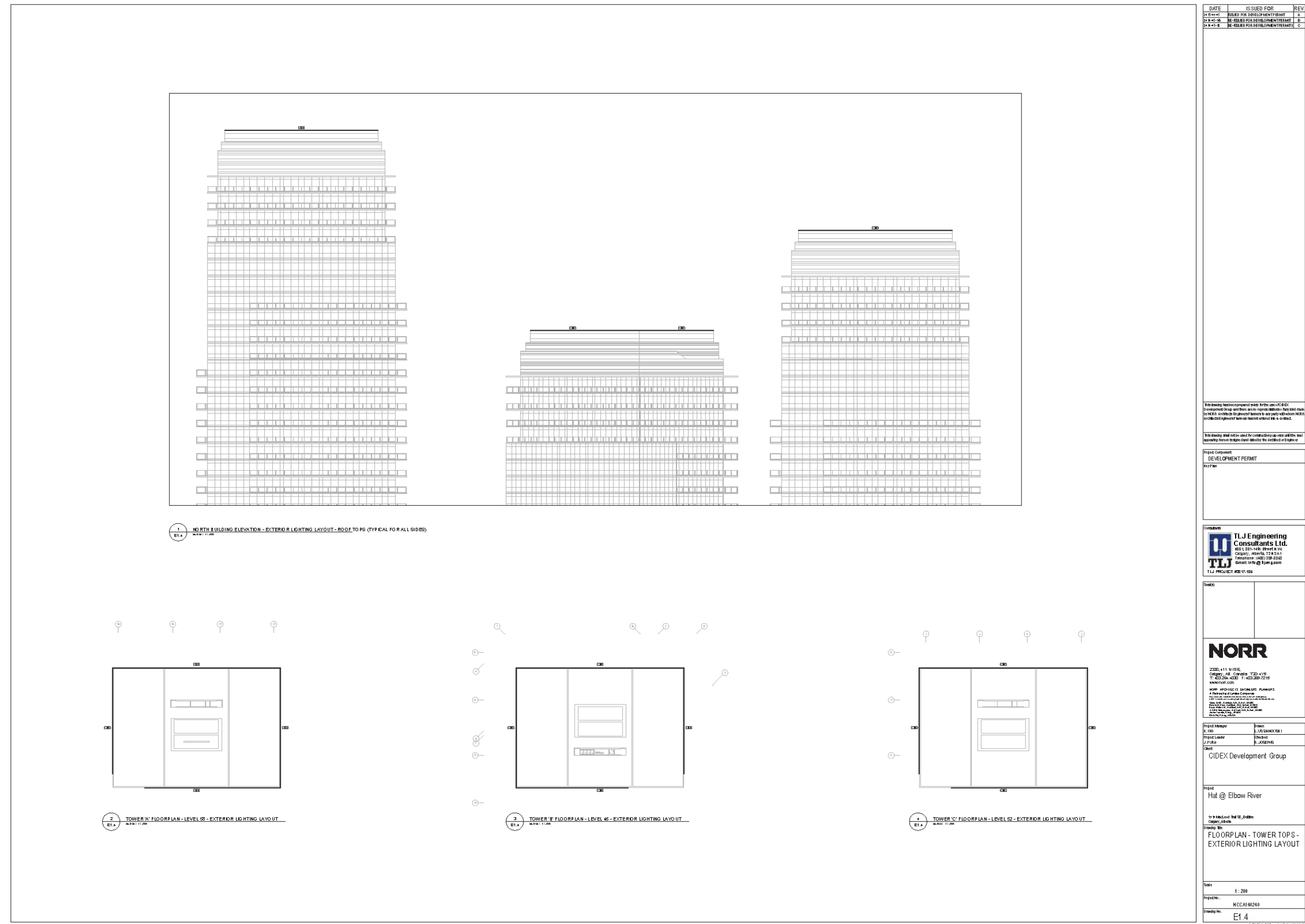
Development Permit Plans



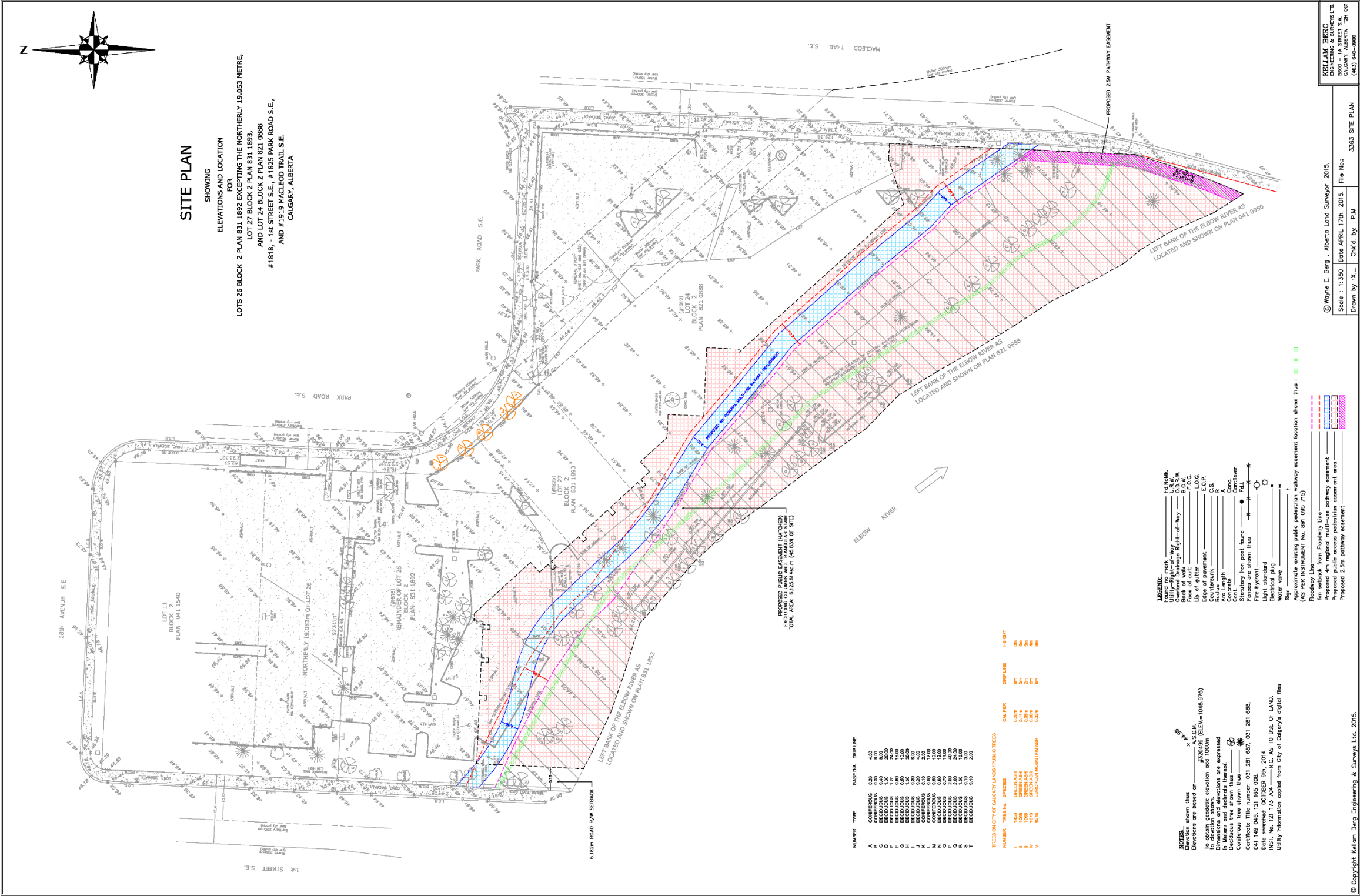
Development Permit Plans



Development Permit Plans



Development Permit Plans



Conditions of Approval

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a total of five (5) complete sets of Amended Plans (file folded and collated) to the Planning Generalist that comprehensively address the Prior To Release conditions of all Departments as specified below.

In order to expedite the review of the Amended Plans, please include the following in your submission:

- a. All of the plan set(s) shall highlight all of the amendments.
- b. Four (4) detailed written response(s) to the Conditions of Approval document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.

Please ensure that all plans affected by the revisions are amended accordingly.

2. Amend the plans to provide an exterior entrance to the Restaurant Lounge on the southwest corner of the ground floor on the southern wall that connects and opens to the pathway to the south.
3. Amend the plans to provide additional exterior entrances to the CRUs on the southern wall that open to the pathway to the south.
4. A Development Agreement for the Publicly Accessible Private Open Space shall be executed and registered on title in conformity with all reports, plans and materials submitted to and approved by the Development Authority, to address:
 - a) Details of responsibilities for and construction of \$2,872,908 (based on 1.062 FAR) of improvements on the subject site within the proposed Publicly Accessible Private Open Space area identified on drawing DP-109-PAPOS;
 - b) Registration of a public access easement over the Publicly Accessible Private Open Space, including perpetual maintenance agreement for the 4.0 metre wide multi-use pathway and 2.5 metre wide asphalt pathway; and
 - c) Details of materials and public art mural (valued at \$51,925) within the Publicly Accessible Private Open Space.
5. A Development Agreement for the affordable housing units shall be executed and registered on title in conformity with all reports, plans and materials submitted to and approved by the Development Authority, to address details of responsibilities for and construction of \$6,234,128 (based on 2.448 FAR) of 22 units of affordable housing totaling 1,930.56 square metres in floor area.

Conditions of Approval

6.
 - a) A Housing Agreement shall be executed and registered on title in conformity with all reports, plans and materials submitted to and approved by the Development Authority and Calgary Housing, to address details of the terms and conditions of the provision of Affordable Housing Units, as per the Development Agreement and the Letter of Understanding dated August 17, 2018. The agreement shall be to the satisfaction of Calgary Housing and the City Solicitor.
 - b) A formal partnership agreement shall be executed with a bona fide non-profit non-market housing provider in conformity with all reports, plans and materials submitted to and approved by the Development Authority and Calgary Housing. The agreement shall be to the satisfaction of Calgary Housing and the City Solicitor.
7. If applicable, remit payment for any outstanding amount of bonused floor area not covered through improvements to the Publicly Accessible Private Open Space or Affordable Housing Units which may be bonused for through a financial contribution to the Beltline Community Investment Fund at a rate of \$270 multiplied by the amount of outstanding bonused floor area, in accordance with the Bonus standards of Part 10 of Land Use Bylaw 1P2007.
8. Apply for a separate signage Development Permit that outlines the details of the public art display/mural along the MacLeod Trail SE frontage.
9. Amend the plans to provide an interim elevation plan for the western facades of Phase A and Phase C using the same metal panel system materials as indicated on the elevation drawings for the rest of the podium facades.
10. Amend the Phasing Plan to include the Publicly Accessible Private Open Space (PAPOS) shown on sheet DP-109-PAPOS as part of Phase A for the following portions of the PAPOS:
 - a. All portions of the 4.0 metre wide regional pathway;
 - b. All portions of the riverbank landscaping south of the 4.0 metre wide regional pathway; and
 - c. All portions of the PAPOS landscaping immediately adjacent Phases A and D as shown on the plans dated 2018 August 27.

In addition, amend the Phasing Plan to incorporate the remainder of the PAPOS area to be included within the respective areas of Phases C and D.

Development Engineering:

11. Provide letters from ATCO, TELUS, ENMAX and SHAW confirming that they have no objections to the proposed development scope.

Conditions of Approval

Note: This may include the relocation of existing utility lines within the subject property.

12. Consolidate the subject parcels. Submit a copy of the registered plan and certificate of title, confirming the consolidation of subject parcels onto a single titled parcel, to the Development Engineering Generalist.
46. Submit a Required Fire Flow (RFF) calculation, which must be prepared by a qualified professional and submitted to Water Resources for review, as per the Water Supply For Public Fire Protection guide (by the Fire Underwriters Survey). Submit (electronically) to WA-ResourcesDevelopmentApprovals@calgary.ca. Depending on the results, public mains may be required to be upgraded in order to provide the required fire protection to the site.

Per Water Resources, the available fire flow available in the adjacent City watermain is 15,000 LPM.

Contact the Planning Engineer, Development Planning, Water Resources at 403-268-4351 for further details.

13. Remit payment (**certified cheque**) for the infrastructure upgrades for the Centre City communities, in the amount of **\$341,804.70**, to Development Engineering. This levy includes both the Centre City Utility Levy approved under the **Centre City Utility Levy Bylaw 38M2009** and an amount approved by Council for community recreation, transportation, parks upgrading, and greenways. The amount identified above is determined by using **\$4,710.00 per meter of site frontage** (typically applied to adjacent avenues and / or east-west roadways only) for the proposed development (72.57m charged along Park RD SE).
14. **After the Development Permit is approved but prior to its release**, the landowner shall execute an Off-Site Levy Agreement for the payment of off-site levies pursuant to Bylaw 2M2016.

Should payment be made prior to release of the development permit, an Off-Site Levy Agreement will not be required. To obtain the off-site levy agreement, contact the Public Infrastructure Coordinator, Calgary Approvals Coordination at 403-268-6739 or email offsitelevy@calgary.ca.

15. Follow the submission requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control (Erosion and Sediment Control Reports and Drawings: Technical Requirements) and either submit the required (2) copies of an Erosion and Sediment Control (ESC) Report and Drawings or the required (2) copies of a Written Notice to Development Engineering, for review and acceptance by Water Resources. The City of

Conditions of Approval

Calgary Guidelines for Erosion and Sediment Control can be accessed at:
www.calgary.ca/ud (under publications).

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Guidelines for Erosion and Sediment Control* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Professional Engineer (P. Eng.), Professional Licensee (P.L. Eng), Professional Agrologist (P. Ag.) or Certified Professional in Erosion and Sediment Control (CPESC). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

If you have any questions, contact 3-1-1. A Service Request (SR) will be created for the Erosion Control Team in Water Resources.

16. Submit three (3) sets of the Development Site Servicing Plan details to Development Servicing, Inspections and Permits, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact developmentservicing2@calgary.ca for additional details.

For further information, refer to the following:

Design Guidelines for Development Site Servicing Plans

http://www.calgary.ca/PDA/pd/Documents/urban_development/publications/DSSP2015.pdf

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/pd/Documents/development/development-site-servicing-plan.pdf>

Note:

The DSSP is a prior to release requirement as the proposed scope will warrant public utility relocations which impacts development servicing and warrants the need for developer to enter into indemnification agreement for the new utility work and associated development site servicing.

17. The proposal to construct public infrastructure, Sanitary sewer and Watermain relocation(s), development service connections, associated manholes and associated rehabilitation (surface work) within City rights-of-way requires the developer to execute an Indemnification Agreement to the satisfaction of the Manager, Urban Development.

To initiate circulation and approval of the Work that will form part of the Indemnification Agreement, submit construction drawings online using your existing VISTA account at calgary.ca/vista. At the time of submission of the construction drawings, the following items shall also be submitted:

Conditions of Approval

- a. An 8 1/2" x 11" site plan indicating the construction boundaries.
- b. Indicate who will be party to the agreement, provide contact information and a certificate of title for adjacent lands associated with the construction of the infrastructure.
- c. A detailed description of the Work that will form part of the Indemnification Agreement (cubic metres of asphalt and/or concrete, pipe diameters for sanitary, storm and watermains and their respective lengths in linear metres) within the City right of way.
- d. A detailed cost estimate for the scope of Work including GST prepared by the contractor.

Note:

In order for Water Resources to support the proposed development scope and anticipated building footprint, the existing 200mm (VCT) sanitary main and 300mm (PVC) water main that both run through the site shall be relocated at the developer's expense, while adhering to the terms and conditions of the above noted agreement.

18. The concrete foundation shown in section 1 of the landscape details sheet DP-L2-05 would qualify as a building, based on the Land Use Bylaw definition, and is prohibited in the floodway. For protection against scour, measures such as dis-articulated rock rip rap, boulder units and/or bioengineering protection are typically used. An alternate design for erosion protection should be proposed and supported by hydraulic and geotechnical analysis.

Transportation:

19. There is an existing catch basin with the storm back within the proposed driveway location on Park Road SE (See image below). Contact Water Resources in regards to relocation, or placement of an additional catch basin if required. Amend plans accordingly.

Conditions of Approval



NOTE: The applicant is to amend plans to show how this catch basin relocation is being accomplished (where it's being relocated, etc). Ensure this is completed on the next circulation.

20. Amend plans to clearly mark what class of bicycle parking is located where (class #1 is the inside- occupant, and class #2 is the outside- visitor), and provide more information on the class #1 bike lockers, such as size, and aisle width between the units. From the looks of the plans, the lockers look very hard to get a bicycle in and out without difficulty.

NOTE: The proposed class #1 bike lockers look adequate, but more information is required, such as aisle size between the lockers, size, door swing, etc. to ensure people can get their bikes in and out without difficulty. Also, ensure plans clearly differentiate between the class #1 and class #2 facilities, and that details are provided for the racks/ lockers, preferably on the same page for clarity.

For more information contact the City bike expert at Kaely.dekker@calgary.ca

21. Amend all applicable plans to show the proposed trees and trenches. For the three trees shown within the 5.182m bylaw setback area on 1 Street S.E., provide the trees within individual above grade removable planter boxes. Trees, structures, etc are not permitted within the bylaw setback.

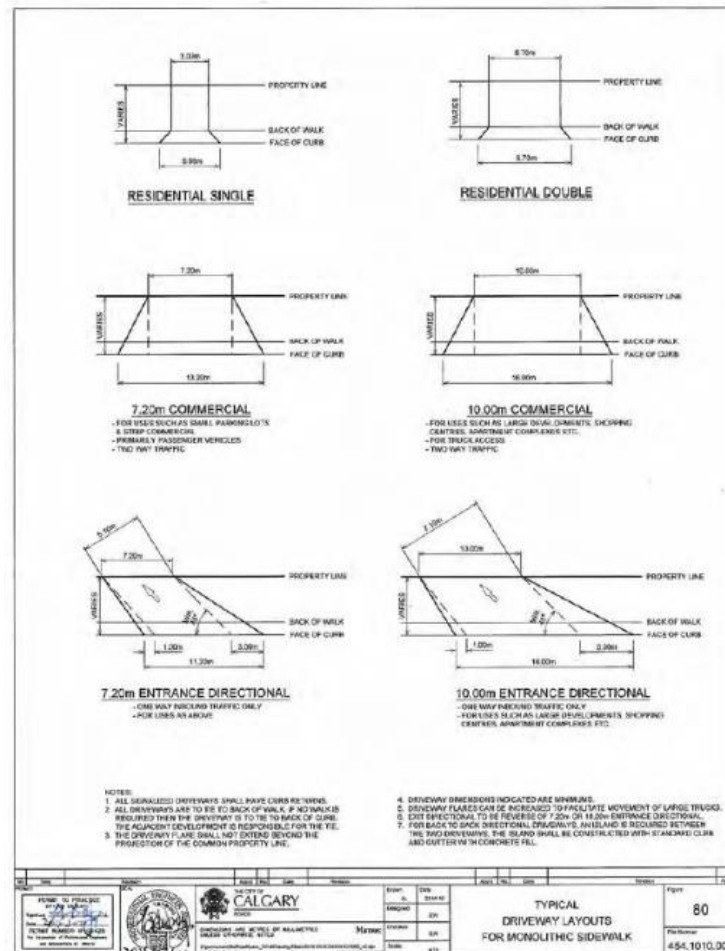
NOTE: Ensure this is completed on the next circulation.

22. Amend the plans to remove the proposed pavers (Mirage NA.ME Collection Swiss grey, Lumnzia, noir belge, gris belge) from within the City rights-of-ways (boulevard), as they are not permitted. Replace with concrete (dyed is okay) with either broom finish or lightly sandblasted, with tooled or saw cut joints.

Conditions of Approval

NOTE: Ensure this is completed on the next circulation.

23. Amend plans to show the widths of all the vehicular site accesses as well as the flares on all applicable plans, including the sweep paths (City spec 454.1010.005 below). It is imperative that the vehicles stay within the confines of the access, and not up and over any curbs. Amend all plans accordingly.



NOTE: Ensure this is completed on the next circulation.

24. Amend plans to provide a wheel chair ramp at the corner of Macleod Trail and Park Road S.E. to go with the bulb out removal. Ensure all applicable plans show this.

NOTE: Ensure this is completed on the next circulation.

25. As the application proposes excavations in close proximity to the Red Line. Calgary Transit will require an indemnification clause, to be established to compensate Calgary Transit for any damage, movement, or settlement of the

Conditions of Approval

LRT track or associated transit infrastructure by the applicant or their agents. A security shall also be required for this purpose (to be negotiated). Contact Chris Knobel at (403) 268-2342.

NOTE: Provide confirmation that this has taken place on the next circulation.

26. Prior to any excavations taking place, shoring plans for excavation are to be circulated to Calgary Transit - Infrastructure for review and approvals.

NOTE: Provide confirmation that this has taken place on the next circulation.

27. The developer shall provide a Letter of Credit for required intersection signalization at 1 Street S.E. / 18 Avenue S.E. and at Macleod Trail S.E. / Park Road S.E. (Estimated at \$300,000.00 each), due to findings in the TIA. The Developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit, and is required to submit payment in support of the proposed Development Permit application.
28. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Construction of 2 new driveway crossings on Park Road S.E.,
 - b. Closure and removal of existing driveway crossings on Macleod Trail S.E.,
 - c. Construction of new sidewalks adjacent to site frontages (Park Road S.E.), including improvements along 1 Street S.E.
 - d. Construction of a wheel chair ramp at the corner of Macleod Trail and Park Road S.E., along with curb bump out being removed,
 - e. Construction of tree trenches to City standards,
 - f. Relocation of existing catch basin on Park Road S.E.,
 - g. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel.
29. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

Conditions of Approval

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

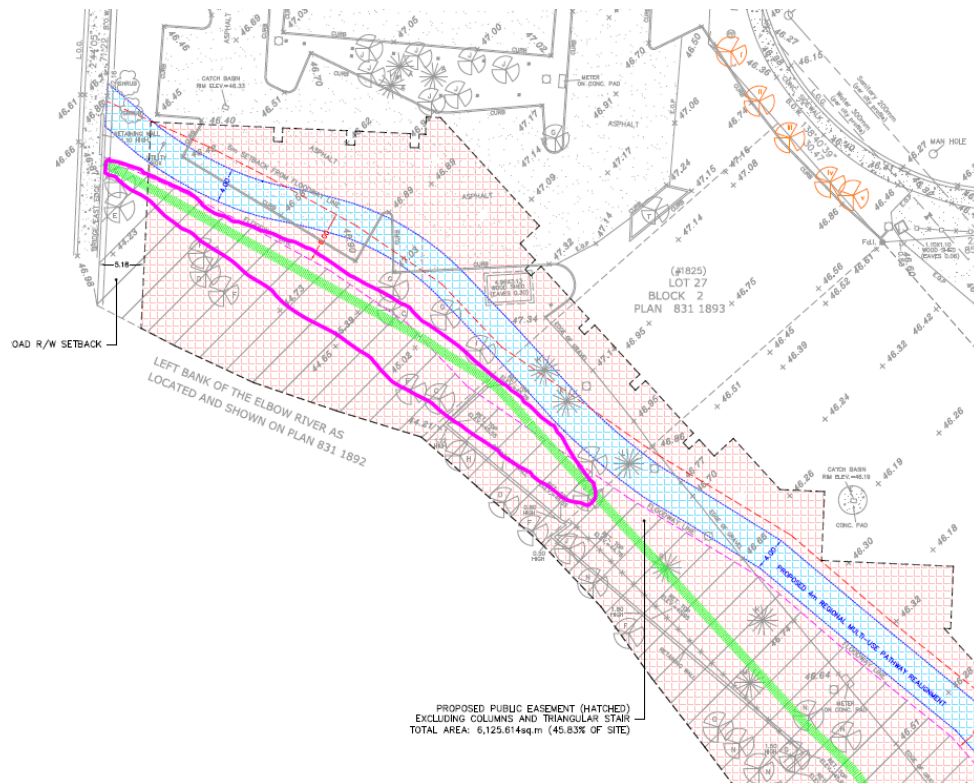
Roads

- a. Street lighting upgrading adjacent to site frontages (If required).

Parks:

30. Submit an updated Biophysical Impact Assessment (BIA) report to resolve minor outstanding issues as per Parks Ecologist's comments (email dated 2017 December 19). Please contact Parks Environmental Planning Lead of Urban Conservation, Marta Sudyk at 403-369-1376 or email marta.sudyk@calgary.ca for further information.
31. Register a public access easement over the Publicly Accessible Private Open Space and incorporate a Pathways Access and Perpetual Maintenance Agreement with the City of Calgary for the 4.0m regional multi-use pathway and 2.5m asphalt regional pathway. The agreement and registerable access right of way plan shall be to the satisfaction of Parks and the City Solicitor (also refer to Planning prior to release condition).
32. Amend the "3363 - Site Plan" plan accordingly:
 - a) Indicate the existing 4m public pedestrian walkway easement agreement (as per instrument No. 891095715) correctly. It appears this easement agreement is only registered against the land title of Plan 8210888 Block 2 Lot 24 (1919 Macleod Trail SE) and not the other two parcels (see image below circled pink).

Conditions of Approval



33. Provide willow planting at the lower riprap area where possible. Amend all relevant landscape plans accordingly.

Permanent Conditions

The following permanent conditions shall apply:

Planning:

34. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
35. No changes to the approved plans shall take place unless authorized by the Development Authority.
36. A Development Completion Permit shall be issued for the development; **before the use is commenced or the development occupied**. A Development Completion Permit is independent from the requirements of Building Permit

Conditions of Approval

occupancy. Call Development Inspection Services at 403-268-5311 to request a site inspection for the Development Completion Permit.

37. The properly executed Development Agreement referenced in Condition #4 shall remain in force and on title throughout the life of this development.
38. The properly executed Special Development Agreement referenced in Condition #5 shall remain in force and on title throughout the life of this development or for so long as the Development Authority, in its sole discretion, determines it necessary to be registered.
39. The properly executed Housing Agreement referenced in Condition #6 shall remain in force and on title throughout the life of this development or for so long as the Development Authority, in its sole discretion, determines it necessary to be registered.
40. Upon completion of the main floor (storey) subfloor of the building, proof of the geodetic elevation of the constructed main floor (storey) subfloor, must be submitted to and approved by the Development Authority prior to any further construction proceeding. Email confirmation to geodetic.review@calgary.ca.
41. The grades indicated on the approved Development Permit plans must match the grades on the Development Site Servicing Plan for the subject site as per the Lot Grading Bylaw.
42. Retaining wall(s) that are 1.0m or greater in height shall be located and constructed as shown on the approved plans released with this permit.
43. All areas of soft landscaping shall be provided with an underground sprinkler irrigation system as identified on the approved plans.
44. All electrical servicing for freestanding light standards shall be provided from underground.
45. The walls, pillars and ceiling of the underground parkade shall be painted white or a comparable light colour.
46. A lighting system to meet a minimum of 54 LUX for the parkades with a uniformity ratio of 4:1 on pavement shall be provided.
47. The light fixtures in the parkade shall be positioned over the parking stalls (not the drive aisles).
48. Handicapped parking stalls shall be located as shown on the approved plans released with this permit. Handicap parking stall(s) shall be clearly designated,

Conditions of Approval

signed and located close to the entrance of the building with barrier-free accessibility.

49. This approval recognizes four (A to D) phases on the approved plans which shall be completed in sequence, however Phase D must be completed concurrently with Phase A. Occupancy will not be granted for Phase A until the completion of Phase D and the completion of the entire tower within Phase A.

All the road works, landscaping and provisions for garbage collection shown within each phase shall be completed prior to the issuance of a Development Completion Permit for that phase. Call Development Inspection Services at 403-268-5311 to request site inspections for the Development Completion Permits.

50. If construction of Phase C has not commenced within one year of the issuance of the most recent Development Completion Permit for Phase A, the façade of the western elevation of the podium of Phase A must be completed to the same quality as the rest of the podium façade using the metal panel system materials identified on the elevation drawings.
51. If construction of Phase D has not commenced within one year of the issuance of the most recent Development Completion Permit for Phase C, the façade of the western elevation of the podium of Phase C must be completed to the same quality as the rest of the podium façade using the metal panel system materials identified on the elevation drawings.
52. If construction of the subsequent phase(s) has not commenced within one year of the issuance of the most recent Development Completion Permit, the undeveloped portion of the site shall be hydro-seeded should the existing asphalt parking lot on the undeveloped portion be removed. This is to ensure an acceptable visual appearance and to eliminate problems such as dust, weeds and erosion.
53. Fascia signage shall be placed only in the designated sign area as indicated on the approved plans. Any damage to the building face, as a result of the sign installation or removal, shall be repaired to the satisfaction of the Development Authority.

Development Engineering:

54. The subject parcels must remain on a single certificate of title for the duration of the development unless subdivision is approved by the subdivision authority.
55. Single retaining walls 1.2m in height or greater or terraced retaining walls 1.2m in height or greater with a horizontal separation between walls of less than 3.6m (3x height) require the approval of a Building Permit prior to construction.

For retaining wall(s) that meet these criteria, the developer may either:

Conditions of Approval

- a. Include the retaining walls with the Building Permit for the building, or
- b. Apply for a separate Building Permit for the retaining walls.

It should be noted that the Building Permit for the building on site will not be released until the separate Building Permit for site retaining walls is approved.

56. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
- a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
57. No outside storage is permitted in the Floodway, as per Land Use Bylaw 1P2007 Part 3, Division 3.
58. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more

Conditions of Approval

frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

59. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.
60. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
61. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.
70. No trees, shrubs, buildings, permanent structures or unauthorized grade changes are permitted within utility rights-of-way.
62. Pursuant to Bylaw 2M2016, off-site levies are applicable.
63. Prior to issuance of a Development Completion Permit or any occupancy of the building, payment shall be made for off-site levies pursuant to Bylaw 2M2016. To obtain a final estimate, contact the Public Infrastructure Coordinator, Calgary Approvals Coordination at 403-268-6739 or email offsitelevy@calgary.ca

Transportation:

64. No disruption to LRT operations will be permitted at any time.
65. The developer shall be responsible for the cost of public work and any damage during construction in City road right-of-ways, as required by the Manager, Transportation Planning. All work performed on public property shall be done in accordance with City standards.
66. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the applicant's expense, upon completion of the foundation. **Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.**

Conditions of Approval

Parks:

67. Public trees located on the boulevard adjacent to the development site shall be retained and protected unless otherwise authorized by Urban Forestry.
68. The submitted plans indicate that the removal of existing public trees along property frontage. As per the City of Calgary Tree Protection By-law, a letter of authorization to remove public trees is required from Parks Urban Forestry. The applicant is to contact Urban Forestry at 311 or email tree.protection@calgary.ca to make arrangements for the letter and compensation.
69. Any tree planting in the City boulevard shall be performed and inspected in accordance with Parks Development Guidelines and Standard Specifications – Landscape Construction (current edition). Applicant is to contact the Parks Development Inspector at (403) 620-3216 or at (403) 268-5204 to arrange an inspection.

Urban Design Review Panel Comments & Applicant Response (Review #1)

Urban Design Review Panel Comments – Review #1

Date: November 1, 2017
Time: 3.45 pm
Attendance:
Panel Members:

Present:	Absent:
Janice Liebe (chair)	Robert LeBlond
Brian Horton (vice-chair)	Bruce Nelligan
Chad Russill	Yogeshwar Navagrah
Philip Vandermey	
Terry Klassen	

Advisor: David Down, Chief Urban Designer
Application number: DP2017-4075
Municipal address: 1818 1 ST SE, 1825 Park Rd SE, 1919 Macleod Tr SE
Community: Beltline
Project description: New: Multi-Residential Development, Retail and Consumer Service (1 building, 1250 units)
Review: first
File Manager: Brendyn Seymour
City Wide Urban Design: Lothar Wiwjorra
Applicant: Norr Architects, Engineers, Planners
Architect: Norr Architects, Engineers, Planners
Owner: Albari Holding, Cidex Developments

Ranking: Further Review Recommended

Summary

UDRP commends the applicant for an ambitious proposal on a difficult to develop site. Given its location as a gateway to Calgary's city centre, the eventual development will be a landmark for the City. UDRP supports the density and the programming of the site, but suggests that a number of design changes would improve the overall quality of the site in an urban design context. UDRP is particularly concerned with the architectural design and language of the podium levels and has provided more detailed commentary below.

Although the site is bound by two major roadways, Macleod trail SE and 1 Street SE, UDRP believes that the site could be better integrated with the surrounding urban context. With significant improvements to 17 Avenue east currently underway and the planned extension of 17 Avenue across Macleod Trail into the Stampede Grounds and the ultimate extension of Riverwalk, the panel suggests that the connection of the project to the site to 17 Avenue can be improved by extending the proposed Park Road paving condition to the north. UDRP further recommends that this connection be extended to the river promenade by introducing a public north-south connection through the building from Park road to the river.

Our intention is to extend the proposed paving shown in our Landscape drawings North to 18th Avenue SW, pending approval from Roads / Transportation, and the approval/coordination from/with neighboring landowners to our site. The request to consider the introduction of a public path/opening through the site to access the river promenade would cause circulation, functionality, building operations, safety, CPTED and constructability concerns to Hat @ Elbow River. Additionally this would further complicate the limited servicing/back of house frontage we have at Park Road itself. As an alternative, the ground floor level is now proposed as a continuous double and triple height space with animated uses including thematic retail, coffee, fine dining and gathering/socializing areas which can be accessed from both building frontages (Park Road and the river promenade). The overall height of the lobby and the transparency of the glazing will allow the public to see through the lobby from both directions. To further complement, the existing

Urban Design Review Panel Comments & Applicant Response (Review #1)

sidewalks and City grid will have direct access to the river promenade portion of the site (with the respective agreement) so the public can freely have use of it. Please see Image 1 and Image 2 for clarification.

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	Further Review Recommended
	UDRP Commentary		
	UDRP suggests that the grade level interface could be improved by introducing more active uses on the ground level. In the current proposal, both the north and south frontages are dominated by building lobbies and administrative spaces that do not represent active uses.		
	Applicant Response		
	<i>Active uses have been introduced (see Image 1). Although it is the grand lobby of the building, active uses complementary to the tenants' requirements/amenities are proposed, and will be provided dependent on market demand. Transparency of the glazing will help to emphasize the active uses inside (see images 2 and 3).</i>		
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	Further review recommended
	UDRP Commentary		
	UDRP acknowledges that the north and south facades have a significant amount of glazing that will result in permeability, but is concerned that the design lacks a human scale. UDRP suggests that creating a more fine-grained environment at grade and introducing canopies and a variety of warmer materials will help in creating a more human scale. UDRP also recommends internalizing the servicing functions currently located on the east side of the building which will provide an opportunity to introduce active uses on the Macleod Trail edge.		
	Applicant Response		
	<i>Human scale is achieved on both sides of the building, Park Road and the river promenade, by the introduction of the cantilevered floor above at the second level, providing shelter to the public walking by and to avoid antagonizing pedestrians against a 10 storey podium. In some cases this cantilever becomes an arcaded walkway. At the same time, at the building entrances, we are proposing additional human scaled canopies. See images 1 to 5.</i>		
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	Further review required
	UDRP Commentary		
	Please see comment above regarding creating a north-south pedestrian connection from Park Road to the River.		
	Applicant response		
	<i>As explained in the summary response, to break the building ground level in two would create challenges for the efficiency of the building functionality and operations as ultimately this is a private complex. Instead we proposed a transparent, accessible, very attractive and active ground floor that over time will be a destination for the public and the tenants. Please refer to images 1 and 2.</i>		

Urban Design Review Panel Comments & Applicant Response (Review #1)

4	Entry definition / legibility	Entry points are clear and legible	Support
	UDRP Commentary		
	Applicant Response		
	<i>Noted</i>		
5	Residential multilevel units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	N/A
	UDRP Commentary		
	Applicant Response		
	<i>Not applicable. All units are in the upper levels. Only active uses, lobby and service areas are located at ground level.</i>		
6	At grade parking	At grade parking is concealed behind building frontages along public streets.	Support
	UDRP Commentary		
	Applicant Response		
	<i>Noted. To confirm, at grade parking is within the parkade: indoor.</i>		
7	Parking entrances	Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.	Support
	UDRP Commentary		
	Applicant Response		
	<i>Noted. Both parkade entrances are minimized to just the overhead doors in the only access road possible.</i>		
8	Other		
	Applicant Response		
Urban Connectivity Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrianfirst environments.			
	Topic	Best Practice	Ranking
9	LRT station connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	Support with comment
	UDRP Commentary		

Urban Design Review Panel Comments & Applicant Response (Review #1)

As noted above, extending Park Road to the north will improve one pedestrian route option to the LRT. UDRP requests that the applicant works with the City to create a riverfront pedestrian and cycle connection east which will act as another route to the LRT station.			
Applicant Response			
<i>Noted. As mentioned, it is the intent to extend the paving to the North to 18th Ave SW, pending on approvals.</i>			
10	Regional pathway connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	Support with Comment
UDRP Commentary			
In addition to the comment above, UDRP recommends that the applicant work with the City in determining if a riverfront pathway connection to the west is feasible. If determined not feasible, UDRP requests that an alternate crossing of 1 Street SE be explored.			
Applicant Response			
<i>Noted. Under the bridge on Macleod Trail to the West there is no headroom clearance, so an alternative should be explored by Transportation in conjunction with Parks and Roads.</i>			
11	Cycle path connections	Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.	Support with Comment
UDRP Commentary			
<i>Noted. Bike storage is provided throughout the development, as self-standing wall mounted bike lockers or inside storage lockers for the tenants.</i>			
Applicant Response			
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	Further review required
UDRP Commentary			
See comment above regarding extending Park Road			
Applicant Response			
<i>We are proposing to provide and extend paving for all pedestrian areas (even roadway area) on Park Road and the river promenade frontage, to create an extended plaza experience.</i>			
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.	Further review required
UDRP Commentary			
See comment above regarding creating a new north-south connection through the site.			
Applicant Response			
<i>Answer also addressed in previous responses. Please see above.</i>			

Urban Design Review Panel Comments & Applicant Response (Review #1)

14	Open space networks and park systems	Connects and extends existing systems and patterns.	Further review required
UDRP Commentary			
Although proposed park and open space connections will extend existing systems and patterns, details are lacking – requiring further review, including health of existing trees, slope stability/erosion management, existing retaining structure assessment, context-specific consideration of this site and its location regarding riverfront values, access, landing at water's edge, ecological significance of fisheries and interpretive messaging/public art that narrates how			
mountain rivers are important to the Rivers District, Calgary, and surrounding region. The Open space network, its connection to the Riverwalk, and its ability to meet at the water's edge is fundamentally important to successful development at this site, for which partnering with CMLC (and their precedent downstream experience) is recommended.			
Applicant Response			
<i>BIA study has been commissioned to provide comments and suggestions about this regard.</i>			
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.	Further review required.
UDRP Commentary			
The view and on top of bank is truncated by the two bridges that couple Macleod Trail around the site. The view at the water's edge extends under the bridges upstream and downstream – an opportunity for a riverfront site in its high-profile gateway location that should not be missed.			
Applicant Response			
<i>The proposal celebrates the river front with an extensive and continuous walkway to which the building itself is facing. It also promotes the connection for the Regional Pathway, and people accessing the water. The fact the site is on the North side has provided the opportunities to develop the building along the river and exploit all the views to the South.</i> <i>The openness of the glazing and the active internal uses proposed at ground level will promote the intensive use and interaction of this outdoor public space.</i> <i>On the east end of the site is a gateway to the City from the South, therefore the intention is to create a welcoming structure with architectural / sculptural details that express the openness of the amenity spaces inside like the SE corner structure which will catch the interest of people driving into the City (see image 8)</i>			
16	Vehicular interface		Support
UDRP Commentary			
Applicant Response			
<i>Noted. All parking is screened with dynamic façade to facilitate interest.</i>			
17	Other		
Applicant Response			
Contextual Response Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities			
Topic		Best Practice	Ranking

Urban Design Review Panel Comments & Applicant Response (Review #1)

18	Massing relationship to context	Relationship to adjacent properties is sympathetic	Further review recommended
UDRP Commentary			
<p>The overall scale and distribution of the towers and base is supported. The architecture of the podium however severs the towers from the base, and is of an entirely different language and scale. It is the opinion of the panel that further design iterations of the podium that soften the language, provides greater opportunity for occupied spaces other than parking at the street faces, the potential for the tower architecture to extend down to street level should be considered. Breaking the podium up into separate buildings should also be explored. Most importantly, the triangular portion of the southeast corner is a dramatic element which could be celebrated in a significant way architecturally. This project will become a gateway element to the downtown core, and this triangular component t is highly visible from the one-way traffic heading north into the city.</p>			
<p>The design response could take more advantage of this important position to provide a very significant piece of architecture for the prow. The massing of the podium should take into consideration of any future repurposing of portions or whole of parkade is appropriate given the high-profile gateway location.</p>			
Applicant Response			

Urban Design Review Panel Comments & Applicant Response (Review #1)

<p><i>First of all, to understand the overall concept of the proposed design, it is necessary to explain that being right next to the River, we based our inspiration in the nautical theme.</i></p> <p><i>While not literal, the SE corner piece represents the prow of the boat, the podium represents the boat itself and the towers represent the chimneys. This concept was the result of finding a way to adapt the building to the surrounding context.</i></p> <p><i>The comments provided by UDRP were actually either details overseen in the presentation package (maybe due to lack of a deeper explanation from our part) or have been addressed in this new iteration:</i></p> <p><i>Tower A, on the East, and Tower C, on the West end, have been designed to show some of its elements coming down to the ground (please see images 6 and 7).</i></p> <p><i>The proportion and color of all the glazing panels in the building (towers and podium) are similar and there are glazing elements that are spilling down through the podium to the ground that conserve the same proportions to keep the same language throughout.</i></p> <p><i>However, the approach was also to differentiate the uses within the building and also to emphasize the different masses (image 9 and 10).</i></p> <p><i>To differentiate the uses, the application of different materials is key. The towers are glazed throughout and have continuous wrapping balconies and this represents the residential use. The glazing and continuous balconies in the podium represent the residential use again, but its horizontality denotes different unit types, in this case the smaller shotgun type 1 bedroom units.</i></p> <p><i>Below that, the parkade is screened with dark gray metal panels, but the introduction of staggered glazed horizontal openings, with vertical glazing breaks add to its articulation. Following UDRP commentary, additional vertical breaks were introduced to this metal paneling to avoid having large horizontal masses. These vertical breaks also act as a metaphor to the appearance of the podium with the towers.</i></p> <p><i>The ground floor in response to the requirement for more active uses, now has retail, and other businesses complimentary to the proposed residential and amenities.</i></p> <p><i>The corner piece was further detailed, articulated and adjusted after receiving UDRP comments to have more interest, being an important piece in the building as the downtown gateway. It expresses the mixed use of restaurant at the base (clear glazing), screened parkade on top (metal panel), and the amenity spaces in the two upper levels (glazing); the corner shows a clear glazing piece that denotes vertical circulation that integrates the three parts of this sculptural element (see image 8).</i></p> <p><i>In the case of the massing, the podium had to clearly read as the base of the three towers and not just as a widening of the tower footprint: There had to be a break from both masses. The triangular SE corner piece is an extension of the podium so its language is part of the overall podium concept, but reads as the most important part of the nautical concept: the prow of the boat. In case there will be a market shift in the future, portions of the parkade could be repurposed with other uses.</i></p>			
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites	Support
UDRP Commentary			
<p><i>Noted. The location does not impede sun penetration at critical times of the day nor cast shadows on any parks or public spaces during the key hours as stated in the Beltline ARP</i></p>			
Applicant Response			
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Further review recommended
UDRP Commentary			

Urban Design Review Panel Comments & Applicant Response (Review #1)

The large mass of the podium needs more articulation of active uses at street level to create a human scale. UDRP recommends splitting the podium into several buildings related to the towers to create more fine-grained development.		
Applicant Response		
<i>The podium was further articulated by the introduction of additional vertical breaks (glazing) in the parkade levels to avoid extended horizontal portions (see images 9 and 10). In addition, active uses have been proposed at ground level, and to maintain human scale, the second level is cantilevered or arcaded to mitigate the impact of having a 10 storey wall next to a pedestrian walkway and canopies at the building entrances are proposed (see images 1 to 5).</i>		
21	Massing distribution on site	Support
UDRP Commentary		
Applicant Response		
<i>Noted.</i>		
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade Further review recommended
UDRP Commentary		
As proposed the massing contribution to public realm opportunities need to be better articulated with wrap-around and walk-through benefits.		
Applicant Response		
<i>The front and back of the building have been extensively Landscaped to improve the pedestrian realm. Regional Pathway has been reviewed to provide connection to the East and to be incorporated into the Landscape (see image 11). As mentioned, walkthrough will be accessing the lobby/ground floor to use the proposed active uses within when possible (see images 1 and 2).</i>		
23	Other	
Applicant Response		
Safety and Diversity Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.		
Topic	Best Practice	Ranking
24 Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	
UDRP Commentary		
Details not comprehensively available for review.		
Applicant Response		
<i>Safety will improve with the development. There will be a 24hour concierge and security system that will look after the public and tenants inside and outside the building.</i>		

Urban Design Review Panel Comments & Applicant Response (Review #1)

25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	
	UDRP Commentary		
	Details not comprehensively available for review.		
	Applicant Response		
	<i>A wind study will be provided as a condition of DP release. The building being on the North side of the river, will provide shelter to prevailing winds from the NW direction into the river promenade (see images 4 and 5)</i>		
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.	
	UDRP Commentary		
	Details not comprehensively available for review.		
	Applicant Response		
	<i>Same as above, the building will provide shelter to the river promenade and being the main entrances on the East side, these will also be sheltered from snow drifting (see images 4 and 5)</i>		
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.	
	UDRP Commentary		
	Details not comprehensively available for review.		
	Applicant Response		
	<i>Second level is cantilevered / arcaded. Also, canopies are provided at the entrances (see images 4 and 5)</i>		
28	Night time design		
	UDRP Commentary		
	Details not comprehensively available for review. Inspirational images shown of penthouse lighting to support the buildings serving a gateway function, but no details provided to confirm in the proposed design.		
	Applicant Response		
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.	
	UDRP Commentary		
	Details not comprehensively available for review.		
	Applicant Response		
	<i>Barrier free principles have been addressed throughout.</i>		

Urban Design Review Panel Comments & Applicant Response (Review #1)

30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.	
	UDRP Commentary		
	Details not comprehensively available for review.		
	Applicant Response		
	See response items 25 to 27		
31	Other		
	Applicant Response		
Service / Utility Design Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.			
Topic		Commentary	Ranking
32	Waste / recycling		TBD
33	Enmax (Power) / Atco (Gas)		TBD
34	Transformer / switchgear		TBD
35	Exhaust / intake		TBD
36	Electrical vaults		TBD
37	Loading	2 loading stalls provided, plus small tenant move-in vehicles (vans or small trucks) can be accommodated in P1 underground level.	Further review recommended
38	Fire truck access		Endorse
39	Other		

Urban Design Review Panel Comments & Applicant Response (Review #2)

Urban Design Review Panel Comments – Review #2

Date:	May 2, 2018																				
Time:	2.45 pm																				
Attendance:																					
Panel Members:	<table><tr><td>Present:</td><td>Absent:</td></tr><tr><td>Chad Russill (chair)</td><td>Janice Liebe</td></tr><tr><td>Terry Klassen</td><td>Jack Vanstone</td></tr><tr><td>Glen Pardoe</td><td>Eric Toker</td></tr><tr><td></td><td>Chris Hardwicke</td></tr><tr><td></td><td>Bruce Nelligan</td></tr><tr><td></td><td>Yogeshwar</td></tr><tr><td></td><td>Navagrah</td></tr><tr><td></td><td>Robert LeBlond</td></tr><tr><td></td><td>Gary Mundy</td></tr></table>	Present:	Absent:	Chad Russill (chair)	Janice Liebe	Terry Klassen	Jack Vanstone	Glen Pardoe	Eric Toker		Chris Hardwicke		Bruce Nelligan		Yogeshwar		Navagrah		Robert LeBlond		Gary Mundy
Present:	Absent:																				
Chad Russill (chair)	Janice Liebe																				
Terry Klassen	Jack Vanstone																				
Glen Pardoe	Eric Toker																				
	Chris Hardwicke																				
	Bruce Nelligan																				
	Yogeshwar																				
	Navagrah																				
	Robert LeBlond																				
	Gary Mundy																				
Advisor:	David Down, Chief Urban Designer																				
Application number:	DP2017-4075																				
Municipal address:	1818 1 ST SE, 1825 Park Rd SE, 1919 Macleod Tr SE																				
Community:	Beltline																				
Project description:	New: Multi-Residential Development, Retail and Consumer Service (1 building, 1250 units)																				
Review:	Second (previous November 1, 2017)																				
File Manager:	Brendyn Seymour																				
City Wide Urban Design:	Lothar Wiwjorra																				
Applicant:	Norr Architects, Engineers, Planners																				
Architect:	Norr Architects, Engineers, Planners																				
Owner:	Albari Holding, Cidex Developments																				
Ranking:	Endorse																				

Summary May 2, 2018

The revised presentation package shows positive advancement for the project and relates to several aspects previously discussed by the Panel. In the current form, few issues remain outstanding and require further review to address urban design components.

Some of the notable revisions to the previous design include:

- enhancements to the podium level (materiality and articulation), reduced by one storey in height
- additional active uses introduced into the ground level floor plan plus added transparency
- greater building setback from 1st Street SE
- tower massing articulation developed

Adjustments to the Elbow River interface with the proposed promenade layout continue to be a work in progress by the Applicant. It is obvious that much desire to create a creative and active edge are constrained by various setback regulations and fill restrictions as it relates to profile of the riverbank within this area. The submitted drawings specific to this scope were outdated at the time of review and sketch options more relevant to reflecting the regulations were presented. As a general statement, the Panel appreciates the original effort focused on this interface and understands the parameters restricting various design decisions. While the final design is still a work in progress, UDRP believes that the direction is suitable and outstanding details will be resolved as part of the remaining process.

Urban Design Review Panel Comments & Applicant Response (Review #2)

Applicant Response April 23, 2018

Noted.

Summary November 1, 2017: Further Review Recommended

UDRP commends the applicant for an ambitious proposal on a difficult to develop site. Given its location as a gateway to Calgary's city centre, the eventual development will be a landmark for the City. UDRP supports the density and the programming of the site, but suggests that a number of design changes would improve the overall quality of the site in an urban design context. UDRP is particularly concerned with the architectural design and language of the podium levels and has provided more detailed commentary below.

Although the site is bound by two major roadways, Macleod trail SE and 1 Street SE, UDRP believes that the site could be better integrated with the surrounding urban context. With significant improvements to 17 Avenue east currently underway and the planned extension of 17 Avenue across Macleod Trail into the Stampede Grounds and the ultimate extension of Riverwalk, the panel suggests that the connection of the project to the site to 17 Avenue can be improved by extending the proposed Park Road paving condition to the north. UDRP further recommends that this connection be extended to the river promenade by introducing a public north-south connection through the building from Park road to the river.

Applicant Response December 18, 2017

Our intention is to extend the proposed paving shown in our Landscape drawings North to 18th Avenue SW, pending approval from Roads / Transportation, and the approval/coordination from/with neighboring landowners to our site. The request to consider the introduction of a public path/opening through the site to access the river promenade would cause circulation, functionality, building operations, safety, CPTED and constructability concerns to Hat @ Elbow River. Additionally this would further complicate the limited servicing/back of house frontage we have at Park Road itself. As an alternative, the ground floor level is now proposed as a continuous double and triple height space with animated uses including thematic retail, coffee, fine dining and gathering/socializing areas which can be accessed from both building frontages (Park Road and the river promenade). The overall height of the lobby and the transparency of the glazing will allow the public to see through the lobby from both directions. To further complement, the existing sidewalks and City grid will have direct access to the river promenade portion of the site (with the respective agreement) so the public can freely have use of it. Please see Image 1 and Image 2 for clarification.

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	Support
	UDRP Commentary		
	Applicant Response		
	Noted.		

Urban Design Review Panel Comments & Applicant Response (Review #2)

2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	Support
	UDRP Commentary		
	Applicant Response		
	Noted.		
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	Support with comment
	UDRP Commentary		
	It is noted that the Applicant is pursuing patterned paving for the roadway that extends beyond the subject property line for an enhanced pedestrian environment and plaza-type quality. This is subject to review by Roads/Transportation however UDRP strongly supports the Applicant in the pursuit of this element.		
	Applicant response Noted. Based on further review by Roads / Transportation our intention would be to pursue the patterned paving for the roadway beyond the subject property up to the 18 th Ave. SE to enhance the pedestrian environment; creating a plaza quality experience at the entry of the building.		
4	Entry definition / legibility	Entry points are clear and legible	Support
	UDRP Commentary		
	Applicant Response		
	Noted.		
5	Residential multilevel units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	NA
	UDRP Commentary		
	Applicant Response		
	Noted.		
6	At grade parking	At grade parking is concealed behind building frontages along public streets.	Support
	UDRP Commentary		
	Applicant Response		
	Noted.		
7	Parking entrances	Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.	Support
	UDRP Commentary		

Urban Design Review Panel Comments & Applicant Response (Review #2)

Applicant Response		
Noted.		
8	Other	
Applicant Response		
Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrianfirst environments.</i>		
Topic	Best Practice	Ranking
9	LRT station connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.
		Support
UDRP Commentary		
Applicant Response		
Noted.		
10	Regional pathway connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.
		Further review recommended
UDRP Commentary		
See comment #14 as final design of Elbow River ongoing.		
Applicant Response		
Noted.		
11	Cycle path connections	Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.
		Support
UDRP Commentary		
Applicant Response		
Noted.		
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.
		Support with comment
UDRP Commentary		
See comment #3.		
Applicant Response		
Noted.		
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.
		Support with comment
UDRP Commentary		
See comment #3.		
Applicant Response		

Urban Design Review Panel Comments & Applicant Response (Review #2)

	Noted.		
14	Open space networks and park systems	Connects and extends existing systems and patterns.	Further review recommended
UDRP Commentary			
The Panel appreciates the original effort focused on this interface and understands the parameters restricting various design decisions. UDRP believes that the Applicant's intended direction is suitable and outstanding details will be resolved as part of the remaining process; it therefore notes further review recommended, until the final design is determined.			
Applicant Response			
Noted. Development of designs along the floodway are progressing based on discussions with the CPAG Team and River Engineering.			
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.	Further review recommended
UDRP Commentary			
See comment #14.			
Applicant Response			
Noted.			
16	Vehicular interface		Support
UDRP Commentary			
Applicant Response			
17	Other		
Applicant Response			
Contextual Response Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities			
Topic		Best Practice	Ranking
18	Massing relationship to context	Relationship to adjacent properties is sympathetic	Support with comment
UDRP Commentary			
The Applicant provided a detailed response to the previous comments as it relates to massing and context. Most of the current revisions since UDRP presentation #1 improve upon the previous design including the aspect of incorporating tower massing components into the podium. No items require further review, though refinement is encouraged should the Applicant wish to consider: <ul style="list-style-type: none">- randomized podium pattern VS a meta-pattern expression; review if a planned artistic design reinforced in the patterning could strengthen overall massing and aesthetic- the prow of the boat appears somewhat disjointed from the rest of the building; if the true intention is that the podium represents the boat itself and towers the chimneys (per response comments), being relatively arbitrary for the prow itself does not reinforce this design approach. The Panel suggests the Applicant consider subtle design moves that strengthen the narrative, to be holistic in the execution. This exercise could happen in tandem with the above patterning comment.			
Applicant Response			

Urban Design Review Panel Comments & Applicant Response (Review #2)

Noted. Further investigation regarding the podium patterning will be explored to link the design language of the prow throughout the podium through the use of color and material differentiation.			
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites	Support
	UDRP Commentary		
	Applicant Response		
	Noted.		
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
	UDRP Commentary		
	Applicant Response		
	Noted.		
21	Massing distribution on site		Support
	UDRP Commentary		
	Applicant Response		
	Noted.		
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Support with comment
	UDRP Commentary		
	Adjustments to the podium have improved the massing at grade/public realm including the articulation, materiality study and reduction by one storey in height.		
	Applicant Response		
	Noted.		
23	Other		
	Applicant Response		
Safety and Diversity Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.			
Topic	Best Practice		Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Support
	UDRP Commentary		
	Applicant Response		
	Noted.		

Urban Design Review Panel Comments & Applicant Response (Review #2)

25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	TBD
	UDRP Commentary		
	Applicant Response		
	Noted. A pedestrian level wind analysis will be pursued with particular attention focused on winter conditions when the river plaza design is further developed so that more details are understood for this specific condition.		
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.	TBD
	UDRP Commentary		
	Applicant Response		
	Noted. Refer to Response s#25.		
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.	Support
	UDRP Commentary		
	Applicant Response		
	Noted.		
28	Night time design		TBD
	UDRP Commentary		
	Opportunities exist for detailed review of soffit design and lighting approach.		
	Applicant Response		
	Noted.		
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.	TBD
	UDRP Commentary		
	Applicant Response		
	Noted.		
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.	TBD
	UDRP Commentary		
	Applicant Response		
	Noted.		
31	Other		

Urban Design Review Panel Comments & Applicant Response (Review #2)

Applicant Response		
Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i>		
Topic	Commentary	Ranking
32 Waste / recycling		TBD
33 Enmax (Power) / Atco (Gas)		TBD
34 Transformer / switchgear		TBD
35 Exhaust / intake		TBD
36 Electrical vaults		TBD
37 Loading		Support
38 Fire truck access		Support
39 Other		

Beltline Neighbourhood Association Comments



September 03, 2018

The City of Calgary – Planning, Development & Assessment
Municipal Building
800 Macleod Trail SE
Calgary, AB T2P 2M5

ATTN: Brendyn Seymour, File Manager

RE: DP2017-4075 – “Hat @ Elbow River” – DTR Resubmission

Dear Brendyn,

I am writing this letter on behalf of the Beltline Neighbourhoods Association (BNA) in response to the above-noted development application. The Beltline Urban Development Committee (BUDC) has reviewed the application and the updated DTR re-submission materials that were circulated. The BUDC also had an opportunity to meet with the applicant to review the original submission on January 30, 2018. In reviewing the re-submitted materials, it is clear that a number of our comments appear to have been considered. In consideration of the comments below, the BNA is generally in support of the amended application as submitted.

Since this project occupies a vital gateway site at a primary vehicular entrance to the Centre City from the south, and due to its scale, it is imperative that other impacted stakeholders are engaged. If the applicant has not already done so, we would strongly encourage them to reach out to neighbouring community associations. The project is also within the boundaries of the Rivers District Master Plan and adjacent to Stampede Park, and for that reason, we would also encourage the applicant to maintain an active dialogue with the Stampede and the CMLC so that impacts to the area vision can be addressed.

The BNA would also like to provide the following comments based on the review materials that were circulated and our correspondence with the applicant:

- The building's podium is very large and the BUDC would like to ensure that the applicant considers strategies and perhaps (such as additional glazing) to help reduce the visual mass and breakdown the scale. We would also note that this is an important gateway, and so our expectations for the quality and treatment of this facade and the urban interfaces are extremely high.
- Upon the initial review of the project, there appeared to have been some significant phasing challenges, and there was concern that the primary active portion of the podium facing Macleod Trail would be constructed after the remainder of the development. While we want to continue to stress the importance of building the active

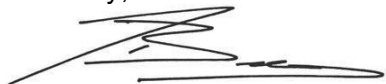
Beltline Neighbourhood Association Comments

(eastern) portion of the podium in the initial phases, it does appear that our initial concerns have been resolved in the resubmission.

- The project's position along the Elbow river provides a great opportunity to provide a publicly accessible riverfront area and improve the pathway network – the BUDC would like to ensure the applicant considers strategies to improve the public realm on both the street and along the riverfront and take steps to improve pedestrian connectivity in the area. We also expect to see significant public amenities along the riverfront, and would encourage the consideration of new and creative ways of engaging with the river. Ample bicycle amenities are also essential.

Thank you for giving the BNA an opportunity to provide a comment.


Sincerely,

A handwritten signature in black ink, appearing to read 'Tyson Bolduc', with a stylized, sweeping flourish at the end.


Tyson Bolduc

Director of Planning and Urban Development, Beltline Neighbourhoods Association

HAT @ Elbow River CPC Presentation



NORR PROJECT NUMBER NCCA140240




HAT @ Elbow River


1818 - 1st Street SE;
1825 Park Rd SE;
1919 McLeod Trail SE, Beltline
Calgary, AB

CPC PRESENTATION


DP2017-4075

HIGH RISE MULTI-RESIDENTIAL





HAT @ Elbow River DATE 2018 | 09 | 20 PROJECT # NCCA140240 PROJECT MANAGER B.M. DRAWN F.A./B.A. SCALE N.T.S PAGE 1



HAT @ Elbow River CPC Presentation



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NORR
ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation



OVERVIEW:
SITE LOCATION
CONTEXT
CONCEPT
SHADOW STUDY
CONNECTIVITY
SITE PHOTOS
FLOODWAY SETBACK



HAT @ Elbow River DATE 2018 | 09 | 20 PROJECT # NCCA140240 PROJECT MANAGER B.M. DRAWN F.A./B.A. SCALE N.T.S PAGE 3



HAT @ Elbow River CPC Presentation

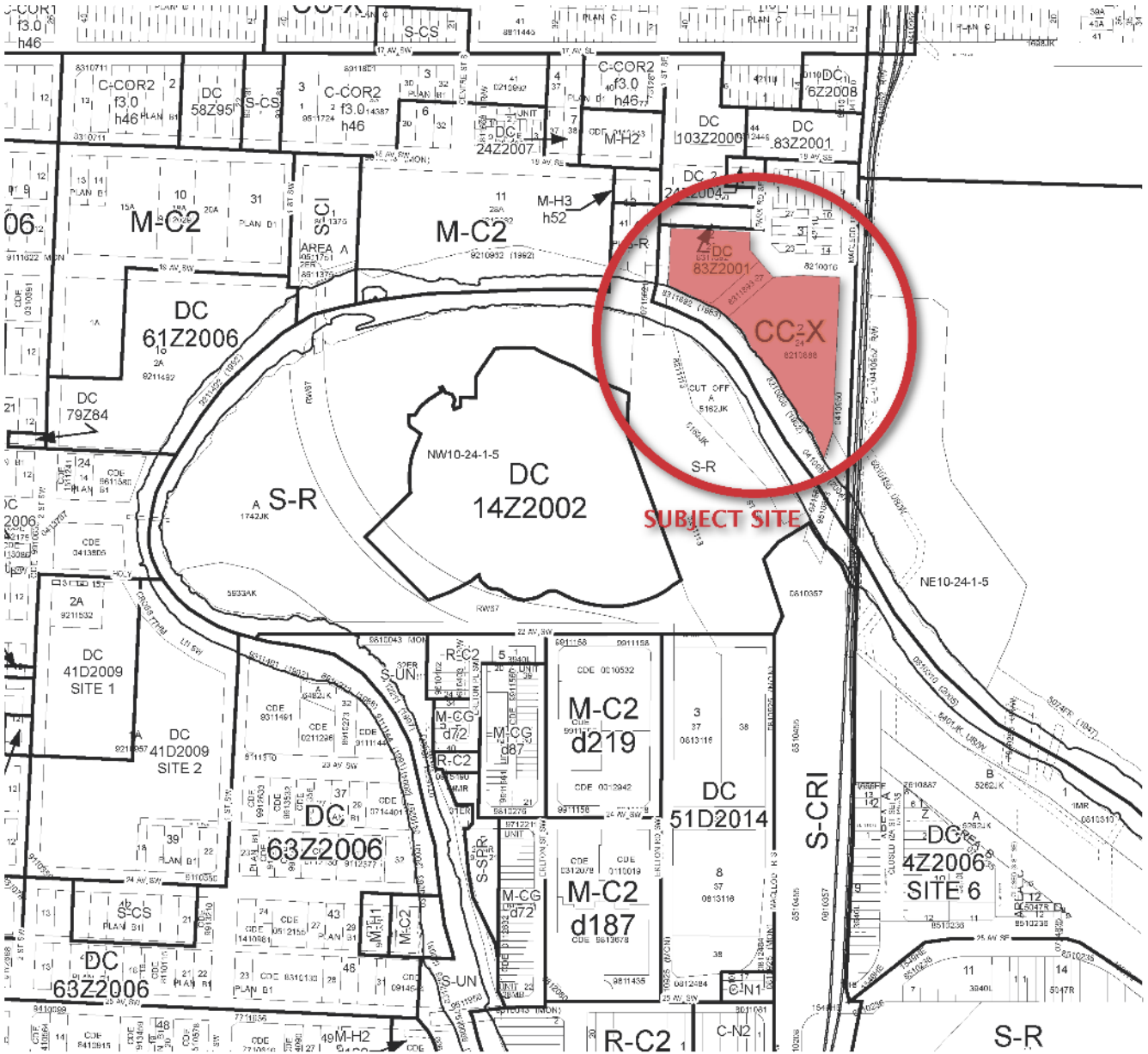



SITE LOCATION

LEGAL DESCRIPTION				
LOT	LOT 26 (EXCEPT 19.053m NORTHERLY) / LOT 27 / LOT 24			
BLOCK	2 / 2 / 2			
PLAN	831 1892 / 831 1893 / 821 0888			
MUNICIPAL ADDRESS	1818 - 1st Street SE; 1825 Park Rd SE; 1919 Macleod Tr SE CALGARY, AB			
COMMUNITY				
LAND USE	CC-X CENTRE CITY MIXED USE DISTRICT			
SITE AREA	m²	sq.ft.	acre	Ha
	13,364.57	143,855.00	3.30	1.34
NET AREA	9,275.83	99,844.25	2.29	0.93
FAR maximum	12.00	160,374.80		m²
FAR proposed	11.51	153,815.23		m²


General Description:

Hat at Elbow River is a High-rise Mixed use project, Residential and Commercial/Retail at the ground level. It is comprised by 2 levels of underground parkade, a 9 storey podium from which the double and triple height ground level is used as the entrance lobby and concierge with proposed future uses of retail and restaurants; levels 2 to 6 are parkade levels and levels 7 to 9 contain residential units. Above the podium there are 3 residential towers, totaling 56, 44 and 50 storeys respectively. On levels 7 and 10, which are rooftop levels, extensive areas of outdoor amenity have been proposed, including a running track, pet park, outdoor seating, and outdoor exercise and yoga corner, BBQ, etc. Also, indoor amenities like guest suites (up to 8) and possible indoor pool and meeting/gathering/lounge/gym spaces have been proposed. The total maximum residential units Hat at Elbow River will accommodate is 1244, plus 8 guest units. Adequate vehicular parking, storage lockers and bicycle storage have been provided.





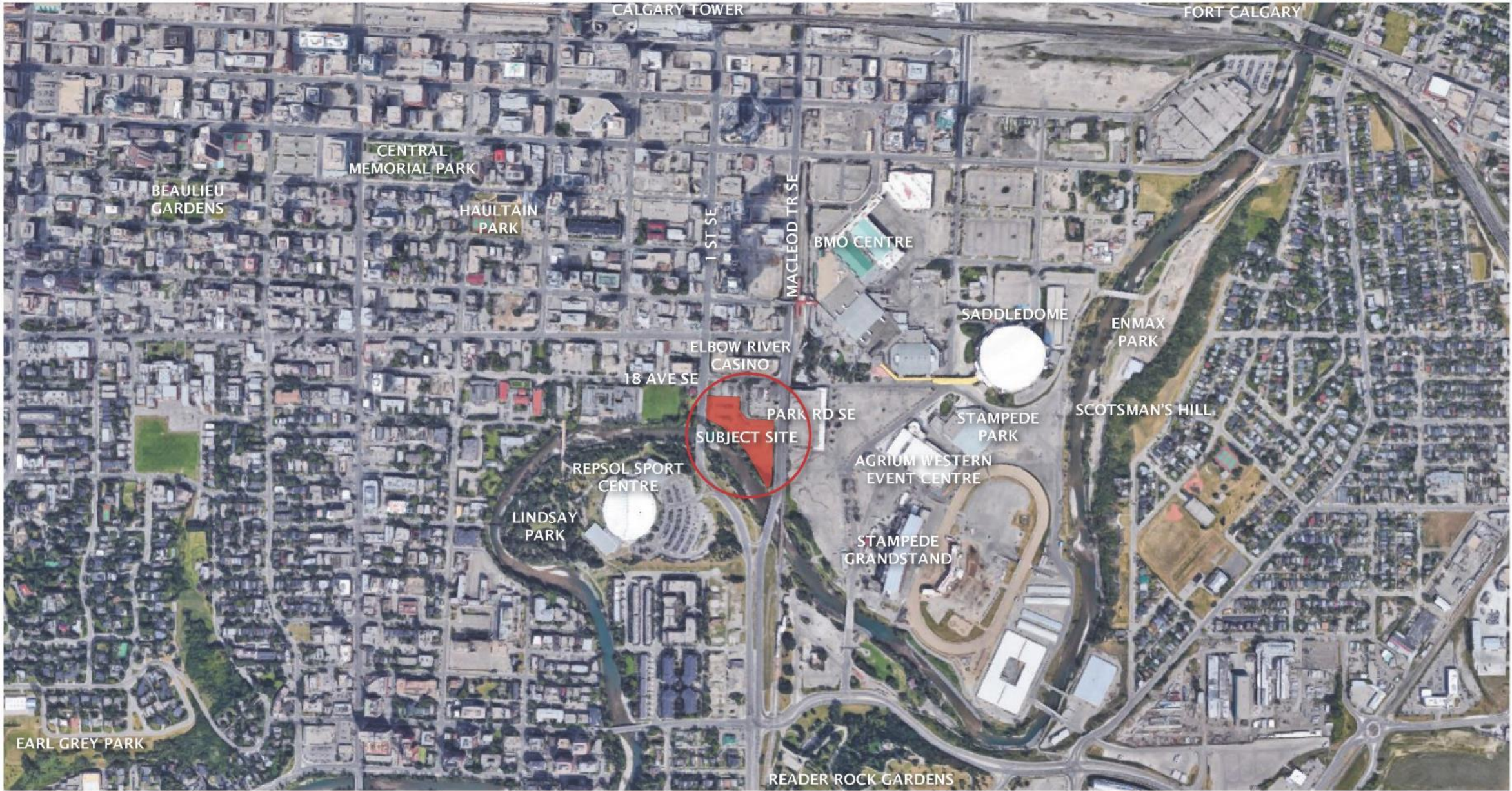
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HAT @ Elbow River CPC Presentation



CONTEXT





HAT @ Elbow River

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HAT @ Elbow River CPC Presentation



CONCEPT

Due to the prominent location of Hat at Elbow River, it will be considered as an important gateway to Downtown Calgary from the South, therefore the proposed building had to respond to this role. It also had to be perceived as if the building would only pertain to this particular location and not anywhere else. The prominent location also determined that massing and proportion had to be carefully studied and that it will be important to use high quality durable materials.

The concept of the proposed design, is that being right next to the Elbow River, the inspiration is based on the nautical theme. While not literal, the SE corner piece represents the prow of the boat, the podium represents the boat itself and the towers represent the chimneys. This concept was the result of finding a way to adapt the building to the specific surrounding context. With the purpose of integrating the overall design, portions of the towers have been allowed to show some of its elements coming down to the ground.

The proportion and color of all the glazing panels in the building (towers and podium) are similar and there are glazing elements that are spilling down through the podium to the ground that conserve the same proportions to keep the same language throughout. However, the approach is also to differentiate the uses within the building and also to emphasize the different masses, for which, the application of different materials is key. The towers are glazed throughout and have continuous wrapping balconies and this represents the residential use. The glazing and continuous balconies in the podium represent the residential use again, but its horizontality denotes different unit types, in this case the smaller one bedroom units. Below that, the parkade is screened with dark gray metal panels, but the introduction of staggered glazed openings, with vertical glazing breaks add to its articulation and to avoid having large horizontal masses.

The ground floor responds to the requirement for active uses, and has retail, and other businesses complimentary to the proposed residential and amenities. The corner triangular piece (prow) is detailed and articulated to have more permeability and interest, to the public uses within. It expresses the mixed use of restaurant at the base (clear glazing), screened parkade on top (metal panel), and the amenity spaces in the two upper levels (glazing); the corner shows a clear glazing piece that denotes vertical circulation that integrates the three parts of this sculptural element.

In the case of the massing, the podium had to clearly read as the base of the three towers and not just as a widening of the tower footprint: There had to be a break from both masses. The triangular SE corner piece is an extension of the podium so its language is part of the overall podium concept, but reads as the most important part of the nautical concept: the prow of the boat.

To maintain human scale, the second level is cantilevered or arcaded to mitigate the impact of having a 9 storey wall next to a pedestrian walkway and canopies at the building entrances are proposed. The front and back of the building have been extensively landscaped to improve the pedestrian realm and barrier free principles have been addressed throughout. The Regional Pathway has been reviewed to provide connection to the East and to be incorporated into the Landscape. Walkthrough to the River Bank will be complimented by accessing the lobby/ ground floor to use the proposed active uses within.



HAT @ Elbow River

DATE 2018 1 09 1 20

PROJECT # NCCA140240

PROJECT MANAGER B.M.

DRAWN F.A./B.A.

SCALE N.T.S

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HAT @ Elbow River CPC Presentation



URBAN DESIGN ELEMENTS

1. Creativity and Innovation

The HAT at Elbow River is a highly creative and innovative solution on a challenging site. The transformation from a surface parking lot to a high density urban mixed use residential and commercial project takes great advantage of this south facing riverbank creating a link to the surrounding communities. It breathes new life into the area and creates a vibrant new community. The main challenge here is working with an odd shaped site and tight constraints adjacent to the Elbow River floodway.

2. Context and Appropriateness

The site is an island between north and southbound vehicular corridors. It is also in a highly visible location. It is a destination for the residents and must identify itself as such. The building program seeks to provide opportunities for a full scale community within the project with appropriate retail, recreational and residential experiences.

3. Connectivity and Continuity

The HAT design allows access from all sides of the project for pedestrians. Although this is a private resident destination, over time the location will diversify with more interactive uses at grade and more permeable edges to the public.

4. Functional and Aesthetic integration

The podium has a functional purpose for both active uses (residential and recreation) and passive uses (automobile parking). Aesthetically the podium, being adjacent to the Elbow River, is inspired in the nautical theme and is articulated as the bow of a boat. The towers are obviously designed for both functional and aesthetic reasons. The functional being the views and efficiency, the aesthetic being the simplicity of sculptural form complemented by various material types to increase visual interest.

5. Legibility and Accessibility

The base of the podium is legible and permeable to almost 70% with active and passive uses. Over time as the neighbourhood develops the uses will evolve to a more interactive nature. Complete accessibility to the ground floor (and elevators) will be managed through ramping and at grade entrances. All suites and amenity areas are accessible through high speed elevators.

6. Enclosure and Human Scale

As said previously the ground floor is permeable and encourages human interaction. The entrance statement to the HAT is strongly expressed on the north side as the mass wraps around the main entrance foyers to enclose them and create a strong sense of arrival and place.

7. Comfort and Safety

The HAT is a modern building complex. As such, it seeks to create a home for approximately 2000 people. In that regard, amenities and places that create comfort and security are paramount. The HAT community will benefit from the full time concierge services and maintenance staff on site. Underground and enclosed parking will be monitored full time for security as will amenity spaces. Similar security systems to a hotel complex will be in place at the HAT.

8. Quality and Durability

Rental projects inherently require a high level of durability. Finishes must withstand high traffic and constant move in/ move outs. To this end we have designed the several locations for loading and movement of belongings. On the ground floor large scale trucks can deliver and pick up in two locations with access to the elevator cores. In the first level of underground parking additional height will insure that medium sized moving vans can access the core areas without conflicting with main floor functions. Overall the exterior materials have been selected for durability and quality. High efficiency glazing and panel systems combined with masonry at the street level will insure durability over time while expressing quality to the viewer. Interior selections, from flooring to appliances, are specifically chosen for their durability as well. The overall message to the residents at the HAT is one of quality and classic timelessness.

9. Vitality and Animation

The scale of HAT at Elbow River will encourage community development and a sense of belonging. High quality amenities will animate the development and create vitality. The variety of unit types will attract all types of urban dwellers and create opportunities for demographic vitality and interactive uses.

10. Flexibility and Adaptability

The design of the HAT at Elbow River includes thoughtfulness for future transition to a more interactive neighbourhood. Ground floor lobby and public areas will transition over time to active uses serving both the residents and public interacting with the site. Parking levels above grade in the podium are designed to eventually transition to residential units with more bike and miscellaneous storage for residents.

11. Diversity and Variety

The sheer number and variety of unit types will create a high level of diversity and of residents. This will in turn result in a need to flexibility of uses and amenities. Over time as the development matures the hope is that new needs will emerge and that a sense of community will develop. The Developer Cidex have a vision for an extremely diverse community with a healthy mix of culture and interaction.

12. Sustainability and Accountability

The HAT at Elbow River will strive for LEED certification or similar sustainable certification. Over time as resident needs evolve the building will result in less parking and more units. Overall the energy efficiency of the project will meet or exceed the NECB 2015 standard. Complete material and organic recycling will be accommodated.

13. Wayfinding and Orientation

The Hat at Elbow River will require a complex set of wayfinding signage and orientation. This is aided through a comprehensive design that foresees a central information management source linked by digital communication and custom websites for residents. Constant information update and a reinforcing of community activities a sense of place will create a powerhouse place to live and play. Over time this may also include a place to both work and interact.



HAT @ Elbow River CPC Presentation



SHADOW STUDY



MARCH 21 - 10AM



MARCH 21 - 12PM



MARCH 21 - 2PM



MARCH 21 - 4PM



SEPTEMBER 21 - 10AM



SEPTEMBER 21 - 12PM



SEPTEMBER 21 - 2PM



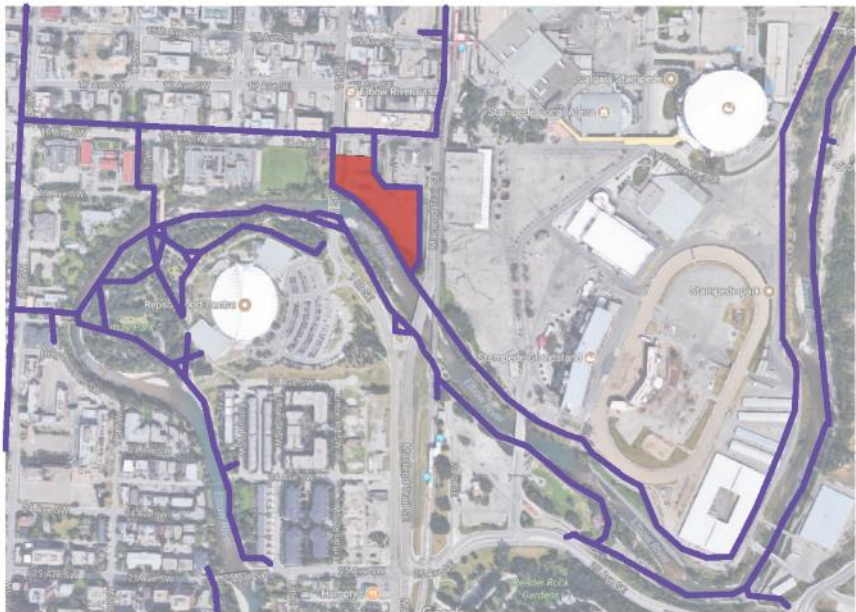
SEPTEMBER 21 - 4PM



HAT @ Elbow River CPC Presentation



CONNECTIVITY



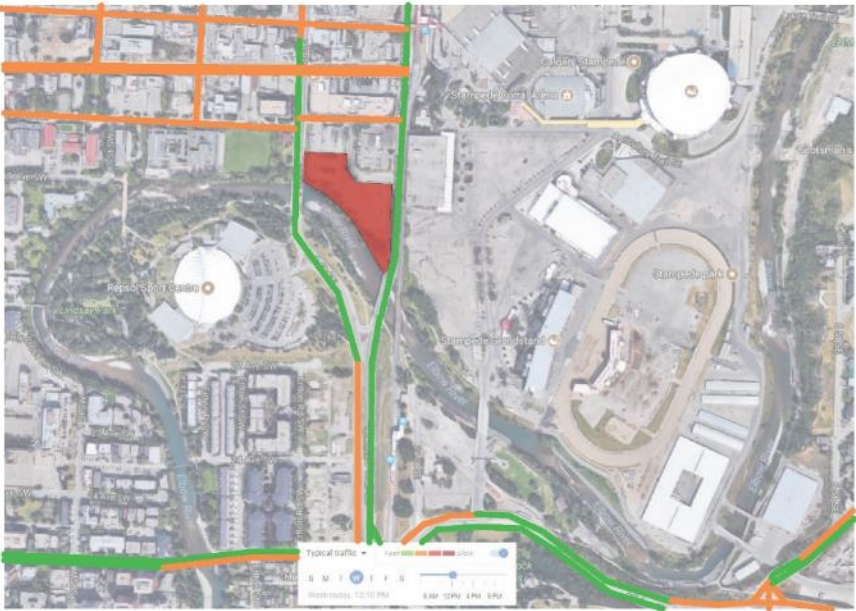
PEDESTRIAN CIRCULATION



TRANSIT STOPS



BICYCLE CIRCULATION



VEHICLE CIRCULATION



HAT @ Elbow River CPC Presentation



SITE PHOTOS



01. EAST VIEW



02. SOUTH EAST VIEW



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SITE PHOTOS



03. SOUTH WEST VIEW



04. WEST VIEW



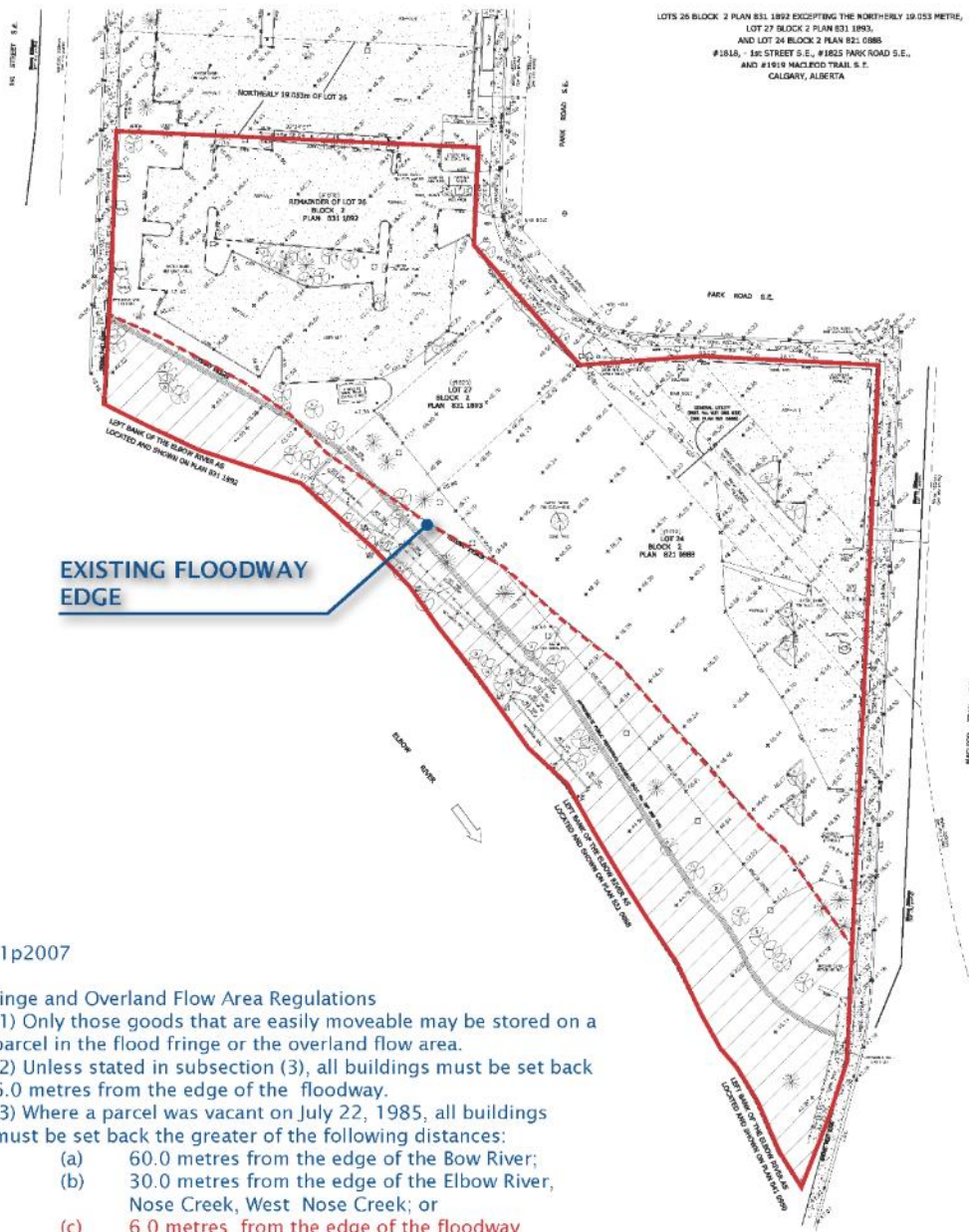
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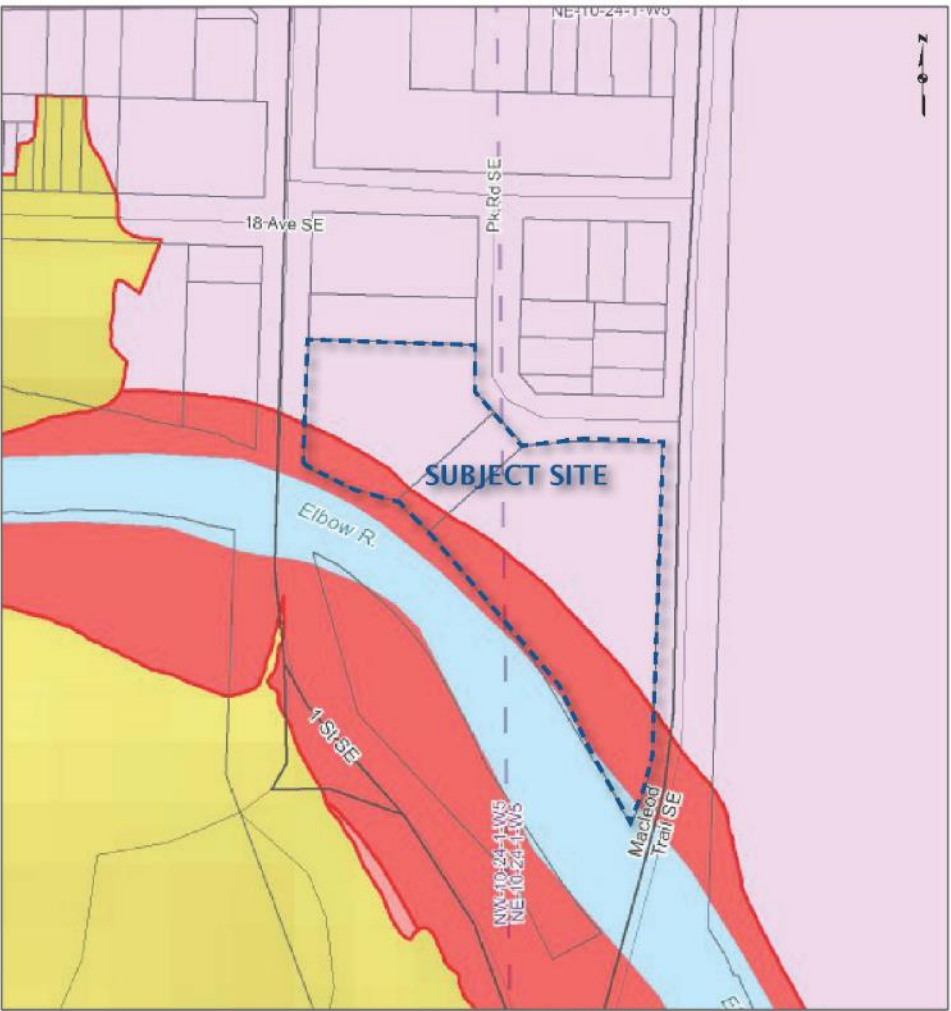
FLOODWAY SETBACK



Per LUB 1p2007

Flood Fringe and Overland Flow Area Regulations

- 59 (1) Only those goods that are easily moveable may be stored on a parcel in the flood fringe or the overland flow area.
(2) Unless stated in subsection (3), all buildings must be set back 6.0 metres from the edge of the floodway.
(3) Where a parcel was vacant on July 22, 1985, all buildings must be set back the greater of the following distances:
(a) 60.0 metres from the edge of the Bow River;
(b) 30.0 metres from the edge of the Elbow River, Nose Creek, West Nose Creek; or
(c) 6.0 metres from the edge of the floodway



Legend

- Floodway
Flood Fringe
Overland Flow (Flood Fringe)
Under Review
Cross Section and Design Flood Level
Water Body
First Nation Boundary
Municipal Boundary

Information as depicted is subject to change, therefore the Government of Alberta assumes no responsibility for discrepancies at time of use.
Base Data provided by Spatial Data Warehouse Ltd.
National Framework Data © Department of Natural Resources Canada. All rights reserved.
Alberta Road Network data provided by GeoBase ©
Alberta Environment and Sustainable Resource Development
© 2013 Government of Alberta

Flood Hazard Map			
Projection	Alberta 10TM	Datum	NAD 83
Date	2014-Aug-27		
http://www.envinfo.gov.ab.ca/FoodHazard/			



HAT @ Elbow River CPC Presentation



CONCEPT:
MATERIALS BOARD
GLAZING DETAIL



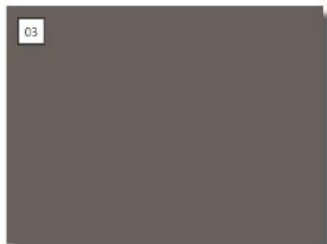
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MATERIAL BOARD: OPTION 1



MATERIAL LEGEND	
01	INSULATED METAL PANEL (TEXTURE: FLAT) MATCH MANUFACTURER: KONGSKAM BENCHMARK - SPECIALTY FINISHES: BURNT RUST
02	INSULATED METAL PANEL (TEXTURE: FLAT) MATCH MANUFACTURER: KONGSKAM BENCHMARK - PRISCUS METALS: CLEAR MATT CLASS 1
03	INSULATED METAL PANEL (TEXTURE: FLAT) MATCH MANUFACTURER: KONGSKAM BENCHMARK - PRISCUS METALS: ANODIZED
04	INSULATED METAL PANEL (TEXTURE: FLAT) MATCH MANUFACTURER: KONGSKAM BENCHMARK - PRISCUS METALS: SOLID WHITE
05	CONCRETE
06A	BLUE GREY CONCRETE PAINTING (BASE TONE)
06B	STEEL BLUE CONCRETE PAINTING (DARK TONE)
06C	SHADOW GREY CONCRETE PAINTING (ACCENT TONE)



NORTH-EAST PERSPECTIVE



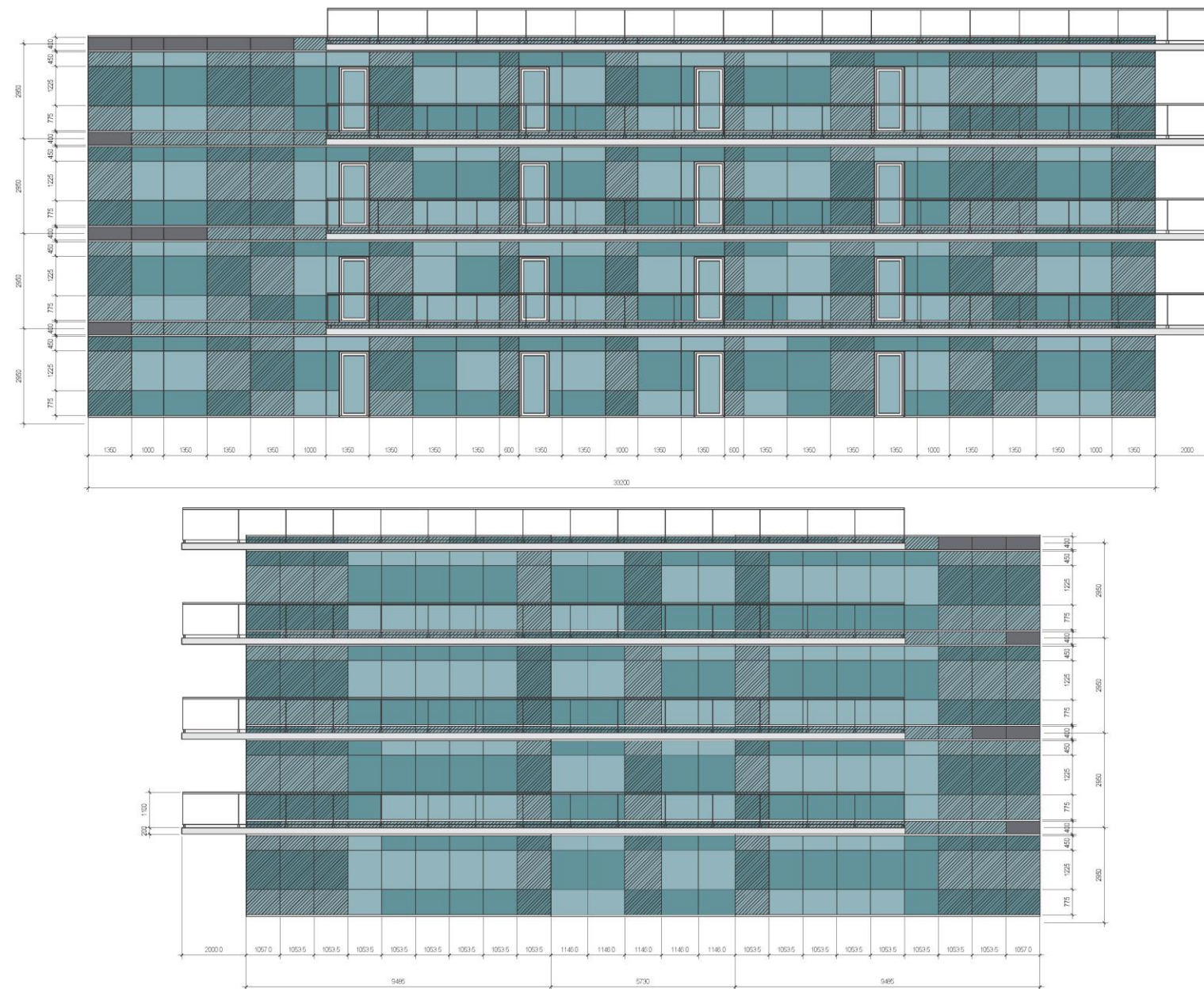
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TYPICAL TOWER GLAZING DETAIL



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3 D R E N D E R I N G S :



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3D AERIAL VIEW



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3 D VIEWS



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3 D VIEWS



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3 D VIEWS



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3 D VIEWS



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3 D VIEWS



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3 D VIEWS



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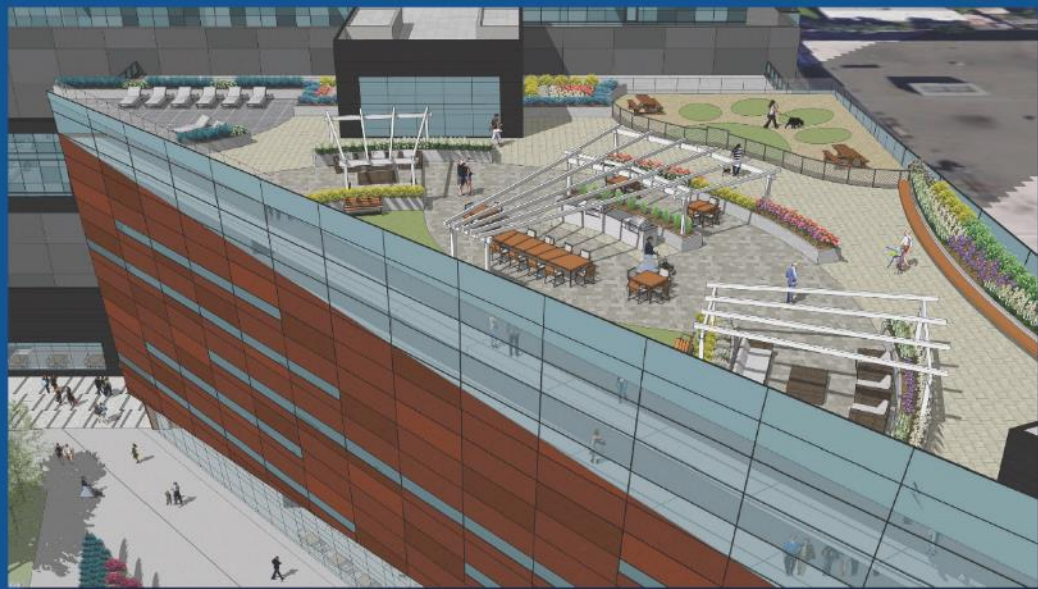
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3D VIEWS - Outdoor Amenity 1



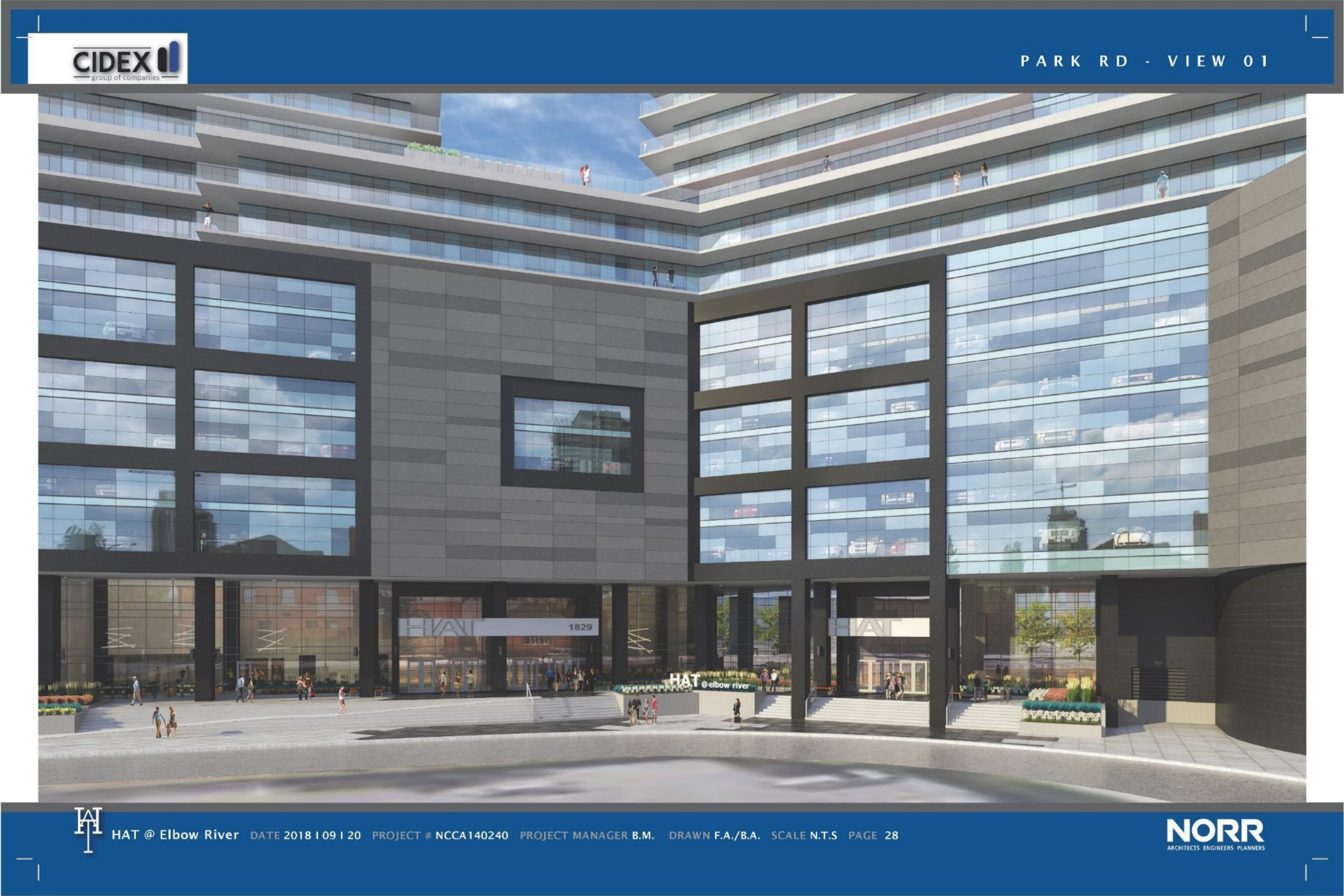
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3D VIEWS - Outdoor Amenity 2



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PARK RD - VIEW 02



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STAMPEDE VIEW



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MACLEOD TR VIEW



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ELBOW RIVER VIEW



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ELBOW RIVER - ENHANCED VIEW



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ELBOW RIVER - URBAN MARKET VIEW 1



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ELBOW RIVER - URBAN MARKET VIEW 2



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ELBOW RIVER OVERALL VIEW



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3D VIEWS - NIGHT



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3 D VIEWS - NIGHT



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LANDSCAPE PLANS:
OVERALL GROUND FLOOR LANDSCAPE PLAN AND SECTIONS
OUTDOOR AMENITY LEVEL 6.5
OUTDOOR AMENITY LEVEL 10
INDOOR AMENITY



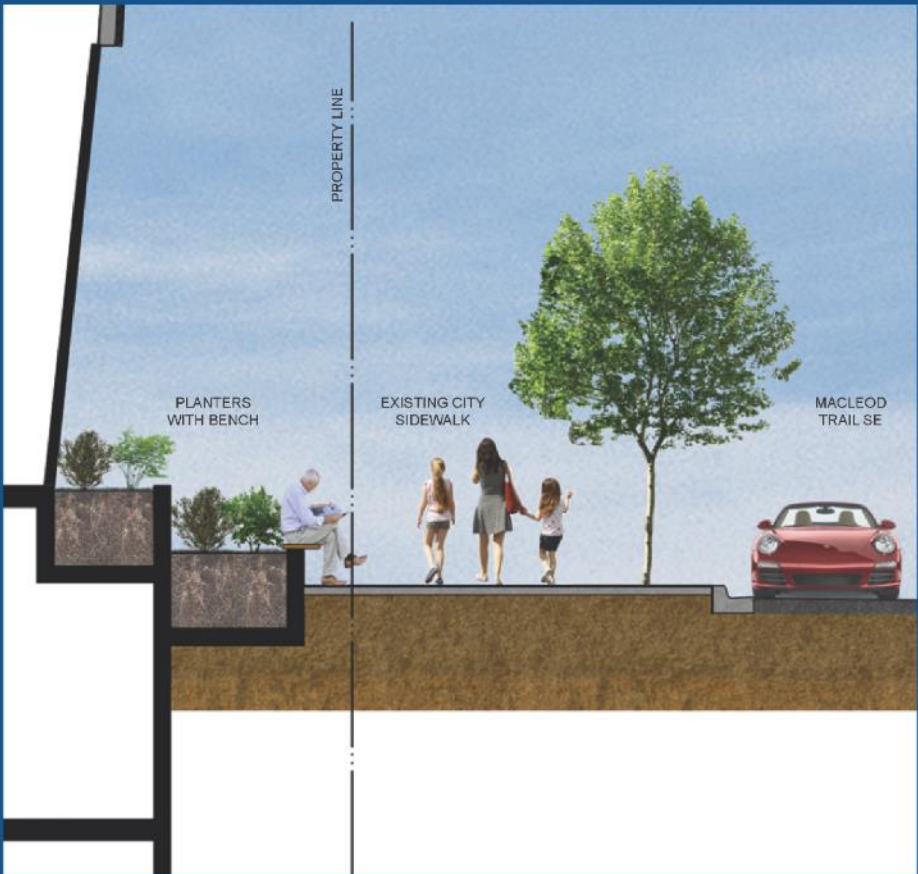
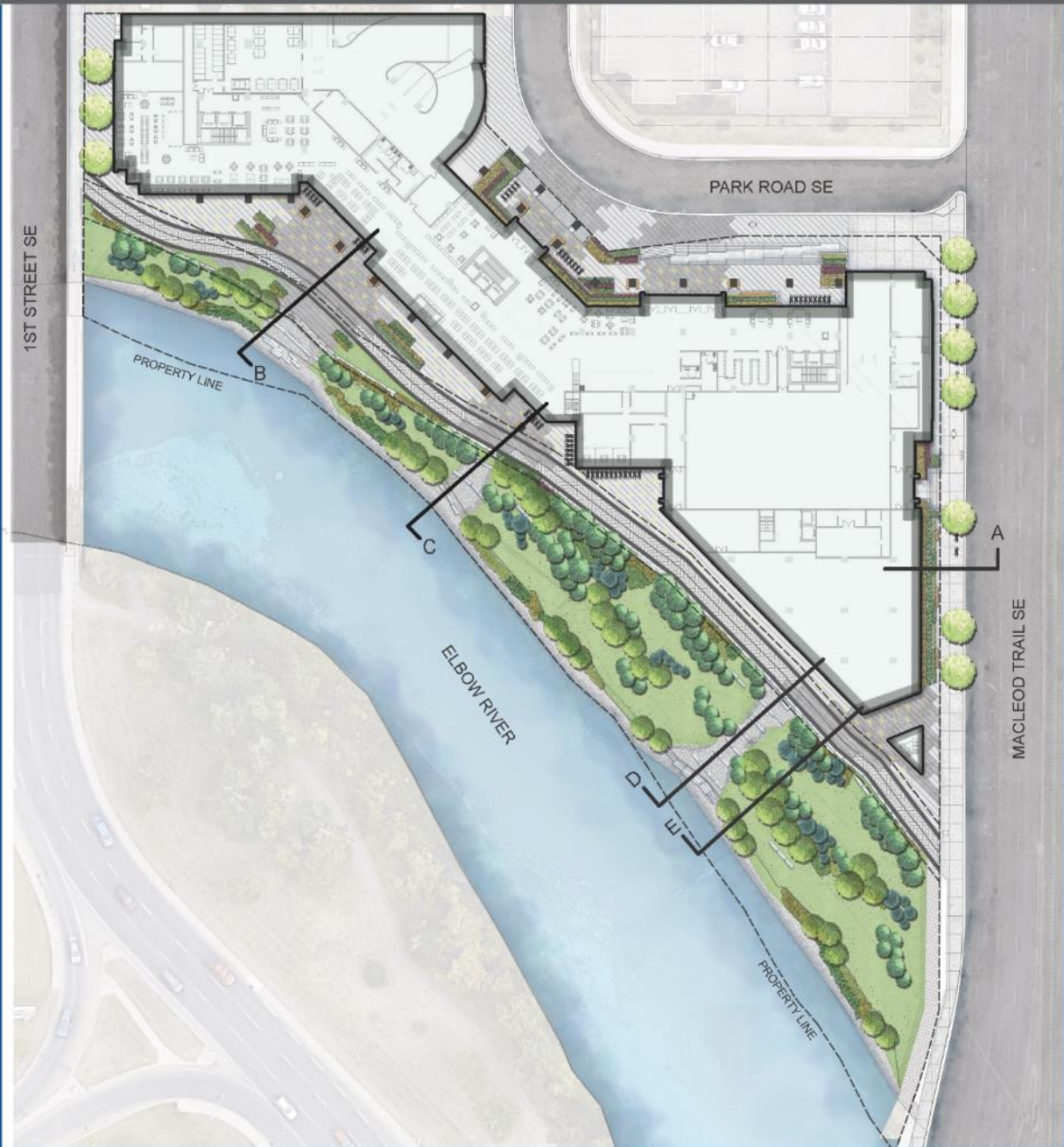
HAT @ Elbow River DATE 2018 1 09 1 20 PROJECT # NCCA140240 PROJECT MANAGER B.M. DRAWN F.A./B.A. SCALE N.T.S PAGE 39



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MAIN FLOOR - LANDSCAPE PLAN



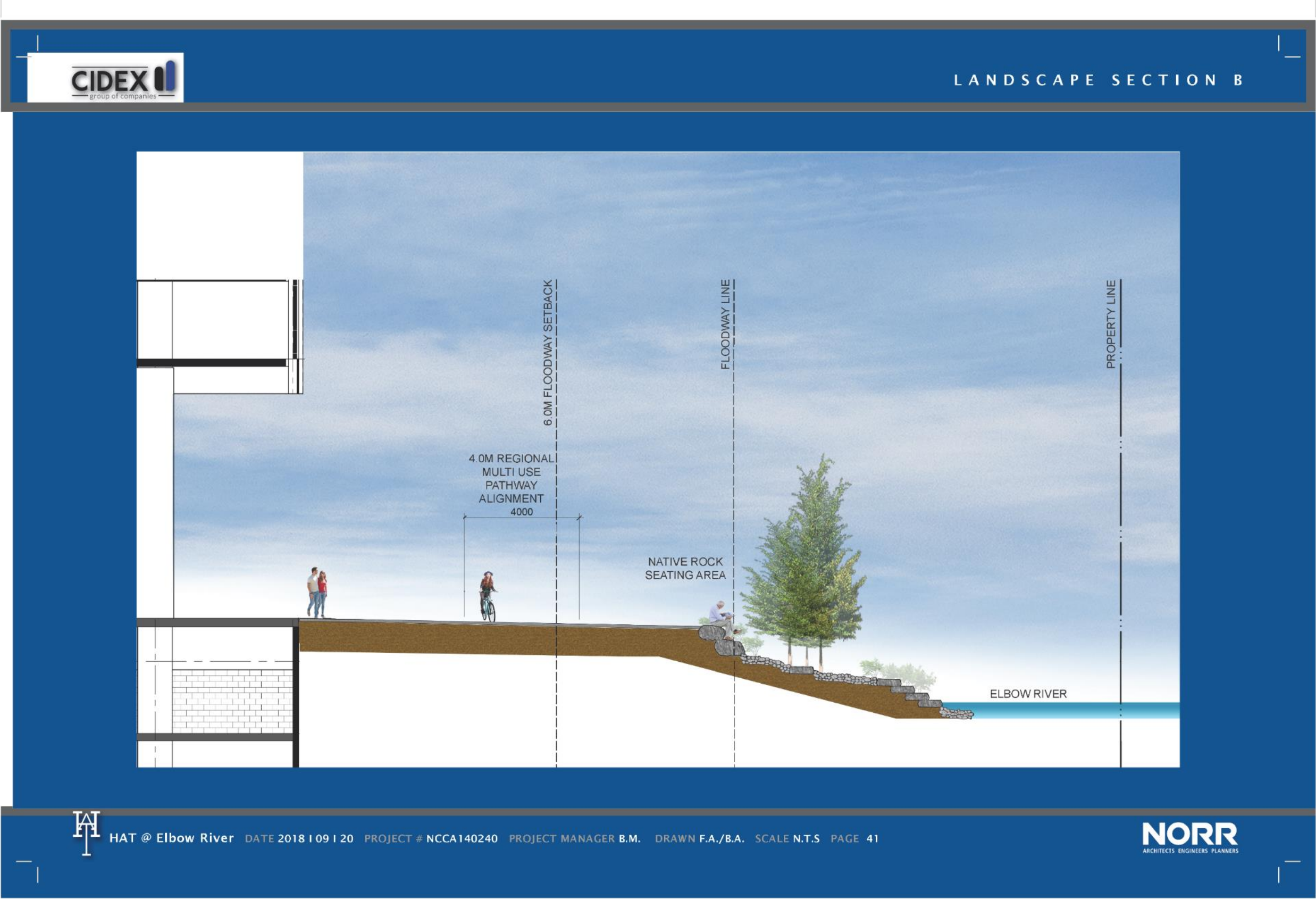
LANDSCAPE SECTION A



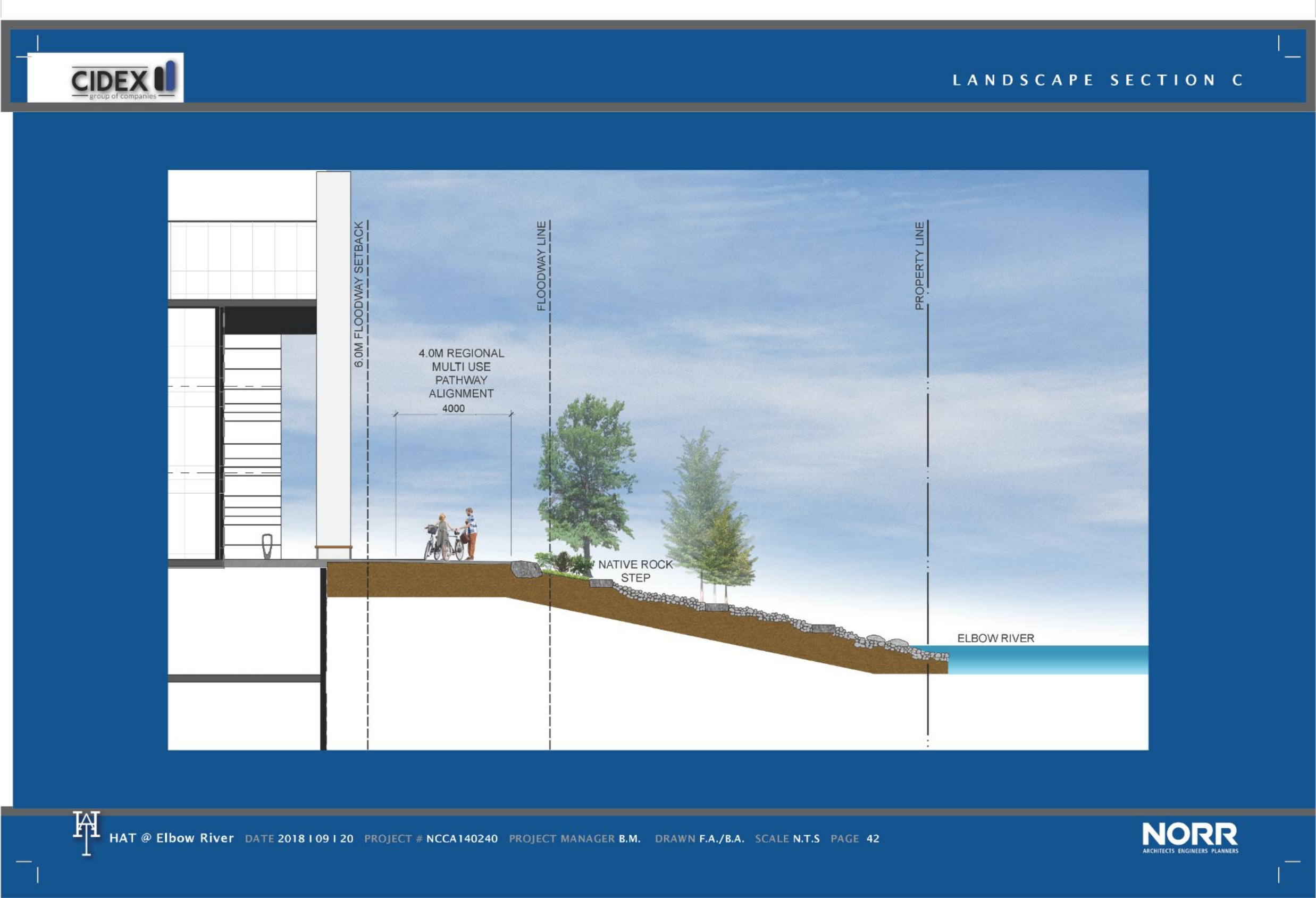
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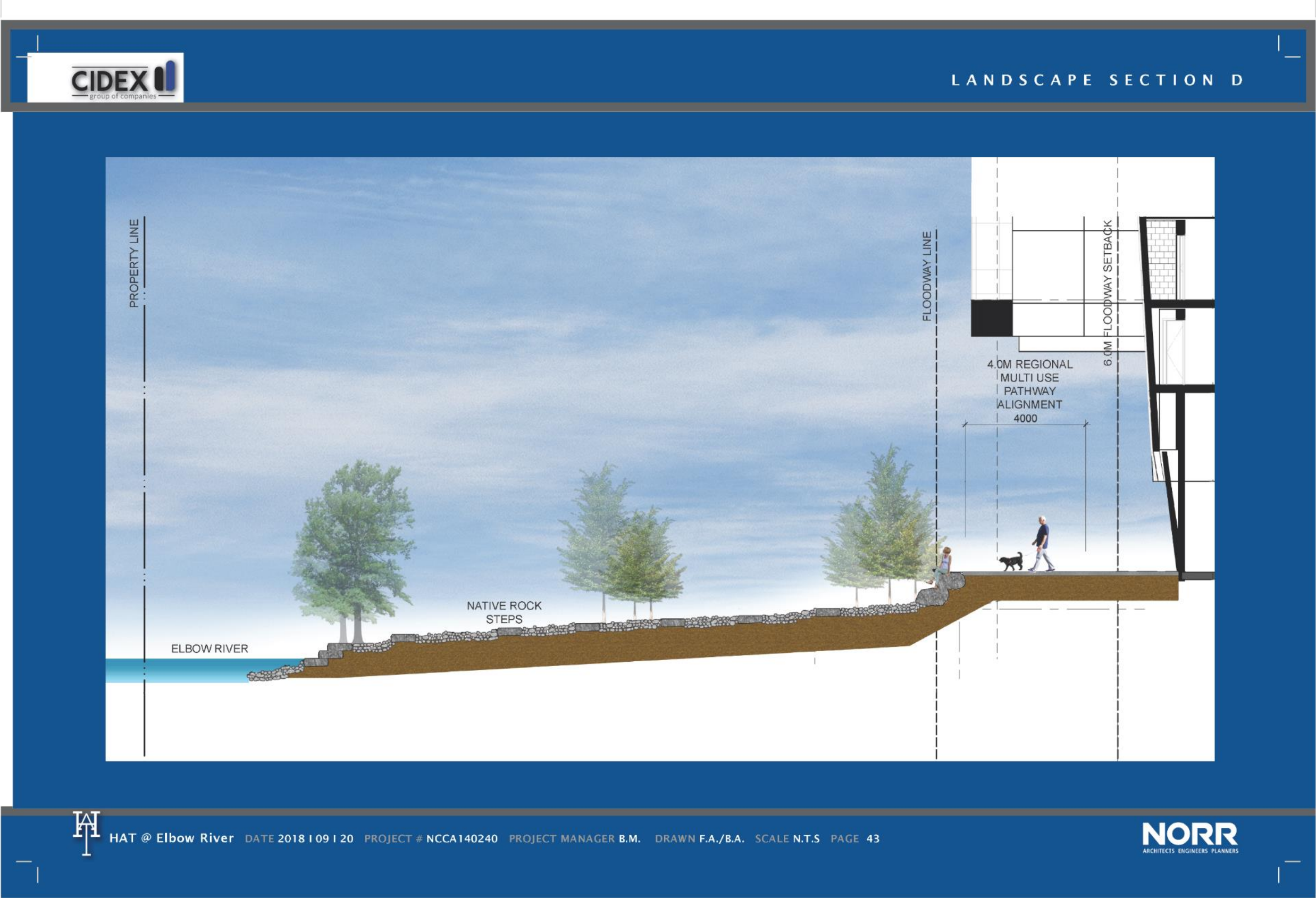
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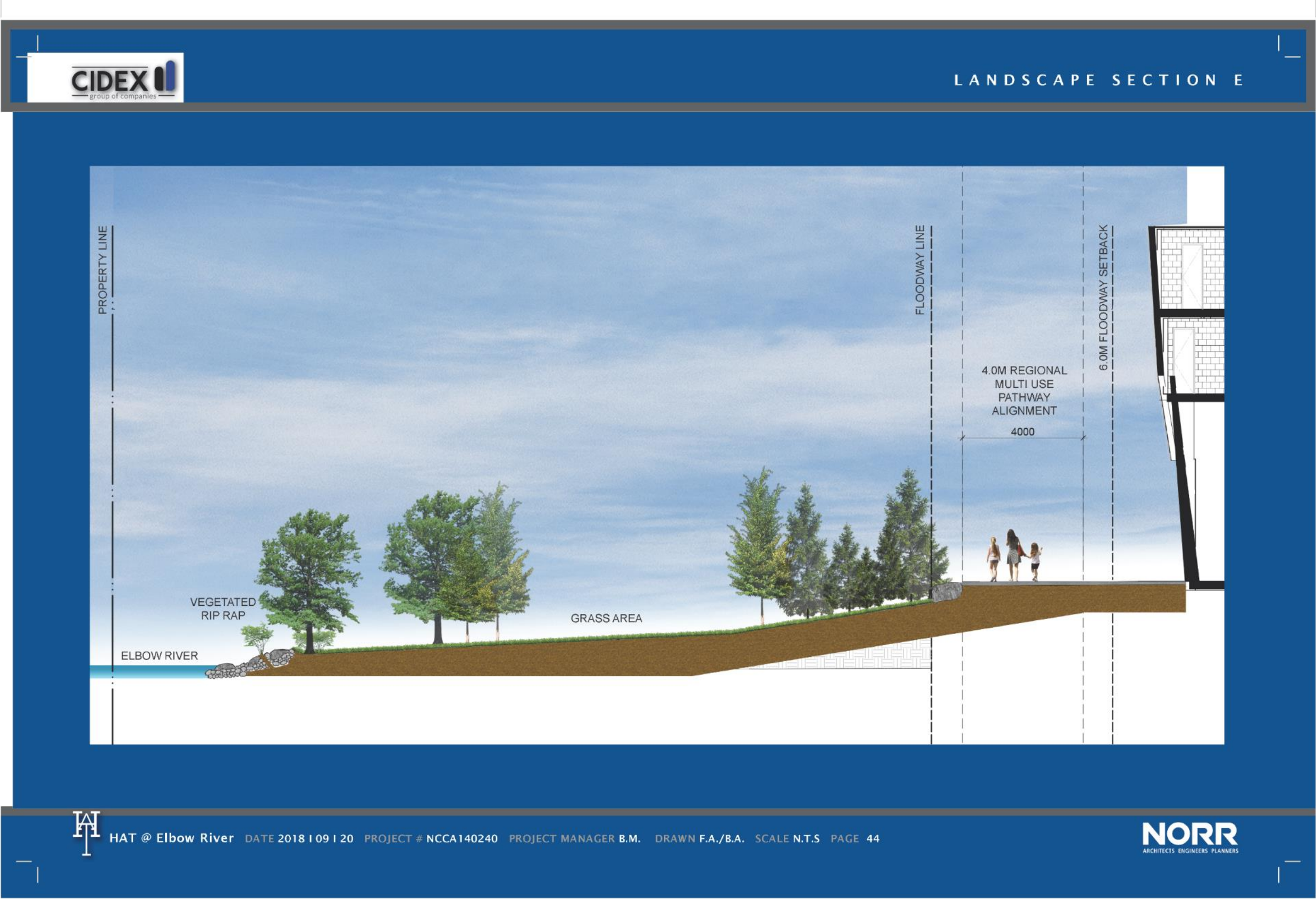
HAT @ Elbow River CPC Presentation



HAT @ Elbow River CPC Presentation



HAT @ Elbow River CPC Presentation



HAT @ Elbow River CPC Presentation



LANDSCAPE FEATURES



- BENCHES
- BIKE RACKS
- LED LIGHT STRIPS
- PAVERS
- PUBLIC PEDESTRIAN WALKWAY
- GRASSES AND PERENNIAL PLANTING
- NATIVE ROCK STEP AND SEATING
- 'TOUCH THE WATER' MOMENT
- VEGETATED ROCK RIPRAP
- NATIVE PLANTING





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ARCHITECTS ENGINEERS PLANNERS

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LANDSCAPE PLAN - Level 6.5





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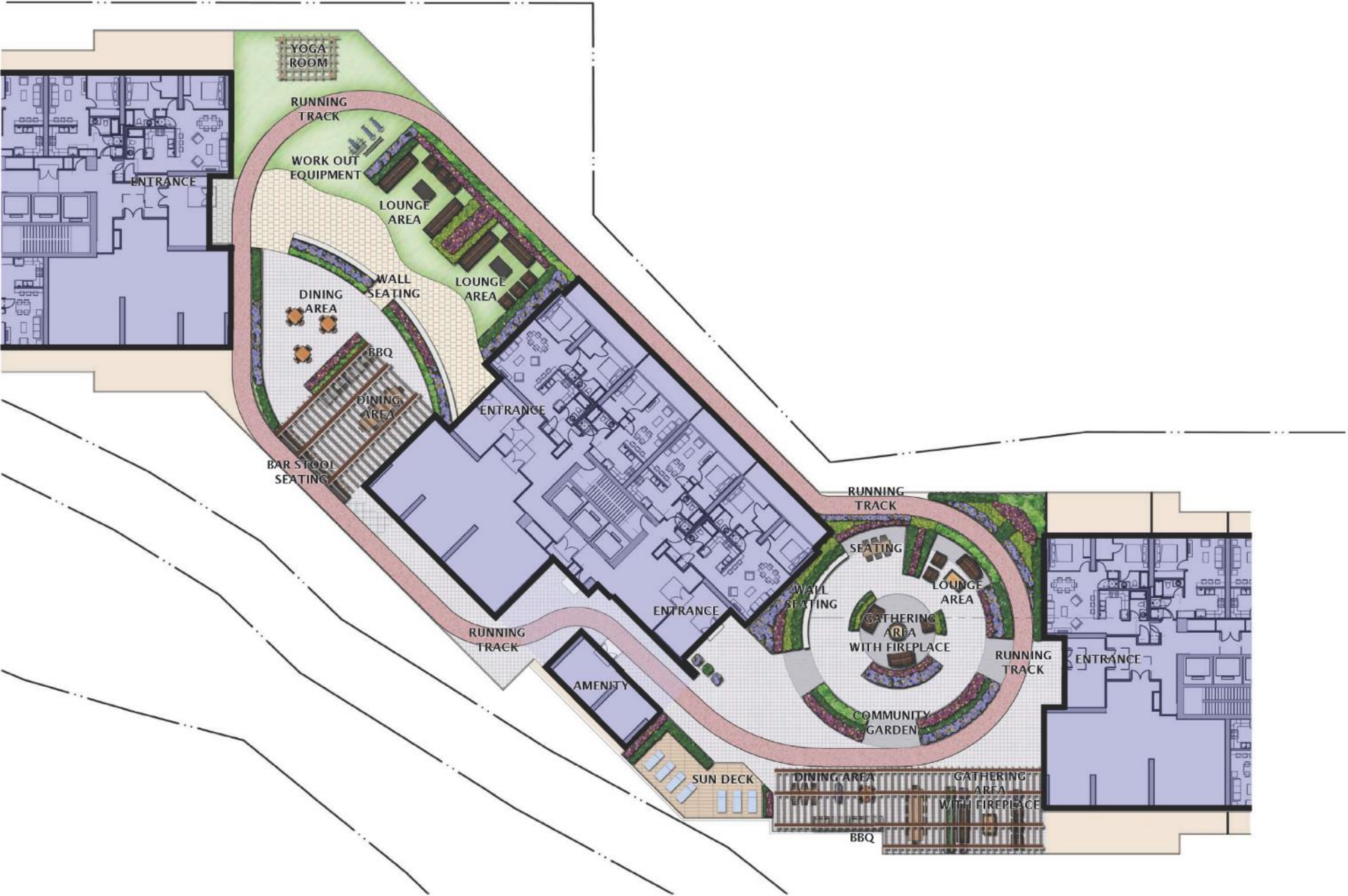
NORR
ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation



group of companies

LANDSCAPE PLAN - Level 10





HAT @ Elbow River

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ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation



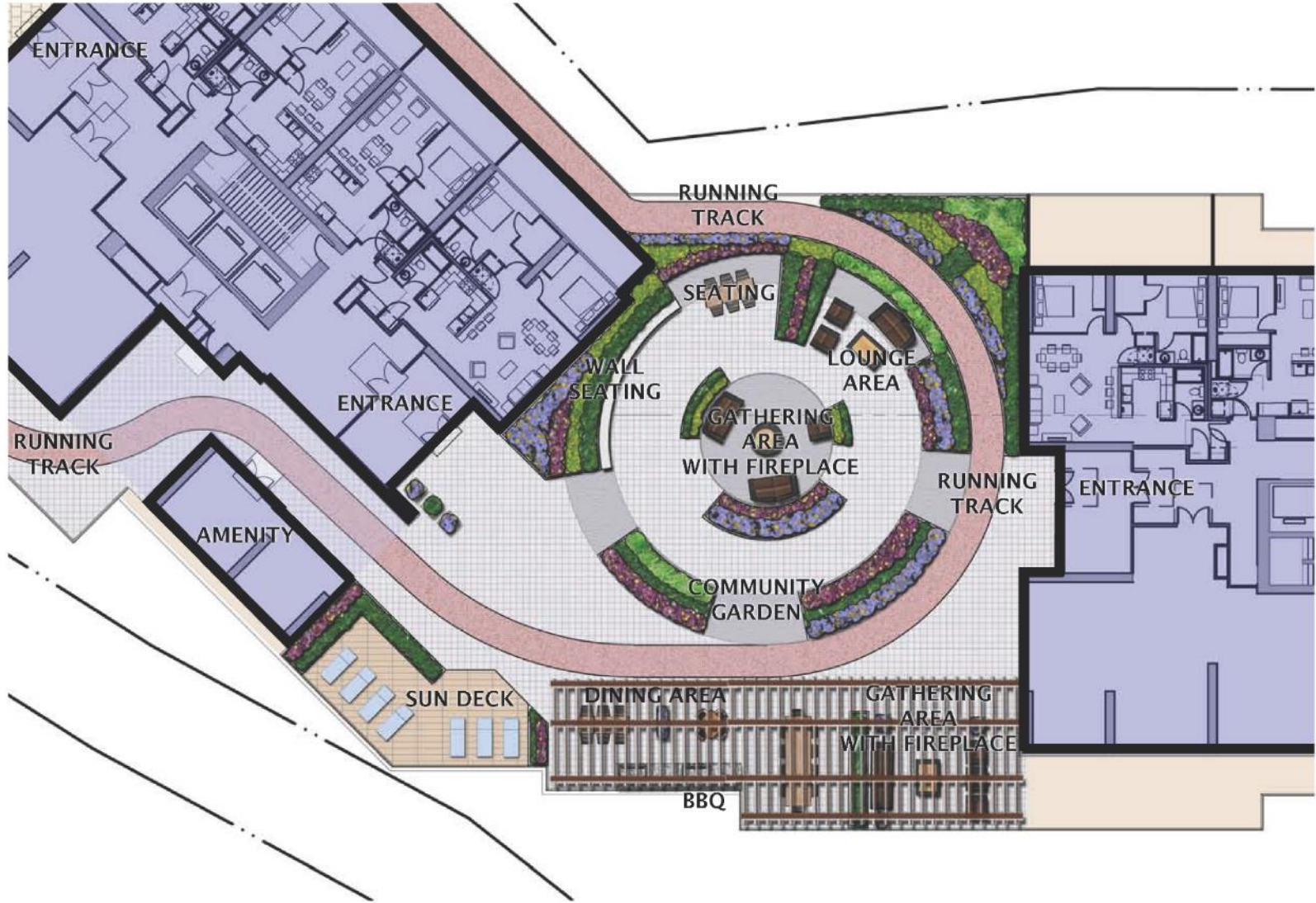
LANDSCAPE PLAN - Level 10 West Blow-up



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LANDSCAPE PLAN - Level 10 East Blow-up



HAT @ Elbow River CPC Presentation



FLOORPLANS - Tower 'A' - Level 10

TOWER 'A' AMENITIES
PROPOSED PROGRAM:

- 1) Community Space (Multi-Purpose Room, Meeting Room, Kitchen)
- 2) Two Temporary Rental Units for tenant's guests
- 3) Indoor Gym (By removing one more building unit)



TOWER 'A'
LEVEL 10 - AMENITY



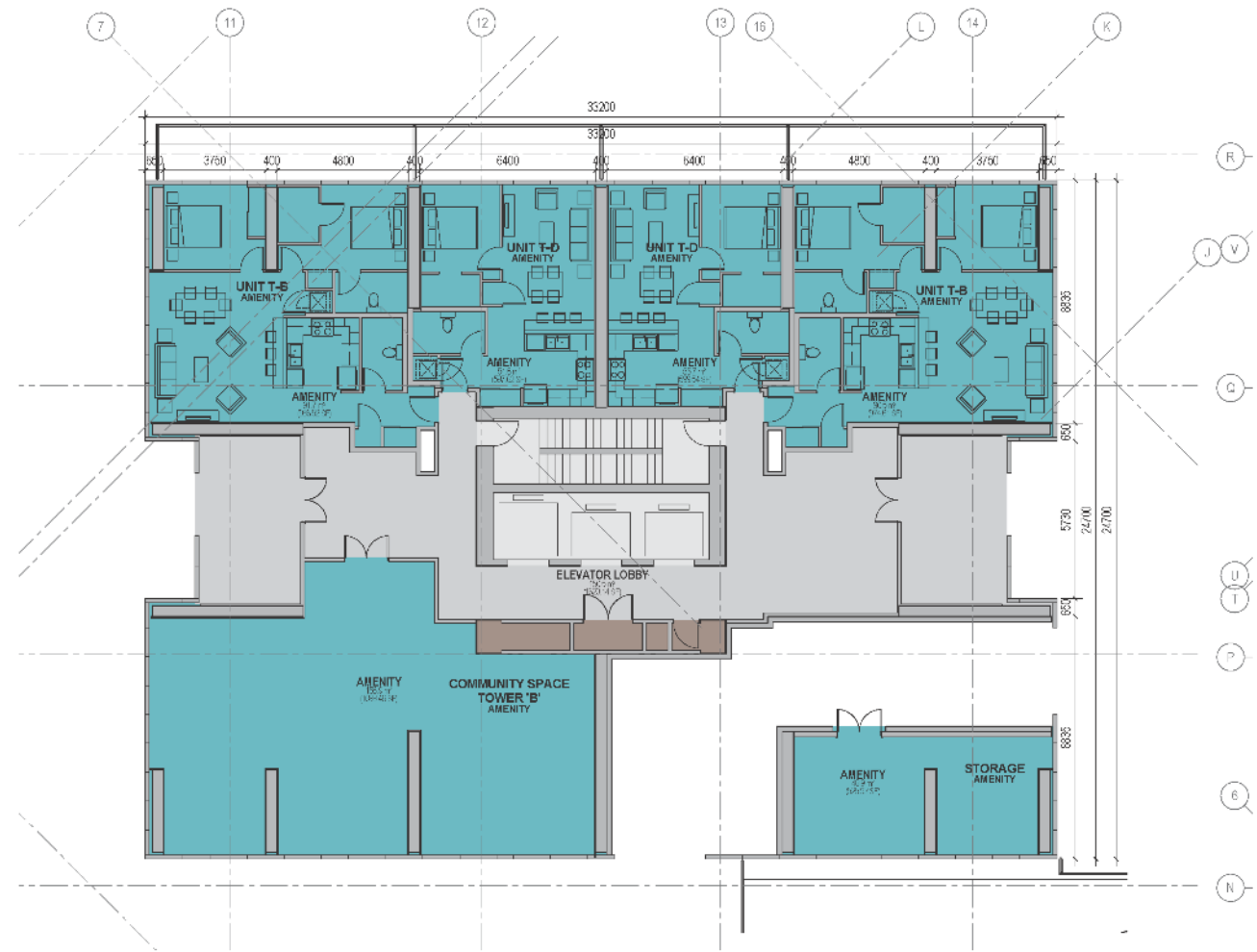
HAT @ Elbow River DATE 2018 1 09 1 20 PROJECT # NCCA140240 PROJECT MANAGER B.M. DRAWN F.A./B.A. SCALE N.T.S PAGE 50



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FLOORPLANS - Tower 'B' - Level 10



TOWER 'B' AMENITIES
PROPOSED PROGRAM:

- 1) Community Space (Multi-Purpose Room, Meeting Room, Kitchen)
- 2) Two Temporary Rental Units for tenant's guests
- 3) Indoor Gym
- 4) Movie Theatre
- 5) Storage / Washroom

TOWER 'B'
LEVEL 10 - AMENITY



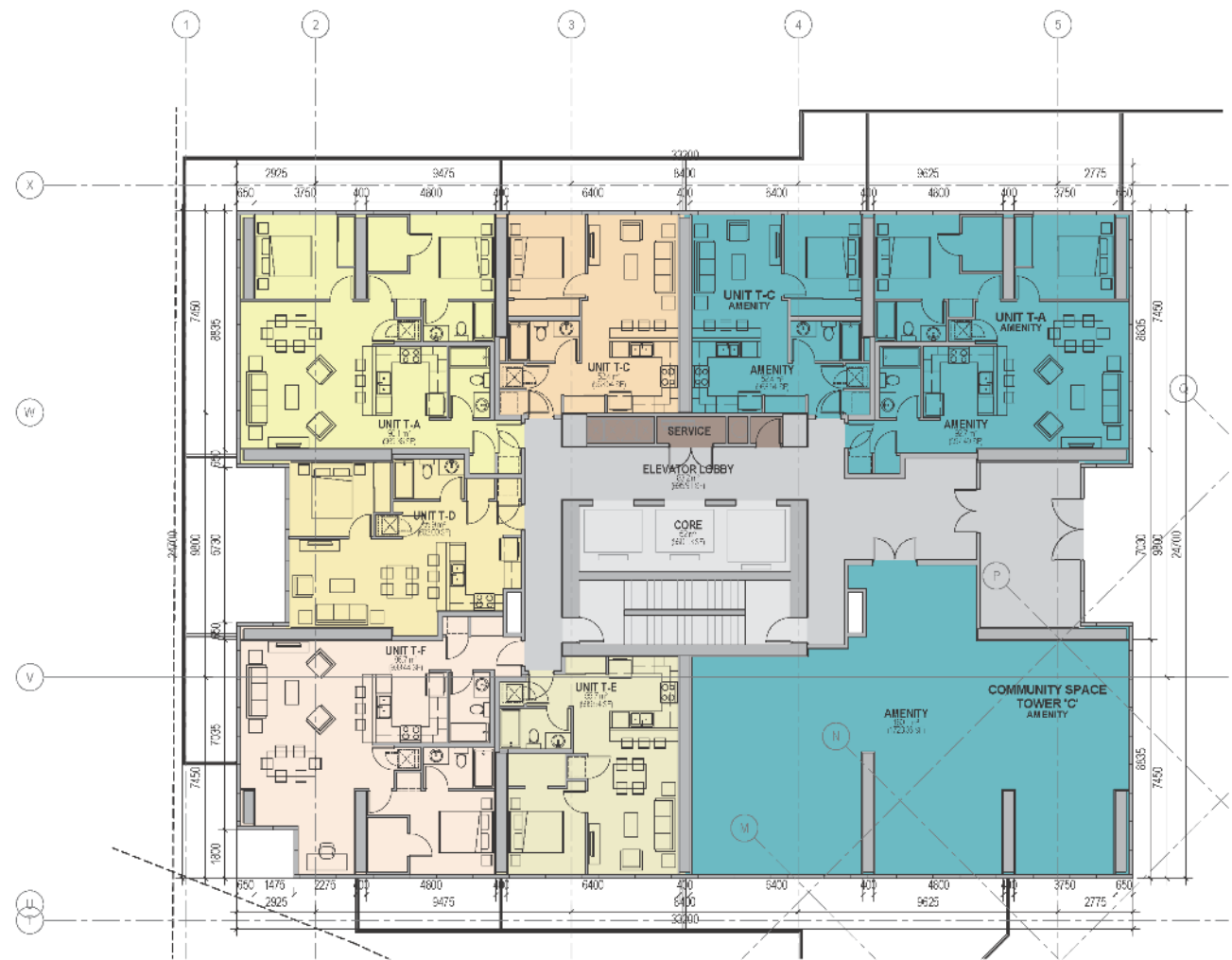
HAT @ Elbow River DATE 2018 1 09 1 20 PROJECT # NCCA140240 PROJECT MANAGER B.M. DRAWN F.A./B.A. SCALE N.T.S PAGE 51



HAT @ Elbow River CPC Presentation



FLOORPLANS - Tower 'C' - Level 10



TOWER 'C' AMENITIES
PROPOSED PROGRAM:

- 1) Community Space (Multi-Purpose Room, Meeting Room, Kitchen)
- 2) Two Temporary Rental Units for tenant's guests
- 3) Indoor Gym (By removing one more building unit)

TOWER 'C'
LEVEL 10 - AMENITY



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FLOORPLANS:
UNDERGROUND PARKING
PODIUM LEVELS
TOWER LEVELS



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ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation



FLOORPLAN - Level P2

USES LEGEND

- ADMIN

AMENITY

CIRCULATION

COFFEE

CORE

CORRIDOR

CRU

ELEVATOR LOBBY

FLEX

LOADINGS 01

LOBBY AMENITY

LOBBY TOWER 'A'

LOBBY TOWER 'B'

LOBBY TOWER 'C'

MECHANICAL

OUTDOOR AMENITY

PARKING LEVEL 01

PARKING LEVEL 01 - MEZZANINE 01

PARKING LEVEL 01 - MEZZANINE 02

PARKING LEVEL 02

PARKING LEVEL 03

PARKING LEVEL 04

PARKING LEVEL 05

PARKING LEVEL 06

PARKING LEVEL P1

PARKING LEVEL P2
- RESTAURANT

SERVICE

STORAGE

UNIT P-A

UNIT P-A1

UNIT P-B

UNIT P-B1

UNIT P-C

UNIT P-C1

UNIT P-D

UNIT P-E

UNIT P-F

UNIT P-G

UNIT P-H

UNIT P-J

UNIT P-K

UNIT P-L

UNIT P-M

UNIT P-H-A

UNIT P-H-B

UNIT P-H-C

UNIT P-H-D

UNIT T-A

UNIT T-B

UNIT T-C

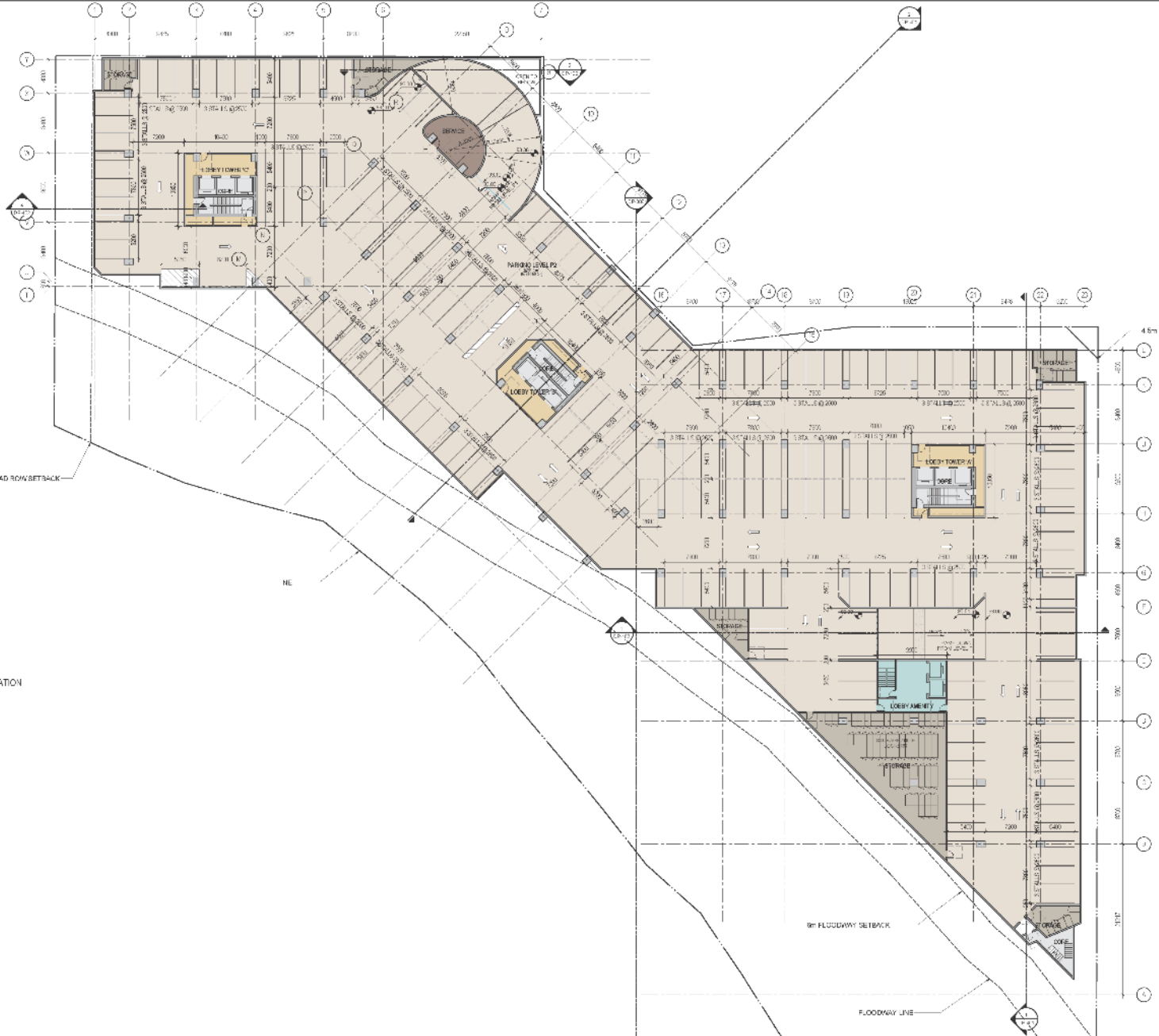
UNIT T-D
- UNIT T-E

UNIT T-F

UNIT T-G

UNIT T-H

VERTICAL CIRCULATION



HAT @ Elbow River CPC Presentation



group of companies

FLOORPLAN - Level P1

USES LEGEND

- ADMIN

AMENITY

CIRCULATION

COFFEE

CORE

CORRIDOR

CRU

ELEVATOR LOBBY

FLEX

LOADING 01

LOBBY AMENITY

LOBBY TOWER 'A'

LOBBY TOWER 'B'

LOBBY TOWER 'C'

MECHANICAL

OUTDOOR AMENITY

PARKING LEVEL 01

PARKING LEVEL 01 - MEZZANINE 01

PARKING LEVEL 01 - MEZZANINE 02

PARKING LEVEL 02

PARKING LEVEL 03

PARKING LEVEL 04

PARKING LEVEL 05

PARKING LEVEL 06

PARKING LEVEL P1

PARKING LEVEL P2
- RESTAURANT

SERVICE

STORAGE

UNIT P-A

UNIT P-A1

UNIT P-B

UNIT P-C

UNIT P-C1

UNIT P-D

UNIT P-E

UNIT P-F

UNIT P-G

UNIT P-H

UNIT P-J

UNIT P-K

UNIT P-L

UNIT P-M

UNIT P-H-A

UNIT P-H-B

UNIT P-H-C

UNIT P-H-D

UNIT T-A

UNIT T-B

UNIT T-C

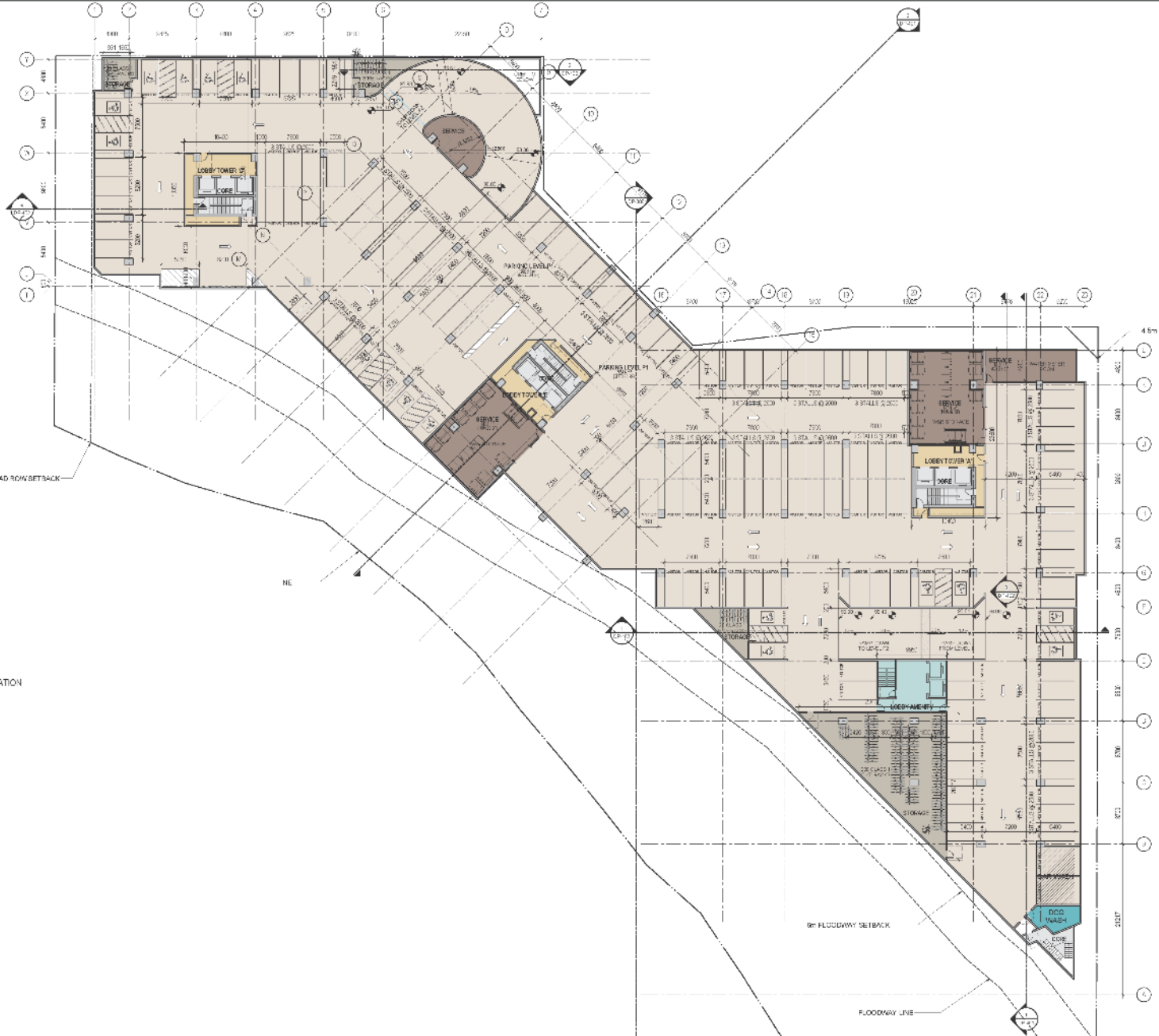
UNIT T-D
- UNIT T-E

UNIT T-F

UNIT T-G

UNIT T-H

VERTICAL CIRCULATION



HAT @ Elbow River CPC Presentation



FLOORPLAN - Level 1 - Main Floor

USES LEGEND

- ADMIN

AMENITY

CIRCULATION

COFFEE

CORRIDOR

CRU

ELEVATOR LOBBY

FLEX

LOADING 01

LOBBY AMENITY

LOBBY TOWER 'A'

LOBBY TOWER 'B'

LOBBY TOWER 'C'

MECHANICAL

OUTDOOR AMENITY

PARKING LEVEL 01

PARKING LEVEL 01 - MEZZANINE 01

PARKING LEVEL 01 - MEZZANINE 02

PARKING LEVEL 02

PARKING LEVEL 03

PARKING LEVEL 04

PARKING LEVEL 05

PARKING LEVEL 06

PARKING LEVEL P1

PARKING LEVEL P2
- RESTAURANT

SERVICE

STORAGE

UNIT P-A

UNIT P-A1

UNIT P-B

UNIT P-B1

UNIT P-C

UNIT P-C1

UNIT P-D

UNIT P-E

UNIT P-F

UNIT P-G

UNIT P-H

UNIT P-J

UNIT P-K

UNIT P-L

UNIT P-M

UNIT P-H-A

UNIT P-H-B

UNIT P-H-C

UNIT P-H-D

UNIT T-A

UNIT T-B

UNIT T-C

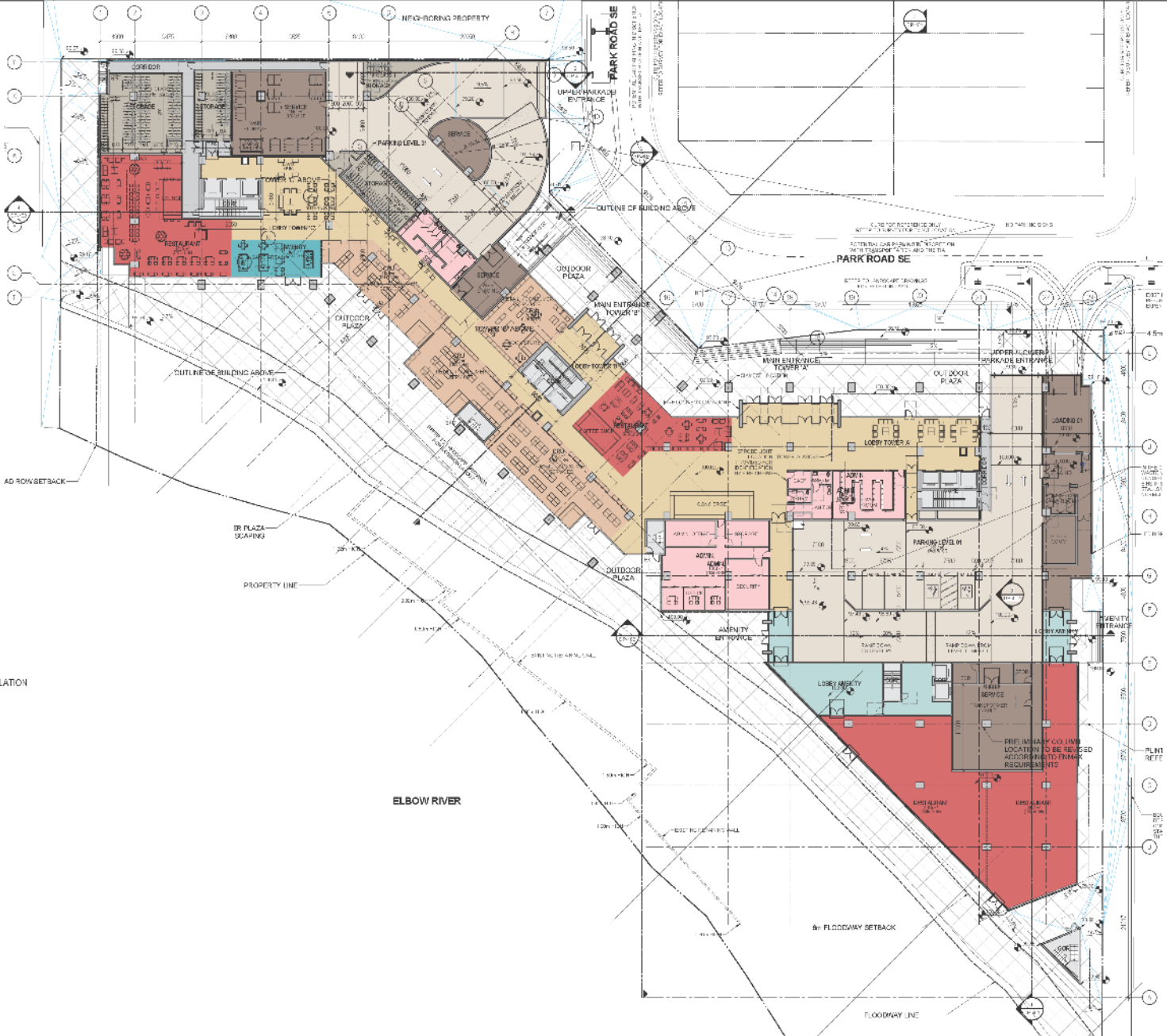
UNIT T-D
- UNIT T-E

UNIT T-F

UNIT T-G

UNIT T-H

VERTICAL CIRCULATION



HAT @ Elbow River CPC Presentation

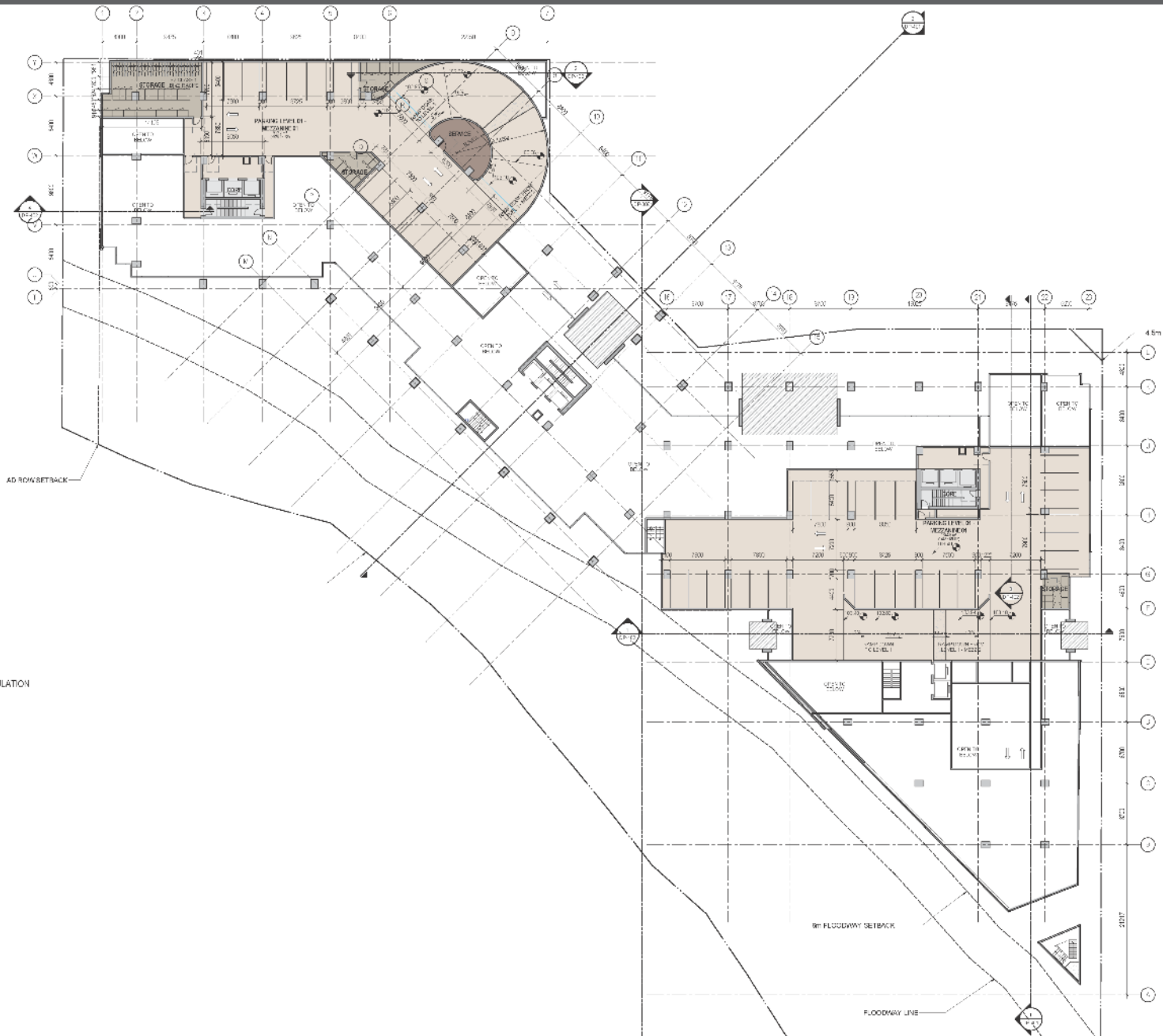


FLOORPLAN - Level 1 - Mezzanine 1

USES LEGEND

- | | |
|---------------------------------|------------|
| ADMIN | RESTAURANT |
| AMENITY | SERVICE |
| CIRCULATION | STORAGE |
| COFFEE | UNIT P-A |
| CORE | UNIT P-A1 |
| CORRIDOR | UNIT P-B |
| CRU | UNIT P-B1 |
| ELEVATOR LOBBY | UNIT P-C |
| FLEX | UNIT P-C1 |
| LOADING 01 | UNIT P-D |
| LOBBY AMENITY | UNIT P-E |
| LOBBY TOWER 'A' | UNIT P-F |
| LOBBY TOWER 'B' | UNIT P-G |
| LOBBY TOWER 'C' | UNIT P-H |
| MECHANICAL | UNIT P-I |
| OUTDOOR AMENITY | UNIT P-J |
| PARKING LEVEL 01 | UNIT P-K |
| PARKING LEVEL 01 - MEZZANINE 01 | UNIT P-L |
| PARKING LEVEL 01 - MEZZANINE 02 | UNIT P-M |
| PARKING LEVEL 02 | UNIT P-H-A |
| PARKING LEVEL 03 | UNIT P-H-B |
| PARKING LEVEL 04 | UNIT P-H-C |
| PARKING LEVEL 05 | UNIT P-H-D |
| PARKING LEVEL 06 | UNIT T-A |
| PARKING LEVEL P1 | UNIT T-B |
| PARKING LEVEL P2 | UNIT T-C |
| | UNIT T-D |

- | |
|----------------------|
| UNIT T-E |
| UNIT T-F |
| UNIT T-G |
| UNIT T-H |
| VERTICAL CIRCULATION |



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NORR
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HAT @ Elbow River CPC Presentation

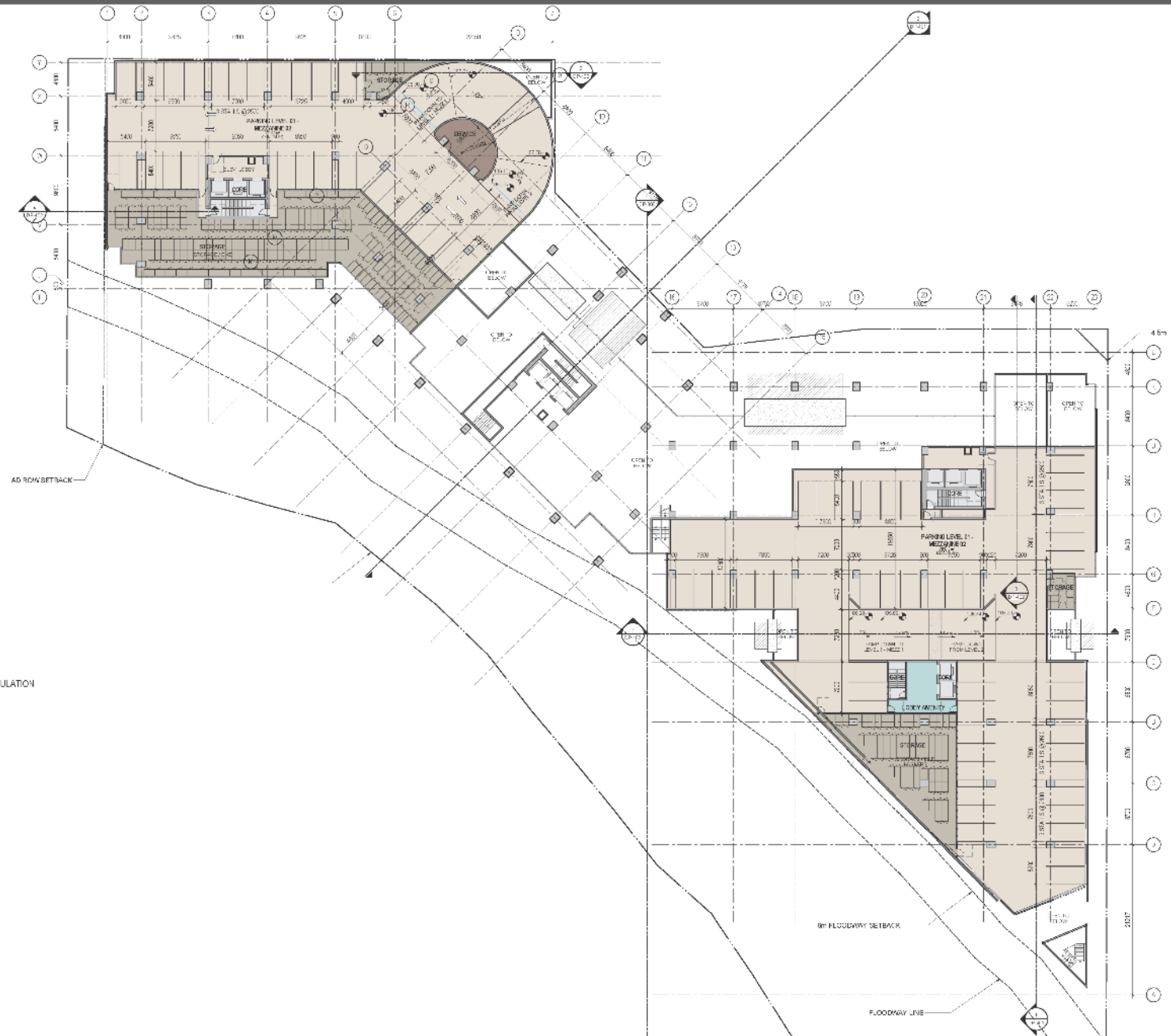


FLOORPLAN - Level 1 - Mezzanine 2

USES LEGEND

ADMIN	RESTAURANT
AMENITY	SERVICE
CIRCULATION	STORAGE
COFFEE	UNIT P-A
CORE	UNIT P-A1
CORRIDOR	UNIT P-B
CRU	UNIT P-B1
ELEVATOR LOBBY	UNIT P-C
FLEX	UNIT P-C1
LOADING 01	UNIT P-D
LOBBY AMENITY	UNIT P-E
LOBBY TOWER 'A'	UNIT P-F
LOBBY TOWER 'B'	UNIT P-G
LOBBY TOWER 'C'	UNIT P-H
MECHANICAL	UNIT P-I
OUTDOOR AMENITY	UNIT P-K
PARKING LEVEL 01	UNIT P-L
PARKING LEVEL 01 - MEZZANINE 01	UNIT P-M
PARKING LEVEL 01 - MEZZANINE 02	UNIT P-H-A
PARKING LEVEL 02	UNIT P-H-B
PARKING LEVEL 03	UNIT P-H-C
PARKING LEVEL 04	UNIT P-H-D
PARKING LEVEL 05	UNIT T-A
PARKING LEVEL 06	UNIT T-B
PARKING LEVEL P1	UNIT T-C
PARKING LEVEL P2	UNIT T-D

UNIT E
UNIT T-F
UNIT T-G
UNIT T-H
VERTICAL CIRCULATION



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NORR
ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation

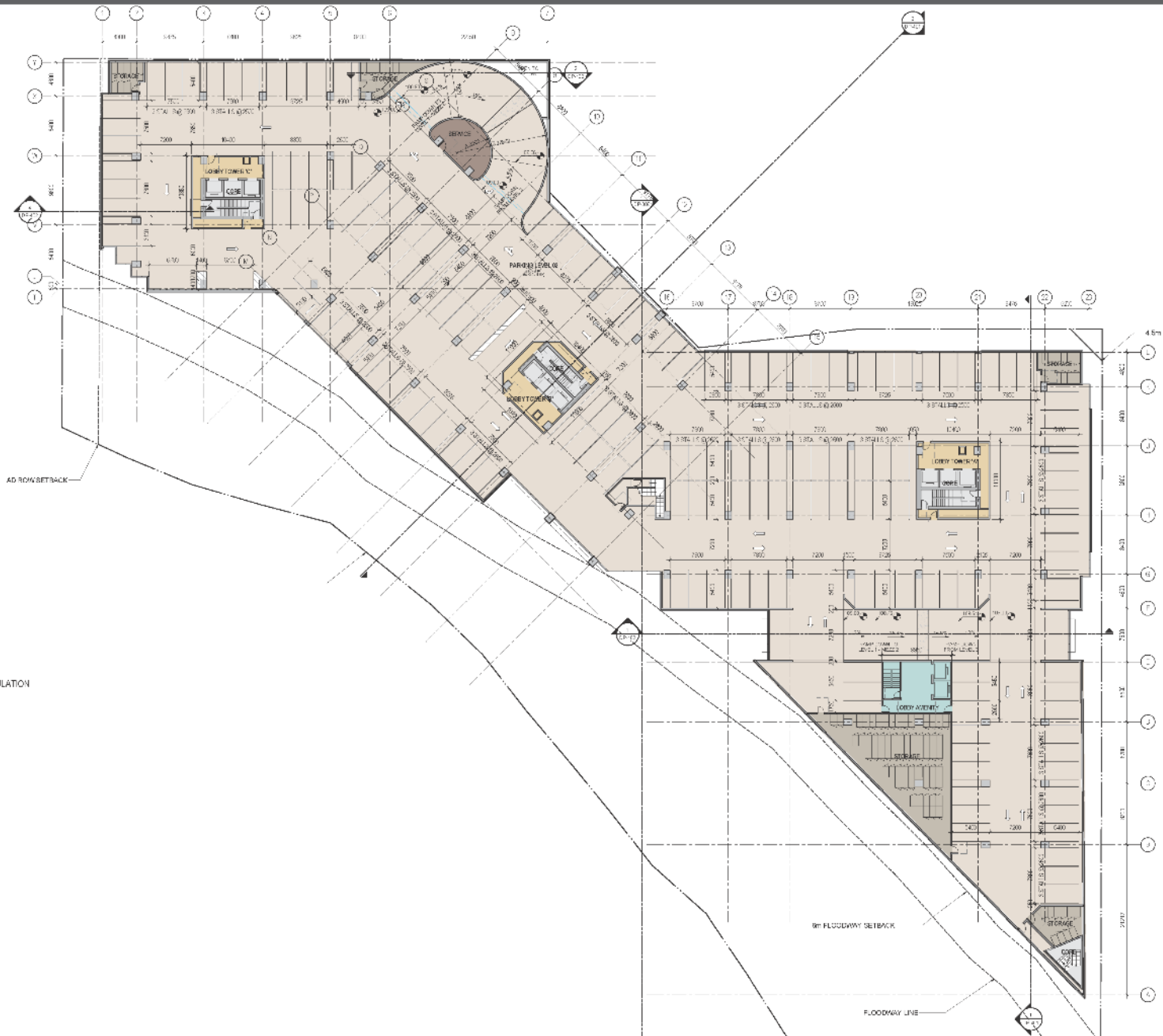


FLOORPLAN - Level 2 to 4 - Parkade

USES LEGEND

- | | |
|---------------------------------|------------|
| ADMIN | RESTAURANT |
| AMENITY | SERVICE |
| CIRCULATION | STORAGE |
| COFFEE | UNIT P-A |
| CORE | UNIT P-A1 |
| CORRIDOR | UNIT P-B |
| CRU | UNIT P-B1 |
| ELEVATOR LOBBY | UNIT P-C |
| FLEX | UNIT P-C1 |
| LOADING 01 | UNIT P-D |
| LOBBY AMENITY | UNIT P-E |
| LOBBY TOWER 'A' | UNIT P-F |
| LOBBY TOWER 'B' | UNIT P-G |
| LOBBY TOWER 'C' | UNIT P-H |
| MECHANICAL | UNIT P-J |
| OUTDOOR AMENITY | UNIT P-K |
| PARKING LEVEL 01 | UNIT P-L |
| PARKING LEVEL 01 - MEZZANINE 01 | UNIT P-M |
| PARKING LEVEL 01 - MEZZANINE 02 | UNIT P-H-A |
| PARKING LEVEL 02 | UNIT P-H-B |
| PARKING LEVEL 03 | UNIT P-H-C |
| PARKING LEVEL 04 | UNIT P-H-D |
| PARKING LEVEL 05 | UNIT T-A |
| PARKING LEVEL 06 | UNIT T-B |
| PARKING LEVEL P1 | UNIT T-C |
| PARKING LEVEL P2 | UNIT T-D |

- | |
|----------------------|
| UNIT T-E |
| UNIT T-F |
| UNIT T-G |
| UNIT T-H |
| VERTICAL CIRCULATION |



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NORR
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HAT @ Elbow River CPC Presentation

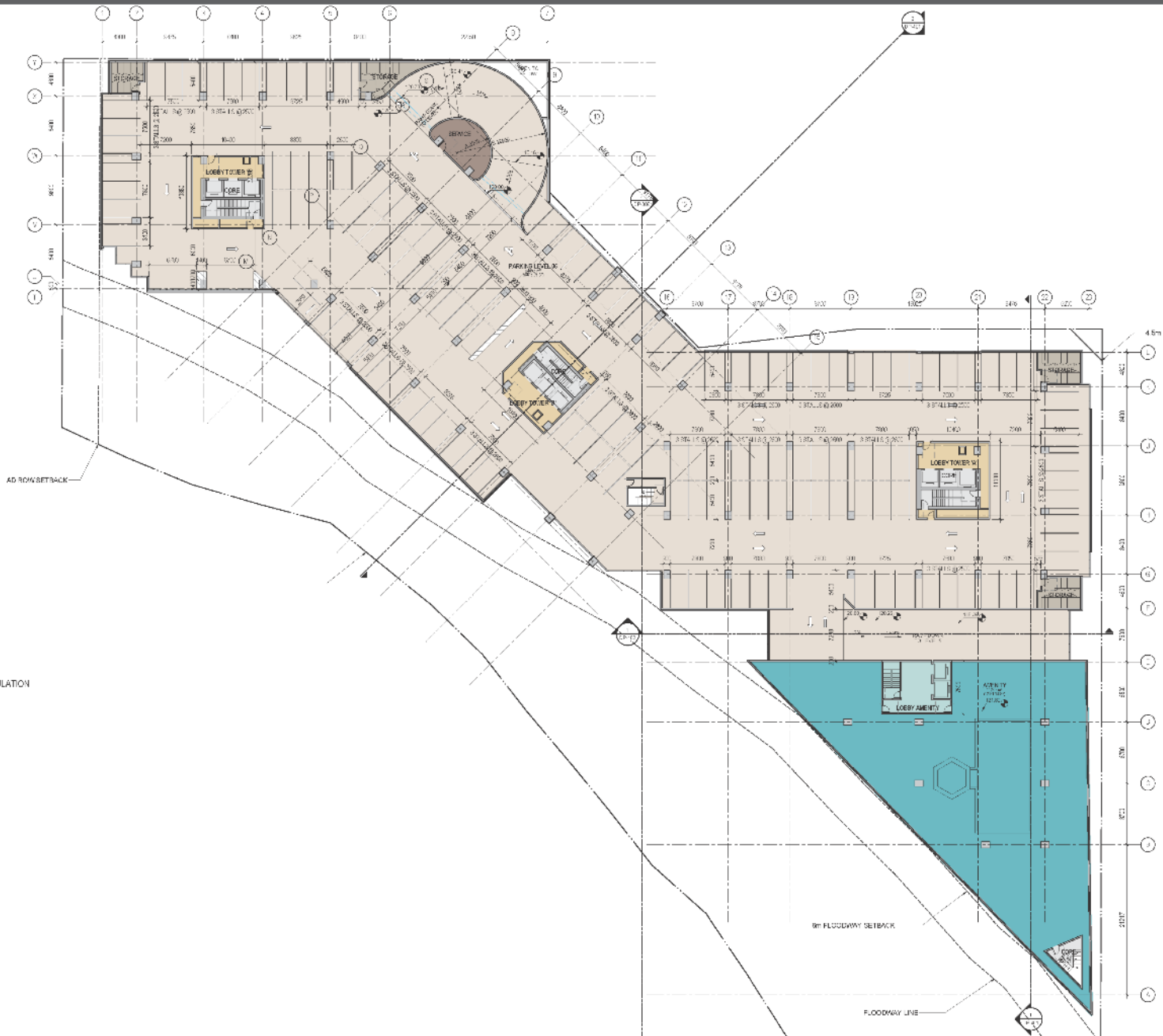


FLOORPLAN - Level 5 & 6 - Parkade & Amenity

USES LEGEND

- | | |
|---------------------------------|------------|
| ADMIN | RESTAURANT |
| AMENITY | SERVICE |
| CIRCULATION | STORAGE |
| COFFEE | UNIT P-A |
| CORE | UNIT P-A1 |
| CORRIDOR | UNIT P-B |
| CRU | UNIT P-B1 |
| ELEVATOR LOBBY | UNIT P-C |
| FLEX | UNIT P-C1 |
| LOADING 01 | UNIT P-D |
| LOBBY AMENITY | UNIT P-E |
| LOBBY TOWER 'A' | UNIT P-F |
| LOBBY TOWER 'B' | UNIT P-G |
| LOBBY TOWER 'C' | UNIT P-H |
| MECHANICAL | UNIT P-I |
| OUTDOOR AMENITY | UNIT P-K |
| PARKING LEVEL 01 | UNIT P-L |
| PARKING LEVEL 01 - MEZZANINE 01 | UNIT P-M |
| PARKING LEVEL 01 - MEZZANINE 02 | UNIT P-H-A |
| PARKING LEVEL 02 | UNIT P-H-B |
| PARKING LEVEL 03 | UNIT P-H-C |
| PARKING LEVEL 04 | UNIT P-H-D |
| PARKING LEVEL 05 | UNIT T-A |
| PARKING LEVEL 06 | UNIT T-B |
| PARKING LEVEL P1 | UNIT T-C |
| PARKING LEVEL P2 | UNIT T-D |

- | |
|----------------------|
| UNIT T-E |
| UNIT T-F |
| UNIT T-G |
| UNIT T-H |
| VERTICAL CIRCULATION |



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NORR
ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation

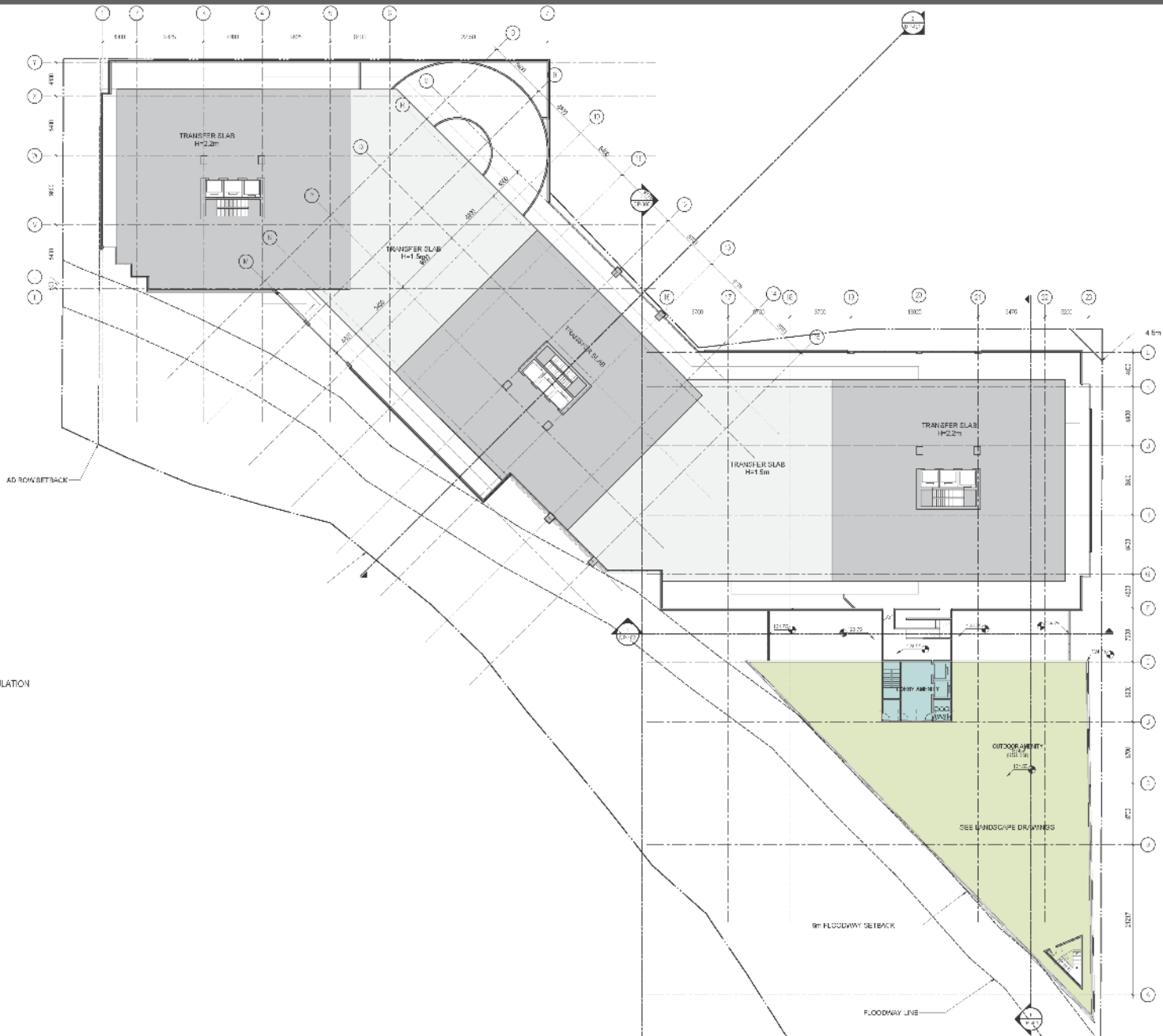


FLOORPLAN - Level 6.5 - Outdoor Amenity

USES LEGEND

ADMIN	RESTAURANT
AMENITY	SERVICE
CIRCULATION	STORAGE
COFFEE	UNIT P-A
CORE	UNIT P-A1
CORRIDOR	UNIT P-B
CRU	UNIT P-B1
ELEVATOR LOBBY	UNIT P-C
FLEX	UNIT P-C1
LOADING 01	UNIT P-D
LOBBY AMENITY	UNIT P-E
LOBBY TOWER 'A'	UNIT P-F
LOBBY TOWER 'B'	UNIT P-G
LOBBY TOWER 'C'	UNIT P-H
MECHANICAL	UNIT P-I
OUTDOOR AMENITY	UNIT P-K
PARKING LEVEL 01	UNIT P-L
PARKING LEVEL 01 - MEZZANINE 01	UNIT P-M
PARKING LEVEL 01 - MEZZANINE 02	UNIT P-H-A
PARKING LEVEL 02	UNIT P-H-B
PARKING LEVEL 03	UNIT P-H-C
PARKING LEVEL 04	UNIT P-H-D
PARKING LEVEL 05	UNIT T-A
PARKING LEVEL 06	UNIT T-B
PARKING LEVEL P1	UNIT T-C
PARKING LEVEL P2	UNIT T-D

UNIT E
UNIT T-F
UNIT T-G
UNIT T-H
VERTICAL CIRCULATION



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NORR
ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation



group of companies

FLOORPLAN - Level 7 - Podium

USES LEGEND

- ADMIN

AMENITY

CIRCULATION

COFFEE

CORE

CORRIDOR

CRU

ELEVATOR LOBBY

FLEX

LOADING 01

LOBBY AMENITY

LOBBY TOWER 'A'

LOBBY TOWER 'B'

LOBBY TOWER 'C'

MECHANICAL

OUTDOOR AMENITY

PARKING LEVEL 01

PARKING LEVEL 01 - MEZZANINE 01

PARKING LEVEL 01 - MEZZANINE 02

PARKING LEVEL 02

PARKING LEVEL 03

PARKING LEVEL 04

PARKING LEVEL 05

PARKING LEVEL 06

PARKING LEVEL P1

PARKING LEVEL P2
- RESTAURANT

SERVICE

STORAGE

UNIT P-A

UNIT P-A1

UNIT P-B

UNIT P-B1

UNIT P-C

UNIT P-C1

UNIT P-D

UNIT P-E

UNIT P-F

UNIT P-G

UNIT P-H

UNIT P-J

UNIT P-K

UNIT P-L

UNIT P-M

UNIT P-H-A

UNIT P-H-B

UNIT P-H-C

UNIT P-H-D

UNIT T-A

UNIT T-B

UNIT T-C

UNIT T-D
- UNIT T-E

UNIT T-F

UNIT T-G

UNIT T-H

VERTICAL CIRCULATION



HAT @ Elbow River DATE 2018 1 09 1 20 PROJECT # NCCA140240 PROJECT MANAGER B.M. DRAWN F.A./B.A. SCALE N.T.S PAGE 62

NORR
ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation



FLOORPLAN - Level 8-9 Typical - Podium

USES LEGEND

- | | |
|---------------------------------|------------|
| ADMIN | RESTAURANT |
| AMENITY | SERVICE |
| CIRCULATION | STORAGE |
| COFFEE | UNIT P-A |
| CORE | UNIT P-A1 |
| CORRIDOR | UNIT P-A6 |
| CRU | UNIT P-C |
| ELEVATOR LOBBY | UNIT P-C1 |
| FLEX | UNIT P-D |
| LOADING 01 | UNIT P-E |
| LOBBY AMENITY | UNIT P-F |
| LOBBY TOWER 'A' | UNIT P-G |
| LOBBY TOWER 'B' | UNIT P-H |
| LOBBY TOWER 'C' | UNIT P-J |
| MECHANICAL | UNIT P-K |
| OUTDOOR AMENITY | UNIT P-L |
| PARKING LEVEL 01 | UNIT P-M |
| PARKING LEVEL 01 - MEZZANINE 01 | UNIT P-H-A |
| PARKING LEVEL 01 - MEZZANINE 02 | UNIT P-H-B |
| PARKING LEVEL 02 | UNIT P-H-C |
| PARKING LEVEL 03 | UNIT P-H-D |
| PARKING LEVEL 04 | UNIT T-A |
| PARKING LEVEL 05 | UNIT T-B |
| PARKING LEVEL 06 | UNIT T-C |
| PARKING LEVEL P1 | UNIT T-D |
| PARKING LEVEL P2 | |

- | |
|----------------------|
| UNIT T-E |
| UNIT T-F |
| UNIT T-G |
| UNIT T-H |
| VERTICAL CIRCULATION |



HAT @ Elbow River DATE 2018 1 09 1 20 PROJECT # NCCA140240 PROJECT MANAGER B.M. DRAWN F.A./B.A. SCALE N.T.S PAGE 63

NORR
ARCHITECTS ENGINEERS PLANNERS

HAT @ Elbow River CPC Presentation



CIDEX
group of companies

FLOORPLAN - Level 10 - Podium Top

USES LEGEND

- ADMIN

AMENITY

CIRCULATION

COFFEE

CORE

CORRIDOR

CRU

ELEVATOR LOBBY

FLEX

LOADINGS 01

LOBBY AMENITY

LOBBY TOWER 'A'

LOBBY TOWER 'B'

LOBBY TOWER 'C'

MECHANICAL

OUTDOOR AMENITY

PARKING LEVEL 01

PARKING LEVEL 01 - MEZZANINE 01

PARKING LEVEL 01 - MEZZANINE 02

PARKING LEVEL 02

PARKING LEVEL 03

PARKING LEVEL 04

PARKING LEVEL 05

PARKING LEVEL 06

PARKING LEVEL P1

PARKING LEVEL P2
- RESTAURANT

SERVICE

STORAGE

UNIT P-A

UNIT P-A1

UNIT P-B

UNIT P-B1

UNIT P-C

UNIT P-C1

UNIT P-D

UNIT P-E

UNIT P-F

UNIT P-G

UNIT P-H

UNIT P-I

UNIT P-K

UNIT P-L

UNIT P-M

UNIT P-H-A

UNIT P-H-B

UNIT P-H-C

UNIT P-H-D

UNIT T-A

UNIT T-B

UNIT T-C

UNIT T-D
- UNIT T-E

UNIT T-F

UNIT T-G

UNIT T-H

VERTICAL CIRCULATION



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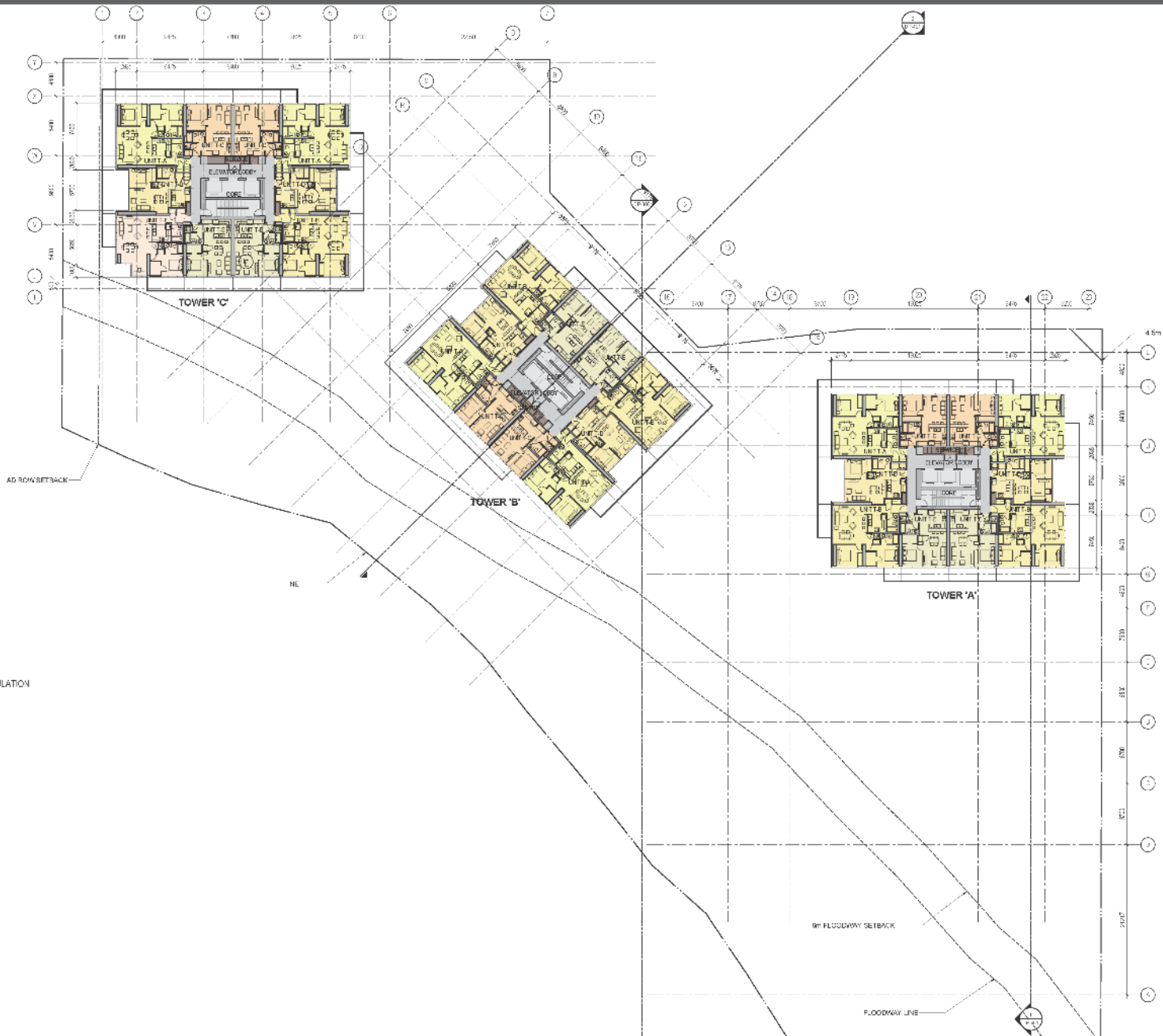


FLOORPLAN - Level 11 & UP - Towers Typical Level

USES LEGEND

ADMIN	RESTAURANT
AMENITY	SERVICE
CIRCULATION	STORAGE
COFFEE	UNIT P-A
CORE	UNIT P-A1
CORRIDOR	UNIT P-B
CRU	UNIT P-B1
ELEVATOR LOBBY	UNIT P-C
FLEX	UNIT P-C1
LOADING 01	UNIT P-D
LOBBY AMENITY	UNIT P-E
LOBBY TOWER 'A'	UNIT P-F
LOBBY TOWER 'B'	UNIT P-G
LOBBY TOWER 'C'	UNIT P-H
MECHANICAL	UNIT P-I
OUTDOOR AMENITY	UNIT P-K
PARKING LEVEL 01	UNIT P-L
PARKING LEVEL 01 - MEZZANINE 01	UNIT P-M
PARKING LEVEL 01 - MEZZANINE 02	UNIT P-H-A
PARKING LEVEL 02	UNIT P-H-B
PARKING LEVEL 03	UNIT P-H-C
PARKING LEVEL 04	UNIT P-H-D
PARKING LEVEL 05	UNIT T-A
PARKING LEVEL 06	UNIT T-B
PARKING LEVEL P1	UNIT T-C
PARKING LEVEL P2	UNIT T-D

UNIT T-E
UNIT T-F
UNIT T-G
UNIT T-H
VERTICAL CIRCULATION



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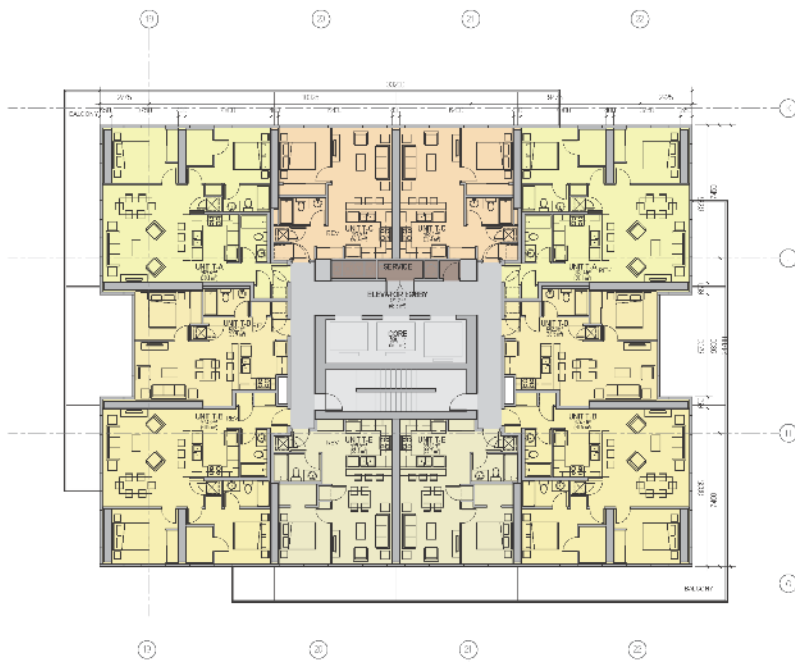
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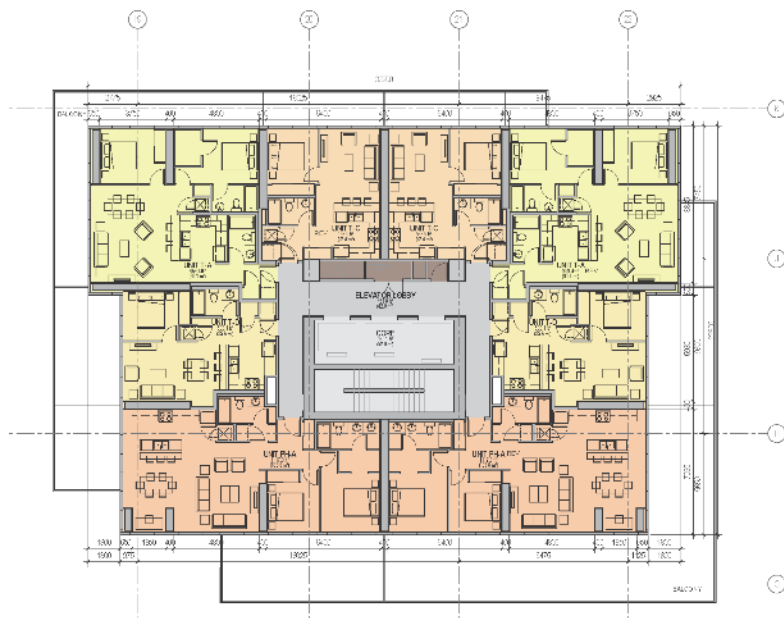
FLOORPLANS - Tower 'A'



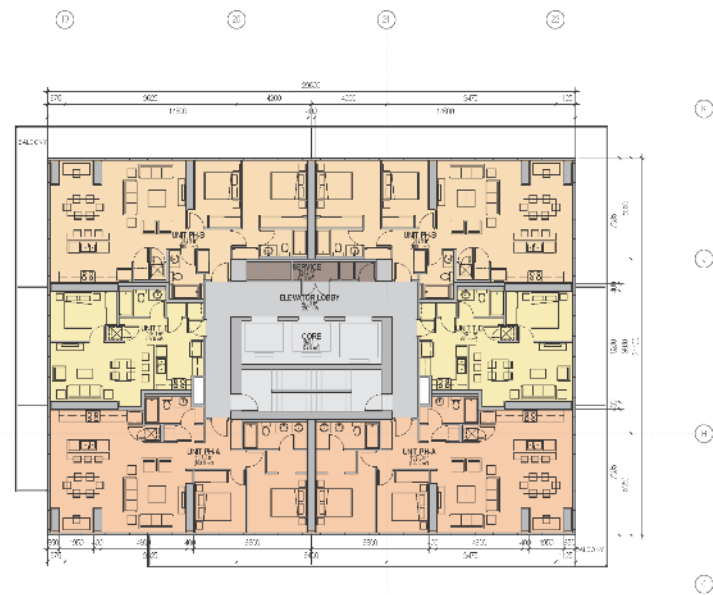
TOWER 'A'
LEVEL 10 - AMENITY



TOWER 'A'
TYPICAL LEVEL 11 TO 43



TOWER 'A'
TYPICAL LEVEL 44 TO 51



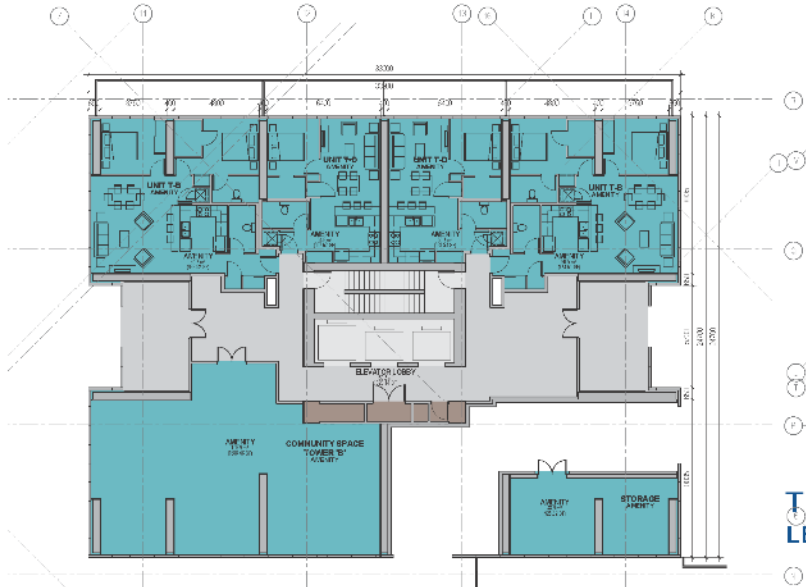
TOWER 'A'
TYPICAL LEVEL 52 TO 55



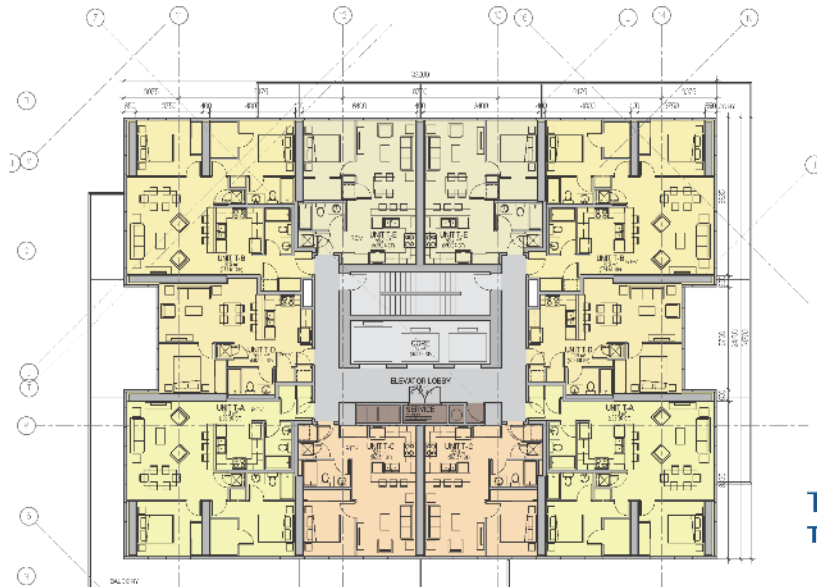
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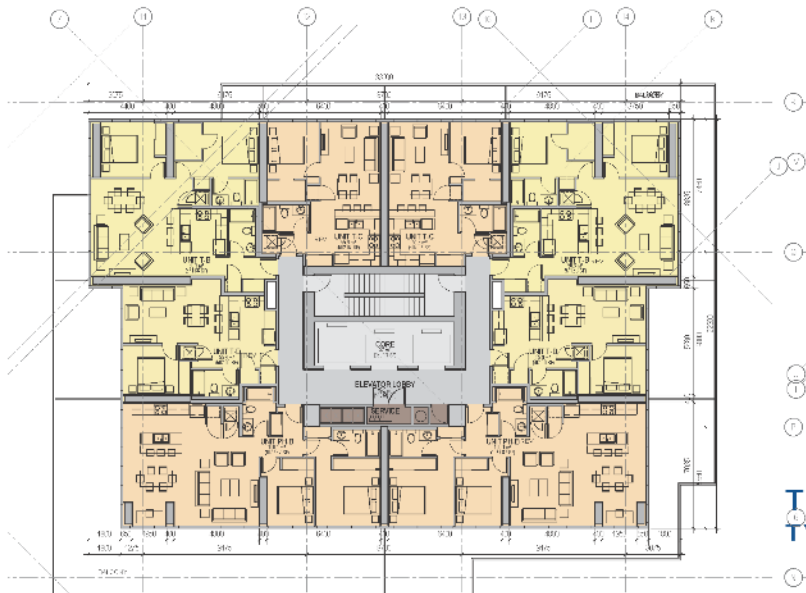
FLOORPLANS - Tower 'B'



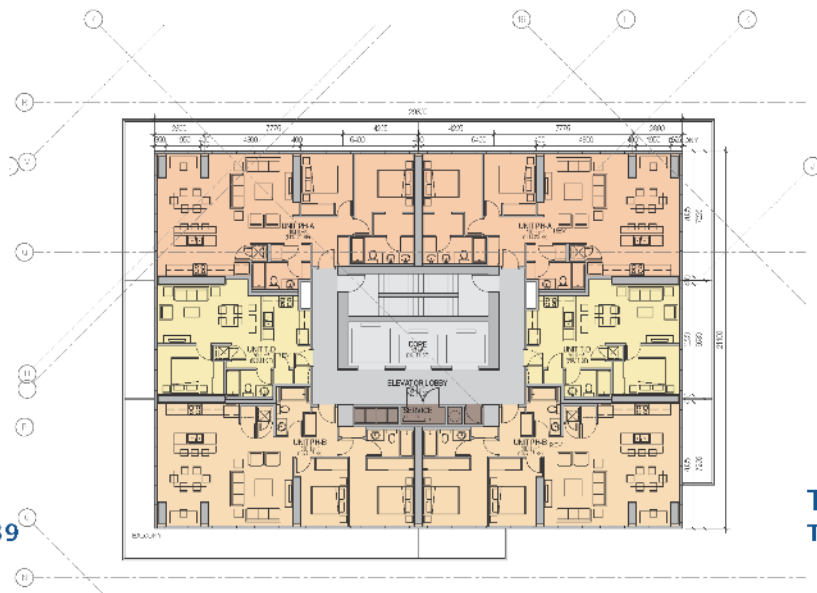
TOWER 'B'
LEVEL 10 - AMENITY



TOWER 'B'
TYPICAL LEVEL 11 TO 31



TOWER 'B'
TYPICAL LEVEL 32 TO 39



TOWER 'B'
TYPICAL LEVEL 40 TO 43



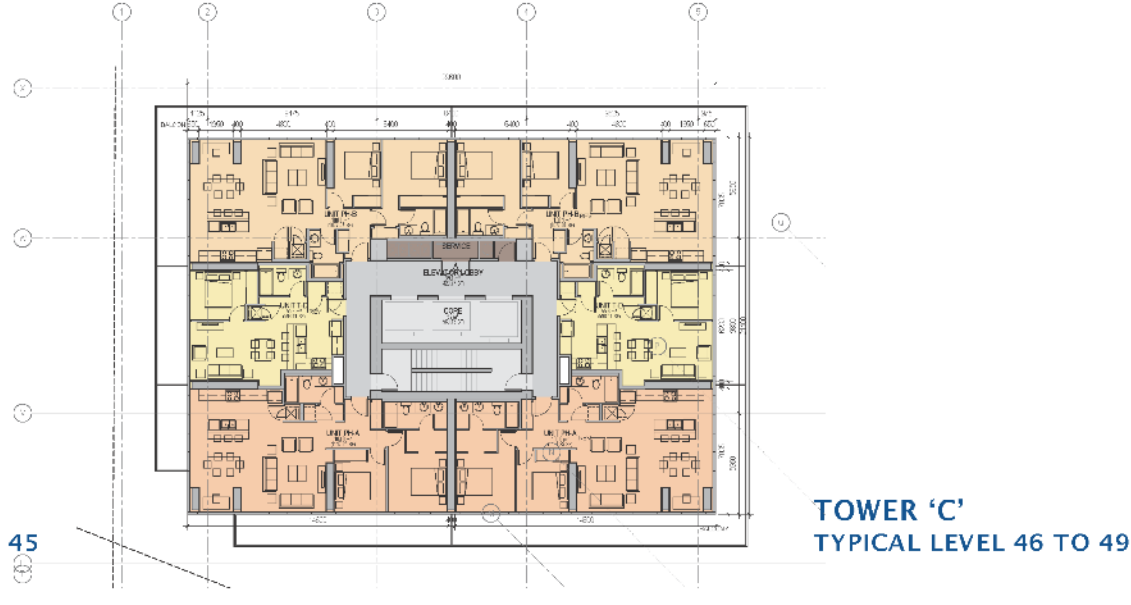
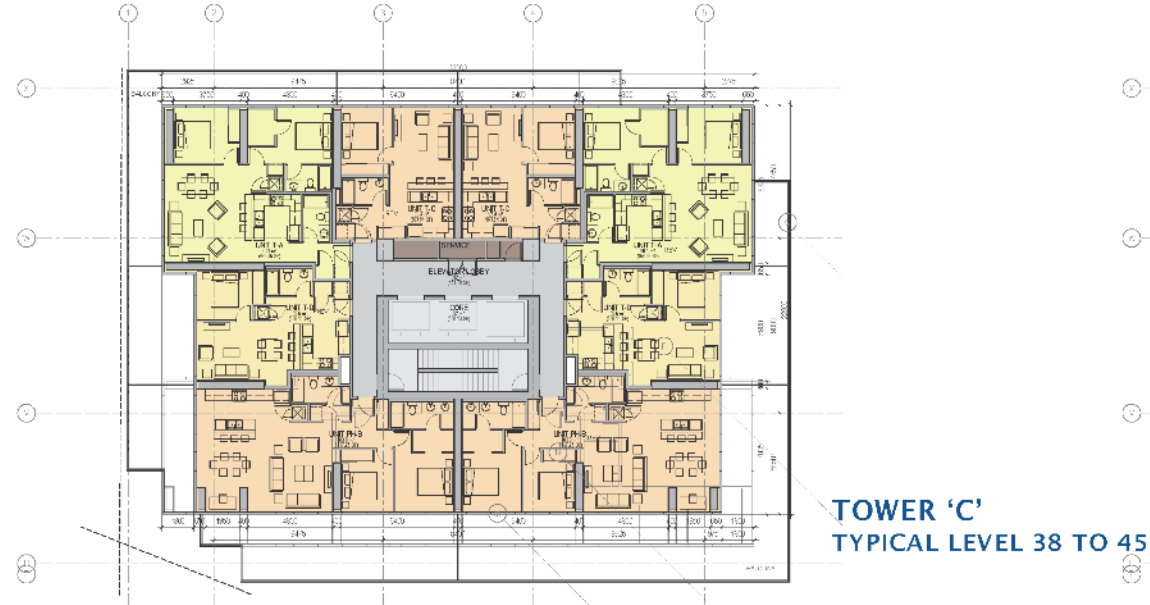
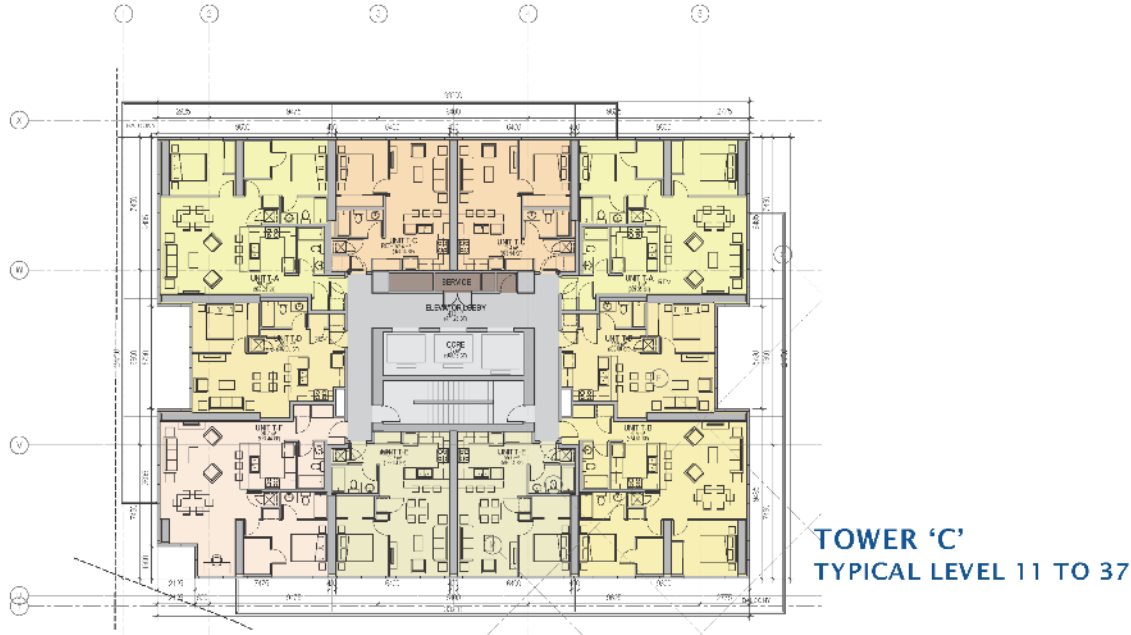
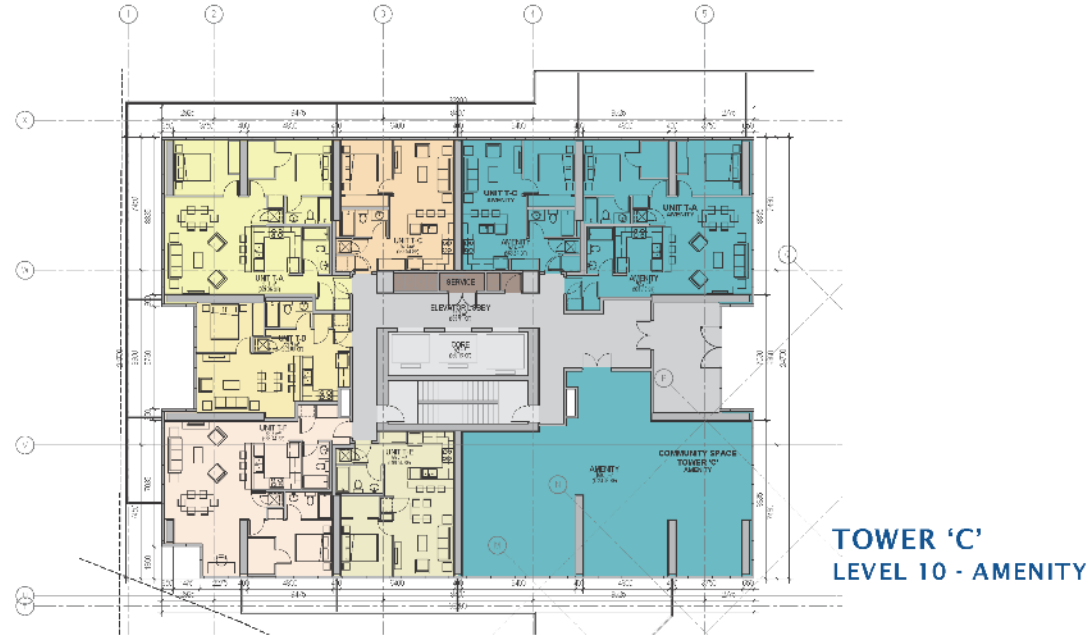
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FLOORPLANS - Tower 'C'



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UNIT PLANS:
PODIUM UNITS
TOWER UNITS
PENTHOUSE UNITS



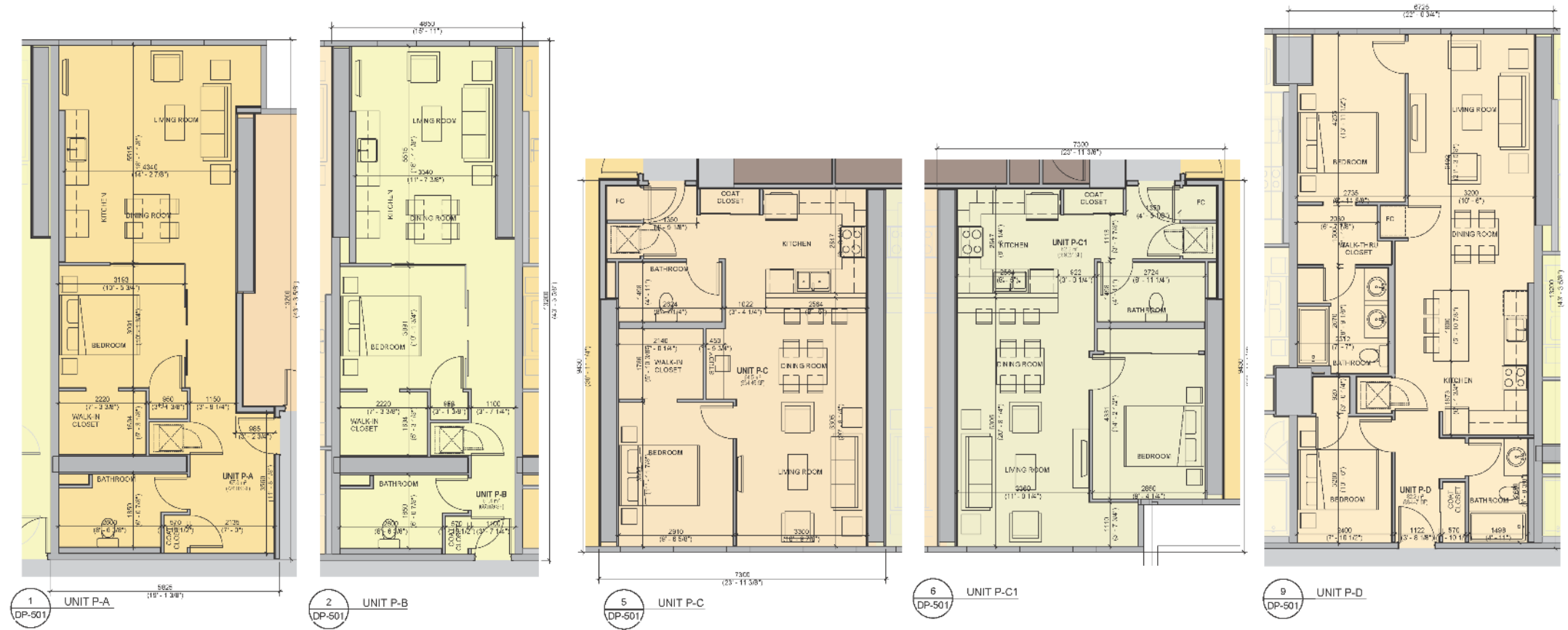
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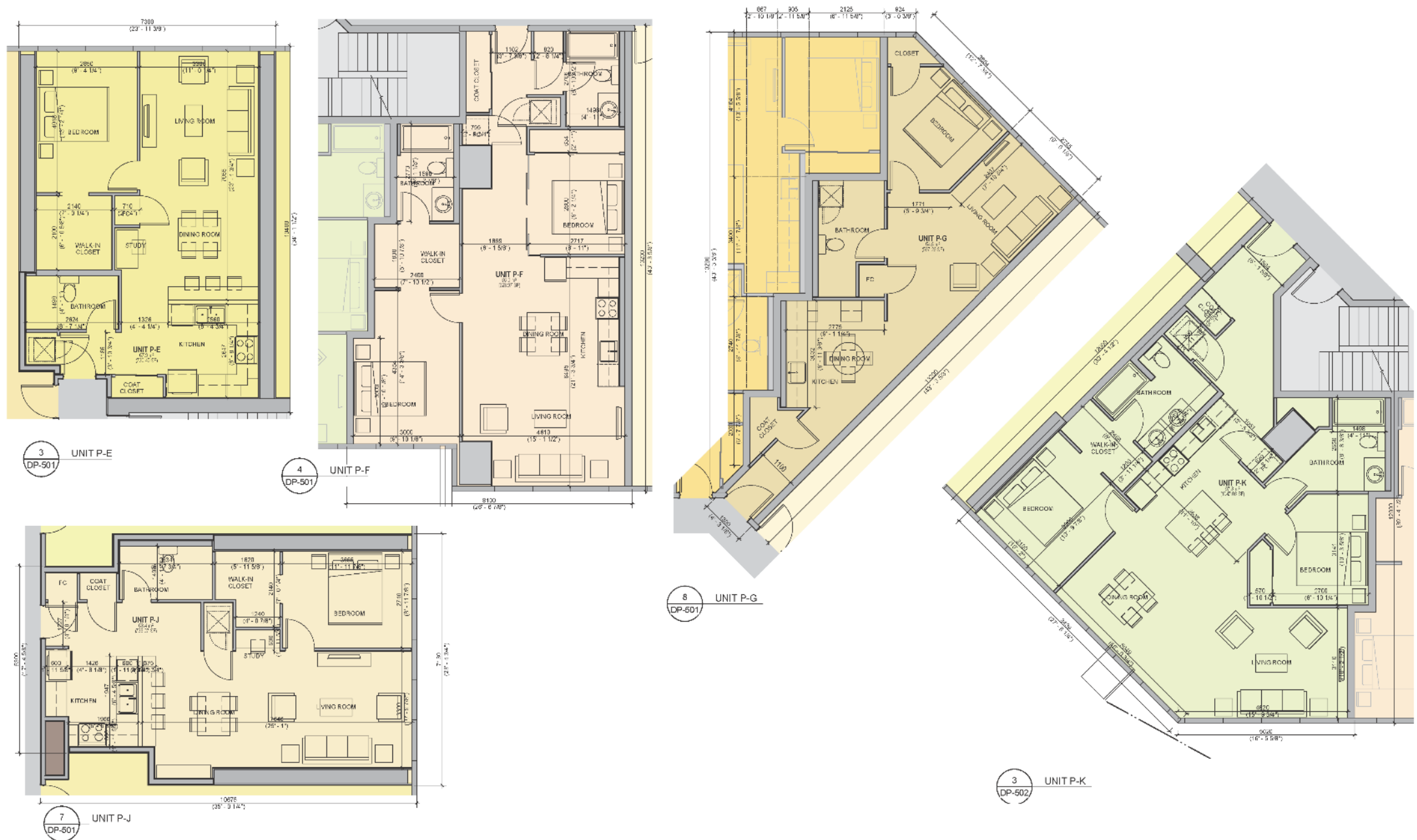
UNIT PLANS - Podium Levels 1



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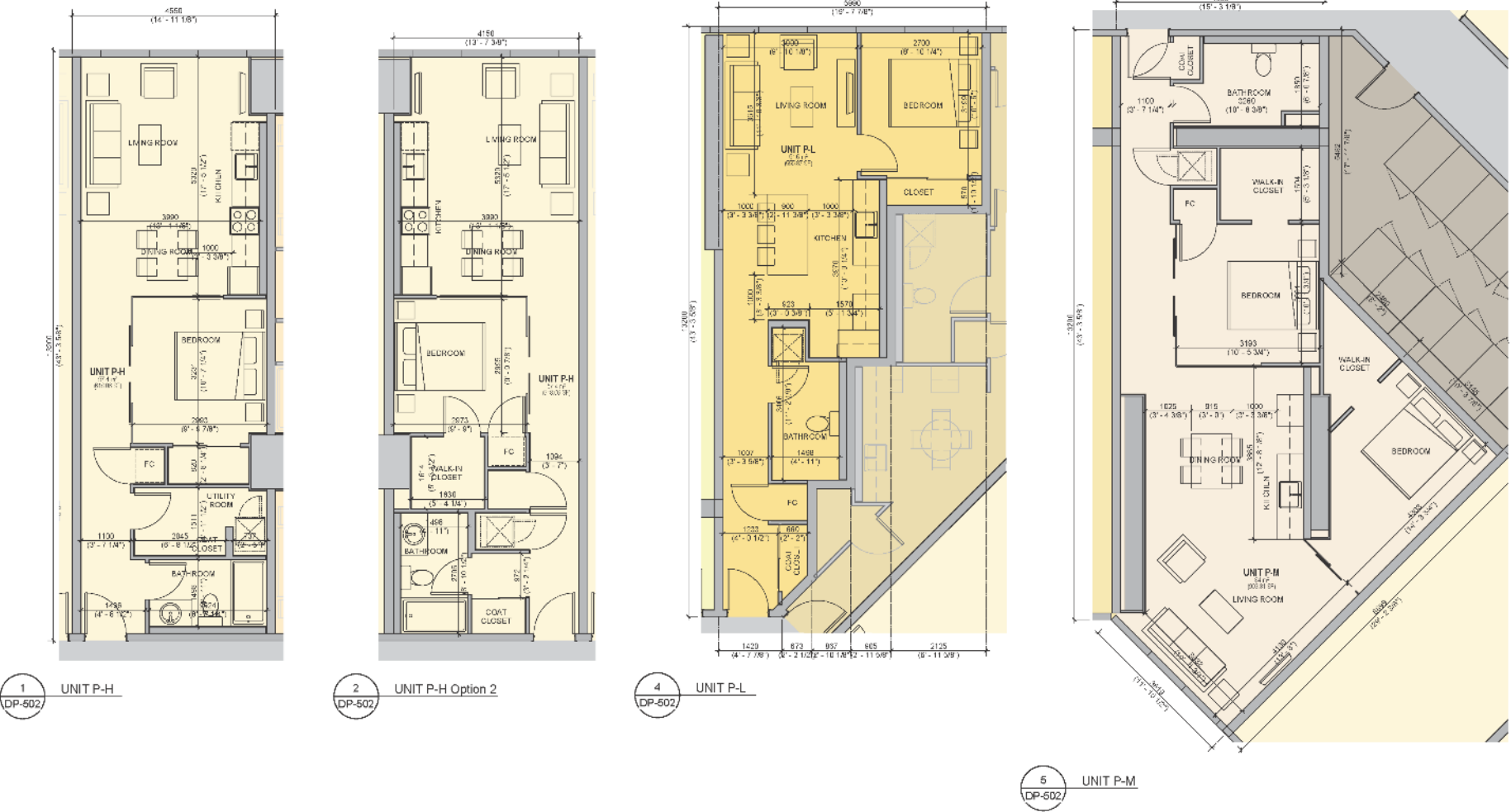
UNIT PLANS - Podium Levels 2




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UNIT PLANS - Podium Levels 3





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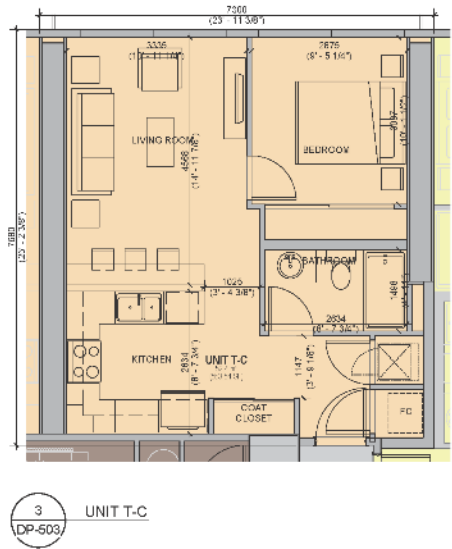
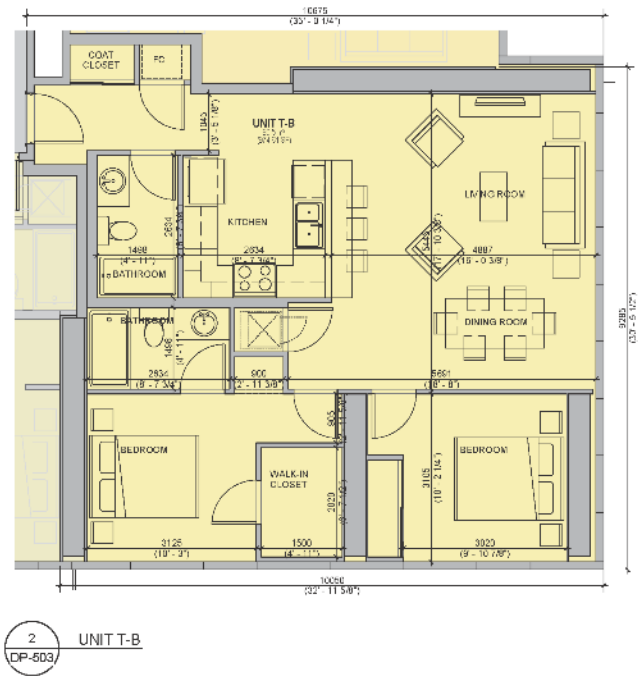
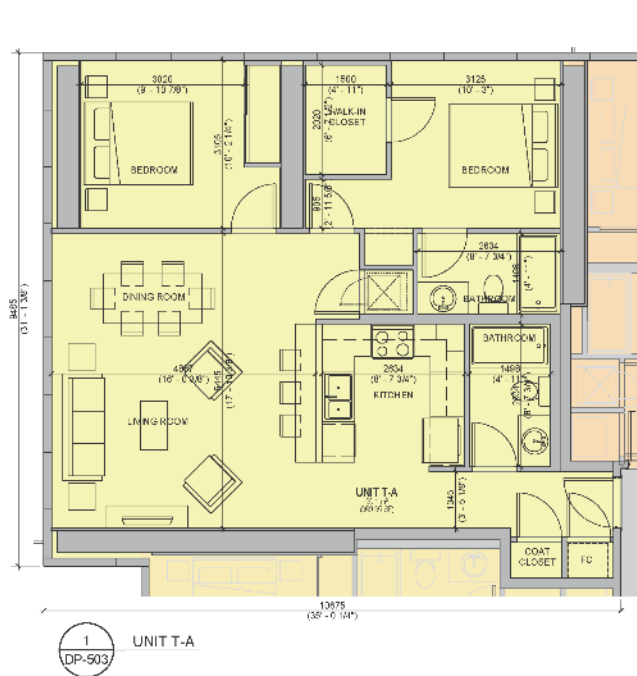


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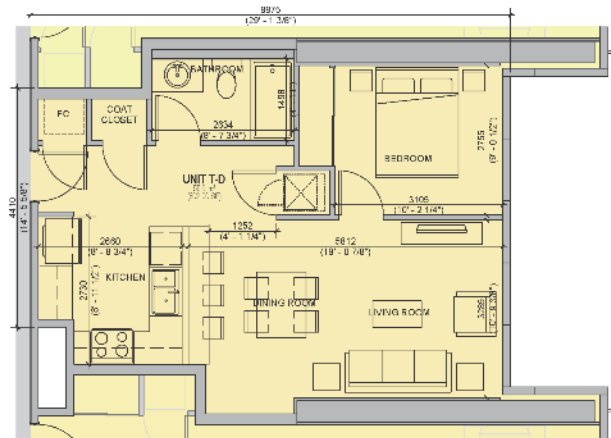
UNIT PLANS - Tower Typical 1



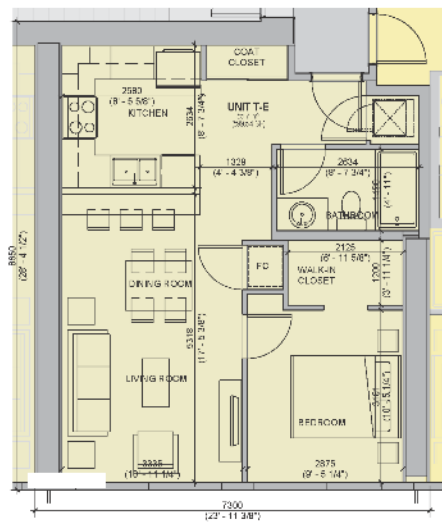
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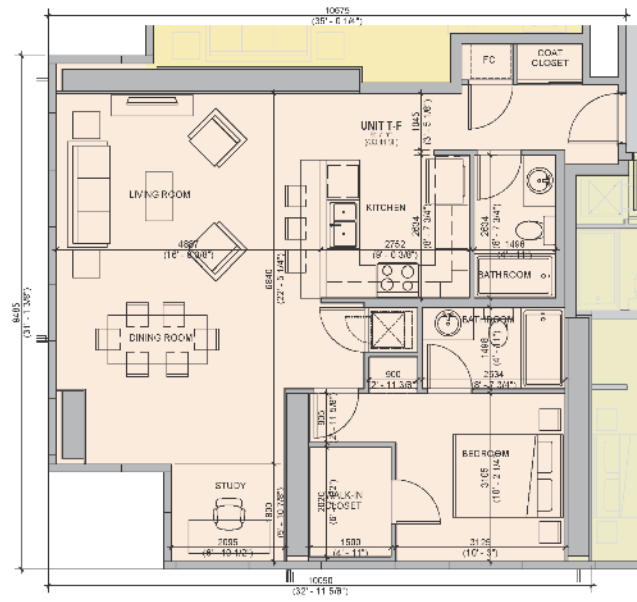
UNIT PLANS - Tower Typical 2



4 UNIT T-D
DP-503



5 UNIT T-E
DP-503



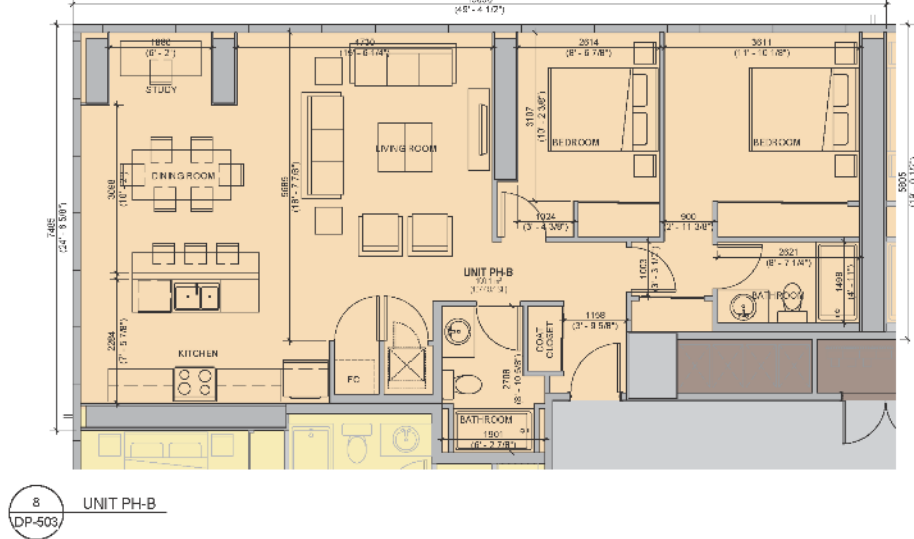
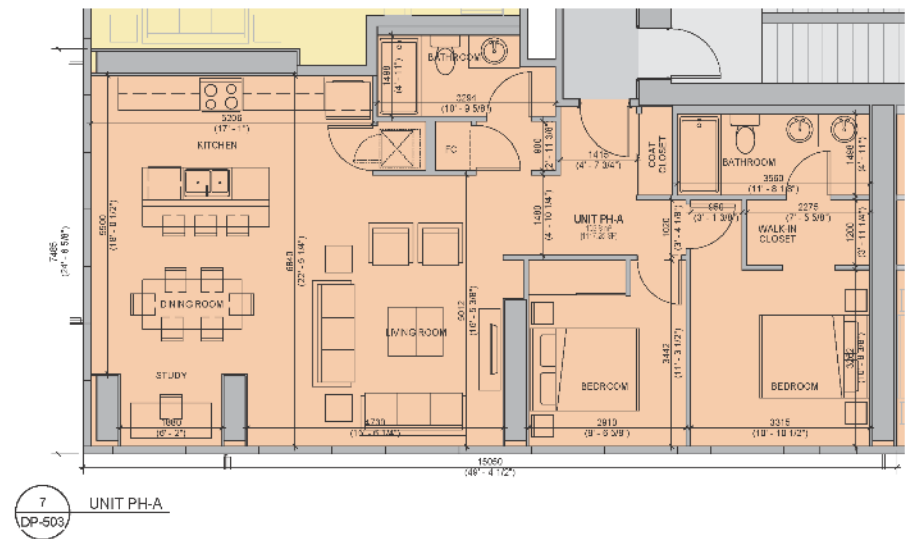
6 UNIT T-F
DP-503



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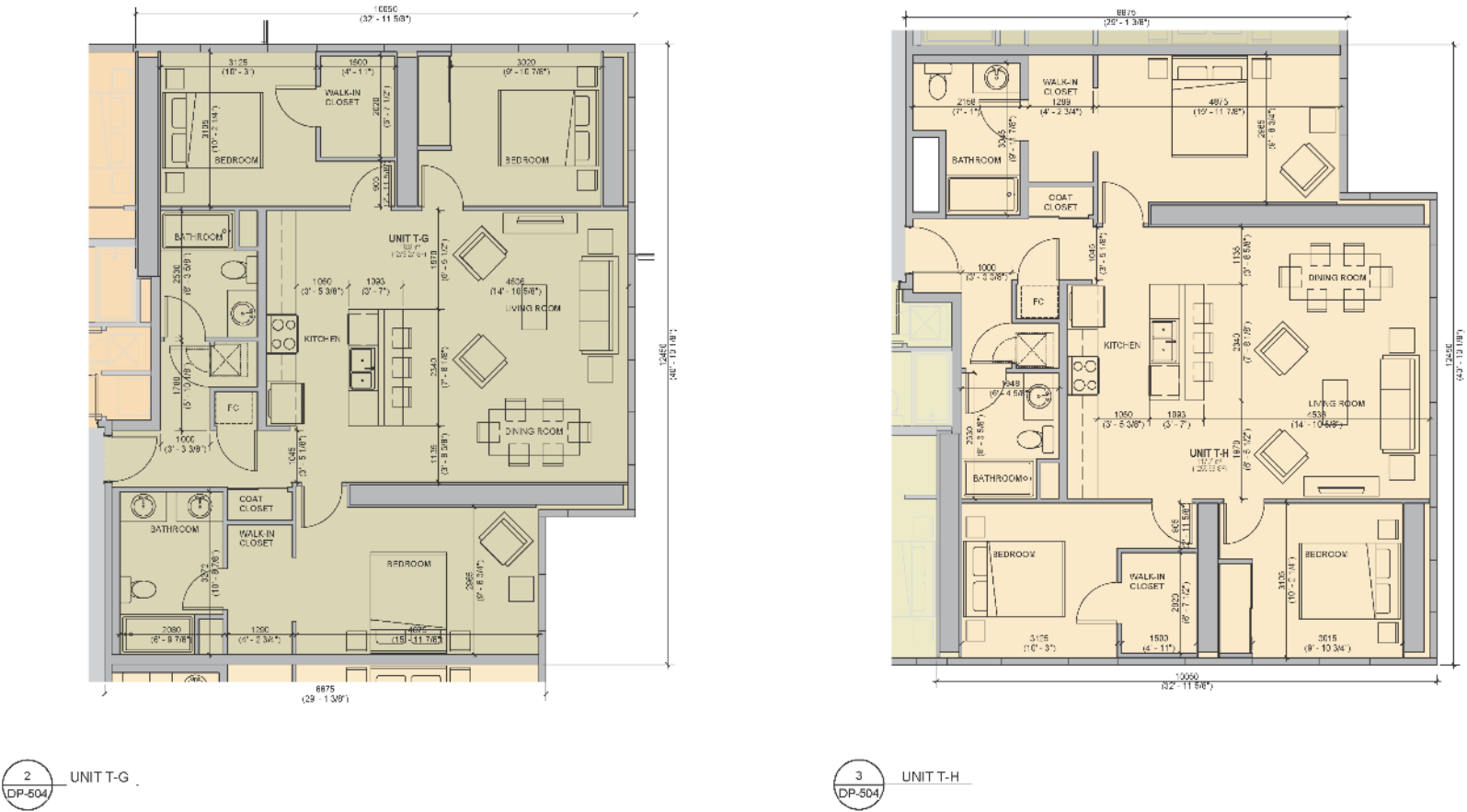
UNIT PLANS - Penthouse Levels




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UNIT PLANS - Penthouse Levels





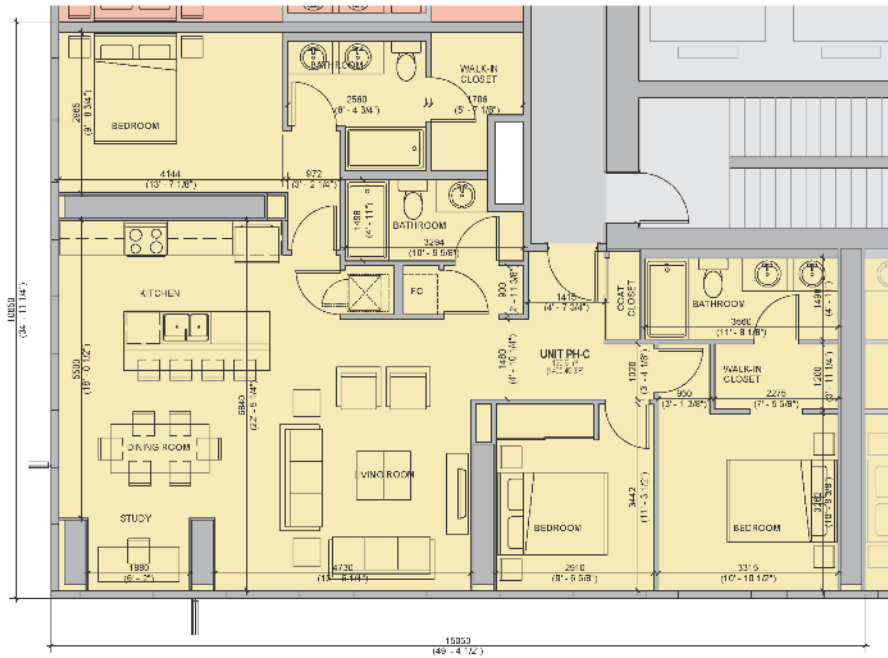
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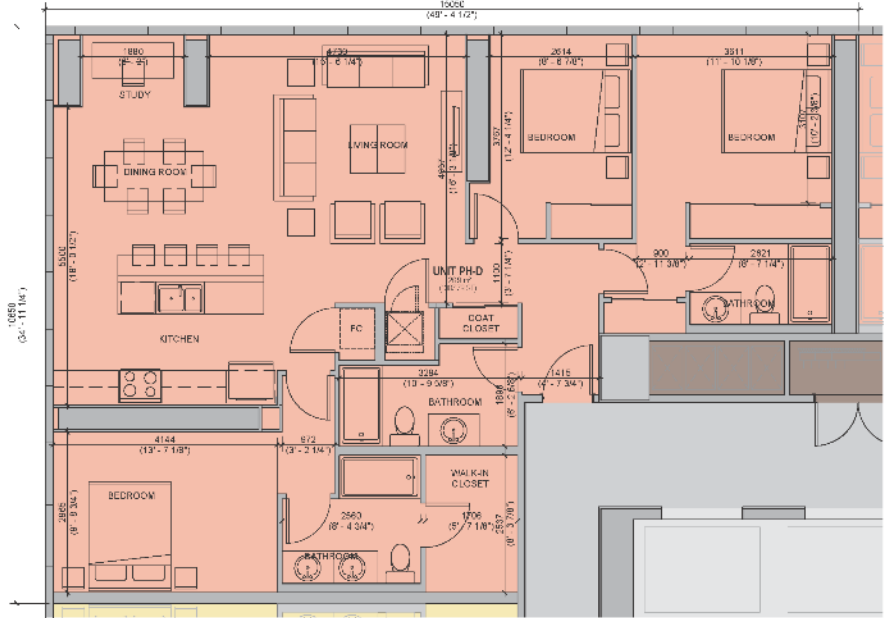
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UNIT PLANS - Penthouse Levels



2 UNIT PH-C
DP-505



3 UNIT PH-D
DP-506
SCALE: 1/8" = 1'-0"



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BUILDING ELEVATIONS
PHASING PLAN
STATISTICS



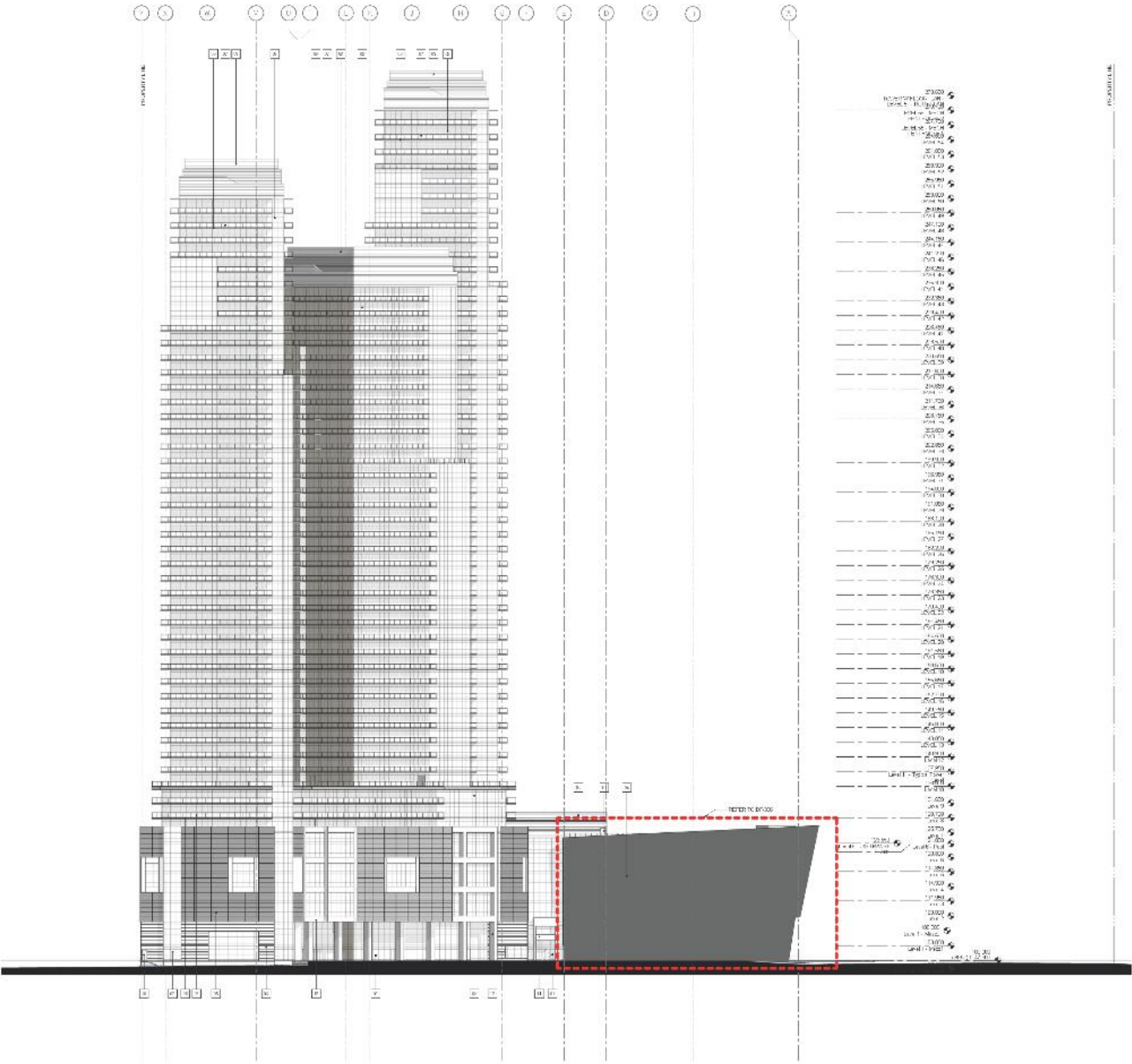
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SOUTH BUILDING ELEVATION





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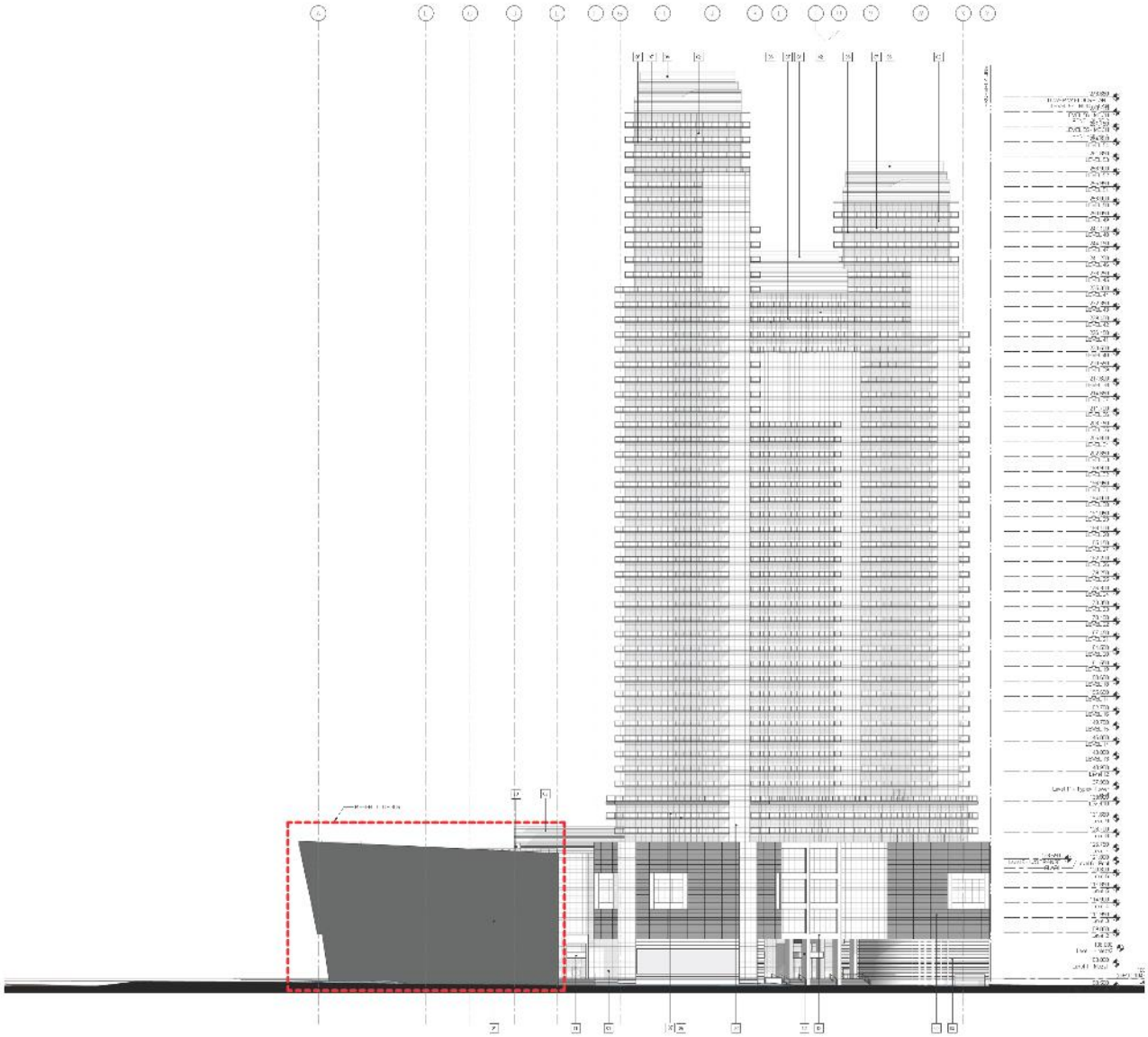


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EAST BUILDING ELEVATION



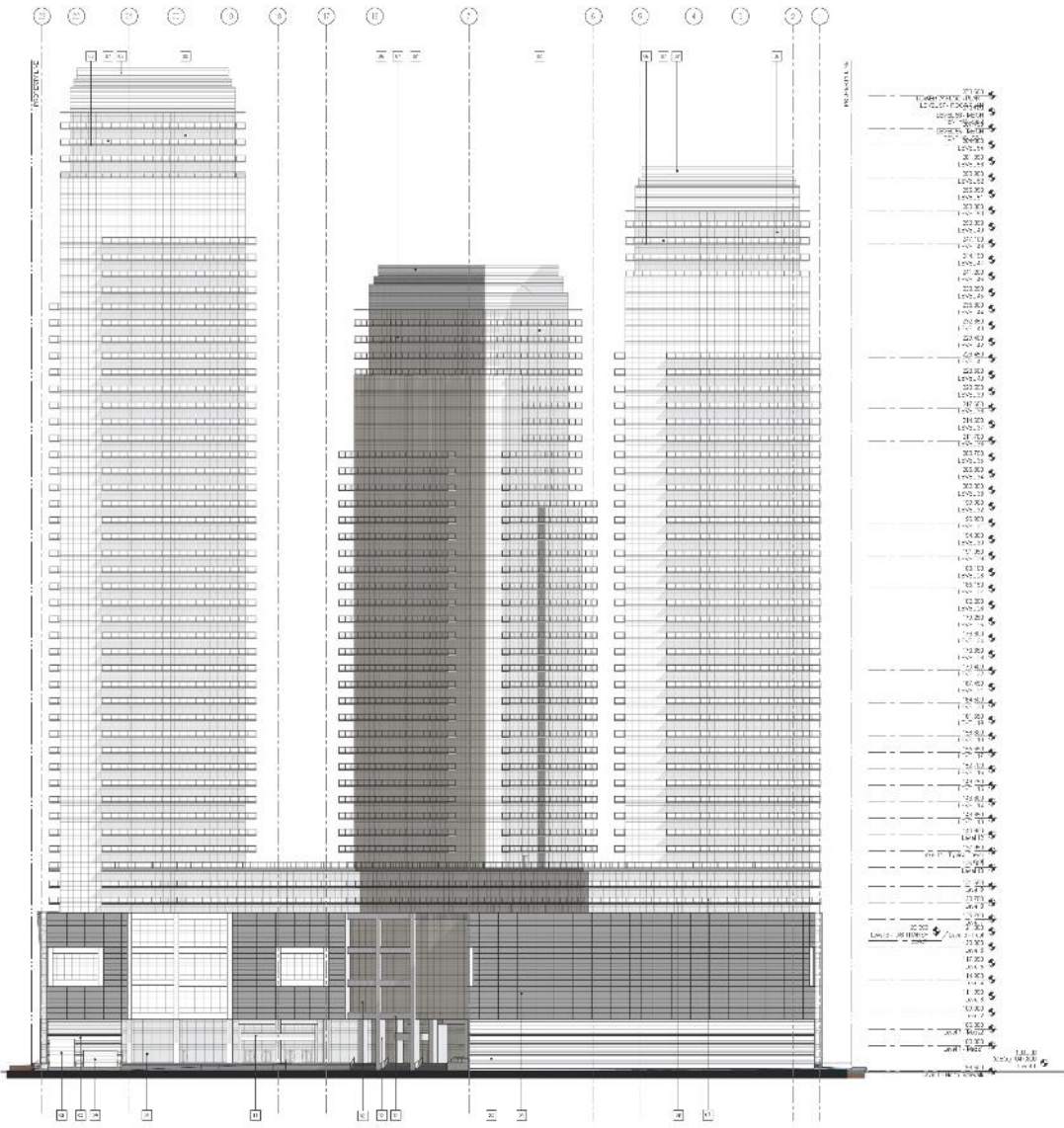
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NORTH BUILDING ELEVATION





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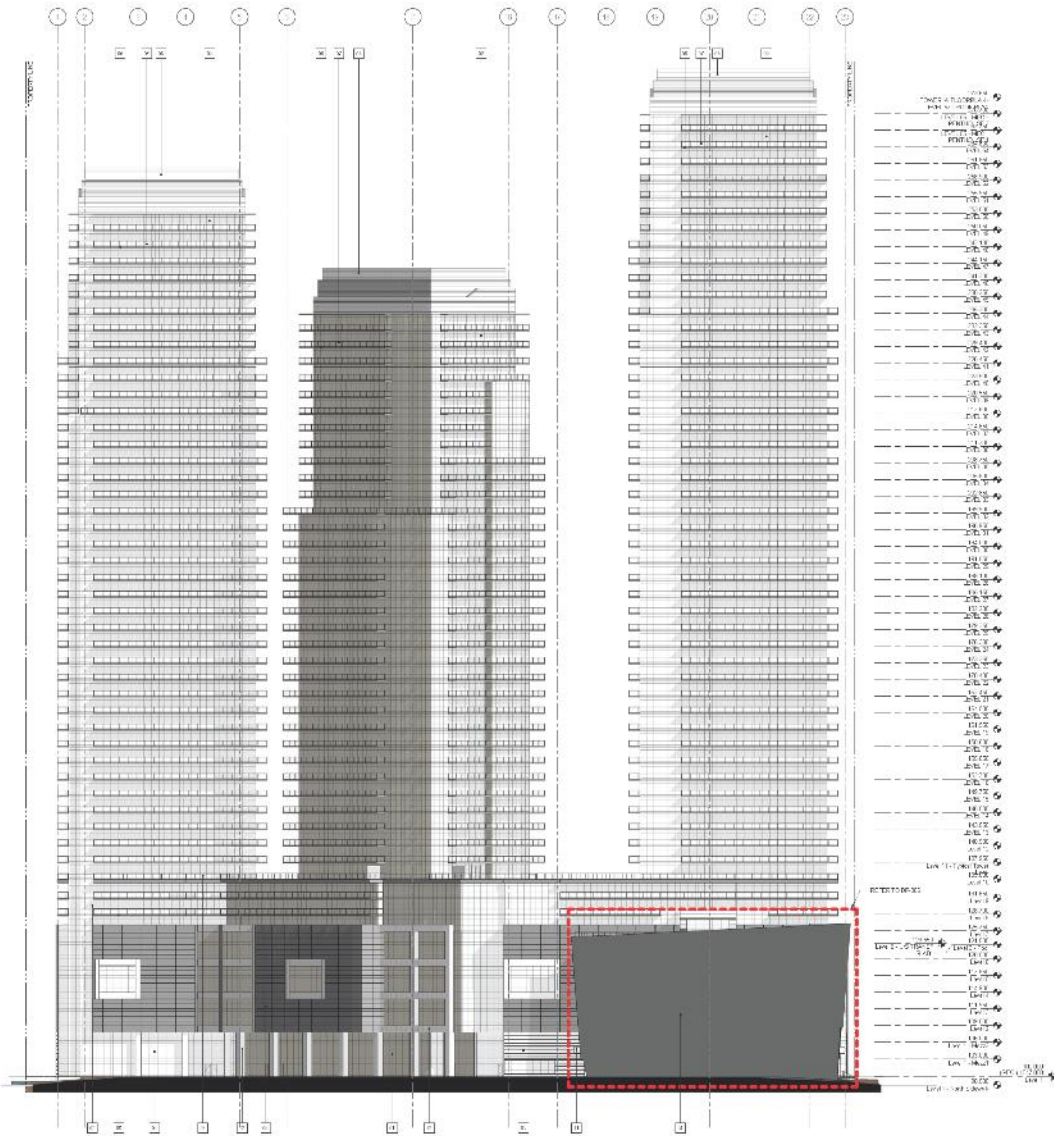


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WEST BUILDING ELEVATION





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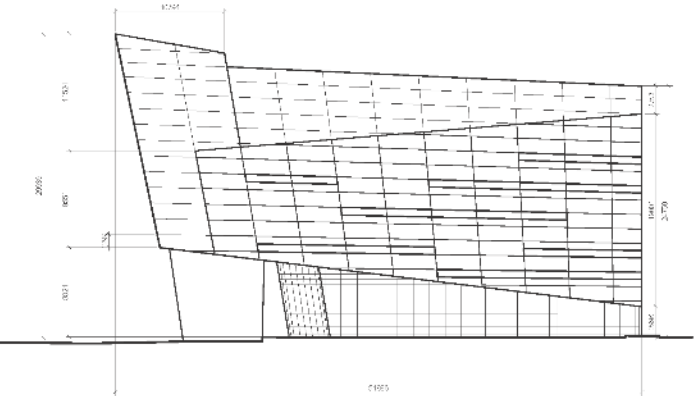
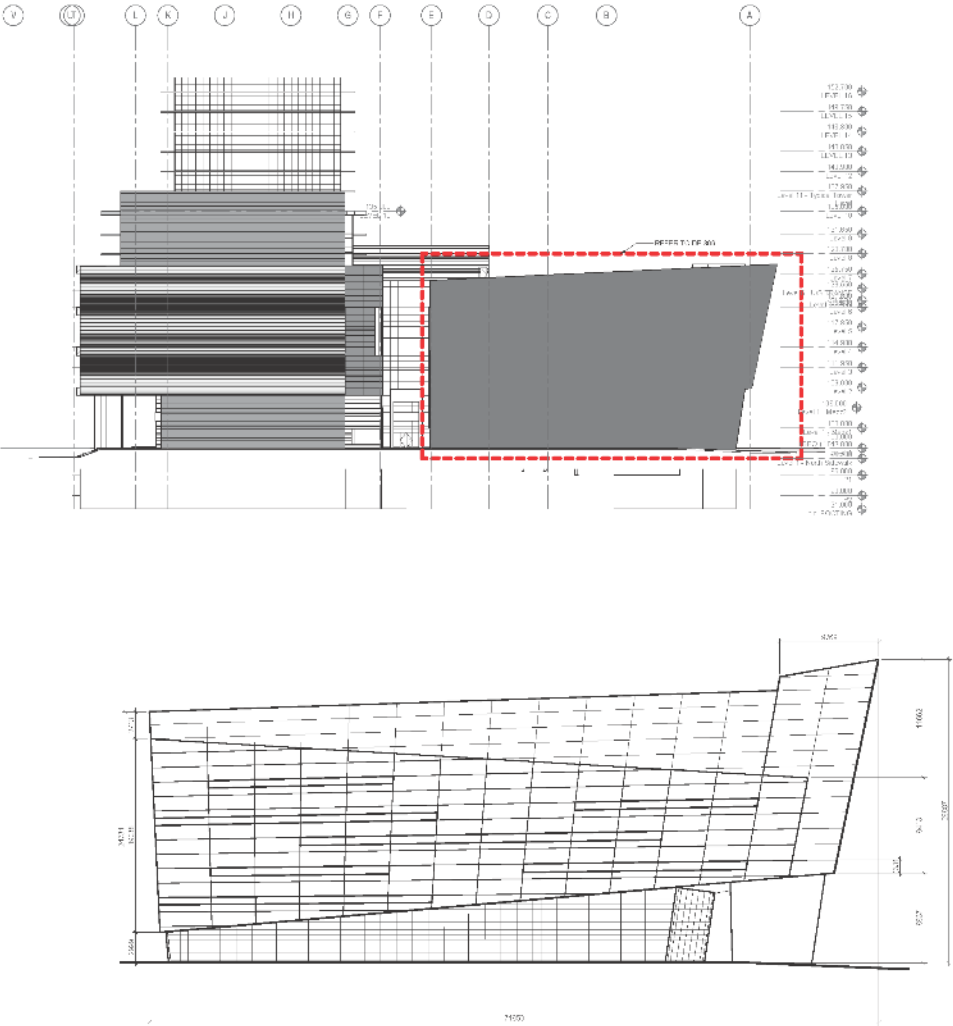


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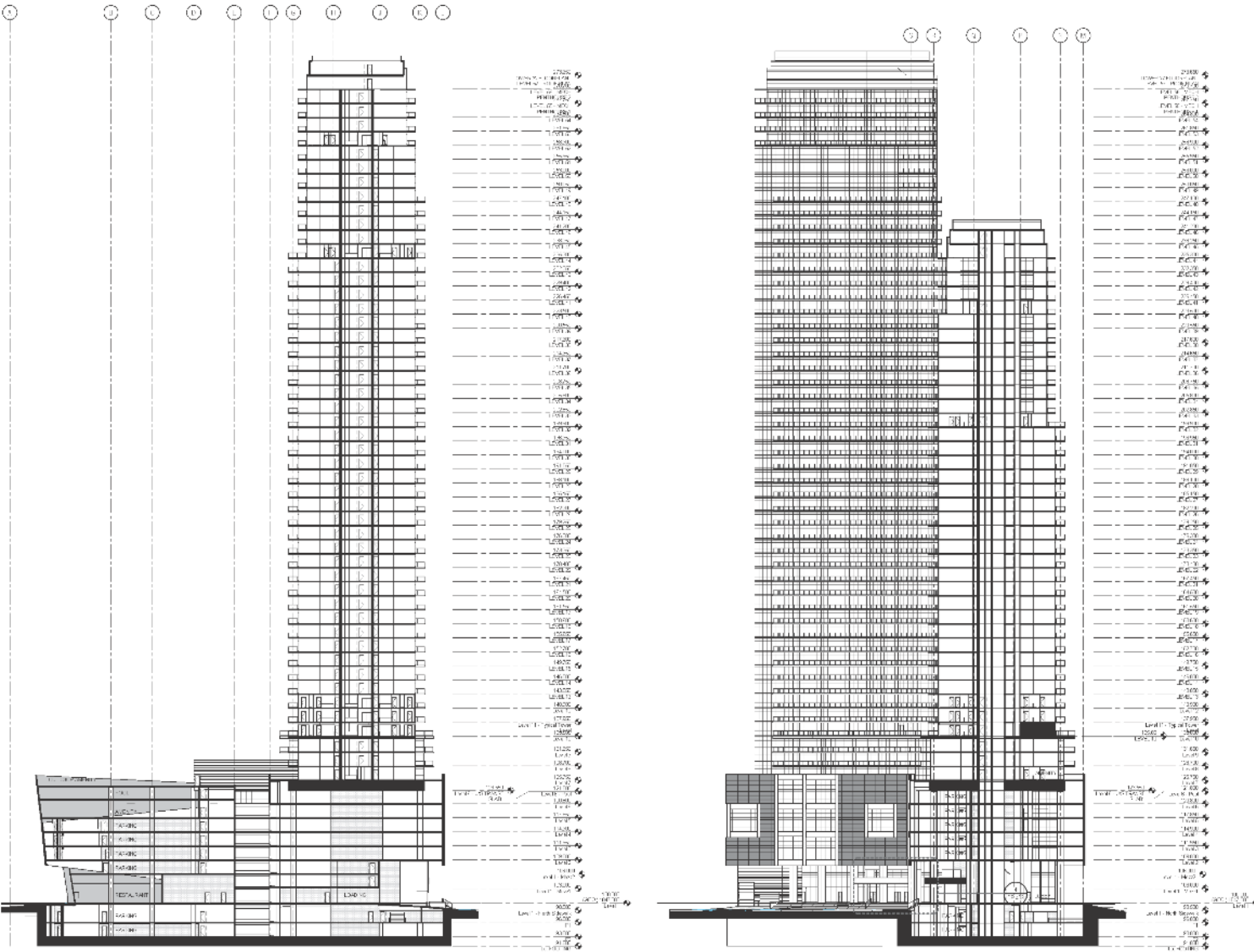
'PROW' ELEVATIONS




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TYPICAL BUILDING SECTIONS





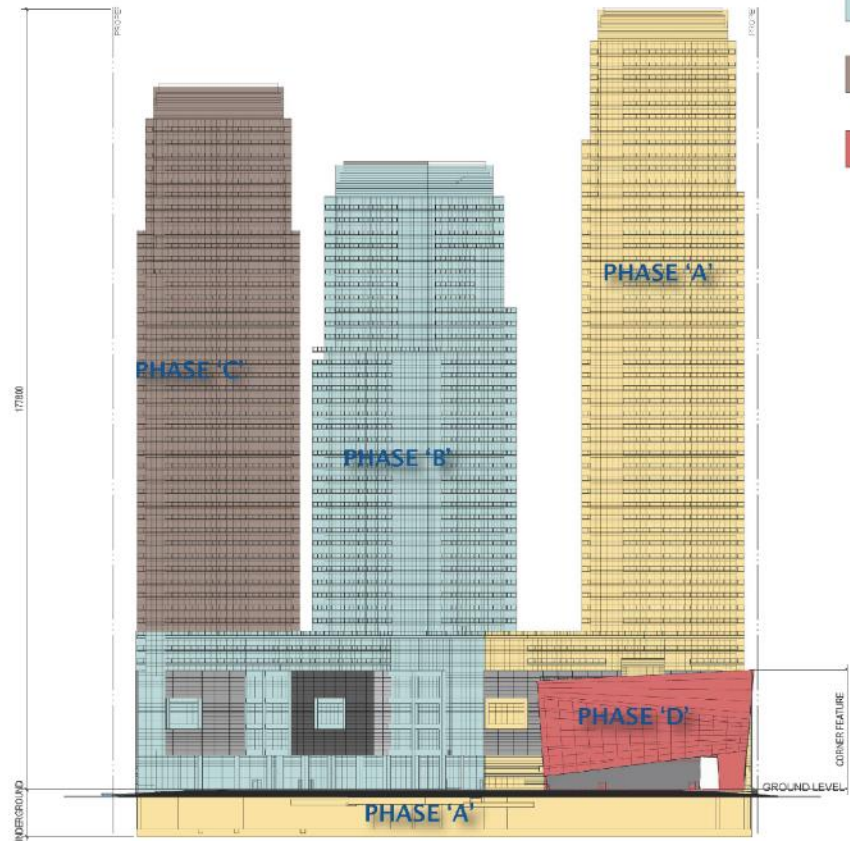
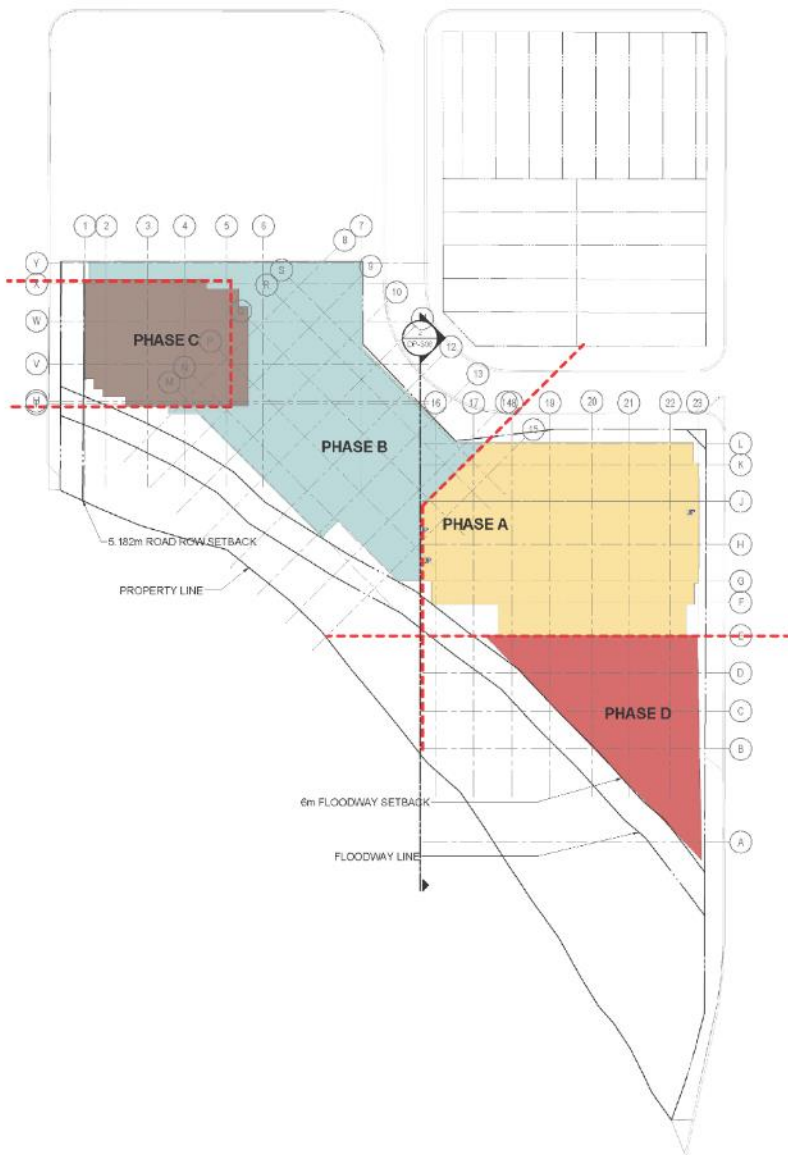
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PHASING PLAN



PHASE DEPLOYMENT

- PHASE "A"**
- 1) COMPLETED BEFORE ALL THE OTHER PHASES
 - 2) INCLUDES LOT CONSOLIDATION, SERVICING / UTILITIES
 - 3) COMPLETE UNDERGROUND LEVELS P1 AND P2
 - 4) ALL PODIUM LEVELS (1 TO 9) FOR THIS PHASE, AS MARKED
 - 5) PARK ROAD SURFACE IMPROVEMENTS AND LANDSCAPING
 - 6) RIVERBANK LANDSCAPING, AS REQUIRED / ALLOWED
 - 7) TOWER 'A', ALL LEVELS (10 TO 57) INCLUDING MECHANICAL PENTHOUSE
- | PARKING PROVIDED | | PARKING REQUIRED | |
|------------------|--------------|------------------|--------------|
| P1 + P2 | = 328 STALLS | TENANT | = 361 STALLS |
| L1 TO L6 | = 332 STALLS | VISITOR | = 49 STALLS |
| TOTAL | = 660 STALLS | TOTAL | = 410 STALLS |
- PHASE "B"**
- 1) COMPLETE REMAINDER OF PODIUM LEVELS (1 TO 9)
 - 2) TOWER 'B', ALL LEVELS (10 TO 45) INCLUDING MECHANICAL PENTHOUSE
- PHASE "C"**
- 1) TOWER 'C', ALL LEVELS (10 TO 51) INCLUDING MECHANICAL PENTHOUSE
 - 2) ROOF TOP LANDSCAPING OF ALL PODIUM
- PHASE "D"**
- 1) THIS PHASE MUST BE COMPLETED TOGETHER WITH PHASE A
 - 2) INCLUDES SOUTHEAST TRIANGULAR CORNER OF BUILDING, LEVELS 1 TO 6, AS MARKED
 - 3) THIS PHASE ROOF TOP LANDSCAPING



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STATISTICS 1: Summary

SITE AREA

		m²	sq.ft.	Ha	acre
TOTAL		13,354.57	143,850.00	1.34	3.30
NET		9,275.83	99,844.29	0.93	2.29

FAR CALCULATION

BY-LAW BASE FAR	12.00	160,374.00 m²
TARGET FAR	12.00	160,374.00 m²
TOTAL FAR PROPOSED	11.51	153,815.23 m²

PROGRAM

		GROSS	GIFA (estimate)
GROUND LEVEL		m²	sq.ft.
LEVEL 1	LOBBY / SERVICE / BOH	5,748.92	61,880.83
PODIUM			
LEVEL 2 TO 10	PARKADE / PODIUM UNITS	53,813.70	580,572.19
TOWER			
LEVEL 11 TO 55	RESIDENTIAL	94,152.62	1,013,450.37
TOTAL		153,815.23	1,655,653.39

RESIDENTIAL UNIT COUNT

	# OF LEVELS	UNITS/LEVEL	TOTAL
APT + PH	LEVEL 7 TO 55	VARIES	1,252
TOTAL UNITS			1,252

PARKING REQUIRED (BY-LAW)

	AREA / UNITS	# STALLS	TOTAL REQ.	TOT. PROV.
LEVEL 1 GROSS	0 stall per 100m² GIFA	0.00	0	0
LEVEL 2 to 10 - Resid	0.75 stall per resid. unit	160	120	120
Level 11 - 54 - Resid. units	0.75 stall per resid. unit	1,092	819	934
By-law Visitor	0.1 stall per resid. unit	125.2	125	126
RESTAURANT	0 stalls if mixed use	0.00	0	0
TOTAL PARKING REQUIRED			1,065	1,180

PARKING DEFICIENCY

	REQUIRED	PROVIDED
DEFICIENCY	1,065	1,180

PARKADE

	STORAGE LOCKERS	BIKE RACKS	PARKING EFFICIENCY	PARKING RATIO
			PARKING FLOOR PLATE AREA	(1 STALL PER)
			m²	sq.ft.
P2	100	0	7,251.51	78,054.00
P1	0	280	7,251.51	78,054.00
L1	0	297	1,561.00	16,878.06
L1 MEZZ 1	40	52	2,437.78	26,230.30
L1 MEZZ 2	282	0	4,159.78	44,775.45
L2	100	0	7,000.63	75,354.10
L3	101	0	7,024.23	75,605.13
L4	101	0	7,046.44	75,847.26
L5	25	0	1,075.05	11,574.53
L6	36	0	7,087.26	76,394.22
L7	49	0		
L8	29	0		
L9	29	0		
TOTAL	895	647		

BICYCLE PARKING

				CLASS	BIKE PARKING
					REQUIRED
					PROPOSED
CRU	NOT REQUIRED	0.00	0.00	CLASS 1	0
	5% OF PARKING	0.00	0.00	CLASS 2	0
RESTAURANT	NOT REQUIRED	0.00	0.00	CLASS 1	0
	5% OF PARKING	0.00	0.00	CLASS 2	0
RESIDENTIAL	0.5 per unit	0.50	1,252.00	CLASS 1	626
	0.1 per unit	0.10	125.20	CLASS 2	126
TOTAL				CLASS 1	626
				CLASS 2	126

FLOOR PLATE AREAS

	GROSS	GIFA OR NFA (%)	EFFICIENCY	UNIT COUNT	USE	TOTALS
	m²	sq.ft.	%			GFA m²
LEVEL 1	5,748.92	61,880.83	1,437.23	15,470.21	25.00%	0
MEZZ 1	2,437.78	26,230.30	0.00	0.00	0.00%	0
MEZZ 2	4,159.78	44,775.45	0.00	0.00	0.00%	0
LEVEL 2	7,000.63	75,354.10	0.00	0.00	0.00%	0
LEVEL 3	7,024.23	75,605.13	0.00	0.00	0.00%	0
LEVEL 4	7,046.44	75,847.26	0.00	0.00	0.00%	0
LEVEL 5	7,076.85	76,174.53	1,196.27	12,876.55	16.90%	0
LEVEL 6	7,097.26	76,394.22	1,220.19	13,133.98	17.19%	0
LEVEL 7	4,157.47	44,750.52	3,681.01	39,622.01	88.54%	52
LEVEL 8	3,956.65	42,588.98	3,480.18	37,460.37	87.96%	54
LEVEL 9	3,956.65	42,588.98	3,480.18	37,460.37	87.96%	54
LEVEL 10	2,309.93	24,863.88	1,994.10	21,464.35	86.33%	18
LEVEL 11	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 12	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 13	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 14	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 15	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 16	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 17	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 18	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 19	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 20	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 21	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 22	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 23	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 24	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 25	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 26	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 27	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 28	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 29	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 30	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 31	2,394.41	25,773.21	2,078.58	22,373.59	86.81%	30
LEVEL 32	2,306.99	24,832.23	1,991.16	21,432.71	86.31%	28
LEVEL 33	2,306.99	24,832.23	1,991.16	21,432.71	86.31%	28
LEVEL 34	2,306.99	24,832.23	1,991.16	21,432.71	86.31%	28
LEVEL 35	2,306.99	24,832.23	1,991.16	21,432.71	86.31%	28
LEVEL 36	2,306.99	24,832.23	1,991.16	21,432.71	86.31%	28
LEVEL 37	2,306.99	24,832.23	1,991.16	21,432.71	86.31%	28
LEVEL 38	2,223.39	23,912.37	1,907.56	20,532.84	85.80%	26
LEVEL 39	2,223.39	23,912.37	1,907.56	20,532.84	85.80%	26
LEVEL 40	2,135.96	22,991.28	1,820.13	19,591.75	85.21%	24
LEVEL 41	2,135.96	22,991.28	1,820.13	19,591.75	85.21%	24
LEVEL 42	2,135.96	22,991.28	1,820.13	19,591.75	85.21%	24
LEVEL 43	2,135.96	22,991.28	1,820.13	19,591.75	85.21%	24
LEVEL 44	2,048.54	22,050.90	1,592.17	17,137.96	77.72%	16
LEVEL 45	1,669.80	17,973.58	1,459.25	15,707.23	87.39%	16
LEVEL 46	1,336.55	14,388.50	1,126.00	12,120.15	84.25%	14
LEVEL 47	1,336.55	14,388.50	1,126.00	12,120.15	84.25%	14
LEVEL 48	1,336.55	14,388.50	1,126.00	12,120.15	84.25%	14
LEVEL 49	1,336.55	14,388.50	1,126.00	12,120.15	84.25%	14
LEVEL 50	1,336.55	14,388.50	985.45	10,607.54	73.73%	8
LEVEL 51	957.81	10,309.78	852.53	9,176.60	89.01%	8
LEVEL 52	624.56	6,722.71	519.28	5,589.53	83.14%	6
LEVEL 53	624.56	6,722.71	519.28	5,589.53	83.14%	6
LEVEL 54	624.56	6,722.71	519.28	5,589.53	83.14%	6
LEVEL 55	624.56	6,722.71	519.28	5,589.53	83.14%	6
LEVEL 56	624.56	6,722.71	0.00	0.00	0.00%	0
LEVEL 57	245.82	2,645.88	0.00	0.00	0.00%	0
TOTAL	153,815.23	1,655,653.39	94,652.59	1,018,832.05	61.54%	1,252

(*) NET EXCLUDES CORE AND ALL MECHANICAL
RENTABLE AS PER ROMA 96 FOR OFFICE

resid. units



HAT @ Elbow River CPC Presentation



STATISTICS 2: GFA Detail per Level / per Tower

		PODIUM			TOWER 'A'			TOWER 'B'			TOWER 'C'			TOTAL GFA		UNIT COUNT
		m²	sq.ft.	unit count	m²	sq.ft.	unit count	m²	sq.ft.	unit count	m²	sq.ft.	unit count	m²	sq.ft.	
VEL 1	PD1	5,748.92	61,880.83	0										5,748.92	61,880.83	0
	MEZZ1	2,437.78	26,239.99	0										2,437.78	26,239.99	0
	MEZZ2	4,159.78	44,775.45	0										4,159.78	44,775.45	0
VEL 2	PD2	7,000.63	75,354.10	0										7,000.63	75,354.10	0
	PD3	7,024.23	75,608.13	0										7,024.23	75,608.13	0
	PD4	7,046.44	75,847.20	0										7,046.44	75,847.20	0
VEL 3	PD5	7,076.85	76,174.59	0										7,076.85	76,174.59	0
VEL 4	PD6	7,097.26	76,394.22	0										7,097.26	76,394.22	0
VEL 5	PD7	4,157.47	44,750.62	52										4,157.47	44,750.62	52
VEL 6	PD8	3,956.65	42,588.98	54										3,956.65	42,588.98	54
VEL 7	PD9	3,956.65	42,588.98	54										3,956.65	42,588.98	54
VEL 10	T1				799.41	8,604.73	7	714.93	7,695.44	4	795.59	8,563.66	7	2,309.93	24,863.88	18
VEL 11	T2				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 12	T3				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 13	T4				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 14	T5				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 15	T6				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 16	T7				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 17	T8				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 18	T9				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 19	T10				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 20	T11				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 21	T12				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 22	T13				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 23	T14				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 24	T15				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 25	T16				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 26	T17				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 27	T18				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 28	T19				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 29	T20				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 30	T21				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 31	T22				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 32	T23				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 33	T24				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 34	T25				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 35	T26				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 36	T27				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 37	T28				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 38	T29				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 39	T30				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 40	T31				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 41	T32				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 42	T33				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 43	T34				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 44	T35				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 45	T36				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 46	T37				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 47	T38				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 48	T39				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 49	T40				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 50	T41				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 51	T42				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 52	T43				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 53	T44				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 54	T45				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 55	T46				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 56	T47				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
VEL 57	T48				799.41	8,604.73	10	799.41	8,604.73	10	795.59	8,563.66	10	2,394.41	25,773.21	30
OTALS		59,662.61	642,203.02	160	36,244.48	390,132.34	425	26,567.08	285,965.67	302	31,341.06	337,352.36	365	153,815.23	1,655,653.39	1,252.00



HAT @ Elbow River CPC Presentation



STATISTICS 3: Unit Count / Unit Mix

		PODIUM				TOWER 'A'			TOWER 'B'			TOWER 'C'			UNIT COUNT	
		1 BDRM	2 BDRM	1.5BDRM	STUDIO	TOTAL	1 BDRM	2 BDRM	3BDRM	TOTAL	1 BDRM	2 BDRM	3BDRM	TOTAL	TOTAL	TOTAL
LEVEL 1	P1					0				0				0		
MEZZ1						0				0				0		
MEZZ2						0				0				0		
LEVEL 2	P2	0	0	0	0	0				0				0		
LEVEL 3	P3	0	0	0	0	0				0				0	0	
LEVEL 4	P4	0	0	0	0	0				0				0	0	
LEVEL 5	P5	0	0	0	0	0				0				0	0	
LEVEL 6	P6	0	0	0	0	0				0				0	0	
LEVEL 7	P7	44	8	0	0	52				0				0	52	
LEVEL 8	P8	46	8	0	0	54				0				0	54	
LEVEL 9	P9	46	8	0	0	54				0				0	54	
LEVEL 10	T1					0	4	3		7	2	2		4	7	18
LEVEL 11	T2					0	6	4		10	6	4		10	7	30
LEVEL 12	T3					0	6	4		10	6	4		10	7	30
LEVEL 13	T4					0	6	4		10	6	4		10	7	30
LEVEL 14	T5					0	6	4		10	6	4		10	7	30
LEVEL 15	T6					0	6	4		10	6	4		10	7	30
LEVEL 16	T7					0	6	4		10	6	4		10	7	30
LEVEL 17	T8					0	6	4		10	6	4		10	7	30
LEVEL 18	T9					0	6	4		10	6	4		10	7	30
LEVEL 19	T10					0	6	4		10	6	4		10	7	30
LEVEL 20	T12					0	6	4		10	6	4		10	7	30
LEVEL 21	T11					0	6	4		10	6	4		10	7	30
LEVEL 22	T12					0	6	4		10	6	4		10	7	30
LEVEL 23	T13					0	6	4		10	6	4		10	7	30
LEVEL 24	T14					0	6	4		10	6	4		10	7	30
LEVEL 25	T15					0	6	4		10	6	4		10	7	30
LEVEL 26	T16					0	6	4		10	6	4		10	7	30
LEVEL 27	T17					0	6	4		10	6	4		10	7	30
LEVEL 28	T18					0	6	4		10	6	4		10	7	30
LEVEL 29	T19					0	6	4		10	6	4		10	7	30
LEVEL 30	T20					0	6	4		10	6	4		10	7	30
LEVEL 31	T21					0	6	4		10	6	4		10	7	30
LEVEL 32	T22					0	6	4		10	4	4		8	7	28
LEVEL 33	T23					0	6	4		10	4	4		8	7	28
LEVEL 34	T24					0	6	4		10	4	4		8	7	28
LEVEL 35	T25					0	6	4		10	4	4		8	7	28
LEVEL 36	T26					0	6	4		10	4	4		8	7	28
LEVEL 37	T27					0	6	4		10	4	4		8	7	28
LEVEL 38	T28					0	6	4		10	4	4		8	4	26
LEVEL 39	T29					0	6	4		10	4	4		8	4	26
LEVEL 40	T30					0	6	4		10	2	4		6	4	24
LEVEL 41	T31					0	6	4		10	2	4		6	4	24
LEVEL 42	T32					0	6	4		10	2	4		6	4	24
LEVEL 43	T33					0	6	4		10	2	4		6	4	24
LEVEL 44	T34					0	4	4		8	0	0		4	4	16
LEVEL 45	T35					0	4	4		8				4	4	16
LEVEL 46	T36					0	4	4		8				2	4	14
LEVEL 47	T37					0	4	4		8				2	4	14
LEVEL 48	T38					0	4	4		8				2	4	14
LEVEL 49	T39					0	4	4		8				2	4	14
LEVEL 50	T40					0	4	4		8		0		0	0	8
LEVEL 51	T41					0	4	4		8						8
LEVEL 52	T42					0	2	4		6						6
LEVEL 53	T43					0	2	4		6						6
LEVEL 54	T44					0	2	4		6						6
LEVEL 55	T45					0	2	4		6						6
LEVEL 56	T46					0	0	0		0						0
TOTALS		136	24	0	0	160	242	183	0	425	168	134	0	302	234	1,252

TOTALS PER # OF BEDROOMS

1 BDRM	2 BDRM	1.5BDRM	STUDIO	TOTAL
780	472	0	0	1252
62.30%	37.70%	0.00%	0.00%	100.00%



HAT @ Elbow River CPC Presentation



HAT @ Elbow River DATE 2018 | 09 | 20 PROJECT # NCCA140240 PROJECT MANAGER B.M. DRAWN F.A./B.A. SCALE N.T.S PAGE 90



Letter of Understanding – Affordable Housing



August 17, 2018

Sarah Itani
Business Development Manager
Cidex Group of Companies
1301 9 Avenue SW
Calgary, AB T3C 0H9

Dear Sarah:

RE: Proposed Terms and Conditions for Density Bonusing – Affordable Housing

Property: The Hat at Elbow River – 1919 MacLeod Trail SE (DP2017-4075)

Further to our conversations regarding your request for bonus density at the above cited property, this letter is to confirm our mutual understanding of the proposed terms and conditions set out in Attachment "A" (the "Proposal").

This letter does not constitute an agreement to grant the additional density by the City, but is only an expression of the understanding of the parties. Please note that the Proposal is subject to modification throughout the City's corporate approval process (the "Corporate Approval"). The City shall notify Cidex in writing of any modifications made to the Proposal.

The Proposal, as may be modified by Corporate Approval, shall form the basis for the legal agreement in the form of a Housing Agreement. There shall be no binding obligations on any party until all of the legal agreement(s) are fully executed in the City's Standard Form.

Any costs or expenses incurred by Cidex in association with the Proposal shall be entirely at Cidex's own risk and expense, unless the executed legal agreement(s) provide otherwise.

If Cidex would like Calgary Housing to proceed as set out below, please return this letter acknowledged by the appropriate signature on or before August 30, 2018.

Upon our receipt of this acknowledged letter and submission of the requested due diligence materials, Calgary Housing will, after review of the materials, determine if a recommendation can be made to 1) approve Cidex as a non-market housing provider and 2) recommend the Proposal for Corporate Approval. Please note that the recommendation may not be endorsed at any stage of the Corporate Approval process.

If you have questions or concerns, please contact Teresa Goldstein, Manager, Affordable Housing at (403)268-5407 or via email at teresa.goldstein@calgary.ca

Yours truly,

A handwritten signature in blue ink, appearing to read "J. Tchinkova", written over a horizontal line.

Jana Tchinkova
Acting Director
Calgary Housing
Rocky Mountain Plaza, 8th Floor, 165 Macleod Trail SE
P.O. Box 2100, Station M, Calgary, AB Canada T2P 2M5

Letter of Understanding – Affordable Housing**ACKNOWLEDGEMENT**

CIDEX, Group of Companies, agrees to Proposed Terms and Conditions for Density Bonusing – Affordable Housing as specified in the attached Attachment "A." Please sign and initial the corresponding pages.

Sarah Titani
Authorized Agent

Name (print):

Sarah Titani

April 1, 2019
Date

Julie Pallasigui
Witness

Name (print):

Julie Pallasigui

April 1, 2019
Date

Urban Strategy Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0404

Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

EXECUTIVE SUMMARY

Located along the boundary of four communities; Altadore, South Calgary, Garrison Woods and Richmond, the 33 Avenue SW main street area, includes 33 and 34 Avenues SW. This main street is identified as a Neighbourhood Main Street by the *Municipal Development Plan* (MDP). The purpose of this report is to bring forward city-initiated land use amendments to various properties and proposed amendments to the *Marda Loop Area Redevelopment Plan* in accordance with Council's direction and local stakeholder input.

The Council approved Main Streets Implementation Plan (PUD2017-0241 and PUD2018-0347) provides direction and a process to support the achievement of goals and targets contained within the MDP. The initiative is creating more opportunity for private investment to build a more complete community, along with staged infrastructure investment by the City of Calgary in the public realm for priority MDP areas.

Administration is recommending that Council approve the land use and policy amendments to create certainty for local stakeholders, achieve the MDP goals for Main Streets and shorten approval time for redevelopment with the appropriate land use districts. Specifically:

- redesignate portions of the main street to Mixed Use - General (MU-1) District and Mixed Use - Active Frontage (MU-2) District (Attachment 3); and
- amend the *Marda Loop Area Redevelopment Plan* (ARP) as shown in Attachment 4.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and:

1. **ADOPT**, by bylaw, the proposed amendments to the Marda Loop Area Redevelopment Plan (Attachment 4); and
2. Give three readings to the proposed Bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 5.3 hectares \pm (13.1 acres \pm) located at various addresses (See Attachment 3) from Commercial – Neighbourhood 1 (C-N1), Commercial – Neighbourhood 2 (C-N2), Multi-Residential-Contextual Low Profile (M-C1) District, Commercial – Corridor 1 f1.0h10 (C-COR1f1.0h10) District, Commercial – Corridor 1 f3.0h16 (C-COR1f3.0h16) District, Commercial – Corridor 1 f3.0h23 (C-COR1f3.0h23) District, Commercial – Corridor 2 f1.0h10 (C-COR2 f1.0h10) District, Commercial – Corridor 2 f2.0h12 (C-COR2 f2.0h12) District, Commercial – Corridor 2 f2.5h16 (C-COR2 f2.5h16) District to Mixed Use - General (MU-1f3.0h16), Mixed Use - Active Frontage (MU-2f3.0h16), Mixed Use -Active Frontage (MU-2f3.0h23) District; and
4. Give three readings to the proposed Bylaw.

Urban Strategy Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0404

Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

PREVIOUS COUNCIL DIRECTION / POLICY

At the 2017 April 10 Combined Meeting of Council, Council made a decision to:

ADOPT, Moved by Councillor Chabot, Seconded by Councillor Woolley, that the SPC on Planning and Urban Development Recommendation contained in Report PUD2017-0241, be adopted, as follows:

1. That Council approve the Main Streets Implementation Plan.

At the 2018 May 07 Regular Public Hearing Meeting of Council, Council decided to:

ADOPT, Moved by Councillor Magliocca, Seconded by Councillor Farrell, that the SPC on Planning and Urban Development Recommendation contained in Report PUD2018-0347, be adopted, as follows:

1. Approve the Scope of Work outlined in Attachment 1 and direct Administration to Report back to Council through the SPC on Planning and Urban Development no later than Q4 2019; and
2. Approve, as amended, the Main Streets Implementation Plan as shown in Attachment 2.

BACKGROUND

In 2017 April, Council approved the Main Streets Implementation Plan with a focus on the application of policies, goals and targets contained within the MDP. The implementation plan also included a prioritization of the Main Street areas, with 33 Avenue SW being one of twenty areas for near term action (2017 to 2022).

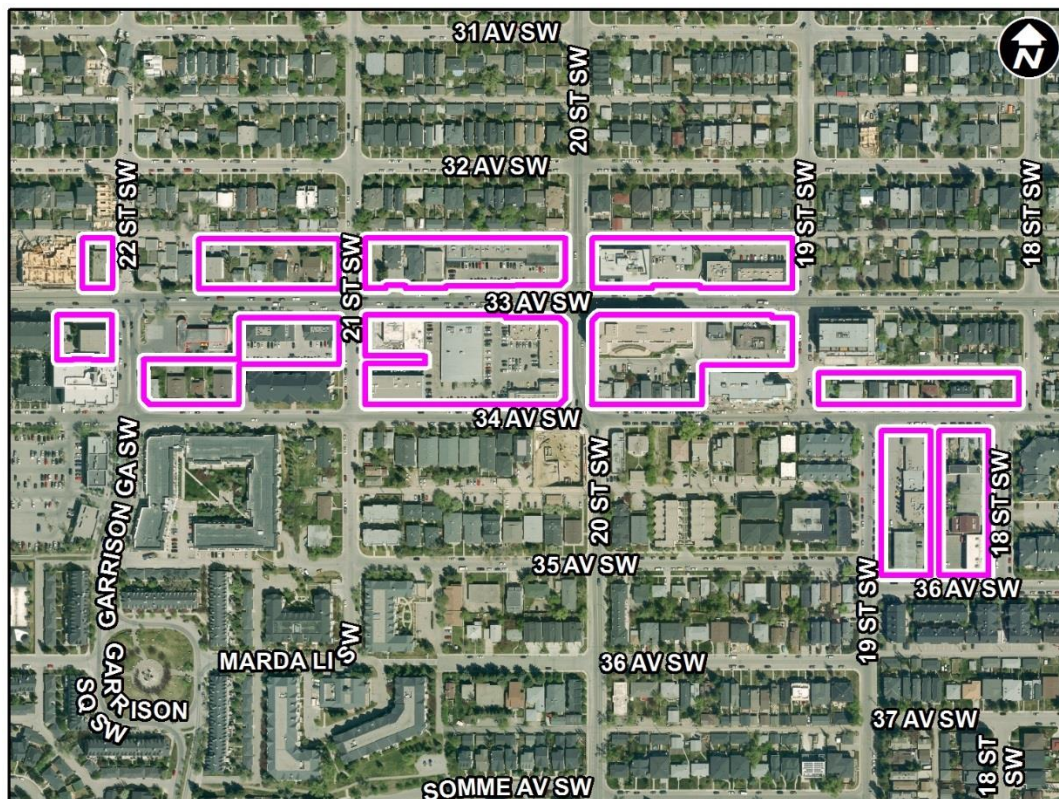
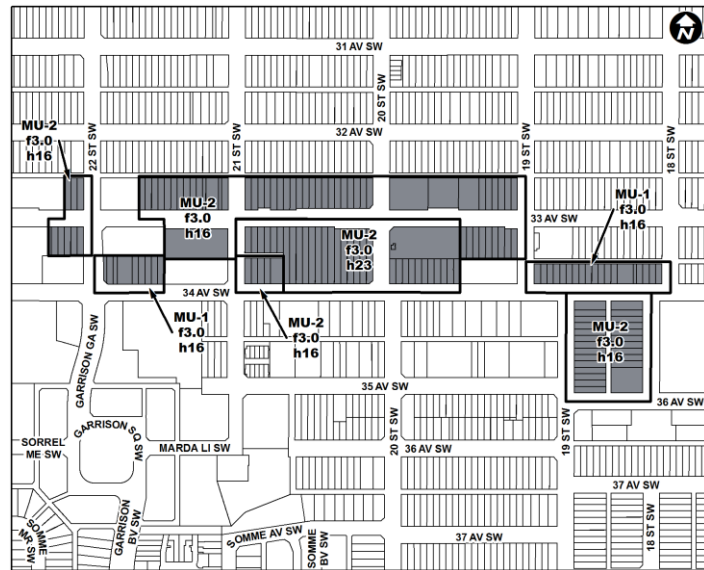
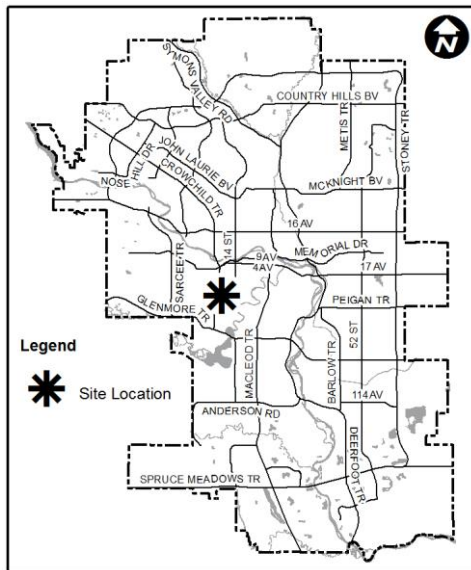
Implementation plan actions include an updated land use framework of new land use districts and amended local area policy, followed by examination of the main street public right-of-way for potential improvements. The recommendation in this report includes minor amendments to the Marda Loop ARP with proposed changes to the Maximum Building Height Map (Map 4.2).

The 33 Avenue SW Main Street area includes the broader Marda Loop retail district and a variety of commercial and residential properties along both 33 and 34 Avenues SW. The main street is characterized by standalone commercial buildings and integrated mixed use development, generally between two and four storeys, with some developments at six storeys.

The Main Street area has adjacent low density residential areas to the north and a variety of residential land uses to the south. Thirty-third Avenue SW is a primary transportation link for the area, providing automotive connection to the regional skeletal network at Crowchild Trail SW with 33 Avenue SW being part of the Primary Transit Network.

Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

Location Maps



Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

INVESTIGATION: ALTERNATIVES AND ANALYSIS

From November 2014 to June 2016, the Main Streets Initiative focused on analysis and evaluation of a variety of factors that influence growth and change in all the Main Streets areas.

This included detailed analysis of feedback from stakeholders, review of the existing policy, land use districts and parking requirements. It also looked at current and future market demand and growth forecasts, infrastructure capacity within the Main Street areas and costs to redevelop streets and sidewalks that align with the *Calgary Transportation Plan* (CTP) and *Complete Streets*.

Background research prior to the creation of the implementation plan and the determination of the priority main street areas included market assessment and analysis of demand for office, retail and residential in all MDP main street areas. With the exception of smaller-scale local serving uses (such as medical or professional services), 33 Avenue SW is not identified as a likely location for the future development of major office space. Background research suggested that multi-residential development shows the highest potential for growth.

The neighbourhoods surrounding this main street area have seen a lot of growth. Garrison Woods has only be separated out of civic census since 2016, prior to that the population was counted within Altadore. Three of the four surrounding neighbourhoods had their peak population within the last 3 years, with Richmond nearing its all-time peak level. Considering all four neighbourhoods, from 2008 to 2018, the population grew from 16,102 to 19,051 (18.3 percent, 2,949 persons).

Figure 1: Community Peak Population

	ALTADORE*	GARRISON	SOUTH CALGARY	RICHMOND
Peak Population Year	2015	2016	2018	1968
Peak Population	9,867	3,188	4,154	5,080
2018 Current Population	6,831	3,184	4,154	4,882
Difference in Population (Number)	3,036	4	0	198
Difference in Population (Percent)	30.7	0.12	0	3.89

*Altadore included Garrison Woods until 2016
Source: *The City of Calgary 2018 Civic Census*

Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

Considering there will be a new local planning process beginning along this main street area, and across the broader surrounding communities in 2020, the city initiated policy and land use amendments are only recommended within the boundary of the existing *Marda Loop Area Redevelopment Plan*, the section of the main street with the largest amount of growth and change. The recommended policy amendments do not make use of the current version of the DAG as an updated statutory plan will be considered by Council soon and current ARP policy is aligned with the MDP.

Local Area Planning

Adopted by Council in 2014, the *Marda Loop ARP* identifies 33 and 34 Avenues SW as growth and redevelopment areas and provides up-to-date guidance for mixed-use and commercial development. Since this local plan was adopted well after approval of the MDP, the goals, objectives and targets of the MDP are reflected in the plan. This application includes minor amendments to the Maximum Building Heights Map (Map 4.2) to better reflect existing land use designation height limits and current subdivision patterns.

Land Use

Current Land Use

The MDP identifies 33 Avenue SW as a Neighbourhood Main Street, serving the local community with a variety of commercial services and may provide some retail/restaurant uses with a regional draw. Neighbourhood Main Streets provide the opportunity for moderate levels of intensification of both jobs and population over time.

Current land use districts do not allow for Neighbourhood Main Street to reach the population employment targets outlined in the MDP. The MDP identified main street segment runs from approximately Crowchild Trail SW to 14 Street NW. The proposed rezoning focusses only on the area located within the current ARP, which is located from 18 Street SW to Crowchild Trail SW.

Proposed Land Use

Community input has informed and influenced decisions about growth and land use change along the 33 and 34 Avenues SW main street area. The project team reviewed input on the following:

- Residential and commercial development types along the main street;
- Residential transitioning from the main street to surrounding areas;
- Where retail/commercial development is appropriate and desired; and
- Policy to support the retention of historic character homes in the transition area.

The proposed land use districts for 33 and 34 Avenues SW encourages a vibrant public realm, a greater variety of retail and small business, and helps to increase the potential for local services and housing choices.

Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

Figure 3 (below) indicates that the intensity (people and jobs per gross developable hectare) achievable under the existing land use districts is 80; however, actual existing intensity of the area is approximately 65.

Considering the full Main Street area, the proposed land use amendments will be able to achieve an intensity of 136, which exceeds the MDP target of 100. Since the recommended amendments are only for a gross developable area of 8 hectares, compared to the overall main street area of 41 hectares, the future intensity of the full main street area would be greater.

Figure 3: 33 and 34 Avenue SW Density and Intensity

33 and 34 Avenues SW (data area shown on figure 2)			
	EXISTING	ALLOWED BY CURRENT LAND USE	POTENTIAL ALLOWED BY NEW LAND USE DISTRICTS
Number of jobs	702	900	1,215
Number of people	1,952	2,371	4,361
Number of dwelling units	626	883	1,626
Intensity (Jobs + Population / gross developable hectare) (Target 100)	65	80	136

To achieve goals and objectives of the MDP, the proposed land use designations are structured to support the necessary range of uses and appropriate scale of buildings along 33 and 34 Avenues SW. Figure 4 (below) indicates the proposed land use districts within the recommendation.

This application proposes to redesignate the subject sites from Commercial – Neighbourhood 1 (C-N1), Commercial – Neighbourhood 2 (C-N2), Multi-Residential-Contextual Low Profile (M-C1) District, Commercial – Corridor 1 f1.0h10 (C-COR1f1.0h10) District, Commercial – Corridor 1 f3.0h16 (C-COR1f3.0h16) District, Commercial – Corridor 1 f3.0h23 (C-COR1f3.0h23) District, Commercial – Corridor 2 f1.0h10 (C-COR2 f1.0h10) District, Commercial – Corridor 2 f2.0h12 (C-COR2 f2.0h12) District, Commercial – Corridor 2 f2.5h16 (C-COR2 f2.5h16) District to the land use districts listed in Figure 4:

Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

Figure 4: Proposed Land Use Districts

District	Hectares	Acres
Mixed Use - General (MU-1f3.0h16) District	0.327	0.808
Mixed Use - Active Frontage (MU-2f3.0h16) District	3.620	8.941
Mixed Use - Active Frontage (MU-2f3.0h23) District	1.353	3.342

The Mixed Use - General (MU-1) District is a recent land use district approved by Council in 2017. It was developed to support growth in key areas such as Main Streets. Characterized by street-oriented buildings typically between four and six storeys in height requiring a transition to lower scale residential uses on adjacent parcels through building location, building massing and landscaping. The main floor can be commercial or residential. The Mixed Use - Active Frontage (MU-2) District is very similar in building, site and use regulation as MU-1, but the main floor must be commercial uses and a slightly broader range of discretionary commercial uses are allowed.

Development and Site Design

Future development permit applications will be evaluated with respect to compliance with the MDP and the Marda Loop ARP. Details regarding urban design, including building interface with the public realm, vehicular access, ground floor uses, and landscape, will continue to be addressed as development permit applications are made. The proposed policy amendments allow for building height to shift further south, away from 33 Avenue SW (in alignment with existing land use and parcel boundaries) and allow applicants to benefit from the use of the mixed-use districts in the land use bylaw.

Environmental

An Environmental Site Assessment was not required. Efficient use of land and resources is a key aspect of working toward environmental sustainability. The approach to concentrate jobs and population on Main Streets that are well served by public transit, and where people can walk or cycle to meet many of their daily needs results in reduced dependence on the private automobile and greater mobility choice. Reduced automobile dependence also leads to corresponding reductions in greenhouse gas emissions, as 36 percent of all emissions in Calgary come from vehicular transportation. More efficient use of land for development also contributes to reducing impacts on the environment and natural systems both within the city and regionally.

Future development permit applications resulting from the proposed land use redesignations will continue to be evaluated to identify any environmental concerns on a site by site basis through the development permit process.

Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

Transportation

Redevelopment of sites along the Main Street will benefit from:

- a robust grid of streets, allowing vehicle access from defined intersections and dispersal
- of local access and on-street visitor parking across multiple streets;
- frequent transit service within walking distance along the Primary Transit Network; and
- established walking and cycling infrastructure including complete sidewalk networks, on and off-street cycling routes, and the city's pathway network.

Although these land use redesignations are strategically located, the Main Streets program and other City initiatives, such as RouteAhead and Step Forward, have identified critical improvements to the infrastructure for all modes of transportation that will require further investment to support The City's long-term vision.

The sites included in the proposed land use redesignations are served by a transit route on 33 Avenue SW with connections to the Centre City (Route 7). Through the grid of neighbourhood streets and collector roads, redevelopment sites have easy access to arterial streets, urban boulevards, neighbourhood boulevards and the skeletal road network (e.g. Crowchild Trail SW).

Future development permit applications resulting from the proposed land use redesignations will continue to be evaluated on a site by site basis through the development permit process.

Utilities and Servicing

As the Main Streets program continues to develop, Administration is working on a number of initiatives aimed at viable and successful redevelopment. This work includes:

1. Contribution to Calgary Growth Strategies' Industry/City Work Plan: Established Areas Initiative. This initiative undertakes cross-departmental actions to reduce barriers to redevelopment. This includes, but is not limited to:
 - (a) Piloting communities and specific development sites to identify, plan, and prioritize technical requirements to enable redevelopment.
 - (b) Review underutilized road rights-of-way on key corridors to increase flexibility of use of space.
 - (c) Development of funding tools to support developed areas investment.
 - (d) Review of the Centre City Levy program within updated legislative framework.
 - (e) Work towards a strategic growth plan for the established areas.
2. An ongoing commitment from Water Resources to support redevelopment where applicant-triggered infrastructure upgrades may create opportunities to improve utility networks in ways that benefit a larger set of redevelopment sites.

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Future development permit applications resulting from the proposed land use redesignation will continue to be evaluated by Development Engineering on a site by site basis.

Stakeholder Engagement, Research and Communication

The communities surrounding 33 and 34 Avenues SW (Marda Loop) were provided opportunities to comment on the Main Street Initiative. A two-phase engagement and communications strategy was developed to be wide-reaching in the affected community and to facilitate meaningful opportunities for engagement both in-person and online. The *Engage Spectrum* level for this project is Listen and Learn which is defined as “We will listen to stakeholders and learn about their plans, views, issues, concerns, expectations and ideas.”

Stakeholders were asked to review the proposed land use districts and consider the balance of commercial and residential uses. Additionally, stakeholders were able to share any other ideas or comments on the information provided. See Attachment 5 for the Final Engagement Summary Report.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The site is located within the ‘City, Town’ area as identified on Schedule C: *South Saskatchewan Regional Plan* Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this corridor, the proposal is consistent with policies on Land Use Patterns.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use and policy amendments build on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory - 2009)

The *Municipal Development Plan* (MDP) proposes a compact urban form for Calgary by encouraging increased population and job growth to occur in the existing, built-up areas of the city. Activity centres, corridors, and other areas that are well connected to the existing and future Primary Transit Network are identified as the primary areas to accommodate significant residential and employment growth. The MDP identifies this area as a Neighbourhood Main Street typology with emphasis as a multi-modal environment fronted by a mix of higher intensity residential and business uses that foster a vibrant street environment.

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To recognize the local context and create an urban environment that supports and integrates new development with existing communities, the specific density target for each corridor must be established through a Local Area Plan. The MDP identifies the remainder of the community of South Calgary within the Contextual Area as Inner City and Established typologies. These land use typologies are intended to experience gradual intensification over time.

Calgary Transportation Plan (Non-Statutory - 2009)

The *Calgary Transportation Plan* (CTP) identifies 33 Avenue SW as a Neighbourhood Boulevard where highest priority should be given to walking, cycling and transit and where *Complete Streets* policies should be implemented. This application aligns land use designations with these designations.

Marda Loop Area Redevelopment Plan (Statutory - 2014)

The current ARP was approved in 2014 with only minor changes since then. The ARP provides policy that promotes and encourages growth in the commercial area along the main street and aims to maintain this as a key part of the community. This application includes minor amendments (see Attachment 4) to the Maximum Building Height Map (Map 4.2) to reconcile policy level height limits with already existing land use designation height limits and the current subdivision pattern.

Social, Environmental, Economic (External)

The proposed recommendations will guide redevelopment towards the creation of a vibrant, transit-focused, liveable and distinctive mixed use community. These changes will allow more Calgarians the freedom to choose a broader range of housing, employment, and recreational opportunities in close proximity to frequent transit and in locations served by existing infrastructure, as called for by the MDP.

Long term land use policy and City-initiated land use district amendments represent a strategic approach to ensure an adequate supply of housing and employment space to keep pace with demands in the established communities. Supporting such supply increases in locations with existing and planned infrastructure is a key component in reducing the fiscal impact of growth, while also acting to attenuate pressure on housing affordability.

Efficient use of land and resources is a key aspect of working toward environmental sustainability. The approach to concentrate jobs and population on Main Streets that are well served by public transit, and where people can walk or cycle to meet many of their daily needs results in reduced dependence on the private automobile and greater mobility choice.

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Reduced automobile dependence also leads to corresponding reductions in greenhouse gas emissions, as 36 percent of all emissions in Calgary come from vehicular transportation. More efficient use of land for development also contributes to reducing impacts on the environment and natural systems both within the city and regionally.

Future development permit applications resulting from the proposed land use redesignations will continue to be evaluated to identify any environmental concerns on a site by site basis through the development permit process.

Financial Capacity

Current and Future Operating Budget

Calgary Transit and Roads are providing operating budgets for regular service transit facilities and general road right-of-way maintenance. Transit, Roads, Parks and Recreation, as part of normal business operations, monitor and determine future needs for expanding services levels. As these departments identify these new citizen needs, those departments will determine budget requirements. This land use and policy amendment has no known impact to current or future operating budgets.

Current and Future Capital Budget

The next phases of the Main Streets program are the design and build phases. The 33 and 34 Avenues SW Master Plan is near completion and will be moving into the detail design phase later this year in conjunction with the completion of infrastructure modelling and review. The funding to undertake this work is currently through the Main Streets Streetscape Program that is managed by Urban Strategy, where some Capital funds are available for a segment of the construction work.

Risk Assessment

Citizen and stakeholder interest and potential monetary investment in new development is very high for the Main Street areas. This current opportunity will be leveraged with City of Calgary actions to facilitate growth in identified strategic areas such as 33 and 34 Avenues SW. Not proceeding with guiding policy and land use redesignations could slow momentum created with many stakeholders and potentially develop mistrust with citizens and the development industry.

REASON(S) FOR RECOMMENDATION(S):

The proposed amendments respond to Council direction to action the Main Streets Implementation Plan, provide additional direction and certainty for area residents, landowners and applicants and allow for and enable an appropriate scale of development and intensity within the context of the Marda Loop.

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Policy Amendment and Land Use Amendment (City-Initiated) in Richmond Knob Hill, South Calgary, and Altadore, at Multiple Properties, LOC2017-0370

ATTACHMENT(S)

1. Municipal Addresses for City-Initiated Redesignation
2. Proposed Land Use - Colour Graphics
3. Proposed Land Use Map (Bylaw)
4. Proposed Amendments to the Marda Loop ARP
5. Final Engagement Summary Report

Municipal Addresses for City-Initiated Redesignation

	LEGAL DESCRIPTION	FROM	TO
33 Avenue SW			
2001 TO 2015 33 AV SW	PLAN 4479P BLOCK 69 LOTS 1 TO 9	C-COR1 f1.0h10	MU-2f3.0h16
2008 33 AV SW	PLAN 4479P BLOCK 58 LOTS 1 TO 6	C-COR2 f1.0h10	MU-2f3.0h16
2018 33 AV SW	PLAN 0812280 BLOCK 58 LOT 41	C-COR2 f1.0h10	MU-2f3.0h16
2031 33 AV SW	PLAN 0911651 BLOCK 69 STRATA LOT 22	C-COR1 f3.0h23	MU-2f3.0h23
2032 33 AV SW	PLAN 0812280 BLOCK 58 LOT 42	C-COR2 f1.0h10	MU-2f3.0h16
2101 TO 2127 33 AV SW	PLAN 4479P BLOCK 70 LOTS 1 TO 13	C-COR1 f1.0h10,C-COR1 f3.0h23	MU-2f3.0h23
2129 TO 2137 33 AV SW	PLAN 4479P BLOCK 70 LOTS 14 TO 20	C-COR1 f3.0h23	MU-2f3.0h23
2106 33 AV SW	PLAN 4479P BLOCK 57 LOTS 1 TO 4	C-COR2 f2.0h12	MU-2f3.0h16
2120 TO 2138 33 AV SW	PLAN 4479P BLOCK 57 LOTS 5 TO 20	C-COR2 f1.0h10	MU-2f3.0h16
2202 33 AV SW	PLAN 4479P BLOCK 56 LOTS 1 AND 2	M-C1	MU-2f3.0h16
2206 33 AV SW	CONDOMINIUM PLAN 8010282 UNITS 1 TO 4	M-C1	MU-2f3.0h16
2212 33 AV SW	PLAN 4479P BLOCK 56 LOTS 4 TO 7	M-C1	MU-2f3.0h16
2216 TO 2222 33 AV SW	PLAN 4479P BLOCK 56 LOTS 8 TO 11	C-COR2 f2.5h16	MU-2f3.0h16
2226 33 AV SW	PLAN 4479P BLOCK 56 LOTS 12 TO 14	M-C1	MU-2f3.0h16
2215 33 AV SW	PLAN 9911073 BLOCK 71 LOTS 23 AND 24	C-COR1 f2.0h12	MU-2f3.0h16
2402 33 AV SW	PLAN 4479P BLOCK 55 LOTS 1 TO 3	M-C1	MU-2f3.0h16
2403 AND 2409 33 AV SW	PLAN 4479P BLOCK 72 LOTS 3 TO 6	C-COR2 f2.0h12	MU-2f3.0h16
34 Avenue SW			
1902 AND 1908 34 AV SW	PLAN 4479P BLOCK 68 LOTS 1 TO 5	M-C1	MU-1f3.0h16
1914 34 AV SW	CONDOMINIUM PLAN 0512697 UNITS 1 TO 10	M-C1	MU-1f3.0h16
1918 TO 1940 34 AV SW	PLAN 4479P BLOCK 68 LOTS 8 TO 20	M-C1	MU-1f3.0h16

Municipal Addresses for City-Initiated Redesignation

1907 TO 1915 34 AV SW	PLAN 3890AG BLOCK B LOTS 1 TO 3	C-N2	MU-2f3.0h16
2020 AND 2022 34 AV SW	PLAN 1210081 BLOCK 69 LOTS 23 AND 24	C-COR1 f1.0h10	MU-2f3.0h23
2024 TO 2040 34 AV SW	PLAN 4479P BLOCK 69 LOTS 12 TO 20	C-COR1 f3.0h23	MU-2f3.0h23
2132 34 AV SW	PLAN 7741JK BLOCK 70 LOT A	C-N1	MU-2f3.0h16
2136 AND 2140 34 AV SW	PLAN 4479P BLOCK 70 LOTS 17 TO 20	C-N1	MU-2f3.0h16
2224 AND 2230 34 AV SW	PLAN 4479P BLOCK 71 LOTS 11 TO 15	M-C1	MU-1f3.0h16
2234 34 AV SW	CONDOMINIUM PLAN 7711614 UNITS 1 TO 4	M-C1	MU-1f3.0h16
2236 AND 2240 34 AV SW	PLAN 4479P BLOCK 71 LOTS 18 TO 20	M-C1	MU-1f3.0h16
36 Avenue SW			
1920 36 AV SW	PLAN 3890AG BLOCK "B" LOTS 15 TO 19	DC	MU-2f3.0h16
18 Street SW			
3501A 18 ST SW	CONDOMINIUM PLAN 1312527 UNITS 1 TO 3	C-N2	MU-2f3.0h16
3505 TO 3519 18 ST SW	PLAN 3890AG BLOCK B LOTS 5 TO 10	C-N2	MU-2f3.0h16
3527 18 ST SW	PLAN 1312488 BLOCK B LOT 29	C-N2	MU-2f3.0h16
19 Street SW			
3504 AND 3512 19 ST SW	PLAN 3890AG BLOCK B LOTS 19 TO 28	DC	MU-2f3.0h16
3512 19 ST SW	PLAN 3890AG BLOCK B LOT 19 TO 24	DC	MU-2f3.0h16
20 Street SW			
3332 20 ST SW	PLAN 1510031 BLOCK 58 LOT 43	C-COR1 f3.0h16	MU-2f3.0h16

The 33 Avenue S.W. Main Street area runs from Crowchild Trail to 14 Street S.W., with active uses along both 33 and 34 Avenues. It is located on the edge of the communities of Richmond and South Calgary, while the community of Altadore borders it to the south. The Marda Loop area is a popular destination for Calgarians with coffee shops, yoga studios and retail stores. Providing direction for redevelopment with land use planning policies, the Marda Loop Area Redevelopment Plan was approved in 2014. Decisions on growth are also informed by city wide policies in the Municipal Development Plan (MDP) and the recently approved Developed Areas Guidebook. Updates to the existing land use districts (zoning) and the Area Redevelopment Plan will provide more direction for growth and change in Marda Loop. These updates will be reviewed with City Council and are shown in this document.

Existing Land Use (Zoning)

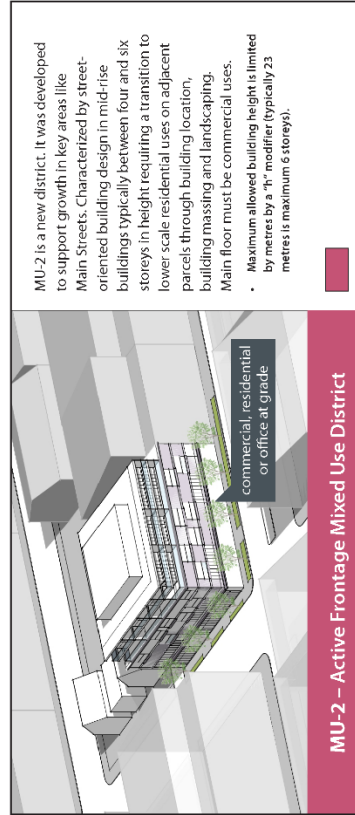
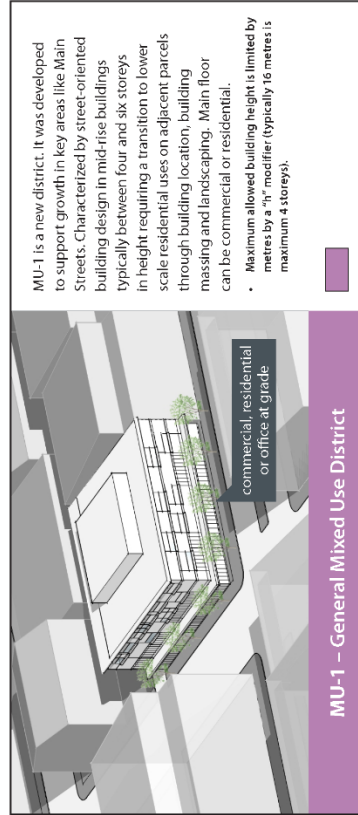
33 and 34 Avenues S.W. are a key retail and commercial service area for the surrounding residential neighbourhoods. Current zoning does allow for development to reach the minimum population and employment targets outlined in the Municipal Development Plan, but only to the minimum target and limits opportunity for full growth potential. Along the Main Street itself, the current zoning does allow for low-rise mixed use development, but the current land use population allowances are not necessarily sufficient enough to prompt redevelopment. Rezoning could allow greater flexibility for mixed use within the Main Street area, allowing more households and businesses to choose Marda Loop.

Proposed Land Use (Zoning)

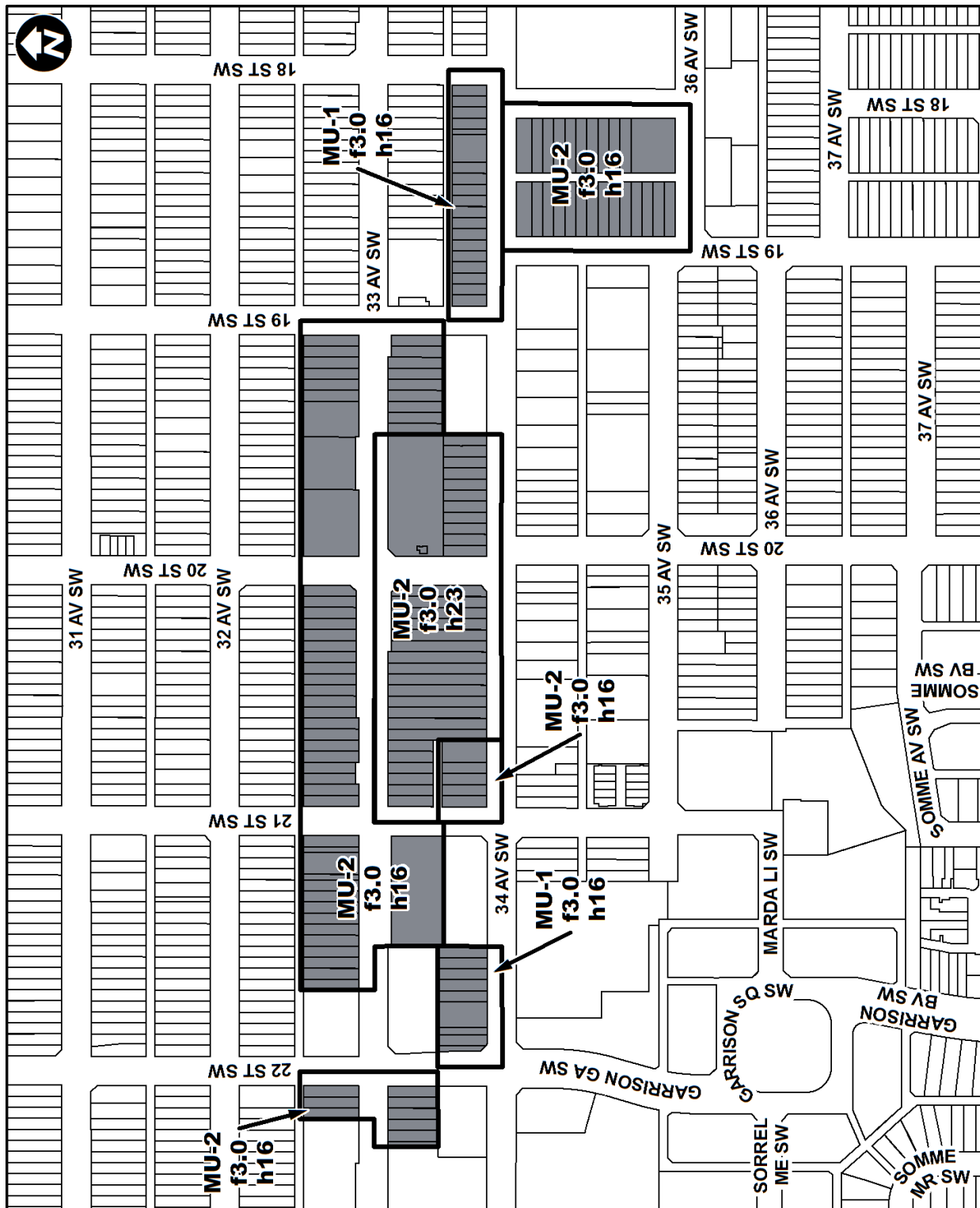
When residents provided input about the future of this area, they shared their hope that commercial business would continue to serve the local community and be a destination for all Calgarians; there be more potential for future markets, events and festivals; and that an improved street environment that is safe and vibrant would provide improved connectivity with the city.

In February, May and September 2018, local residents provided detailed feedback at public open houses and through The City's online engagement portal on a proposed land use framework to meet this growth potential. These comments were considered when refining this proposal. This new zoning proposal will provide opportunity for new forms of housing and commercial space. Additional housing options will benefit businesses in the neighbourhood and the main street area can grow to meet the desires expressed by the community.

Proposed Land Use – Colour Graphics



Proposed Land Use Map (Bylaw)



Proposed Amendments to the Marda Loop ARP

WHEREAS it is desirable to amend the Marda Loop Area Redevelopment Plan Bylaw 3P2014, as amended;

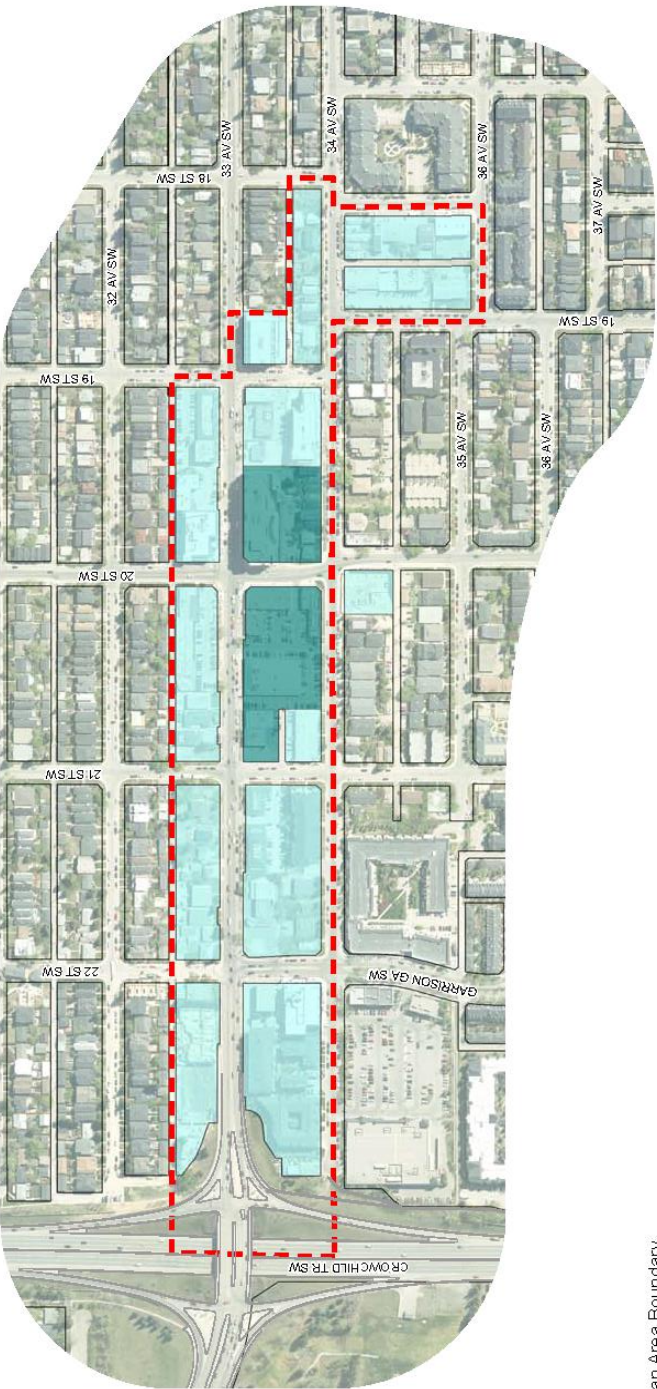
AND WHEREAS Council has held a public hearing as required by Section 692 of the Municipal Government Act, R.S.A. 2000, c.M-26, as amended:

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

1. The Marda Loop Area Redevelopment Plan attached to and forming part of Bylaw 3P2014, as amended, is hereby further amended as follows:
 - (a) Replace Figure 3 entitled 'Land Use Policy Areas' with the revised Figure 3 entitled 'Land Use Policy Areas' attached as Schedule "A".

Proposed Amendments to the Marda Loop ARP

SCHEDULE A



Legend

- Plan Area Boundary
- 4 Storeys in 16 metres
- 6 Storeys in 23 metres



This map is conceptual only. No measurements of distances or areas should be taken from this map.

Final Engagement Summary Report

Engagement 2014 to 2015

Phase 1 of the Main Streets initiative was stakeholder discussion and public engagement. From November 2014 to May 2015 there were five public engagement opportunities for each of the 24 MDP corridors or main street areas. This resulted in 1,335 hours of discussion and 4,112 comments provided by over 2,200 Calgarians at events and online. As well, the Main Streets team presented project details at 20 meetings to community associations, business revitalization zones and other groups during this same time. Following this public engagement phase, Local Viewpoint Maps were created for most main street areas, including 33/34 Avenues SW Main Street area.

33/34 Avenues Main Street Area - Public Meetings 2018

Three information sessions with 6 events were held, the first one was a re-introduction of the Main Street initiative to residents of the area in February of 2018. In May and September of 2018, five workshop events were paired with online opportunities to provide input at the same time. This was followed by additional discussions with landowners, the Marda Loop Communities Association and the Marda Loop Business Improvement Area.

The first workshop sessions (May 8, 10 and 12, 2018 - "Discussing Opportunities") presented a model for a new land use framework using the scale of development ranges in the Developed Areas Guidebook, supported by examples of building forms and types. Worksheets asked participants about the range of building types, heights and locations of land uses to determine an appropriate land use district pattern. This new land use district pattern was vetted with residents and stakeholders. Detailed What We Heard report is available at:

<https://www.calgary.ca/engage/Documents/MardaLoop/6.2-What%20we%20heard-Storefront%20May%202018%20ap.pdf>

The second workshop sessions (September 22 and 25, 2018 - "Reviewing Outcomes") presented a detailed map with the new land use districts and presented the necessity of amending the Marda Loop Area Redevelopment Plan. This new land use framework was refined based on input from the May sessions. Worksheets asked participants about the range of land use districts, building heights and locations of commercial/residential land use districts. Detailed What We Heard report is available at:

http://www.calgary.ca/_layouts/cocis/DirectDownload.aspx?target=http%3a%2f%2fwww.calgary.ca%2fengage%2fDocuments%2f6.2-Engagement%2520Report%2520Back%2520Template%2520-%2520What%2520we%2520heard%252033%252034%2520Fall%25202018.pdf&noredirect=1&sf=1

A total of 800 people attended the events in February, May and September. In addition, approximately 350 pieces of feedback from in person and online opportunities were received. Additional revisions occurred based feedback from the event and where community associations or residents requested more discussion. The finalized land use pattern determined the required local area plan amendments. This proposal was mailed to all impacted property owners with specific details on the recommended changes for the main street area.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0454

Land Use Amendment in West Hillhurst (Ward 7) at 2402 Westmount Road NW, LOC2019-0014

EXECUTIVE SUMMARY

This land use amendment application was submitted by Elizabeth Barry on behalf of the landowners, Nicholas and Lorraine Ryan on 2019 February 01. The application proposes to change the land use designation of 2402 Westmount Road NW from Residential – Contextual One / Two Dwelling (R-C2) District to Multi-Residential – Contextual Grade-Oriented (M-CG) District to accommodate:

- multi-residential buildings (e.g. townhouses, apartment buildings);
- a maximum building height of 12.0 metres (an increase from the current maximum of 10.0 metres);
- a maximum of four dwelling units (an increase from the current maximum of two dwelling units); and
- the uses listed in the M-CG District.

The proposed land use is compatible with surrounding land uses and is in alignment with the applicable policies of the *Municipal Development Plan*.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.045 hectares \pm (0.111 acres \pm) located at 2402 Westmount Road NW (Plan 1197FW, Block 9, Lot 11) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Multi-Residential – Contextual Grade-Oriented (M-CG) District.
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

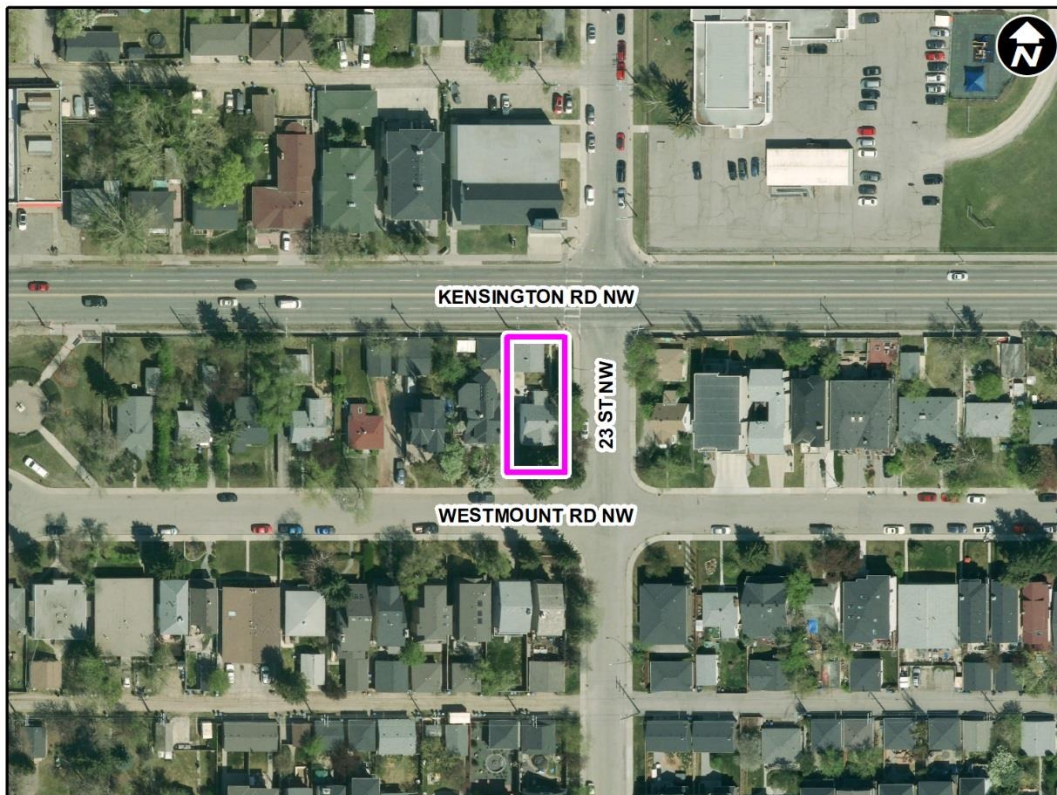
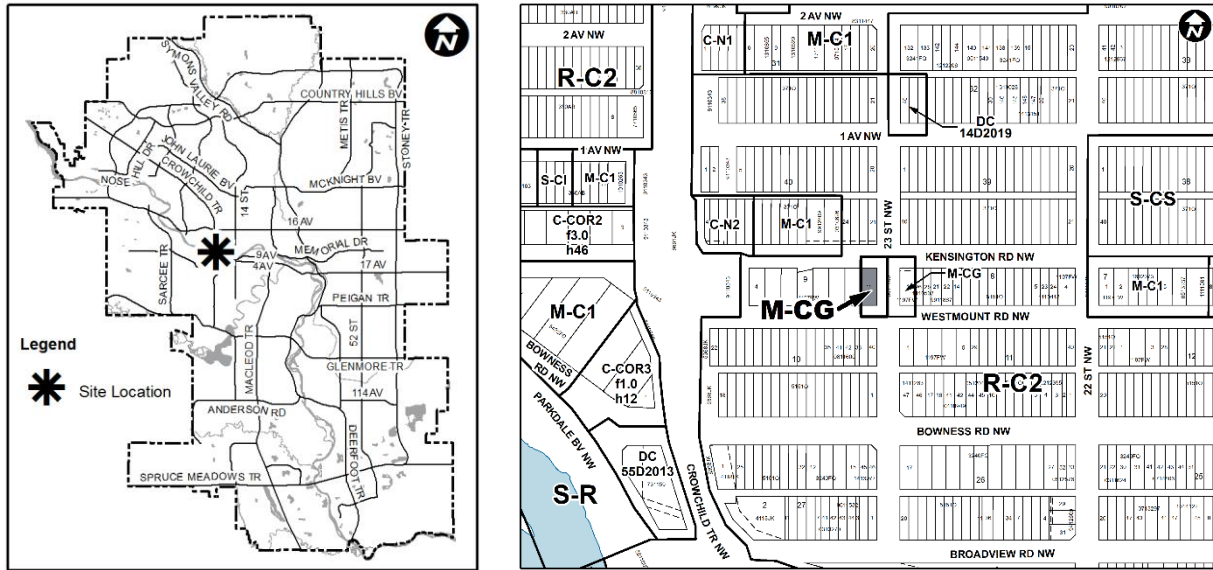
This land use amendment application was submitted by Elizabeth Barry on behalf of the landowners, Nick and Lorraine Ryan on 2019 February 01. While no development permit application has been submitted at this time, the future intent of the landowners is to explore redevelopment of the site in the form of townhouses or similar design, as noted in the Applicant's Submission (Attachment 1).

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0454

Land Use Amendment in West Hillhurst (Ward 7) at 2402 Westmount Road NW,
LOC2019-0014

Location Maps



**Land Use Amendment in West Hillhurst (Ward 7) at 2402 Westmount Road NW,
LOC2019-0014**

Site Context

The subject site is located at the corner of Westmount Road and 23 Street NW in the community of West Hillhurst. The parcel is approximately 13.8 metres wide by 36.2 metres deep, and is currently developed with a single detached dwelling that fronts onto Westmount Road NW. With street frontage on three of four sides, upon redevelopment, the parcel offers a unique opportunity to provide an especially active street interface.

Contextually, the site is located in an area of the community that is typically characterized by low and medium density residential development. Land use districts to the north across Kensington Road NW include both the R-C2 District and the M-C1 District. These parcels contain an apartment building, a church, and a school. The R-C2 District is predominant to the east, west, and across Westmount Road NW to the south. These parcels are mainly comprised of single and semi-detached dwellings. The parcel directly east, across 23 Street NW was previously redesignated from R-C2 District to M-CG District on 2018 February 20. A subsequent multi-residential development permit was approved 2018 August 29.

As identified in *Figure 1*, the community of West Hillhurst reached its population peak in 1968 with 6,871 residents.

Figure 1: Community Peak Population

West Hillhurst	
Peak Population Year	1968
Peak Population	6,871
2018 Current Population	6,507
Difference in Population (Number)	-364
Difference in Population (Percent)	-5%

Source: City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [West Hillhurst](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposed land use allows for a range of building types that have the ability to be compatible with the established building form of the existing neighbourhood. The proposal meets the objectives of applicable policies as discussed in the Strategic Alignment section of this report.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

**Land Use Amendment in West Hillhurst (Ward 7) at 2402 Westmount Road NW,
LOC2019-0014**

Land Use

The current land use for the site is Residential – Contextual One / Two Dwelling (R-C2) District and is intended to accommodate residential development in the form of duplex, semi-detached and single detached dwellings in developed areas of the city. This district allows for a maximum of two dwelling units and a maximum building height of 10.0 metres.

The proposed Multi-Residential – Contextual Grade-Oriented (M-CG) District is intended to accommodate multi-residential development in the form of townhouses and low-rise apartment buildings in developed areas of the city where some or all of the dwelling units have direct at-grade access. This district allows for a maximum height of 12.0 metres, with contextually sensitive rules that further restrict building height within proximity of low density residential districts and public streets.

The M-CG District allows for a maximum of 111 units per hectare. On the subject site, this amounts to 4.995 dwelling units which, per the bylaw, rounds down to four allowable dwelling units. It is, however, possible that a relaxation could be granted at the discretion of the Development Authority to allow for a maximum of five dwelling units. The one additional unit would only be considered upon review of the development permit application, and would be dependent on the building design, site layout, and supporting rationale.

Development and Site Design

The rules of the M-CG District provide guidance for the future site development including appropriate uses, number of dwelling units, building height and massing, landscaping and parking. Given the context of the subject site, other considerations that would be taken into account through the development permit process include, but are not limited to:

- emphasizing individual at-grade entrances;
- providing an appropriate street interface along all three frontages of the parcel;
- addressing potential design impacts of the interchange at Crowchild Trail NW and Kensington Road NW on the north side of the site;
- addressing the state of the retaining wall on the north side of the site along Kensington Road NW;
- defining front and rear amenity spaces; and
- privacy, overlooking and any other impacts on adjacent residential uses.

Environmental

An Environmental Site Assessment was not required as part of this application.

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Land Use Amendment in West Hillhurst (Ward 7) at 2402 Westmount Road NW, LOC2019-0014

Transportation

The recently completed Crowchild Trail Study envisions an interchange at Kensington Road NW and Crowchild Trail NW. While the impact of the design is yet to be defined, it may affect the northerly edge of the subject site. Any redevelopment proposals will be required to take this into account at the development permit stage.

The subject site is located less than 100 metres walking distance from a Primary Transit stop (Route 1) on Kensington Road NW. This segment of Kensington Road NW is part of the Primary Transit Network, and Route 1 is considered frequent bus service.

At the development permit stage, vehicular access and parking will be required to be designed to the satisfaction of Administration. No vehicular access to Kensington Road NW will be permitted.

Utilities and Servicing

Water, sanitary, and storm utilities are available to service the proposed land use. The subject site is not located in an area of known infrastructure concern or servicing review.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice was posted on-site. Notification letters were sent to adjacent land owners and the application was advertised on-line. No public open houses were held for this application.

Administration received comments from the West Hillhurst Community Association in support of the proposal (Attachment 2).

Administration received three letters in opposition to the application. The chief concerns expressed within these letters included:

- the lack of corresponding development permit plans;
- potential parking and traffic issues;
- potential massing impacts;
- the impact of the interchange at Kensington Road NW and Crowchild Trail NW envisioned within the Crowchild Trail Study;
- the future prospect of existing trees on the subject parcel; and
- additional garbage bins on the street.

Following Calgary Planning Commission (CPC), notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, CPC's recommendation and date of Public Hearing will be advertised.

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ISC: UNRESTRICTED
CPC2019-0454

Land Use Amendment in West Hillhurst (Ward 7) at 2402 Westmount Road NW, LOC2019-0014

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSRP) which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The subject site is situated within the Neighbourhood Main Street typology along Kensington Road NW as designated on Map 1 – Urban Structure of the *Municipal Development Plan* (MDP).

Neighbourhood Main Streets provide the opportunity for moderate levels of intensification of both jobs and population over time and should provide for a broad mix of residential uses. Appropriate transitions to adjacent residential neighbourhoods from these areas is essential and should be sensitive to the scale, form and character of the surrounding community. While this is a Main Street area, The City has no plan to develop additional Main Street policy or perform a City initiated land use redesignation on the subject site within the near future.

The proposed M-CG District aligns with the policies of the MDP by allowing for modest residential intensification while remaining contextually sensitive to the scale and character of the adjacent low density neighbourhood.

There is no local area policy plan in effect for this area.

Social, Environmental, Economic (External)

The proposed land use allows for a wider range of housing types than the existing R-C2 District and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics in West Hillhurst.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

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**Land Use Amendment in West Hillhurst (Ward 7) at 2402 Westmount Road NW,
LOC2019-0014**

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposed land use of Multi-Residential – Contextual Grade-Oriented (M-CG) District is in keeping with the applicable policies of the *Municipal Development Plan*. The district is contextually sensitive and will accommodate a greater variety of housing on the subject site that is compatible with the existing character of the surrounding low density residential development.

ATTACHMENT(S)

1. Applicant's Submission
2. Community Association Letter

Applicant's Submission

This 0.045 hectare parcel located at 2402 Westmount Road NW in the community of West Hillhurst is currently zoned RC2. We are requesting a land use redesignation to M-CG. This is a corner lot has a single family home on it at present, but with a push for density in the inner city communities, we see this parcel as a prime location to provide an opportunity for future redevelopment.

Being cognisant of the City of Calgary's goals within the MDP via a focus on Main Streets, this parcel is in a prime location to support that initiative. To the North of the parcel is Kensington Road NW, to the East is 24th Street NW, to the West is an adjacent large single family home, and to the South is Westmount Road NW. In fact, the corner parcel to the East of the site, sharing 24th Street NW was recently approved for this exact redesignation (2340 Westmount Road NW).

At this time, the applicant and Owner only wish to pursue a land use redesignation with future intentions to explore redevelopment of the site. Appropriate redevelopment might look like four-to-five townhouse homes or similar. Sensitivity to the surrounding neighbours would be taken into consideration at time of redesign of the site.

We feel that this corner parcel possesses an opportunity to enhance the site, increase density in a sensitive way, and support the City of Calgary's goals within the established areas of the city. We look forward to hearing your comments on this request for land use redesignation.

Community Association Letter

Sent: Thursday, February 28, 2019 9:15 AM

To: CPAG Circ <CPAGCirc@calgary.ca>

Cc: Siriphokham, Jessica C. <Jessica.Siriphokham@calgary.ca>; CAWard7 - Dale Calkins <caward7@calgary.ca>

Subject: [EXT] Re: LOC2019-0014 - Circulation Package

Hello Jessica,

The WHCA Planning Committee has reviewed LOC2019-0014 and offers the following comments:

- The committee supports the re-designation and looks forward to seeing an appropriate façade and active interface with Kensington Road
- Increased density in the area is supported as is improving the south streetscape of Kensington Road |

Thank you,

Karen Dahl

Planning Committee Coordinator

West Hillhurst Community Association

planning@westhillhurst.com

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0433

Land Use Amendment in Greenview Industrial Park (Ward 4) at 4140 – 6 Street NE, LOC2019-0010

EXECUTIVE SUMMARY

This land use amendment application was submitted by Se7en Deziign on behalf of landowner 879076 Alberta Ltd (Samir Omar) on 2019 January 16. This application proposes to change the designation of the subject site from Industrial – General (I-G) District to a DC Direct Control District based on the Industrial – Commercial (I-C) District with the additional Discretionary Use of Place of Worship – Large to allow for:

- industrial developments with support commercial uses (e.g. warehouses with commercial storefronts, restaurants, retail stores, industrial buildings with offices);
- a maximum building height of 12 metres (a decrease from the current maximum of 16 metres); and
- the uses listed in the proposed Industrial - Commercial (I-C) designation with an additional use of Place of Worship – Large.

The proposed land use amendment to DC Direct Control District is supported by Administration as it aligns with the applicable policies of the *Municipal Development Plan*.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.68 hectares ± (1.69 acres ±) located at 4140 – 6 Street NE (Plan 8710998, Block Q, Lot 10) from Industrial – General (I-G) District to DC Direct Control District to accommodate a greater variety of commercial uses and allow the additional discretionary use of Place of Worship – Large, with guidelines (Attachment 3); and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

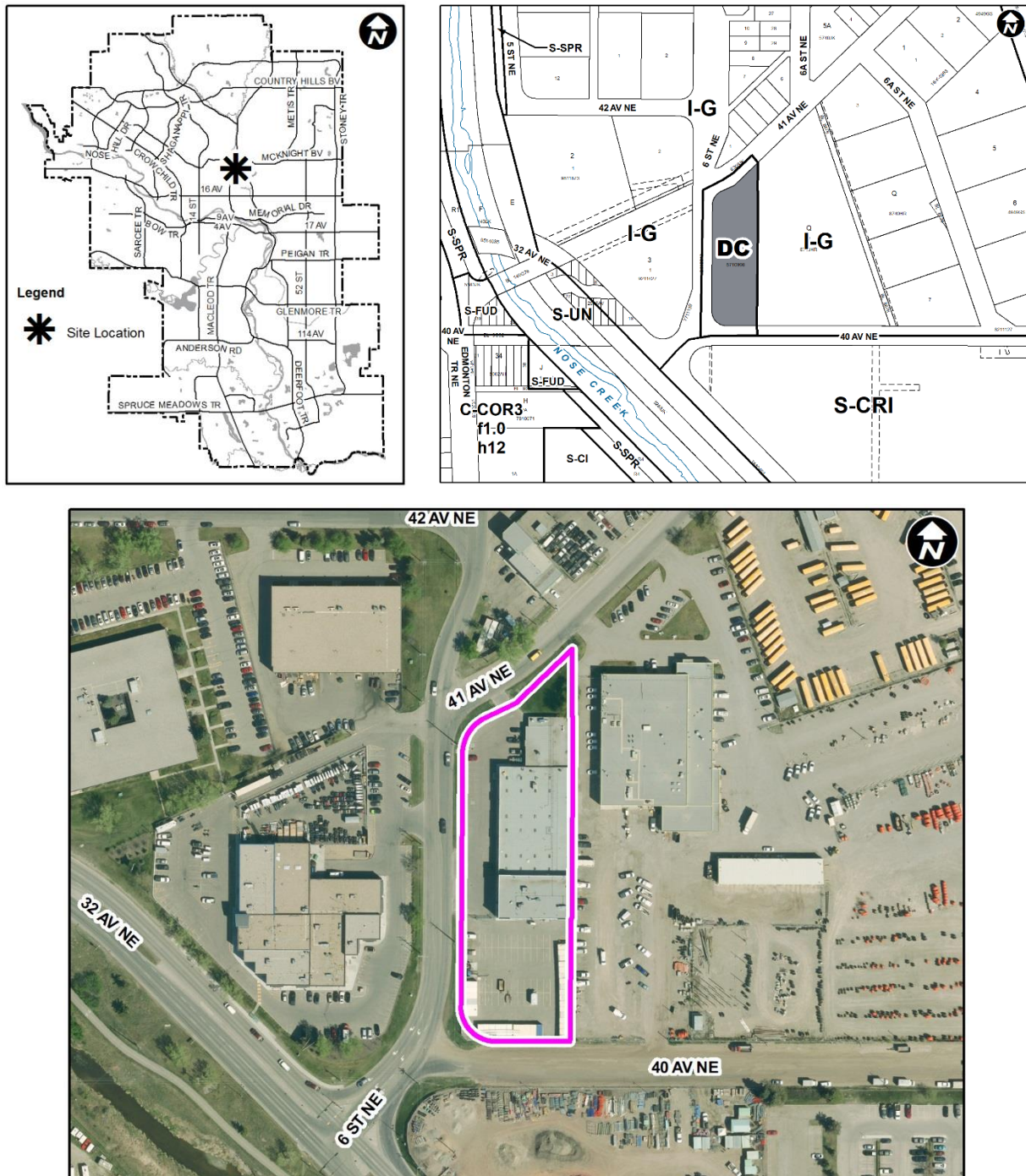
This land use amendment application was submitted by Se7en Deziign on behalf of landowner 879076 Alberta Ltd (Samir Omar) on 2019 January 16. No development permit application has been submitted at this time. The Applicant's Submission (Attachment 1) notes that the applicant is looking for a wider variety of commercial uses to attract future tenants, and is looking to accommodate a current tenant already approved and operating as a Place of Worship – Large.

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ISC: UNRESTRICTED
CPC2019-0433

Land Use Amendment in Greenview Industrial Park (Ward 4) at 4140 - 6 Street NE,
LOC2019-0010

Location Maps



Land Use Amendment in Greenview Industrial Park (Ward 4) at 4140 - 6 Street NE, LOC2019-0010

Site Context

The subject site is located in the community of Greenview Industrial Park, north of 40 Avenue NE, east of 6 Street NE, and south of 41 Avenue NE. Sixth Street NE is the primary entranceway to the industrial park, making this site visually prominent and accessible to everyone entering or exiting the area. The site is approximately 0.68 hectares in size and is currently developed with light industrial and commercial uses in a primarily one-storey building that includes a two-storey bay on the south end of the building. There are also several accessory buildings (sea cans) located on the perimeter of the parking area on the south portion of the parcel. Surrounding development is characterized by a broad mix of general industrial and supporting commercial uses to the north, west and east such as Hearts Choices Café and Market, Master Meats, Cycle Works, and Burwood Distillery. To the south is Spring Gardens, a municipal works depot. The predominant land uses in this area are Industrial – General (I-G) District and Special Purpose – City and Regional Infrastructure (S-CRI) District.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for the protection of industrial lands, while providing additional commercial options, and also allows for the existing operating Place of Worship – Large use to remain without becoming a non-conforming use. These additional commercial uses have the ability to be compatible with and complementary to existing uses in the area.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

The existing Industrial – General (I-G) District is an industrial designation that is for a wide variety of light and medium general industrial uses and a limited number of support commercial uses. The I-G District allows for a maximum building height of 16 metres and a maximum floor area ratio of 1.0.

A DC Direct Control District has been proposed based on the I-C District. The I-C District is intended to be characterized by a mix of light industrial uses and compatible small scale commercial uses located on the perimeter of industrial areas, ideally within 200 metres of a major street or expressway. This site is on the edge and is slightly less than 200 metres from 32 Avenue NE. Additionally, there are controls within the I-C District to address concerns that may arise in the future related to aesthetics and ensure that these highly visible parcels are attractive and provide an appropriate transition between other land use districts and the Industrial – General (I-G) District. The required side setback for a property line shared with an industrial district is 1.2 metres in the I-C District when next to an industrial parcel. The current building is directly on the east property line which is next to an I-G parcel, creating a non-conforming setback. Land Use Bylaw 1P2007 has provisions for non-conforming buildings, allowing for minor structural changes such as a new exterior door, permitting the building to keep the non-

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Land Use Amendment in Greenview Industrial Park (Ward 4) at 4140 - 6 Street NE, LOC2019-0010

conforming status. Should larger structural changes or additional floor area be proposed, the development authority has the opportunity to approve a relaxation to the side setback, bringing the building into conformity. Merits of the proposal would be reviewed through a development permit and approval is not guaranteed.

The proposed DC Direct Control District adds the additional discretionary use of Place of Worship – Large, allowing an existing tenant to remain a listed use in the district. The I-C District allows for additional small scale commercial uses such as counselling services, medical clinics, retail and fitness centres, which may be complimentary to the existing uses in the area.

Development and Site Design

The rules of the I-C District, which the proposed DC Direct Control District is based on, will provide basic guidance for future site development including landscaping, parking and access. The site is already essentially built-out, with a building that has seen recent re-investment and modernization, so proposed new development is more likely to occupy existing and vacant spaces within the building.

Environmental

There are no environmental concerns associated with the site or this proposal.

Transportation

Vehicular access to the site is available from 41 Avenue NE and 6 Street NE. No sidewalks exist in the area. The area is served by Calgary Transit bus service (Route 38) with stops located approximately 200 metres walking distance on 32 Avenue NE providing service to the Brentwood LRT station and the Whitehorn LRT station. On-street parking directly adjacent to the site is prohibited. However, there is a large surface parking lot on the south portion of the subject parcel. A Transportation Impact Assessment was not required as part of this application.

Utilities and Servicing

Water, sanitary and storm sewer mains are available and can accommodate potential redevelopment of the subject site without the need for off-site improvements at this time. Stormwater management will be considered and reviewed at development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's practices, this application was circulated to external stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

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Land Use Amendment in Greenview Industrial Park (Ward 4) at 4140 - 6 Street NE, LOC2019-0010

In response to the notifications, Administration received a letter of support from Thorncliffe Greenview Community Association (Attachment 2) and no objection from Highland Park Community Association. Administration did not receive any other comments in response to this application from the public.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSRP) which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory - 2009)

The subject parcel is located within the Industrial – Employee Intensive area as identified on Map 1: Urban Structure in the *Municipal Development Plan* (MDP). The MDP notes that Industrial Areas should be predominantly industrial in nature, and seeks to protect industrial land from encroachment of non-industrial uses. The MDP also supports complimentary commercial uses that support the function of the local businesses and support the day-to-day needs of the businesses and their employees. The I-C District allows the development of a broad range of industrial uses and small-scale compatible commercial uses in alignment with the land use policies of the MDP, and is appropriate for this parcel given its context within this industrial park.

There is no local area plan affecting the parcel.

Social, Environmental, Economic (External)

The recommended land use maintains the industrial character of the area, while allowing for a wider range of commercial uses. The land use supports the viability of small businesses and provides an opportunity for investment in the area and additional job creation.

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ISC: UNRESTRICTED
CPC2019-0433

**Land Use Amendment in Greenview Industrial Park (Ward 4) at 4140 - 6 Street NE,
LOC2019-0010**

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the *Municipal Development Plan*. The proposed DC Direct Control District is compatible with and complementary to the existing uses in the area, fits within the context of the area, and it provides an opportunity for a mix of light industrial and small scale commercial uses. The proposed redesignation is consistent with the intent of the I-C District and can be accommodated by existing infrastructure.

ATTACHMENT(S)

1. Applicant's Submission
2. Community Association Letter
3. Proposed Direct Control District Guidelines

Applicant's Submission

The landowner of 4140 6 Street N.E. wishes to rezone their parcel of land from I-G (Industrial General) to DC/I-C (Direct Control/Industrial Commercial).

To provide you site context and some history on this parcel of land. My client has owned the property since 2013 and took over ownership from the original building owner who had owned it since the 1960's. Unfortunately the previous owners had let the building fall into a state of disrepair operating as a self-storage facility. My client has made a substantial investment in the purchase of the building as well as incurring additional expenses in renovating the building to its current state from the brutalism architecture of the 1960's. Given the protracted economic recovery in Calgary as well as the limitations of the I-G zoning the owner of the land is struggling with finding appropriate tenancies willing to lease the remaining spaces within the building. IN the interim the owner has been donating a portion of the space to a non-profit organization (Youth Centre) so the building maintains a sense of occupancy to the adjacent landowners and does not become derelict due to vacancy. More recently the owner has donated an additional portion of the building to another non-profit organization to operate (Place of Worship – Large).

The owner's primary request for rezoning this parcel of land from I-G (Industrial General) to DC/I-C (Direct Control/Industrial Commercial) is to provide a greater variety of leasable uses (i.e. medical clinic, veterinary clinic, financial institution, etc). The owner intends to retain the existing uses within the building, which as a result will require us to also request a DC land-use district with the only added use being that to accommodate a "Place of Worship – Large" resulting in the following land use designation DC/I-C.

The proposed rezoning of this parcel to I-C/DC (Industrial Commercial/Direct Control) would be in alignment with the Northeast Industrial Area Structure Plan more specifically:

- a.) "Vision of the future" the Northeast Industrial Area, in the year 2030, has been transformed into a vital and successful business area that generates employment opportunities, provides good and services for the surrounding communities, compliments the Calgary International Airport and creates a gateway for the City.
- b.) The area is a fully serviced, containing predominantly industrial uses together with office, retail and service commercial development at select locations
- c.) In total, the Northeast Industrial Area comprises a prominent business area that conveys a positive image of growth, prosperity and diversity. The area performs a strong service and employment function, creates a quality environment in which to shop and work and contributes to a thriving and healthy city.

The intent to rezone to DC/I-C (Direct Control/Industrial Commercial) will benefit the existing businesses nearby and in fact will be complimentary in nature. It will be a compatible interface with the adjacent community and an attractive appearance when

Applicant's Submission

viewed from the adjoining public road (6 Street N.E.). The location is ideally situated being 300 meters of a major street (32nd Avenue N.E.).

We have engaged the Community Association as well as the Councillor Sean Chu on our proposed development via phone conversation and e-mails with respect to the land-use re-designation of the lot. The Thorncliffe/Greenview Community Association is very supportive of rezoning of this parcel of land (see attached letter of support). Ward 4 Councillor Sean Chu indicated that he has “no issues with this application” (see attached e-mail response). With respect to engagement with the adjacent land owners they have generally been receptive to the change in zoning from I-G to DC/I-C. In our e-mail correspondence with Neil MacDonald with Community Planning with the City of Calgary, Neil had indicated that the probability of success on such an application would be 50% based on his preliminary analysis and Neil had indicated that if we received support from the aforementioned stakeholders the likelihood of approval would be greatly increased.

Should you or your office require any further clarification on the plans please feel free to contact me.

Community Association Letter



THORNCLIFFE GREENVIEW COMMUNITY ASSOCIATION

5600 Centre Street N
Calgary, Alberta T2K 0T3

Administration Office: (403) 274-6840 Facsimile: (403) 275-7310 Email: admin@tgcacalgary.com

November 16 2018

To City of Calgary Planning Department, Councillor Sean Chu, Se7ven DeZigns,
and all others concerned,
Re: LOC for 4116 through 4140 6th Street NE

A few weeks ago TGCA was approached by a representative for a property at 41st Avenue and 6th Street NE in Greenview Industrial Park. Their intent being a land use change from IG to IC. Our nearly reflexive response was that of strong support for this initiative.

The current owner has sustainably retained the existing building while re-invigorating its exterior look and internal layout. In so doing has attracted new businesses to open on this significant corner of Greenview Industrial. This same level of rejuvenation has been happening across 6th Street with another similar vintage building complex. The result has been perhaps the most re-energized corner in Greenview Industrial. Given these progressive development changes to this short corridor of 6th Street NE, which is the only entrance point to the largest segment of Greenview Industrial Park, and the near proximity these properties have to 32 Avenue Connector, an IC designation we feel would be most fitting. The owner's representative has expressed a desire for IC in order to expand use potential for the building which still has some vacancy. We fully support this intent partially for the owner and partially for the vitality such a change could bring to this corner and the greater community.

Sincerely;

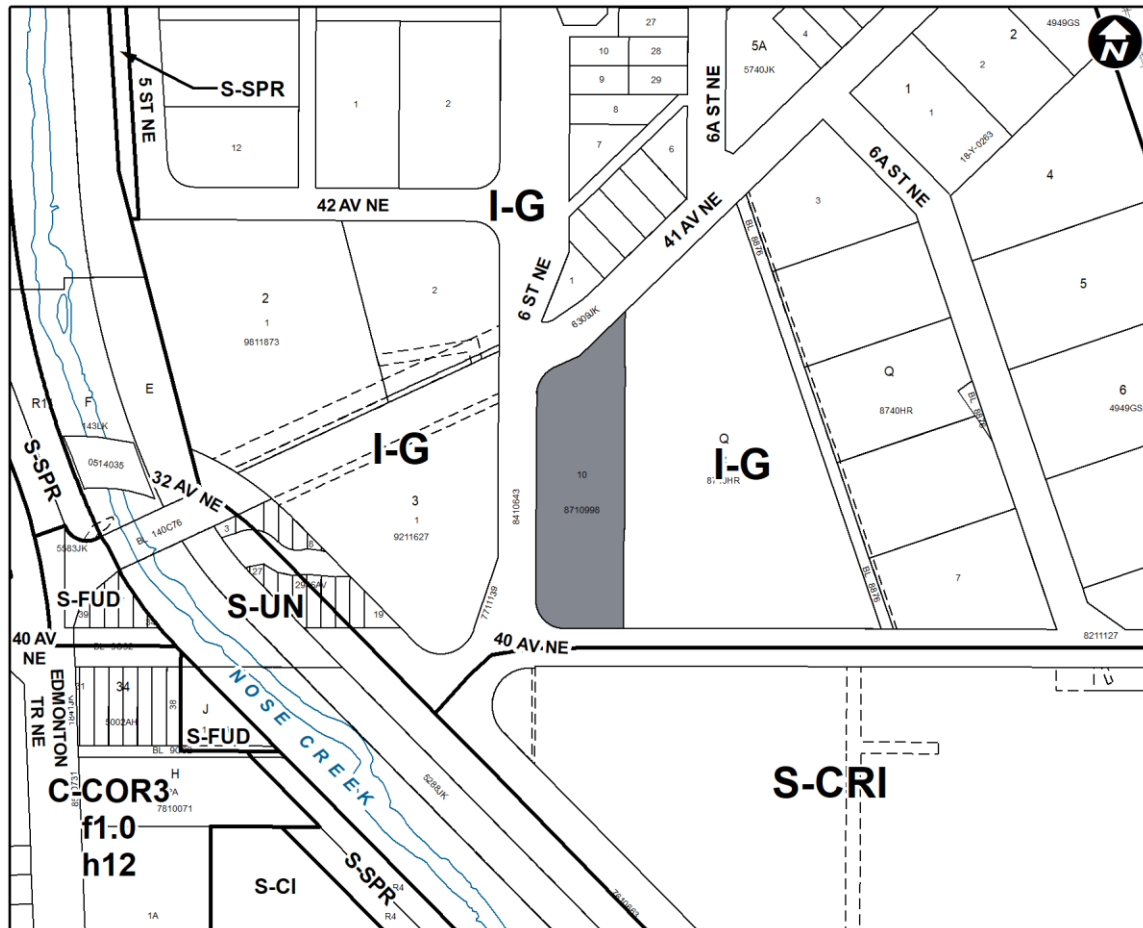


Marvin Quashnick
TGCA

Proposed Direct Control District Guidelines

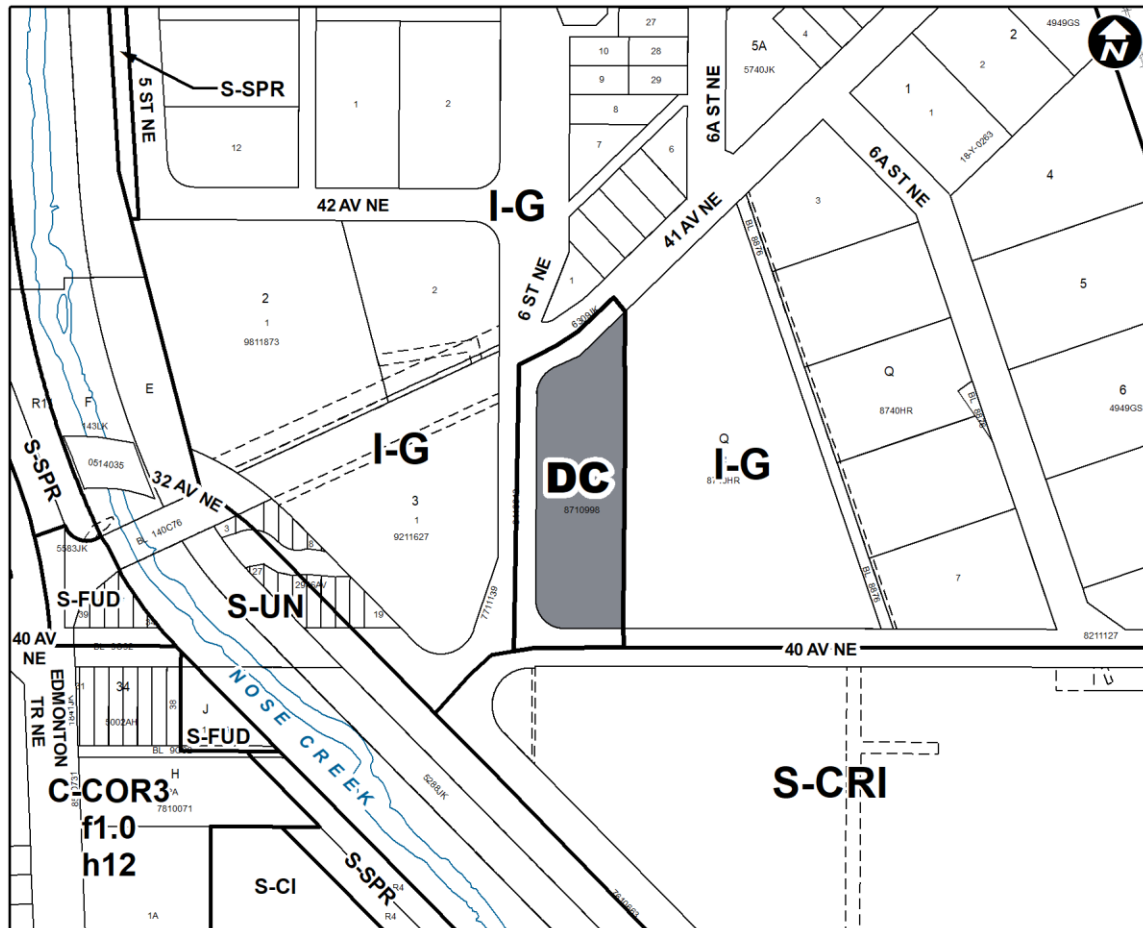
1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".

SCHEDULE A



Proposed Direct Control District Guidelines

SCHEDULE B



DIRECT CONTROL DISTRICT

Purpose

- 1 This Direct Control District is intended to:
 - (a) allow the additional **use** of **Place of Worship – Large**.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The **permitted uses** of the Industrial – Commercial (I-C) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Proposed Direct Control District Guidelines

Discretionary Uses

- 5 The ***discretionary uses*** of the Industrial – Commercial (I-C) District of Bylaw 1P2007 are the ***discretionary uses*** in this Direct Control District with the addition of:
- (a) **Place of Worship – Large.**

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Industrial – Commercial (I-C) District of Bylaw 1P2007 apply in this Direct Control District.

TEXT FOR DISCUSSION ONLY

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0461

Land Use Amendment in Arbour Lake (Ward 2) at 800 Crowfoot Crescent NW, LOC2018-0245

EXECUTIVE SUMMARY

This application was submitted by Quantumplace Developments on 2018 November 06 on behalf of the landowner, 1240725 Alberta Ltd (Landstar Development Corporation). The application proposes to change the designation of the property from DC Direct Control District (Bylaw 105Z83) to a DC Direct Control District to allow for:

- mixed-use development (e.g. commercial store fronts with apartments above);
- a maximum building height of 46 metres (consistent with the current maximum of 46 metres);
- rules designed to restrict the maximum height towards the rear of the site; a maximum building floor area of approximately 33,800 square metres based on a building floor to parcel area ratio (FAR) of 4.5; and
- the uses listed in the MU-2 District.

The proposal allows for a land use with a density and height that are compatible with surrounding development and in alignment with the applicable policies of the *Municipal Development Plan* and the *Transit Oriented Development Policy Guidelines*.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.75 hectares \pm (1.85 acres \pm) located at 800 Crowfoot Crescent NW (Plan 9810828, Block 3, Lot 18) from DC Direct Control District **to** DC Direct Control District to accommodate mixed-use development with guidelines (Attachment 2); and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

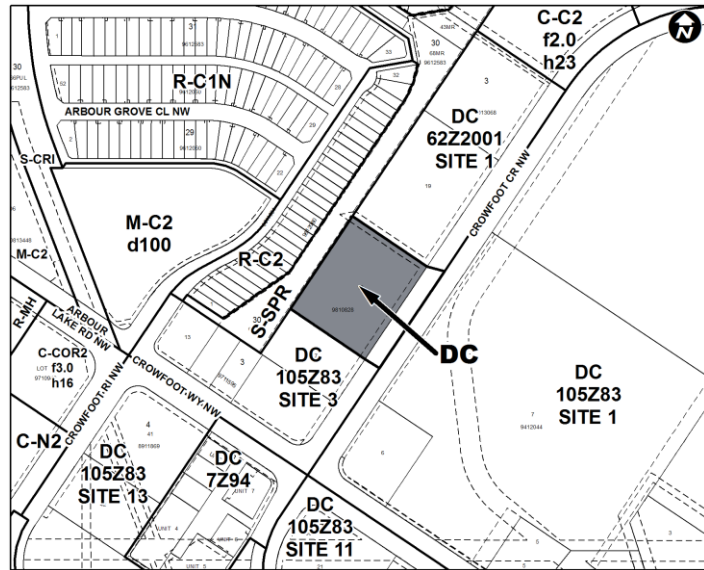
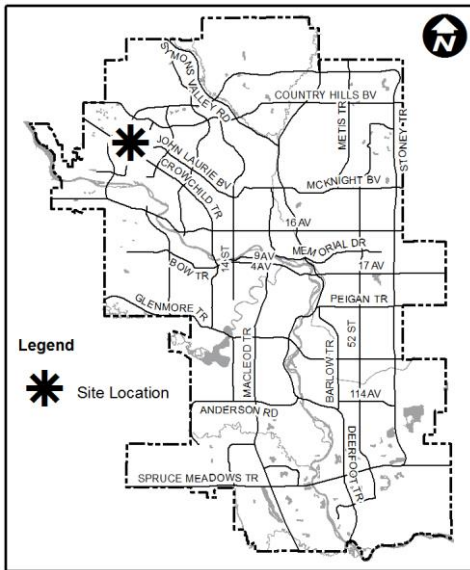
This application was submitted by Quantumplace Developments on behalf of the landowner 1240725 Alberta Ltd (Landstar Development Corporation) on 2018 November 06. No development permit application has been submitted at this time, however, as noted in the applicant's submission (Attachment 1), the applicant intends to develop a mixed-use building on the site with the possibility of seniors housing in a mix of unit types.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0461

Land Use Amendment in Arbour Lake (Ward 2) at 800 Crowfoot Crescent NW,
LOC2018-0245

Location Maps



Land Use Amendment in Arbour Lake (Ward 2) at 800 Crowfoot Crescent NW, LOC2018-0245

Site Context

The subject parcel is located along Crowfoot Crescent NW, north of Crowfoot Way NW in the community of Arbour Lake. The site is relatively large measuring 96 metres wide by 78 metres deep. The subject site is currently occupied by two single story commercial buildings with surface parking located between the buildings and the street. Surrounding the subject site is primarily low intensity commercial development to the north, south and east. To the west is a regional pathway and open space with low density residential beyond. This site is located approximately 600 metres from the Crowfoot LRT Station.

As identified in *Figure 1*, the community of Arbour Lake reached its population peak in 2014 and has seen a slight decline since.

Figure 1: Community Peak Population

Arbour Lake	
Peak Population Year	2014
Peak Population	10,987
2017 Current Population	10,713
Difference in Population (Number)	-274
Difference in Population (Percent)	-2%

Source: The City of Calgary 2017 Civic Census

Additional demographic and socio-economic information may be obtained online on the [Arbour Lake](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for a density that is transit supportive and includes more housing opportunities within the area. It also allows for a built form that has the ability to be compatible with the established building form of the existing area. The proposal meets the objectives of applicable policies as discussed in the Strategic Alignment of this report.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

The site is currently governed by a DC Direct Control District (Bylaw 105Z83) based on the C-2 District from Land Use Bylaw 2P80. The DC Direct Control District was developed for the whole of the Crowfoot commercial centre and currently allows for a maximum building height of 46 metres and 12 storeys, prescribes very specific caps on the amount of gross floor area allowed for commercial office uses and ties the maximum number of units to other sites within the area.

**Land Use Amendment in Arbour Lake (Ward 2) at 800 Crowfoot Crescent NW,
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A standard land use district was considered for the site, however due to concerns from Administration and the public around the allowable building height and the existing context to the rear of the site, a modified DC Direct Control District was proposed. A DC Direct Control District was also used to provide flexibility around the location of parking given the current lack of on-street parking in the area. The proposed DC Direct Control District is based on the Mixed Use – Active Frontage (MU-2) District of Land Use Bylaw 1P2007 (Attachment 2). Section 20 of the Land Use Bylaw indicates that DC Direct Control Districts must only be used for developments that, due to their unique characteristics, innovative ideas or unusual site constraints, require specific rules unavailable in other land use districts. A DC Direct Control District has been used for this application to provide for a transition in building height and massing towards the rear of the site and to restrict the shadowing impacts of this height on the region pathway. In addition to allowing for mixed-use development, the key components of the proposed DC Direct Control District include:

- a maximum height of 46.0 metres and a maximum density of 4.5 FAR;
- rules designed to restrict the maximum height towards the rear of the site;
- a provision to allow for a limited amount of parking between the building and street; and
- a rule to ensure the building will not be placed to the rear of the site.

Development and Site Design

The rules of the proposed DC Direct Control District, along with the rules of the base MU-2 District, will provide guidance for the development of the site including the height and building massing, landscaping and parking. The applicant provided a development concept for the site that proposes a twelve storey, mixed-use development with commercial at grade facing Crowfoot Crescent NW and residential units above. This proposal will need to be confirmed through a future development permit application process. Given the context of the subject site, issues that will be carefully considered through the development permit process include, but are not limited to:

- ensuring an engaging built interface along the street frontage, including placement of the building towards the front of the site;
- ensuring surface parking is limited and located appropriately;
- ensuring a sensitive building form with respect to the surrounding context, including low-density context to the west;
- minimizing shadowing impacts on the adjacent pathway and existing low-density development; and
- emphasizing individual at-grade entrances for both the residential and commercial.

Environmental

There are no environmental concerns associated with the site or this proposal.

**Land Use Amendment in Arbour Lake (Ward 2) at 800 Crowfoot Crescent NW,
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Transportation

The subject site is located 180 metres from a transit stop along Crowfoot Crescent NW as well as 600 metres from the Crowfoot LRT Station. Vehicular access is available from Crowfoot Crescent NW. A Traffic Impact Assessment (TIA) was submitted in support of the proposed land use redesignation. The TIA was reviewed and approved by Transportation Planning and no additional transportation infrastructure requirements were identified. Should the proposed concept change at the development permit stage, further analysis may be required.

Utilities and Servicing

Water, sanitary and sewer services are available and can accommodate the potential additional gross floor area without the need for off-site improvements at this time. Adjustments to on-site servicing may be required depending on the nature of future development proposals and would be determined at the development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised on-line.

The Arbour Lake Community Association was circulated as part of this application. They indicated that they had no concerns with the proposed land use.

The applicant held an open house within the community on 2018 October 10. The open house was held prior to the submission of the application and was intended to introduce the application to the community and gather initial feedback. Approximately 35 people attended the open house and feedback indicated that people were primarily concerned with building height and shadowing as well as traffic and parking.

There were six letters received from the surrounding residents in opposition to the proposed land use. Those opposed were concerned with the building height, potential shadowing impacts and loss of privacy, loss of existing businesses and traffic and safety issues.

Administration considered the relevant planning issues specific to the proposed redesignation and has determined the proposal to be appropriate with the specific provisions of the DC included. Compliance with relevant policies and bylaws, as well as design compatibility of discretionary uses with respect to the surrounding neighborhood context, traffic and access will be reviewed at the development permit stage.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

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Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to the site, the proposal is consistent with policies on Land Use Patterns.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory - 2009)

The subject site is located within a Major Activity Centre (MAC) as identified on Map 1 of the *Municipal Development Plan*. The MACs are located strategically across the city to provide major mixed-use destinations central to larger residential or business catchment areas. They are intended to provide opportunities for people to work, live, shop, recreate, be entertained and meet their daily needs. The MDP indicates a minimum intensity of 200 jobs and population per hectare for a MAC. Given the existing primarily low scale commercial development within the area, the current intensity is significantly below this minimum intensity target. The proposed land use meets the MAC policies by allowing for both a mix of uses and the potential for range of housing choices. This proposal represents one of the first higher density developments to add population intensity to the MAC along with the potential increase in jobs.

Transit Oriented Development Policy Guidelines (Non-Statutory - 2005)

The *Transit Oriented Development Policy Guidelines* provide direction for the development of areas typically within 600 metres of a Transit Station. The Guidelines encourage the type of development that creates a higher density, walkable, mixed-use environment within station areas to optimize use of existing transit infrastructure, create mobility options for Calgarians, and benefit local communities and city-wide transit riders alike. The proposed land use meets the key policy objectives of the Guidelines including ensuring transit supportive land uses, optimizing existing sites and infrastructure, as well as increasing density around transit stations.

Social, Environmental, Economic (External)

The recommended land use allows for greater density and potentially a wider range of housing types than the existing land use district, which allows limited residential development. As such, the proposed change may better accommodate the diverse housing choice of different age groups, lifestyles and demographics within the area. The land use will also continue to allow for a variety of shops and services that meet daily needs of the community.

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Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the *Municipal Development Plan* and the *Transit Oriented Development Guidelines*. The proposal represents an increase in density for this parcel of land and allows for a development that can be compatible with the character of the existing neighborhood. In addition, the subject parcel is located within walking distance of transit stops and the Crowfoot LRT Station.

ATTACHMENT(S)

1. Applicants Submission
2. Proposed DC Direct Control District Guidelines

Applicants Submission

On behalf of Landstar Development Corporation, and property owner of 1240725 Alberta Ltd., QuantumPlace Developments is pleased to submit this application to redesignate 800-838 Crowfoot Crescent NW, a 1.86 acre property in Crowfoot Centre. Retail shops and services are currently located on the site in a low-density format. The landowner would like to redesignate the property to facilitate a mixed-use building development with retail uses on the ground floor and residential uses above. The landowner envisions the possibility of seniors' housing for the residential portion, with a likely mix of independent and assisted living units. This would provide much-needed housing options for the aging population in that quadrant of the city. The site is located approximately 500 metres from the existing Crowfoot LRT station, making it a prime candidate for transit-oriented development. The site is also located within a Major Activity Centre (MAC), as identified in the 2009 Municipal Development Plan (MDP).

Existing Land Use District: Development on the site is currently governed by a Direct Control (DC) land use district that dates back to 1983. This DC district (DC105Z83) allows for a 12-storey building with office, retail or service commercial purposes and/or residential dwelling units, with a maximum FAR of 2.0. Although the existing DC district does allow for residential development, it is based on the Land Use Bylaw from 1980 and it does not conform to today's current city policies and nor does it allow for the type of mixed-use development (i.e., residential and commercial uses mixed within a single building) projected for this site. Furthermore, the existing 1983 DC district does not allow nor recognize the use of "Assisted Living," which is a key part of the future development envisioned for the site. Given the proximity of the site to the Crowfoot LRT station and The City's desire to provide more housing near major transit amenities, the site is perfectly suited to accommodate mid-rise development that is sensitive to the surrounding context. Much has changed since the existing DC bylaw came into effect in 1983, including the introduction of an LRT station at Crowfoot. A land use redesignation is a necessity for the site to bring the land use district into conformity with present day policies and to allow for a new development that meets contemporary planning objectives and recognizes the future context of the Crowfoot as a MAC.

New Land Use District: The landowner is seeking a redesignation to a new DC district based on the Mixed Use-Active Frontage (MU-2) district. The MU-2 district is intended to facilitate a mixed use building with active commercial uses at grade and is ideal to facilitate transit-oriented development in a MAC. The landowner is seeking a new DC district because of the unique context of the site. Situated on a commercial street (Crowfoot Crescent NW), the site is adjacent to low density residential housing to the west in the community of Arbour Lake. However, the adjacent housing is buffered from the site by a regional pathway, and the topography is such that the houses sit far above the site. It is recognized by the landowner that respecting community context is important, and as such it is intended that the DC district will allow for the creation of site-specific rules intended to ensure context-appropriate development that is friendly, graduated, and through its design achieves maximal natural integration into the area's landscape while also minimizing shadowing impacts on adjacent development. It is important to note that no change to the maximum height allowed on the site is sought as part of this application. The existing DC district allows for a maximum of 12 storeys (46 metres), and the landowner is seeking to maintain the existing allowed height limit at 46 metres. However, the landowner is seeking to increase the density allowed on the site to 4.5 FAR. The existing DC district allows for a maximum of 2.0 FAR, which will not allow for a commercially viable mixed-use seniors' housing development, as a minimum scale is required for it to be economically viable. An FAR of 4.5 and a maximum height of 46 metres will ensure that future development is feasible and takes full advantage of the site's location.

Applicants Submission

Major Activity Centre: Crowfoot Centre is identified as a Major Activity Centre (MAC) in the Municipal Development Plan (MDP). MACs are intended to incorporate the highest densities and levels of activity outside of the centre city. As per the MDP, Crowfoot is envisioned to incorporate high density development with a mix of residential and commercial land uses. The proposed land use redesignation is appropriate to facilitate the type of development envisioned for MACs and would be the first higher density proposal for the Crowfoot area in 35 years.

Transit-Oriented Development: The site is located within 500 metres of the Crowfoot LRT Station and is an approximately 10 minute walk from the station. Pedestrian connections to the station are direct and convenient via a legible route with crossing lights and marked crosswalks. The proposed land use designation is appropriate to facilitate a building form that is transit-supportive.

Mixed Use Development: The proposed DC district would facilitate mixed-use development, allowing for residential uses in Crowfoot. This is in alignment with both the MDP and the Transit Oriented Development Policy Guidelines, which identify mixed use as being a desirable building for within MACs and TODs respectively.

Need for Seniors' Housing and Opportunity for Aging in Place: A variety of demographic projections and forecasts indicate that Calgary's population is aging and seniors' housing is needed in all parts of the city. This need will persist into the future and Crowfoot is an ideal location for seniors' housing. This development will allow residents of Arbour Lake and other surrounding neighbourhoods to stay within the area when their housing requirements change (i.e., aging in place), and would allow for greater diversity of housing options for those that wish to remain within the area they are familiar with. The mix of independent and assisted living would also allow for seniors to stay within the building if the level of care they require changes over time.

Sensitive Building Design: The use of a DC district will allow for the inclusion of rules specifically intended to ensure a sensitive interface with the adjacent low-density residential housing, and that any future development is well harmonized into the existing landscape. These rules include the use of stepbacks to transition the height downward towards the rear property line and sensitively interface with the community, and vertical articulation to minimize shadowing impacts to the extent possible, while also balancing other key objectives for the site including maximizing density and activating the commercial frontages.

Improved Pedestrian Connectivity: The site is adjacent to a regional pathway to the west that serves the community of Arbour Lake. Currently, there is no formal connection from the Arbour Lake community to the site. However, pedestrians have been informally using the site to access Crowfoot Crescent and destinations beyond. There is a clear "desire line" through the site, as evidenced by cuts made in the fence surrounding the property and pedestrian activity observed through the site throughout the day. This application presents an opportunity to provide a formalized well-designed pedestrian/cycling connection through the site, greatly enhancing the connectivity between Arbour Lake and Crowfoot. It would also improve those pedestrian connections to the LRT and bus service.

Amenities: The site is an ideal location for residential and commercial development due to its proximity to primary transit and to amenities. Within 700 metres of the site, the following amenities are provided: two grocery stores; three pharmacies; a recreation centre; a high school; a library; medical offices; restaurants; coffee shops; a movie theatre complex; a variety of retail stores; and personal services. The site is also located adjacent to a bus stop with direct

Applicants Submission

access to Routes 76 and 130. Within a ten minute walk (500 metres) of the site there is access to an additional seven bus routes, including Routes 40, 97, 113, 120, 134, 199 and 299.

Community Engagement: Prior to submitting a formal application, the landowner and applicant undertook engagement to learn about opportunities for the site and potential concerns of area users and interested residents. Meetings were held with the Arbour Lake Community Association and the Arbour Lake Residents' Association in August and September 2018. These meetings were positive in nature and comments were made about welcoming good and responsible development in the Crowfoot area. Members of these associations indicated their desire to be involved throughout the land use redesignation process, and suggested that an open house may be helpful to seek input from the wider community. As such, a public open house was held on October 10, 2018, prior to submitting a formal application, to introduce the notion of the application to the community and to gather early feedback. Furthermore, the applicant met with the Community Association on an ongoing basis for the duration of the project.

Approximately 35 people attended the open house. Comments received were generally positive regarding the notion of redeveloping the site. Attendees were generally very supportive of seniors' housing in this location and several attendees commented that Crowfoot has "everything but housing." There were also positive comments received about the idea of a pedestrian connection through the site.

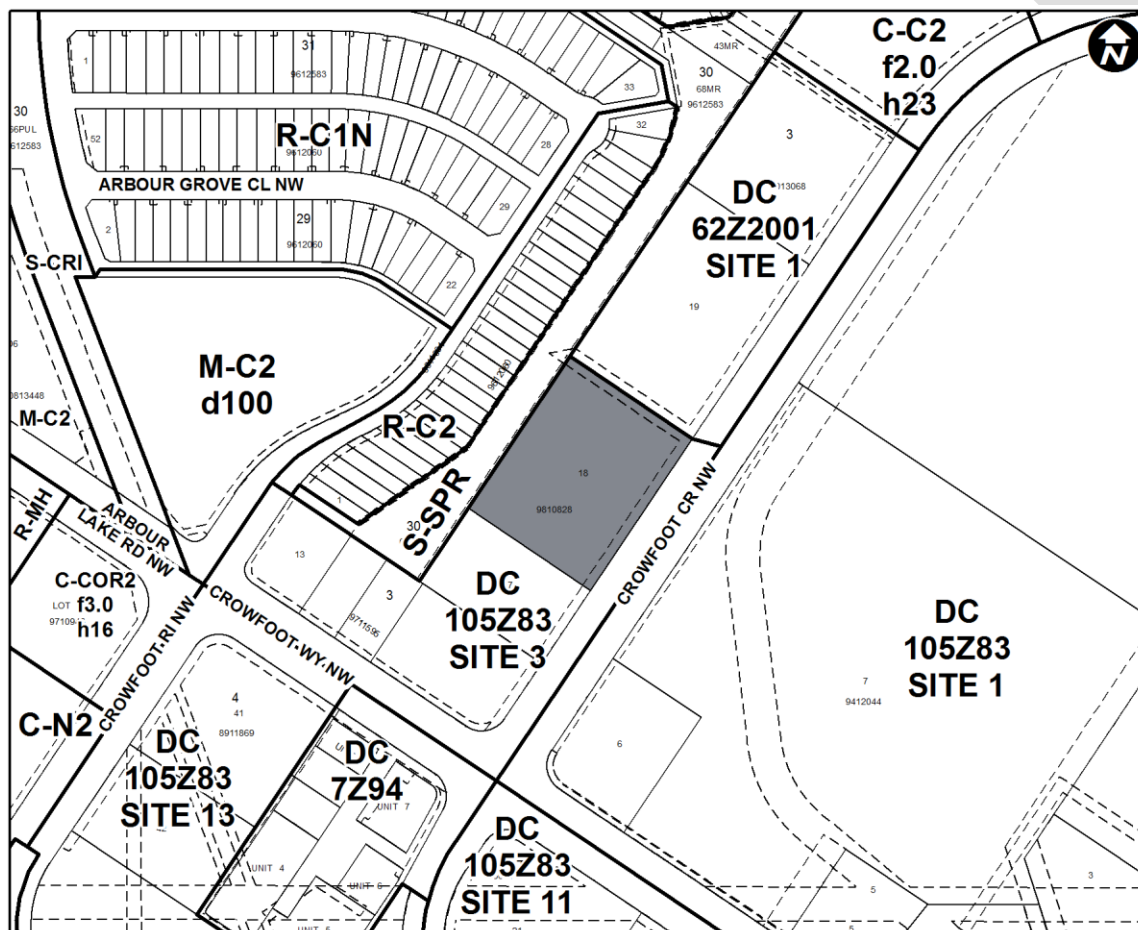
Attendees shared their concerns about the building height that is currently allowed as per the existing DC and about the potential loss or turnover of some of the businesses currently located on the site. There were also concerns expressed by some individuals about pedestrian safety and traffic along Crowfoot Crescent.

Summary: This application proposes to allow for a mixed-use building in a designated Major Activity Centre on a site located within 500 metres of an LRT station. The use of a DC district will allow for site-specific rules to be created to ensure a sensitive interface with the adjacent residential area. It will also allow for the development of seniors' housing on the site, providing a much needed housing option for our aging population. On behalf of the landowner / applicant, it is hoped that The City would welcome the opportunity and real interest to intensify and densify the Crowfoot site along the lines of the policies The City's planning department have been espousing in the past decade, and that this application for land use redesignation would be fully supported through the various stages of approval.

Proposed DC Direct Control District Guidelines

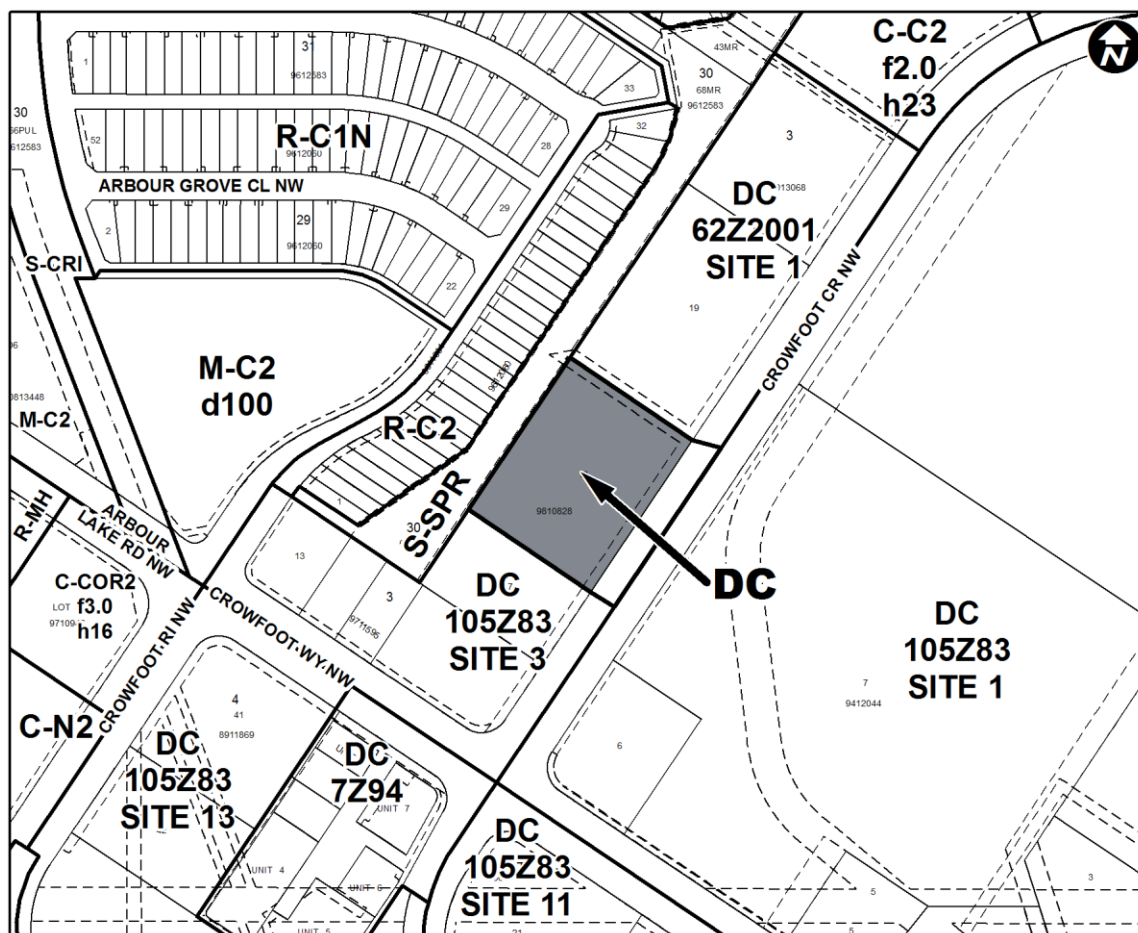
1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".

SCHEDULE A



Proposed DC Direct Control District Guidelines

SCHEDULE B



DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control District is intended to:

- (a) provide for mid-rise mixed-use **development**;
- (b) provide for commercial **uses** that contribute to an active **street** located on the ground floor of **buildings** along the **street**; and
- (c) create a built form where **building height** transitions from high to low from the **street** to the rear of a **parcel**.

Compliance with Bylaw 1P2007

2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Proposed DC Direct Control District Guidelines

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The **permitted uses** of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Discretionary Uses

- 5 The **discretionary uses** of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District.

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 apply in this Direct Control District.

Floor Area Ratio

- 7 The maximum **floor area ratio** is 4.5.

Building Height

- 8 (1) Unless otherwise referenced in subsection (2), the maximum **building height** is 46.0 metres.
- (2) The maximum **building height** within 6.0 metres of the **property line** shared with a **special purpose district** is 18.0 metres and increases proportionately to a maximum of 46.0 metres measured at **grade** at a distance of 25.0 metres from the **property line** shared with a **special purpose district**, as shown on Illustration 1.

Setback Area

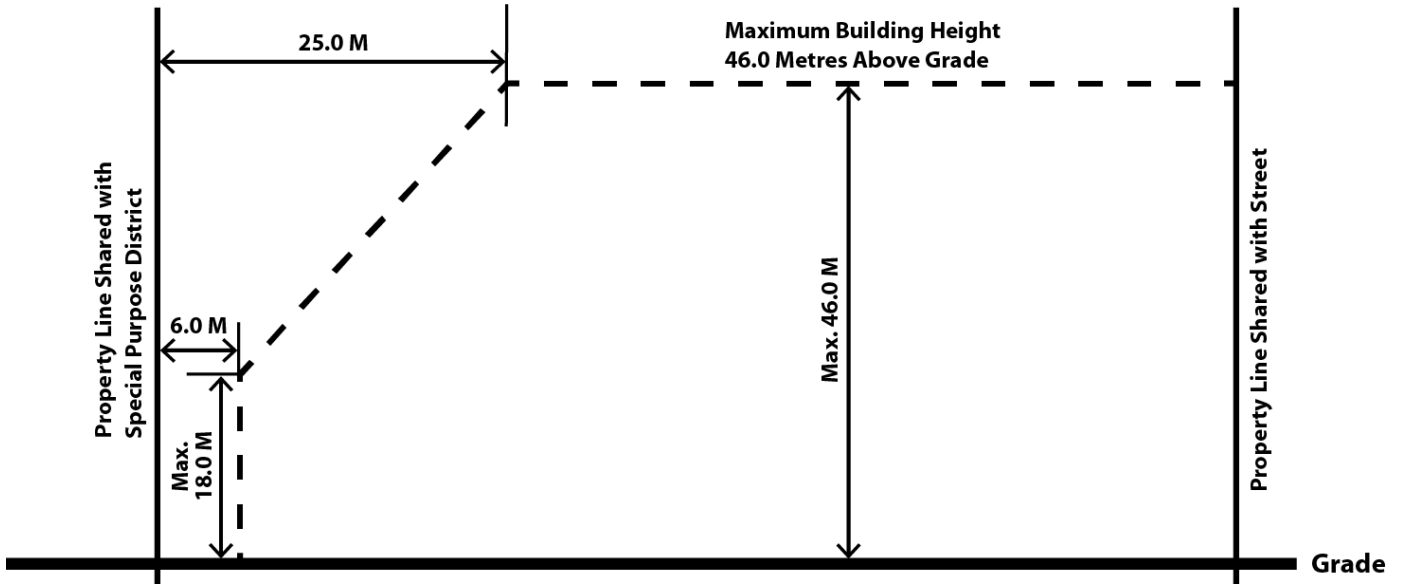
- 9 (1) Where a **parcel** shares a **property line** with a **parcel** designated as a **special purpose district**, the **rear setback area** must have a minimum depth of 6.0 metres.
- (2) For the **storey** closest to **grade**, the maximum **building setback** from a **property line** shared with a **street** is 22.0 metres for the length of the **building** façade that faces the **street**.

Vehicle Access

- 10 (1) Unless otherwise referenced in subsection (2), **motor vehicle parking stalls** and **loading stalls** must not be located between a **building** and a **street**.
- (2) **Motor vehicle parking stalls** may be located between a **building** and a **street** where there is not more than one row of **motor vehicle parking stalls** provided.

Proposed DC Direct Control District Guidelines

Illustration 1: Building Height in subsection 8(2).



TEXT FOR DISCUSSION ONLY

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2019 April 18

ISC: UNRESTRICTED
CPC2019-0468

Land Use Amendment in Residual Sub-Area 02L (Ward 2) at 15454 Symons Valley Road NW, LOC2018-0140

EXECUTIVE SUMMARY

This application has been submitted by Stantec Consulting on behalf of the landowners Cheryl Rowlandson, Wayne Rowlandson, Cheryl Elaine Rowlandson and Nancy Whatmore and was submitted on 2018 June 15. This application proposes to redesignate the subject property to Special Purpose – City and Regional Infrastructure (S-CRI) District and Special Purpose – Urban Nature (S-UN) District to allow for:

- a stormwater retention facility to service a related outline plan (LOC2017-0368); and
- to provide associated environmental reserve adjacent to Nose Creek.

The land use redesignation is being pursued in advance of the decision on the related outline plan in order to facilitate a time sensitive subdivision and transfer of the subject lands to the developer of the related outline plan, as the current land use district does not allow for a subdivision of parcel of land of this size.

Administration is prepared to support the proposed land use redesignation in this case as the proposed land use is an appropriate use of the parcel of land, the application will facilitate a necessary transfer of lands and the proposed use does not pose any significant risks. The proposal is in keeping with the intent and policies of the *Glacier Ridge Area Structure Plan*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT** by bylaw the proposed redesignation of 12.98 hectares \pm (32.09 acres \pm) located at 15454 Symons Valley Road NW (Portion of NW1/4 1-26-2-5) from Special Purpose – Future Urban Development (S-FUD) District **to** Special Purpose – City and Regional Infrastructure (S-CRI) District and Special Purpose – Urban Nature (S-UN) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

The subject lands were previously part of a larger outline plan and land use redesignation application (LOC2017-0368) that was submitted on 2017 December 06 by Stantec Consulting on behalf of the landowners. During the review of that application, Administration was informed that the subject lands were part of a conditional land sale that is intended to transfer the lands to

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Land Use Amendment in Residual Sub-Area 02L (Ward 2) at 15454 Symons Valley Road NW, LOC2018-0140

the developer of the larger outline plan area subject to LOC2017-0368, owned by Ronmor/Wenzel.

The outline plan included within LOC2017-0368 is an outline plan for 369.57 hectares \pm (913.20 acres \pm), which includes a significant portion of the central portions of the *Glacier Ridge Area Structure Plan (ASP)*, and includes all corresponding land use elements; park and school spaces; and supporting infrastructure requirements; to comprehensively plan and develop this portion of Glacier Ridge.

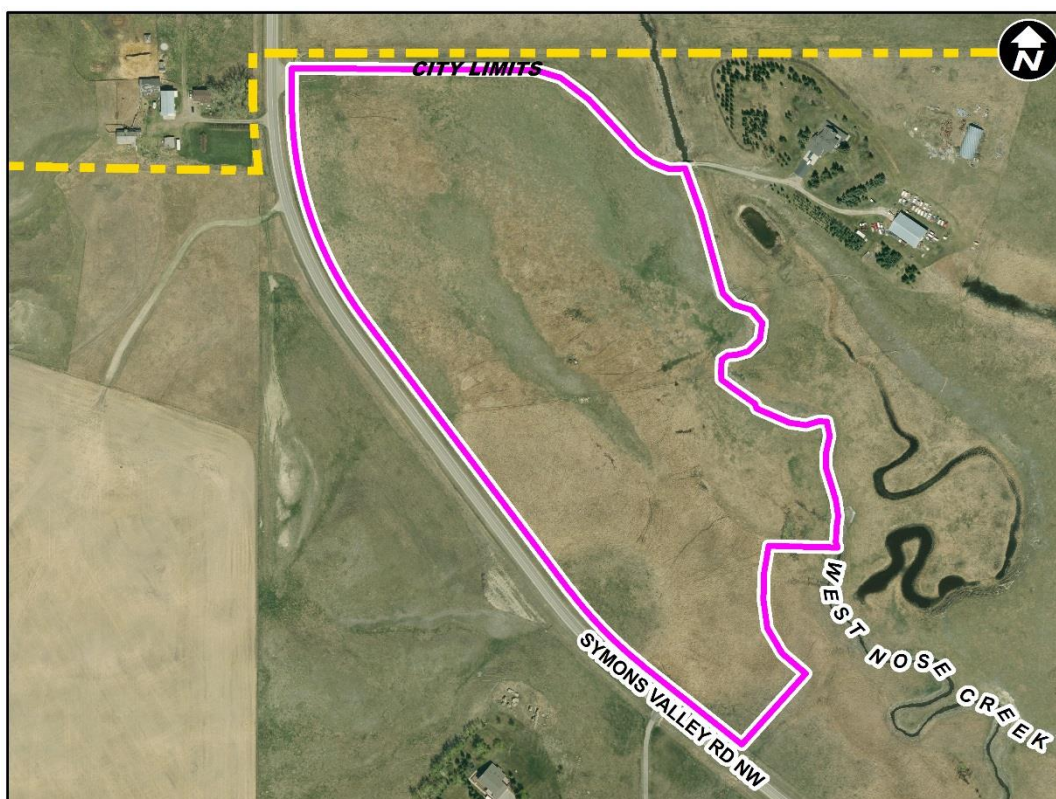
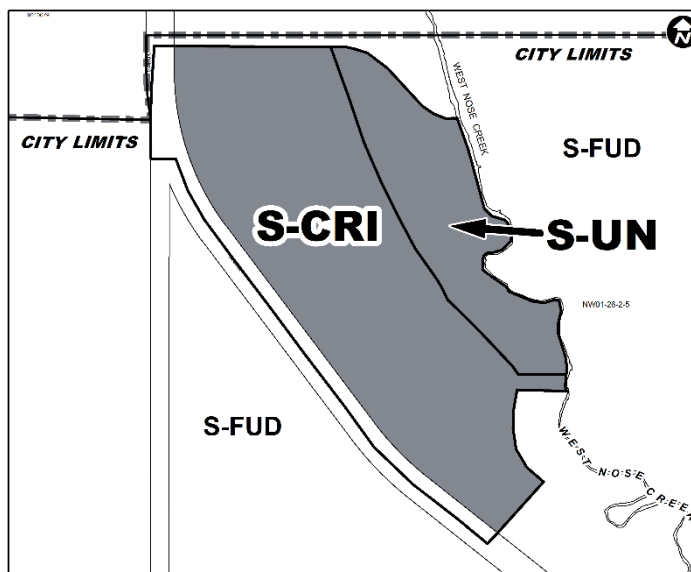
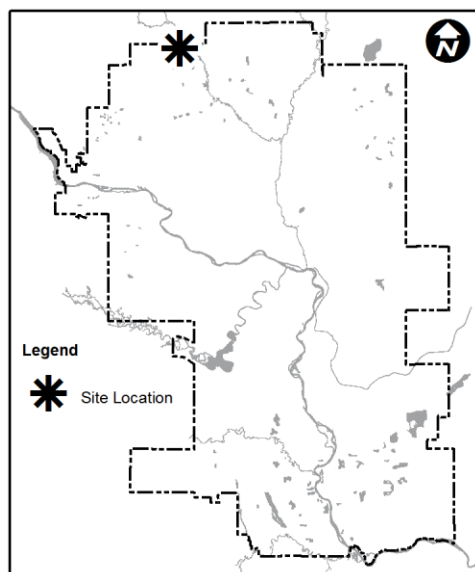
It was identified through the outline plan application review that a subdivision of the subject lands from the parent parcel would first require the approval of a land use redesignation, as the proposed subdivision would not meet parcel area requirements under the Special Purpose – Future Urban Development (S-FUD) District. Given the time constraints on the conditions of sale of the subject lands, the subject lands were accepted under this separate land use redesignation application in order to separate the timelines of the applications and allow for the timely subdivision and transfer of the subject lands prior to the approval of the larger outline plan application.

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Location Maps



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Land Use Amendment in Residual Sub-Area 02L (Ward 2) at 15454 Symons Valley Road NW, LOC2018-0140

Site Context

The subject site is located in the community of Residual Sub-Area 02L, and is situated between Nose Creek and Symons Valley Road. The site consists of a portion of a parcel of land that constitutes approximately 100 acres of land and is located to the northeast of Symons Valley Road NW. The subject site itself is approximately 12.98 hectares (32.09 acres). To the north and northwest of the site is Rocky View County and to the east and south is agricultural land.

The site itself is a generally flat and grassed area adjacent to Nose Creek, and contains a Class III wetland with intermittent streams. The site is bordered by Symons Valley Road NW to the west and south and Nose Creek to the east.

The site is located within the *Glacier Ridge Area Structure Plan* as identified on Attachment 2.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This land use application will facilitate the construction of a future stormwater facility to service an adjacent development, as well as allowing for the subdivision of the subject site from the parent parcel of land.

It is unusual to be proceeding with a land use redesignation for a stormwater pond associated with an unapproved outline plan. The planning considerations below, highlight the specific circumstances and conditions which ultimately led to Administration's recommendation.

Planning Considerations

Land Use

The subject site is currently designated Special Purpose – Future Urban Development (S-FUD) District. The intent of this district is to protect lands for future urban forms of development by restricting premature subdivision and development. The district has a minimum parcel size of 64.0 hectares and does not allow for the subdivision of the subject parcel from the existing "parent" parcel. Therefore, the existing land use district does not allow for the necessary transfer of land to occur.

The proposed land use for the western portion of the subject site is Special Purpose – City and Regional Infrastructure (S-CRI) District. The purpose of this district is to provide for a variety of infrastructure and utility facilities, maintenance facilities and uses operated by various levels of government. In this case, the intention of the proposed land use is to accommodate a stormwater management facility.

The proposed land use for the eastern portion of the subject site, those lands directly adjacent to Nose Creek is Special Purpose – Urban Nature (S-UN) District. This district is intended to be applied to lands that have been dedicated as environmental reserve. This portion of the subject site falls within the 30m setback from Nose Creek and as such are classified as environmental reserve.

Land Use Amendment in Residual Sub-Area 02L (Ward 2) at 15454 Symons Valley Road NW, LOC2018-0140

These proposed land uses will allow for the future provision of a stormwater facility, protection of the natural areas through the dedication of environmental reserve, as well as allow for the subdivision of the subject lands in order to facilitate a land purchase for the purposes of servicing the future adjacent development. Although that outline plan remains unapproved at this time, Administration notes that the proposed land uses are consistent with the servicing review being contemplated and an appropriate and necessary use of the land, regardless of the final layout of the outline plan.

Development and Site Design

The proposed land use will facilitate a stormwater pond to service a nearby proposed development. The S-CRI portion of the lands will be comprised entirely of a stormwater retention facility and associated infrastructure, while the S-UN portion of land will be left in a natural state and dedicated as Environmental Reserve (ER).

Environmental

An Ecological Inventory was completed as part of *Glacier Ridge Area Structure Plan* (2015) which identified areas for future study within the subject lands at the time of an outline plan application. As required, a subsequent Biophysical Impact Assessment (BIA) was submitted for the LOC2017-0368 outline plan area. Currently, the BIA for LOC2017-0368 is not approved, but has undergone two comprehensive reviews by Parks Urban Conservation, with only minor amendments remaining prior to formal approval of the report. The conclusions and terms identified within the BIA, as well as compliance with provincial and municipal policies for protection of water bodies and wildlife have been well established for this application. This understanding has supported Administration for the advancement of this separate land use application.

Transportation

The site is bound by the existing Symons Valley Road NW to the south-and-west, and by the City limits to the north.

The *Glacier Ridge ASP* identifies changes to the regional (arterial) road network in the area. The changes include an extension of Shaganappi Trail to the north which will facilitate a new intersection with Township Road 261A in Rockyview County; and a future bridge replacement of the existing crossing of West Nose Creek along Symons Valley Road. Symons Valley Road will extend north of the future crossing and tie in with Mountain View Road (known as Range Road 20 further north in Rockyview County).

A closure of Symons Valley Road between Mountain View Road and Shaganappi Trail NW will also be implemented. The closed section of Symons Valley Road NW will be replaced by a Regional Pathway as indicated in Section 4 of the *Glacier Ridge ASP*. Symons Valley Road NW will need to remain open until such time that the extension of Shaganappi Trail to Township Road 261A is complete.

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These road network updates are per the *Glacier Ridge ASP* which facilitate a second north-south connection with Rockyview County relative to the one which exists currently.

As the ultimate network is implemented, the south-and-west boundary conditions of the proposed pond will be modified from interfacing an existing road (Symons Valley Road NW), to interface a Regional Pathway per the ASP. A portion of the site will be bound by the future Shaganappi Trail extension.

The applicant provided cross-sections to illustrate the interface of the pond with the future Shaganappi Trail NW extension. The Transportation Impact Assessment provided in support of the larger outline plan application included analysis of the intersection of Shaganappi Trail NW with Township Road 261A, and the applicant provided concepts illustrating potential road alignments adjacent the pond and designs for the intersection of Shaganappi Trail NW and Township Road 261A which comply with the ASP and with the required transportation standards.

There is an existing access from Symons Valley Road NW to a homestead on the Rowlandson property. This access will be maintained, and formalized via an easement at the time of subdivision.

Utilities and Servicing

No services are required to service the proposed development. The site is intended to be utilized for stormwater management purposes to service the greater Glacier Ridge development area. By securing this site for stormwater management purposes, the number of storm ponds required to service the full catchment area will be reduced. This will lead to an overall cost savings from both a capital and operating perspective.

Stakeholder Engagement, Research and Communication

In keeping with Administration's practices, this application was circulated to stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online. No comments were received by CPC report submission date. No community association exists in the area.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)* which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

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Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendments builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The site is identified as belonging to the “Future Greenfield” land use typology as identified in Map 1 of the *Municipal Development Plan*. The *Municipal Development Plan* policies for Future Greenfield Areas stipulate that the plans for these areas will be established through Area Structure Plans and outline policies for those Area Structure Plans. The *Glacier Ridge Area Structure Plan*, discussed below, is the relevant Area Structure Plan for the subject site.

Glacier Ridge Area Structure Plan (Statutory – 2015)

The subject site falls within the Environmental Open Space Study Area of the *Glacier Ridge Area Structure Plan*. The Environmental Open Space Study Area identifies areas that may be environmentally significant and may require further study at the outline plan or land use stages. As detailed in the Environmental section above, these studies have been completed and provide more detailed analysis of the environmental significance of the subject lands. The studies identified that the 30 metre setback from Nose Creek is classified as Environmental Reserve, which is reflected in the proposed land use, Special Purpose – Urban Nature (S-UN) District.

Environmental studies identified the portion of the site proposed to contain a stormwater facility as containing a Class III wetland. While it is Administration’s desire to protect wetlands wherever possible, in this situation, the needs of wetland protection had to be balanced against the need to provide stormwater retention for the development, and to provide a consolidated stormwater facility. The proposed location of the stormwater facility was deemed to be preferable to other alternatives for a variety of reasons, including topographical considerations (the proposed pond is located below the proposed development, reducing the need for pumping of stormwater), capital and operating considerations for one consolidated storm pond as opposed to several smaller storm ponds, as well as the effect of several smaller storm ponds would have on the proposed community with regards to the contiguity, functionality and grading challenges.

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Social, Environmental, Economic (External)

Financial Capacity

Current and Future Operating Budget

The proposed land use redesignation will facilitate the construction of a large, consolidated stormwater facility to serve the adjacent community. Providing a consolidated pond will eliminate the need for multiple ponds within the plan area, reducing ongoing maintenance costs.

Current and Future Capital Budget

The proposed land use redesignation will facilitate the construction of a large, consolidated stormwater facility to serve the adjacent community. Providing a consolidated pond will reduce the City-funded capital requirements for the storm trunk from \$3.9M to \$0.5M, a savings of \$3.4M.

Risk Assessment

The applicant has been informed that approval of the subject land use redesignation does not imply the approval of the associated Staged Master Drainage Plan or of the related outline plan and land use and outline plan application (LOC2017-0368). Furthermore, as the subject site is constrained on all sides either by a road, a meander belt for Nose Creek or a jurisdictional boundary (Rocky View County), there is little risk of a change to the proposed land use boundaries in the future to accommodate a larger stormwater facility. Any additional stormwater management measures that may be required to service the related outline plan area will be managed through measures other than an expansion of the area of the pond. These measures may include increased storage depth, an increased release rate to Nose Creek, or use of stormwater for irrigation purposes.

Approval of the subject application does not carry significant risk of approving uses that would be inappropriate on the site, nor does the proposed land use constrain the ability of Administration to review the related outline plan in a fulsome manner.

REASON(S) FOR RECOMMENDATION(S):

The proposed land use redesignation will facilitate the transfer of land to the developer of the adjacent outline plan, providing needed stormwater facilities for the associated outline plan development. The proposed land use is an appropriate use for the parcel and does not pose and significant risks to the City of Calgary.

ATTACHMENT(S)

1. Applicant's submission
2. Location Plan

Applicant's Submission

Stantec Consulting Ltd. ('Stantec') is pleased to submit the attached Land Use Redesignation (LUR) application for Glacier Ridge on behalf of Ronmor Developers Inc. ('Ronmor'). The LUR application pertains to the lands located within the Glacier Ridge Area Structure Plan (ASP), civically addressed as 15454 Symons Valley Road NW, and legally described as:

MERIDIAN 5 RANGE 2 TOWNSHIP 26
SECTION 1

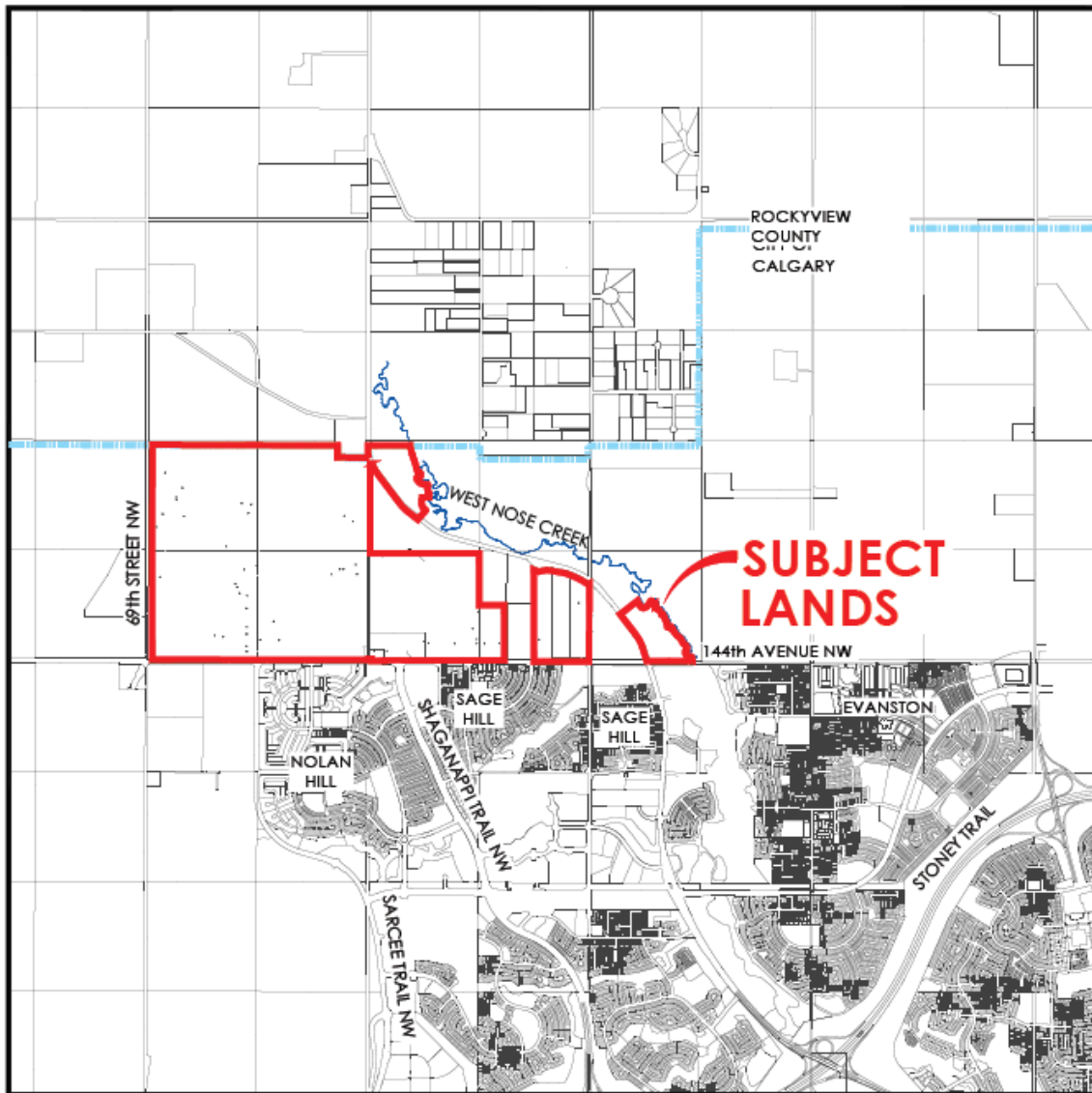
THAT PORTION OF THE NORTH WEST QUARTER
WHICH LIES TO THE NORTH EAST OF THE ROADWAY ON PLAN 1060Q
CONTAINING 43.1 HECTARES (106.6) ACRES MORE OR LESS
EXCEPTING THEREOUT THE ROAD WIDENING ON PLAN 5447JK
CONTAINING .733 HECTARES (1.81 ACRES) MORE OR LESS.

The following LUR is being proposed as part of this application: S-FUD to S-CRI and S-UN.

15454 Symons Valley Road NW is currently included within LOC2017-0368 (the Glacier Ridge Outline Plan and Land Use Redesignation application). However, as part of this submission we are requesting that the subject lands be removed from the LOC2017-0368 application, and be reviewed independently. The proposed 9.90 hectare (24.47 acre) S-CRI district will accommodate a storm pond as part of initial phases of the Glacier Ridge Outline Plan. This storm pond location is adjacent to the Symons Valley corridor, and is in alignment with the approved Glacier Ridge Master Drainage Plan (MDP). There is also 2.95 hectares (7.30 acres) of proposed Environmental Reserve (ER) within the S-UN parcel.

Allowing the storm pond site to proceed to a Land Use approval independently will facilitate the subdivision and purchase of these lands by the pre-determined July closing date. Development of this pond will reduce the City-funded capital requirements for the storm trunk to approximately \$0.5M from \$3.9M. Not only does this option result in a \$3.4M capital infrastructure savings to the City, it also reduces City maintenance costs (1 pond vs. 3).

LOCATION PLAN



Location Plan

Portions of Sec 1 & 2, Twp 26, Rge2, W5, & Sec 6, Twp26, Rge 1, W5
Located within the Glacier Ridge ASP

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**Land Use Amendment in Saddle Ridge (Ward 5) at 9320 and 9325 - 52 Street NE,
LOC2018-0170**

EXECUTIVE SUMMARY

This application was submitted by B&A Planning Group on 2018 July 25 on behalf of the landowner Genstar Titleco Limited. The land use amendment proposes the redesignation of 22.00 hectares \pm (54.36 acres \pm) in the Savanna neighbourhood of the northeast community of Saddle Ridge. This proposal is intended to:

- provide more flexible land use districts to accommodate a range of lower density ground-oriented housing forms to address the housing needs in this sector of the city and to respond to existing market conditions;
- provide an updated outline plan to account for a revised street, infrastructure, and subdivision development pattern; and
- respond to Council's motion arising regarding the redesignation of the Light Rail Transit (LRT) Park and Ride lands.

This application is accompanied by an outline plan application (CPC2019-0318) that contemplates minor revisions to the previously approved subdivision layout for this portion of the Savanna neighbourhood.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 20.49 hectares \pm (50.63 acres \pm) located at 9320 and 9325 – 52 Street NE (Portion of NW1/4 Section 14-25-29-4; Plan 1412743; Block 1, Lot 1) from Residential – Narrow Parcel One Dwelling (R-1N) District, Residential – One Dwelling (R-1s) District, Residential – One / Two Dwelling (R-2) District, Residential – Low Density Multiple Dwelling (R-2M) District, Multi-Residential – Low Profile (M-1d62) District, Multi-Residential – Medium Profile Support Commercial (M-X2d185) District, and Multi-Residential – Medium Profile (M-2d124) District **to** Residential – Low Density Mixed Housing (R-G, R-Gm) District, Multi-Residential – Medium Profile (M-2) District and DC Direct Control District to accommodate lower density ground-oriented residential uses with guidelines (Attachment 2).
2. Give three readings to the proposed bylaw;
3. **ADOPT**, by bylaw, the proposed redesignation of 1.51 hectares \pm (3.74 acres \pm) located at 9320 – 52 Street NE (Portion of NW1/4 Section 14-25-29-4) from Special Purpose – City and Regional Infrastructure (S-CRI) District and Multi-Residential – Medium Profile (M-2d124) District **to** DC Direct Control District to accommodate transit infrastructure and future mixed-use development with guidelines (Attachment 3); and
4. Give three readings to the proposed bylaw.

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PREVIOUS COUNCIL DIRECTION / POLICY

At the time of the original land use approval for this area (LOC2014-0208) at the Combined Meeting of Council held on 2015 November 09, the following direction was provided by Council with respect to the future LRT Park and Ride site:

MOTION ARISING, Moved by Councillor Carra, Seconded by Councillor Stevenson, that with respect to CPC2015-191, Council direct Administration to prepare and bring forward for Council's consideration, a City initiated land use redesignation for the S-CRI site which in addition to a transit hub and park and ride facility, will allow for the future redevelopment of a portion of the site north of the anticipated transit hub with a mix of uses and a public plaza area and potentially affordable housing.

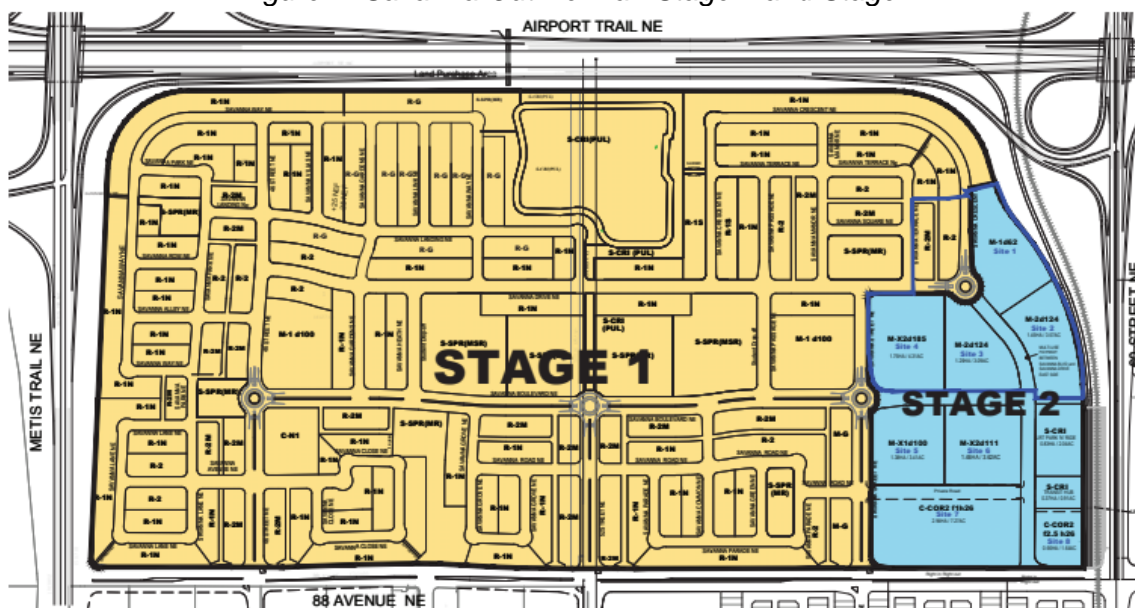
BACKGROUND

This application was submitted to The City on behalf of the owners, Genstar Titleco Limited, by B&A Planning Group on 2018 July 25.

The Savanna neighbourhood received previous approvals through two stages of outline plan and land use redesignation applications, and associated policy amendments to the *Saddle Ridge Area Structure Plan (ASP)*. Stage 1 (LOC2013-0036) represents much of the Savanna neighbourhood, and Stage 2 (LOC2014-0208) represents the lands within the vicinity of the future LRT station.

The subject application proposes changes to the approved land uses within Stages 1 and 2 as shown in Figure 1.

Figure 1: Savanna Outline Plan Stage 1 and Stage 2

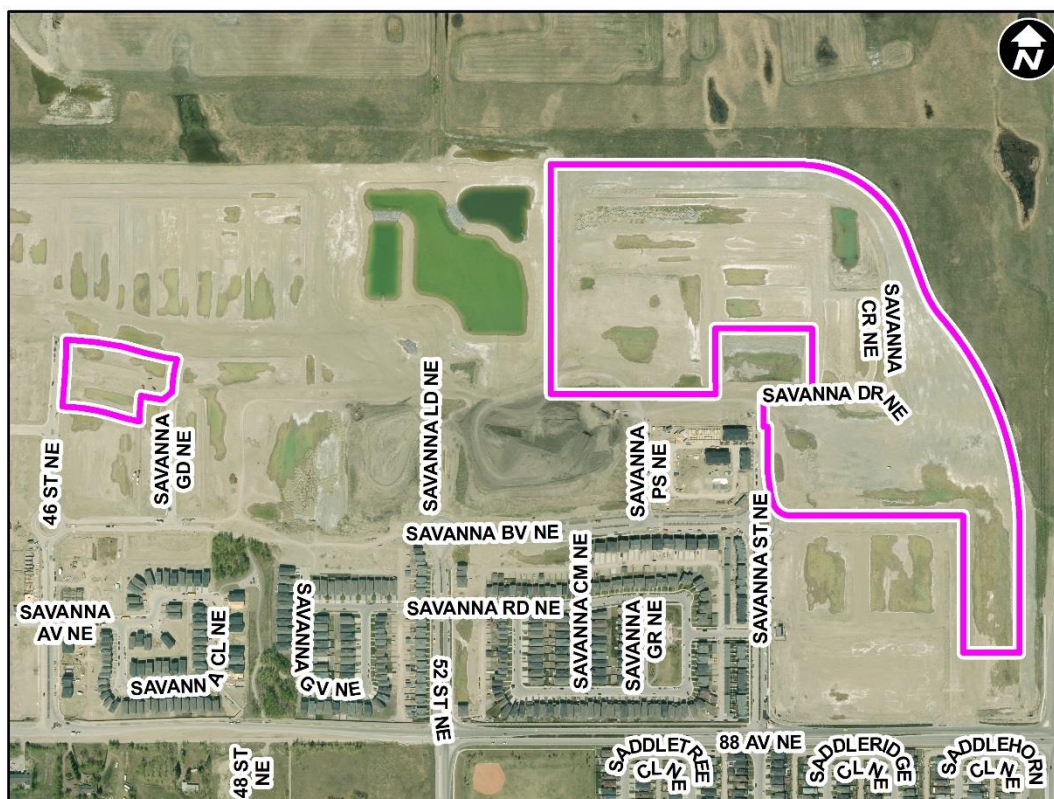
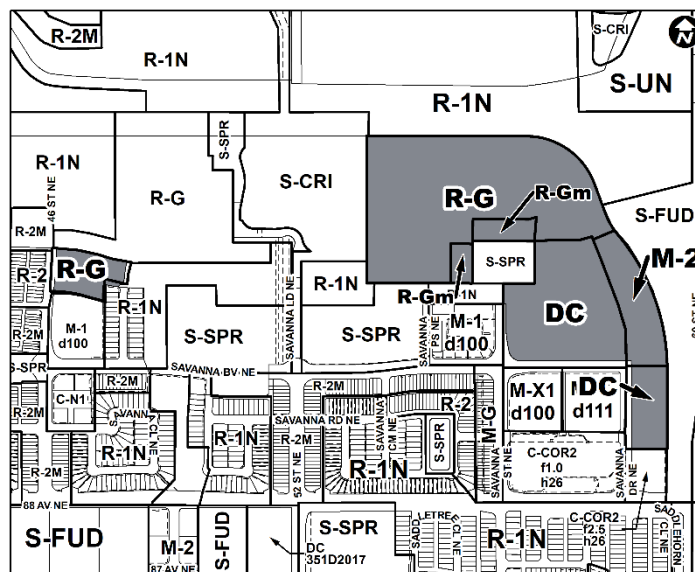
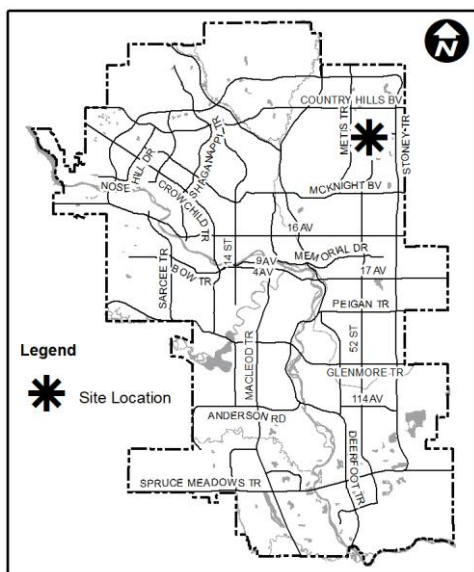


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Location Maps



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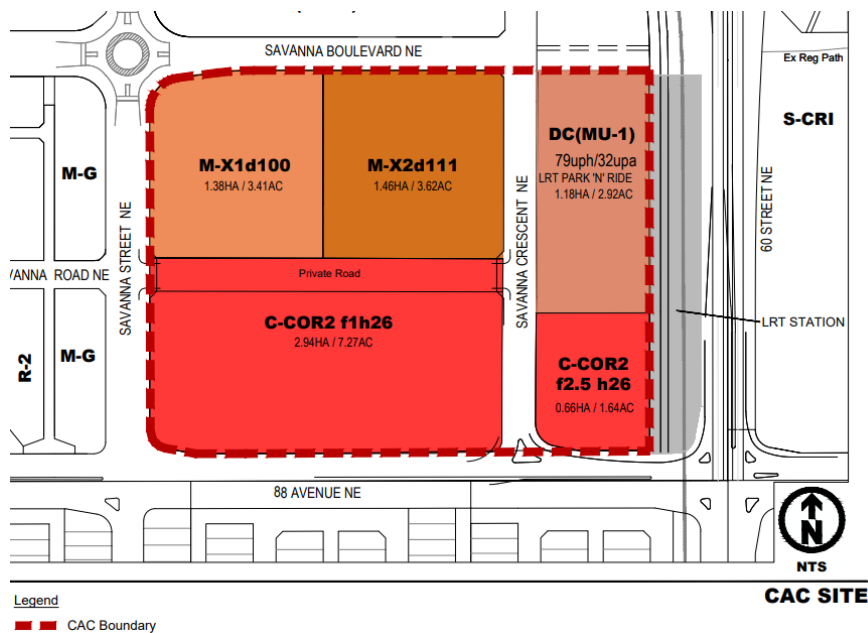
Site Context

The subject lands comprise approximately 22.00 hectares (54.36 acres) in the Savanna neighbourhood of the community of Saddle Ridge and are currently undeveloped. Stripping and grading of the lands has been completed. The Savanna neighbourhood is bounded to the north by Airport Trail, to the east by 60 Street NE, to the south by 88 Avenue NE, and to the west by Metis Trail NE.

The neighbourhood contains a broad mix of residential land uses and park spaces, with approximately 400 dwelling units constructed and occupied to date within the neighborhood. Future multi-residential land uses, and commercial land uses are located within the Community Activity Centre (CAC) located in the southeast corner of the neighbourhood, and a Neighbourhood Activity Centre (NAC) accommodating higher density residential uses and local-scale commercial uses the western portion of the neighbourhood.

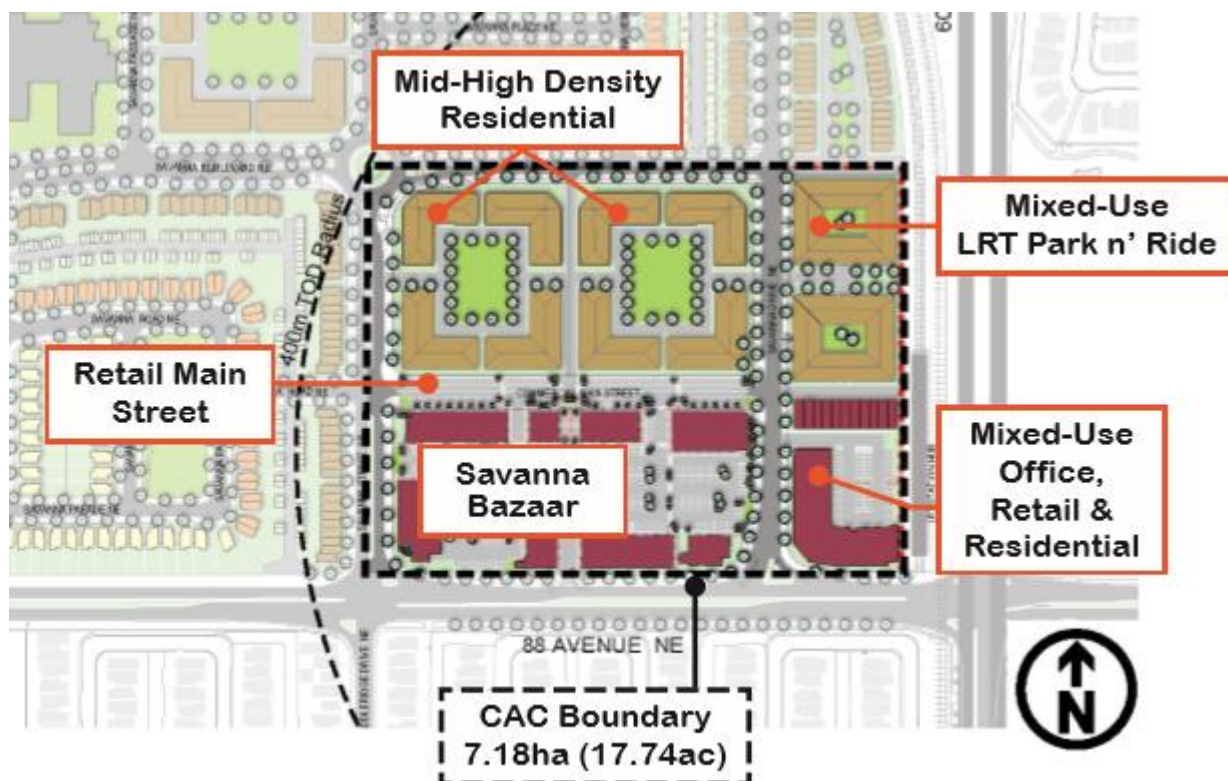
This application is subject to the policies of the *Municipal Development Plan* and the *Saddle Ridge ASP* and meets all policy requirements as detailed in the Strategic Alignment section of this report. The proposed DC Direct Control District for the future LRT Park and Ride site falls within a Community Activity Centre (CAC), as shown in Figures 2 and 3 below.

Figure 2. Savanna Community Activity Centre Land Use Districts



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Figure 3. Savanna Community Activity Centre Development Concept



Approximately 12.53 hectares (30.96 acres) of the subject lands fall within a Transit Station Planning Area, which consists of a 600-metre radius around the future LRT station, as shown in Figure 4 below. Transit Oriented Policy Guidelines apply to these lands and all applicable policies are met.

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Figure 4. Transit Station Planning Area Development Concept



The CAC is identified by the *Saddle Ridge ASP* to be located northwest of the intersection of 88 Avenue NE and 60 Street NE. The CAC will host a future transit hub, LRT station for the extension of the Blue Line, and transit connection to the Calgary International Airport. The CAC is designed to promote a pedestrian and cycle-friendly environment and a mix of uses including medium-high density multi-residential development, mixed-use development, and community-scale commercial development. The 'Savanna Bazaar' is currently in concept development and

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includes a community scale food store and a variety of small and medium format retail and office uses lining a central commercial main street, which leads from the western portions of the neighborhood to the future LRT station. The remainder of the CAC is undeveloped currently.

The LRT Park and Ride site is directly adjacent to a future LRT station to be located near 60 Street NE and 88 Avenue NE. The Park and Ride is approximately 1.51 hectares (3.74 acres) in size and is intended to accommodate a bus transit hub, as well as an approximately 200 stall surface parking lot as an interim condition, and ultimately a parking structure to accommodate approximately 600 parking stalls. Per Council's Motion Arising, the parking structure is intended to include a mix of uses and incorporate a public plaza.

Figure 5 provides population statistics for the community of Saddle Ridge.

Figure 5: Community Peak Population

Saddle Ridge	
Peak Population Year	2018
Peak Population	21,567
2018 Current Population	21,567
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [Saddle Ridge](#) community profile.

The applicant has indicated that there is a significant demand for multi-generational households located in this sector of the city. Such households may have a higher ratio of occupants per dwelling unit as compared to the city as a whole. The proposed land use districts can accommodate a wide range of home sizes, and the applicant has stated an intention to develop the subject lands in the R-G, R-Gm, and DC/R-Gm Districts with very large homes capable of accommodating multi-generational families. Lot sizes will be established through subdivision and unit sizes will be determined at the development permit stage.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This land use amendment application, along with the associated outline plan application (Attachment 4), generally aims to accomplish two things at a land use district level. Firstly, it proposes to reduce large blocks of future multi-family development and provide for greater variety of ground-oriented residential land uses to facilitate development in alignment with the applicant's assessment of current market conditions in this sector of the city. Second, it includes a proposed DC Direct Control District to respond to Council's motion arising related to the LRT park and ride site. Although the motion arising directed Administration to bring forward a City-initiated land use redesignation, this application represented an opportunity to accomplish the intent of the direction in collaboration with the applicant.

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These applications have been evaluated in the context of applicable policies, including the *Municipal Development Plan* and *Saddle Ridge ASP*. This application responds to the applicant's assessment of current market conditions, including a strong preference for lower-density, ground-oriented residential development forms, as well as larger dwelling units that can accommodate multi-generational families. Administration requested market study information to be provided for supporting justification of the proposal; however, the Applicant did not provide this.

Planning Considerations

The proposed changes to the approved outline plan and land use districts were assessed to ensure continuing adherence to the minimum intensity targets of the *Municipal Development Plan* and *Saddle Ridge ASP* for the overall outline plan area, as well as enhanced intensities within the Community Activity Centre and Transit Station Planning Area. The DC Direct Control District for the LRT Park and Ride site is intended to facilitate a vibrant, mixed-use, pedestrian-oriented environment in support of future transit facilities and in alignment with applicable policies.

Land Use

Existing land use designations are summarized in Figure 6 below and include higher-density multi-residential districts in proximity to the future LRT station, and a range of lower density residential districts. Existing land use districts would provide approximately 951 to 1,120 dwelling units and minimum population of 2,040 people, while the proposed districts will facilitate an anticipated 604 dwelling units and population of approximately 1,607 people.

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Figure 6: Existing Land Use Districts Analysis

Existing District		Area	# Units (min-max)	Possible Housing Types
R-1s	Residential – One Dwelling	0.53 ha± (1.31 ac±)	29 - 32	Single detached dwellings.
R-1N	Residential – Narrow Parcel One Dwelling	5.15 ha± (12.73 ac±)	164 - 199	Single detached dwellings.
R-2	Residential – One / Two Dwelling	1.32 ha± (3.26 ac±)	53 – 60	Semi-detached and single detached dwellings.
R-2M	Residential – Low Density Multiple Dwelling	1.31 ha± (3.24 ac±)	71 - 78	Rowhouse, townhouse, duplex, semi-detached, and single detached dwellings.
M-1	Multi-Residential Low Profile	1.53 ha± (3.78 ac±)	76 – 94	Multi-residential development.
M-2	Multi-Residential – Medium Profile	2.7 ha± (6.67 ac±)	235 - 334	Multi-residential development.
M-X2	Multi-Residential – Medium Profile Support Commercial	1.75 ha± (4.35 ac±)	105 - 323	Multi-residential development.

Figure 7: Proposed Land Use Districts Analysis

Proposed District		Area	# Units	Possible Housing Types
R-G	Residential – Low Density Mixed Housing	8.61 ha± (21.27 ac±)	259	Cottage housing clusters, duplex, rowhouse, semi-detached and single detached dwellings.
R-Gm	Residential – Low Density Mixed Housing	0.61 ha± (1.51 ac±)	35	Cottage housing clusters, duplex, rowhouse, semidetached and single detached dwellings.
DC (R-Gm)	Residential – Low Density Mixed Housing	3.52 ha± (8.70 ac±)	206	Cottage housing clusters, duplex, rowhouse, semidetached and single detached dwellings.
M-2	Multi-Residential – Medium Profile	1.74 ha± (4.30 ac±)	104	Multi-residential development.
DC (MU-1)	Mixed Use – General	1.51 ha± (3.74 ac±)	TBD	Multi-residential development (future residential units possible, but not accounted for at this time).

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The Residential – Low Density Mixed Housing (R-G) District is intended to encourage a wide diversity of low density development forms and allow for intensification of a neighbourhood over time. Parcels designated as R-Gm District are not intended to accommodate single detached dwellings except where subdivision results in remnant single lots, where carriage house lots are added, or where single detached dwellings are planned comprehensively with a majority of attached dwelling forms.

The DC Direct Control District based on the Residential – Low Density Mixed Housing (R-Gm) District is intended to allow for low density housing in alignment with current market conditions, while limiting the amount of single detached dwellings and semi-detached dwellings. The Multi-Residential – Medium Profile (M-2) District allows multi-residential development in a variety of forms with medium height and density and is intended to be developed with townhouse-style development.

The proposed DC Direct Control District for the LRT Park and Ride site is based on the Mixed-Use – General (MU-1) District and is intended to provide for park and ride facilities, as well as enabling the future development of the site with a range of active uses in support of the transit facilities. The DC Direct Control District stipulates that development will require either surface parking facilities, or a parking structure, to accommodate future transit Park and Ride needs. The district guidelines were created in consultation with Transit Infrastructure and a design consultant based on preliminary concepts for development of the site. As development forms are uncertain and development of this site is not anticipated to proceed for many years, no units have been included in the intensity calculations.

Density

Applicable policies set targets for both density (units per hectare) and intensity (people and jobs per hectare).

The proposed land use redesignations will result in lower intensity development than the approved 2013/2014 outline plans and land use districts. Approved land use districts would require a minimum intensity of 102 people and jobs per hectare, and the proposed land use districts will achieve an anticipated intensity of 81 people and jobs per hectare. Intensity rates are calculated based on a standard rate of 3.3 people per dwelling unit; however, census data indicates that within the general area density of approximately 3.9 people per dwelling unit is typical and therefore higher intensities may be realized.

Section 4.2 of the *Saddle Ridge ASP* requires a minimum average residential density of 17.3 units per gross developable hectare. The existing approved land use districts would achieve a density of approximately 45 to 53 units per hectare, and the proposed land use districts will achieve a density of approximately 29 to 36 units per hectare. This represents an anticipated reduction of approximately 16 to 17 units per hectare, while still exceeding the minimum average residential density as outlined in the ASP.

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The *New Communities Planning Guidebook (Municipal Development Plan Volume 2)* requires a minimum intensity of 100 people and jobs per hectare within the Transit Station Planning Area, which constitutes a 600-metre radius around the future LRT station. Existing land use districts would facilitate an intensity of 106 people and jobs per hectare, and the proposed land use redesignations will facilitate an intensity of 100 people and jobs per hectare within the 600 metre radius

The *New Communities Planning Guidebook (Municipal Development Plan Volume 2)* requires that each Community Activity Centre shall achieve a minimum intensity of 150 people and jobs per gross developable hectare. This intensity is achieved by existing land use districts and will not be impacted by the proposed redesignations. No people or jobs for the future Park and Ride DC/MU-1 site are included in intensity calculations due to the long-term timeline for potential development. If the maximum intensity of the site is achieved an intensity of 269 people and jobs per gross developable hectare will be realized in the Community Activity Centre. The approved and proposed densities and intensities are compared against policy requirements in Figure 8 below.

Figure 8. Density and Intensity Requirements

	Policy	Statutory Requirement	Previously Approved (LOC2013-0036 and LOC2014-0208)	Proposed
Residential Density	Saddle Ridge ASP	17.3 units per gross developable hectare	45 – 53 units per gross developable hectare	29 – 36 units per gross developable hectare
Transit Station Planning Area Intensity	New Communities Planning Guidebook	100 people and jobs per gross developable hectare	106 people and jobs per gross developable hectare	100 people and jobs per gross developable hectare
Community Activity Centre Intensity	New Communities Planning Guidebook	150 people and jobs per gross developable hectare	206 people and jobs per gross developable hectare	206 people and jobs per gross developable hectare

Subdivision Design

The proposed outline plan (Attachment 3) includes minor reconfiguration of the approved street network which was designed to accommodate large blocks of multi-residential development, to provide a more fine-grained street network that will support lower density ground-oriented residential uses. Rear lanes for lower density uses will support a safe and attractive pedestrian realm, and a roundabout has been removed to facilitate more efficient use of land. No other

**Land Use Amendment in Saddle Ridge (Ward 5) at 9320 and 9325 - 52 Street NE,
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major changes to approved infrastructure are contemplated. No Municipal Reserve or Environmental Reserve lands are included within the outline plan area with no changes being contemplated.

Environmental

Environmental concerns for the subject lands were considered during the original Savanna outline plan approved in 2014, and no further reports or investigations were required for this application.

Transportation

The associated outline plan (Attachment 4) proposes minor changes to the approved street network, including removal of a roundabout based on minor re-alignment of the collector network, and imposition of a more fine-grained street network to facilitate lower density ground-oriented residential development. Transportation is in support of the proposed changes to the street network.

Local transit service for the Savanna neighbourhood is currently provided by bus Route 59. Future LRT service to the area is planned through an extension of the Blue Line; however, this extension is unfunded, and no timeline has been established for construction. A transit hub, Park and Ride site, and future transit connection to the Calgary International Airport are intended to be co-located adjacent to the future LRT station. Administration is currently conducting preliminary design work for this infrastructure.

Utilities and Servicing

Water and sanitary servicing plans to service this area were established through the Savanna outline plan approval in 2014. The existing servicing plans are sufficient to accommodate the proposed land use changes.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

No submissions were received from the public.

The Saddle Ridge Community Association was circulated on this application and provided a letter of support (Attachment 5). The Association favours larger homes with more parking to suit the needs of multi-generational families, and more rapid build-out of the area to support local commercial uses.

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Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Internal Business Units identified as stakeholders for the future LRT Park and Ride site were engaged regarding the proposed redesignation. Real Estate and Development Services and Transit Infrastructure were the primary stakeholders, with Calgary Housing as a secondary stakeholder.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The site is located within the City, Town area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to the site, the proposal is consistent with policies on Land Use Patterns (Section 8.14).

Interim Growth Plan (2018)

The proposed outline plan and land use amendment aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory - 2009)

Map 1: Urban Structure of the *Municipal Development Plan* (MDP) identifies the subject lands as a Planned Greenfield Area subject to an Area Structure Plan. Section 3.6.1 of the MDP directs that the applicable Area Structure Plan provides detailed guidance for the review of planning applications.

A portion of the subject lands fall within the Transit Station Planning Area of the future LRT Station to be located at 60 Street NE and 88 Avenue NE. A Transit Station Planning Area includes lands located within a 600-metre radius of an LRT station and is intended to develop in alignment with the Transit Oriented Development Guidelines, as detailed below. The proposed land use redesignations are aligned with applicable policies for Transit Station Planning Areas and achieves the minimum intensity requirement of 100 people and jobs per gross developable hectare.

The future LRT Park and Ride site is located within a Community Activity Centre, which is defined as an area of moderate job and population growth convenient to one or more communities and supported by the Primary Transit Network. The proposed DC Direct Control District for the Park and Ride site will facilitate higher intensity and a broad mix of uses, in support of existing land use districts that facilitate higher density multi-residential development and a range of commercial uses in the future. Community Activity Centres must achieve a

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minimum intensity of 150 people and jobs per gross developable hectare, and this intensity is achieved by the existing land use districts.

Saddle Ridge Area Structure Plan (Statutory - Updated 2015)

The subject site is located within the Cell E Planning Area on Map 5 of the *Saddle Ridge ASP* and is identified as Residential on the Land Use Plan depicted in Map 6. Overall, Cell E has been designed in accordance with the Cell E residential policies. Of relevance to this application, a minimum average residential density of 20 units per hectare is required across Cell E. The proposed land use redesignation and associated outline plan serves to lower the density of this portion of the Savanna neighbourhood but will achieve an anticipated density of 29.2 units per hectare, well in excess of the requirements of the ASP.

The future LRT Park and Ride site falls within a Community Activity Centre, which is intended to provide a mix of residential and commercial uses along with an appropriate amount of amenity space. A block-based network of interconnected streets is intended to provide walkability and a cycle-friendly environment. The policy requirements of the Community Activity Centre are achieved through the existing outline plan and land use districts, and the proposed DC Direct Control District will further enhance the area through allowance of a broad mix of uses and facilitation of a public transit plaza.

The outline plan area falls within the Transit Station Planning Area (TSPA), which comprises a 600 metre radius around the future LRT station. Development within the TSPA should be in accordance with the City's Transit Oriented Development Policy Guidelines.

Transit Oriented Development Policy Guidelines (Non-statutory - 2005)

The guidelines provide direction for development of areas within 600 metres of a Transit Station, and encourages higher density, walkable, mixed-use environments in order to optimize use of transit infrastructure, create mobility options, and benefit local communities and transit riders. While the existing outline plan and land use districts provide higher densities, which are better aligned transit-oriented development guidelines, the proposed land use redesignations and associated outline plan will continue to provide appropriate land uses, density, and pedestrian connectivity. The modified grid street network provides for a high degree of connectivity and will support active transportation modes. The proposed DC Direct Control District for the Park and Ride site is intended to facilitate a vibrant, active, mixed-use node adjacent to the future LRT station.

Policy and Land Use Analysis Summary

The approved outline plans and land use designations are fully aligned with applicable policies and would support a dense, vibrant, and transit-supportive urban environment. The proposed outline plan and land use changes are less supportive of the original vision for the area and may reduce the ridership base in the interim and until the full build-out of the Park and Ride site occurs for the planned LRT extension. The proposed changes are intended to respond to current market conditions including strong demand for lower density housing forms and will

**Land Use Amendment in Saddle Ridge (Ward 5) at 9320 and 9325 - 52 Street NE,
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continue to meet minimum targets for intensity and density. While intensity and density projections suggest a reduction, the applicant has indicated that the anticipated development forms will house multi-generational families at substantially higher occupancy rates than are typically observed city-wide.

Social, Environmental, Economic (External)

The recommended land use framework will provide for a mix and intensity of uses that supports near-term development. Changes to the approved outline plans and existing land uses represent a less intense use of land and infrastructure by reducing potential people and jobs; however, the proposal continues to meet or exceed all required density and intensity targets and is in alignment with applicable policies. The proposed land use districts are intended to facilitate the development of large homes capable of serving multi-generational households, which is an underserved housing typology in new greenfield communities. This would allow for greater housing choice and variety within the Savanna neighbourhood, thus contributing to a complete community with a diversity of housing forms.

Financial Capacity

Current and Future Operating Budget

A business case for operation and maintenance of the future park and ride facilities will be made once funding for the planned LRT extension is approved.

Current and Future Capital Budget

Section 644 (Acquisition of land designated for a public use) of the *Municipal Government Act* may be triggered by this redesignation. This section would require the City to commence proceedings to acquire the future Park and Ride site within a reasonable time. Administration is currently in the process of developing a business case for acquisition of the lands.

Risk Assessment

Redesignation of the future LRT Park and Ride site from S-CRI to DC Direct Control District based on an MU-1 base district at this point in time will render the lands available for development before they are owned by The City. Currently there are no City funds allocated for the purchase of these lands. The proposed DC Direct Control district allows for a broader range of development than the current land use, which has a low risk potential for development of the land in a way that may not align with how the City intends to develop in the future.

In this regard, a Development Restriction section has been included in the proposed DC in an effort to mitigate this risk; however, uses beyond what The City would prefer to have on the land prior to its ownership will still be possible. This risk is further mitigated though a number of factors. The City is the Development Authority for the subject lands, a purchase agreement to be entered into between The City and the landowner is required prior to subdivision, and the

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City and the area developer have maintained a mutual understanding and a good relationship in developing these lands.

The proposed DC Direct Control District fulfills the intent of Council's direction for this site, as it allows for a broader range of development than the current land use district, and is based on preliminary development concepts which have been developed by The City's stakeholder team. Risks are mitigated through the DC Direct Control Bylaw and the associated approval roles The City maintains on these lands.

REASON(S) FOR RECOMMENDATION(S):

Administration has worked with the applicant to help mitigate some of the concerns associated with the amendments and associated reduction of density on lands in proximity to a future LRT station. The outcomes are positive for both the Applicant and The City through the provision of varied housing choice and continuing to meet all required density and intensity targets in the Transit Station Planning Area. As such, this application is supported because:

- a) the recommended land use redesignations are consistent with the applicable policies of the *Municipal Development Plan* and the *Saddle Ridge Area Structure Plan*;
- b) the proposal will facilitate the development of ground-oriented residential uses which will contribute to the vibrancy of the area and respond to unique housing needs in this sector of the city; and
- c) the mix of uses and minimum intensity requirements of the Community Activity Centre and Transit Station Planning Area shall be achieved through the existing and proposed land use districts.

The DC Direct Control District responds to Council's Motion Arising regarding the future LRT station Park and Ride site and will facilitate vibrant mixed-use development in accordance with *Transit Oriented Development Policy Guidelines*.

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed Direct Control District Guidelines (DC/R-Gm)
3. Proposed Direct Control District Guidelines (DC/MU-1)
4. Proposed Outline Plan
5. Previously Approved Outline Plan
6. Saddle Ridge Community Association's Letter

Applicant's Submission

INTRODUCTION

On behalf of Genstar Development Company (Genstar), B&A has prepared a Land Use and Outline Plan application that proposes to revise a portion of the approved Savanna neighbourhood.

Savanna is located within the Community of Saddleridge, bounded by Métis Trail NE, Airport Trail NE, 60 Street NE and 88 Avenue NE. The lands affected are approximately 24.30 ha (60.05 ac) in size, and are municipally addressed as 9320 – 52 Street NE (legally described as NW ¼ Sec, Twp 25, Rge 29, W 4th Mer).

The subject lands are contained within later phases of development within Savanna, and are surrounded by a flurry of construction activity for new commercial developments, like Royop's Savanna Bazaar, and other residential developments. Genstar has a good understanding of residential uptake within the market and purchasing trends within the Savanna neighbourhood are skewed towards larger single detached homes that could accommodate multi-generational families – parents, children and grandparents with adequate parking and amenity space to meet the needs of these unique family structures. This application proposes to revisit the subdivision pattern and land uses associated with the subject lands and surrounding blocks.

COUNCIL DIRECTION

In April 2014, the Savanna neighbourhood of 95.79 ha, 236.69 ac was approved at CPC (file number LOC2013-0036). The lands that surrounded the future LRT station in the southeast portion of the neighbourhood, accounting for 15.15 ha (37.44 ac) of land abutting 60 Street and 88 Avenue NE, was later approved as Stage 2 of Savanna in September 2015 at CPC (file number LOC2014-0208). At the Council hearing for the land use component, a motion by Councillor Carra was made to ask Administration to bring forward a land use amendment for the Park 'n' Ride lands that would allow for mixed use and future intensification of the site. The motion reads:

MOTION ARISING, Moved by Councillor Carra, Seconded by Councillor Stevenson, that with respect to CPC2015-191, Council direct Administration to prepare and bring forward for Council's consideration, a City initiated land use redesignation for the S-CRI site which in addition to a transit hub and park and ride facility, will allow for the future development of a portion of the site north of the anticipated transit hub with a mix of uses and a public plaza area and potentially affordable housing.

PROPOSED CHANGE TO APPROVED OUTLINE PLAN & LAND USES

The application proposes to address the above motion arising through a proposal of a DC district that utilizes the Mixed Use – Active Frontage (M-U2) district with components of the Special Purpose – City and Regional Infrastructure (S-CRI) District that would facilitate both a parking structure needed by Transit, and true Transit Oriented Development within the site through the addition of retail, office and multi-residential uses.

A review of the demographic and market trends has found that there are more people living within Savanna than originally projected in the planning stages 4-5 years ago. This is in part due to the high number of multi-generational families who are choosing to make Savanna their home.

A review of this last phase of development within the northeast corner of Savanna has revealed that there are not enough desirable product types for the multi-generational family. Some of the family members are often new arrivals to Canada, and will thrive when given the ability to live with their relatives. As such, 5-6+ bedroom homes are becoming more common within the

Applicant's Submission

neighbourhood. Although there is an argument that the intensity calculation of 3.3 persons per household is much higher in Savanna, the density and intensity already exceeds MDP, CTP and ASP metrics.

This application proposes to re-evaluate the road network, block structure and land uses within one of the last phases of development. The roundabout within Savanna Drive is proposed to be removed, which will make more efficient use of the land with a smaller 3-way intersection design and will promote safer pedestrian crossing. Further, the realignment of the road network facilitates a rectangular block structure that would promote a variety of lower-density grade-oriented housing forms with enhanced pedestrian experiences on public roads. The intensity and density within the Community Activity Centre and Transit Station Planning Area (within 400m of the future LRT station) have been protected to ensure minimum targets are being met through smaller more efficient lot sizes and the elimination of single-detached products through a DC district on the collector roadways.

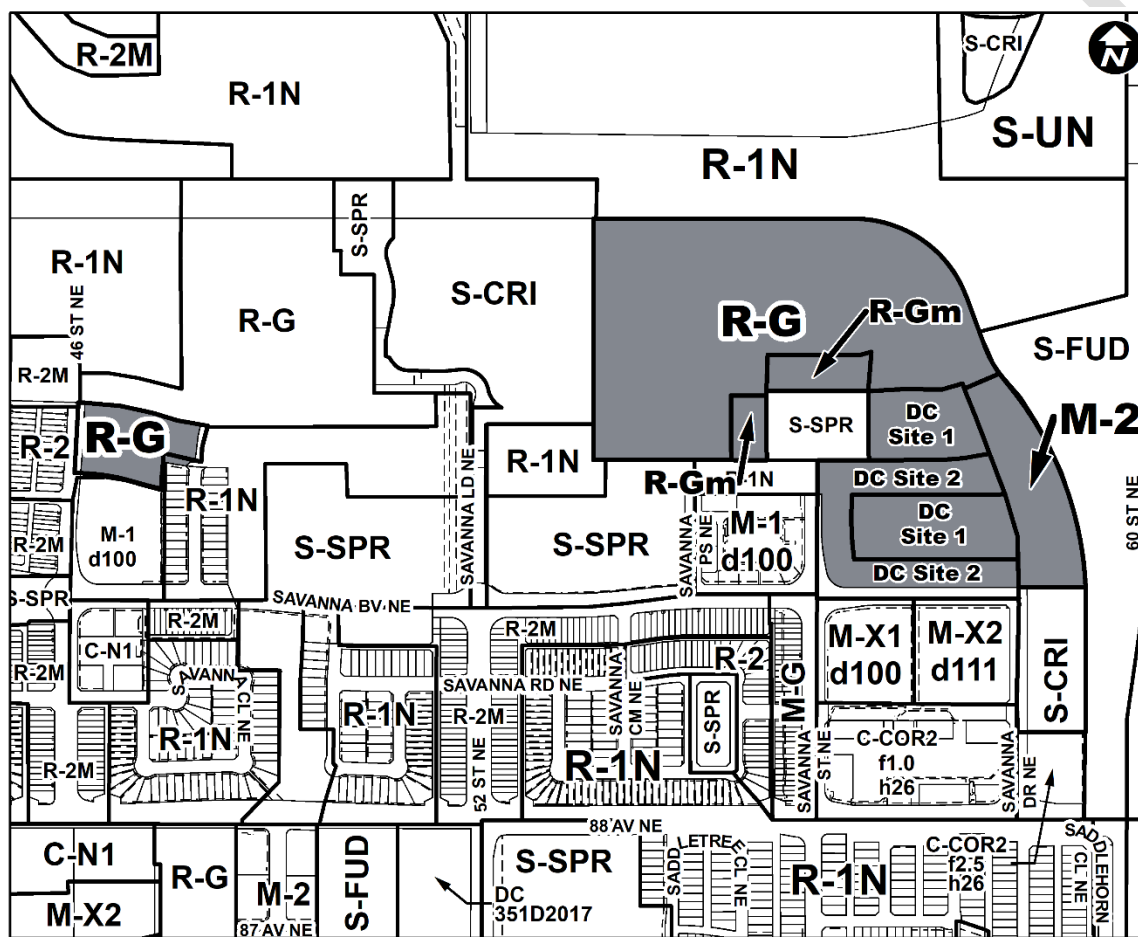
SUMMARY

The application proposed is responsive to market and demographic trends, makes the best use of private-public sector coordination and pursues a sustainable future for the developing neighbourhood of Savanna. Furthermore, the applications are consistent with the goals and objectives of the Municipal Development Plan, and exceed the density and intensity targets for Greenfield Areas and Community Activity Centres identified within the New Community Planning Guidebook and the Saddle Ridge ASP.

Proposed Direct Control District Guidelines (DC/R-Gm)

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by amending that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "A".

SCHEDULE A



DIRECT CONTROL DISTRICT

Purpose

- 1 This Direct Control district is intended to:

- (a) facilitate **higher** density ground-oriented residential **development** within a Transit Station Planning Area.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Proposed Direct Control District Guidelines (DC/R-Gm)

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Bylaw 1P2007 District Rules

- 4 Unless otherwise specified, the rules of the Residential – Low Density Mixed Housing (R-Gm) District of Bylaw 1P2007 apply in this Direct Control District.

Parcel Area

- 5 The minimum area of a *parcel* is 100.0 square metres per **Dwelling Unit**.

Additional Rules for Single Detached Dwellings

- 6 **Single Detached Dwellings** must only be permitted on *carriage house lots*.

Site 1 (1.90 ha ±)

Application

- 7 The provisions in sections 8 through 9 apply only to Site 1.

Permitted Uses

- 8 The **permitted uses** of the Residential – Low Density Mixed Housing (R-Gm) District of Bylaw 1P2007 are the **permitted uses** of this Direct Control District.

Discretionary Uses

- 9 The **discretionary uses** of the Residential – Low Density Mixed Housing (R-Gm) District of Bylaw 1P2007 are the **discretionary uses** of this Direct Control District.

Site 2 (1.62 ha ±)

Application

- 10 The provisions in sections 11 through 12 apply only to Site 2.

Permitted Uses

- 11 The **permitted uses** of the Residential – Low Density Mixed Housing (R-Gm) District of Bylaw 1P2007 are the **permitted uses** of this Direct Control District with the exception of:

- (a) **Semi-Detached Dwellings**.

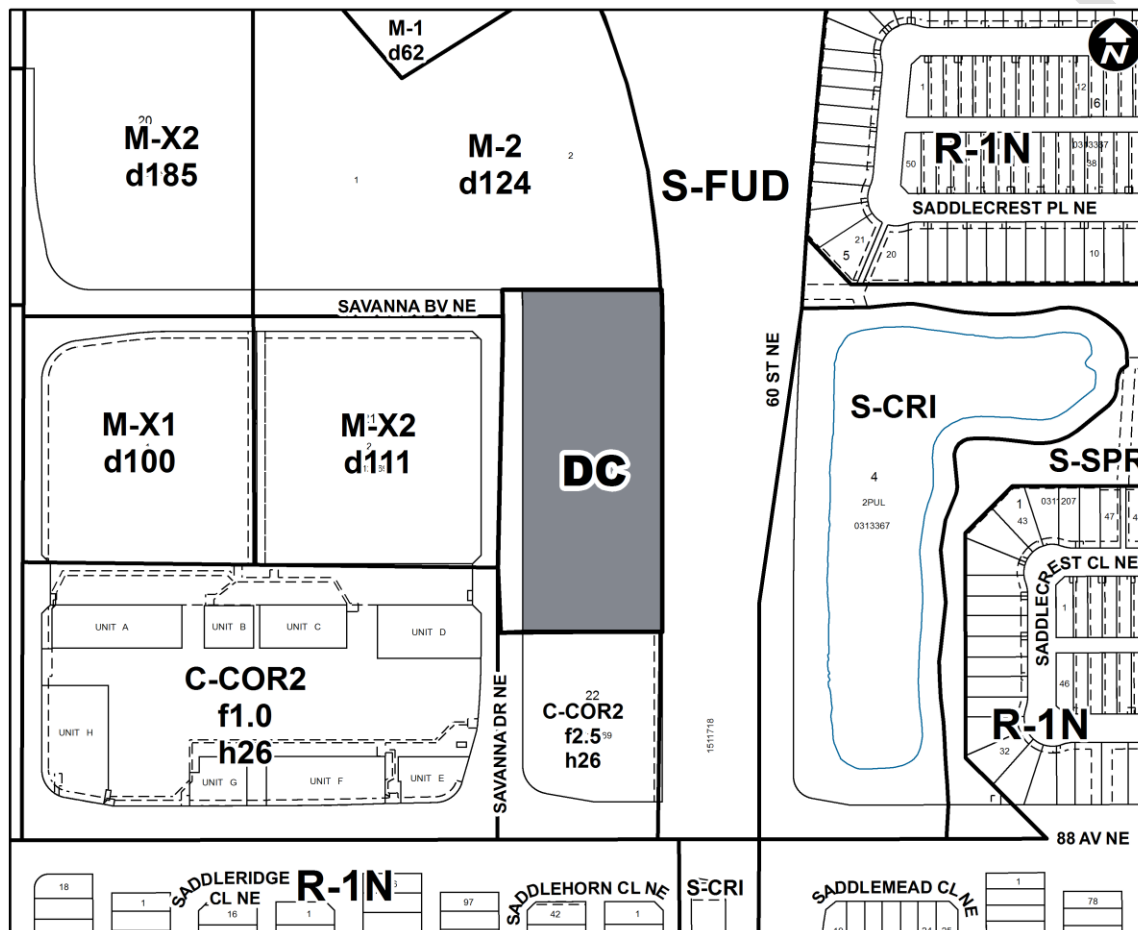
Discretionary Uses

- 12 The **discretionary uses** of the Residential – Low Density Mixed Housing (R-Gm) District of Bylaw 1P2007 are the **discretionary uses** of this Direct Control District.

Proposed Direct Control District Guidelines (DC/MU-1)

- The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by amending that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "A".

SCHEDULE A



DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control district is intended to provide:

- a public **Parking Lot – Structure** or a public **Parking Lot – Grade** to support transit **uses**; and
- the future intensification of transit oriented **development** with retail, residential, and office **uses** that promote an active pedestrian realm.

Proposed Direct Control District Guidelines (DC/MU-1)

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Bylaw 1P2007 District Rules

- 4 Unless otherwise specified, the rules of the Mixed Use – General (MU-1) District of Bylaw 1P2007 apply in this Direct Control District.

Permitted Uses

- 5 The **permitted uses** of the Mixed Use – General (MU-1) District of Bylaw 1P2007 are the **permitted uses** of this Direct Control District with the addition of:

- (a) **Municipal Works Depot.**

Discretionary Uses

- 6 The **discretionary uses** of the Mixed Use – General (MU-1) District of Bylaw 1P2007 are the **discretionary uses** of this Direct Control District with the addition of:

- (a) **Parking Lot – Grade.**

Building Height

- 7 The maximum **building height** is 64.0 metres.

Floor Area Ratio

- 8 The maximum **floor area ratio** is 2.5.

Development Restrictions

- 9 **Development permits** for permanent **uses** must also include a **Parking Lot – Structure or Parking Lot – Grade** to support transit **uses**, to the satisfaction of the **Development Authority**.

Relaxations

- 10 The **Development Authority** may relax any of the rules contained in this Direct Control District provided the test for relaxation in Bylaw 1P2007 is met.

Proposed Outline Plan



prime consultant:
B&A Planning Group
800, 215 - 9th Ave SW
[Calgary, Alberta | T2P 1K3] | bapp.ca
t: 403 269 4733 f: 403 262 4480

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Contributors shall verify and be responsible for all
dimensions and conditions shown on the drawing.

client:
GENSTAR
BRINGING LAND TO LIFE

sub-consultant:
P **L.A. West**

- legend:
- Land Use Boundary
 - Outline Plan Boundary
 - Contour Interval 0.5m
 - 1.5m Mono Sidewalk
 - 2.0m Mono Sidewalk
 - 2.0m Separate Sidewalk
 - 3.0m Multi-Use Pathway
 - 3.0m Regional Pathway
 - 2.5m Local Pathway
 - Bus Pad Location
 - CAC - Community Activity Centre
 - Clear Services - Proposed
 - Storm Sewer/Manhole
 - Sanitary Sewer/Manhole
 - Water Main/Hydrant
- Notes:
- Residential roads are:
less than 8.5m (8.5m/16.0m), laned (8.0m/16.0m)
unless otherwise noted.
- All lanes are 7.0m wide unless otherwise
noted.

file description:
file number: LOC2018-0170
pre-app: PE 2017-01564
Stage 1 Approved: LOC 2018-0036
Stage 2 Approved: LOC 2018-0204

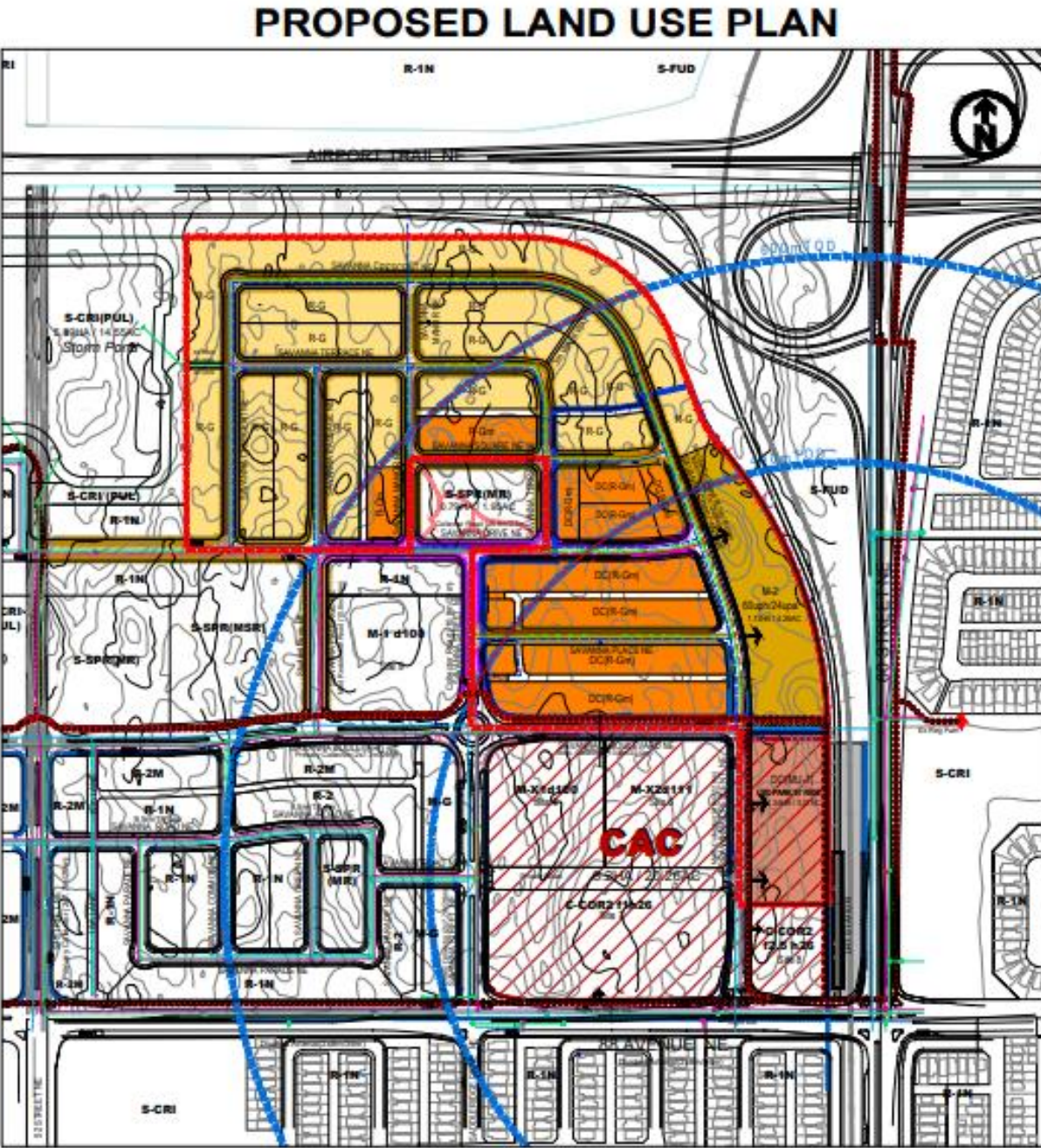
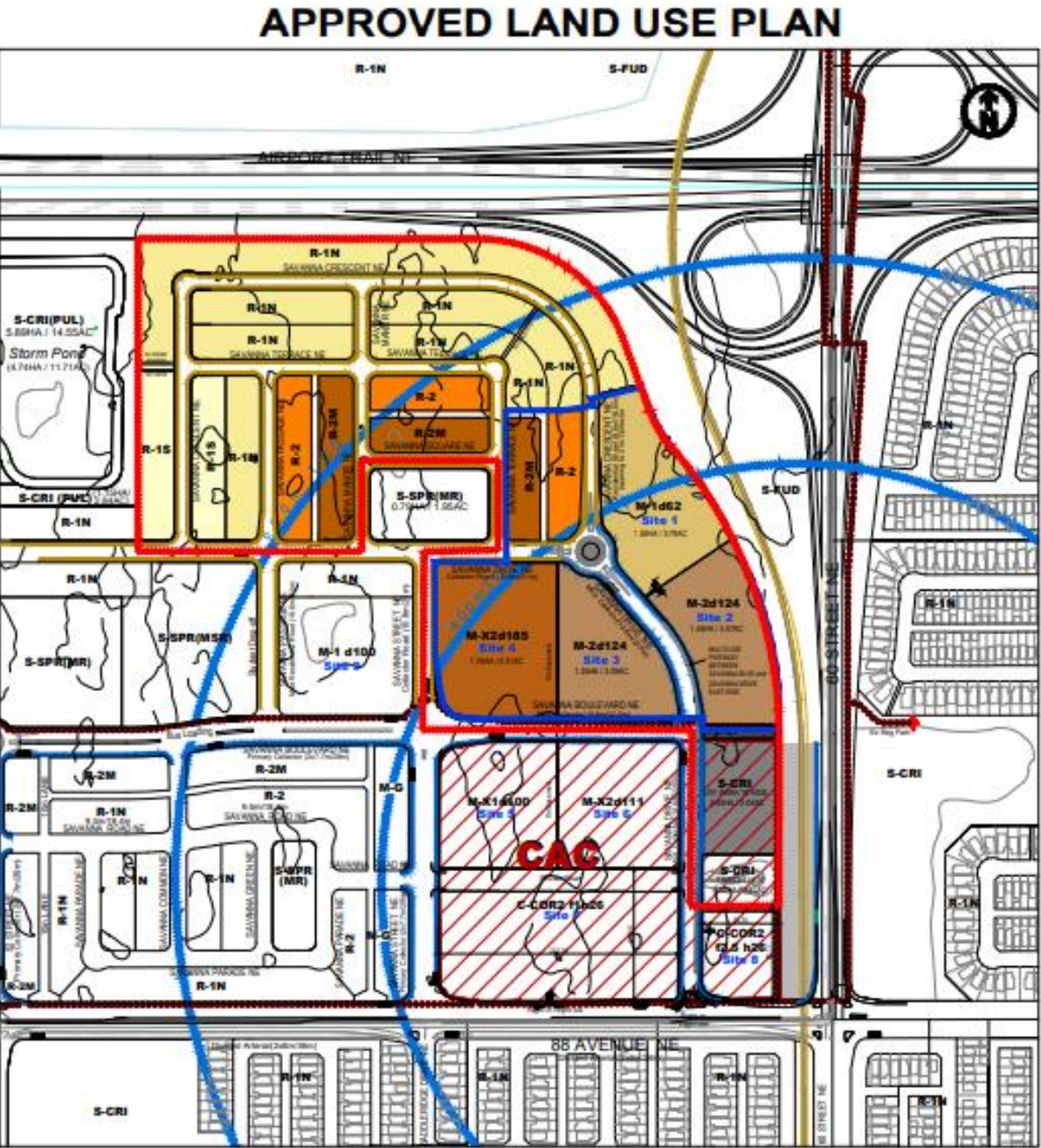
file info:
project no.: 1499c
drawn by: EB
start date: September, 2017
current date: Apr 02, 2019



sheet title:
Outline Plan & Land Use
Redesignation
Site 1

exhibit no.:

1.0



Land Use Boundary
Outline Plan Boundary



Saddle Ridge Community Association's Letter

27,7555 Falconridge Blvd. N.E.,
Calgary, Ab.,
T3J 0C9
Jan.17, 2019

Re: LOC2018-0170

The Saddle Ridge Community Association is in favour of the changes proposed in LOC2018-0170. Larger homes with more parking would suit the needs of the multigenerational families who choose to live in Saddle Ridge. This would lead to the area being built out sooner, and would be good for the retail and service providers in the neighbourhood.

We also are pleased to see the emphasis on transit oriented development around the future LRT station, and would hope for the extension of LRT service to this area in the not too distant future.

Respectfully submitted
Judy Brown
Land Use Committee
Saddle Ridge Community Association

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CPC2019-0318

Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

EXECUTIVE SUMMARY

This application was submitted by B&A Planning Group on 2018 July 25 on behalf of the landowner Genstar Titleco Limited. This outline plan proposes minor amendments to approved outline plans within a 7.73 hectare \pm (19.10 acres \pm) area of the Savanna neighbourhood in the northeast community of Saddle Ridge. This proposal is intended to:

- provide more flexible land use districts to accommodate the housing needs in this sector of the city and to respond to existing market conditions; and
- provide minor modifications to the street network to facilitate lower density, ground-oriented development.

The proposed outline plan and associated land use application (CPC2019-0317) meets the minimum targets of the *Municipal Development Plan* and the *Saddle Ridge Area Structure Plan*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commissions **APPROVE** the proposed outline plan located at 9320 – 52 Street NE (Portion of NW1/4 Section 14-25-29-4) to subdivide 7.73 hectares \pm (19.10 acres \pm) with conditions (Attachment 2).

PREVIOUS COUNCIL DIRECTION / POLICY

None.

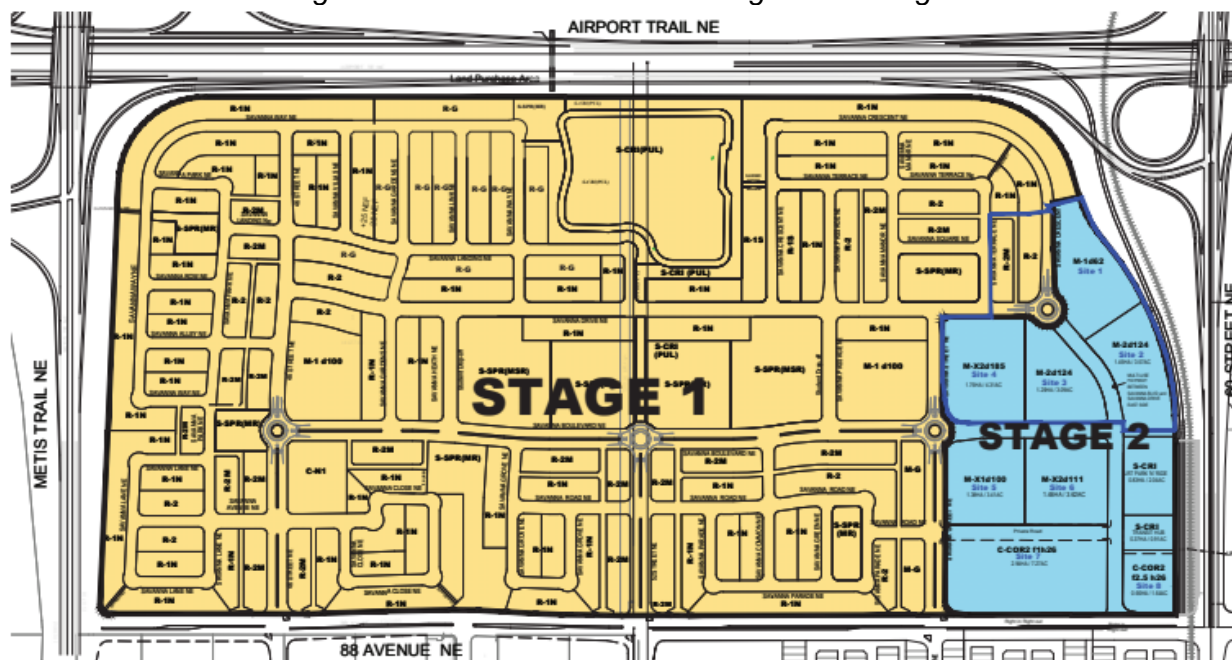
BACKGROUND

This application was submitted on behalf of the owners, Genstar Titleco Limited, by B&A Planning Group on 2018 July 25.

The Savanna neighbourhood received previous approvals through two stages of outline plan and land use redesignation applications, and associated policy amendments to the *Saddle Ridge Area Structure Plan*. Stage 1 (LOC2013-0036) represents the majority of the Savanna neighbourhood, and Stage 2 (LOC2014-0208) represents the lands within the vicinity of the future Light Rail Transit station.

The subject application proposes changes to the approved outline plan and land uses within Stages 1 and 2 as shown in Figure 1 below.

Figure 1: Savanna Outline Plan Stage 1 and Stage 2



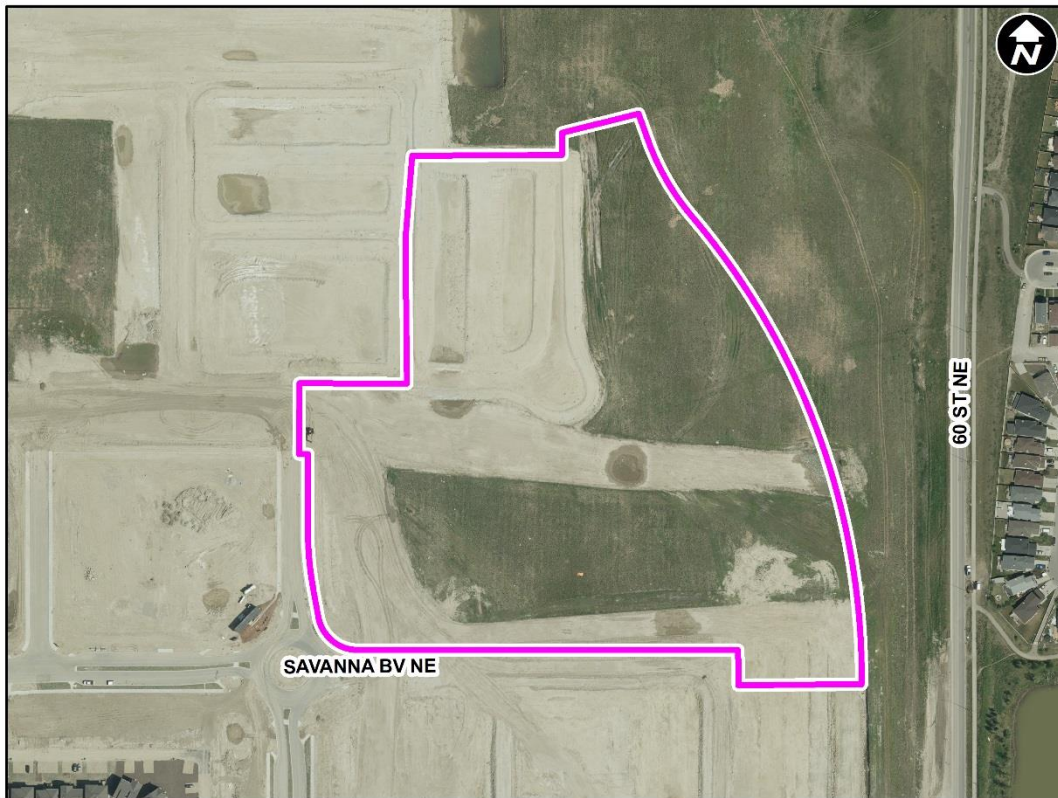
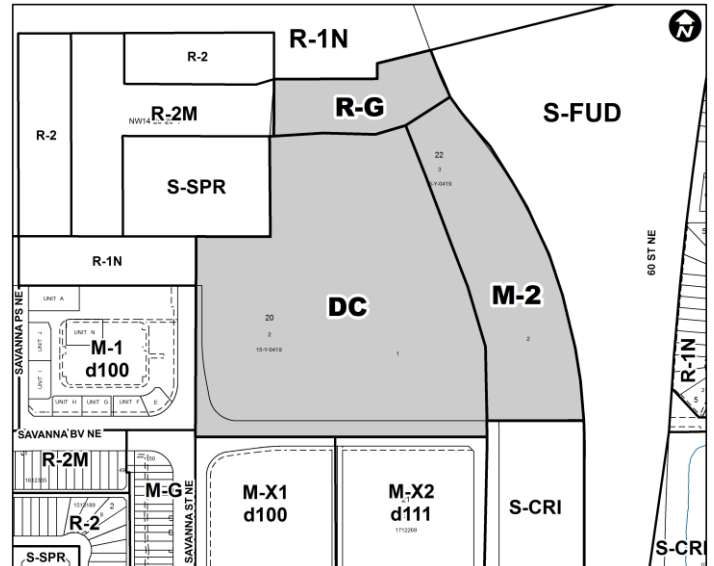
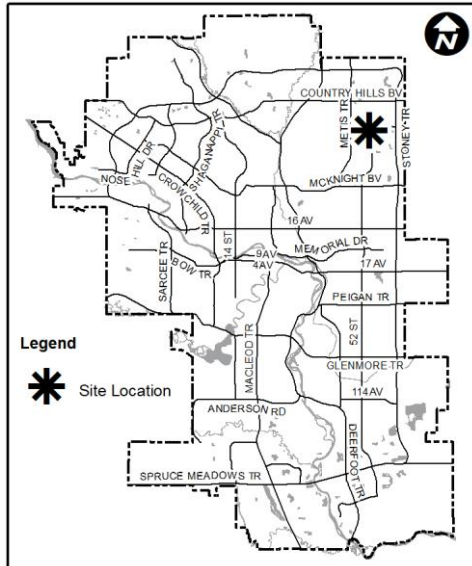
Approval(s): T. Goldstein concurs with this report. Author: S.Gagnon

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Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0318

Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

Location Maps



Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

Site Context

The subject lands comprise approximately 7.73 hectares (19.10 acres) in the community of Saddle Ridge, and are currently undeveloped. Stripping and grading of the lands has been completed. The Savanna neighbourhood is bounded to the north by Airport Trail, to the east by 60 Street NE, to the south by 88 Avenue NE, and to the west by Metis Trail NE.

The neighbourhood contains a broad mix of residential land uses and park spaces, with approximately 400 dwelling units constructed and occupied to date within the neighborhood. Future multi-residential land uses and commercial land uses are located within the Community Activity Centre (CAC) located in the southeast corner of the neighbourhood, and a Neighbourhood Activity Centre (NAC) accommodating higher density residential uses and local-scale commercial uses is located in the western portion of the neighbourhood.

Figure 2 provides population statistics for the community of Saddle Ridge.

Figure 2: Community Peak Population

Saddle Ridge	
Peak Population Year	2018
Peak Population	21,567
2018 Current Population	21,567
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [Saddle Ridge](#) community profile.

The applicant has indicated that there is a significant demand for multi-generational households located in this sector of the city. Such households may have a higher ratio of occupants per dwelling unit as compared to the city as a whole. For example, census data for this sector of the city indicates that there are on average approximately 3.9 people per dwelling unit (compared to a city wide average of 3.3 people per dwelling unit).

The proposed land use districts are capable of accommodating a wide range of home sizes, and the applicant has stated an intention to develop the subject lands with very large homes capable of accommodating multi-generational families. Lot sizes will be established through subdivision and unit sizes will be determined at the development permit stage.

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Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This outline plan (Attachment 3), along with the associated land use amendment application (CPC2019-0317), proposes to reduce potential density and provide for greater variety of ground-oriented residential land uses in order to facilitate development in alignment with the applicant's assessment of current market conditions in this sector of the city. Minor amendments to the proposed street network will support ground-oriented housing forms and a pedestrian-friendly environment within a Transit Station Planning Area.

Planning Considerations

These applications have been evaluated in the context of applicable policies, including the *Municipal Development Plan* and *Saddle Ridge Area Structure Plan*. This application represents a response to the applicant's assessment of current market conditions, including a strong preference for larger dwelling units that can accommodate multi-generational families, lower unit density, and ground-oriented residential development forms. Administration requested market information to be provided for supporting justification of the proposal; however, the Applicant did not provide this.

Subdivision Design

The proposed outline plan (Attachment 3) includes minor reconfiguration of the approved street network which was designed to accommodate large blocks of multi-residential development, to provide a more fine-grained street network that will support lower density ground-oriented residential uses. Rear lanes for lower density uses will support a safe and attractive pedestrian realm, and a roundabout has been removed to facilitate more efficient use of land. No major changes to approved infrastructure are contemplated. No Municipal Reserve or Environmental Reserve lands are included within the outline plan area.

Land Use

The associated land use amendment application (CPC2019-0317) proposes to redesignate to the Multi-Residential – Medium Profile (M-2) District, Residential – Low Density Mixed Housing (R-G), Residential – Low Density Mixed Housing (R-Gm) District, and a DC Direct Control District based on the R-Gm District that limits single and semi-detached housing within certain areas of the Transit Station Planning Area. The proposed land use districts will allow the development of lower density product (primarily semi-detached dwellings, rowhouses, and townhouses) in alignment with current market conditions. Additional land use redesignations outside of the outline plan boundary similarly propose to facilitate lower density ground-oriented uses. Also proposed is redesignation of the future LRT Park and Ride site to a DC Direct Control District based on the Mixed Use – General (MU-1) District in order to facilitate mixed-use development in addition to transit infrastructure uses.

Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

Density

Applicable policies set targets for both density (units per hectare) and intensity (people and jobs per hectare).

The proposed land use redesignations will result in lower intensity development than the approved outline plans and land use districts. Approved land use districts would require a minimum intensity of 102 people and jobs per hectare, and the proposed land use districts will achieve an anticipated intensity of 81 people and jobs per hectare. The proposed outline plan represents a smaller area than the associated land use redesignation application, and therefore the intensity rates indicated in the land use redesignation report will differ. Intensity rates are calculated based on a standard rate of 3.3 people per dwelling unit; however, census data indicates that within the general area density of approximately 3.9 people per dwelling unit is typical and therefore higher intensities may be realized.

Section 4.2 of the *Saddle Ridge Area Structure Plan* requires a minimum average residential density of 17.3 units per gross developable hectare. The existing approved land use districts will achieve a density of approximately 45-53 units per hectare, and the proposed land use districts will achieve a density of approximately 29-36 units per hectare. This represents an anticipated reduction of approximately 16-17 units per hectare, while still exceeding the minimum average residential density.

The *Municipal Development Plan* requires a minimum intensity of 100 people and jobs per hectare within the Transit Station Planning Area, which constitutes a 600 metre radius around the future LRT station. Existing land use districts would facilitate an intensity of 106 people and jobs per hectare, and the proposed land use redesignations will facilitate an intensity of 100 people and jobs per hectare.

Environmental

Environmental concerns for the subject lands were considered during the original outline plan approvals, and no further reports or investigations were required for this application.

Information on proposed building specifications in regards to energy performance were requested from the applicant but are not available at this time.

Transportation

The associated outline plan (Attachment 4) proposes minor changes to the approved street network, including removal of a roundabout based on minor re-alignment of the collector network, and imposition of a more fine-grained street network to facilitate lower density ground-oriented residential development.

Local transit service for the Savanna neighbourhood is currently provided by bus Route 59. Future LRT service to the area is planned through an extension of the Blue Line; however, this extension is unfunded and no timeline has been established for construction. A transit hub, Park and Ride site, and future transit connection to the Calgary International Airport are intended to be collocated adjacent to the future LRT station.

Planning & Development Report to
Calgary Planning Commission
2019 April 18

ISC: UNRESTRICTED
CPC2019-0318

Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

Utilities and Servicing

Water and sanitary servicing plans to service this area were established through the Savanna outline plan approval in 2014. The existing servicing plans are sufficient to accommodate the proposed land use changes.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

No submissions were received from the public.

The Saddle Ridge Community Association was circulated on this application and provided a letter of support (Attachment 6). The Association favours larger homes with more parking to suit the needs of multi-generational families, and more rapid build-out of the area to support local commercial uses.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Internal Business Units identified as stakeholders for the future LRT Park and Ride site were engaged regarding the proposed redesignation. Real Estate & Development Services and Transit Infrastructure were the primary stakeholders.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The site is located within the City, Town area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to the site, the proposal is consistent with policies on Land Use Patterns (Section 8.14).

Interim Growth Plan (2018)

The proposed outline plan and land use amendment are in keeping with the principles of the *Interim Growth Plan*, which promotes efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

Municipal Development Plan (Statutory - 2009)

Map 1: Urban Structure of the *Municipal Development Plan* (MDP) identifies the subject lands as a Planned Greenfield Area subject to an Area Structure Plan. Section 3.6.1 of the MDP directs that the applicable Area Structure Plan (*Saddle Ridge ASP*) provides detailed guidance for the review of planning applications.

A portion of the subject lands fall within the Transit Station Planning Area of the future LRT Station to be located at 60 Street NE and 88 Avenue NE. A Transit Station Planning Area includes lands located within a 600 metre radius of an LRT station, and is intended to development in alignment with the Transit Oriented Development Guidelines, as detailed below. The proposed land use redesignations are aligned with applicable policies for Transit Station Planning Areas and achieves the minimum intensity requirement of 100 people and jobs per gross developable hectare.

Saddle Ridge Area Structure Plan (Statutory - Updated 2015)

The subject site is located within the Cell E Planning Area on Map 5 of the *Saddle Ridge Area Structure Plan* (ASP) and is identified as Residential on the Land Use Plan depicted in Map 6. Overall, Cell E has been designed in accordance with the Cell E residential policies. Of relevance to this application, a minimum average residential density of 20 units per hectare is required across Cell E. The proposed land use redesignation and associated outline plan serves to lower the density of this portion of the Savanna neighbourhood, but will achieve an anticipated density of 29.2 units per hectare, well in excess of the requirements of the ASP.

The outline plan area falls within the Transit Station Planning Area (TSPA), which comprises a 600 metre radius around the future LRT station. Development within the TSPA should be in accordance with the City's Transit Oriented Development Policy Guidelines.

Transit Oriented Development Policy Guidelines (Non-statutory - 2005)

The guidelines provide direction for development of areas within 600 metres of a Transit Station, and encourages higher density, walkable, mixed-use environments in order to optimize use of transit infrastructure, create mobility options, and benefit local communities and transit riders. While the existing outline plan and land use districts provide higher densities than the proposed amendments, the proposed outline plan and land use redesignations will continue to provide appropriate land uses, density, and pedestrian connectivity.

Policy and Land Use Analysis Summary

The proposed land use changes associated with the outline plan are less supportive of the overall transit oriented development vision The City has (which is high density, street oriented, mixed use development forms – which all contribute to walkability and vibrancy). However, the lands are outside of the CAC area, the proposed changes continue to meet minimum density and intensity policy targets while responding to current local market conditions in this sector of The City, and new lanes in the area will contribute to maintaining a walkable neighbourhood.

Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

While intensity and density projections represent reductions, the applicant has indicated that the anticipated development forms will house multi-generational families at substantially higher occupancy rates than are typically observed city-wide. The associated revised outline plan, street layouts and land use designations are fully aligned with applicable policies and would support continued build out of this area as a dense, vibrant, walkable, and transit-supportive urban environment.

Social, Environmental, Economic (External)

The recommended land use framework will provide for a mix and intensity of uses that supports near-term development. Changes to the approved outline plans and existing land uses represents a less intense use of land and infrastructure by reducing potential population and jobs; however, the proposal continues to meet or exceed all required density and intensity targets and is in alignment with applicable policies.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendments do not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are some notable concerns with downzoning applications in a Transit Station Planning Area; however, the appropriate land use and planning framework to arrive at a successful neighborhood design outcome is maintained with this application, these being: minimum policy density and intensity targets are achieved; a block based grid pattern ensuring walkability is maintained; and the appropriate mix of uses and intensity of the CAC is maintained through existing approvals and land uses.

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Outline Plan in Saddle Ridge (Ward 5) at 9320 - 52 Street NE, LOC2018-0170 (OP)

REASON(S) FOR RECOMMENDATION(S):

Administration has worked with the applicant to help mitigate some of the concerns associated with the amendments and associated reduction of density on lands in proximity to a future LRT station. The outcomes are seen as positive for both the Applicant and The City through the provision of varied housing choice and continuing to meet all required density and intensity targets in the Transit Station Planning Area. As such, this application is supported because:

- a) the recommended land use redesignations are consistent with the applicable policies of the Municipal Development Plan and the Saddle Ridge Area Structure Plan;
- b) the proposal will facilitate the development of ground-oriented residential uses which will contribute to the vibrancy and varied housing mix of the area, responding to unique housing needs in this sector of the city; and
- c) the mix of uses and minimum intensity requirements of the Transit Station Planning Area shall be achieved through the existing and proposed land use districts.

ATTACHMENT(S)

- 1. Applicant's Submission
- 2. Conditions of Approval
- 3. Proposed Outline Plan
- 4. Proposed Land Use District Map
- 5. Subdivision Data Sheet
- 6. Saddle Ridge Community Association's Letter

Applicant's Submission

INTRODUCTION

On behalf of Genstar Development Company (Genstar), B&A has prepared a Land Use and Outline Plan application that proposes to revise a portion of the approved Savanna neighbourhood.

Savanna is located within the Community of Saddleridge, bounded by Métis Trail NE, Airport Trail NE, 60 Street NE and 88 Avenue NE. The lands affected are approximately 24.30 ha (60.05 ac) in size, and are municipally addressed as 9320 – 52 Street NE (legally described as NW ¼ Sec, Twp 25, Rge 29, W 4th Mer).

The subject lands are contained within later phases of development within Savanna, and are surrounded by a flurry of construction activity for new commercial developments, like Royop's Savanna Bazaar, and other residential developments. Genstar has a good understanding of residential uptake within the market and purchasing trends within the Savanna neighbourhood are skewed towards larger single detached homes that could accommodate multi-generational families – parents, children and grandparents with adequate parking and amenity space to meet the needs of these unique family structures. This application proposes to revisit the subdivision pattern and land uses associated with the subject lands and surrounding blocks.

COUNCIL DIRECTION

In April 2014, the Savanna neighbourhood of 95.79 ha, 236.69 ac was approved at CPC (file number LOC2013-0036). The lands that surrounded the future LRT station in the southeast portion of the neighbourhood, accounting for 15.15 ha (37.44 ac) of land abutting 60 Street and 88 Avenue NE, was later approved as Stage 2 of Savanna in September 2015 at CPC (file number LOC2014-0208). At the Council hearing for the land use component, a motion by Councillor Carra was made to ask Administration to bring forward a land use amendment for the Park 'n' Ride lands that would allow for mixed use and future intensification of the site. The motion reads:

MOTION ARISING, Moved by Councillor Carra, Seconded by Councillor Stevenson, that with respect to CPC2015-191, Council direct Administration to prepare and bring forward for Council's consideration, a City initiated land use redesignation for the S-CRI site which in addition to a transit hub and park and ride facility, will allow for the future development of a portion of the site north of the anticipated transit hub with a mix of uses and a public plaza area and potentially affordable housing.

PROPOSED CHANGE TO APPROVED OUTLINE PLAN & LAND USES

The application proposes to address the above motion arising through a proposal of a DC district that utilizes the Mixed Use – Active Frontage (M-U2) district with components of the Special Purpose – City and Regional Infrastructure (S-CRI) District that would facilitate both a parking structure needed by Transit, and true Transit Oriented Development within the site through the addition of retail, office and multi-residential uses.

A review of the demographic and market trends has found that there are more people living within Savanna than originally projected in the planning stages 4-5 years ago. This is in part due to the high number of multi-generational families who are choosing to make Savanna their home.

A review of this last phase of development within the northeast corner of Savanna has revealed that there are not enough desirable product types for the multi-generational family. Some of the family members are often new arrivals to Canada, and will thrive when given the ability to live with their relatives. As such, 5-6+ bedroom homes are becoming more common within the

Applicant's Submission

neighbourhood. Although there is an argument that the intensity calculation of 3.3 persons per household is much higher in Savanna, the density and intensity already exceeds MDP, CTP and ASP metrics.

This application proposes to re-evaluate the road network, block structure and land uses within one of the last phases of development. The roundabout within Savanna Drive is proposed to be removed, which will make more efficient use of the land with a smaller 3-way intersection design and will promote safer pedestrian crossing. Further, the realignment of the road network facilitates a rectangular block structure that would promote a variety of lower-density grade-oriented housing forms with enhanced pedestrian experiences on public roads. The intensity and density within the Community Activity Centre and Transit Station Planning Area (within 400m of the future LRT station) have been protected to ensure minimum targets are being met through smaller more efficient lot sizes and the elimination of single-detached products through a DC district on the collector roadways.

SUMMARY

The application proposed is responsive to market and demographic trends, makes the best use of private-public sector coordination and pursues a sustainable future for the developing neighbourhood of Savanna. Furthermore, the applications are consistent with the goals and objectives of the Municipal Development Plan, and exceed the density and intensity targets for Greenfield Areas and Community Activity Centres identified within the New Community Planning Guidebook and the Saddle Ridge ASP.

Conditions of Approval

Planning:

1. The Developer shall submit a density phasing plan with each Tentative Plan submission, showing the proposed phasing within the Outline Plan area and the projected number of dwelling units within each phase to demonstrate compliance with the minimum required densities of the Area Structure/Community Plan and Municipal Development Plan.
2. If the total area for Roads & Public Utility Lot dedication is over 30%, note that compensation in the order of \$1 for over dedication is deemed to be provided.
3. Land Purchase Option Agreement is required for the Park and Ride site along the east boundary of the Plan Area for future LRT Park and Ride facilities prior to the registration of the final instrument.
4. The standard City of Calgary Party Wall Agreement shall be registered for all rowhouse, townhouse, and semi-detached dwellings in the DC(R-Gm), R-G, R-Gm, and M-2 Land Use District areas concurrent with registration of the final instrument.

Development Engineering:

5. The parcels shall be developed in accordance with the development restriction recommendations outlined in the following report(s):
 - Geotechnical Report, prepared by AMEC Environmental & Infrastructure (File No CG08919), dated February 2013.
6. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.
7. **Concurrent with the registration of the final instrument**, execute and register on all parcels with double frontage lots that are adjacent to a collector road, a neighbourhood boulevard, an urban boulevard, an arterial road, a skeletal road, or a Transportation Utility Corridor, a Screening Fence Access Easement Agreement with the City of Calgary. The agreement and registerable access right of way plan shall be approved by the Manager, Infrastructure Planning and the City Solicitor **prior to endorsement of the final instrument**. A standard template for the agreement will be provided by the Development Engineering Generalist. Prepare and submit three (3) copies of the agreement for the City's signature.
8. **Prior to endorsement of any Tentative Plan/prior to release of a Development Permit**, execute a Development Agreement. Contact the Infrastructure Strategist, Calgary Approvals Coordination for further information at 403-268-5706 or email galina.karadakova-mineva@calgary.ca.
9. Make satisfactory cost sharing arrangements with Saddlecrest Land Inc. for part cost of the existing sanitary and storm sewer installed in 60 Street NE that was constructed by Saddlecrest Land Inc. under Saddle Ridge, Phase 02 (DA2003-0030)
10. Make satisfactory cost sharing arrangements with Saddlecrest Land Inc. for part cost of the existing storm pond installed in Lot 2PUL in Block 4 that was constructed by Saddlecrest Land Inc. under Saddle Ridge, Phase 02 (DA2003-0030)

Conditions of Approval

11. Off-site levies, charges and fees are applicable. Contact the Public Infrastructure Coordinator, Calgary Approvals Contact the Infrastructure Strategist, Calgary Approvals Coordination for further information at 403-268-5706 or email galina.karadakova-mineva@calgary.ca.
12. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:
 - a) Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.
 - b) Construct the underground utilities and surface improvements in the west two lanes of the four (4) lane divided road in 60 Street NW, along the east boundary of the plan area.
 - c) Construct the underground utilities and surface improvements within the full width of Savanna Street NE, Savanna Terrace NE, Savanna Boulevard NE, Savanna Drive NE, and Savanna Crescent NE along the boundaries of the plan area.
 - d) Construct the underground utilities and surface improvements (including lanes) within and along the boundaries of the plan area.
 - e) Construct the onsite and offsite storm water management facilities (wet pond, wetlands, etc) to service the plan area according to the most current City of Calgary Standard Specifications Sewer Construction, Stormwater Management and Design Manual and Design Guidelines for Subdivision Servicing.
 - f) Construct a wood screening fence, chain link fence, sound attenuation fence, whichever may be required, inside the property line of the residential lots along the boundaries of the plan area.
 - g) Construct the regional pathway within and along the boundaries of the plan area, to the satisfaction of the Director of Parks Development.

Transportation:

13. No direct vehicular access shall be permitted to or from 60 Street NE and a restrictive covenant shall be registered **concurrent with the registration of the final instrument** to that effect at the Tentative Plan stage.
14. No direct vehicular access shall be permitted to or from Airport Trail NE and a restrictive covenant shall be registered **concurrent with the registration of the final instrument** to that effect at the Tentative Plan stage.
15. One (1) access only will be permitted to or from Savanna Crescent, between 88 Avenue and Savanna Boulevard NE. This access is to align centreline to centreline with the private road extension of Savanna Road NE through Sites 5-7. A restrictive covenant shall be registered **concurrent with the registration of the final instrument** to that effect at the Tentative Plan stage.

Conditions of Approval

16. **Concurrent with the registration of the final instrument** of the affected tentative plan for the DC(MU-1) site, a 5m wide public access easement and right of way plan shall be executed and registered on title.
- The 5m public access easement shall connect between Savanna Crescent and the 60 Street NE/LRT right-of-way. The public access easement shall be located in alignment with the pathway on the north side of Savanna Boulevard, and is required to provide pedestrian access to the LRT Station.
17. **Concurrent with the registration of the final instrument** of the affected tentative plan for the DC(MU-1) site, a 5m wide public access easement and right of way plan shall be executed and registered on title.
- The public access easement shall be registered along the easterly property line adjacent to the LRT Station to accommodate the multi-use pathway connection between 88 Avenue NE and Savanna Boulevard NE.
18. **In conjunction with the applicable Tentative Plan or Development Permit** for the staged construction of the road network, transit stops shall be provided to the satisfaction of the Director, Transit and the Director, Transportation Planning. All bus zones shall be located:
- where commercial areas are concentrated;
 - where the grades and site lines are compatible to install bus zones;
 - where pedestrian walkways, pathways, and roadway crossing opportunities are provided.
19. **In conjunction with the applicable Tentative Plan or Development Permit**, bus bays shall be provided on roadways identified by Calgary Transit and Transportation Development Services.
20. Temporary oil and gravel bus turnaround / cul-de-sac with a minimum radius of 15.25 meters is required at the terminus of each construction phase. Where the developer intends to fence the turnaround, the minimum radius shall be increased to 16.25 meters. If road construction and/or construction phasing affects the operations of transit service, the Developer is required to provide an interim transit route replacement, to the satisfaction of the Director, Transit and the Director, Transportation Planning.
21. A Traffic Noise Analysis Report for the development(s) adjacent to Airport Trail and 60 Street NE, certified by a Professional Engineer with expertise in the subject of acoustics related to land use planning, and including finalized lot and building grades, shall be submitted to Transportation Planning for approval at the applicable Tentative Plan or Development Permit stage.

Note that where sound attenuation is not required adjacent to Arterial roadways, a uniform screening fence shall be provided, in accordance with the 2014 Design Guidelines for Subdivision Servicing.

Conditions of Approval

22. All noise attenuation features (noise walls, berms, etc.), screening fence, and ancillary facilities required in support of the development shall be constructed entirely within the development boundary (location of noise walls, berms, screening fence, etc) and associated ancillary works shall not infringe onto the road right-of-ways. Noise attenuation features and screening fences shall be at the Developer's sole expense.
23. **In conjunction with each Tentative Plan or Development Permit**, further traffic and transportation analysis (Transportation Impact Assessment) is required to demonstrate and confirm that capacity is available on the network to support the proposed application, that the application does not exceed the development cap in place at that time, and that all travel modes are accommodated in a contiguous, consistent manner. Any upgrades to the local and regional transportation network required to support the proposed application shall be designed and constructed at the Developer's sole expense, to the satisfaction of the Director, Transportation Planning, and the Director, Roads subject to normal oversize, endeavours to assist, and boundary cost recoveries.
24. **In conjunction with the applicable Tentative Plan**, the developer shall provide a Letter of Credit for a pedestrian-actuated crossing signal at the regional pathway crossing of Savanna Crescent along the north side of Savanna Boulevard.

The Developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit, and is required to submit payment in support of the proposed Tentative Plan applications.

Parks:

25. Construct all regional pathway routes within and along the boundaries of the plan area according to Parks' ***Development Guidelines and Standard Specifications – Landscape Construction*** (current version), including setback requirements, to the satisfaction of the Director, Parks.
26. Plant all public trees in compliance with the approved Public Landscaping Plan.
27. **Concurrent with Registration of the Final Instrument**, public access easements shall be registered on any pathway routes located on private lands.
28. **Concurrent with Registration of the Final Instrument**, public access easements shall be registered on any publicly accessible private open space.

Proposed Outline Plan



prime consultant:
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client:
GENSTAR
BRINGING LAND TO LIFE

sub-consultant:
P **L.A. West**

- legend:
- Land Use Boundary
 - Outline Plan Boundary
 - Contour Interval 0.5m
 - 1.5m Mono Sidewalk
 - 2.0m Mono Sidewalk
 - 2.0m Separate Sidewalk
 - 3.0m Multi-Use Pathway
 - 3.0m Regional Pathway
 - 2.5m Local Pathway
 - Bus Pad Location
 - CAC - Community Activity Centre
 - Clear Services - Proposed
 - Storm Sewer/Manhole
 - Sanitary Sewer/Manhole
 - Water Main/Hydrant
- Notes:
- Residential roads are:
less than 8.5m (8.5m/16.0m), laned (8.0m/16.0m)
unless otherwise noted.
- All lanes are 7.0m wide unless otherwise noted.

file description:
file number: LOC2018-0170
pre-app: PE 2017-01564
Stage 1 Approved: LOC 2018-0036
Stage 2 Approved: LOC 2018-0036

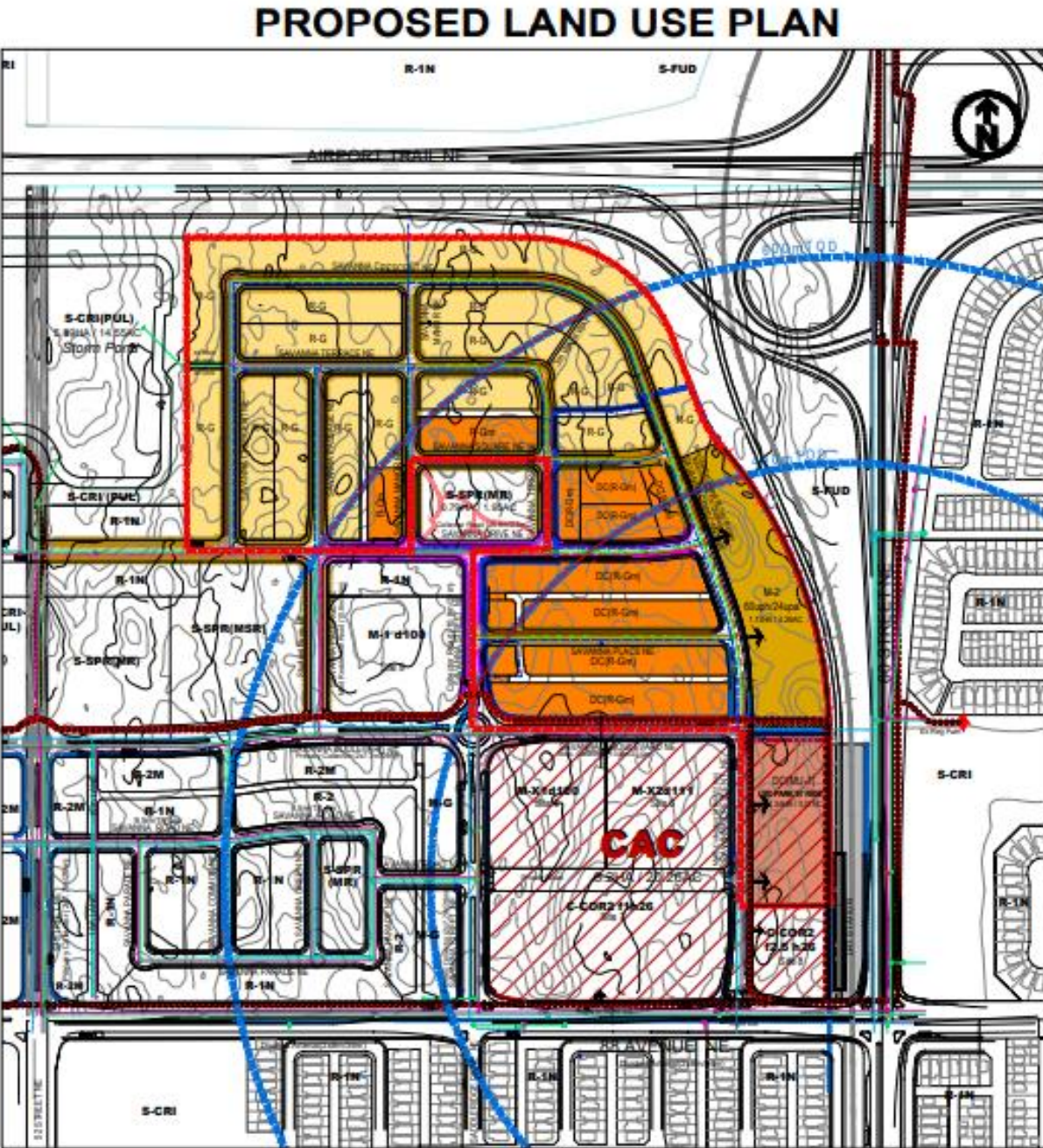
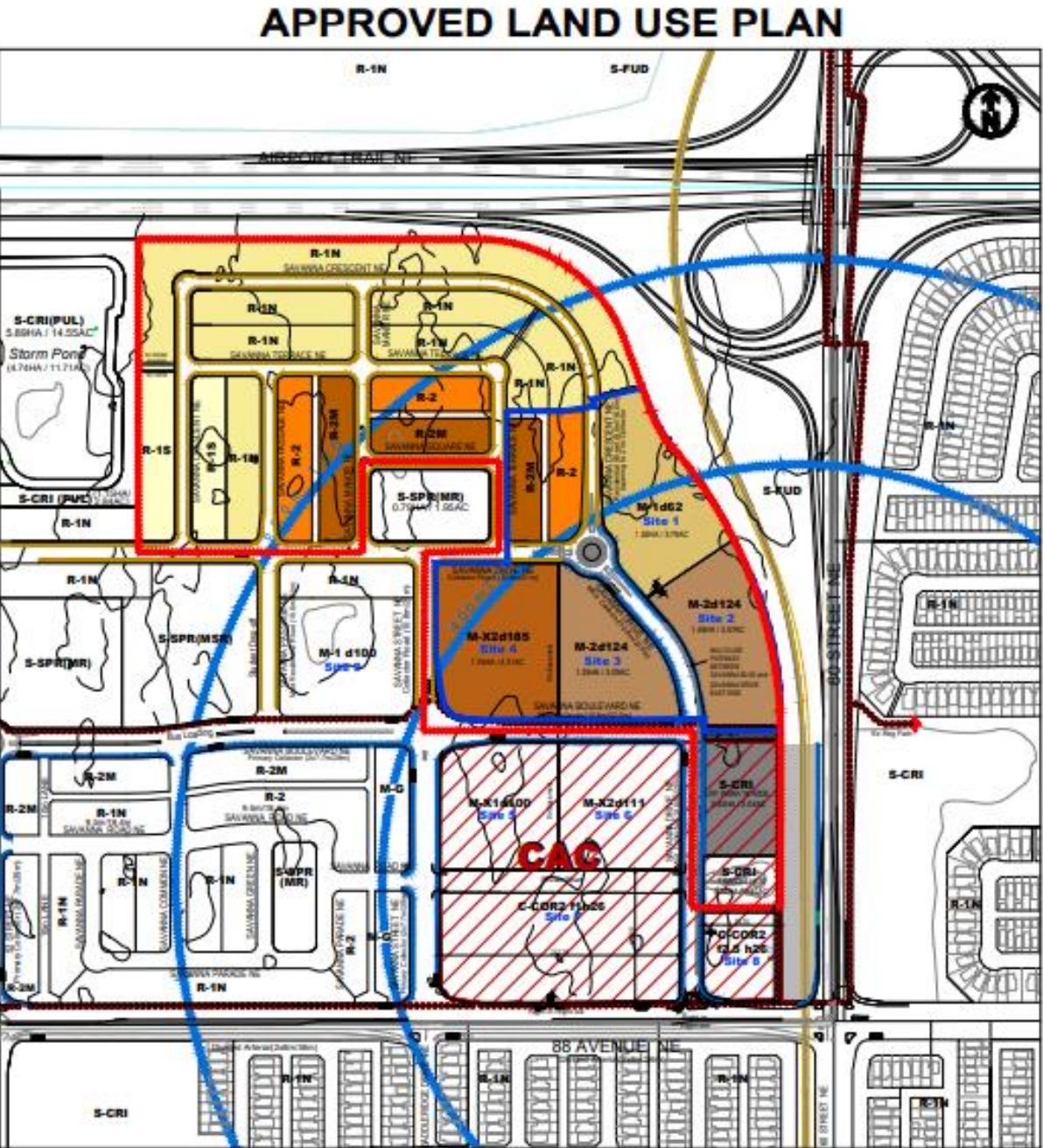
file info:
project no.: 1499c
drawn by: EB
start date: September, 2017
current date: Apr 02, 2019



sheet title:
Outline Plan & Land Use
Redesignation
Site 1

exhibit no.:

1.0



Land Use Boundary
Outline Plan Boundary

Subdivision Data Sheet

	HECTARES	ACRES
GROSS AREA OF PLAN	7.73	19.10
NET DEVELOPABLE AREA	7.73	19.10

LAND USE (Residential)	HECTARES	ACRES	# OF LOTS	# OF UNITS (FOR MULTI RESIDENTIAL)
R-G	0.54	1.33	19	
DC/R-Gm	3.52	8.69	206	
M-2	1.81	4.47		109
Total Residential	5.87	14.49	225	109

	HECTARES	ACRES	% OF NET AREA
ROADS (CREDIT)	1.86	4.60	24.1%

RESERVES	HECTARES	ACRES	% OF NET AREA
MR	0.00	0.00	0.0%
MSR	0.00	0.00	0.0%
MSR (NON-CREDIT)	0.00	0.00	0.0%

Saddle Ridge Community Association's Letter

27,7555 Falconridge Blvd. N.E.,
Calgary, Ab.,
T3J 0C9
Jan.17, 2019

Re: LOC2018-0170

The Saddle Ridge Community Association is in favour of the changes proposed in LOC2018-0170. Larger homes with more parking would suit the needs of the multigenerational families who choose to live in Saddle Ridge. This would lead to the area being built out sooner, and would be good for the retail and service providers in the neighbourhood.

We also are pleased to see the emphasis on transit oriented development around the future LRT station, and would hope for the extension of LRT service to this area in the not too distant future.

Respectfully submitted
Judy Brown
Land Use Committee
Saddle Ridge Community Association