



AGENDA

CALGARY PLANNING COMMISSION

October 18, 2018, 1:00 PM
IN THE COUNCIL CHAMBER
Members

Director M.Tita, Chair
Director R. Vanderputten, Vice-Chair
Councillor J. Gondek
Councillor E. Woolley
Commissioner M. Foht
Commissioner C. Friesen
Commissioner P. Gedye
Commissioner L. Juan
Commissioner A. Palmiere
Commissioner J. Scott
Mayor N. Nenshi

1. CALL TO ORDER
2. OPENING REMARKS
3. CONFIRMATION OF AGENDA
4. CONFIRMATION OF MINUTES
 - 4.1 Unconfirmed Minutes of the Regular Meeting of the Calgary Planning Commission, 2018
October 04
5. CONSENT AGENDA
None
6. POSTPONED REPORTS
(including related/ supplemental reports)
None

7. ITEMS FROM OFFICER, ADMINISTRATION AND COMMITTEES

7.1 DEVELOPMENT ITEMS

- 7.1.1 Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059, CPC2018-0968
- 7.1.2 Development Permit in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, DP2018-3108, CPC2018-1130

7.2 PLANNING ITEMS

- 7.2.1 New Policy: International Avenue Area Redevelopment Plan, CPC2018-1184
- 7.2.2 City Initiated Land Use Amendment - International Avenue (17 Avenue SE) - LOC2017-0371, CPC2018-1185
- 7.2.3 Land Use Amendment in Beddington Heights (Ward 4) at 1310 and 1312 Berkley Drive NW, LOC2018-0105, CPC2018-1147
- 7.2.4 Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114, CPC2018-1101
- 7.2.5 Land Use Amendment in Arbour Lake (Ward 2) at 20 Crowfoot Crescent NW, LOC2018-0133, CPC2018-1152
- 7.2.6 Land Use Amendment in West Hillhurst (Ward 7) at multiple properties, LOC2018-0159, CPC2018-1153
- 7.2.7 Land Use Amendment in Temple (Ward 10) at 3 Templemont Way NE, LOC2018-0132, CPC2018-1195
- 7.2.8 Land Use Amendment in Arbour Lake (Ward 2) at 35 Crowfoot Way NW, LOC2018-0142, CPC2018-1064
- 7.2.9 Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05) at multiple properties, LOC2017-0111, CPC2018-1103
- 7.2.10 Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP), CPC2018-1104
- 7.2.11 Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15 Street SW, LOC2018-0095, CPC2018-1205
- 7.2.12 Land Use Amendment in Beltline (Ward 8), Downtown Commercial Core and Downtown East Village (Ward 7), and Inglewood (Ward 9) at multiple properties, LOC2018-0089, CPC2018-1213

- 7.2.13 Land Use Amendment in Pine Creek (Ward 13) at 507 – 210 Avenue SW, LOC2018-0151, CPC2018-1151
- 7.2.14 Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 – 26 Avenue SW, LOC2018-0167, CPC2018-1113
- 7.2.15 Land Use Amendment in Valleyfield (Ward 9) at 4575 – 25 Street SE, LOC2018-0177, CPC2018-1112
- 7.2.16 Land Use Amendment in Section 23 (Ward 12) at 5758, 5820, 5920 and 6020 – 94 Avenue SE, LOC2018-0163, CPC2018-1202
- 7.2.17 Land Use Amendment in Inglewood (Ward 9) at 740 - 19 Street SE, LOC2018-0066, CPC2018-1179

7.3 MISCELLANEOUS ITEMS
None

8. URGENT BUSINESS

9. ADJOURNMENT



INDEX FOR THE 2018 OCTOBER 18 REGULAR MEETING OF CALGARY PLANNING COMMISSION

NOTE:
PLANS SUBMITTED TO THE COMMISSION, AS PART OF THE DEPARTMENTAL REPORTS,
ARE INCLUDED FOR THE SOLE PURPOSE OF ASSISTING THE COMMISSION IN MAKING
A DECISION AND ARE NOT TO BE USED FOR ANY OTHER PURPOSE.

ISC: Unrestricted



CALGARY
**PLANNING
COMMISSION**
CENTENNIAL

A CENTURY OF VISION 1911 - 2011

DEVELOPMENT ITEMS

ITEM NO.: 7.1.1 Christine Leung/Fraser McLeod
(Related to Item 7.1.2)

COMMUNITY: Bridgeland-Riverside (Ward 9)

FILE NUMBER: LOC2018-0059 (CPC2018-0968)

PROPOSED AMENDMENTS: Amendments to the Bridgeland/Riverside Area
Redevelopment Plan

PROPOSED REDESIGNATION: From: DC Direct Control District
To: Mixed Use – General (MU-1f5.3h50) District

MUNICIPAL ADDRESS: 1018 McDougall Road NE

APPLICANT: Casola Koppe

OWNER: The City of Calgary

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.1.2 Christine Leung/Fraser McLeod
(Related to Item 7.1.1)

COMMUNITY: Bridgeland-Riverside (Ward 9)

FILE NUMBER: DP2018-3108 (CPC2018-1130)

PROPOSED DEVELOPMENT: New: Retail and Consumer Service; Restaurant: Food
Service Only – Small; Restaurant: Licensed – Medium;
Outdoor Café; Dwelling Unit; Live Work Unit (1 building,
2 phases)

MUNICIPAL ADDRESS: 1018 McDougall Road NE

APPLICANT: Casola Koppe

OWNER: The City of Calgary

ADMINISTRATION RECOMMENDATION: **APPROVAL**

PLANNING ITEMS

ITEM NO.: 7.2.1
Jihad Bitar
(Related to Item 7.2.2)

COMMUNITY: Albert Park/Radisson Heights, Forest Lawn-Forest and Heights/Hubalta (Ward 9)

FILE NUMBER: CPC2018-1184

PROPOSED POLICY: International Avenue Area Redevelopment Plan

ADMINISTRATION RECOMMENDATION: **FOR INFORMATION ONLY**

ITEM NO.: 7.2.2
Jyde Heaven
(Related to Item 7.2.1)

COMMUNITY: Albert Park/Radisson Heights, Forest Lawn-Forest and Heights/Hubalta (Ward 9)

FILE NUMBER: LOC2017-0371 (CPC2018-1185)

PROPOSED REDESIGNATION: From: Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District, Multi-Residential – Contextual-Oriented (M-CG) District, Commercial – Corridor 2 (C-COR2) District, Special Purpose – Community Institution District (S-C1), Multi-Residential – Contextual Low Profile (M-C1) District, Commercial – Corridor 1 (C-COR1) District and DC Direct Control District

To: Residential – Grade-Oriented Infill (R-CG) District, Multi-Residential – Contextual-Oriented (M-CG) District, Multi-Residential – Contextual Low Profile (M-C1) District, Multi-Residential – High Density Low Rise (M-H1) District, Multi-Residential – Contextual Medium Profile (M-H2) District, Mixed Use - General (MU-1f3.5h22) District, Mixed Use - General (MU-1f4.5h20) District, Mixed Use - General (MU-1f2.0h12) District, Mixed Use - General (MU-1f3.0h46) District, Mixed Use - General (MU-1f5.0h37) District, Mixed Use - Active Frontage (MU-2f2.0h10) District, Mixed Use - Active Frontage (MU-2f3.0h23) District, Mixed Use - Active Frontage (MU-2f3.0h27) District, Mixed Use - Active Frontage (MU-2f4.5h20) District, Mixed Use - Active Frontage (MU-2f3.0h46) District, Mixed Use - Active Frontage (MU-2f4.5h23) District, Mixed Use - Active Frontage (MU-2f5.0h26) District and Mixed Use - Active Frontage (MU-2f4.5h20) District.

MUNICIPAL ADDRESS: Various addresses

APPLICANT: The City of Calgary

OWNER: Various owners

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.3
COMMUNITY: Coleen Auld
FILE NUMBER: Beddington Heights (Ward 4)
PROPOSED REDESIGNATION: LOC2018-0105 (CPC2018-1147)
From: Residential – Contextual One/Two Dwelling (R-C2) District
To: DC Direct Control District based on the Residential – Contextual One/Two Dwelling (R-C2) District to accommodate a Child Care Service
MUNICIPAL ADDRESS: 1310 and 1312 Berkley Drive NW
APPLICANT: Rick Balbi Architect
OWNER: Little Scholars Daycare Inc
Gargi Devi Saini
ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.4
COMMUNITY: Shane Gagnon
FILE NUMBER: Hillhurst (Ward 7)
PROPOSED POLICY AMENDMENTS: LOC2018-0114 (CPC2018-1101)
Amendments to the Hillhurst/Sunnyside Area Redevelopment Plan
PROPOSED REDESIGNATION: From: Commercial – Corridor 1 (C-COR1f2.8h13) District and Multi-Residential – Contextual Grade-Oriented (M-CGd72) District
To: DC Direct Control District to accommodate multi-residential development and the preservation of a heritage building
MUNICIPAL ADDRESS: 1110, 1114, 1120, 1124, 1126 Gladstone Road NW
APPLICANT: Battistella Developments
OWNER: Churchstone Enterprises Limited
Elaine Lai-King Wong
David Wong
Midelco Holdings Limited
ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.5 Shane Gagnon
COMMUNITY: Arbour Lake (Ward 2)
FILE NUMBER: LOC2018-0133 (CPC2018-1152)
PROPOSED REDESIGNATION: From: DC Direct Control District
To: Commercial – Community 1 (C-C1) District
MUNICIPAL ADDRESS: 20 Crowfoot Crescent NW
APPLICANT: Bart Carswell
OWNER: Artis Crowfoot Village Ltd
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 7.2.6 Shane Gagnon
COMMUNITY: West Hillhurst (Ward 7)
FILE NUMBER: LOC2018-0159 (CPC2018-1153)
PROPOSED REDESIGNATION: From: DC Direct Control District
To: Mixed Use – General (MU-1f2.0h10) District
MUNICIPAL ADDRESS: 106 and 110 - 19 Street NW and 1982 Kensington Road NW
APPLICANT: Mediated Solutions
OWNER: Kensington Village Holdings Ltd
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 7.2.7
COMMUNITY: Jennifer MacLaren
FILE NUMBER: Temple (Ward 10)
PROPOSED REDESIGNATION: LOC2018-0132 (CPC2018-1195)
From: Residential – Contextual One Dwelling (R-C1) District
To: Residential – Contextual One / Two Dwelling (R-C2) District
MUNICIPAL ADDRESS: 3 Templemont Way NE
APPLICANT: Sarbjit Singh Viridi
OWNER: Sarbjit Singh Viridi
Gurmohan Singh Braich
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 7.2.8
COMMUNITY: David Mulholland
FILE NUMBER: Arbour Lake (Ward 2)
PROPOSED REDESIGNATION: LOC2018-0142 (CPC2018-1064)
From: DC Direct Control District
To: Commercial – Community 2 f1.0h18 (C-C2f1.0h18) District
MUNICIPAL ADDRESS: 35 Crowfoot Way NW
APPLICANT: BCW Architects
OWNER: Calgary Co-operative Association Limited
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 7.2.9 Morgan Huber
(related to Item 7.2.10)

COMMUNITY: Residual Sub-Area 5G (Ward 05)

FILE NUMBER: LOC2017-0111 (CPC2018-1103)

PROPOSED POLICY AMENDMENTS: Amendments to the East Stoney Area Structure Plan

PROPOSED REDESIGNATION: From: Special Purpose – Future Urban Development (S-FUD) District and Special Purpose – Transportation and Utility Corridor (S-TUC) District

To: Residential – Low Density Mixed Housing (R-G) (R-Gm) District, Residential Manufactured Home (R-MH) District, Multi-Residential – At Grade Housing (M-G) District, Commercial Community 1 (C-C1) District, Industrial (I-G) District, Special Purpose – Community Service (S-CRI) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, and Special Purpose – Urban Nature (S-UN) District

MUNICIPAL ADDRESS: 7055, 7111, 7697, 8393 - 84 Street NE

APPLICANT: Urban Systems Survey

OWNER: Pacific Calgary GP Ltd

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.10 Morgan Huber
(related to Item 7.2.9)

COMMUNITY: Residual Sub-Area 5G (Ward 05)

FILE NUMBER: LOC2017-0111(OP) (CPC2018-1104)

PROPOSED OUTLINE PLAN: Subdivision of 96.15 hectares ± (237.59 acres ±)

MUNICIPAL ADDRESS: 7055, 7111, 7697, 8393 - 84 Street NE

APPLICANT: Urban Systems Survey

OWNER: Pacific Calgary GP Ltd

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.11 Tom Schlodder

COMMUNITY: Bankview (Ward 8)

FILE NUMBER: LOC2018-0095

PROPOSED POLICY AMENDMENT: Amendment to the Bankview Area Redevelopment Plan

PROPOSED REDESIGNATION: From: Residential Contextual One / Two Dwelling (R-C2) District
To: Multi-Residential – Contextual Grade Orientated (M-CG) District

MUNICIPAL ADDRESS: 2502 - 15 Street SW

APPLICANT: Inertia

OWNER: Huijin Ouyang
Bijun Wu

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.12 Richard Goecke

COMMUNITY: Beltline (Ward 8), Downtown Commercial Core, Downtown East Village (Ward 7), and Inglewood (Ward 9)

FILE NUMBER: LOC2018-0089 (CPC2018-1213)

PROPOSED REDESIGNATION: From: Special Purpose – City and Regional Infrastructure (S-CRI) District
To: DC Direct Control District based on Special Purpose – City and Regional Infrastructure with the additional discretionary uses to accommodate digital third party advertising signs on railway overpasses owned by the Canadian Pacific Railway within the Centre City

MUNICIPAL ADDRESS: 115R - 9 Avenue SW, 1797 - 17 Avenue SE, 221R - 9 Avenue SE, 537 – 9 Avenue SE, 830R – 10 Avenue SW and 915 – 4 Street SW

APPLICANT: Van Horne Outdoor Limited Partnership

OWNER: Canadian Pacific Railway

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.13 Kristen Wishlow

COMMUNITY: Pine Creek (Ward 13)

FILE NUMBER: LOC2018-0151 (CPC2018-1151)

PROPOSED REDESIGNATION: From: Residential - One Dwelling (R-1s) District
To: Residential – Low Density Mixed Housing (R-G) District

MUNICIPAL ADDRESS: 507 – 210 Avenue SW

APPLICANT: Stantec Consulting

OWNER: United Lands Corp
Catherine A. Duffin
Timothy A. Duffin

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.14 Jarred Friedman

COMMUNITY: Richmond (Ward 8)

FILE NUMBER: LOC2018-0167 (CPC2018-1113)

PROPOSED POLICY AMENDMENTS: Amendment to Richmond Area Redevelopment Plan

PROPOSED REDESIGNATION: From: Residential – Contextual One/Two Dwelling (R-C2) District
To: Mixed Use – Active Frontage (MU-2f1.0h10) District

MUNICIPAL ADDRESS: 2104 - 26 Avenue SW

APPLICANT: Arup Datta Architecture Ltd

OWNER: Kasa Enterprises Inc

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.15
COMMUNITY: Mona Ha
FILE NUMBER: Valleyfield (Ward 9)
PROPOSED REDESIGNATION: LOC2018-0177 (CPC2018-1112)
From: DC Direct Control District
To: Commercial Corridor 3 f2.0h16 (C-COR3 f2.0h16) District
MUNICIPAL ADDRESS: 4575 – 25 Street SE
APPLICANT: Certus Developments Inc
OWNER: Certus Developments Inc
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 7.2.16
COMMUNITY: Angelique Dean
FILE NUMBER: Section 23 (Ward 12)
PROPOSED REDESIGNATION: LOC2018-0163 (CPC2018-1202)
From: DC Direct Control District
To: DC Direct Control District to accommodate the additional use of Asphalt, Aggregate and Concrete Plant use
MUNICIPAL ADDRESS: 5758, 5820, 5920 and 6020 - 94 Avenue SE
APPLICANT: Stantec Consulting
OWNER: Powell Chuckwagon Racing Inc
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 7.2.17

Lisette Burga Ghersi

COMMUNITY:

Inglewood (Ward 9)

FILE NUMBER:

LOC2018-0066 (CPC2018-1179)

PROPOSED REDESIGNATION:

From: Commercial Corridor 3 f2.8h12 (C-COR3
f2.8h12) District

To: DC Direct Control District to accommodate a
mixed use development

MUNICIPAL ADDRESS:

740 - 19 Street SE

APPLICANT:

B&A Planning Group

OWNER:

CANHUSK Real Estate ULC

ADMINISTRATION RECOMMENDATION:

APPROVAL



MINUTES

CALGARY PLANNING COMMISSION

**October 4, 2018, 1:00 PM
IN THE COUNCIL CHAMBER**

PRESENT: Director M. Tita, Chair
Director R. Vanderputten, Vice-Chair
Councillor J. Gondek
Councillor E. Woolley
Commissioner C. Friesen
Commissioner P. Gedye
Commissioner L. Juan
Commissioner J. Scott

ALSO PRESENT: Principal Planner I. Cope
Senior Planner K. Melanson
Legislative Assistant J. Palaschuk
Acting CPC Secretary T. Rowe

1. CALL TO ORDER

Director Tita called the meeting to order at 1:02 p.m.

2. OPENING REMARKS

Director Tita provided opening remarks at today's Meeting.

3. CONFIRMATION OF AGENDA

Moved by Director Vanderputten

That the Agenda for today's meeting be amended by bringing forward item 7.2.10, Report CPC2019-1003 to be dealt with immediately following the Consent Agenda.

MOTION CARRIED

Moved by Commissioner Scott

That the Agenda for the 2018 October 04 Regular Meeting of the Calgary Planning Commission be confirmed, **as amended**.

MOTION CARRIED

4. CONFIRMATION OF MINUTES

The following clerical corrections were noted to the Minutes of the 2018 September 06 Meeting of the Calgary Planning Commission:

The Minutes of the 2018 September 06 be corrected by deleting the words “Hold a public hearing during a 2018 October Public Hearing of Council” and replacing with the words “Hold a public hearing during a 2018 November Public Hearing of Council” in Recommendation 1 of the following Reports, respectively:

1. Item 5.1.7. Land Use Amendment in Albert Park/Radisson Heights (Ward 9) at 2736 - 12 Avenue SE, LOC2018-0149, CPC2018-0995;
2. Item 5.1.8. Land Use Amendment in Albert Park/Radisson Heights (Ward 9) at 903 - 35 Street SE, LOC2018-0160, CPC2018-1014;
3. Item 5.1.9. Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1912 – 40 Avenue SW, LOC2018-0126, CPC2018-0963; and
4. Item 5.1.10. Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 4929 – 20 Street SW, LOC2018-0128, CPC2018-0992;

4.1 Unconfirmed Minutes of the 2018 September 20 Calgary Planning Commission

The following clerical corrections were noted to the Minutes of 2018 September 20 Meeting of the Calgary Planning Commission:

- Page 2 of 7, Item 5.1, CPC2018-1010, Recommendations 1 and 3, by adding the words "**Area A**" immediately following the words "Plan 1810992", in both Recommendations respectively.

Moved by Commissioner Juan

That the **Corrected** Minutes of the Regular Meeting of the Calgary Planning Commission, held on September 20, be confirmed.

MOTION CARRIED

5. CONSENT AGENDA

- 5.1 Street Names in Belvedere (Ward 9) at 8 and 9 Avenue, 100 Street 26 Avenue Alignment and Stoney Trail and 84 Street SE, SN2017-0002, CPC2018-0286

A revised Cover Report to Report CPC2018-0286 was distributed.

Moved by Councillor Gondek

That with respect to **revised** Report CPC2018-0286, the following be approved, **as amended**:

That the Calgary Planning Commission recommends that Council:

1. Adopt, by Resolution, the proposed street names of “Belwood” “Belhaven”; and
2. **File the Administration recommendation of REFUSAL of the proposed street names of "Belvedere" and "Belview" and ADOPT, by resolution, the proposed street names of "Belvedere" and "Belview".**

Against: Director Vanderputten

MOTION CARRIED

- 5.2 Community Name and Street Names in Residual Sub - Area 12F and 12H (Ward 12) at 186 Avenue SE, 88 Street SE, 210 Avenue SE and 52 Street SE, SN2018-0006, CPC2018-1085

The following clerical correction was noted to Report CPC2018-1085:

Page 1 of 3, Executive Summary, first paragraph, first sentence, by deleting a space between the word "Section" and the number "23"

Moved by Commissioner Scott

That with respect to Report CPC2018-1085, the following be approved:

That the Calgary Planning Commission recommends that Council:

1. Adopt, by Resolution, the proposed community name Rangeview; and
2. Adopt, by Resolution, the proposed street names Rangeview, Bartlett, Gala, Heirloom, Lavender, Savoy and Sugarsnap.

MOTION CARRIED

- 5.3 Community Name and Street Names in Residual Sub-Area 5G (Ward 5) for Homestead, SN2018-0008, CPC2018-1122

Moved by Commissioner Scott

That with respect to Report CPC2018-1122, the following be approved:

That the Calgary Planning Commission recommends that Council:

1. Adopt, by Resolution, of the proposed community name of Homestead; and
2. Adopt, by Resolution, proposed street names of Homestead, Homeridge, Homecrest, Homewood, Homecroft and Homeglen.

MOTION CARRIED

6. POSTPONED REPORTS

None

7. ITEMS FROM OFFICER, ADMINISTRATION AND COMMITTEES

7.1 DEVELOPMENT ITEMS

- 7.1.1 Development Permit in Chinatown (Ward 7) at 117, 121, 123, 125, and 129 - 2 Avenue SW and 116, 120, 124, 130 and 134 - 3 Avenue SW, DP2018-2769, CPC2018-1093

Commissioner Juan declared a pecuniary interest, and abstained from discussion and voting, with respect to the following:

- Item 7.1.1, Report CPC2018-1093

Commissioner Juan left the Council Chamber at 2:23 p.m. and returned at 4:02 p.m., after the vote on Report CPC2018-1093 was declared.

The following clerical corrections were noted with respect to Report CPC2018-1093:

- On page 2 of 12, in the recommendations section, by capitalizing the letter "u" in dwelling units and by placing a space between the hyphen between the words 'licensed' and 'medium' ;
- On page 7 of 12, in the site access and parking section, by deleting the number "46" and substituting the number "67"; and
- On page 11 of 12, in the Bylaw relaxations table, by deleting the number '340' and substituting with the number '337' and by deleting the number '53' and substituting with the number '56'.

Distributions with respect to Report CPC2018-1093:

- a letter from the Chinatown District BIA, dated 2018 October 02;
- a letter from Calgary Economic Development, dated 2018 September 06;
- a letter from Tourism Calgary, dated 2018 September 18;
- a revised attachment 1 of two additional page 'site plan map including exterior materials examples' to the end of the attachment; and
- a document entitled "Chinatown Development Community Engagement Summary".

The Commission, by general consent, recessed to the Call of the Chair at 2:42 p.m. in order to read the letters that were distributed with respect to Item 7.1.1. Commission resumed at 2:50 p.m. with Director Tita in the chair.

Commission recessed at 3:45, by general consent, and resumed at 4:02 with Director Tita in the chair.

Moved by Commissioner Scott

That the letters distributed in respect to Report CPC2018-0286 be forwarded onto Council as an attachment of the report.

MOTION CARRIED

Moved by Commissioner Scott

That the letters distributed in respect to Report CPC2018-0286 be accepted by the Calgary Planning Commission.

MOTION CARRIED

Moved by Director Vanderputten

That **revised** Attachment 2 be amended as follows:

1. On page 2 of 8, in Section 3 (a) by adding the words "and cyclist" after 'pedestrian'
2. On page 4 of 9, in Section 14, by deleting the word 'vehicular' following the words "internal pedestrian and" and substitute with "emergency vehicle"
3. On page 4 of 9, in Section 16 by deleting the word 'woonerf/' following the words "adequate lighting in"

4. On page 5 of 9, in Section 22(a) by deleting the following sentence “Construction of 4 new driveway crossings on 2nd Avenue SW (2) & 3rd Avenue SW (2)” and replace with “Construction of 2 new driveway crossings on 2nd Avenue SW (1) & 3rd Avenue SW (1)”

MOTION CARRIED

Moved by Director Vanderputten

That **revised** Attachment 2, as amended, be further amended by the following:

On page 4 of 9, in Section 18, by adding the following sentence “Design features to prevent unauthorized vehicular access while accommodating emergency vehicle access should be incorporated in the design (e.g. rolled curb, removable bollards, etc.). Amend the Plan to the satisfaction of the Director, Transportation Planning.” following the words “(Drawing A01-05, viewpoint #2).”

Against: Commissioner Gondek

MOTION CARRIED

Moved by Commissioner Gedye

That with respect to **revised** Report CPC2018-1093, the following be approved, **as amended**:

That Calgary Planning Commission:

1. Receive and accept this report and attachments for information; and
2. Recommend the Development Authority, without having to return to Calgary Planning Commission, approve Development Permit DP2018-2769 of a New: Retail and Consumer Service, Restaurant: Licensed - Medium (3), Hotel, and Dwelling Units, at 117, 121, 123, 125, and 129 2 Avenue SW and 116, 120, 124, 130, 134 3 Avenue SW (Plan C; Block 8; Lots 6 to 12 and 28 to 40) with conditions (**amended** Attachment 2), subject to the approval of the bylaw amendment associated with LOC2012-0101 by Council.

MOTION CARRIED

- 7.1.2 Development Permit in Centre City East Village (Ward 7) at 399 - 9 Avenue SE, DP2018-2774, CPC2018-1119

A revised Attachment 2 respect to Report CPC2018-1119 was distributed.

Speakers:

1. Carolyn Huddock; and
2. Ken Batista

Moved by Commissioner Juan

That with respect to Report CPC2018-1119, the following be approved:
That the Calgary Planning Commission, **as amended**:

1. Receive and accept this report and attachments for information; and
2. Recommend the Development Authority, without having to return to Calgary Planning Commission, approve Development Permit DP2018-2774 of a New: Parking Lot - Structure, Office, Instructional Facility, General Industrial - Light, Restaurant: Neighbourhood at 399 9 Avenue SE (Portion of Plan 1711864, Block 3, Lot 5) with conditions (**revised** attachment 2), subject to the approval of the bylaw amendment associated with LOC2015-0170 by Council.

MOTION CARRIED

7.1.3 Development Permit in Red Carpet (Ward 9) at 901 – 68 Street SE, DP2018-3019, CPC2018-1088

Moved by Commissioner Scott

That with respect to Report CPC2018-1088, the following be approved:
That Calgary Planning Commission:

1. Approve the proposed development permit application for New: Multi-Residential Development (8 buildings, 9 phases); Sign - Class A: Address Sign (2) at 901 – 68 Street SE (Plan 1612085, Block 8, Lot 1) with conditions (Attachment 2).

Against: Commissioner Friesen

MOTION CARRIED

7.2 PLANNING ITEMS

7.2.1 Policy Amendment, Road Closure and Land Use Amendment in Belvedere (Ward 9) at 1550 - 84 Street SE, LOC2016-0189, CPC2018-1123

A revised Cover Report to Report CPC2018-1123 was distributed.

Moved by Commissioner Juan

That with respect to **revised** Report CPC2018-1123, the following be approved, **after amendment**:

That Calgary Planning Commission recommend that Council:

1. **Hold a Public Hearing during a future Public Hearing of Council;**
2. **Adopt, by Bylaw, the proposed amendments to the *Belvedere Area Structure Plan*;**
3. Give three reading to the proposed Bylaw;
4. Adopt, by resolution, the proposed revisions to the Appendix section of the *Belvedere Area Structure Plan*, that does not form part of Bylaw 2P2013 (Attachment 5);
5. Adopt the road closure Bylaw, to close 0.07 hectares ± (0.17 acres ±) of road (Plan 1811693, Area A and Area B), adjacent to 17 Avenue SE, with conditions (Attachment 2);

6. Give three readings to the proposed road closure Bylaw;
7. Adopt, by Bylaw, the proposed redesignation of 50.14 hectares ± (123.89 acres ±) located at 1550 - 84 Street SE and the closed road (Plan 1013597; Block 1, Lot 2; Plan 1811693, Area A and Area B) from Special Purpose – Future Urban Development (S-FUD) District, and Undesignated Road Right-of-Way **to** Residential – One Dwelling (R-1s) District, Residential – Low Density Mixed Housing (R-G) (R-Gm) District, Residential – Low Density Multiple Dwelling (R-2M) District, Multi-Residential – At Grade Housing (M-G) District, Multi-Residential – Medium Profile (M-2) District, Multi-Residential – High Density Low Rise (M-H1) District, Commercial – Neighborhood 1 (C-N1) District, Commercial – Community 2 (C-C2) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – Urban Nature (S-UN) District, Special Purpose – City and Regional Infrastructure (S-CRI) District, and Special Purpose – Recreational (S-R) District; and
8. Give three readings to the proposed Bylaw.

MOTION CARRIED

- 7.2.2 Outline Plan in Belvedere (Ward 9) at 1550 - 84 Street SE, LOC2016-0189, CPC2018-1124

Moved by Director Vanderputten

That Attachment 2 with Respect to Report CPC2018-1124 be amended by Deleting condition 32, on page 5 of 10, in its entirety and renumbering accordingly.

MOTION CARRIED

Moved by Commissioner Juan

That with respect to Report CPC2018-1124, the following be approved, **after amendment:**

That the Calgary Planning Commission recommends that Council:

1. Approve the proposed outline plan to subdivide 50.14 hectares ± (123.89 acres ±) at 1550 - 84 Street SE (Plan 1013597, Block1, Lot 2) with conditions (**amended** Attachment 2).

MOTION CARRIED

- 7.2.3 Road Closure and Land Use Amendment in Residual Sub-Area 12H (Ward 12) at 19019 - 88 Street SE, LOC2017-0345, CPC2018-1075

A revised Cover Report to Report CPC2018-1075 was distributed.

A revised Attachment 6 to Report CPC2018-1075 was distributed.

A revised Attachment 7 to Report CPC2018-1075 was distributed.

A revised Attachment 8 to Report CPC2018-1075 was distributed.

Speakers:

1. Brad Wright; and
2. Bela Syal

Moved by Commissioner Juan

That with respect to Report CPC2018-1075, the following be approved, **after amendment:**

That Calgary Planning Commission recommends that Council:

1. **Hold a Public Hearing during a future Public Hearing of Council;**
2. Adopt, by Bylaw, the road closure to close a 1.54 hectare \pm (3.81 acre \pm) portion of road east of the SE1/4 Section 22, Township 22, Range 29, west fourth Meridian and west of the SW1/4 Section 23, Township 22, Range 29, west fourth Meridian that lies south of the northerly 25.20 (twenty five and two tenths) in perpendicular width and north of the southerly 15.00 metres (fifteen) in perpendicular width, **with conditions (attachment 5);**
3. Give three readings to the proposed Bylaw;
4. Adopt, by Bylaw, the proposed redesignation of 93.3 hectares \pm (230.7 acres \pm) of a portion of 19019 - 88 Street SE (SW1/4 Section 23-22-29-4; portion of SE1/4 Section 23-22-29-4; portion of road east of the SE1/4 Section 22, Township 22, Range 29, west fourth Meridian and west of the SW1/4 Section 23, Township 22, Range 29, west fourth Meridian that lies south of the northerly 25.50 (twenty five and two tenths) in perpendicular width and north of the southerly 15.00 metres (fifteen) in perpendicular width) from Special Purpose – Future Urban Development (S-FUD) District and Undesignated Road Right-of-Way to Residential – Low Density Mixed Housing (R-G)(R-Gm) District, Multi-Residential – Low Profile (M-1) District, Multi-Residential – Medium Profile (M-2) District, Mixed Use – General (MU-1) District, Mixed Use – Active Frontage (MU-2) District, Special Purpose – Community Service (S-CS) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – Urban Nature (S-UN) District, Special Purpose – City and Regional Infrastructure (S-CRI) District and DC Direct Control District to allow for Resident's Facility and associated uses, with guidelines (Attachment 6);
5. Give three readings to the proposed Bylaw;
6. Adopt, by bylaw, the proposed redesignation of 93.3 hectares \pm (230.7 acres \pm) of a portion of 19019 - 88 Street SE (portion of S1/2 Section 23-22-29-4) from Special Purpose – Future Urban Development (S-FUD) District to DC Direct Control District to allow for food growing and associated uses, with guidelines (Attachment 7);
7. Give three readings to the proposed Bylaw;
8. Adopt, by Bylaw, the proposed redesignation of 93.3 hectares \pm (230.7 acres \pm) of a portion of 19019 - 88 Street SE (portion of SW1/4 Section 23-22-29-4) from Special Purpose – Future Urban Development (S-FUD) District to DC Direct Control District based on M-G, with guidelines (Attachment 8);
9. Give three readings to the proposed Bylaw;

10. Adopt, by Bylaw, the proposed redesignation of 37.0 hectares ± (91.4 acres ±) of a portion of 19019 - 88 Street SE (Portion of SE1/4 Section 23-22-29-4) from Special Purpose – Future Urban Development (S-FUD) District to Residential – Low Density Mixed Housing (R-G) (R-Gm) District, Multi-Residential – Medium Profile (M-2) District, Commercial – Neighbourhood 1(C-N1) District, Special Purpose – School, Park and Community Reserve (S-SPR) District and DC Direct Control District to allow for Food Growing and associated uses;
11. Give first reading to the proposed Bylaw; and
12. Withhold second and third readings until confirmation is received from the Regulator that the sour gas pipeline has been abandoned.

MOTION CARRIED

7.2.4 Outline Plan in Residual Sub-Area 12H (Ward 12) at 19019 - 88 Street SE, LOC2017-0345(OP), CPC2018-1074

A revised Attachment 1 to Report CPC2018-1074 was distributed.

Moved by Director Vanderputten

That **revised** Attachment 1 with respect to Report CPC2018-1074 be amended by the following:

On page 10, by deleting and replacing condition **57** with the following:

“In conjunction with the applicable Tentative Plan, the developer shall provide a Letter of Credit for pedestrian-actuated crossing signals that are agreed upon by the developer and the Director, Transportation Planning. Pedestrian-actuated crossing signals shall be considered to the satisfaction of the Director, Transportation Planning:

- where regional pathways or multi-use pathways intersect with the street;
- at mid-block crossings; and
- Intersection of Rangeview Drive and Heirloom Boulevard, and the Intersection of Rangeview Avenue and Bartlett Street.

The Developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit, and is required to submit payment in support of the proposed Tentative Plan applications.”

MOTION CARRIED

Moved by Commissioner Juan

That with respect to Report CPC2018-1074, the following be approved, **as amended**:

That Calgary Planning Commission:

1. Approve the proposed outline plan located at 19019 - 88 Street SE (S1/2 of Section 23-22-29-4) to subdivide 130.3 hectares \pm (322.1 acres \pm) with conditions (**revised** Attachment 1, **as amended**).

MOTION CARRIED

- 7.2.5 Land Use Amendment in Silverado (Ward 13) at 19369 Sheriff King Street SW, LOC2018-0141, CPC2018-1079

Moved by Councillor Woolley

That with respect to Report CPC2018-1079, the following be approved, **after amendment**:

That Calgary Planning Commission recommend that Council:

1. **Hold a Public Hearing during a future Public Hearing of Council;**
2. Adopt, by Bylaw, the proposed redesignation of 4.64 hectares \pm (11.44 acres \pm) located at 19369 Sheriff King Street SW (Plan 0812861, Block 22, Lot 4) from DC Direct Control District to Commercial – Community 2 (C-C2f0.5h18) District; and
3. Give three readings to the proposed Bylaw.

MOTION CARRIED

- 7.2.6 Land Use Amendment in Bridgeland-Riverside (Ward 9) at 118 - 8 Street NE, LOC2018-0156, CPC2018-1083

Moved by Councillor Woolley

That with respect to Report CPC2018-1083, the following be approved, **after amendment**:

That Calgary Planning Commission recommend that Council:

1. **Hold a Public hearing during a future Public Hearing of Council;**
2. Adopt, by Bylaw, the proposed redesignation of 0.20 hectares \pm (0.49 acres \pm) located at 118 - 8 Street NE (Condominium Plan 0713392) from DC Direct Control District to Mixed Use – Active Frontage (MU-2f3.0h16) District; and
3. Give three readings to the proposed Bylaw.

MOTION CARRIED

- 7.2.7 Land Use Amendment in Albert Park/Radisson Heights (Ward 9) at 1122 and 1124 – 35 Street SE, LOC2018-0179, CPC2018-1090

Moved by Commissioner Scott

That with respect to Report CPC2018-1090, the following be approved, **after amendment**:

That Calgary Planning Commission recommends that Council:

1. **Hold a Public Hearing during a future Public Hearing of Council;**
2. Adopt, by Bylaw, the proposed redesignation of 0.06 hectares \pm (0.14 acres \pm) located at 1122 and 1124 – 35 Street SE (Plan 5498T, Block 8, Lots 9 and 10) from Residential – Contextual One / Two Dwelling

- (R-C2) District to Residential – Grade Oriented Infill (R-CG) District;
and
3. Give three readings to the proposed Bylaw.

MOTION CARRIED

- 7.2.8 Land Use Amendment in Residual Sub - Area 9K (Ward 9) at 7007 – 84 Street SE, LOC2018-0155, CPC2018-1078

Moved by Commissioner Scott

That with respect to Report CPC2018-1078, the following be approved,
after amendment:

That Calgary Planning Commission recommend that Council:

1. **Hold a Public Hearing during a future Public Hearing of Council;**
2. Adopt, by Bylaw, the proposed redesignation of 1.85 ± hectares (4.57 acres±) located at 7007 – 84 Street SE (Portion of Plan 0910744, Block 2, Lot 2) from DC Direct Control District to DC Direct Control District to allow for Waste Disposal and Treatment Facility; and
3. Give three readings to the proposed Bylaw.

MOTION CARRIED

- 7.2.9 Land Use Amendment in Rosemont (Ward 4) at 5 Cambrian Drive NW, LOC2018-0173, CPC2018-1084

The following clerical correction was noted with respect to Report CPC2018-1084:

- On page 1 of 6, Background, first paragraph, by deleting the word "Rosewood" and replacing with the word "Rosemont"

Moved by Commissioner Gedye

That with respect to Report CPC2018-1084, the following be approved,
after amendment:

That Calgary Planning Commission recommend that Council:

1. **Hold a Public Hearing during a future Public Hearing of Council;**
2. Adopt, by Bylaw, the proposed redesignation of 0.07 hectares ± (0.16 acres ±) located at 5 Cambrian Drive NW (Plan 5971HB, Block 15, Lot 8) from Residential – Contextual One Dwelling (R-C1) District to Residential – Contextual One / Two Dwelling (R-C2) District; and
3. Give three readings to the proposed Bylaw.

MOTION CARRIED

- 7.2.10 New Policy: Chinook Station Area Redevelopment Plan (Wards 9 and 11), CPC2018-1003

A letter from BCI, dated 2018 October 02, was distributed, with respect to Report CPC2018-1003.

A revised Attachment 1 was distributed, with respect to Report CPC2018-1003

A document entitled "Item 7.2.10 Chinook Station Area Redevelopment Plan – CPC Discussion Summary" was received for the Corporate Record

Moved by Councillor Woolley

That with respect to Report CPC2018-1003, the following be approved, **after amendment**:

1. That Calgary Planning Commission receive this report for information.
2. **That Calgary Planning Commissioner comments be received for information**

MOTION CARRIED

7.3 MISCELLANEOUS ITEMS

None

8. URGENT BUSINESS

None.

9. ADJOURNMENT

Moved by Councillor Woolley

That this Meeting adjourn at 6:33 p.m.

MOTION CARRIED

THE FOLLOWING ITEMS HAVE BEEN FORWARDED TO THE DECEMBER 10 PUBLIC HEARING OF COUNCIL:

- Street Names in Belvedere (Ward 9) at 8 and 9 Avenue, 100 Street 26 Avenue Alignment and Stoney Trail and 84 Street SE, SN2017-0002, CPC2018-0286
- Community Name and Street Names in Residual Sub - Area 12F and 12H (Ward 12) at 186 Avenue SE, 88 Street SE, 210 Avenue SE and 52 Street SE, SN2018-0006, CPC2018-1085
- Community Name and Street Names in Residual Sub-Area 5G (Ward 5) for Homestead, SN2018-0008, CPC2018-1122
- Policy Amendment, Road Closure and Land Use Amendment in Belvedere (Ward 9) at 1550 - 84 Street SE, LOC2016-0189, CPC2018-1123
- Outline Plan in Belvedere (Ward 9) at 1550 - 84 Street SE, LOC2016-0189, CPC2018-1124
- Road Closure and Land Use Amendment in Residual Sub-Area 12H (Ward 12) at 19019 - 88 Street SE, LOC2017-0345, CPC2018-1075
- Outline Plan in Residual Sub-Area 12H (Ward 12) at 19019 - 88 Street SE, LOC2017-0345(OP), CPC2018-1074
- Land Use Amendment in Silverado (Ward 13) at 19369 Sheriff King Street SW, LOC2018-0141, CPC2018-1079
- Land Use Amendment in Bridgeland-Riverside (Ward 9) at 118 - 8 Street NE, LOC2018-0156, CPC2018-1083
- Land Use Amendment in Albert Park/Radisson Heights (Ward 9) at 1122 and 1124 – 35 Street SE, LOC2018-0179, CPC2018-1090

- Land Use Amendment in Residual Sub - Area 9K (Ward 9) at 7007 – 84 Street SE, LOC2018-0155, CPC2018-1078
- Land Use Amendment in Rosemont (Ward 4) at 5 Cambrian Drive NW, LOC2018-0173, CPC2018-1084

CHAIR

ACTING CPC SECRETARY

UNCONFIRMED

Planning & Development Report to
Calgary Planning Commission
2018 October 18

ISC: UNRESTRICTED
CPC2018-0968
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Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

EXECUTIVE SUMMARY

This policy and land use amendment application was submitted on 2018 March 13, by Casola Koppe, on behalf of the developer Bucci Development Ltd, and with authorization from the owner, The City of Calgary. This application proposes to redesignate the subject parcel from DC Direct Control District to Mixed Use – General (MU-1f5.3h50) District to allow for:

- transit supportive mixed-use development;
- street-oriented building design for residential developments and active uses for commercial developments along 9 Street NE;
- a maximum building height of 50.0 metres / approximately 15 storeys (an increase from the 23.0 metres / approximately 7 storeys);
- a maximum floor area ratio (FAR) of 5.3 (no changes from the existing DC);
- more appropriate parking requirements considering the site's proximity to the Bridgeland-Memorial LRT station; and
- greater flexibility of massing options, to break up massing into separate forms instead of one large block mass.

Map and textual amendments to the *Bridgeland-Riverside Area Redevelopment Plan* are included in this application to accommodate the proposed land use amendment.

The proposal is in keeping with the applicable policies of the *Municipal Development Plan*, the *Transit Oriented Development Policy Guidelines*, and the *Bridgeland-Riverside Area Redevelopment Plan*, as amended. A development permit was reviewed concurrently, and will be heard on the same Calgary Planning Commission agenda (DP2018-3108 / CPC2018-1130) with a recommendation of approval (Attachment 3).

ADMINISTRATION'S RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to the Bridgeland-Riverside Area Redevelopment Plan (Attachment 2);
2. Give three readings to the proposed bylaw;
3. **ADOPT**, by bylaw the proposed redesignation of 0.42 hectares ± (1.03 acres ±) located at 1018 McDougall Road NE (Plan 0815793; Block 10; Lot 2) from DC Direct Control District to Mixed Use – General (MU-1f5.3h50) District; and
4. Give three reading to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
1018 McDougall Road NE, LOC2018-0059**

BACKGROUND

This policy and land use amendment application was submitted on 2018 March 13, by Casola Koppe representing the developer Bucci Development Ltd, and with authorization from the owner, The City of Calgary. Prior to the formal application submission, the applicant met with the Bridgeland Riverside Community Association on 2018 February 20 and held an open house at the Bridgeland-Riverside Community Hall on 2018 February 28 to discuss the proposed development with the community. A pre-application meeting was held with Administration and the applicant (PE2018-00310) to discuss the proposed land use amendment and the schematic design of the proposed mixed-use development. The pre-application submission was also presented to Urban Design Review Panel on 2018 March 28 (Attachment 4).

During the review of the policy and land use amendment application, Administration recommended a joint review of the land use amendment and a development permit application through a concurrent process. As outlined to the applicant, the concurrent process provides benefits to all stakeholders through enabling a clear understanding of the intent and outcome of the land use amendment and development permit. Concurrent applications are ideal in instances where the built form outcomes are critical and central to the merits of the policy and land use amendments.

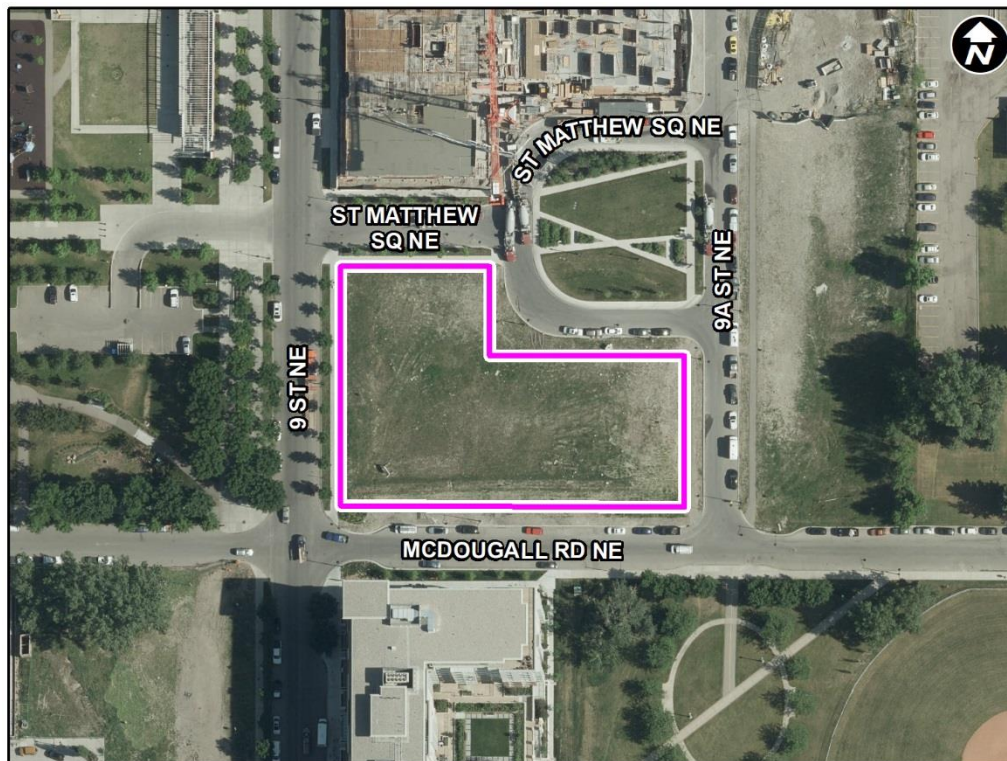
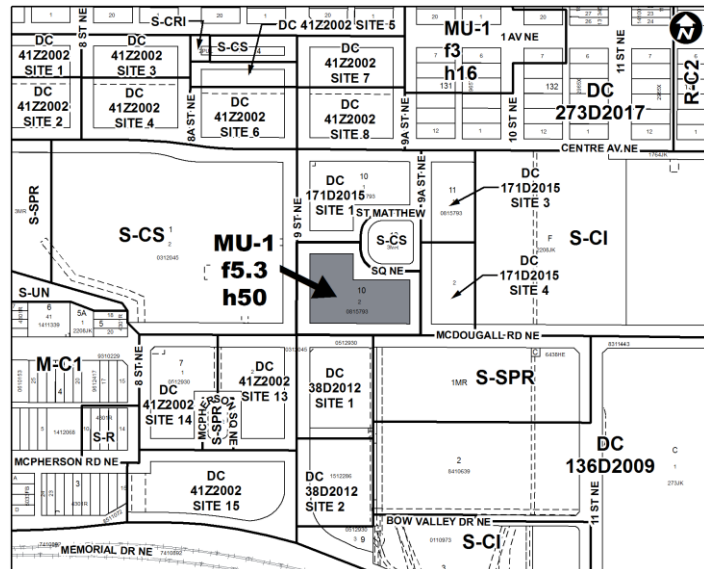
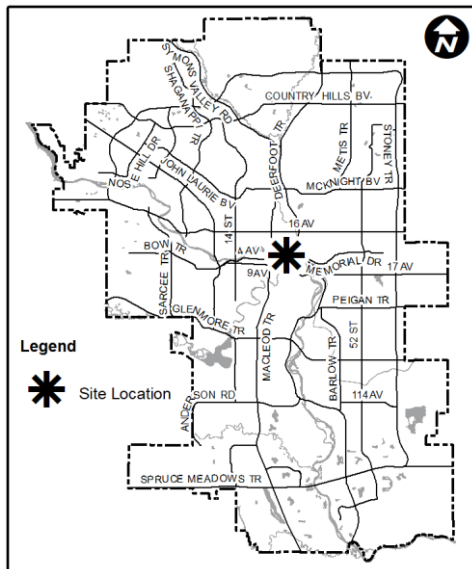
Administration further recommended that the concurrent development permit review reach a point where the review is completed prior to the land use amendment application going to Calgary Planning Commission for recommendation.

A concurrent development permit application (DP2018-3108 / CPC2018-1130) for a New: Retail and Consumer Service, Restaurant: Food Service Only – Small, Restaurant: Licensed – Medium, Outdoor Café, Dwelling Unit, Live Work Unit (1 building, 2 phases) was submitted on 2018 June 27. After the first detailed team review, it was determined that the development permit application was inconsistent with the land use amendment application as the proposed FAR was 5.8 and the land use application proposed 5.3 FAR.

The applicant elected to revise the land use amendment application to 5.8 FAR. The revised application was re-notice posted and re-circulated to affected stakeholders. After the review of all comments received from circulation and based on review of current planning policy work in the Bridgeland-Riverside community, it is Administration's recommendation that the FAR should remain at 5.3. Administration supports the proposed height increase as proposed to provide more flexibility in design, and the rationales are contained in this report. Both the recommendations on the proposed development permit and this land use amendment will be heard at the same agenda of Calgary Planning Commission. The proposed development permit application cannot be determined prior to the approval of bylaw amendments associated with the proposed policy and land use amendment by Council.

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
1018 McDougall Road NE, LOC2018-0059

Location Maps



Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

Site Context

The subject site is located in the northeast community of Bridgeland-Riverside. Following the demolition of the Calgary General Hospital in 1998, the subject site and adjacent City owned lands were part of a City initiated policy plan known as 'The Bridges'. The overall area of The Bridges is 14.9 hectares and is comprised of 16 sites, envisioned to be developed in three phases. Since the approval of The Bridges plan in 2002, a significant amount of redevelopment has occurred, both in terms of private residential and commercial development, as well as development of public parks and community facilities. The Bridges plan is near completion with the exception of four undeveloped sites, including the subject site remaining in Phase 3.

Presently vacant, the parcel comprises an area of approximately 0.42 hectares \pm (1.03 acres \pm). The subject site is located within 250 metres of the Bridgeland-Memorial LRT Station, walking distance to local commercial services, significant park spaces, and has easy access to the downtown core. The subject lands are moderately sloping and slope downward south towards the Bow River.

To the north, a new Multi-Residential Development, known as Radius, is nearing construction completion. St. Matthew Square, a small public park, is located to the northeast of the subject site. The vacant lands to the east are also subject to the same existing DC Direct Control District as the subject site. A mixed-use development, consisting of a seven-storey and an 11-storey building, exists directly south of the site. Lands to the west comprise of Murdoch Park, which houses the Bridgeland-Riverside Community Association building, community gardens, a playground, playfields, multi-use pathways, and a surface parking lot.

As identified in *Figure 1*, Bridgeland-Riverside's peak population was in 2017, reaching 6,332 residents.

Figure 1: Community Peak Population

Bridgeland-Riverside	
Peak Population Year	2017
Peak Population	6,332
2017 Current Population	6,332
Difference in Population (Number)	0
Difference in Population (Percentage)	0

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Bridgeland-Riverside](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposed land use amendment is to allow for mixed-use development and more flexible building design options within a 250 metre radius of the Bridgeland-Memorial LRT station. The proposal meets the objectives of applicable policies as discussed in the Strategic Alignment section of this report.

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

This application is to redesignate the site from the existing DC Direct Control District (Bylaw 171D2015) to Mixed Use – General (MU-1f5.3h50) District. The subject site is identified as ‘Site 2’ within the existing DC. The existing DC does not allow for any commercial uses and limits the uses to only those that are residential in nature. The DC allows for a maximum floor area ratio (FAR) of 5.3 and a maximum building height of 23.0 metres (approximately 7 storeys). At this maximum building height, the site can only enable a development with an approximate FAR of 4.3. Another challenge with the existing DC District is that while it references the 1P2007 Land Use Bylaw, the DC is not based on any standard land use districts of Bylaw 1P2007. By not referencing a standard Bylaw district, the uses in the DC are ‘frozen in time’ and the Development Authority cannot consider the introduction of new uses nor grant relaxations to any of the specific rules. The existing DC also requires parking ratio of 1.25 parking stalls per dwelling unit, which includes visitor parking stalls.

The proposed MU-1f5.3h50 District is intended to:

- increase the maximum building height from 23.0 metres (approximately 7 storeys) to 50.0 metres (approximately 15 storeys);
- facilitate a broad range of uses, including commercial and retail opportunities;
- allow for active uses at-grade that will promote greater street activation and pedestrian activity along 9 Street NE and McDougall Road NE; and
- allow for the parking requirements in the mixed-use district, which includes:
 - for each Dwelling Unit, 0.75 stalls per unit for resident parking and 0.1 stalls per unit for visitor parking; and
 - for Live Work Unit, 0.5 stalls per unit for resident parking and 0.5 stalls per unit for visitor parking.

As part of Administration’s review, alternative land use districts, including Direct Control District options were reviewed and explored. It was determined that the MU-1f5.3h50 District with the proposed policy amendments could best achieve the proposed development outcome.

Development and Site Design

The rules of the proposed M-U1f5.3h50 district will provide guidance for site development including appropriate uses, height, building setbacks, and landscaping. The increase in building height enables proposed development to achieve the allowable FAR of 5.3, which was envisioned for this Transit Oriented Development site. The underlying premise of this land use amendment application is to establish flexible land use direction that will foster creativity in built form, and in turn, allow for more exciting architectural potential. The concurrent development permit exhibits a high-quality architectural design, at grade live-work units, and the introduction

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

of commercial uses along 9 Street NE, which all provide a positive contribution to the surrounding community.

Transportation

A Transportation Impact Assessment (TIA) and parking study were required as part of this land use amendment. The TIA identified the existing road network can accommodate the proposed density. The subject site is within 250 metre walking distance to the Bridgeland-Memorial LRT station platform. The site is also within a 50 metre walking distance to the route 90 bus stop on 9 Street NE. The subject parcel will have vehicular access via McDougall Road NE. The site has good pedestrian connections to the Bridgeland-Memorial LRT station and adjacent bus stop.

Utilities and Servicing

Sanitary sewers are presently available to service the development. As part of the concurrent development permit application process, a sanitary servicing study must be submitted to determine whether upgrades are required to the existing public infrastructure.

Water mains are presently available. As part of the concurrent development permit application process, a fire flow calculation letter must be submitted to determine whether upgrades are required to the existing public infrastructure.

Storm sewers are available to service the development without the need for off-site improvements.

Environmental

As part of the concurrent development permit application review, the applicant must address the removal of soil contaminant recommendations as outlined in the 2016, Revised Final Phase II ESA report by Golder and Associates.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)*, which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Municipal Development Plan (Statutory, 2009)

In accordance with the Urban Structure Map (Map 1) of the *Municipal Development Plan (MDP)*, the subject site is identified as being located within the Developed Residential Area – Inner City Area. The land use policies in section 3.5.2 states that Inner City areas should maintain and expand, where warranted by increased population, local commercial development that provides retail and service uses in close proximity to residents, especially in the highest density locations.

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
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Buildings should maximize front door access to the street and principle areas to encourage pedestrian activity.

The MDP's City-wide policies, Section 2 and specifically Section 2.2 Shaping a More Compact Urban Form provides directions to encourage transit use, make optimal use of transit infrastructure, and improve the quality of the environment in communities. The intent of these policies is to direct future growth of the city in a way that fosters a more compact and efficient use of land, creates complete communities, allows for greater mobility choices and enhances vitality and character in local neighbourhoods.

The relevant policies within Section 2.2.2 are:

- a. Locate transit-supportive land uses, including higher density residential and employment developments, within Activity Centres and Corridors supported by Primary Transit Network.
- b. Increase development densities in proximity of the Primary Transit Network by targeting residential and employment intensities within 400 metres of transit stops, in areas deemed appropriate through the Local Area Planning process and in accordance with the Typology thresholds identified in Part 3.
- c. Locate land uses that will generate counter-flow transit ridership during peak-hour commuting times and support non-peak hour ridership.

The subject site is located within a 250 metre radius of the Bridgeland-Memorial LRT Station. The proposed increase to the maximum building height, enables the site to achieve the allowable maximum density of 5.3 FAR. The current DC District cannot achieve the maximum FAR with its existing maximum building height of 23.0 metres. The proposed land use amendment supports mixed-use development that is transit-oriented and is in keeping with the above MDP policies.

The MDP also contains city wide policies on urban design. The policies speak to the design of tall buildings, which are defined as buildings whose height is greater than the width of the right-of-way of the street that they front. In accordance with this definition, the proposed development, reviewed concurrent to this application, is considered to be a tall building. The MDP states that tall buildings are appropriate in the Centre City and Major Activity Centres, or in Community Activity Centres and Urban Main Streets where deemed appropriate through a Local Area Plan. While the subject site is not identified as one of the above typologies, Administration considers the subject location to be appropriate for a tall building. As such, policy amendments to the *Bridgeland-Riverside ARP* accompany this land use amendment application, the details of which are identified in the following section.

In accordance with the MDP, the proposed development is encouraged to make a positive contribution to the city by creating an interesting skyline, acting as a landmark and contributing to orientation and way-finding within the community of Bridgeland-Riverside. The concurrent development permit application was reviewed and supported by the Urban Design Review

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

Panel, as well as City Wide Urban Design, the details of which are provided in CPC2018-1130. Comments from UDRP regarding the concurrent applications are included in Attachment 5.

Bridgeland-Riverside Area Redevelopment Plan (Statutory, 1980)

The *Bridgeland-Riverside Area Redevelopment Plan (ARP)* was adopted by Council in 1980. In 2002, a major amendment (25P2000) to the ARP was approved to include the Bow Valley Centre lands (former Calgary General Hospital). The amendments included policies and goals for The Bridges planning area. The policies were integrated in the *Bow Valley Centre Concept Plan*, which forms section 9 of the *Bridgeland-Riverside ARP*. See the sub-section below for further information.

It should be noted that The City of Calgary has started the process of drafting a new ARP for Bridgeland-Riverside. Since 2002, major infrastructure improvements have taken place in the area (i.e. LRT, community centre site) and the new MDP policies have been adopted in 2009. Draft policies and land uses are being developed and reviewed. This proposal has been assessed under the current ARP and Administration is recommending amendments to the existing ARP to support the proposed development. The proposed policy amendments are consistent with the MDP, existing and planned context of the area, and is expected to comply with the new ARP policies. The draft ARP direction is also intending to consolidate the Bow Valley Concept Plan within the new Bridgeland-Riverside ARP. The new ARP is anticipated to be presented to Council by mid-2019.

Bow Valley Centre Concept Plan (BVC), Section 9 of the Bridgeland-Riverside Area Redevelopment Plan (Statutory, 2000)

This section of the ARP is for The Bridges planning area. Section 9.1 of the BVC states the objectives of the plan including:

- sensitive housing intensification;
- wider range of housing choices for different age and income levels;
- transit supportive development, including mixed-use development;
- pedestrian and cycling supportive public systems that connect well with adjacent lands;
and
- an improved physical environment.

The subject site is located within Multi-Family Residential typology as identified on Figure 19 – Conceptual Land Use map of the BVC. Residential area is represented by a variety of multi-residential development is encouraged throughout the site, including street townhouses, stacked townhouses, apartments, and ‘live/work’ units. Development will be characterized by an orientation to the street (i.e. ground floor units will have direct access to the street) and by building heights that are in the mid-range (3-8 storeys). A summary table of land use policy areas (Table 1) is included in the BVC and indicates the subject site being appropriate for development with a height of up to 23.0 metres and up to 5.3 FAR. Section 9.4.1 Residential, Parking policies requires parking of 1.25 parking stalls per unit. Policy amendments, including height and parking, are required to accommodate the proposed land use amendment.

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

Proposed ARP Amendments

Amendments to the ARP are required to accommodate the proposed land use amendment (Attachment 2). A summary of the proposed amendments is as follows:

1. Allowed tall buildings (defined in the MDP as a building whose height is greater than the width of the right-of-way of the street that it front) to be in this location. The amendments also include additional policies to be applied to the building design to mitigate the impacts of the tall building.
2. Added additional policies in the ARP to guide future development on sites within 400 metres of the LRT station and located along 9 Street NE. This includes the new 'active frontage' policies which requires street-oriented building design for residential developments and active uses for commercial developments.
3. Parking requirements shall be in accordance with the current Land Use Bylaw. The existing policy's parking requirement is outdated and not align with current parking bylaw requirements and does not allow flexibility for parking reduction.
4. Textual housekeeping amendments to clearly identify the Bow Valley Concept Plan as section 9 of the Bridgeland-Riverside ARP.

Administration believes that these proposed amendments are consistent with the MDP, and the planned context of the area. If supported by Council, the proposed amendments can be adopted into future ARP policies or provide directions for new policies.

Transit Oriented Development Policy Guidelines (updated 2005)

The subject site is located within a 250 metre walking distance from the Bridgeland-Memorial LRT station and is considered a Transit Oriented Development (TOD). The TOD Policy Guidelines contain six key policy objectives that apply to sites in close proximity to transit:

1. Ensure transit supportive land uses.
2. Increase density around Transit Stations.
3. Create pedestrian-oriented design.
4. Make each station area a 'place'.
5. Manage parking, bus, and vehicle traffic.
6. Plan in context with the local communities.

This land use amendment was reviewed against the TOD Policy Guidelines in order to determine if the proposed land use district, the list of uses and the rules (such as height, density, and FAR) are appropriate. Some of the TOD Guideline objectives that address site and building design, parking, edge conditions, access and circulation will require further consideration at the development permit stage.

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

Stakeholder Engagement, Research and Communication

Communications

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online through Planning and Development Map (PDMaP).

In addition, a City [project page](#) was launched on 2018 April 09. The project webpage was used to outline the application process, timelines, and engagement opportunities available. It also served as a resource for project information, links, and contact information.

Following Calgary Planning Commission, notifications for the Public hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Applicant-led engagement

The applicant led their own engagement program which included two meetings with the Bridgeland-Riverside Community Association, two open houses, and one information session. The applicant mailed out post cards through Canada Post to adjacent residents in advance of each applicant-led engagement event in 2018 February, 2018 May, and 2018 September. Administration attended one open house and the information session to share information on the planning review process and answer questions. Comments heard during the two engagements paralleled key themes received directly by Administration as a result of notice posting and circulation that are summarized below. The applicant also has provided a summary of their engagement efforts, which can be found in Attachment 6.

City-led engagement

Administration assessed the application and it was deemed that additional City-led engagement was not required.

In keeping with Administration's standard practices, stakeholders were given the opportunity to comment online through the Planning and Development Map (PDmap) or by contacting the planner directly by mail, phone, or email. Administration received 37 letters regarding the application and three letters from the Bridgeland-Riverside Community Association.

Comments from the Bridgeland-Riverside Community Association (BRCA) were received on 2018 April 19. These comments relate to the original application submission. Subsequent, the applicant amended the application to increase the FAR to 5.8. The BRCA provided further comments on the proposed land use amendment on 2018 August 20. An additional letter from the BRCA was received on 2018 October 05, prior to CPC. The letters from the BRCA can be found in Attachment 7.

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

What we heard / what we did

In response to the notice posting and the applicant-led engagement, 37 letters were received regarding the proposed development. Comments received included two letters of support and 35 letters of objection, including one letter from the Pontefino II Condo Board. Given that the concurrent development permit was circulated during the land use amendment application review period, the comments received were based on the shared information of the concurrent applications.

The two letters of support received by Administration identified support for this type of development and density near the community centre, community parks, and LRT station as opposed to the areas elsewhere in the neighbourhood that have a higher concentration of single-detached homes. The letters also indicated that supporting these concurrent applications will allow for land that has been bare for far too long to be a home for more new neighbours and new customers to support local businesses.

The main reasons for opposition that were received by Administration in the 35 letters of objection are summarized in the table below, including a response in how this feedback was addressed by Administration and/or the applicant to inform the final proposal.

Theme	What we heard (issue or opportunity identified)	What changes were made and/or response to the issue identified
Building Height	<ul style="list-style-type: none"> • Height of 50 metres is too high; • Does not fit the scale of the community and surrounding buildings; • Bridgeland is not a community of high rises; • The community assumed the existing height (7 storeys) would stay in place, and that expectation should be maintained; • Tall buildings or high-rise buildings would destroy the residential feel of Bridgeland and should be located closer to downtown where they are expected; • Allowing the height would be precedent setting; • The majority of this area has already been built and questions about why the proposed height changing now; • 8 storey is more than enough; and • Bridgeland Crossing near the station is the only 10 storey building in The Bridges area. 	<p>Concerns about height was raised by several residents. Although the proposed height is increasing, the allowable density is the same (5.3 FAR). The intent of the proposal is to allow for greater flexibility of massing options (break up massing into separate forms and instead of one large block) than is currently allowed to achieve the maximum density.</p> <p>Additional shadow and massing studies have been reviewed and accepted.</p> <p>The concurrent DP also demonstrates the massing of the taller building is mitigated through a thoughtful building design. The ground floor of the development demonstrates a strong relationship to the human scale and contribute positively to the public realm and street. The proposed development includes building setbacks, step-backs, articulations/ recesses to help mitigate the perception of mass and height.</p>
Views	<ul style="list-style-type: none"> • Diminishing views for nearby residents; 	<p>Access to view is not regulated in the Bylaw and generally not a planning matter that can be considered.</p>

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

	<ul style="list-style-type: none"> • Radius I purchasers paid a premium for the property with the view; • Decrease quality of life; • Decrease the desirability of the area as the new building will not be cohesive with the existing developments; and • The location of the north tower will not have any setback from street level, the proposed tower is adjacent to the Murdoch Park and the building will take away the openness of the park by impeding the view of the sky from the park. 	The north tower includes a two metre step-back from the commercial podium.
Shadowing	<ul style="list-style-type: none"> • Potential shadowing impacts to Murdoch Park; and • Potential shadowing on the Radius building roof top garden. 	Sun shadow studies have been provided by the applicant and the potential locations of shadows are acceptable. The studies indicate part of the park will be partially and temporally shadowed during early morning hours (i.e. 8 am) in June, March, September, and December.
Property Values	<ul style="list-style-type: none"> • Concerns related to potential loss in property values due to proposed development. 	Property values are not a planning matter that can be considered.
Parking / traffic	<ul style="list-style-type: none"> • Reduced parking of 0.53 units or 187 stalls for 347 units will put pressure on an already very limited parking area; • Implications of density on existing limited parking in the area; • It is untrue that people who live in TODs rely on public transportation and do not own a vehicle; • Summer time sees parking as more difficult with outdoor markets, proximity to LRT, and parking for the East Village; • More units will create more traffic to the surrounding area; • Traffic study must be required; and • With reduced parking on-site, more street parking will be required and will take up the available street parking for the community hall. Regular events are held at the community hall and renters of the hall uses the street parking. 	<p>Generally, parking demand in areas well served by transit, that are density populated, that are designed to support pedestrians and cyclists and where daily needs can be met in local shops and services have lower parking needs.</p> <p>A Transportation Impact Assessment and parking study was submitted, reviewed, and accepted for the proposed land use amendment.</p> <p>As per the concurrent DP review, there is no parking relaxation proposed for the residential and commercial uses. The parcel provides for 222 parcel stalls for 316 Dwelling Units and 18 Live Work Unit. Visitor parking is deficient by 31 stalls; however, the development is proposing shared visitor and commercial parking as per the TIA.</p>
ARP comments	<ul style="list-style-type: none"> • The existing community assumed the zoning that allows for 7 storeys 	Administration has responded to the residents' concern on the land use amendment and explained that a

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE, LOC2018-0059

	<p>would stay in place, and that expectation should be maintained;</p> <ul style="list-style-type: none"> • The current ARP allows a maximum height of 8 stories; and • Concern that the new ARP process is still underway and not finalized. Proposed new developments that substantially deviate from the existing ARP should wait. 	<p>landowner or authorized agent can apply and seek land use amendment the property. Administration must review the proposal and make a recommendation to CPC and Council.</p> <p>Although the ARP is still in progress, Administration must review and make a recommendation on the proposed application.</p>
<p>Communication</p>	<ul style="list-style-type: none"> • The Radius purchasers were not well informed of the proposed land use amendment; 	<p>The City's notification process included notice posting on site, and notification letters were sent to adjacent land owners (for the land use amendment). Since the owners of the Radius building had not moved in the building yet, they would not have received the notification. The City also does not have the contact information for these purchasers. A lack of notification was indicated by Radius purchasers and Administration has brought this issue to the applicant's attention. Bucci Development provided Radius purchasers the details of the project via email and encouraged comments to be submitted. Notification of the information session was sent directly to Radius purchasers.</p>
<p>Commercial development</p>	<ul style="list-style-type: none"> • Supports the proposed commercial development; • Community needs more local grocery stores; • Cannabis store and grocery store is inappropriate in this location, this location is very transient with lots of crime; and • Grocery store should be located on 1 Avenue. 	<p>Administration supports development of a mix of residential and commercial/retail uses at this location. According to the concurrent development permit, no grocery store or cannabis store is proposed.</p> <p>The proposed policy amendments also encourage active commercial uses along 9 Street NE, which would prohibit uses such as offices, medical office.</p>
<p>Other Comments</p>	<ul style="list-style-type: none"> • A rental building would change the character of the area; • No cell phone towers on top of the buildings; and • Concerned if this development is not approved, a future design may not be as attractive for the City and the neighbourhood. 	<p>In terms of rental vs condo units, Planning is not concerned with the user of the site but the proposed use – which are all considered Dwelling Unit.</p> <p>There are no cell phone towers proposed on top of the proposed development.</p>

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
 1018 McDougall Road NE, LOC2018-0059**

Social, Environmental, Economic (External)

The proposed policy and land use amendment will implement policy goals of providing more compact, compete communities with a diversity of housing and a variety of neighborhood shops and services that meet daily needs. The proposal will help promote high quality walking environments along 9 Street NE, which is a primary pedestrian connection into the community, anchoring the LRT station to the south and the intersection of 1 Avenue and the General Plaza to the north.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposed policy and land use amendment is consistent with applicable policies of the *Municipal Development Plan* and the *Bridgeland-Riverside Area Redevelopment Plan*, as amended. The proposal enables a range of development outcomes that keep with the existing floor area ratio by proposing an increase to the maximum building height. The proposal aligns with policy goals of providing a compact urban form and more complete communities with a greater range of uses that serve community needs. The proposal also supports transit supportive mixed-use development and high quality walking environments that act as a focal point and destination within a 250 metres walking distance of the Bridgeland-Memorial LRT station.

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed Amendments to the Bridgeland-Riverside Area Redevelopment Plan
3. Proposed Development Permit (DP2018-3108) Summary
4. Urban Design Review Panel Comments (PE2018-00310)
5. Urban Design Review Panel Comments (LOC2018-0059 / DP2018-3108)
6. Applicant Led Engagement Summary
7. Community Association Letters

Applicant's Submission



Tony Casola Architect Ltd. Hans Koppe Architect Ltd.
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Ph: 403 287-9960 Fax: 403 287-9962 Web: ckarch.ca

September 28, 2018

Applicant:

Bucci Riverside Projects Ltd.

Revised Land Use Amendment Application (LOC2018-0059)

The original land use amendment application was submitted in March 2018 to redesignate the parcel at 1018 McDougall Road NE from existing DC Direct Control District to MU-1 District to allow for mixed-use development. This letter provides an update on the proposed land use amendment application based on the latest working development permit plans. The concurrent development permit application was submitted on June 27, 2018.

The existing DC allowable FAR is 5.3 and the latest development permit plans in response to the detailed team review by the city also indicates a 5.3 FAR.

1018 McDougall Road NE is in the Bridgeland-Riverside community. It is one of Calgary's last undeveloped park-side properties in this master-planned community and will become an integral hub and landmark that enhances the neighborhood. The project will provide strong retail connections, improved pedestrian spaces, and landmark architecture designed by an international architect that strengthens Calgary's strong natural and urban interface and contributes to a unique sense of place.

The mixed-use project is comprised of three elements: two residential towers and a ground floor podium with commercial, residential and live-work uses. The proposed palette of forms and materials have also been engaged to architecturally express the base and tower elements of the project in a manner that enhances, compliments and responds to the current context of the neighborhood. The restaurants and shops will animate these frontages and will contribute to Bridgeland's livability, character and enjoyability.

There is a strong emphasis on the parks, public realm, pedestrian and resident activities. The portion of Bridgeland, between 1st Avenue, Murdoch and McDougall Parks and 9th Street is of increasing importance and there is a strong focus to create inspired places, strong pedestrian opportunities, and improved commercial connections that becomes a landmark for the community.

Applicant's Submission



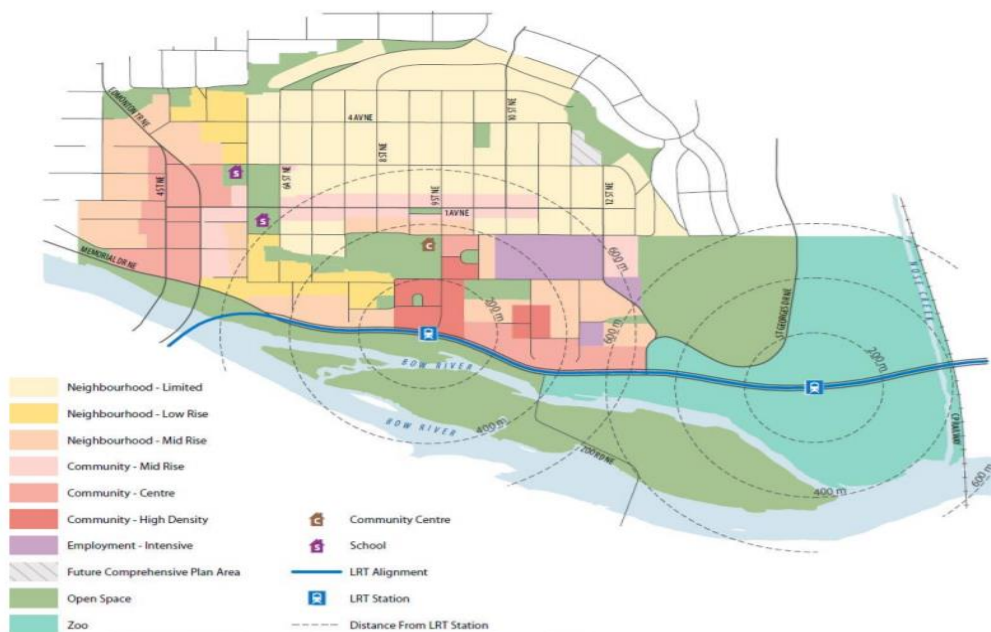
Location and Context:

The subject site is located at 1018 McDougall Rd NE and is approximately 1.02 acres (0.414 hectares) in size. It is located within the community of Bridgeland Riverside and situated within 200m of the Bridgeland LRT station. The City of Calgary started the process of creating a new Area Redevelopment Plan for Bridgeland-Riverside in 2017 to address future development types. The June 2018 draft plan classifies the site as community-high density and is intended to maximize density to support transit, commercial, mixed-use and other growth goals for the community.

Draft Land Use Map:

Bridgeland-Riverside Area Redevelopment Plan | DRAFT | June 23, 2018

Map 3: Land Use Concept



**This map shown is not final. Further changes may be forthcoming.*

Applicant's Submission

The subject site has no service lanes and is bordered by St. Matthews Square Park to the North East and McDougall Rd and 9 Street NE to the South and West. McDougall Park and the Bridgeland Community Hall are located immediately to the west of the subject site and could use greater support from additional mixed-use activation than what currently exists.

Limits of Current DC Zone (DC171D2015):

The current land use for the subject site (DC171D2015) does not allow for any commercial uses and essentially limits the uses to only those that are residential in nature. The zone limits the height of proposed buildings to 23m or effectively 7 storeys. The neighbouring development of Radius (mirror image to subject site and identical in size) has confirmed that, at this height maximum, the site can only render 4.3 FAR density which misses the maximum allowable residential floor area ratio of 5.3 by almost 20%. This equates to at least 60 additional units of housing that are not being realized for this transit-oriented site. The existing DC District contains other prescriptive requirements such as maximum and minimum setbacks, and massing setbacks above 2-3 storeys.

Proposed Land Use Amendments

- Increase maximum height from current 23m to 50m
- Include commercial uses such as:
 - a) Fitness Centre;
 - b) Retail and Consumer Service;
 - c) Brewery, Winery and Distillery;
 - d) Outdoor Café;
 - e) Restaurant: Food Service Only – Medium;
 - f) Restaurant: Licensed – Medium;
 - g) Supermarket and Live-work; and
 - h) Temporary Residential Sales Centre
- Allow for parking requirements to be reduced due to its proximity to the LRT Station in support of the City's Transit Oriented Development (TOD).

Reasons for proposed land use amendment:

- Allow for ability to achieve allowable floor area ratio closer to what was envisioned for this site (currently unachievable with height maximum of 23m).
- Provide for commercial, retail, and live-work opportunities (The restaurants and shops will animate these frontages and will contribute to Bridgeland's livability, character and enjoyability)
- Allow for greater flexibility of massing options (break up massing into separate forms and phases instead of one large block) than is currently allowed.
- Allow for greater flexibility for planning administration to relax aspects related to built-form, massing, to enable development of unique and extraordinary architecture.
- Allow for commercial uses at grade that will promote greater street activation and pedestrian activity along 9th Street NE and McDougall Rd. NE
- Allow for more appropriate parking requirements considering proximity to LRT station and TOD nature of site.

Engagement Summary:

- Extensive discussions and open houses with the City, Community and Bridgeland Riverside Community Association Board including:
- Meeting with senior planning officials to discuss opportunities for the site. (Feb. 6, 2018)
- Meeting with Bridgeland Riverside Community Association Board (BRCA) (Feb. 20, 2018)

Applicant's Submission

- Public Open House at Bridgeland Riverside Community Hall (Feb. 28, 2018). Over 12,000 notices were sent by Canada Post to the surrounding neighbourhood. Approximately 17 groups of residents attended and provided feedback on the initial concepts and ideas presented.
- Public Open House at Bridgeland Riverside Community Hall (May 22, 2018). Over 12,000 notices were sent by Canada Post to the surrounding neighborhood. Approximately 60 groups of residents attended and provided feedback on the development permit concept.
- Meeting with Bridgeland Riverside Community Association Board (BRCA) (July 18, 2018)
- Project website: City's website and applicant's website (www.bucci.com/dominion) to provide information on the applications online.

Proposed Amendments to the Bridgeland-Riverside Area Redevelopment Plan

WHEREAS it is desirable to amend the Bridgeland-Riverside Area Redevelopment Plan Bylaw 11P80, as amended;

AND WHEREAS Council has held a public hearing as required by Section 692 of the Municipal Government Act, R.S.A. 2000, c.M-26, as amended:

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

1. The Bridgeland-Riverside Area Redevelopment Plan attached to and forming part of Bylaw 11P80, as amended, is hereby further amended as follows:
 - (a) Delete the text “Chapter 9 Bow Valley Centre Concept Plan under separate cover” in the Table of Contents, Part 2: Bridgeland-Riverside Area Redevelopment Plan and replace with “9. The Bridges: The *Bow Valley Concept Plan*, under separate cover is Part 2, Section 9 of this ARP”.
 - (a) Delete the existing Figure 3 entitled “Generalized Land Use” and replace with revised Figure 3 entitled “Generalized Land Use”, as attached as Schedule A.
2. The Bow Valley Centre Concept Plan, Bridgeland-Riverside Area Redevelopment Plan attached to and forming part of Bylaw 11P80, as amended is hereby further amended as follows:
 - (a) Delete the title on the front cover page, and replace with “BOW VALLEY CENTRE CONCEPT PLAN PART 2, SECTION 9 OF THE BRIDGELAND-RIVERSIDE AREA REDEVELOPMENT PLAN”.
 - (b) Delete the footer at the bottom left corner of the front cover page, and replace with “Part 1 and 2, Sections 1 to 8 under separate cover”.
 - (c) Delete the title on the inside cover page, and replace with “BOW VALLEY CENTRE CONCEPT PLAN / PART 2, SECTION 9 OF THE BRIDGELAND-RIVERSIDE AREA REDEVELOPMENT PLAN”.
 - (d) Delete the footer at the bottom left corner of the inside cover page, and replace with “Part 1 and 2, Sections 1 to 8 under separate cover”.
 - (e) Delete the subtitle below the title of the table of contents page and replace with “Part 1 and Part 2, Sections 1 to 8 under separate cover”.
 - (f) Delete the text “Preface...3” in the table of contents.
 - (g) Delete the Preface section in its entirety.
 - (h) Delete the first sentence of the third paragraph of section 9.0 Introduction, Background and replace with “This section of the Area Redevelopment Plan establishes the policies and guidelines that will guide the redevelopment of the BVC”.

Proposed Amendments to the Bridgeland-Riverside Area Redevelopment Plan

- (i) Insert a tenth bullet to section 9.1 Redevelopment Objectives that states “Promote street-oriented building design for residential developments and active uses for commercial developments along a continuous block face on 9 Street NE.”.
- (j) Delete the second and third sentences in the first paragraph under the Transit-Supportive Development (LRT Station Area) bullet point in section 9.2.3 Major Land Use Components, and replace with “Transit supportive developments including a mix of high density residential, commercial, and live-work uses that are oriented to the public street, have direct pedestrian access, and encourage frequent walk-up pedestrian activities should be located along 9 Street NE and within 400 metre radius of the Bridgeland-Memorial LRT station.”
- (k) Delete Figure 19 entitled “Conceptual Land Use Plan” and replace with revised Figure 19 entitled “Conceptual Land Use Plan”, as attached as Schedule B.
- (l) Delete the second sentence under the Residential bullet point in section 9.2.3 Major Land Use Components, and replace with “Development will be characterized by an orientation to the street (i.e. ground floor units will have direct access to the street).”.
- (m) Delete the third sentence under the Relationship between Built Form and Open Space bullet point in section 9.2.4 Urban Design Approach, and replace with “For example, the large park will be enclosed on three sides by buildings with a consistent street wall of 2-3 storeys.”.
- (n) Delete Table 1 Summary of Land Use Policy Areas in its entirety from section 9.3 Land Use Policies.
- (o) Delete Figure 20 entitled “Transit Service” and replace with revised Figure 20 entitled “Transit Service”, as attached as Schedule C.
- (p) Delete Figure 21 entitled “Residential” and replace with revised Figure 21 entitled “Residential”, as attached as Schedule D.
- (q) Insert a new policy after policy 7 in section 9.3.1 Transit-Supportive Design and Development, Policies, that states “8. Street-oriented building design for residential developments and active uses for commercial developments are required for developments along 9 Street NE from the Bridgeland-Memorial LRT Station to Centre Avenue NE, as indicated in Figure 21.”.
- (r) Insert a new policy after the proposed policy 8 of section 9.3.1, Transit-Supportive Design and Development, Policies, that states “9. Tall buildings with thoughtful building and site designs may be appropriate along 9 Street NE within 400 metres of the Bridgeland-Memorial LRT Station. The ground floor of tall buildings should demonstrate a strong relationship to the human scale and contribute positively to the public realm and street. Tall buildings are generally defined in the Municipal Development Plan as a building whose height is greater than the width of the right-of-way of the street that it fronts.”.

Proposed Amendments to the Bridgeland-Riverside Area Redevelopment Plan

- (s) Delete policy 5 in section 9.3.2 Residential, Policies in its entirety and replace with “5. Multi-residential, commercial, or mixed-use developments should provide setbacks, after the second or third storey, to reduce the massing of a taller building from the streets. The ground and lower levels of developments should demonstrate a strong relationship to the human scale and contribute positively to the public realm and street.”.
- (t) Delete the first and second paragraph in section 9.4.1 Residential in their entirety and replace with “Parking requirements are determined as per the current Land Use Bylaw.”.
- (u) Delete policy 3 and policy 5 in section 9.4.1 Residential, Policies, and renumber policies accordingly.
- (v) Delete the text “The ARP amendment” in the first sentence of the first paragraph in section 9.6 Implementation and replace with “The Bow Valley Centre Concept Plan”.
- (w) Delete the text “The ARP amendment and” in the first sentence of the first paragraph in Section 9.6.1 Outline Plan/Land Use Redesignations, and replace with “The Bow Valley Centre”.
- (x) Delete the text “Land Use Bylaw 2P80” in the third sentence of the second paragraph in section 9.6.1 Outline Plan/Land Use Redesignations and replace with “the Land Use Bylaw”.
- (y) Delete the text “including this Amendment, supplement” in the fourth sentence of the second paragraph in section 9.6.1 Outline Plan/ Land Use Redesignations and replace with “supplements”.

Proposed Amendments to the Bridgeland-Riverside Area Redevelopment Plan

SCHEDULE A

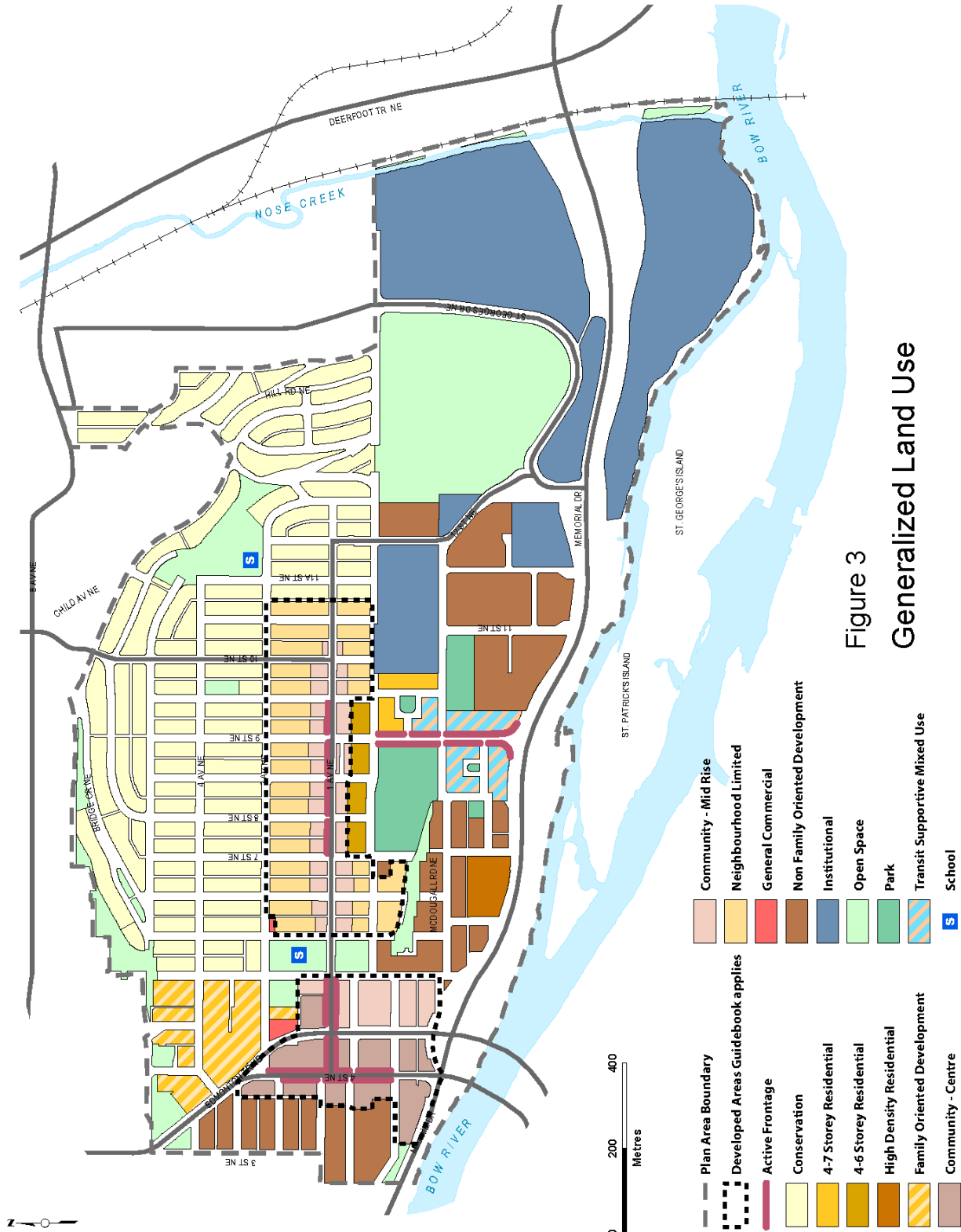
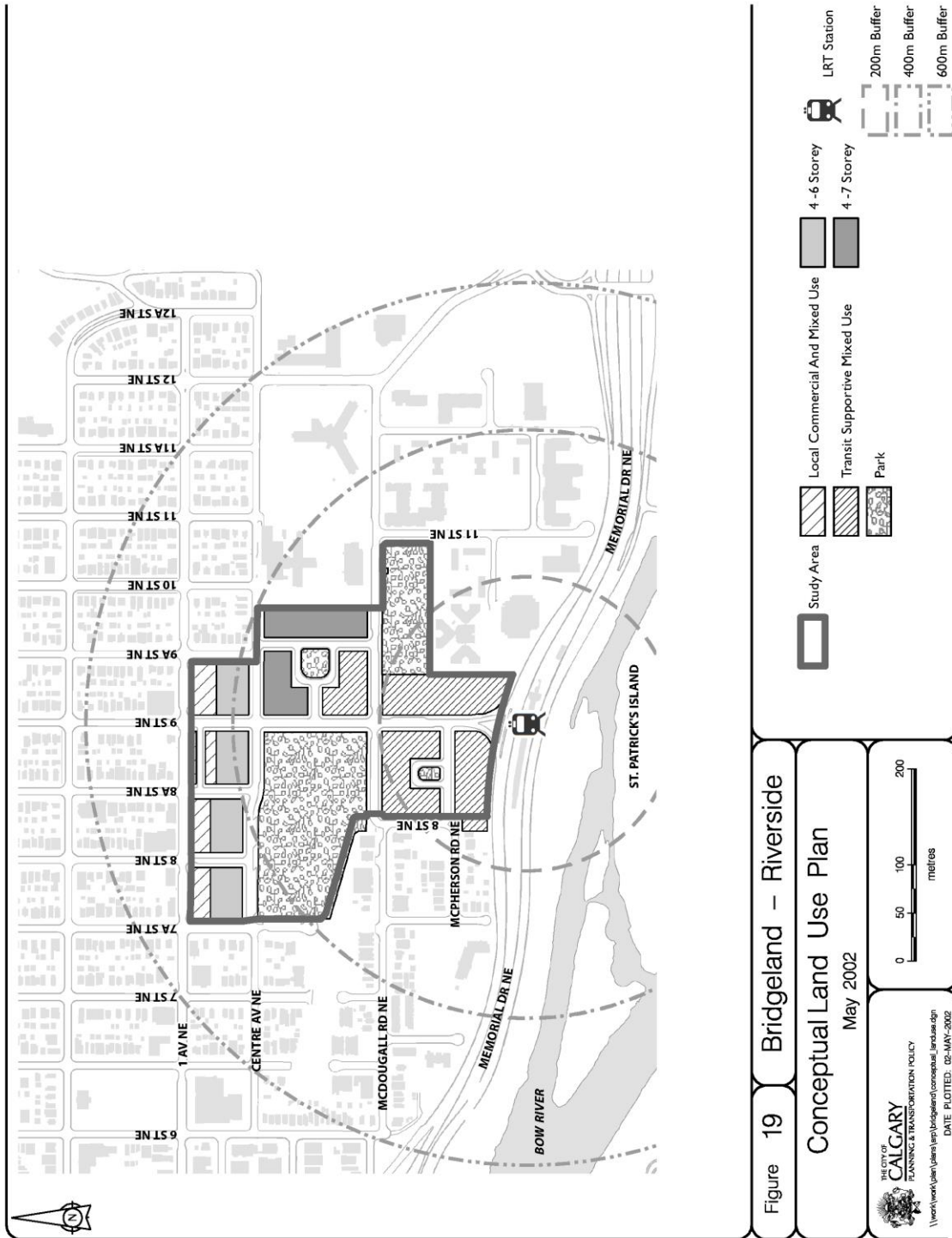


Figure 3
Generalized Land Use

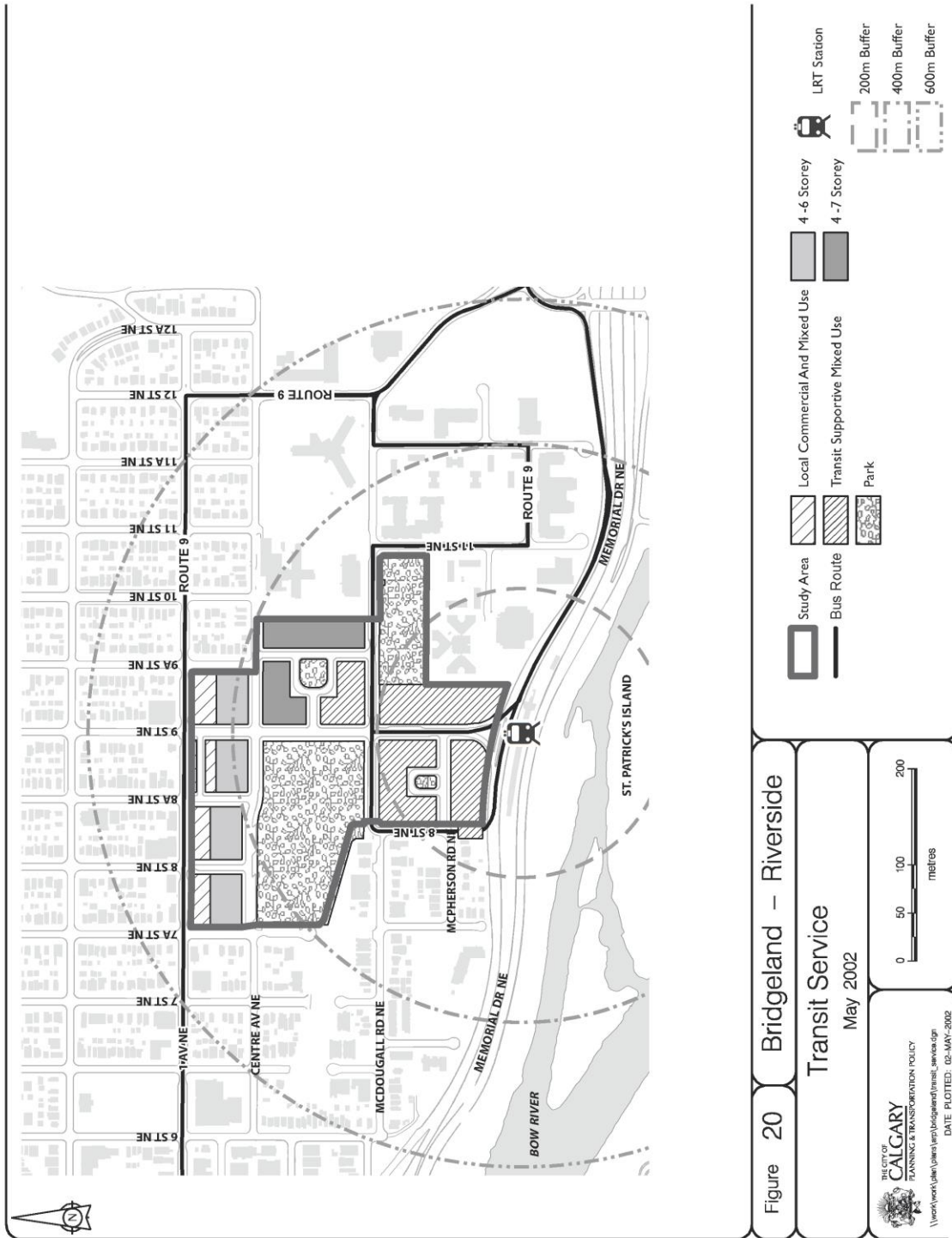
Proposed Amendments to the Bridgeland-Riverside Area Redevelopment Plan

SCHEDULE B



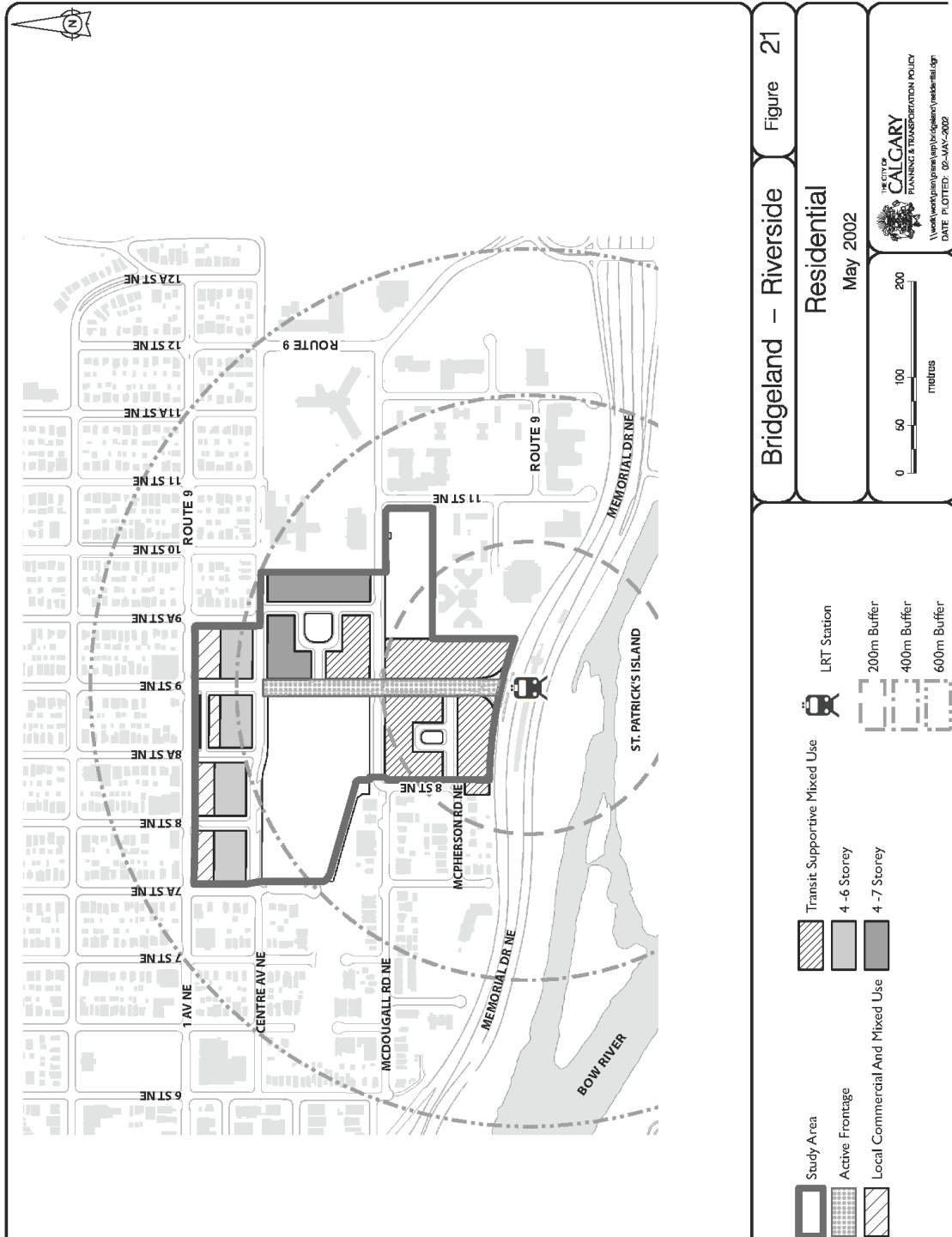
Proposed Amendments to the Bridgeland-Riverside Area Redevelopment Plan

SCHEDULE C



Proposed Amendments to the Bridgeland-Riverside Area Redevelopment Plan

SCHEDULE D



Bridgeland – Riverside Figure 21

Residential

May 2002

CITY OF CALGARY
PLANNING & TRANSPORTATION POLICY
\\work\proj\plan\bridgeland\bridgeland.dwg
DATE PLOTTED: 02-MAY-2002

Proposed Development Permit (DP2018-3108) Summary

A concurrent development permit application (DP2018-3108), referred to as 'Dominion' was submitted by Casola Koppe Architects on 2018 June 27, on behalf of the developer Bucci Development Ltd, and with authorization from the owner The City of Calgary. The development permit application is for Retail and Consumer Service, Restaurant: Food Service Only – Small, Restaurant: Licensed – Medium, Outdoor Café, Dwelling Unit, Live Work Unit (1 building, 2 phases). The proposed mixed-use development consists of:

- one building, with two residential towers at 14 storeys and 15 storeys high and a ground floor podium containing commercial and live-work uses;
- a total of 316 Dwelling Units and 18 Live Work Units;
- approximately 850 square metres (9,153 square feet) of commercial space;
- active frontages and active uses at-grade; and
- a total of 266 underground parking stalls.

The following excerpts (Figure 1, 2, 3 & 4) from the development permit submission provide a general overview of the proposal and are included for information purposes only. The proposed development permit (DP2018-3108 / CPC2018-1130) will be heard on the same Calgary Planning Commission agenda as this policy and land use amendment.

Proposed Development Permit (DP2018-3108) Summary

Figure 1: Rendering of Proposed Development (view from 9 Street NE and McDougall Road NE corner)



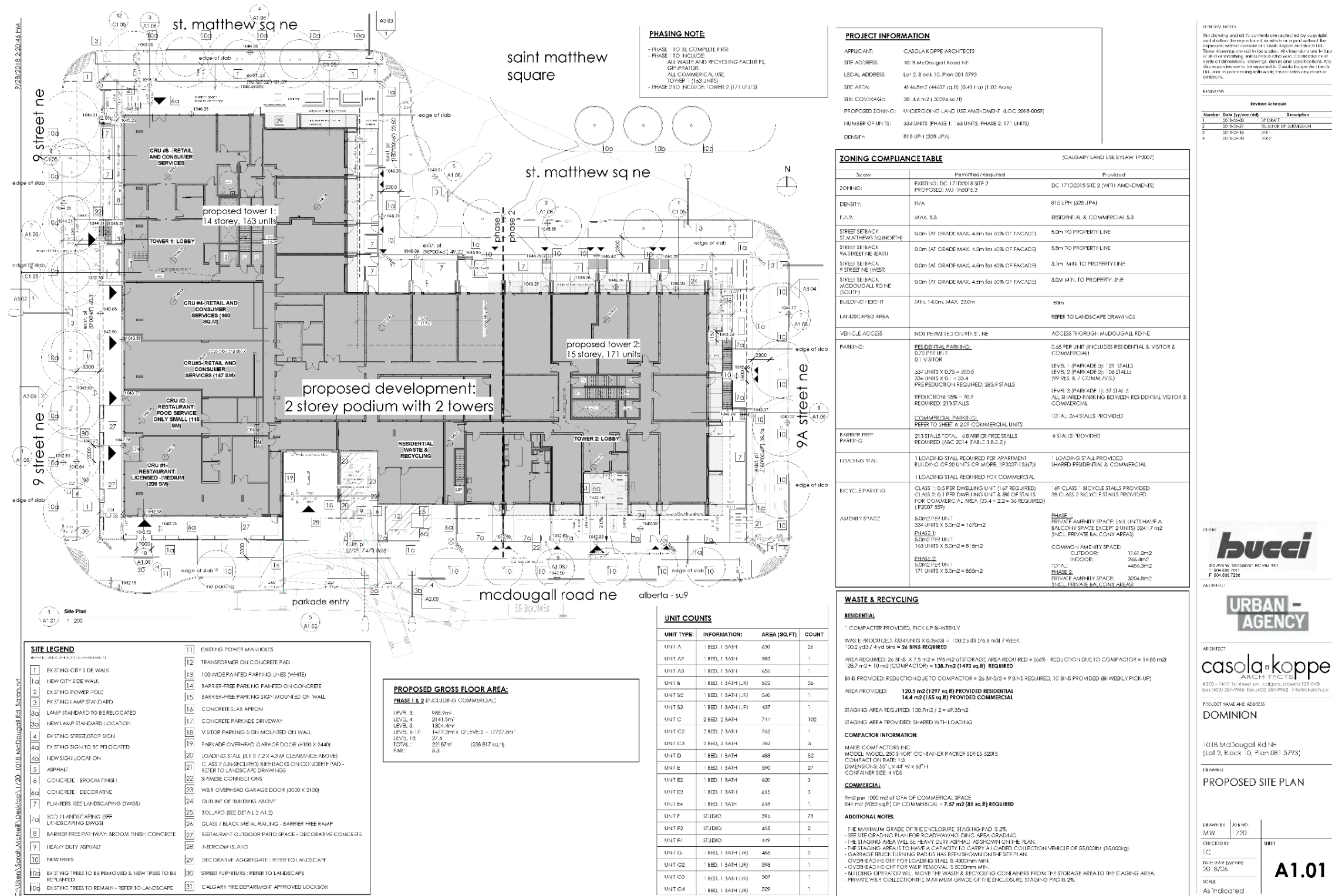
Proposed Development Permit (DP2018-3108) Summary

Figure 2: Rendering of Proposed Development (street edge and roof top garden on 9 Street NE and McDougall Road NE)



Proposed Development Permit (DP2018-3108) Summary

Figure 3: Proposed Site Plan



Proposed Development Permit (DP2018-3108) Summary

Figure 4: Proposed Shadow Studies



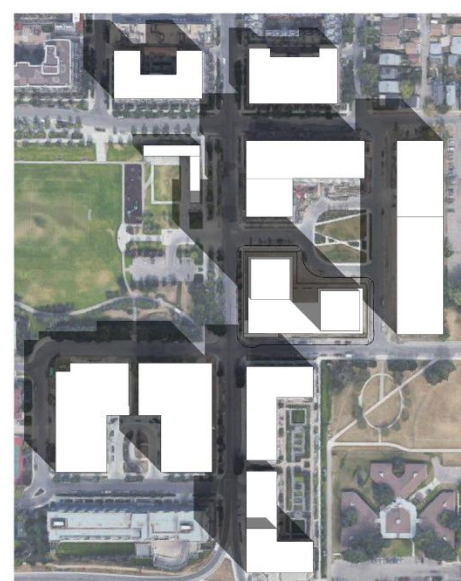
Proposed Development Permit (DP2018-3108) Summary

PROPOSED LAND USE DISTRICT (TOWER)

SHADOW STUDY - MARCH / SEPTEMBER



8am



10am



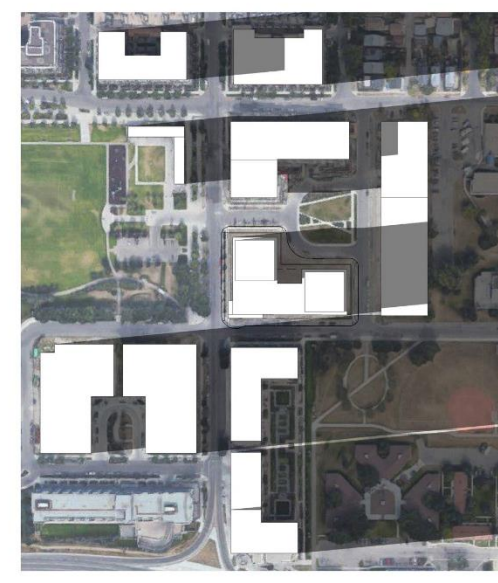
12pm



2pm



4pm



6pm

PROPOSED SHADOW STUDY

PROPOSED ZONING: MU-1, MIXED USE

FAR: 5.3

HEIGHT: 50M

DOMINION - 1018 MCDOUGALL ROAD NE

casola-koppe
ARCHITECTS
#300 - 1410 St. Charles Avenue, Alberta T2K 0A8
Tel: (403) 267-9960 Fax: (403) 267-9960 info@ckarch.ca

URBAN -
AGENCY

bucci

Proposed Development Permit (DP2018-3108) Summary

PROPOSED LAND USE DISTRICT (TOWER)

SHADOW STUDY - DECEMBER



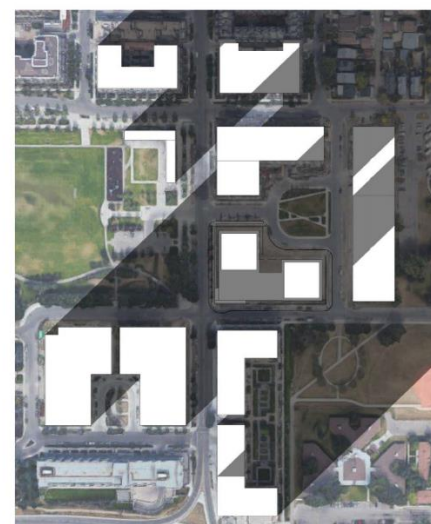
10am



12pm



2pm



4pm

PROPOSED SHADOW STUDY

PROPOSED ZONING: MU-1, MIXED USE
FAR: 5.3
HEIGHT: 50M

DOMINION - 1018 MCDOUGALL ROAD NE

casola koppe
ARCHITECTS
#330-440-1010 | @casolakoppe | 1018 McDougall Rd NE, Vancouver, BC V6E 2K1

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AGENCY

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Urban Design Review Panel Comments (PE2018-00310)

Date:	March 28, 2018	
Time:	2:45 pm	
Panel Members:	Present:	Absent:
	Janice Liebe	Chad Russill (chair)
	Chris Hardwicke	Bruce Nelligan
	Glen Pardoe	Gary Mundy
	Robert Leblond	Jack Vanstone
	Terry Klassen	Yogeshwar Navagrah
		Eric Toker
Advisor:	David Down, Chief Urban Designer	
Application number:	PE2018-00310	
Municipal address:	1018 McDougall Rd NE	
Community:	Bridgeland/Riverside	
Project description:	Radius II – mixed use development	
Review:	first	
File Manager:	Christine Leung	
City Wide Urban Design:	Lothar Wiwjorra	
Applicant:	Casola Koppe	
Architect:	Casola Koppe	
Owner:		
Ranking:	Endorse with comment	

Summary

The proposed project is a mixed-use midrise project that is comprised of two towers joined by a low podium. The Panel expressed support for the general urban strategy of the project including breaking the mass into two towers to provide a variegated elevation rather than a single slab. Recognizing that the project has no “back” as it is surrounded by parks or public streets on all sides the Panel supports the attempt to activate all frontages with commercial uses or live-work townhouses at grade. The Panel’s comments and concerns focused primarily on the challenges of providing active uses given the steep grades and the treatment of the servicing area. The landscape design is yet to be fully developed. In particular the boulevard condition in front of the retail units is currently shown as a grass boulevard typical of residential frontage. Opportunities to incorporate LID, resolution of universal accessibility and the specific detail of the stepped terraces and proposed retaining walls should be explored in future explorations. Similarly, the landscape in front of the townhouses is undeveloped showing only a simple walkway to the front door. Patio space, public/private separation and separation between units should be explored. The patio/playground courtyard on the north side of the project presents public/private interface issues and is also undeveloped. The edge condition along the blank wall to the south of the bike parking room does not show design mitigations to screen or articulate the blank wall. Finally, the applicant noted that the current design of the tower facades with a full building screen in front of deep balconies will likely change due to energy considerations and that future submissions may be different in aesthetic. For these reasons and others noted below the Panel endorses the project with comment.

Missing or incomplete information provided:

The applicant did not provide all the information required for a UDRP submission. Streetscape elevations, materials board, a written urban design response, and lighting design were missing. Although landscape drawings for at grade, patio levels and rooftop were provided they did not provide sufficient detail for evaluation. Some of the renderings in the document were not labelled as precedent images and thus did not represent the design. The applicant is asked to obtain the requirements from the assigned planner and to provide all missing information at the next meeting.

Urban Design Review Panel Comments (PE2018-00310)

Applicant Response

July 3, 2018

The proposed mixed-use development has been submitted for development permit and is currently showing that it will provide 347 residential units and approximately 9000 s.f. of commercial space to the site.

The proposed commercial retail frontage along 9th Street SE and western end of McDougall Road SE is a fundamentally important urban design element and is critical to the community, given that it can potentially provide the biggest public realm contribution to the immediate area. The shops cafes and restaurants will provide much needed pedestrian activation of this important location. The 9th Street frontage will consist of a conventional 3 zone public streetscape:

1. On site (setback area) uses such as outdoor seating, dining and merchandising. This area will be defined by a contrasting paving pattern and bordered in some areas by moveable fencing and landscaped planters (where licensed patios will be anticipated).
2. The main pedestrian path (uninterrupted city sidewalk).
3. Boulevard strip between city sidewalk and street curb. This area will provide space for public benches, trees, street and sidewalk lighting, waste and recycling receptacles and bicycle parking. This area will ensure that the city sidewalk remains unobstructed and supported at the same time.

The McDougall Road SE edge will start at its western end with the last portion of commercial frontage (it is anticipated that this will provide the greatest opportunity for a restaurant or pub patio given its solar exposure and potential to activate the 9 Street / McDougall Road SE corner. Moving eastward along McDougall Road SE, the parkade entrance, loading and waste/recycling and transformer will be contained to as small a frontage as possible to minimize its impact on the street. Landscape type screening will be relied upon to minimize its visual impact from the street. Beyond these important building service entry points the podium transitions to a grade oriented residential frontage that will incorporate private outdoor garden entrances defined by landscape and low fencing to effectively define the public/private edge. This type of interface will continue for the majority of the podium perimeter being interrupted only by the south tower lobby entrance that will have a large extending canopy highlighting its location from the street. The 9A Street edge, which has 2 residential addresses, will negotiate the steep grade change from McDougall to St. Matthew Square. Large landscaped planters will be employed to create the appropriate screening of the P1 foundation and help to make the grade transition along this challenging edge.

The St. Matthew Square edge will be lined with more street oriented residential entries that will then turn into the site to create the north courtyard which will provide access to the internal units of the podium. This courtyard will be treated as a semi-private garden that provides access to the residential units.

The north tower portion of the podium will provide access to the enlarged amenity lobby (labeled as the "We Work" lobby) that is envisioned as an opportunity for the residents of the site and surrounding community to have "as needed" collaborative office/meeting space available in a "hotel lobby" type setting. Spaces can be booked and paid for on demand with E-technology similar to that used for car sharing programs. The street entrance for this space will be entered from the St. Matthew Square / 9th Street corner. The north lobby private residential lobby entrance will be provided from a front stair off of 9th Street.

Casola Kopp Architects Ltd.

Urban Design Review Panel Comments (PE2018-00310)

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	Support
	UDRP Commentary		
	The project achieves retail street diversity by supplying retail units along the principal façade.		
	Applicant Response		
Greater development of the challenging grade and expression of the intended use of the retail streetscape space has been presented in the plans and images put forward.			
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	Support
	UDRP Commentary		
	The project appears to maximize glazing in the façade of the retail units.		
	Applicant Response		
Two storey tall transparent glazing dominates the retail frontage along 9th Street SE to maximize the visual connection from within the retail units looking out to the street and allows the best sight lines possible into the commercial units from the street.			
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	Further review recommended
	UDRP Commentary		
	The project meets the goals of pedestrian-first design although street furnishings are not shown on the plans.		
	Applicant response		
Further development of the streetscape defines 3 different zones within this critical frontage. The first 3m strip (setback area) of the property will be reserved for: CRU and residential lobby entry points, flexible outdoor seating and tables, moveable planters and low decorative fencing, potential outdoor merchandising or spill out of retail shop wares, and planters to negotiate sharp grade changes. The next zone will of course be the 2.0m wide city sidewalk which will remain uninterrupted and will be supported by 3rd zone: the boulevard strip between the street curb and sidewalk which will contain trees, public bench seating, bicycle parking, lamp standards, bollard lighting, waste/recycling receptacles.			
4	Entry definition / legibility	Entry points are clear and legible	Further review recommended
	UDRP Commentary		
	The documentation does not depict the entry points in either elevation or rendering.		
	Applicant Response		
Entry points to each CRU, residential lobby, and grade oriented unit are now confirmed and represented on the site plan, landscape plan, and elevations.			
5	Residential multi-level units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Further review recommended
	UDRP Commentary		
	Residential units are provided at grade. Private outdoor patios are not shown in the plans as the landscape design is undeveloped.		
	Applicant Response		
Further development of the grade oriented residential unit interface expresses the well-defined private outdoor garden entry points for each unit.			

Urban Design Review Panel Comments (PE2018-00310)

6	At grade parking	At grade parking is concealed behind building frontages along public streets.	NA
UDRP Commentary			
Parking is supplied underground.			
Applicant Response			
All of the parking (commercial/visitor/residential) is contained within the common parkade. Retail customers will be able to park on the first level of the parkade only and access the street front through a dedicated entrance beside the parkade driveway.			
7	Parking entrances	Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.	Support with comment.
UDRP Commentary			
Parking ramps are concealed and minimized. The effect of the blank wall along the bike storage façade should be mitigated.			
Applicant Response			
The parkade entrance has been recessed and located at a transitional point between commercial frontage and residential grade oriented units.			
8	Other		
Applicant Response			
Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i>			
Topic		Best Practice	Ranking
9	LRT station connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	NA
UDRP Commentary			
NA			
Applicant Response			
Proposed development will increase ridership of LRT. Development seeks to provide 0.65 stalls per residential unit and promote alternative modes of transport (car sharing, cycling, walking, transit)			
10	Regional pathway connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	NA
UDRP Commentary			
NA			
Applicant Response			
Proposed development will activated existing pedestrian connections making them more desirable to use.			
11	Cycle path connections	Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.	NA
UDRP Commentary			
NA			
Applicant Response			
Proposed development will attempt to provide at least one class one stall per unit within the first level of the building to promote cycling as an alternative mode of transport.			
12	Walkability - connection to adjacent neighbourhoods	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	NA

Urban Design Review Panel Comments (PE2018-00310)

	/ districts / key urban features		
	UDRP Commentary		
	NA		
	Applicant Response		
	Proposed development will activate existing pedestrian connections making them more desirable to use.		
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.	NA
	UDRP Commentary		
	NA		
	Applicant Response		
	Proposed development will activate existing pedestrian connections making them more desirable to use.		
14	Open space networks and park systems	Connects and extends existing systems and patterns.	NA
	UDRP Commentary		
	NA		
	Applicant Response		
	Proposed development will activate and support adjacent existing open spaces and parks to promote their use.		
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.	Support
	UDRP Commentary		
	The project massing and orientation appears to maximize views.		
	Applicant Response		
	Proposed towers are offset in an attempt to maximize views to south and west towards, skyline and mountains.		
16	Vehicular interface		NA
	UDRP Commentary		
	NA		
	Applicant Response		
	Although all of the required parking will be in the building, the 9th Street SE edge will provide some short term on street parallel parking which is already currently in place.		
17	Other		
	Applicant Response		
	Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i>		
	Topic	Best Practice	Ranking
18	Massing relationship to context	Relationship to adjacent properties is sympathetic	Support
	UDRP Commentary		
	Massing is sympathetic and improves on previous responses in the surrounding development.		
	Applicant Response		
	Two tower concept breaks up what would otherwise be a solid 8 to 10 storey full block massing.		
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites	Further review recommended
	UDRP Commentary		
	<i>No shadow studies were included in the package.</i>		
	Applicant Response		

Urban Design Review Panel Comments (PE2018-00310)

	Shadow studies have been included with the development permit application. Shadows cast will mostly affect the St. Matthew's park to the north east of the site. Ultimately, the surface area of shadow of the two tower massing will be close to the area of shadow cast by an uninterrupted solid 10 storey block massing.		
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
	UDRP Commentary		
	The project is successful in relating built-form to the surrounding streets.		
	Applicant Response		
	Proposed building engages the street edges at all possible opportunities.		
21	Massing distribution on site		Support
	UDRP Commentary		
	The massing is sensitively distributed on the site and breaks down built-form bulk into two distinct and complimentary towers.		
	Applicant Response		
	Two tower concept breaks up what would otherwise be a solid 8 to 10 storey full block massing.		
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Further review recommended
	UDRP Commentary		
	Although the towers do not step back from the street they have minimal street lengths and are joined by a one-story podium that minimizes the effect of a continuous slab alternative. The current resolution of the south west corner could use further study to create a more human scale at street level. The west retail face is flat to the street providing no cover or relief for pedestrians from the elements or cover at entry points.		
	Applicant Response		
	Entry points to residential tower lobbies have been provided large canopies to define these important locations and provide some cover for pedestrians. Outdoor seating, and dining areas will most likely incorporate free-standing trellis/pergola structures, and retractable umbrellas to provide some measure of solar cover during high exposure times of the year and day.		
23	Other		
	Applicant Response		
	Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i>		
	Topic	Best Practice	Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Support with comment
	UDRP Commentary		
	The north courtyard might present some view line issues.		
	Applicant Response		
	The north courtyard is now essentially a semi-private landscaped courtyard that will provide access to the half dozen residential units that front it. These units will provide the necessary passive visual surveillance of the area to make it a safe place to inhabit.		
25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	Further review recommended
	UDRP Commentary		
	No wind analysis or mitigation strategy was presented.		

Urban Design Review Panel Comments (PE2018-00310)

	Applicant Response		
	Landscaping will be utilized to mitigate and anticipated high wind areas.		
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.	Further review recommended
	UDRP Commentary		
	No snow drifting analysis or mitigation strategy was presented.		
	Applicant Response		
	Streetscape frontage will be sufficiently flat, smooth and uninterrupted to allow for efficient snow removal by shovel or motorized brush. Snow storage will be provided by the boulevard between the sidewalk and street curb.		
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.	Further review recommended
	UDRP Commentary		
	Not enough information in the package to comment.		
	Applicant Response		
	Entry points to residential tower lobbies have been provided large canopies to define these important locations and provide some cover for pedestrians.		
28	Night time design		Further review recommended
	UDRP Commentary		
	No lighting designs were included in the package.		
	Applicant Response		
	Lighting and landscape plans have been provided to show lighting types and quantities at the perimeter of the site and at the outdoor amenity space on the podium roof. A mix of bollard and building mounted down lighting will be utilized to provide a comfortable lighting level while avoiding and offsite spillage to neighbouring residences.		
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.	Further review recommended
	UDRP Commentary		
	<i>Slopes were not indicated but grades along the east and west of the project might present accessibility challenges. More information required to assess.</i>		
	Applicant Response		
	Special attention has been given to resolve grades along the 9th Street frontage to ensure all commercial and residential lobby entry points are barrier free and useable for outdoor seating and dining.		
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.	Further review recommended
	UDRP Commentary		
	No shadow studies were presented.		
	Applicant Response		
	Shadow studies have been included with the development permit application. The west and south faces of the commercial podium will provide the greatest opportunities for warm sunshine to make outdoor use desirable.		
31	Other		
	Applicant Response		
	Service / Utility Design Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.		

Urban Design Review Panel Comments (PE2018-00310)

Topic	Commentary	Ranking
32	(specify) N/A	TBD
	Applicant Response	
	The parkade access driveway area attempts to consolidate the main service and utility functions of the development. The transformer, loading zone, waste and recycling all define a point of transition from the commercial retail frontage of 9th and McDougall and the street oriented residential entrances further east along McDougall. Other than these important elements, it is anticipated that there will be no other intrusive or unsightly building services that will interrupt the streetscape.	

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

Date: July 11, 2018
Time: 3:00 pm
Panel Members:

Present:	Absent:
Chad Russill (co-chair)	Janice Liebe (chair)
Amelia Hollingshurst	Glen Pardoe
Ben Bailey	Robert LeBlond
Yogeshwar Navagrah	Terry Klassen
Chris Hardwicke	Bruce Nelligan
	Jack Vanstone
	Gary Mundy
	Eric Toker

Advisor: David Down, Chief Urban Designer
Application number: LOC2018-0059/DP2018-3108
Municipal address: 1018 McDougall Rd NE
Community: Bridgeland
Project description: New: Multi-Residential Development, Retail and Consumer Service (2 buildings, 2 phases)
Review: Second (PE2018-00310)
File Manager: Christine Leung
City Wide Urban Design: Lothar Wiwjorra
Applicant: Casola Koppe
Architect: Casola Koppe
Owner: Bucci
Ranking: Endorse

Summary

July 11, 2018

The proposed project was previously presented to UDRP in March 2018. The panel noted that the new package did a strong job describing the project and the applicant has addressed the majority of the comments from the previous submission. Panel members stated that they were excited by the well-considered project that would bring inspired architectural diversity to the neighbourhood. The panel appreciated the exterior expression of the building but expressed some concern about the quality of the clear fritted glass screens as they will predominantly define the image of the building. The success of the project's thesis will largely hinge on the execution of these details.

The panel felt that weather protection along the street edge could be investigated further. The panel suggested that the residential entries are not as developed as other building elements and suggested highlighting the residential entrances by extending the tower language as a consideration. Ground floor residential entrances at grade present some grade issues along the north and east, as openly discussed with the applicant.

The applicant presented a recent sketch (not originally in the submission package) that explored moving the amenity area inside. The panel supported this move in principle as it resolves the inside corner security issues and provides a continuous streetwall condition.

The panel discussed the large footprint of the tower elements that visually appear larger due to the wrap-around balconies but appreciated the massing move of dividing the bulk of the building into two towers rather than a continuous slab.

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

Previous Comments (PE2018-00310)

March 28, 2018

The proposed project is a mixed-use midrise project that is comprised of two towers joined by a low podium. The Panel expressed support for the general urban strategy of the project including breaking the mass into two towers to provide a variegated elevation rather than a single slab. Recognizing that the project has no “back” as it is surrounded by parks or public streets on all sides the Panel supports the attempt to activate all frontages with commercial uses or live-work townhouses at grade. The Panel’s comments and concerns focused primarily on the challenges of providing active uses given the steep grades and the treatment of the servicing area. The landscape design is yet to be fully developed. In particular the boulevard condition in front of the retail units is currently shown as a grass boulevard typical of residential frontage. Opportunities to incorporate LID, resolution of universal accessibility and the specific detail of the stepped terraces and proposed retaining walls should be explored in future explorations. Similarly, the landscape in front of the townhouses is undeveloped showing only a simple walkway to the front door. Patio space, public/private separation and separation between units should be explored. The patio/playground courtyard on the north side of the project presents public/private interface issues and is also undeveloped. The edge condition along the blank wall to the south of the bike parking room does not show design mitigations to screen or articulate the blank wall. Finally, the applicant noted that the current design of the tower facades with a full building screen in front of deep balconies will likely change due to energy considerations and that future submissions may be different in aesthetic. For these reasons and others noted below the Panel endorses the project with comment.

Applicant Response

September 18, 2018

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	Support
	UDRP Commentary		
	The proposal successfully provides retail street diversity on a challenging site.		
	Applicant Response		
	In an effort to provide opportunities for public gathering, we have proposed several new areas around the site for benches and landscape enhancement. These are located both on site (see areas within the north setback facing St. Matthew’s Square and offsite also facing St. Matthew Square Park within the enlarged boulevard area at the curve of St. Matthew. Along 9 Street, it is anticipate that the setback areas fronting the retail spaces will provide opportunities for a wide range of seating possibilities such as bench/planting combinations for informal short term seating (similar to a an Ice cream shop scenario) or a more formal chairs and tables type seating in front of CRU’s that are expected to attract restaurant or licensed establishments. We believe it is merely important to provide enough room, proper grading and visual security to allow these types of outdoor gathering spaces to evolve and adapt to the uses and micro-climates that will define them over time.		
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	Support
	UDRP Commentary		
	Project maximizes retail glazing and transparency.		
	Applicant Response		
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop	Further review recommended


Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

		offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	
	UDRP Commentary		
	The applicant has attempted to mitigate slope issues to provide reasonable accessibility given its challenging site. The pedestrian surface treatment does not appear to span the vehicular entry to the parkade. Some difficult areas still exist and require further study to resolve fully.		
	Applicant response		
	Although continuous pedestrian surfaces are desirable, the change in material for the main access drive surface (complete with standard City of Calgary concrete apron) will provide a visual queue for pedestrians that vehicles may be present. Private patios and onsite commercial setback frontage areas are paved in contrasting pavers to provide a subtle definition of the pedestrian walk and areas of seating and/or layby potential.		
4	Entry definition / legibility	Entry points are clear and legible	Further review recommended
	UDRP Commentary		
	Residential entries are not as developed as other building elements; review of these entries is suggested to strengthen concept further. A possible expression may be to highlight entrances by extending the tower language or other avenues to differentiate these entries nodes from the retail.		
	Applicant Response		
	Although the entry canopies seem undeveloped, the intent here is to execute a simple slab type canopy form that both highlights the entry point while not competing with the simplicity of the rest of the design. A minimalist composite white metal panel clad canopy that appears to pierce the glass wall is the general appearance we are after. This canopy treatment will be repeated at the south tower entrance as well as that of the north tower on 9th Street NE.		
5	Residential multi-level units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Further review recommended
	UDRP Commentary		
	The proposal incorporates residential units at grade. The Panel encourages the applicant to consider the private/public realm transition, incorporating thoughtful buffering in the form of gating and/or landscaping features to promote usable private amenity space at grade.		
	Applicant Response		
	Grade oriented unit entrance areas are defined with careful placement of planters with both low lying vegetation and shading tree elements that define the private/public edge and provide (with mature growth) a bit of a green canopy to each entry. A low pedestrian gate provides secured access for resident's private outdoor amenity. All patio surfaces for these amenities will be finished in modular permeable concrete pavers to provide better drainage and residential scale.		
6	At grade parking	At grade parking is concealed behind building frontages along public streets.	NA
	UDRP Commentary		
	NA		
	Applicant Response		
7	Parking entrances	Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.	Support
	UDRP Commentary		
	The proposal appears to have minimized parking entrances as best as possible. See item #3.		
	Applicant Response		
8	Other		
	Applicant Response		

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i>		
Topic	Best Practice	Ranking
9	LRT station connections Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	Support
UDRP Commentary		
The Design includes a clear north-south pathway connection to nearby LRT station and activates with retail uses at grade.		
Applicant Response		
Inclusion of commercial uses will provide greater activation of 9th Street NE and thus provide greater natural security for this important north/south pedestrian route throughout the day.		
10	Regional pathway connections Supports walkability via intentional urban design connections to pathway systems.	NA
UDRP Commentary		
Applicant Response		
11	Cycle path connections Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.	Support
UDRP Commentary		
Outdoor public bicycle storage at grade has been provided.		
Applicant Response		
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	Support
UDRP Commentary		
The proposal includes continuous sidewalks along all four sides and is mindful of the grade changes as it relates to adjacent uses.		
Applicant Response		
13	Pathways through site Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.	NA
UDRP Commentary		
Due to the site size, pathways through the site are not deemed necessary to support successful urban design for this project given strength of public realm interface.		
Applicant Response		
14	Open space networks and park systems Connects and extends existing systems and patterns.	Support
UDRP Commentary		
Retail/commercial facing 9 th Street and residential facing St. Matthew Sq. and McDougall Road support the current use patterns of adjacent sites.		
Applicant Response		
15	Views and vistas Designed to enhance views to natural areas and urban landmarks.	Support

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

UDRP Commentary		
The two-tower massing improves views to the city. The design also brings an inspired architectural diversity to the neighbourhood, enhancing the local streetscape.		
Applicant Response		
16	Vehicular interface	NA
UDRP Commentary		
Applicant Response		
17	Other	
Applicant Response		
Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i>		
Topic	Best Practice	Ranking
18	Massing relationship to context	Relationship to adjacent properties is sympathetic
		Further review recommended
UDRP Commentary		
Although visually dense the two-tower design breaks up the massing of the building. Continued study of the clear fritted glass screens is recommended as they will predominantly define the image of the building.		
Applicant Response		
During the past couple of months, the project design/engineering team has been exploring different strategies on executing the aesthetic look of the “patterned screen” that is the cornerstone of the application’s identity. Until a few weeks ago, we were leaning towards an “all glass” exterior skin. However, upon rendering under different lighting conditions, it became clear that the behavior of light/glare on the glass surface was going to compromise the reading of the vertical change of the pattern. Therefore, we have decided to move to an all composite metal panel solution that better matches the original renderings and strikes a better balance of constructability and aesthetics. See the latest rendering below:		
		
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites
		Support
UDRP Commentary		

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

	The two-tower massing somewhat minimizes shadow impact on adjacent sidewalks.		
	Applicant Response		
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
	UDRP Commentary		
	The proposal successfully addresses adjacent streets with grade-related uses and form.		
	Applicant Response		
21	Massing distribution on site		Support
	UDRP Commentary		
	Although visually dense the two-tower massing breaks up the massing of the building. The staggered placement of the towers maximizes the separation between the towers and views.		
	Applicant Response		
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Further review recommended
	UDRP Commentary		
	The panel commends the clean lines and architectural expression of the glass box podium though weather protection along the street edge could be investigated further.		
	Applicant Response		
	As previously mentioned, we believe that commercial establishments should be allowed the ability to provide their own seating and canopy solutions. The glass podium provides a common backdrop for any/all solutions to occur. (ie. trellis, umbrella, private planter trees etc.) Furthermore it is our experience that built-in type free draining architectural canopies pose their own challenges during icy/snowy freeze thaw conditions.		
23	Other		
	Applicant Response		
Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i>			
	Topic	Best Practice	Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Further review recommended
	UDRP Commentary		
	Moving the amenity area inside would resolve the inside corner security issues and provide a continuous streetwall condition. This option should be further studied as it was generally supported as shown, though design was not completely resolved at time of review.		
	Applicant Response		
	The north "inset" court has been deleted and the north building edge now engages St. Matthew continuously with additional grade oriented residential units.		
25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	NA
	UDRP Commentary		
	Not reviewed.		
	Applicant Response		

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)



	The towers have significant articulation (continuous deep screen/balcony) that should prevent wind shear or down drafts to occur.		
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.	NA
	UDRP Commentary		
	Not reviewed.		
	Applicant Response		
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.	Further review recommended
	UDRP Commentary		
	The panel recommends weather protection along the street edge could be investigated further.		
	Applicant Response		
	Majority of daily traffic in and out of the building will be through the tower lobby entrances. They are treated with their own special canopies. See note 22 for commercial retail units.		
28	Night time design		Support
	UDRP Commentary		
	The proposal provides active uses along its retail frontage and passive surveillance opportunities along its residential edges.		
	Applicant Response		
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.	Further review recommended
	UDRP Commentary		
	The proposal appears to have attempted to mitigate slopes on a difficult site. The main residential entries provide ramps and there are some residential units with at grade barrier free entries.		
	Applicant Response		
	Barrier free access into and around the public areas of the building are code governed and conformed to throughout. Private residential units are barrier free accessible from within the building corridors if not from the street.		
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.	NA
	UDRP Commentary		
	Not reviewed.		
	Applicant Response		
31	Other		
	Applicant Response		
Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i>			
Topic	Commentary		Ranking
32	(specify)		

Applicant Led Engagement Summary

Dominion – Engagement Overview

The Community of Bridgeland - Riverside

LOC2018-0059 & DP2018-3108

<u>Summary of Engagement Events:</u>		
	Date	Event / Meeting
1	Feb. 6, 2018	Meeting held at City Hall with senior planning officials to discuss opportunities for the site. Present at the meeting was: Wallace Leung, Rollin Stanley, Lynn McKeown, Mike Bucci, Troy Abromaitis and Tony Casola
2	Feb. 20, 2018	Meeting held with Bridgeland-Riverside Community Association Board (BRCA) at the Bridgeland-Riverside Community Hall The majority of the board was present and Ali McMillan chaired the meeting.
3	Feb. 28, 2018	First Public Open House at Bridgeland_Riverside Community Hall Approximately 17 groups attended and 9 comment cards were collected. 
4	May 22, 2018	Second Public Open House at Bridgeland-Riverside Community Hall Approximately 50 groups attended and 31 comment cards were collected. 
5	July 18, 2018	Meeting held with Bridgeland-Riverside Community Association Board (BRCA) and approximately 3 invited non-board member community

Applicant Led Engagement Summary

		residents. This meeting was held at the Bridgeland-Riverside Community Hall.
6	September 24, 2018	An information session was held at the Bridgeland-Riverside Community Hall to provide updates on changes to the project. Comments and questions were directed to Planning officials and to Bucci's project website . Approximately 43 individuals attended the session.

Summary of What was Heard at Engagement Events:

	Date	What was heard/discussed
1	Feb. 6, 2018	<p>At the meeting held with senior planning officials, initial thoughts on the potential for the site were shared. Some of the subjects discussed included:</p> <ol style="list-style-type: none"> Existing land use and challenges with achieving maximum allowable density. Initial concept diagrams and images were shared and distributed showing alternatives in massing and site development. Desire of the applicant to get to construction by February of 2019. Timelines were discussed specifically related to how the proposed site-specific land-use and the already contemplated Bridgeland-Riverside ARP amendment would need to align/coincide. As an initial meeting it set very preliminary expectations and timelines to provide a guide to future conceptual development.
2	Feb. 20, 2018	<p>The first meeting held with Bridgeland-Riverside Community Association Board (BRCA)</p> <p>This meeting was intended as an introduction to the initial concepts and a chance for the applicant to hear initial thoughts and concerns from the members of the board. Some of the subjects discussed included:</p> <ol style="list-style-type: none"> Support for the architectural expression of the initial images but concern about the ability to feasibly execute. Challenges with on-street parking and the pilot project initiative that was being contemplated for the area given recent concerns voiced by residents. Mix of unit types in light of a perceived need for a better mix of including suites suitable for families (3 bedroom units) Strong support for commercial at 9th Street in order to increase pedestrian activity. Some uses discussed included, neighbourhood pub, grocer, fitness, cafe, general retail etc.

Applicant Led Engagement Summary

		<p>5. Strong concern that no medical uses be proposed. 6. Timing of project phases to buildout. Concerns about delayed projects given the economic climate and previous Bridges projects.</p> <p>The majority of the board was present and Ali McMillan chaired the meeting.</p>
<p>3</p>	<p>Feb. 28, 2018</p>	<p>First Public Open House at Bridgeland-Riverside Community Hall</p> <p>This was the public's first opportunity to see the initial concepts being proposed. Approximately 14 Information boards were presented showing the basic concepts for the site, buildings and project statistics. A comment box was made available and the following summarizes the common issues / themes that were put forward:</p> <ol style="list-style-type: none"> 1. Buildings look very attractive. 2. Nice to have retail/commercial as part of the project. 3. Concerns about the proposed height (one tower shown as 15 storeys) and that residents have always "been told" when they purchase their homes in the area that 8 storeys would be the highest level that any future buildings would be. Desire to keep height under at least 10 storeys. 4. Shadowing from buildings a concern. 5. Concerns about entrance to parkade on McDougall Road, given traffic anticipated as the area density increases. Would prefer it if entrance was off of 9A Street instead. 6. On street parking would be nice for retail shoppers. Street parking is already very limited. 7. Site lighting should be shrouded to avoid offsite spill onto neighbouring residence within close proximity such as The Crossings. 8. More eyes on the street will be a benefit to the community. 9. Preference for condos as opposed to rentals. 10. Restaurant, Café and Bakery type uses preferred for the CRU's. 11. Noise from construction a concern. Please follow bylaws and as a courtesy keep loud activities like jack hammering for later in the day. 12. Love the landscaped podium roof top amenity. 13. Additional traffic for the area a big concern. 14. Bicycle parking should be conveniently located at grade. 15. Desire at-grade units facing St. Matthew's Square. 16. Allow for increased height with a guarantee of mixed use in the land use amendment. 17. Limit retail space uses to those that activate the street (not office, fitness, medical, vet clinic) 18. Concern with amount of glass being used and whether that is appropriate for historic Bridgeland. 19. Size of units and number of small units will mean that the park areas and amenities nearby become even more important. 20. Consider CEPTED issues to minimize opportunities for crime.

Applicant Led Engagement Summary

		<p>21. Any allocation of affordable housing? Could changes be made to make it affordable?</p>
<p>4</p>	<p>May 22, 2018</p>	<p>Second Public Open House at Bridgeland-Riverside Community Hall</p> <p>This was the follow up to the initial open house to provide the community an update on the land use application that showed more detail and development of the design. Approximately 16 Information boards were presented showing the basic concepts for the site, buildings and project statistics. A comment box was made available and the following summarizes the common issues / themes that were put forward:</p> <ol style="list-style-type: none"> 1. Commercial space on 9th would be great. 2. Love the look of the building aesthetics. 3. Building height as proposed is out of context with the rest of the neighbourhood. 4. Increase in height would set precedent for increase on other sites in our historic neighbourhood. 5. Proposed buildings could shadow Radius solar panels and cut-off views from St. Matthew church area toward Tom Campbell Hill. 6. Lower podium height at the corner of the site is welcomed. Maybe some of the massing from the towers could be relocated above this lower massing. 7. Too much glass usage for envelope. It's not energy efficient. 8. Consider street benches for the 1000+ seniors who live further to the south east of the neighbourhood. 9. Please factor in Crime prevention, we are getting too many condo thefts. 10. Promote need for traffic light at intersection of 9th Street and McDougall. 11. Concerned about traffic with so many added units and less parking. Especially on McDougall. Maybe more crosswalks? 12. Don't think FAR of 5.3 for this area is appropriate. 13. Concerned about shading of park in the winter months. 14. Don't like the idea of a rental building for the community. 15. Concerned about whether retail space will be successful. 16. Buildings look to blocky/no curves and wouldn't it make sense to step back the upper storeys? 17. Parking not sufficient. Parking in the community is lacking. Hard to find parking on the street already and it could get worse in the future with more development. 18. Entrance to the parkade could be a problem at rush hour periods. It will add more congestion to McDougall. 19. Loading and waste and recycling could be noisy and the trucks always idle while parked. 20. Could the podium roof top garden could be accessible by the public?

Applicant Led Engagement Summary

		<ol style="list-style-type: none"> 21. Slender massing seems to be a good compromise to lessen impact of height & big box look. 22. Traffic study is a must to determine if traffic calming is required along McDougall Rd. McDougall Rd. is currently be used as an alternate shortcut from Memorial Drive congestion. 23. Size of units is not family friendly. 24. Bucci has created quality projects that are all interesting and well designed. 25. Perhaps have parking for Car2Go to encourage less use of personal vehicles and thus traffic. 26. Positive and exciting development for the area. 27. We need more rental units ---- Good! 28. Units are so small and they will attract almost only singles and not families. Consider proposing some 3 bedroom units. 29. Appreciate maximizing density while not impacting comfort. 30. Like the two towers instead of mass building. 31. Developer must commit to executing the look of the building. Lesser quality is not acceptable. 32. Nice modern design.
5	July 18, 2018	<p>Meeting with Bridgeland-Riverside Community Association Board (BRCA)</p> <p>In addition to the board members, 3 invited non-board member community residents. This meeting was held to answer some questions that BRCA Board had about the development permit application. Some of the topics discussed:</p> <ol style="list-style-type: none"> 1. General update on the development permit with discussion of latest developments of the proposed development. 2. Discussion of height and shadowing and impacts on the neighbouring development to the north (Radius). 3. Residents in attendance wanted better clarity on the shadow studies to better understand the impact on the adjacent properties/amenities. 4. Discussion surrounding method and timing of notification of future residents of the community (specifically buyers of Radius condo)

Summary of <u>Response and Actions</u> to What was Heard:		
	Date	Response/Action to what was heard
1	Feb. 6, 2018	<p>Meeting with Senior Planning Officials</p> <p>Although this meeting did not engage the CA or the public directly it did help set the path for the overall scope and timeline of the development. The main actions were:</p>

Applicant Led Engagement Summary

		<ol style="list-style-type: none"> 1. Aim to maximize density; 2. Work with ARP amendment team to align with key elements of their application; and 3. Set targets for March land use amendment application and late spring/early summer DP application.
2	Feb. 20, 2018	<p>The first meeting held with Bridgeland-Riverside Community Association Board (BRCA)</p> <ol style="list-style-type: none"> 1. Given the Board expressed concern about the ability to execute the architectural look of the concept, Bucci and its design team began exploring options for the skin or screen element of the buildings so that a feasible (buildable) solution could be found. Also, discussions regarding concurrent applications and approvals for both the land use amendment and the development permit were initiated with planning to provide better assurance to the CA that the design would be executed to the intent of the land-use amendment. 2. On-street parking concerns raised by the board, led Bucci to engage Watt Consulting Group (Traffic Engineers) to begin preparation of a parking demand study and a traffic impact assessment (TIA). 3. To allow for flexibility of unit plan layouts, structural systems for the typical floor plate were explored. This would hopefully allow for potential of unit combinations and potentially allow for some 3 bedroom units. 4. The board voiced strong support for the commercial component. In response to this, we continued to development the retail street front to create the start of a high street experience along 9th Street. 5. Given the concern with certain uses and support for others, we began compiling the permitted and discretionary uses that we would look to include for the land use amendment application. 6. As full build out timing and execution was raised as a concern, the design team began planning how phasing the project could be delineated to minimize the risk of over building.
3 & 4	Feb. 28, 2018 & May 22, 2018	<p>Public Open Houses at Bridgeland-Riverside Community Hall.</p> <p>The comments received at both open house events were compiled and organized into recurring themes. The following summarizes our responses and what actions and/or changes that occurred because of what was heard:</p>

Applicant Led Engagement Summary

		<ol style="list-style-type: none"><p>1. Assurance of Design Commitment</p><p>As described in the initial BRCA meeting, the CA and several open house attendees, voiced a concern for assurance that the design would be executed as proposed (quality of look and inclusion of mixed use commercial). The concurrent LOC and DP process was recommended by planning as the best way to provide this kind of assurance to the community.</p> <p>2. Unique Architecture and Presence</p><p>We were very pleased to hear so many positive comments about the look of the project. We are also very proud of the project and given that the success of project depends so much on fulfilling this look, the design team will do everything they can to execute the vision of the concept and make it a reality.</p> <p>3. Materiality and Style</p><p>Concerns around the use of glass as it relates to both the context of Bridgeland were raised. Some expressed contextual concerns they have about whether a building that has so much glass could be sensitive to what many consider a long established and historic community. We believe that even within the most historic of settings, unique and exciting things can and should be allowed to occur and still be sensitive to their surroundings. The proposed building will have a material palate consisting of glass, metal and masonry. Together they will combine to create an extraordinary sculptural presence that we hope will make this project as striking, memorable and "photo worthy" as other iconic landmarks in our city. At the same time the project stays true to fundamental urban design principals to ensure that the edges of the site are safe, pedestrian friendly and activated.</p> <p>4. Height</p><p>Concerns about height were raised by several residents with some understanding that some degree of height increase from the old DC zones was inevitable. Although, we feel that the height being proposed is appropriate for a transit-oriented site such as this, we can also appreciate that this may seem like a significant increase when compared to the 7 and 8 storey heights of the existing zones. Our message to the residents regarding this was:</p><ol style="list-style-type: none">a) amendments to the Bridgeland-Riverside ARP will account for the potential of increased heights on sites within a TOD context.b) although the proposed height is increasing, the allowable density would effectively stay the same (5.3 FAR). The goal is to redistribute massing to allow for more diversity and uniqueness to the community.
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Applicant Led Engagement Summary

		<p>c) this massing strategy also allows for height relief at the corner of 9th Street and McDougall with effectively only a 2 storey massing across from the 8 storeys of The Crossings development and a break up of an otherwise very long block into two slimmer towers.</p> <p>d) The site is not immediately adjacent to any low-rise properties that would be sensitive to this height increase and the towers will not cut-off views of the downtown skyline or mountains from Tom Campbell Hill given the grade differential.</p> <p>e) The north tower has been reduced in height by one floor.</p> <p>5. Shadowing Related to the height were concerns about shadowing or shading of the adjacent spaces and buildings by the proposed development. In response to these concerns, we prepared and submitted several shadow studies of the proposed development and of alternate massing arrangements within the context of the surrounding neighbourhood. Each study shows how the difference in massing translates at different times of the day and year for each scenario. The studies also show that, even if the massing changes, as long as the volume (FAR) of the building remains consistent than the surface area of the shadowing also remains similar.</p> <p>The studies also confirm that the proposed development has minimal impact on the most sensitive areas to the west, Murdoch Park soccer field and playground structures. The Radius rooftop garden would only be in shade for approx. 4 hours during the spring and fall and not all during the summer months when it can expect the most use. The Radius roof top solar panels are on the north portion of the roof and will not be affected by the proposed building. To illustrate this further, shadow animations will be made available on the project website that show the exact path, extent of the shadowing and corresponding time lapse throughout the day.</p> <p>The reduction of one floor from the north tower will help in minimizing the impact of shadowing on the community hall and park beyond.</p> <p>6. Light Pollution Any proposed exterior lighting for the development will be shrouded to ensure minimal spillage beyond the subject site. Fixtures such bollards and down lights will all be utilized to maintain safe yet comfortable lighting levels in and around the site.</p> <p>7. Parking Both on-street public parking and private parking were raised as concerns at both open house events. Realizing most often</p>
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Applicant Led Engagement Summary

		<p>that this issue stems from lack or inconvenience of visitor and commercial parking, we have focused on providing all of the visitor and commercial parking on the first level P1 which is accessed immediately off of McDougall with no need for a trip down stairs or elevators. This will ensure not only the convenience to visitors and shoppers, but also the best chance for success of the future businesses that will inhabit the retail units. Close to 40 stalls on this level will be shared between shoppers in the day and then visitors in the evening to make the most efficient use of these stalls throughout the day. The parkade continues 2 storeys below this level to provide close to 229 stalls for 334 apartment suites translating to a ratio of approximately 0.68 stalls per unit.</p> <p>8. Dedicated Car Sharing Stalls Car2Go or other similar car sharing will be explored as part of a comprehensive TOD strategy for the development.</p> <p>9. Traffic Concerns about increased traffic along the two main adjacent roads (9th and McDougall) were raised. Specifically, how the increase in density and additional vehicles would affect the community. And in greater detail, how the entrance to parkade on McDougall Rd. would affect traffic on McDougall Road.</p> <p>As described earlier in this report, Watt Consulting Group was engaged to prepare both a parking demand study and a TIA for the development. This was submitted to planning administration in support of the land-use amendment. The details of these studies were thus made available to the public for their review. Any potential conflicts or traffic related challenges will hopefully be highlighted so that they can be addressed appropriately.</p> <p>The access to the parkade was specifically located to take full advantage of the slope of the site and allow vehicles and pedestrians to access the P1 level (visitor and commercial parking) with full convenience and barrier free accessibility. Vehicles leaving the parkade, do so without having to negotiate a ramp and thus limit their visibility of pedestrians and/or other vehicles on the road. Vehicles entering the parkade will have sufficient queuing length before the high speed roll up door to ensure that cars waiting to enter will not spill out onto the road or straddle the sidewalk.</p> <p>10. Need for traffic light at 9th Street and McDougall Road As noted above Watt Consulting Group (Transportation Engineers) prepared a traffic impact assessment (TIA) for this development. This report did not highlight a need for signaling of this intersection.</p>
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Applicant Led Engagement Summary

		<p>11. Bicycle Parking The need to maximize and improve bicycle parking for this development was mentioned several times by the open house participants. It is for this reason, that both Class 1 and Class 2 parking stall quantities exceed the minimum bylaw amounts and are located in convenient areas (at or close to street level) to ensure they are utilized as much as possible.</p> <p>12. Commercial Uses Although the vast majority of the open house attendees supported the introduction of commercial space along 9th Street, there were many that went further to ask for specific uses that were needed or desired and those that were seen as potentially problematic. As a result of this input, we worked carefully with Planning and the CA to include the exact uses that both the community and CA considered a potential benefit to the area.</p> <p>13. Construction Noise and Disruption Disturbances during construction are unavoidable; however, we have made the commitment to the community that these will be minimized. Due to the site layout and road network, loading or hauling from the western side of the property (9th Street) cannot occur without appropriate permission from the City of Calgary. Almost all access will be from St. Matthews Square and 9A Street.</p> <p>Lane closures will also be required. The unique site conditions make this a challenging build but we have assured the CA that we will adhere to the City of Calgary street use and noise bylaws.</p> <p>14. Condo vs. Rental The Calgary multi-family residential sales are not at level that can allow a project such as this to be economically viable as a market (condo) development. Currently, the only way this development can move forward is if it is conceived as a rental building and maximizes the density that is allowed with an efficient design. With the recent changes in mortgage rules (2% stress tests on loans applicants) and anticipated increases in mortgage rates, it remains unclear as to how long it will take for the condo market to return to favorable levels.</p> <p>15. Size of units and desire to attract families The current proposed floor plans have been conceived with the idea that, if demand dictated, larger units could be developed by way of combining smaller units to create either a greater number of 2 bedroom units or, larger 3 bedroom units. Currently however, market studies continue show trends towards smaller, more price point driven units.</p>
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Applicant Led Engagement Summary

		<p>16. Affordable Housing Dominion will provide competitive affordable rentals because it has been designed and conceived with efficiency in mind. Smaller units that work better allow buyers access to the lifestyle they desire but could not previously afford in an areas as great as Bridgeland-Riverside.</p> <p>17. Safety and Crime Prevention Concerns about safety within and around the proposed development were raised. As with all of our projects, we make every effort within our design intentions to minimize potential undesired behavior and activity around the site. The best way to achieve this is of course to maximize visual surveillance of the street by utilizing as much clear glazing as possible. The grade-oriented units will provide much of this visual connection, while the commercial uses on 9th should provide more activation during typically quieter evening hours. Other CPTED measures will also be employed around and within the parkade, common areas and exits within the building. Visibility into exits and vestibules, high lighting levels, overall brightness, controlled access hardware and security systems /cameras will all contribute to a safer environment overall.</p> <p>18. Public seating around the site Opportunities for the public to gather and just spend some time around the site are important to foster a sense of community. We have proposed several areas within the setback of the site for benches and landscaping. Further opportunities for seating can be allowed to evolve as the use of the retail units becomes clear. For example, if a business such as a café or ice cream shop occupies one of the CRU's then it would make sense for seating to be introduced immediately outside of that shop. These seating areas can also provide areas for older pedestrians to rest as they walk from through the community.</p> <p>19. Lower towers by transferring some their massing to podium level Although it seems like a logical strategy to bring the height of the towers down, transferring massing from the towers would compromise the overall look of the two towers over a podium severely. In order to reduce the towers by even a few storeys each, the podium would most likely increase in height to about 5 storeys in height. The resulting look would be of a large block mass once again with two raised portions of that block on either end of the block. Currently, the sleek low profile podium enhances in contrast the slender nature of the towers.</p>
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Applicant Led Engagement Summary

5	July 18, 2018	<p>Meeting with Bridgeland-Riverside Community Association Board (BRCA)</p> <ol style="list-style-type: none">1. The concerns raised here were from two future residents of the Radius Condo and were aimed at a need for more clarity on the shadowing impact and questioned the massing in relation to the shadowing specifically on the south face of Radius, and its roof top garden. In response to this, we worked with planning to update the shadow studies to show greater context and additional times of the day for each time of the year. Further to that, we have created animations of the shadow studies to better illustrate the shadow path as it sweeps across the surrounding sites.2. With respect to method of contact to be used to reach future Radius residents, Bucci has confirmed additional contact information and will ensure that these stakeholders receive appropriate notification of future engagement and information sessions.
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Troy Abromaitis
Director of Development
Bucci Developments

Tony Casola, Architect, AAA, SAA, MAA, AIBC
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Community Association Letters



Planning Committee
917 Centre Avenue NE Calgary AB T2E0C6
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2 October 2018

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Ali McMillan, BRCA Planning Director (planning@brcacalgary.org)

To Whom It May Concern:

RE: LOC2018-0059 (1018 McDougall Rd NE)

Thank you for the opportunity to comment again with respect to the application for a Land Use Amendment affecting land at 1018 McDougall Rd NE (LOC2018-0059).

This Land Use Application was discussed at meetings of our Planning Committee convened April 3, 2018, April 18, 2018 and again October 1, 2018 meeting. For the first two meetings we gave notice of the meetings to neighbours adjacent to the subject parcel through the Condo Boards, and notices were also dropped to residences north of Centre Avenue. However, for the October meeting, since the Applicant had just then recently held an open house (which due to scheduling constraints we did not attend) the latest updates / changes were reviewed by the Planning Committee only, without notice to or attendance by neighbours. Therefore these updated comments necessarily just build upon our understanding of what we have heard to date while working on this file and community planning matters more generally, but it should be read as an update only. The comments in our previous letters are also important.

1. It is not conceivable for BRCA to have engaged the community on the topic of what is the right direction for this land use initiative in the Bridges, or for the Bridges as a whole, alongside the evolving idea that significant changes to the current area plan are expected to be forthcoming. In our view the work undertaken to date has not itself been consistent enough in its direction, and nor (necessarily) have related engagement efforts produced enough clear feedback, about that larger community-wide topic, in order for us to take an definitive position about this specific—albeit very important—site. Although the latest updated ARP draft introduces the idea that a building height of 10+ stories may be

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appropriate in the subject location, a rationale as to why this is considered appropriate (including given the original Bridges plan and "vision") has not been provided. It is our feeling that the current draft of the updated community ARP has effectively been written to accommodate Bucci's proposal, and not the other way around.

2. As per our previous letters, many in the community feel that this proposed land use redesignation allows for a development that is too high and out of scale with the original Bridges Master Plan. Conversely, others feel this is likely an appropriate area for added height / density due to its TOD nature and because this proposal has been promised to bring a high quality architectural form (that last comment being the subjective view of some only). It is likely a fair simplification to say that the former view ("too high and out of scale") is more prevalent among those living in The Bridges and who are more immediately affected by the proposed deviation from the original Master Plan vision, and that the latter view ("an appropriate area for added height / density") is more prevalent among community members who reside further away. In all cases, however, the maximum height seems to be a reasonably contentious issue with the significance of the proposed heights drawing widespread concern and comment.
3. The Applicant's proposal to reducing the FAR from 5.8 to 5.3, and with the construction proposed, only removes one storey from one tower, and so that FAR change itself (a technical point not readily understood by many) has not much affected the overall height concerns of many residents. There seems, indeed, to be a disconnect between FAR and height in the Bridges – what the original plan stated and what was possible to build do not equate. Of course, developers will always want to default to the higher number, and will push to make the discrepant building-envelope parameters increase to match the largest item, which in this case is the 5.3 FAR.
4. We have heard strong support for active retail and food commercial uses in this development. In the long run, of course, the successful development of such uses is not controllable by land use and instead is impacted by many factors; but facilitating their potential is desirable.
5. Enhanced public realm design and landscaping is strongly supported.
6. There remains skepticism about shadowing impacts of the proposed development on Bridgeland-Riverside's primary or key asset of Murdoch Park / the community ice rink / the BRCA community hall. Also the outcome of the Radius rooftop garden being shaded 10:30 AM - 2:30 PM during spring and fall, as stated by the Applicant, is an undesirable outcome that would not be a problem with a shorter building.
7. The proposed height increases in this case are not being justified by an corresponding increase in density. Instead this is a case of FAR changing shape, from a built form that would present something squatter and along with a more uniform skyline around The Bridges open area perimeter, to a built form that proposes instead to create a visual break in such uniformity with a podium-and-towers approach. At the end of the day, to express a preference as between these two alternatives is to express an aesthetic—and possibly an economic (will something actually get built?)—preference. Yesterday's perceived best practices are today's mistakes, and how are today's perceived best practices perceived tomorrow?

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8. We do not support shared parking strategy for commercial. The Applicant suggests that since stalls are not titled, additional visitor stalls could be assigned as such, if required, at a future date. We would prefer to see the public interest recognized first here, meaning that visitor stalls are allocated at the outset to better protect the public realm and "parking load" that could spill out of the building and then, if they are not so utilized, this designation could be reconsidered after a suitable review / testing period.
9. The financial risk voluntarily taken by the developer to date is not a relevant planning consideration.
10. We see many positives in the upgrades now being presented in terms of the boulevard, outdoor patios, street lighting, and furniture at grade, and we commend Bucci's commitment to active uses at grade that may help foster street vitality and activation.
11. An early suggestion meant to promote consideration of a built form with a larger podium and two somewhat shorter towers, with the western tower framing the park at a lower height, and the eastern tower being relational but slightly taller, was unfortunately not explored. It seems likely that a lower-scaled overall height stepped in relation to nearby buildings would be more palatable overall than two buildings that are double the height of existing new buildings in the area. We would still encourage this to be explored.

Sincerely,

BRIDGELAND-RIVERSIDE COMMUNITY ASSOCIATION

Per: BRCA Board of Directors
Planning Committee

Community Association Letters



Planning Committee
917 Centre Avenue NE Calgary AB T2E0C6
broccalgary.org

20 Aug 2018

Circulation Control
Planning, Development & Assessment #8201
The City of Calgary
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Calgary AB T2P2M5

Attn: CPAG.Ciro@calgary.ca
cc: Christine Leung, File Manager (christine.leung@calgary.ca)
Ali McMillan, BRCA Planning Director (planning@broccalgary.org)

To Whom It May Concern:

RE: LOC2018-0069 (1018 McDougall Rd NE)

Thank you for the opportunity to comment again with respect to the changes to application for a Land Use Amendment affecting land at 1018 McDougall Rd NE (LOC2018-0059).

This Land Use Application was most recently discussed at a meeting of our Planning Committee convened August 7, 2018. Notice of that meeting was given to neighbours adjacent to the subject parcel through the Condo Boards and previous concerned residents via email. Approximately 4 neighbors attended, as did many regular Planning Committee members. The applicant was not invited to the meeting as they had just recently presented to us, however they did provide an updated package for this meeting that highlighted key elements of the project from their viewpoint.

The comments below regarding the LOC application reflect the feedback of both neighbours and the BRCA Planning Committee members present at the meeting.

It is our understanding that the commercial portion of the original land use application was mis-calculated and now the FAR has been increased to match the Development Permit Application (DP2018-3108). We are very supportive of the building having a commercial element, however when we were first presented with this application, it had two towers proposed – one at 12 stories and one at 10 stories. It has grown over the course of the applicant's planning to become two 15 storey towers. We understand the original FAR for the Bridges can be achieved in a variety of forms. The discussion here is what is most appropriate for the community, looking at the tradeoffs and planning merits of each – i.e. 9 storey block-design midrise building or tower-and-podium style design. And in looking at

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these, what is a suitable height for in this area of the community? TOD is important, but so is the impact of over-reaching heights that are suitable for the area. We are appreciative that the applicant is providing a DP at the same time as the land use application to provide some certainty to the land use – although we also feel that the land use needs to be tied to plans.

As previously outlined this application is in The Bridges area, a master planned part of our community (within the “Bow Valley” lands) and our comments regarding this from our prior land use letter still apply. We hope as this application moves forward all of our letters will be included in the packages as well as our comments on the Development Permit.

We are certainly concerned about the creep in FAR/ height with this application. The applicant’s package submitted to us for our meeting attempted to highlight what they feel the strengths of the project are overall. We would like to address each of these points individually although some are more DP comments.

1. *Landmark architecture and urban design, signaling arrival at the Bridges Community (Bucoi).* We are concerned as you will note in our DP comments that this proposed building cannot be built as shown. The applicant is proposing painted glass to get the effect. We are strongly concerned this will not give the look of the renderings being presented to the public, nor is the maintenance and long term feasibility of such an exterior well documented. We hope the Urban Design Review Panel and Calgary Planning Commission will look into this in depth as it is not our area of expertise – but we were certainly interested in this “look” architecturally when it was suggested as a structural design – so we want to ensure that the end product will satisfactorily meet expectations presented.
2. *Introduction of commercial uses on 9th St. to promote pedestrian traffic from 1st Ave to Memorial, the LRT, and live work units to activate at street fronts, and activate Murdoch Park (Bucoi).* We are happy to have ground floor activated units facing McDougall Road and Murdoch Park. We are concerned about the interface with 9th St due to the height of the tower closest and the lack of step-back at the podium level. 15 stories of massing straight up from the street and looming over the park seems out of context to us. We feel the front tower should be lower in scale to step down to the park and relate more to the street edge envisioned in the original Bridges Masterplan for 9th St. We also like the at grade amenity spaces for the units along McDougall.
3. *Enhance the character and vibrancy of the neighbourhood.* The renderings show enhanced landscaping along 9th St NE and a plaza area at the corner of 9th St and McDougall Rd. We like these elements and hope they will be in the final product if this project proceeds. The height however raises whether the character is actually being enhanced, or if a new 15 storey visual landmark in the area is an enhancement or an undesirable aberration. More modest height in line with original discussions that ties into existing buildings would be less likely to be the latter.
4. *Focus on pedestrian friendly, grade oriented streetscape to establish a strong sense of community.* As per comment 2 we are concerned about the dominance of towers, including the west tower over 9th St, and the massing at the pedestrian level not being

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human scaled. We like the podium interface with ample doorways/ entrances and lots of glass to provide "eyes on the street". We also like that the renderings show benches and trees along 9th St with enhanced landscaping to make the street level feel welcoming.

5. *Increased population base to support local retail and transit utilization.* This is misleading and not a strength of the proposal at all. This proposed development provides the same amount of FAR whether it is a 9 storey building or 2 towers. There is no increase in people being provided by this design vs the original format of Bucci 1/ Bridges Masterplan.
6. *Achieve Density of 5.8 FAR (5.3 residential FAR + 0.5 Commercial/ Live Work Unit FAR) by amending the existing height restrictions. Allow for greater flexibility in the built form to avoid monotonous block massing.* The "monotonous block massing" referred to here does not have to be monotonous if designed appropriately. The original Bridges Master Plan won awards for design. Midrise is an increasingly popular urban form. Building mass can be articulated and broken up with balconies and fenestration, as well as use of materials.
7. *Create potential for podium roof top garden with view to river and Calgary Skyline beyond.* This is not a community benefit as this is private space.
8. *Increases diversity in available housing types (studio, 1 bedroom, 2 bedroom and live work units at grade).* We have a lot of condo rentals in the area. We would like to see some 3-bedroom units for families and some variable ownership models made available.

As previously identified we support commercial uses on this site but do not support non-active uses such as medical which already tend to predominate in the community– we hope that the applicant will keep their identification of proposed uses similar to those in the unamended LOC application, as per our first letter. We would also like to see some tangible community improvements and benefits to the area and streetscape.

Parking comments in the original letter still apply.

The community remains concerned, and, in some cases strongly opposed, to the height proposed. The shadow studies are still incomplete. The most recent set provided by the applicant shows shadows of some buildings that are not actually built. We would like to see the shadow comparison of the original planned buildings to the current towers proposed omitting any unbuilt buildings or speculation about what they could be – just showing existing structures on the ground today. We would also like to see the shadowing of the lower towers as proposed earlier in the process. We have significant concern about the impact of the tower on the Community Association Hall, its amenity space, its outdoor playground, the rooftop garden, and the winter skating rink - as well as both parks to the west and south. The new towers would completely shade the rooftop amenity space on Bucci 1 which would be undesirable for residents of that building. None of these impacts are as much of an issue

Community Association Letters

with the original land use height, which is becoming increasingly apparent to us. The proposed benefits according to the applicant thus far do not seem to outweigh these issues.

We strongly ask for the City's support to approach the Bridges from a masterplan perspective. The original design around the park is critical to the Bow Valley Plan. The impacts of a significant departure from that plan must be carefully examined. We are looking with heavy reliance to the expertise of the Urban Design Review Panel and Calgary Planning Commission to get this right. The choice between a more uniform midrise style of construction throughout The Bridges (as originally conceived, more "Washington DC style") and an alternative involving punctuated higher-tower elements is—in the final analysis—a choice about subjective style and aesthetics built form, and about the related topic of commercial marketability. There is no "right" answer to any subjective question of aesthetics, and no doubt those who master planned The Bridges in the first instance thought they got it right. Now, not many years later, a different aesthetic outcome is considered desirable by some. In the context of a master planned community, however, and given that many people only very recently "bought into" that master planned vision, it is perhaps hard not to emphasize the marketability issues. Units in a tower may be easier to market today, but does that come at the expense of vested interests that relied upon a different vision?

Lastly, there has not been enough discussion of an appropriate contribution to public realm improvement in the community from larger scale developers such as the applicant. What is the clear public benefit to such a departure from the master plan? This has not been demonstrated to date in our view, or to that of the other community members who have attended meetings on this application.

Sincerely,

BRIDGELAND-RIVERSIDE COMMUNITY ASSOCIATION

Per: BRCA Board of Directors
Planning Committee

Community Association Letters



Planning Committee
917 Centre Avenue NE Calgary AB T2E0C6
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19 April 2018

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Planning, Development & Assessment #8201
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Ali McMillan, BRCA Planning Director (planning@brcacalgary.org)

To Whom It May Concern:

RE: LOC2018-0059 (1018 McDougall Rd NE)

Thank you for the opportunity to comment with respect to the application for a Land Use Amendment affecting land at 1018 McDougall Rd NE (LOC2018-0059).

This Land Use Application was discussed at a meeting of our Planning Committee convened April 3, 2018 and again April 18, 2018. Notice of that meeting was given to neighbours adjacent to the subject parcel through the Condo Boards and notices dropped to residences north of Centre Ave. Approximately 8 neighbors attended, as did many regular Planning Committee members. The applicant was not invited to the meeting as we had just recently had them present to us in March and nothing on the concept had changed.

The comments below regarding the LOC application reflect the feedback of both neighbours and the BRCA Planning Committee members present at the meeting.

The application is seeking a new land use designation for the site of DC with an increased height to 50m, additional uses (various commercial), a parking requirement reduction and greater flexibility with massing and design requirements.

This application is in The Bridges area, a master planned part of our community (within the "Bow Valley" lands), and which has been evolving since the demolition of the General Hospital in the late 1990s. The Bridges development was originally supposed to have been built out by 2008 but 4 lots in the Master Plan still remain undeveloped today.

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Being that the planned build-out date is now 10 years past, it seems reasonable to us that original plan constrains for the area might require revisiting today. BRCA so advised the applicant before this application was made.

That said, we also told this applicant, pre-application, that the Area Redevelopment Plan for Bridgeland-Riverside is presently being updated after a long period of advocacy by our community. The timing of this application certainly complicates that conversation. We would prefer that the future vision of the community be driven by the community and not by the parcel-by-parcel activities and aspirations of a variety of developers. However, at the same time, we can appreciate that The Bridges is a transit-oriented development site and that this, in general, is obviously an appropriate place for a conversation about increased density.

We have expressed to the applicant that we are open to discussions about tradeoffs in height and density but that high quality architectural design is paramount given the prominent location of this site and the height being requested in contrast to the surrounding buildings. To whatever extent it falls to the community to make the point, we also wish to emphasize that "density bonusing" is an important consideration here. Our community cannot sensibly be asked to accept density (height) beyond that which has been contemplated for many years on a vacant parcel, except with a view to asking at the same time what the community can get in return. As regards the building itself, the high quality of the product to be designed and built is, in fact, something that the community can get in return. Many in the community find it inappropriate to be having conversations of change outside of the original masterplan whilst the new ARP work is being done. It is essential that this application be reviewed as part of the broader plan for the area, which should be community driven not developer spot zoning driven.

The applicant provided concept renderings that show a very high quality architectural design – however, there are at this time no guarantees about what might actually get built. It is our understanding that the developer is still working on how this concept can be delivered and will be submitting a development permit application before this land use application appears before Calgary Planning Commission and Council. This is certainly what we would hope to see, considering the dramatic increase in height and density being requesting, and the overlapping timing with the ARP process. We would request that once the design is officially submitted, that the resulting designed development be directly tied to plans for this land use.

The applicant mentioned in their submission that they would like to hold more public engagement on the design before submitting the DP—which we support—providing this is accessible and broad (which has not been the case to date). The applicant held a pre-application meeting with the Planning Committee where we were told that an open house would be held. The applicant did a mailout, as mentioned in their submission, but in practice many in the community did not receive the mailout until after the date of the February 28, 2018 open house had passed. Several of our committee members attended the open house only to see just a few (<20) community members attend. For a project of this scale in this location this is not a sufficient turnout to call "engagement". We have discussed this with the applicant and they have assured us they will be implementing a more meaningful public engagement plan with better timelines. We cautioned the applicant that due to the reality of Canada Post community mailboxes in Bridgeland-Riverside, people do not check their mail that often so other means of notification should be pursued. We would like to see a DP and information we requested in this letter before this arrives at a council hearing on the land use. Bucci has been very willing to meet with the Planning Committee and we have a good dialogue – we need to caution that broad community engagement is critical beyond just the

Community Association Letters

CA to ensure all community wide concerns are heard and reflected. We will certainly support this increase dialogue however we can.

We feel that the community generally supports the addition of commercial uses at grade facing 9th Street NE. This parcel represents one of the last opportunities to activate 9th Street NE with commercial uses, and the failure for this to have occurred earlier is, we feel, an oversight in the original plan for The Bridges. However, the specific uses will be very important and we are appreciative that the applicant made some changes to the application in this regard. The intent of the community here is to create an active urban street drawing people up from the C-train to the main street of 1st Avenue NE while providing amenities that add vibrancy to the neighbourhood at a variety of hours of the day and night. We are not supportive of medical uses on this site but do in general support the updated uses specified in the applicant letter dated March 26, 2018 although lean to the intent of active uses i.e. restaurants. An urban format grocery store is highly desirable for this community. The community is open to this conversation but aware that every discussion involves tradeoffs that need to be clearly outlined in the discussions.

Regarding the parking requirements, we are generally supportive of less residential parking due to the TOD nature of the area, but since there is no broader parking plan for The Bridges, and because we often receive complaints about the lack of on-street parking for the daycare and other commercial uses, we must qualify our positive feedback to that extent. Visitor parking is also limited. We have asked the Ward 9 Councilor's office to update the parking pilot study that was completed for our community and establish a comprehensive parking plan for The Bridges in general prior to any final decisions on this matter. Please liaise with our ARP lead, Wallace Leung, on this front.

We are conscious that commercial uses need enough parking to be successful so would urge strong consideration on the commercial side of the parking question. The residential uses are perhaps of less concern, as people "buy in" knowing how many stalls they will have. We would request the applicant provide a strong parking rationale for our consideration in conjunction with any plans through the ARP more broadly for the area. Many at the meeting expressed concerns about parking, specifically for the commercial uses. No specific parking ask was submitted in this LOC so we would like that clarified.

Many on the Planning Committee appreciate the tower-and-podium style proposed, but as mentioned already this land use application does not guarantee that built form will be the end result. We understand the request for added FAR and are generally supportive but would not like to see this property "massed up in a block", thus effectively covering the site at a higher height. The breathing room and light that the two tower-and-podium style provides is pleasing to some – how can this be guaranteed within the land use being applied for?

There are however some in the community who also bought into the master planned area of "The Bridges" as a highly desirable mid-rise community. This is a significant departure from that plan and will have to be weighed carefully as part of the overall vision for the area. We would request the City's assistance in engaging the community more broadly at the choice before us. What justifies a departure from the master plan? What are the impacts on the community and development interests in the area more broadly? How will this interface with the plans for West Riverside and East Riverside? We need help gathering feedback from the community about this as there are broad implications for our master planning process in the ARP. We hope you can work in conjunction with the ARP work to provide feedback on this development that will ensure the broader context is being looked at simultaneously.

Community Association Letters

Overall the proposed constructible height seems quite pronounced for this location; it is effectively a doubling of what was originally permitted. We would request detailed shadow studies to show how this would affect single family homes to the north along Center Ave NE and the surrounding parks in order to adequately assess its impact. We also would appreciate some renderings that show the relational height of the proposed building to existing developments, including a broad enough scope to include the tower of Bridgeland Place. Since the ARP process is ongoing, a comprehensive look at relational heights in the area is essential. This land use will set a new tone that has not been contemplated broadly.

The skyline of the community is very important and we would like to fully understand the impacts. The natural escarpment slope must be considered, so rendered views of the impacts of this land use from the escarpment and Tom Campbell's Hill would also help the conversation.

Another concern is the timelines on the Bridges development itself, with the community being promised full buildout in 2008. This was obviously not the case. Any delays are not desirable. How will the two towers be built? If one is used to finance the other what are the timelines this would create. It is not the goal for this area to continue to be a construction zone far into the future although we can appreciate economic timing is a factor and we certainly want the redevelopment to go ahead.

Lastly, a discussion of an appropriate contribution public realm improvement in the community would be appreciated as density with amenity is key to enhancing the livability of this established community.

Sincerely,

BRIDGELAND-RIVERSIDE COMMUNITY ASSOCIATION

Per: BRCA Board of Directors
Planning Committee

**Planning & Development Report to
Calgary Planning Commission
2018 October 18**

**ISC: UNRESTRICTED
CPC2018-1130
Page 1 of 18**

**Development Permit in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE,
DP2018-3108**

EXECUTIVE SUMMARY

This development permit application, referred to as 'Dominion', was submitted by Casola Koppe on 2018 June 27, on behalf of the developer Bucci Development Ltd, and with authorization from the owner The City of Calgary. The application proposes a new mixed-use development consisting of:

- one building, with two residential towers at 14 storeys and 15 storeys high and a ground floor podium containing commercial and live-work uses;
- a total of 316 Dwelling Units and 18 Live Work Units;
- approximately 850 square metres (9,153 square feet) of commercial space;
- active frontages and active uses at-grade; and
- a total of 266 underground parking stalls.

Land use and policy amendments to the *Bridgeland-Riverside Area Redevelopment Plan* are required to accommodate the proposed development. An application to redesignate the site from DC Direct Control District to the Mixed Use – General (MU-1f5.3h50) District and policy amendments (LOC2018-0059 / CPC2018-0968) was reviewed concurrently with this development permit and will be heard on the same Calgary Planning Commission agenda.

The proposed development permit is in keeping with the applicable policies of the *Municipal Development Plan*, the *Transit Oriented Development Policy Guidelines*, and the *Bridgeland-Riverside Area Redevelopment Plan*, as amended.

ADMINISTRATION'S RECOMMENDATION:

That Calgary Planning Commission:

1. **RECEIVE AND ACCEPT** this report and attachments for information; and
2. Recommend the Development Authority, without having to return to Calgary Planning Commission, **APPROVE** development permit DP2018-3108 of a New: Retail and Consumer Service, Restaurant: Food Service Only – Small, Restaurant: Licensed – Medium, Outdoor Café, Dwelling Unit, Live Work Unit (1 building, 2 phases) at 1018 McDougall Road NE (Plan 0815793; Block 10; Lot 2) with conditions (Attachment 2), subject to the approval of the bylaw amendments associated with LOC2018-0059 by Council.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

**Development Permit in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE,
DP2018-3108**

BACKGROUND

This development permit application was submitted on 2018 June 27, by Casola Koppe representing the developer Bucci Development Ltd, and with authorization from the owner, The City of Calgary. This development permit is directly related to the concurrent policy and land use amendment application (LOC2018-0059 / CPC2018-0968), submitted on 2018 March 13 by the same applicant in order to redesignate the site from the existing DC Direct Control District (Bylaw 171D2015) to Mixed Use – General (MU-1f5.3h50) District to allow for:

- transit supportive mixed-use development;
- street-oriented building design for residential developments and active uses for commercial developments along 9 Street NE;
- a maximum building height of 50.0 metres / approximately 15 storeys (an increase from the 23.0 metres / approximately 7 storeys);
- a maximum floor area ratio (FAR) of 5.3 (no changes from the existing DC);
- more appropriate parking requirements considering the site's proximity to the Bridgeland-Memorial LRT station; and
- greater flexibility of massing options, to break up massing into separate forms instead of one large block mass.

During the review of the policy and land use amendment application, Administration recommended a joint review of the land use amendment and a development permit application through a concurrent process. As outlined to the applicant, the concurrent process provides benefits to all stakeholders through enabling a clear understanding of the intent and outcome of the land use amendment and development permit. Concurrent applications are ideal in instances where the built form outcomes are critical and central to the merits of the policy and land use amendments.

Administration also recommended that the development permit review reach a point where the review is completed prior to the land use amendment application going to Calgary Planning Commission for recommendation. A separate report for the policy and land use amendment (LOC2018-0059 / CPC2018-0968) will be heard on the same Calgary Planning Commission agenda.

**Development Permit in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE,
 DP2018-3108**

Site Context

The subject site is located in the northeast community of Bridgeland-Riverside. Following the demolition of the Calgary General Hospital in 1998, the subject site and adjacent City owned lands were part of a City initiated policy plan known as 'The Bridges'. The overall area of The Bridges is 14.9 hectares and is comprised of 16 sites, envisioned to be developed in three phases. Since the approval of The Bridges plan in 2002, a significant amount of redevelopment has occurred, both in terms of private residential and commercial development as well as development of public parks and community facilities. The Bridges plan is near completion with the exception of four undeveloped sites, including the subject site in Phase 3.

Presently vacant, the parcel comprises an area of approximately 0.42 hectares \pm (1.03 acres \pm). The subject site is located within 250 metres of the Bridgeland-Memorial LRT Station, walking distance to local commercial services, significant park spaces, and has easy access to the downtown core. The subject lands are moderately sloping and slope downward south towards the Bow River.

To the north, a new Multi-Residential Development, known as Radius, is nearing construction completion. St. Matthew Square, a small public park, is located to the northeast of the subject site. The vacant lands to the east are also subject to the same existing DC Direct Control District as the subject site. A mixed-use development, consisting of a seven-storey and an 11-storey building, exists directly south of the site. Lands to the west include Murdoch Park, which houses the Bridgeland-Riverside Community Association building, community gardens, a playground, playfields, multi-use pathways, and a surface parking lot.

As identified in *Figure 1*, Bridgeland-Riverside's peak population was in 2017, reaching 6,332 residents.

Figure 1: Community Peak Population

Bridgeland-Riverside	
Peak Population Year	2017
Peak Population	6,332
2017 Current Population	6,332
Difference in Population (Number)	0
Difference in Population (Percentage)	0

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Bridgeland-Riverside](#) community profile.

**Development Permit in Bridgeland-Riverside (Ward 9) at 1018 McDougall Road NE,
DP2018-3108**

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Application Review

Site and Building Design

The proposed development features a landmark design and is intended to signal an arrival to The Bridges and the larger community of Bridgeland-Riverside through unique architecture and design features. The proposed development consists of two residential towers, 14-storey and 15-storey respectively, situated on a two-storey mixed-use and live-work podium to be built-out over two phases. The proposed development is intended to align with the existing floor area ratio (FAR) of 5.3, have a maximum height of 50.0 metres, and contain 316 Dwelling Units and 18 Live Work Units at full-build out. The proposed development is street oriented as the podium provides active commercial frontages, including proposed café, retail, and restaurant units of various sizes, along 9 Street NE and along the southwest edge of the podium along McDougall Road NE. The commercial façade of these key frontages consists of a two-storey high, glazed, transparent edge to maximize the visual connections and sightlines between units and the street. The architecture of this commercial façade extends upwards to form a wind screen for the roof top amenity and garden. Given the significance of the southwest corner of the podium for solar exposure and street activation, a restaurant with patio seating is proposed at the western end of McDougall Road NE.

Transitioning eastward along McDougall Road NE, the parkade entrance, loading and waste and recycling, as well as a transformer are consolidated into a small frontage that minimize impact on the street. Screens and landscaping are proposed to minimize the visual impact of this servicing area from the street. The remainder of the frontage along McDougall Road NE, as well as the frontages along 9A Street and St. Matthew Square are comprised of a series of grade-oriented Live Work Units. Each unit has a separate entry and direct connection to sidewalks around the proposed development as well as private outdoor gardens with low fencing to assist in visually defining the transition between public and private spaces. The at-grade residential architecture is one or two storey (depending on the sloping grade) dark stone façade, visually separating it from the commercial glass façade. One or two storey feature columns provide separation and privacy between each live-work unit. The combination of colour and materials, including dark stone, enhances the contrast with towers above that utilize light metal materials. Residential lobby entrances will have large extending canopies to define and indicate their location from the street in addition to signage hanging from the canopies.

The two residential towers above the podium implement a unique patterned screen that consists of natural brushed aluminum metal composite panels in order to create a gradient pattern effect from top to bottom. The metal composite screen is wrapped around two metre wide balconies surrounding the towers. The envelopes of the towers will consist of floor-to-ceiling vision glass and spandrel window panels that will have a dark grey finish in order to contrast the lighter patterned screen.

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Public Realm and Outdoor Amenities Spaces

The proposed landscaping is designed to complement the unique design of the proposed building and assist to address the sloped site. At the street-level along 9 Street NE, podium commercial frontages are defined and enhanced by three distinct areas within an eight metre wide high-street corridor. These areas include an area for outdoor seating and public gathering that has appropriate grading and sightlines, an extra wide 3.0 metre sidewalk, and a defined edge of boulevard trees between the sidewalk and street curb. Contrasting paving patterns and boulevard trees in trenches are utilized, in contrast to conventional grass medians between the sidewalk and curb, to act as an extension of The City sidewalk and provide visual cues of the commercial active frontage and high-street nature of this segment of 9 Street NE. A 2.3 metre wide public sidewalk continues around the north, east, and south edges of the building. Along the southeast edge of the podium, at-grade live-work units and their associated patios are framed by trees, hedges and pedestrian-scaled privacy screening to assist in providing visual separation between public and private spaces. Live Work Units along the north and east edge of the podium utilize planters with trees, hedges, shrubs and privacy screening as a transition to elevated patio spaces. Public seating is provided along the edge of the sidewalk overlooking St. Matthew Square to provide additional public gathering spaces and seating opportunities as the adjacent park does not contain seating.

Above the podium, a roof top garden is proposed as an outdoor amenity space between the two towers. In contrast to the use of planters, mounded soil berms without any walls are proposed in tandem with a range of diverse trees, grasses and shrubs to create a unique green space on top of the podium. Meandering within the proposed garden amenity space are a series of gathering spaces that utilize movable outdoor seating, fire places, communal barbeques, bistro tables, community garden plots and raised composite wood decking.

Citywide Urban Design

The proposed development was reviewed by the City Wide Urban Design team through the CPAG review process. Citywide Urban Design had no comments on the proposed development permit application and stated that the application has responded well to the comments provided by the Urban Design Review Panel at the pre-application stage.

Urban Design Review Panel

Urban Design Review Panel (UDRP) reviewed the plans for the pre-application submission (PE2018-00310) on 2018 March 28. The pre-application plans provided the schematic design and intent of the land use redesignation. The panel reviewed the proposed development permit application on 2018 July 11 and endorsed the project with comments. UDRP comments from both meetings are contained in their entirety in Attachment 3 and Attachment 4, together with the applicant's response. Administration believes that the proposal has effectively addressed the panel's comments.

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Transportation Networks

A Transportation Impact Assessment (TIA) and parking study were required as part of the concurrent application. The TIA identified the existing road network can accommodate the proposed development. The subject site is within a 300 metre walking distance to the Bridgeland-Memorial LRT station platform. The site is also within a 50 metre walking distance to the Route 90 bus stop on 9 Street NE. The site has good pedestrian connections to the LRT station and adjacent bus stop. The proposed development will have direct vehicular access from McDougall Road NE. This proposed access location from McDougall Road NE protects the integrity of the St. Matthew Square and allows servicing of this development without driveway interruptions. The proposed development provides for sidewalks surrounding the perimeter of the site to facilitate movement of pedestrians and direct transit access.

Parking provided meets the bylaw requirements based on the proposed MU-1f5.3h50 district with the exception of visitor parking provided. The proposed residential and live work units will require a total of 187 parking stalls for residents. Total number of residential and live work unit stalls provided for residents is 222 stalls (a surplus of 35 stalls). Commercial parking requires a total of 44 stalls and 44 stalls are provided. Additional required visitor parking for the residential units and live work units is 31 stalls. Stalls within the underground parkade indicate shared visitor and commercial stalls, therefore, none of the proposed shared visitor and commercial parking has been counted as visitor parking (resulting in a deficiency of 31 visitor stalls). See *Figure 2* below for a summary of the proposed parking.

Figure 2: Proposed Parking Summary

	Required	Provided	Deficiency / Surplus
Resident parking (316 Dwelling Units and 18 Live Work Unit - combined)	187	222	+35
Visitor parking (316 Dwelling Units and 18 Live Work Unit - combined)	31	0 (proposed to be shared with commercial parking)	-31
Commercial	44	44	0

Bicycle parking requirements on site meets the bylaw requirements. The bicycle parking area has been designed for easier user access with electronic doors and ramps.

Utilities and Servicing

Sanitary sewers are presently available to service the development. A Sanitary Servicing Study was submitted, which determined no upgrades are required to the existing public infrastructure. Water mains are presently available. A fire flow calculation letter was also submitted, which determine that no upgrades are required to the existing public infrastructure. Storm sewers are available to service the development without the need for off-site improvements.

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Environmental

The applicant must address the removal of soil contaminant recommendations as outlined in the 2016, Revised Final Phase II ESA report by Golder and Associates. A prior to release condition has been included to address this outstanding item.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)* which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Municipal Development Plan (Statutory – 2009)

In accordance with the Urban Structure Map (Map 1) of the *Municipal Development Plan (MDP)*, the subject site is identified as being located within the Developed Residential Area – Inner City Area. The land use policies in section 3.5.2 states that Inner City areas should maintain and expand, where warranted by increased population, local commercial development that provides retail and service uses in close proximity to residents, especially in the highest density locations. Buildings should maximize front door access to the street and principle areas to encourage pedestrian activity.

The MDP's City-wide policies, Section 2 and specifically Section 2.2 Shaping a More Compact Urban Form provides directions to encourage transit use, make optimal use of transit infrastructure, and improve the quality of the environment in communities. The intent of these policies is to direct future growth of the city in a way that fosters a more compact and efficient use of land, creates complete communities, allows for greater mobility choices and enhances vitality and character in local neighbourhoods.

The relevant policies within Section 2.2.2 are:

- a. Locate transit-supportive land uses, including higher density residential and employment developments, within Activity Centres and Corridors supported by Primary Transit Network.
- b. Increase development densities in proximity of the Primary Transit Network by targeting residential and employment intensities within 400 metres of transit stops, in areas deemed appropriate through the Local Area Planning process and in accordance with the Typology thresholds identified in Part 3.
- c. Locate land uses that will generate counter-flow transit ridership during peak-hour commuting times and support non-peak hour ridership.

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The subject site is located within a 250 metre radius of the Bridgeland-Memorial LRT Station. The proposed development achieves the maximum density of 5.3 FAR and proposes additional commercial uses in the community that is transit-oriented and is in keeping with the above MDP policies.

The MDP also contains city wide policies on urban design. The policies speak to the design of tall buildings, which are defined as buildings whose height is greater than the width of the right-of-way of the street that they front. In accordance with this definition, the proposed development, is considered to be a tall building. The MDP states that tall buildings are appropriate in the Centre City and Major Activity Centres, or in Community Activity Centres and Urban Main Streets where deemed appropriate through a Local Area Plan. While the subject site is not identified as one of the above typologies, Administration considers the subject location to be appropriate for a tall building. As such, policy amendments to the *Bridgeland-Riverside ARP* are required and are included with the concurrent policy and land use amendment application.

In accordance with the MDP, the proposed development is encouraged to make a positive contribution to the city by creating an interesting skyline, acting as a landmark and contributing to orientation and way-finding within the community of Bridgeland-Riverside. The MDP also includes design principles for tall buildings, and states that the ground floor of the development should demonstrate a strong relationship to the human scale and contribute positively to the public realm and street. The proposed development includes building setbacks, step-backs, articulations and recesses to help mitigate the perception of mass and height and addresses the tall building policies in the MDP.

Bridgeland-Riverside Area Redevelopment Plan (Statutory, 1980)

The *Bridgeland-Riverside Area Redevelopment Plan (ARP)* was adopted by Council in 1980. In 2002, a major amendment (25P2000) to the ARP was approved to include the Bow Valley Centre lands (former Calgary General Hospital). The amendments included policies and goals for The Bridges planning area. The policies were integrated in the *Bow Valley Centre Concept Plan*, which forms section 9 of the *Bridgeland-Riverside ARP*. See the sub-section below for further information.

It should be noted that The City of Calgary has started the process of drafting a new Area Redevelopment Plan (ARP) for Bridgeland-Riverside. Since 2002, major infrastructure improvements have taken place in the area (i.e. LRT, community centre site) and the new MDP policies have been adopted in 2009. Draft policies and land uses are being developed and reviewed. This proposal has been assessed under the current ARP and Administration is recommending amendments to the existing ARP to support the proposed development. The proposed policy amendments are consistent with the MDP, existing and planned context of the area, and is expected to comply with the new ARP policies. The draft ARP direction is also intending to consolidate the Bow Valley Concept Plan within the new Bridgeland-Riverside ARP. The new ARP is anticipated to be presented to Council by mid-2019.

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***Bow Valley Centre Concept Plan (BVC), Section 9 of the Bridgeland-Riverside Area
 Redevelopment Plan (Statutory, 2000)***

This section of the ARP is for The Bridges planning area. Section 9.1 of the BVC states the objectives of the plan including:

- Sensitive housing intensification;
- Wider range of housing choices for different age and income levels;
- Transit supportive development, including mixed-use development;
- Pedestrian and cycling supportive public systems that connect well with adjacent lands;
 and
- An improved physical environment.

The subject site is located within Multi-Family Residential typology as identified on Figure 19 – Conceptual Land Use map of the BVC. Residential area is represented by a variety of multi-residential development is encouraged throughout the site, including street townhouses, stacked townhouses, apartments, and ‘live/work’ units. Development will be characterized by an orientation to the street (i.e. ground floor units will have direct access to the street) and by building heights that are in the mid-range (3-8 storeys). A summary table of land use policy areas (Table 1) is included in the BVC and indicates the subject site being appropriate for development with a height of up to 23.0 metres and up to 5.3 FAR. Section 9.4.1 Residential, Parking policies requires parking of 1.25 parking stalls per unit. Policy amendments, including height and parking, are required to accommodate the proposed development. These amendments are contained in the concurrent policy and land use amendment application (LOC2018-00059 / CPC2018-0968).

Transit Oriented Development Policy Guidelines (updated 2005)

The subject site is located within a 250 metre walking distance from the Bridgeland-Memorial LRT station and is considered a Transit Oriented Development (TOD). The TOD Policy Guidelines contain six key policy objectives that apply to sites in close proximity to transit:

1. Ensure transit supportive land uses.
2. Increase density around Transit Stations.
3. Create pedestrian-oriented design.
4. Make each station area a ‘place’.
5. Manage parking, bus, and vehicle traffic.
6. Plan in context with the local communities

The TOD Guideline objectives that address site and building design, parking, edge conditions, access and circulation have been met for the proposed development.

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Land Use Bylaw 1P2007

The proposed development generally complies with the Land Use Bylaw 1P2007 with the exception of a few minor discrepancies. These Bylaw discrepancies are identified in the table below and are supported by Administration as the development still meets the intent of the rules, and relaxations of the rules has no adverse impact on adjacent developments.

Bylaw Relaxations		
Regulation	Standard	Provided
247 Outdoor Café	(f) has a maximum area of 25.0m ² in the C-N1,C-N2,C-C1,C-C2,C-COR1, C-COR2,C-O,C-R1,I-B,CC-MHX, CC-X,CC-COR,CC-ER,CC-ERR, CC-EMU,CC-EIR,CC-EPR,CC-ET,MU-1,MU-2,M-H1,M-H2,M-H3, M-X1 and M-X2 districts.	Plans indicate the proposed Outdoor Café (CRU #1) with an area of 82.65m ² (+57.65m ²). <i>The proposed Outdoor Café is located along the corner of 9 Street and McDougall Road NE. This active use in a location near transit and along the main connection from transit into the community is highly encouraged. Relaxation is supported.</i>
282 Restaurant: Licensed – Medium	(d.1) must not be within 45.0m of a residential district when the use is located within the C-C1, C-C2, C-COR1, C-COR2, CC-COR, CC-X, MU-1, MU-2 and S-R Districts, which must be measure from the building containing the use to the nearest property line of a parcel designated as a residential district.	Plans indicate a separation distance of 23.95m (-21.05m) between the building containing the use and the property line of a parcel designated as a residential district. <i>The policy indicates the planned area is to include a mix of uses including restaurant uses, in proximity to residential uses. Relaxation is supported.</i>
1374 Setback Area	(3) For the storey closest to grade, the maximum building setback from a property line shared with a street is 4.5m for 60% of the length of the building façade that faces the street.	South setback: 27.35% of the façade is within 4.5m of the South property line Northeast setback: 0% of the façade is within 4.5m of the property line (the setback provided is 4.77m) East setback: 0% of the façade is within 4.5m of the property line (the setback provided is 5.77m) <i>The proposed development does not meet this maximum building setback rule on the south, east, and northeast side but does meet the intent of the rule. Patio spaces and active frontages are provided along these street edges. Relaxation is supported.</i>

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1338 Street Wall Stepback	Where the height of a building is greater than 23.0m measured from grade, the façade of the building within 6.0m of a property line shared with a street must have a horizontal separation from the portion of the façade closest to grade where: (b) the horizontal separation occurs between a minimum of 7.5m and a maximum of 23.0m measured from grade.	Plans indicate a horizontal separation of 2.0m from the portion of the façade closest to grade that occurs below 7.5m from grade. <i>The concept is based on a 2 storey podium and the 2.0m step-back occurs above the top of the podium at a height of approximately 6.1m. The intent of the rule is met. Relaxation is supported.</i>
1342 Rules for Commercial Uses Facing a Street	(1) Unless otherwise referenced in subsection (2), the façade of a building located on the floor closest to grade and facing a street must provide windows with unobscured glass that: (a) occupy a minimum of 65.0% of the façade between a height of 0.6m and 2.4m; and	Plans indicate the unobscured glass area on the South façade facing the street occupies 40.54% (-24.46%) of the façade between 0.6m and 2.4m. <i>Portion of the façade not in compliance is the loading area and garage access to the parkade. Where commercial uses are located, the requirement is met. The proposal complies with the intent of the rule and relaxation is supported.</i>
1343 Internal Access to Uses	The uses listed in the Residential Group of Schedule A to this Bylaw, with the exception of a Hotel, may only share an internal hallway with any other use in the Residential Group of Schedule A to this Bylaw, with the exception of a hotel.	Plans indicate an internal hallway shared between residential and commercial uses. <i>The corridor to the north access door is proposed to be shared between Live Work Units and commercial uses. The hallway will be for emergency fire exit only. All Live Work Units have independent access from the street. Relaxation is supported.</i>
Motor Vehicle Parking Stalls	31 visitor parking stalls required.	Plans indicate 0 (-31) visitor parking stalls. <i>The development is proposing shared visitor and commercial parking on site. The required commercial parking is met and the site currently has a surplus with 35 residential stalls. The proposed visitor parking was supported by the TIA. Relaxation is supported.</i>
Loading Stalls	2 loading stalls required.	Plans indicate 1 (-1) loading stalls. <i>Loading location has been reviewed by Transportation and it's deemed the one still relaxation is appropriate. Relaxation is supported.</i>

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Stakeholder Engagement, Research and Communication

Communications

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. The application was also included in the Planning and Development Map (PDMaP), which is an online tool that includes relevant information on planning applications.

In addition, a City [project page](#) was launched on 2018 April 09. The project webpage was used to outline the application process, timelines, and engagement opportunities available. It also served as a resource for project information, links, and contact information.

The decision made by the Development Authority, following approval of policy and land use amendment, will be advertised in accordance with the *Municipal Government Act*.

Applicant-led engagement

The applicant led their own engagement program, which included two meetings with the Bridgeland-Riverside Community Association, two open houses, and one information session. The applicant mailed out post cards through Canada Post to adjacent residents in advance of each applicant-led engagement event in 2018 February, 2018 May, and 2018 September. Administration attended one open house and the information session to share information on the planning review process and answer questions. Comments heard during the two engagements paralleled key themes received directly by Administration as a result of notice posting and circulation that are summarized below. The applicant has provided a summary of their engagement, which can be found in Attachment 5.

City-led engagement

Administration assessed the application and it was deemed that additional City-led engagement was not required.

In keeping with Administration's standard practices, stakeholders were given the opportunity to comment online through the Planning and Development Map or by contacting the planner directly by mail, phone, or email. Administration received 37 letters regarding the application and three letters from the Bridgeland-Riverside Community Association.

The Bridgeland-Riverside Community Association (BRCA) submitted separate letters for the proposed land use amendment and for the development permit application. The original BRCA letter for this development permit was received on 2018 August 20. Subsequently, an updated letter was received on 2018 October 05, prior to CPC. The letters from the BRCA can be found in Attachment 6.

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What we heard / what we did

In response to the notice posting and the applicant-led engagement, 37 letters were received regarding the proposed development. Comments received included two letters of support and 35 letters of objection, including one letter from the Pontefino II Condo Board. Given that the concurrent development permit was circulated during the land use amendment application review period, the comments received were based on the shared information of the concurrent applications.

The two letters of support received by Administration identified support for this type of development and density near the community centre, community parks, and LRT station as opposed to the areas elsewhere in the neighbourhood that have a higher concentration of single-detached homes. The letters also indicated that supporting these concurrent applications will allow for land that has been bare for far too long to be a home for more new neighbours and new customers to support local businesses.

The main reasons for opposition that were received by Administration in the 35 letters of objection are summarized in the table below, including a response in how this feedback was addressed by Administration and/or the applicant to inform the final proposal.

Theme	What we heard (issue or opportunity identified)	What changes were made and/or response to the issue identified
Building Height	<ul style="list-style-type: none"> • Height of 50 metres is too high; • Does not fit the scale of the community and surrounding buildings; • Bridgeland is not a community of high rises; • The community assumed the existing height (7 storeys) would stay in place, and that expectation should be maintained; • Tall buildings or high-rise buildings would destroy the residential feel of Bridgeland and should be located closer to downtown where they are expected; • Allowing the height would be precedent setting; • The majority of this area has already been built and questions about why the proposed height changing now; • 8 storey is more than enough; and • Bridgeland Crossing near the station is the only 10 storey building in The Bridges area. 	<p>Concerns about height was raised by several residents. Although the proposed height is increasing, the allowable density is the same (5.3 FAR). The intent of the proposal is to allow for greater flexibility of massing options (break up massing into separate forms and instead of one large block) than is currently allowed to achieve the maximum density.</p> <p>Additional shadow and massing studies have been reviewed and accepted.</p> <p>The concurrent DP also demonstrates the massing of the taller building is mitigated through a thoughtful building design. The ground floor of the development demonstrates a strong relationship to the human scale and contribute positively to the public realm and street. The proposed development includes building setbacks, step-backs, articulations/ recesses to help mitigate the perception of mass and height.</p>

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Views	<ul style="list-style-type: none"> • Diminishing views for nearby residents; • Radius I purchasers paid a premium for the property with the view; • Decrease quality of life; • Decrease the desirability of the area as the new building will not be cohesive with the existing developments; and • The location of the north tower will not have any setback from street level, the proposed tower is adjacent to the Murdoch Park and the building will take away the openness of the park by impeding the view of the sky from the park. 	<p>Access to view is not regulated in the Bylaw and generally not a planning matter that can be considered.</p> <p>The north tower includes a two metre step-back from the commercial podium.</p>
Shadowing	<ul style="list-style-type: none"> • Potential shadowing impacts to Murdoch Park; and • Potential shadowing on the Radius building roof top garden. 	<p>Sun shadow studies have been provided by the applicant and the potential locations of shadows are acceptable. The studies indicate part of the park will be partially and temporally shadowed during early morning hours (i.e. 8 am) in June, March, September, and December.</p>
Property Values	<ul style="list-style-type: none"> • Concerns related to potential loss in property values due to proposed development. 	<p>Property values are not a planning matter that can be considered.</p>
Parking / traffic	<ul style="list-style-type: none"> • Reduced parking of 0.53 units or 187 stalls for 347 units will put pressure on an already very limited parking area; • Implications of density on existing limited parking in the area; • It is untrue that people who live in TODs rely on public transportation and do not own a vehicle; • Summer time sees parking as more difficult with outdoor markets, proximity to LRT, and parking for the East Village; • More units will create more traffic to the surrounding area; • Traffic study must be required; and • With reduced parking on-site, more street parking will be required and will take up the available street parking for the community hall. Regular events are held at the community hall and renters of the hall uses the street parking. 	<p>Generally, parking demand in areas well served by transit, that are density populated, that are designed to support pedestrians and cyclists and where daily needs can be met in local shops and services have lower parking needs.</p> <p>A Transportation Impact Assessment and parking study was submitted, reviewed, and accepted for the proposed land use amendment.</p> <p>As per the concurrent DP review, there is no parking relaxation proposed for the residential and commercial uses. The parcel provides for 222 parcel stalls for 316 Dwelling Units and 18 Live Work Unit. Visitor parking is deficient by 31 stalls; however, the development is proposing shared visitor and commercial parking as per the TIA.</p>

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ARP comments	<ul style="list-style-type: none"> The existing community assumed the zoning that allows for 7 storeys would stay in place, and that expectation should be maintained; The current ARP allows a maximum height of 8 stories; and Concern that the new ARP process is still underway and not finalized. Proposed new developments that substantially deviate from the existing ARP should wait. 	<p>Administration has responded to the residents' concern on the land use amendment and explained that a landowner or authorized agent can apply and seek land use amendment the property. Administration must review the proposal and make a recommendation to CPC and Council.</p> <p>Although the ARP is still in progress, Administration must review and make a recommendation on the proposed application.</p>
Communication	<ul style="list-style-type: none"> The Radius purchasers were not well informed of the proposed land use amendment; 	<p>The City's notification process included notice posting on site, and notification letters were sent to adjacent land owners (for the land use amendment). Since the owners of the Radius building had not moved in the building yet, they would not have received the notification. The City also does not have the contact information for these purchasers. A lack of notification was indicated by Radius purchasers and Administration has brought this issue to the applicant's attention. Bucci Development provided Radius purchasers the details of the project via email and encouraged comments to be submitted. Notification of the information session was sent directly to Radius purchasers.</p>
Commercial development	<ul style="list-style-type: none"> Supports the proposed commercial development; Community needs more local grocery stores; Cannabis store and grocery store is inappropriate in this location, this location is very transient with lots of crime; and Grocery store should be located on 1 Avenue. 	<p>Administration supports development of a mix of residential and commercial/retail uses at this location. According to the concurrent development permit, no grocery store or cannabis store is proposed.</p> <p>The proposed policy amendments also encourage active commercial uses along 9 Street NE, which would prohibit uses such as offices, medical office.</p>
Other Comments	<ul style="list-style-type: none"> A rental building would change the character of the area; No cell phone towers on top of the buildings; and Concerned if this development is not approved, a future design may not be as attractive for the City and the neighbourhood. 	<p>In terms of rental vs condo units, Planning is not concerned with the user of the site but the proposed use – which are all considered Dwelling Unit.</p> <p>There are no cell phone towers proposed on top of the proposed development.</p>

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Social, Environmental, Economic (External)

The proposal supports the MDP sustainability policies by developing in the Developed Residential Area – Inner City Area in a more compact urban form. The site is located near community amenities, employment opportunities, parks, and transit. The proposal will help promote high quality walking environments along 9 Street NE, which is the key transit connection from the LRT station to the community.

As per the applicant, this project targets Leadership in Energy and Environmental Design (LEEDv4) Homes Multifamily Midrise Certification. Sustainability strategy being considered in the development and construction of this project will include reduction of potable water, rain water management, heat island reduction, use of regional materials, managing indoor environmental quality, and energy efficiency.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASONS FOR RECOMMENDATIONS:

The proposed development aligns with and advances the goals and policies of the *Municipal Development Plan* of encouraging compact urban form, supporting intensification near primary transit network, and ensuring high quality urban design. The proposed development along 9 Street NE enhances the adjacent public realm through providing a human-scaled podium design, and an appropriate mix of uses, including active commercial uses, that contribute to the vitality of this inner city and transit oriented location and key corridor within the larger community. The proposed development is also in keeping with the *Transit Oriented Development Policy Guidelines* and the *Bridgeland-Riverside Area Redevelopment Plan*, as amended.

The proposed development permit application cannot be determined prior to the approval of bylaw amendments associated with LOC2018-0059 / CPC2018-0968 by Council.

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ATTACHMENT(S)

1. Proposed Development Permit Plans
2. Conditions of Approval
3. Urban Design Review Panel Comments (PE2018-00310)
4. Urban Design Review Panel Comments (LOC2018-0059 / DP2018-3108)
5. Applicant Led Engagement Summary
6. Community Association Letters

Proposed Development Permit Plans



DOMINION
1018 McDOUGALL ROAD NE

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T: 403-230-3218



Structural:
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Bryson Markulin
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Mechanical & Electrical:

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Kevin Vaj (Mechanical)
Jason Paine (Electrical)
SRC Engineering
804-268-9091
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Surveyor:
Eclipse Geomatics and Engineering LTD
T: (403) 256-1278



LOC APPLICATION: 2018-0059
DP PRE-APPLICATION: PE2018-00310

ARCHITECTURAL DRAWINGS

- A0.00 TITLE SHEET
- A0.01 SITE SURVEY
- A0.02 EXISTING SITE PHOTOS
- A0.03 SHADOW STUDIES (JUNE)
- A0.04 SHADOW STUDIES (DECEMBER)
- A0.05 SHADOW STUDIES (MAY/SEPT)
- A1.01 PROPOSED SITE PLAN
- A1.02 WASTE & RECYCLING ROOM
- A1.03 SITE DETAILS
- A1.06 BOULEVARD SECTIONS
- A1.07 BICYCLE PARKING DETAILS
- A2.01 LEVEL 1 - P3
- A2.02 LEVEL 2 / P2
- A2.03 LEVEL 3 / MAIN (SOUTH) / P1
- A2.04 LEVEL 4 / MAIN (NORTH) / P1
- A2.05 LEVEL 5 - ROOF GARDEN
- A2.06 LEVEL 6 - 17 (TYPICAL FLOOR PLAN)
- A2.07 ROOF PLAN
- A2.09 COMMERCIAL UNITS
- A2.10 UNIT PLANS (TYPICAL)
- A2.11 UNIT PLANS
- A2.12 UNIT PLANS
- A2.13 UNIT PLANS
- A3.01 SOUTH ELEVATION
- A3.02 WEST ELEVATION
- A3.03 NORTH ELEVATION
- A3.04 EAST ELEVATION
- A4.01 BUILDING SECTIONS (TOWER 1)
- A4.02 BUILDING SECTION (TOWER 2)
- A4.03 BUILDING SECTION

ELECTRICAL DRAWINGS

- E1.01 PLAN SITE LIGHTING
- E1.02 ELEVATION SITE LIGHTING

LANDSCAPE DRAWINGS

- L1 SITE PLAN
- L2 PLANTING PLAN
- L3 PUBLIC PLAZA ENLARGED
- L4 AMENITY ENLARGED
- L5 WALLS GRADES AND HARDSCAPE
- L6 SITE LIGHTING
- L7 ROOF TOP GARDEN
- L8 ROOF TOP PLANTING
- L9 ROOF TOP LIGHTING
- L10 DETAILS
- L11 SPECIFICATIONS

CIVIL DRAWINGS

- C1 SITE GRADING
- C2 SITE SERVICING
- C3 OVERLAND DRAINAGE PLAN

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REVISIONS

Number	Date (yy/mm/dd)	Description
1	2018-06-26	DR 001
2	2018-06-27	DR 002 OF 5 REVISIONS
3	2018-06-28	DR 003
4	2018-06-28	DR 004



PROJECT NAME AND ADDRESS:
DOMINION
1018 McDougall Rd NE
(Lot 2, Block 10, Plan 081 5793)

DRAWING:
TITLE SHEET

DRAWN BY: JORNO
CKARCH1720
CHECKED BY: S. BUI
TC
DATE: 2018/06
A0.00
SCALE

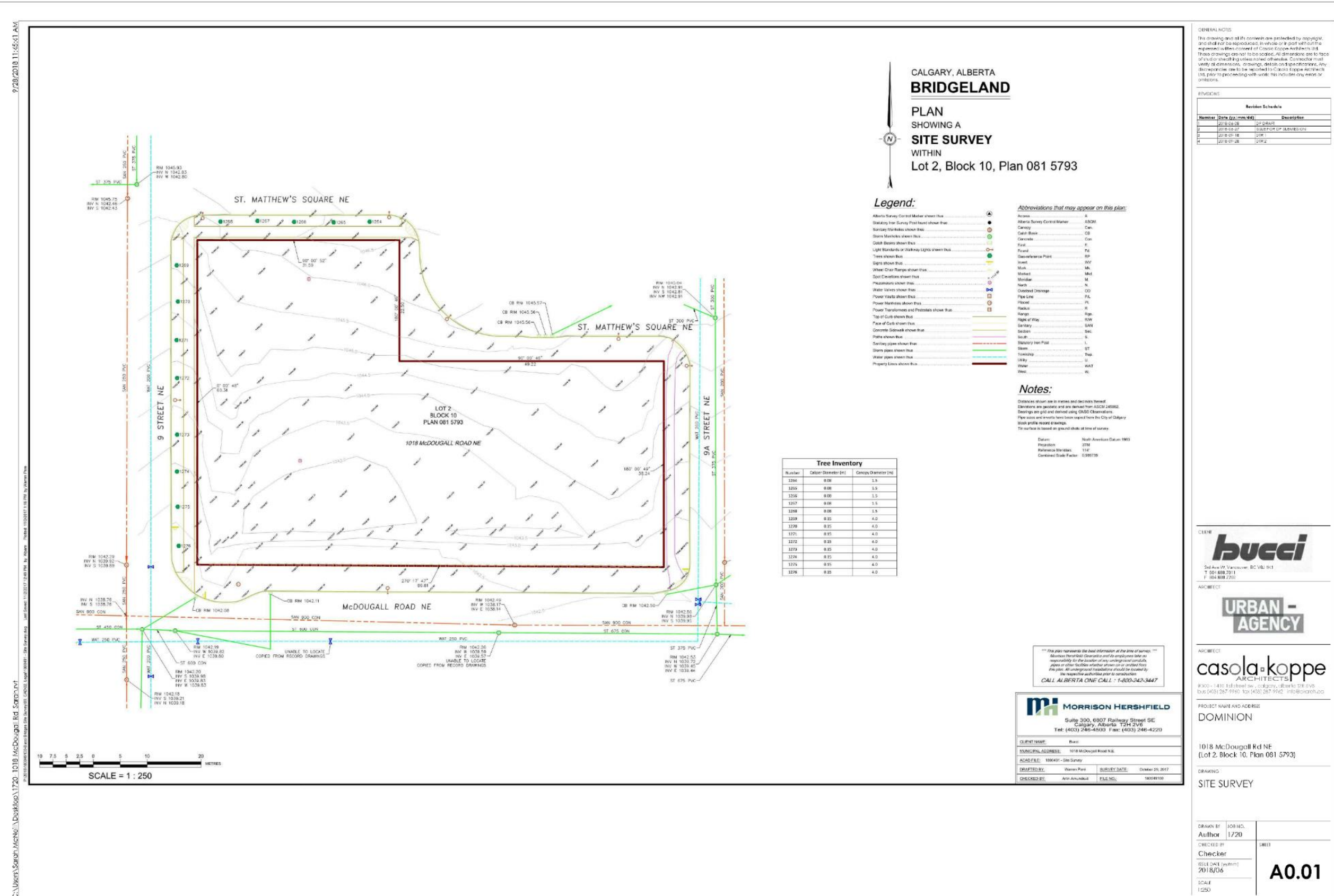
Proposed Development Permit Plans



Proposed Development Permit Plans



Proposed Development Permit Plans



Proposed Development Permit Plans

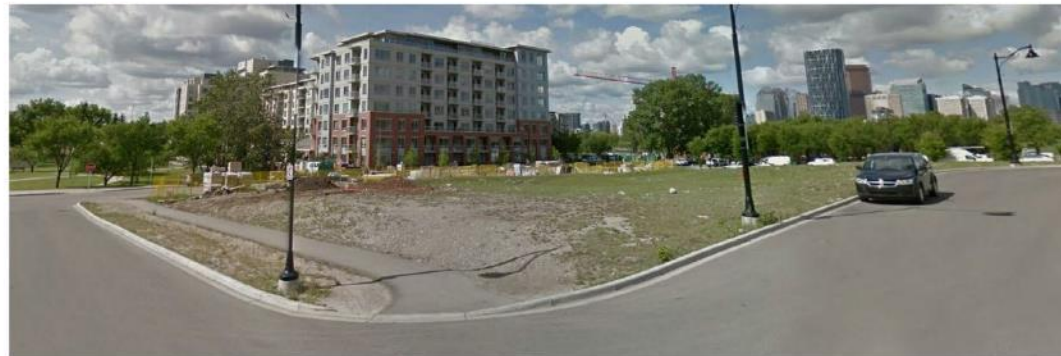
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SITE PLAN



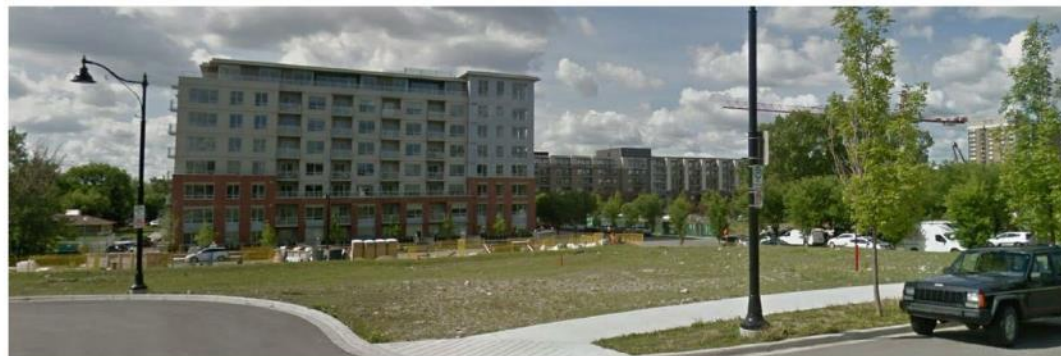
VIEW 1 : NORTHWEST VIEW OF SITE



VIEW 4 : NORTHEAST VIEW OF SITE



VIEW 2 : SOUTHWEST VIEW OF SITE



VIEW 5 : NORTHEAST VIEW OF SITE 2



VIEW 3 : SOUTHEAST VIEW OF SITE

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REVISIONS

Revision Schedule		
Number	Date (yy/mm/dd)	Description
1	2018-09-25	017 DRAFT
2	2018-09-27	LOOK FOR DP SUBMISSION
3	2018-09-18	018 D/R
4	2018-09-25	018 D/R

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PROJECT NAME AND ADDRESS
DOMINION

1018 McDougall Rd NE
(Lot 2, Block 10, Plan 051-5793)

DRAWING
EXISTING SITE PHOTOS

DRAWN BY MW	JOB NO. 1720	SHEET
CHECKED BY TC	ISSUE DATE (yy/mm) 2018/06	
SCALE		A0.02

C:\Users\Sarah.McNeill\Desktop\720_1018 McDougall Rd_Sarah.rvt

Proposed Development Permit Plans

PROPOSED LAND USE DISTRICT (TOWER)

SHADOW STUDY - JUNE



8am



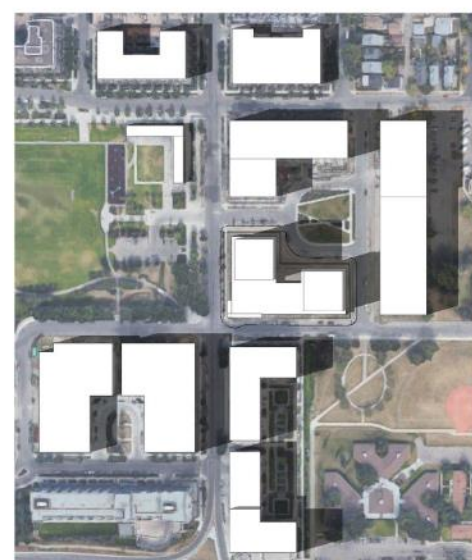
10am



12pm



2pm



4pm



6pm

PROPOSED SHADOW STUDY

PROPOSED ZONING: MU-1, MIXED USE

FAR: 5.3

HEIGHT: 50M

DOMINION - 1018 MCDOUGALL ROAD NE

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URBAN -
AGENCY

hucchi

Proposed Development Permit Plans

PROPOSED LAND USE DISTRICT (TOWER)

SHADOW STUDY - DECEMBER



10am



12pm



2pm



4pm

PROPOSED SHADOW STUDY

PROPOSED ZONING: MU-1, MIXED USE

FAR: 5.3

HEIGHT: 50M

DOMINION - 1018 MCDOUGALL ROAD NE

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Proposed Development Permit Plans

PROPOSED LAND USE DISTRICT (TOWER)

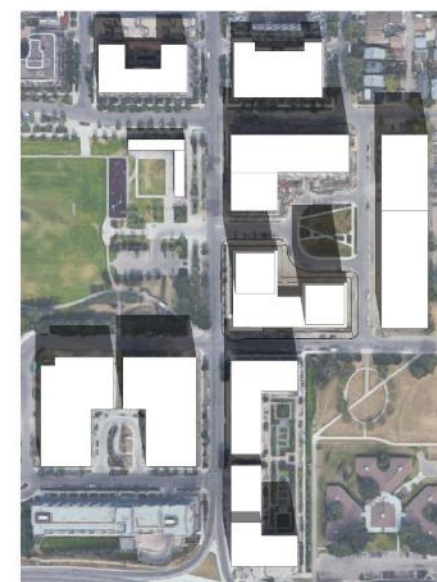
SHADOW STUDY - MARCH / SEPTEMBER



8am



10am



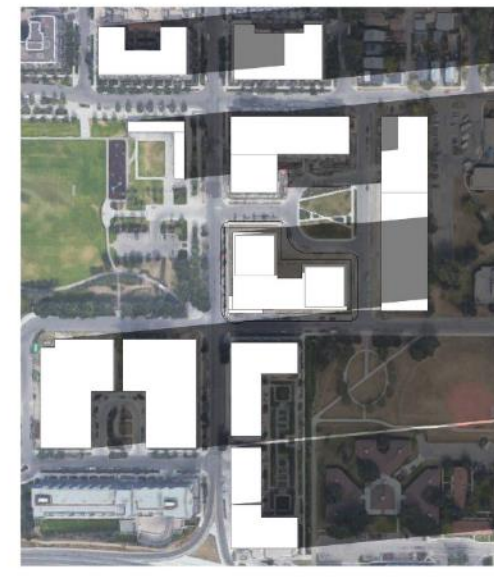
12pm



2pm



4pm



6pm

PROPOSED SHADOW STUDY

PROPOSED ZONING: MU-1, MIXED USE
FAR: 5.3
HEIGHT: 50M

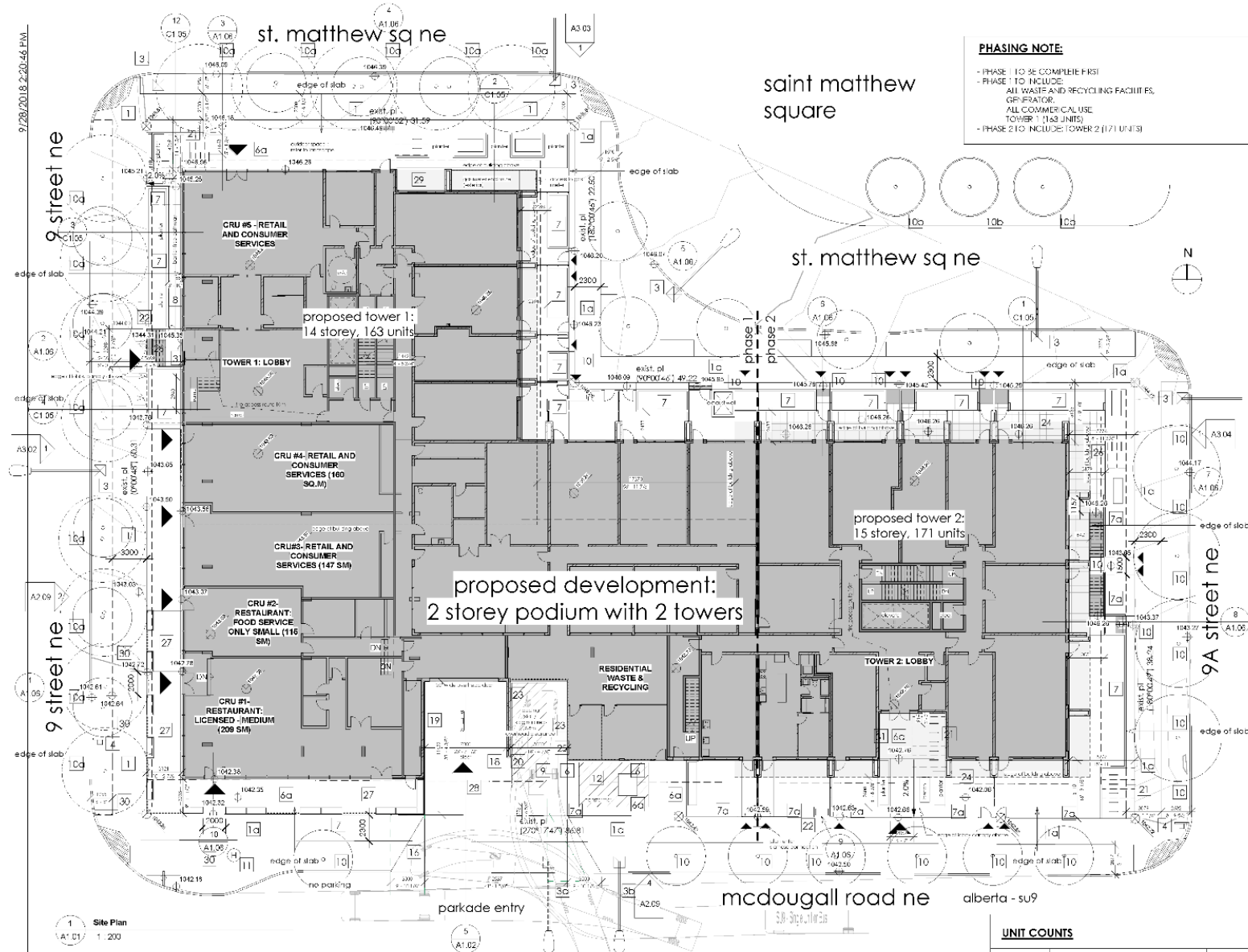
DOMINION - 1018 MCDOUGALL ROAD NE

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AGENCY

bucci

Proposed Development Permit Plans



PHASING NOTE
 - PHASE 1 TO BE COMPLETE FIRST
 - PHASE 1 TO INCLUDE:
 - ALL WASTE AND RECYCLING FACILITIES, GRIPRATOR, ALL COMMERCIAL USE TOWER 2 (171 UNITS)
 - PHASE 2 TO INCLUDE TOWER 1 (163 UNITS)

PROJECT INFORMATION

APPLICANT: CASOLA KOPPE ARCHITECTS
 SITE ADDRESS: 1015 McDougall Road NE
 LEGAL ADDRESS: Lot 2, Block 10, Plan 081 5793
 SITE AREA: 41,468m² (44,637 sq ft) [0.411 ha] (1.02 Acre)
 SITE COVERAGE: 28.4% (11,826 sq ft)
 PROPOSED ZONING: UNDERGOING LAND USE AMENDMENT (LUC 2018-0099)
 NUMBER OF UNITS: 334 UNITS (PHASE 1: 163 UNITS, PHASE 2: 171 UNITS)
 DENSITY: 815 UPH (328 JPA)

ZONING COMPLIANCE TABLE (CALGARY LAND USE BYLAW (LUB) 2007)

By-law	Permitted/required	Provided
ZONING:	EXISTING: DC 171 (2015 SITE 2) PROPOSED: MU 1 (608.5)	DC 171 (2015 SITE 2) (WITH AMENDMENTS)
DENSITY:	N/A	815 LPH (328 JPA)
F.A.R.:	MAX. 5.0	RESIDENTIAL & COMMERCIAL 5.0
STREET SETBACK (ST. MATTHEWS SQ NORTH):	0.0m (AT GRADE MAX. 4.5m for 60% OF FACADE)	5.0m TO PROPERTY LINE
STREET SETBACK (9A STREET NE (EAST)):	0.0m (AT GRADE MAX. 4.5m for 60% OF FACADE)	5.8m TO PROPERTY LINE
STREET SETBACK (9 STREET NE (WEST)):	0.0m (AT GRADE MAX. 4.5m for 60% OF FACADE)	3.1m MIN. TO PROPERTY LINE
STREET SETBACK (MCDUGALL RD NE (SOUTH)):	0.0m (AT GRADE MAX. 4.5m for 60% OF FACADE)	3.0m MIN. TO PROPERTY LINE
BUILDING HEIGHT:	MIN. 14.0m, MAX. 23.0m	50m
LANDSCAPED AREA:		REFER TO LANDSCAPE DRAWINGS
VEHICLE ACCESS:	NOT PERMITTED ON THIS SITE	ACCESS THROUGH MCDUGALL RD NE
PARKING:	RESIDENTIAL PARKING: 0.75 PER UNIT 0.1 VISITOR 334 UNITS X 0.75 = 250.5 334 UNITS X 0.1 = 33.4 PRE REDUCTION REQUIRED: 283.9 STALLS REDUCTION: 15% = 70.9 REQUIRED: 213 STALLS COMMERCIAL PARKING: REFER TO SHEET A 2.09 COMMERCIAL UNITS	0.65 PER UNIT (INCLUDES RESIDENTIAL & VISITOR & COMMERCIAL) LEVEL 1 (PARKADE 3): 121 STALLS LEVEL 2 (PARKADE 2): 106 STALLS (99 RES. & 7 COMM.V.S.) LEVEL 3 (PARKADE 1): 37 STALLS ALL SHARED PARKING BETWEEN RESIDENTIAL VISITOR & COMMERCIAL TOTAL: 264 STALLS PROVIDED
BARRIER FREE PARKING:	213 STALLS TOTAL - 6 BARRIER FREE STALLS REQUIRED (ABC 2014 (TABLE 3.8.2.2))	6 STALLS PROVIDED
LOADING STALL:	1 LOADING STALL REQUIRED FOR APARTMENT BUILDING OF 20 UNITS OR MORE (P2007-559(7)) 1 LOADING STALL REQUIRED FOR COMMERCIAL	1 LOADING STALL PROVIDED SHARED RESIDENTIAL & COMMERCIAL
BICYCLE PARKING:	CLASS 1: 0.5 PER DWELLING UNIT (1.67 REQUIRED) CLASS 2: 0.1 PER DWELLING UNIT & 8% OF STALLS FOR COMMERCIAL AREA (20.4 = 2.2 = 3% REQUIRED) (P2007-559)	1.69 CLASS 1 BICYCLE STALLS PROVIDED 39 CLASS 2 BICYCLE STALLS PROVIDED
AMENITY SPACE:	PHASE 1: 5.0m ² PER UNIT 334 UNITS X 5.0m ² = 1,670m ² PHASE 2: 5.0m ² PER UNIT 163 UNITS X 5.0m ² = 815m ² PHASE 3: 5.0m ² PER UNIT 171 UNITS X 5.0m ² = 855m ²	PHASE 1: PRIVATE AMENITY SPACE (ALL UNITS HAVE A BALCONY SPACE EXCEPT 2 UNITS) 3241.7m ² (INC. PRIVATE BALCONY AREAS) COMMON AMENITY SPACE: OUTDOOR: 1141.2m ² INDOOR: 244.4m ² TOTAL: 1485.6m ² PHASE 2: PRIVATE AMENITY SPACE: 3204.8m ² (INC. PRIVATE BALCONY AREAS)

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REVISIONS

Number	Date (yy/mm/dd)	Description
1	20-05-20	ISSUE
2	20-05-27	FOR PERMIT SUBMISSION
3	20-05-18	REV
4	20-05-26	REV

SITE LEGEND

1	EXISTING CITY'S DE WALK	11	EXISTING POWER MANHOLE
1a	NEW CITY'S DE WALK	12	TRANSFORMER ON CONCRETE PAD
2	EXISTING POWER POLE	13	100 WIDE PAINTED PARKING LINES (WHITE)
3	EXISTING LAMP STANDARD	14	BARRIER-FREE PARKING PAINTED ON CONCRETE
3a	LAMP STANDARD TO BE RELOCATED	15	BARRIER-FREE PARKING SIGN MOUNTED ON WALL
3b	NEW LAMP STANDARD LOCATION	16	CONCRETE SLAB APRON
4	EXISTING STREET/STOP SIGN	17	CONCRETE PARKADE DRIVEWAY
4a	EXISTING SIGN TO BE RELOCATED	18	VISITOR PARKING SIGN MOUNTED ON WALL
5	NEW SIGN LOCATION	19	PARKADE OVERHEAD GARAGE DOOR (6100 X 2400)
6	CONCRETE BROOM FINISH	20	LOADING STALL (3.1 X 7.2 X 4.3 M CLEARANCE ABOVE)
6a	CONCRETE DECORATIVE	21	CLASS 2 (UN-SECURED) BIKE RACKS ON CONCRETE PAD - REFER TO LANDSCAPE DRAWINGS
7	PLANTERS (SEE LANDSCAPING DWGS)	22	SIAMSE CONNECTIONS
7a	SOFFIT / LANDSCAPING (SEE LANDSCAPING DWGS)	23	WWR OVERHEAD GARAGE DOOR (8000 X 2100)
8	BARRIER FREE PATHWAY: BROOM FINISH CONCRETE	24	OUTLINE OF BUILDING ABOVE
9	HEAVY DUTY ASPHALT	25	BOLLARD (SEE DETAIL 2 A1.2)
10	NEW TREES	26	GLASS / BLACK METAL RAILING - BARRIER FREE RAMP
10a	EXISTING TREES TO BE REMOVED & NEW TREES TO BE REPLANTED	27	RESTAURANT OUTDOOR PATIO SPACE - DECORATIVE CONCRETE
10b	EXISTING TREES TO REMAIN - REFER TO LANDSCAPE	28	INTERCOM ISLAND
		29	DECORATIVE AGGREGATE - REFER TO LANDSCAPE
		30	STREET FURNITURE - REFER TO LANDSCAPE
		31	CALGARY FIRE DEPARTMENT APPROVED LOCKBOX

PROPOSED GROSS FLOOR AREA:
 PHASE 1 & 2 (INCLUDING COMMERCIAL)

IFVR: 985.9m²
 LEVEL 4: 2141.5m²
 LEVEL 5: 1304.4m²
 LEVEL 6-12: 1477.3m² X 12 LEVELS = 17727.6m²
 LEVEL 18: 27.6
 TOTAL: 22187m² (238 817 sq ft)
 FAR: 5.3

UNIT COUNTS

UNIT TYPE:	INFORMATION:	AREA (SQ.FT)	COUNT
UNIT A:	1 BED, 1 BATH	630	26
UNIT A2:	1 BFD, 1 BATH	593	1
UNIT A3:	1 BED, 1 BATH	455	1
UNIT B:	1 BED, 1 BATH (LR)	522	26
UNIT B2:	1 BED, 1 BATH (LR)	540	1
UNIT B3:	1 BED, 1 BATH (LR)	437	1
UNIT C:	2 BED, 2 BATH	751	102
UNIT C2:	2 BED, 2 BATH	742	1
UNIT C3:	2 BED, 2 BATH	782	3
UNIT D:	1 BED, 1 BATH	488	52
UNIT E:	1 BED, 1 BATH	590	27
UNIT E2:	1 BED, 1 BATH	620	3
UNIT E3:	1 BED, 1 BATH	615	3
UNIT E4:	1 BED, 1 BATH	618	1
UNIT F:	STUDIO	395	78
UNIT F2:	STUDIO	415	2
UNIT F3:	STUDIO	419	1
UNIT G:	1 BED, 1 BATH (LR)	485	2
UNIT G2:	1 BED, 1 BATH (LR)	598	1
UNIT G3:	1 BED, 1 BATH (LR)	507	1
UNIT C4:	1 BED, 1 BATH (LR)	529	1

WASTE & RECYCLING

RESIDENTIAL
 1 COMPACTOR PROVIDED, PICK UP BI-WEEKLY
 WASTE PRODUCTION: (234 UNITS X 0.3y3ch) = 100.2 yd³ / 6.6 m³ / WEEK
 100.2 yd³ / 4 yd bins = 26 BINS REQUIRED
 AREA REQUIRED: 26 BINS X 7.5 m² = 195 m² OF STORAGE AREA REQUIRED (60% REDUCTION DUE TO COMPACTOR = 14.85 m²)
 128.7 m² = 10 m² (COMPACTOR) = 138.7 m² (1493 sq ft) REQUIRED
 BINS PROVIDED: REDUCTION DUE TO COMPACTOR = 26 BINS / 9 BINS REQUIRED, 10 BINS PROVIDED (BI-WEEKLY PICK-UP)
 AREA PROVIDED: 120.5 m² (1297 sq ft) PROVIDED RESIDENTIAL
 14.4 m² (155 sq ft) PROVIDED COMMERCIAL

STAGING AREA REQUIRED: 138.7m² / 2 = 69.35m²
 STAGING AREA PROVIDED: SHARED WITH LOADING

COMPACTOR INFORMATION:
 MAKE: COMPACTORS INC
 MODEL: MODEL 250 S 10' CONTAINER PACKER SERIES 520S
 COMPACTOR RATE: 1.3
 DIMENSIONS: 35' x 44' W x 6' H
 CONTAINER SIZE: 4 YDS

COMMERCIAL
 9m² per 1000 m² of GFA OF COMMERCIAL SPACE
 841 m² (9053 sq ft) OF COMMERCIAL = 7.57 m² (81 sq ft) REQUIRED

ADDITIONAL NOTES:
 - THE MAXIMUM GRADE OF THE ENCLOSURE STAGING PAD IS 2%
 - SEE SITE GRADING PLAN FOR ROADWAY/HOLDING AREA GRADING
 - THE STAGING AREA WILL BE HEAVY DUTY ASPHALT AS SHOWN ON THE PLAN
 - THE STAGING AREA IS TO HAVE A CAPACITY TO CARRY A LOADED COLLECTION VEHICLE OF 55,000kg (25,000kg)
 - GARBAGE TRUCK TURNING PAD IS HAS BEEN SHOWN ON THE SITE PLAN
 - OVERHEAD THE CHIT FOR LOADING STALL IS 4000mm MIN.
 - OVERHEAD THE CHIT FOR W&R REMOVAL IS 3000mm MIN.
 - BUILDING OPERATOR WILL MOVE THE WASTE & RECYCLING CONTAINERS FROM THE STORAGE AREA TO THE STAGING AREA
 - PRIVATE W&R COLLECTION THE MAXIMUM GRADE OF THE ENCLOSURE STAGING PAD IS 2%

CLIENT

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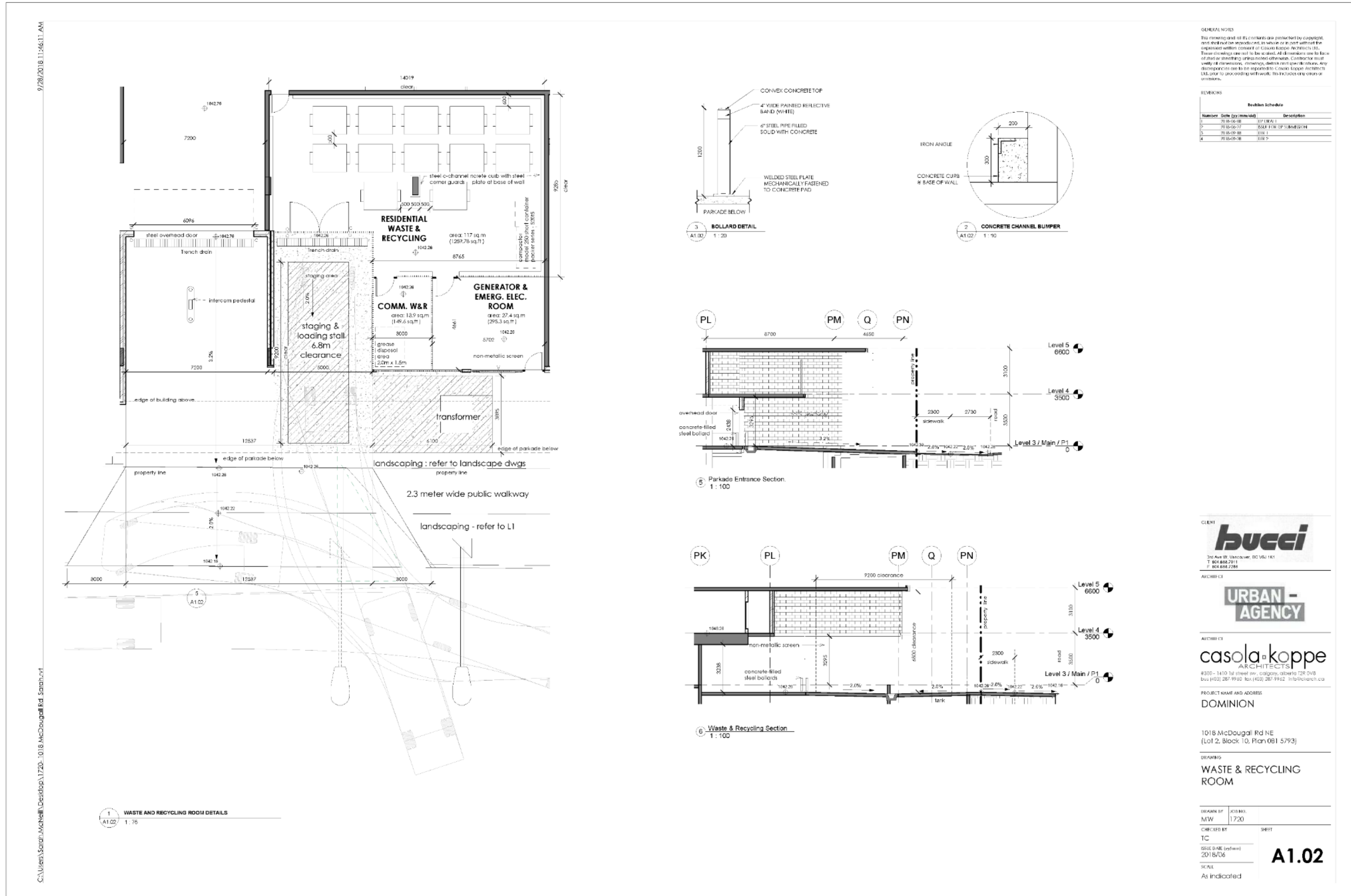
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PRODUCT NAME AND ADDRESS
 DOMINION
 1018 McDougall Rd NE
 [Lot 2, Block 10, Plan 081 5793]

DRAWING
 PROPOSED SITE PLAN
 DRAWN BY: MW JOB NO.: 1720
 CHECKED BY: TC SHEET NO.: 20 R/06
 SCALE: As Indicated
A1.01

Proposed Development Permit Plans



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REVISIONS

Number	Date (yy/mm/dd)	Description
1	2018-06-18	ISSUE 1
2	2018-06-27	SOUL FIRE UP SUBMISSION
3	2018-06-28	ISSUE 2
4	2018-06-28	ISSUE 3

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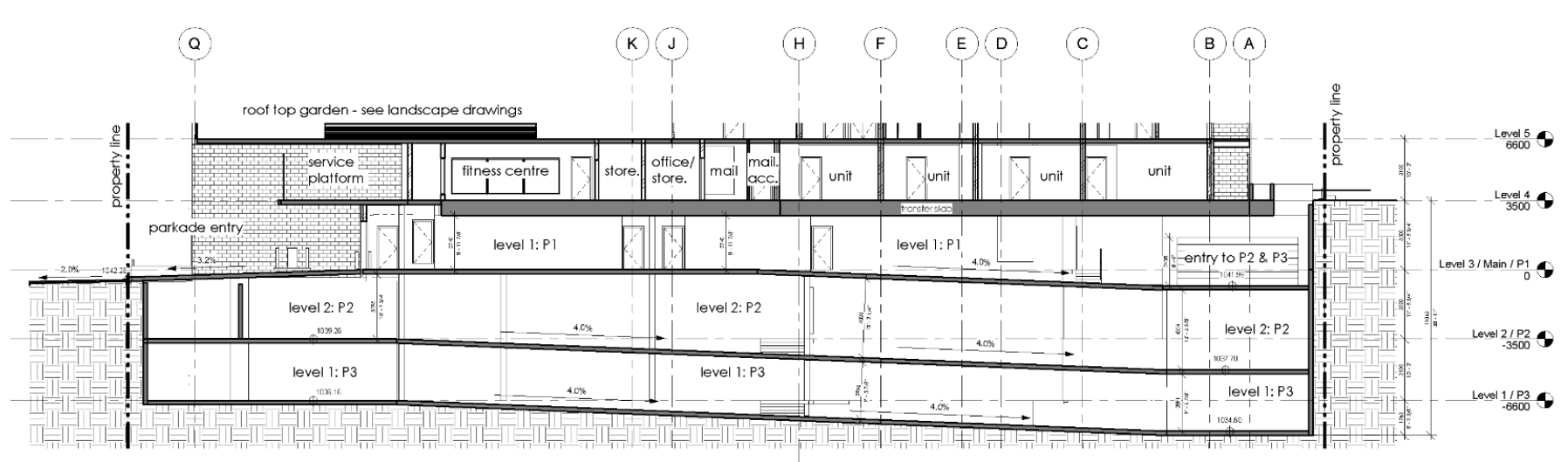
PROJECT NAME AND ADDRESS
DOMINION
1018 McDougall Rd NE
(Lot 2, Block 10, Plan 081 5793)

DRAWING
WASTE & RECYCLING ROOM

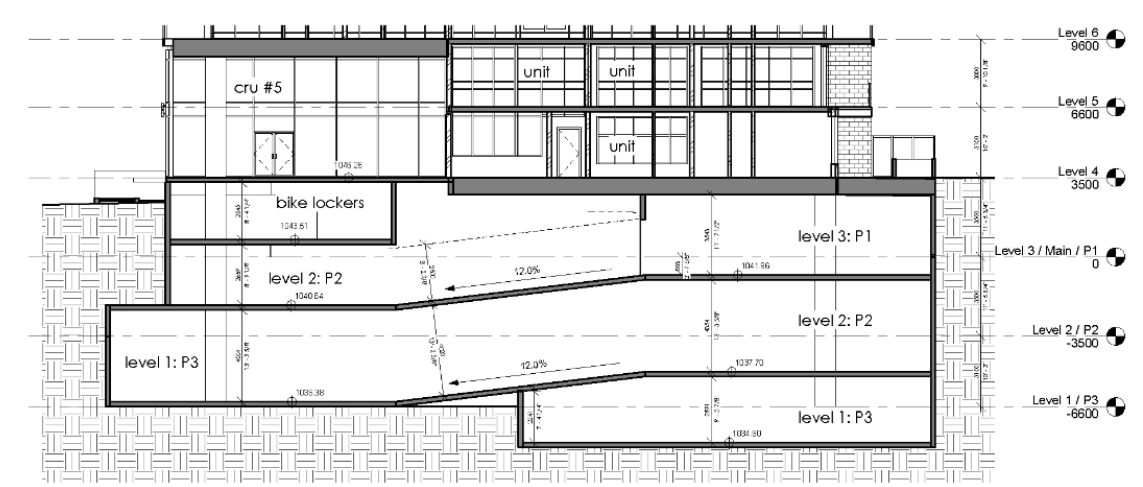
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CHECKED BY SHEET
TC
ISSUE DATE (yy/mm) 2018/06
SCALE As indicated
A1.02

Proposed Development Permit Plans

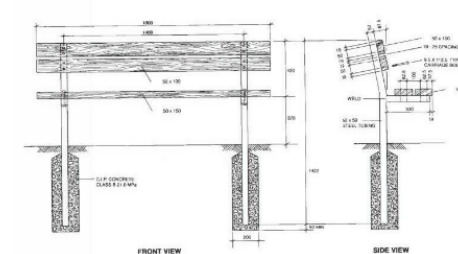
9/28/2018 11:46:19 AM



1 Parkade Ramp Section
A1.03 1:125



3 Parkade Ramp Section (12%)
A1.03 1:125



2 Bench Detail
A1.03 1:75

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REVISIONS

Revision Schedule		
Number	Date (yy/mm/dd)	Description
1	2018-06-06	CONCEPT
2	2018-06-27	FINAL FOR EX SUBMISSION
3	2018-09-18	2/18
4	2018-09-28	2/18.2

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PROJECT NAME AND ADDRESS
DOMINION

1018 McDougall Rd NE
(Lot 2, Block 10, Plan 081 5793)

DRAWING
SITE DETAILS

DRAWN BY MW	JOB NO. 1720	A1.03
CHECKED BY TC	SHEET	
ISSUE DATE (yy/mm/dd) 2018/06		
SCALE As indicated		

Proposed Development Permit Plans

9/28/2018 11:46:29 AM

① Boulevard Section - CRU #1 Entrance
1:75

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REVISIONS		
Revision Schedule		
Number	Date (YYYYMMDD)	Description
1	20180918	CRU 1
2	20180928	CRU 2

② Boulevard Section - Main Entrance
1:75

③ Boulevard Section - St. Matthew Sq. 1
1:75

④ Boulevard Section - St. Matthew Sq. 2
1:75

⑤ Boulevard Section - St. Matthew Sq. 3
1:75

⑥ Boulevard Section - St. Matthew Sq. 4
1:75

⑦ Boulevard Section - 9a St. 1
1:75

⑧ Boulevard Section - 9a St. 2
1:75

⑨ Boulevard Section - McDougall Rd. 1
1:75

⑩ Boulevard Section - McDougall Rd. 2
1:75

9 Street NE

St. Matthew Sq. NE

9a Street NE

McDougall Rd. NE

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PROJECT NAME AND ADDRESS

DOMINION

1018 McDougall Rd NE
(Lot 2, Block 10, Plan 081 5793)

DRAWING

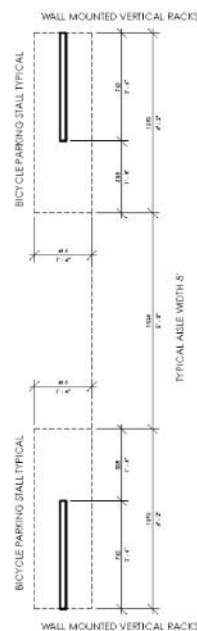
BOULEVARD SECTIONS

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Author	1720
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ISSUE DATE (YYYYMM)	
2018/06	
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A1.06

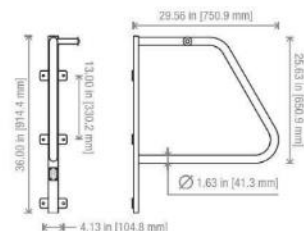
Proposed Development Permit Plans

9/28/2018 11:46:54 AM



Urban High Density Vertical Rack

Model: UB-1900



Guidelines

Single unit details
In addition to the following configurations, each rack is available individually to allow completely custom configurations.

Recommended aisle widths
High traffic: At least 70 in (175 cm)
Typical: 60 in (150 cm)
Minimum: 48 in (125 cm)

Handlebar clearance
At all times, a minimum of 16 in (40 cm) must be left on each side of the structure, calculated from the center of the last and first bicycle parking place.

Joining bicycle racks
Standalone models segments can be joined by a single post at each end to preserve 16 in (40 cm) between each bicycle parking space.

Wall clearance
Wall clearance from the wall for single-sided stand-alone structures is a minimum of 4 in (10 cm), calculated from the center of the post closest to the wall.

Specifications

Capacity	
Number of bicycles	1 per rack
Bicycle spacing	16 in (406.4 mm)
Weight	
Total weight	18.50 lbs (8.41 kg)
Materials	
Assembly material	Steel
Pipe radius	2.37 in (60.20 mm)
Pipe thickness	0.15 in (3.91 mm)
Rod thickness	0.39 in (9.91 mm)
Anti-theft feature	
Optional tamper-proof security bolts	
Available finishes	
Powder coated (basic and custom colors)	
Hot-dipped galvanized	
Stainless steel	

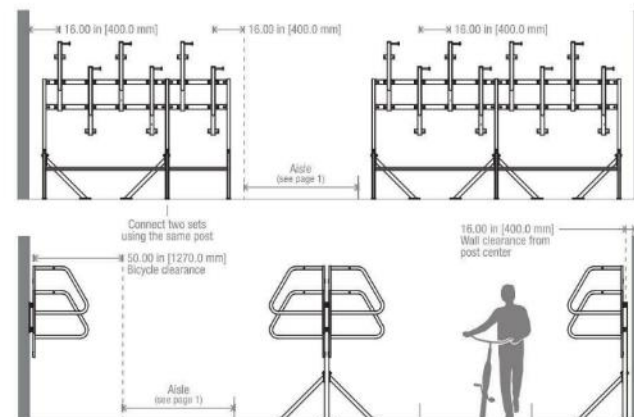
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URBAN RACKS
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1-888-717-8881 sales@urbanracks.com
For more product and company information, please visit us at www.urbanracks.com

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Urban High Density Vertical Rack

Model: UB-1900



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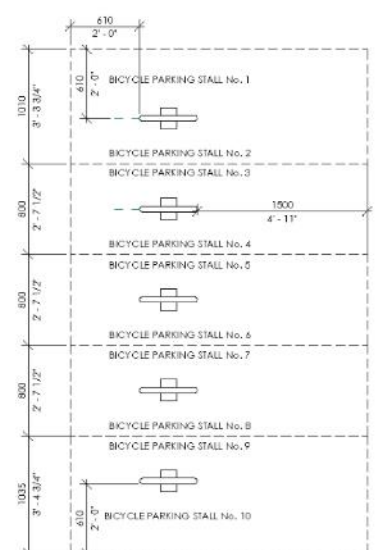
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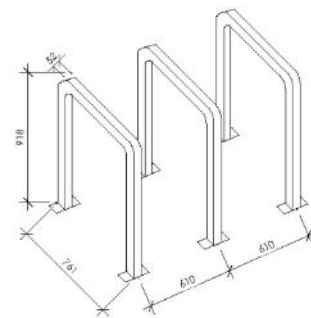
REVISIONS

Number	Date (yy/mm/dd)	Description
1	2018-09-10	DRW 1
2	2018-09-28	DRW 2

1 CLASS 1 - BICYCLE PARKING DETAILS
A1.01 1:20



2 CLASS 2 - BICYCLE PARKING LAYOUT (TOWER 1&2)
A1.01 1:25



3 Class 2 - Bicycle Rack Detail
A1.01 1:20

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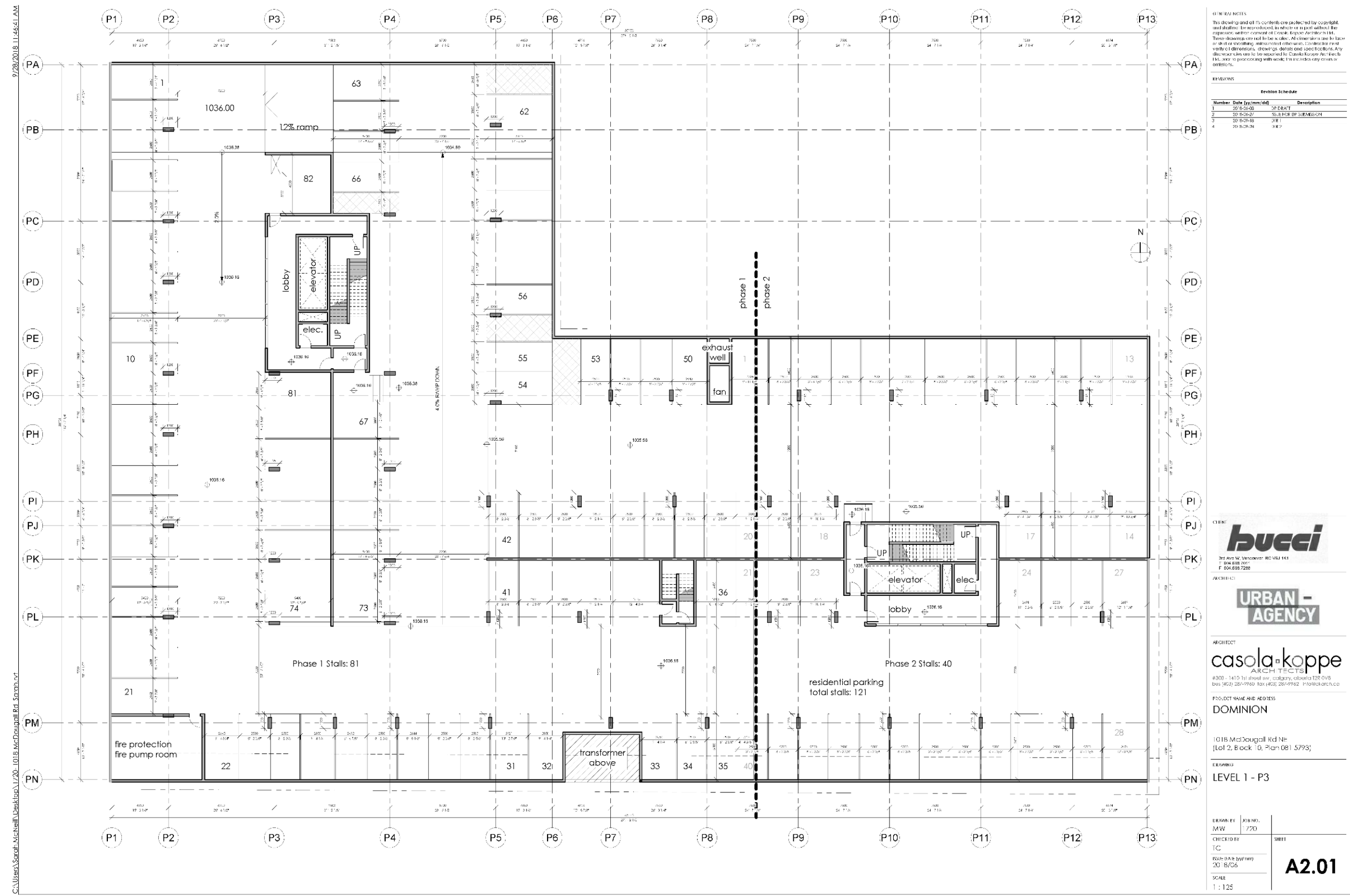
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PROJECT NAME AND ADDRESS
DOMINION
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DRAWING
BICYCLE PARKING DETAILS

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SCALE	As indicated	

Proposed Development Permit Plans



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2	20-08-07	FOR SUBMISSION
3	20-08-18	FOR
4	20-08-26	FOR



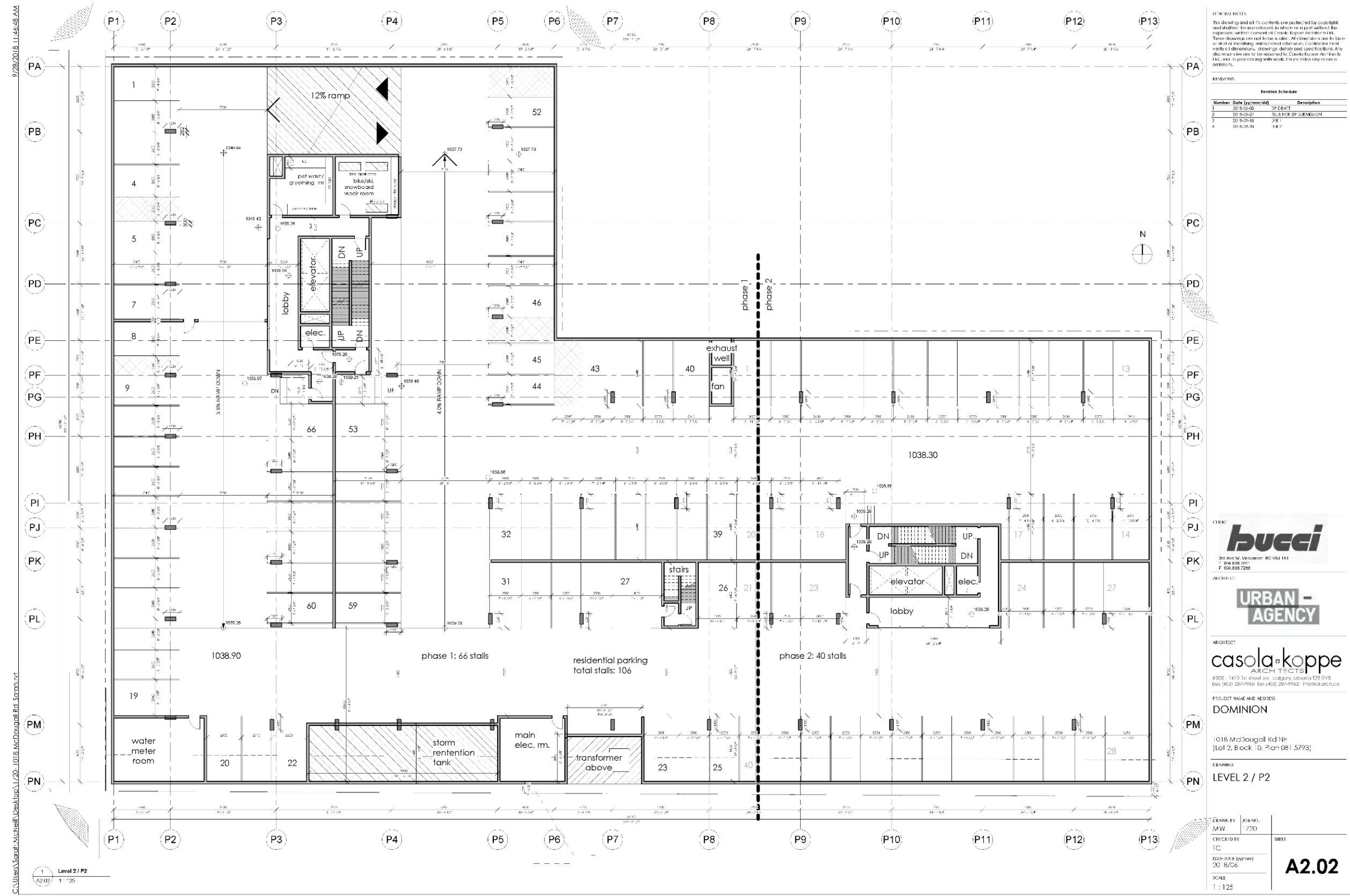
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DOMINION
1018 MacDougall Rd NE
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DRAWING
LEVEL 1 - P3

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DATE 20/08/20	A2.01
SCALE 1:125	

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3	20-08-18	REV
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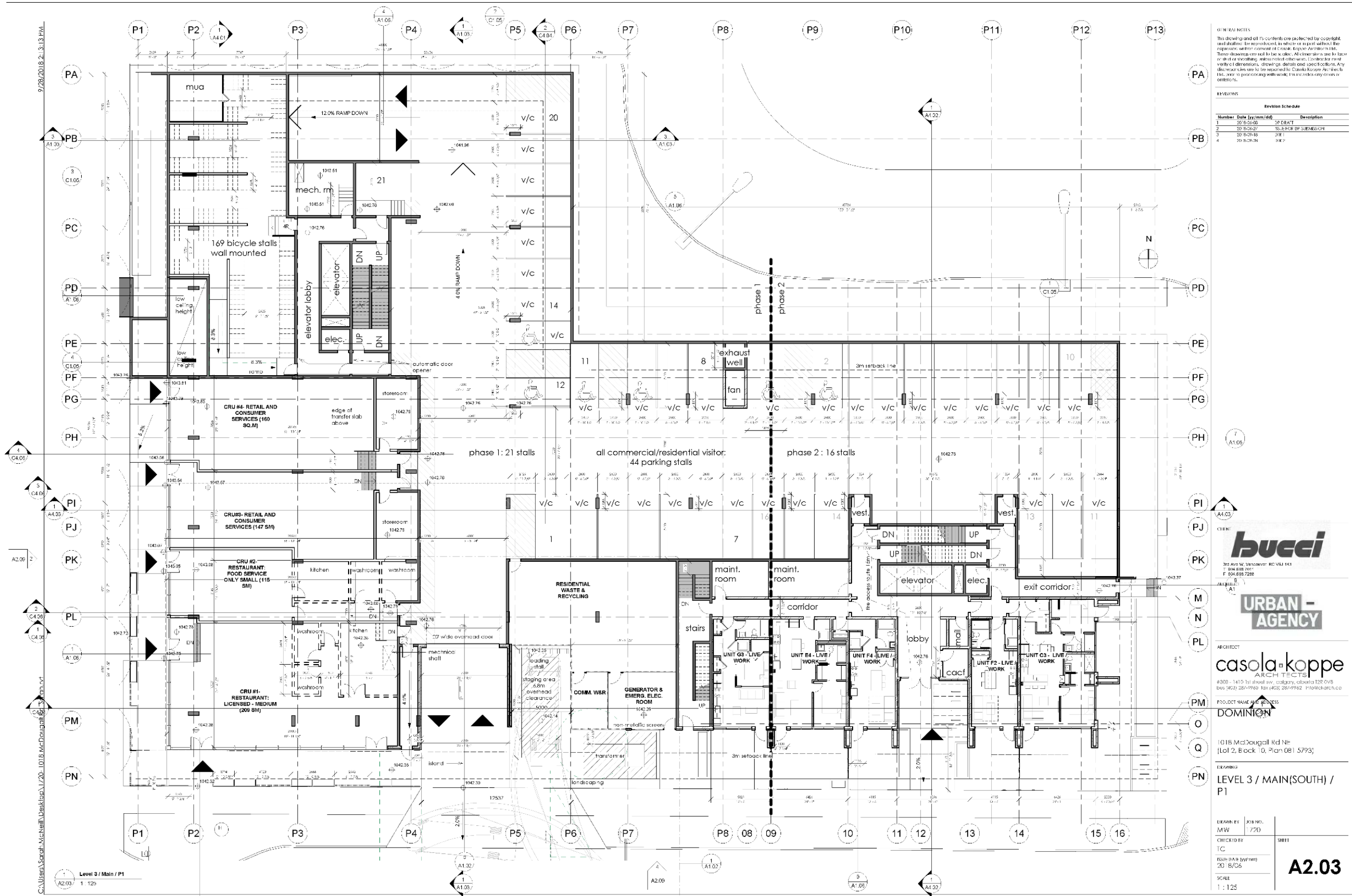
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DOMINION
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DRAWING
LEVEL 2 / P2

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A2.02

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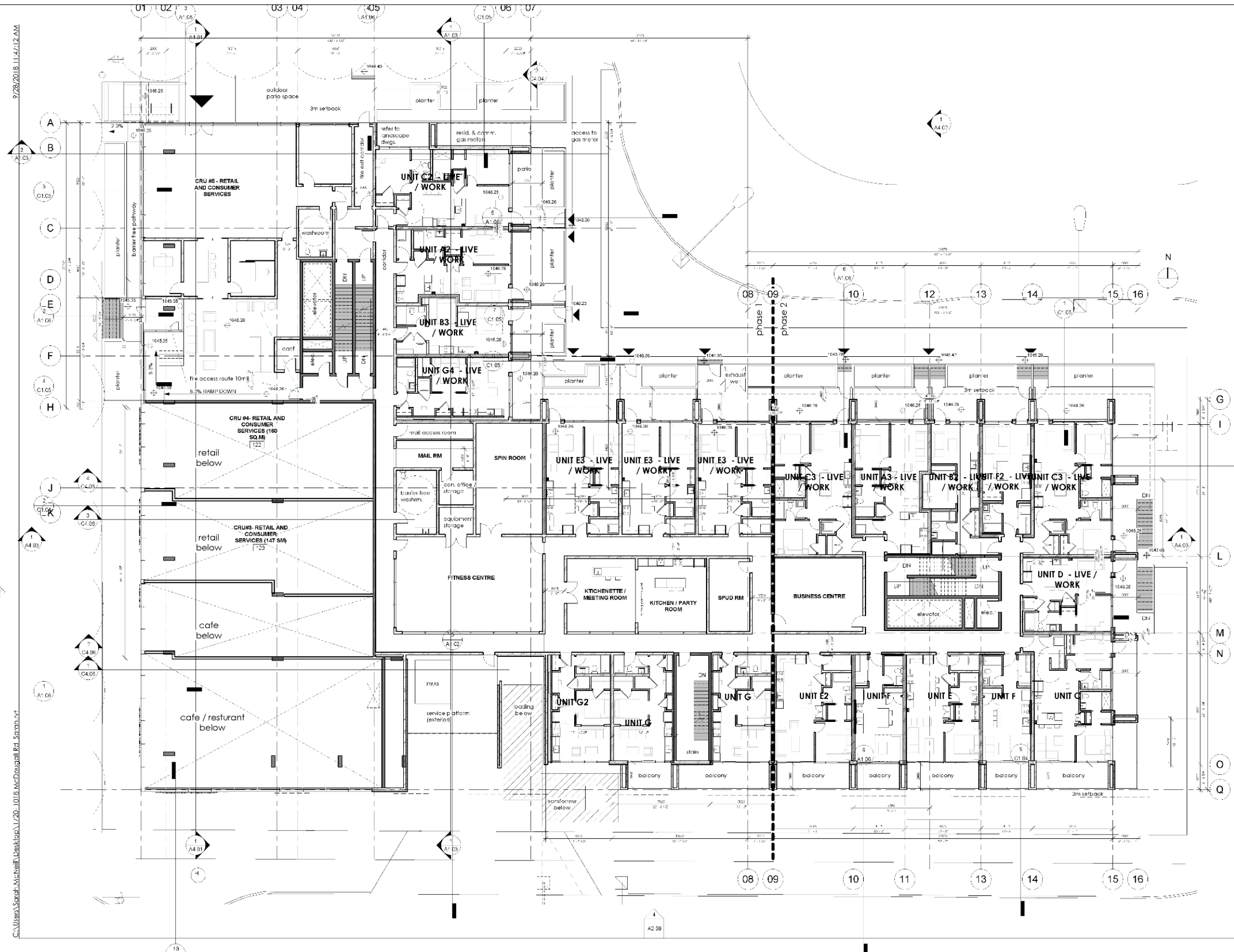
PRODUCT NAME AND BUSINESS
DOMINION
1018 MacDougall Rd NE
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DRAWING
LEVEL 3 / MAIN(SOUTH) / P1

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A2.03

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1	20/08/20	ISSUE
2	20/08/20	REVISED
3	20/08/20	REVISED

Revision Schedule

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1	20/08/20	ISSUE
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3	20/08/20	REVISED

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DOMINION
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DRAWING
LEVEL 4 / MAIN (NORTH)

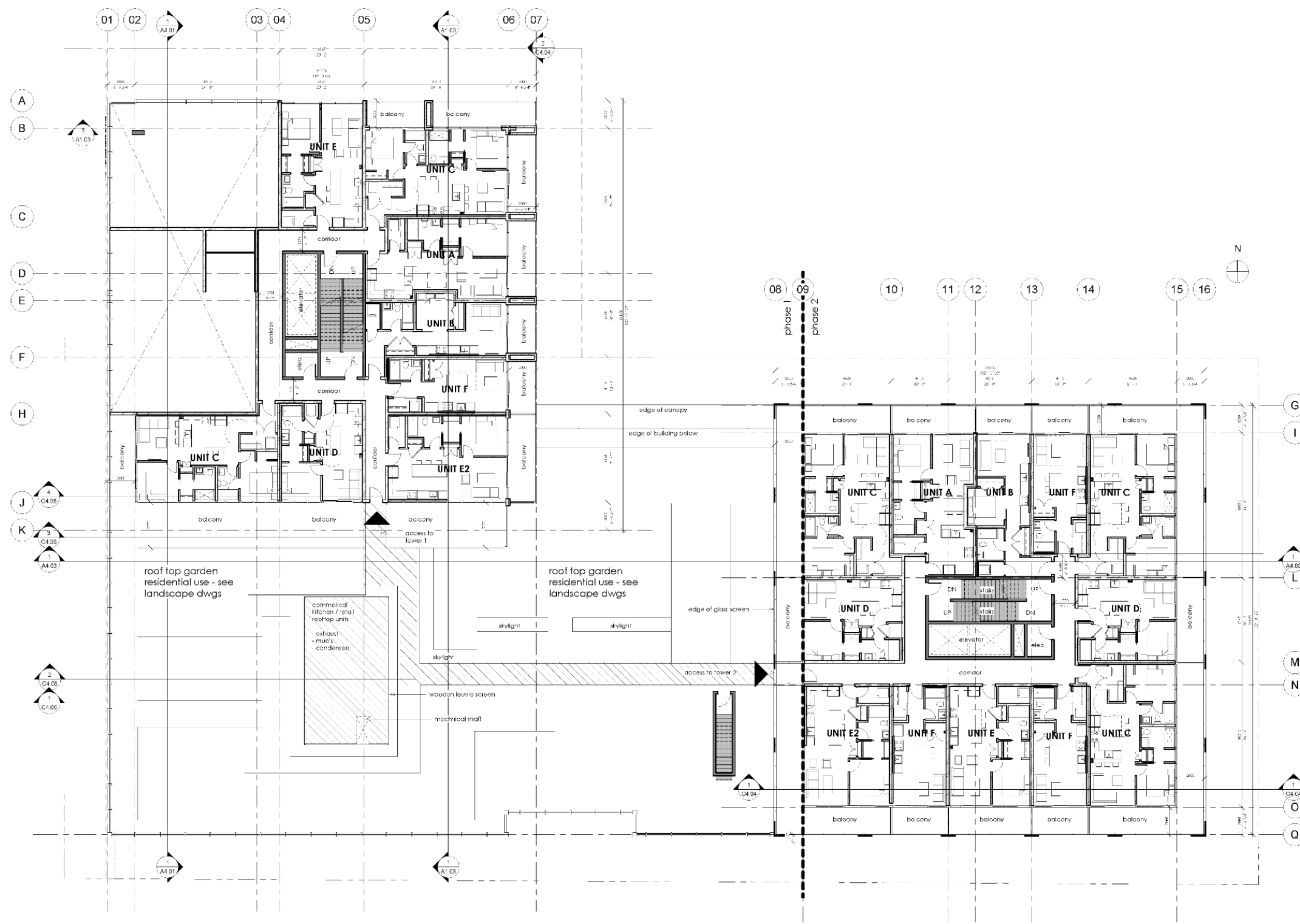
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A2.04

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4	20 8-26-18	DR

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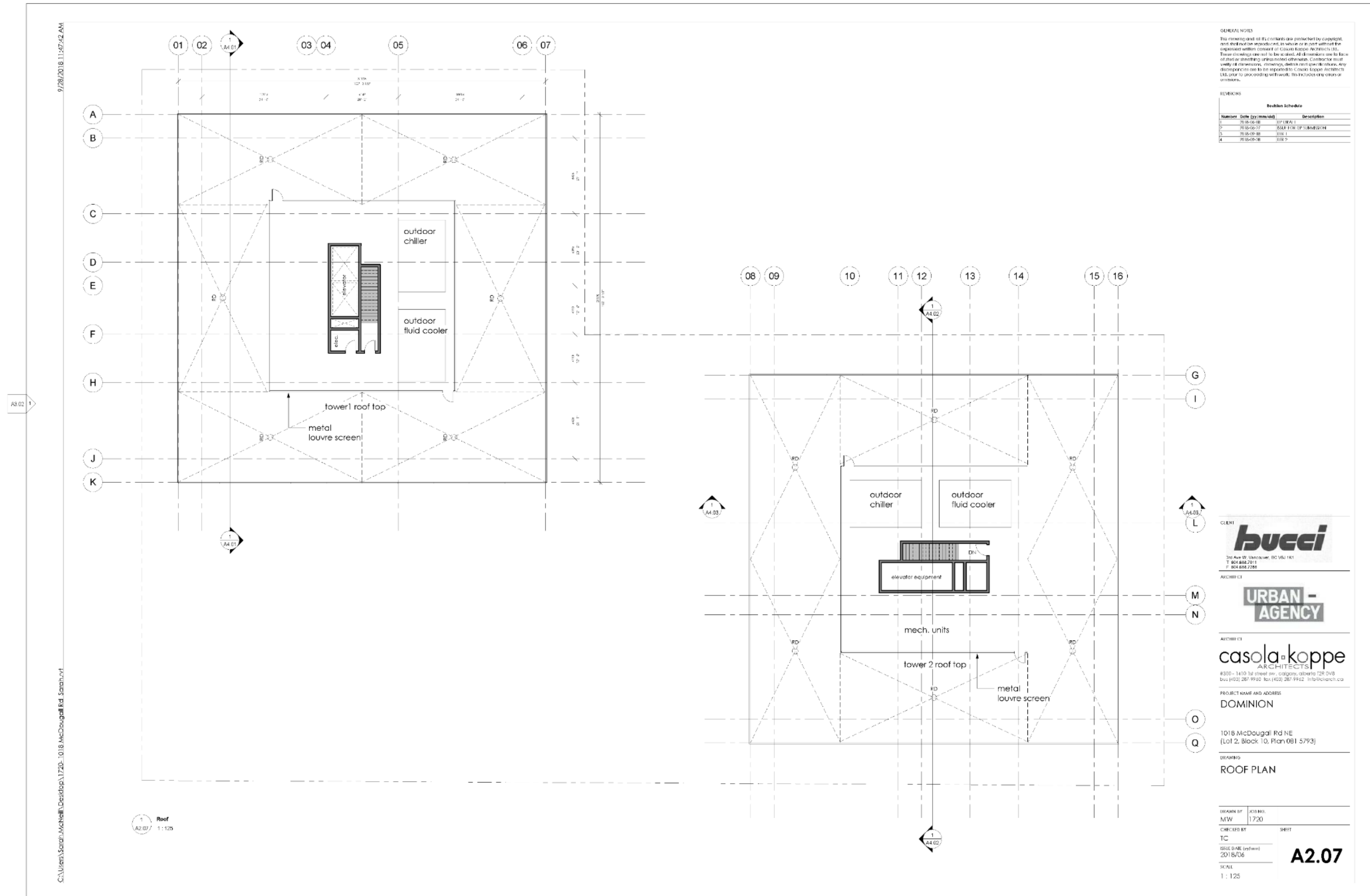
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DOMINION
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DRAWING
LEVEL 5 - ROOF GARDEN

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ISSUE DATE (yy/mm/dd)	
20 8/06	
SCALE	
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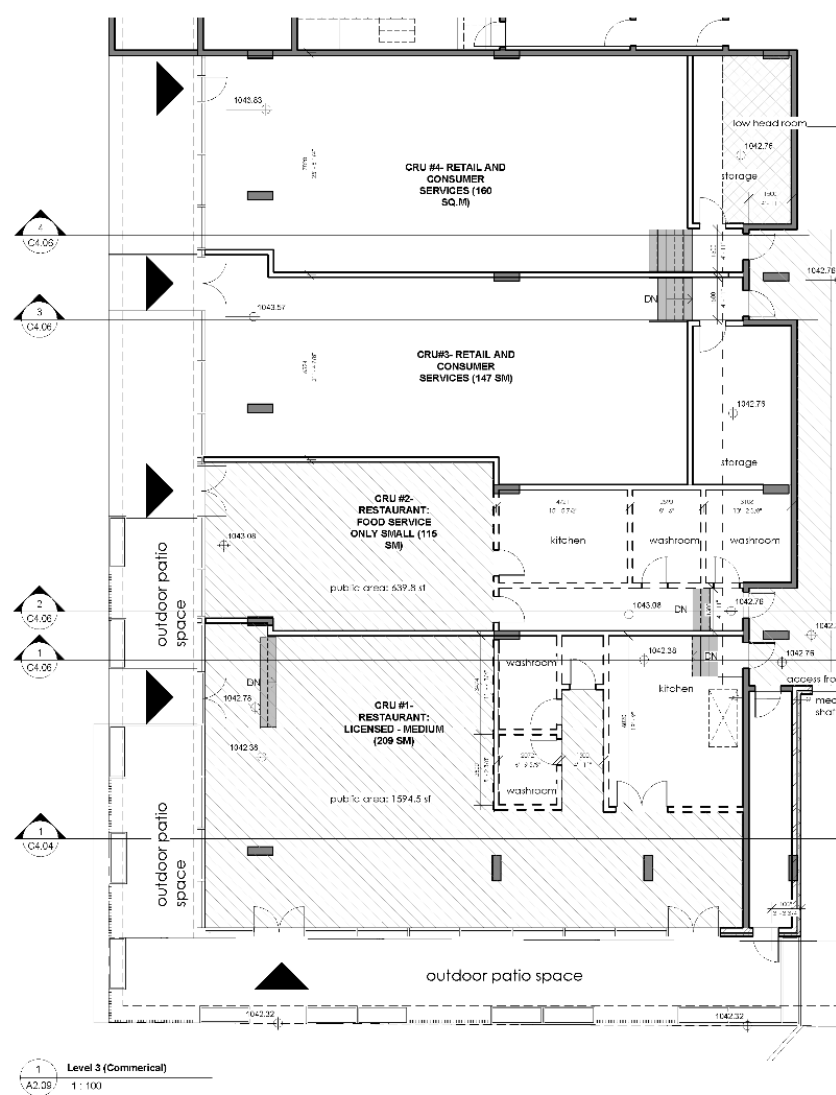
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Proposed Development Permit Plans

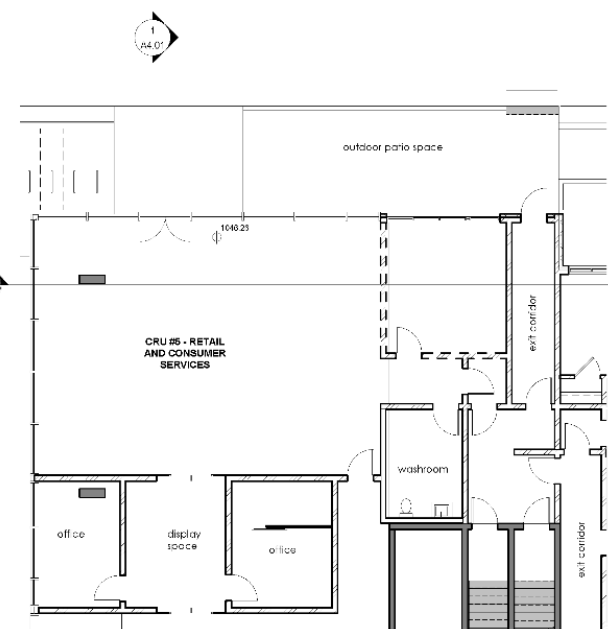


Proposed Development Permit Plans

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1 Level 3 (Commercial)
A2.08 1:100



3 Level 4 (Commercial)
A2.09 1:100

COMMERCIAL AREA INFORMATION:

COMMERCIAL GROSS FLOOR AREA:

Level	CRU #	CRU Description	Area (m²)	Area (sq ft)
LEVEL 3	CRU #1	Restaurant, Licensed - Medium	209.7	(2246 sq ft)
		Public Area:	148.1	(1595 sq ft)
	CRU #2	Restaurant, Food Service Only - Small	114.8	(1230 sq ft)
		Public Area:	58.4	(629.5 sq ft)
LEVEL 4	CRU #3	Retail and Consumer Services	147.1	(1583 sq ft)
	CRU #4	Retail and Consumer Services	160.1	(1723 sq ft)
LEVEL 4 (Retail and Consumer Services)			210.4	(2285 sq ft)
TOTAL:			841.1	(9033 sq ft)

NOTE: MEDIUM RESTAURANT TO ALLOW FOR ADMITTANCE OF MINORS TO HAVE LIQUOR LICENSE AND WILL NOT PROVIDE VLTs

PARKING & LOADING STALL REQUIREMENT:
LOADING TO SHARE WITH RESIDENTIAL USE

PARKING:

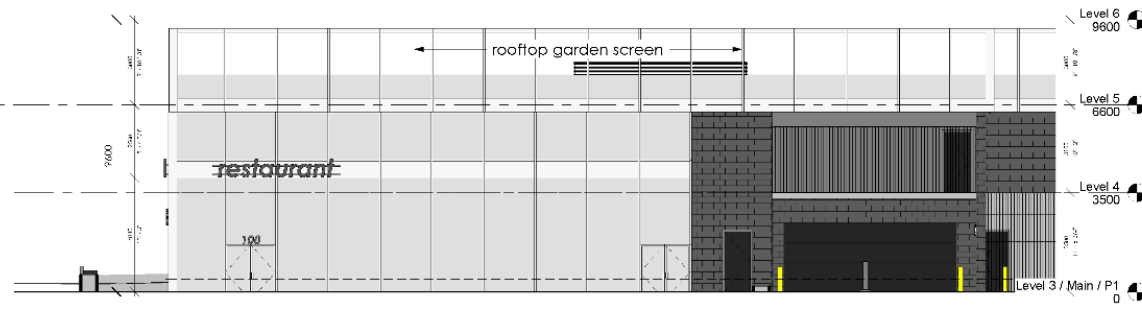
CRU #	Area (SQ M)	Ratio	Stalls
CRU #1	148.1	2.85 / 10 =	42.2
CRU #2	58.4	2.85 / 10 =	16.4
CRU #3	147.1	2 / 100 =	2.9
CRU #4	160.1	2 / 100 =	3.2
CRU #5	210.4	2 / 100 =	4.2
REDUCTION: 25% & MAX 3 (RETAIL & COMMERCIAL USES)			16.7
TOTAL REQUIRED: 44 STALLS			
TOTAL PROPOSED: 57 STALLS			

BIKE STALLS REQUIREMENT:
5% OF TOTAL REQUIRED COMMERCIAL PARKING 2.2 STALLS TO SHARE WITH RESIDENTIAL USE

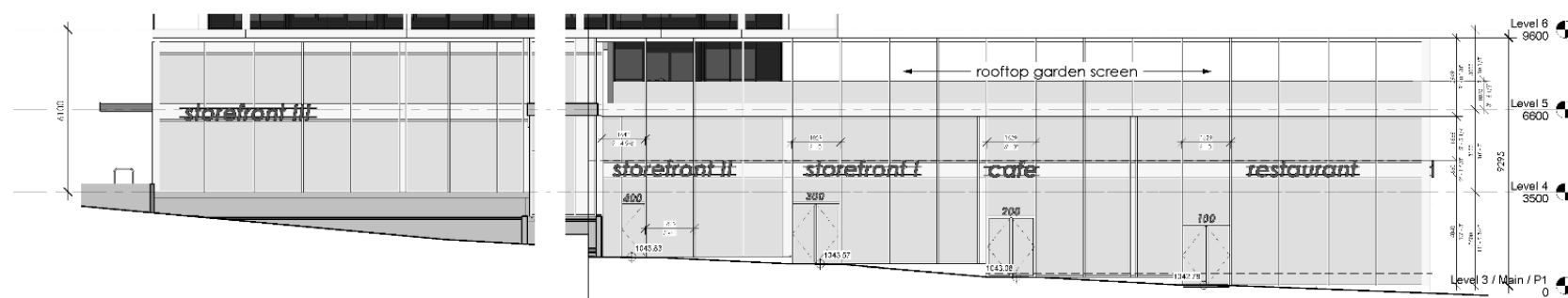
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4 Commercial - South Elevation
A2.09 1:100



2 Commercial - West Elevation
A2.09 1:100



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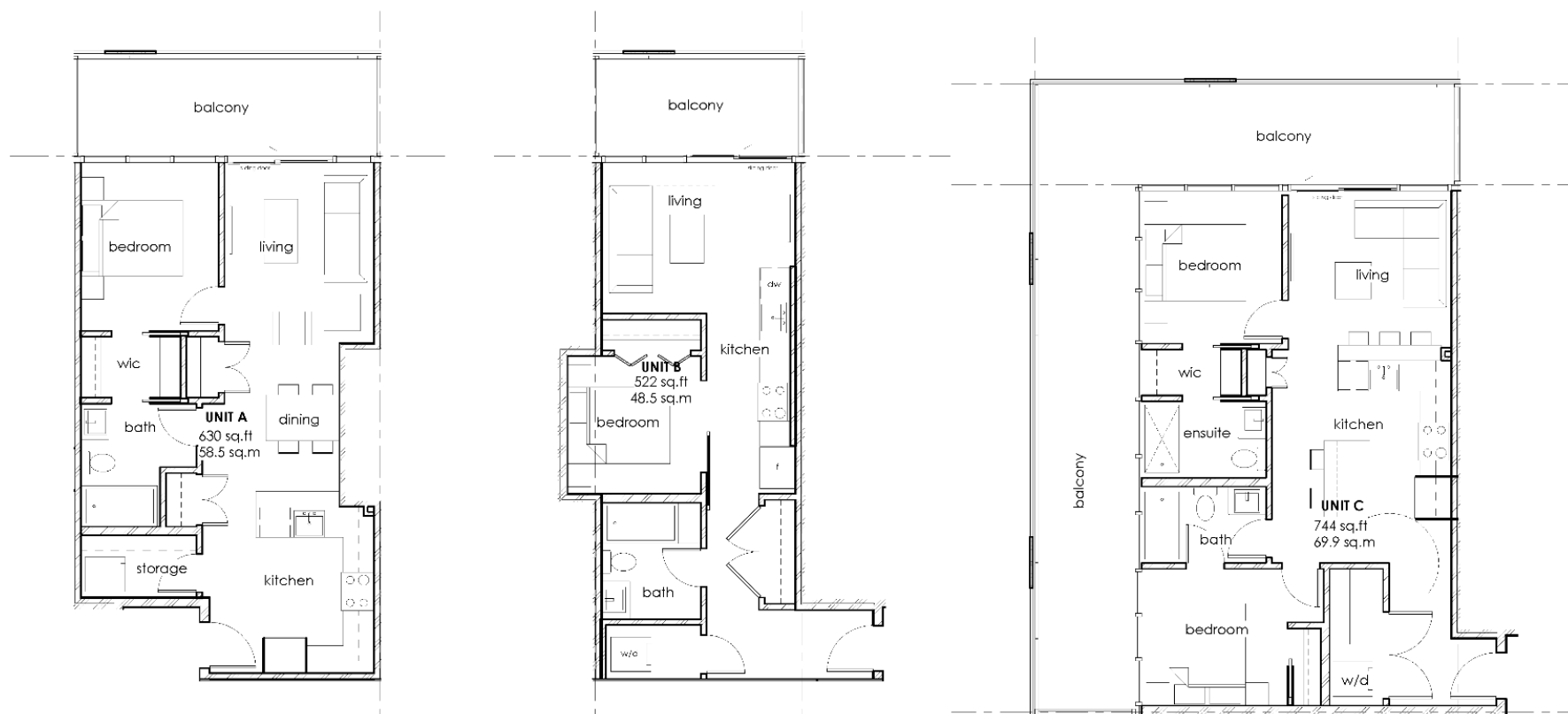
COMMERCIAL UNITS

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A2.09

Proposed Development Permit Plans

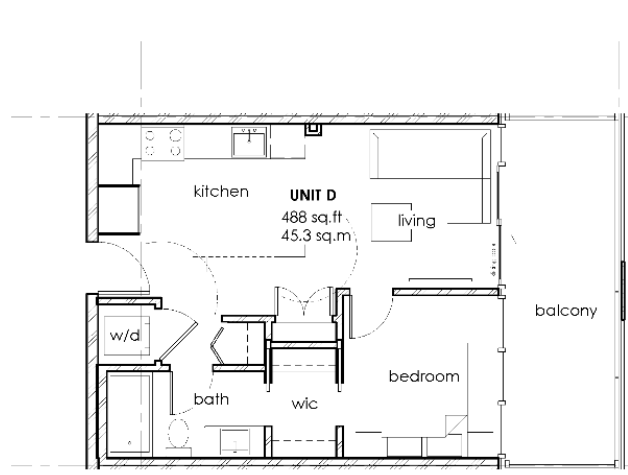
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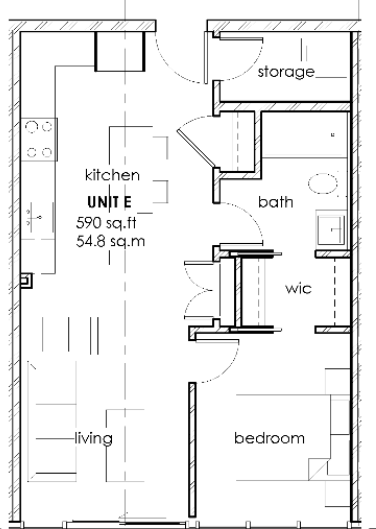
1 Unit A
A2.10 1:50

2 Unit B
A2.10 1:50

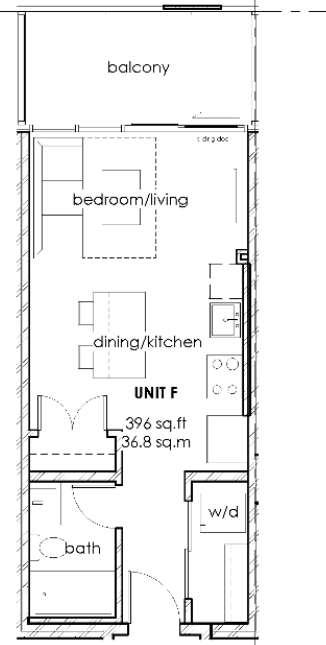
3 Unit C
A2.10 1:50



4 Unit D
A2.10 1:50



5 Unit E
A2.10 1:50



6 Unit F
A2.10 1:50

UNIT COUNTS

UNIT TYPE:	INFORMATION:	AREA (SQ.FT)	COUNT
UNIT A	1 BED, 1 BATH	630	26
UNIT A2	1 BED, 1 BATH	593	1
UNIT A3	1 BED, 1 BATH	656	1
UNIT B	1 BED, 1 BATH (JR)	522	26
UNIT B2	1 BED, 1 BATH (JR)	540	1
UNIT B3	1 BED, 1 BATH (JR)	437	1
UNIT C	2 BED, 2 BATH	744	102
UNIT C2	2 BED, 2 BATH	762	1
UNIT C3	2 BED, 2 BATH	782	3
UNIT D	1 BED, 1 BATH	488	52
UNIT E	1 BED, 1 BATH	590	27
UNIT E2	1 BED, 1 BATH	620	3
UNIT E3	1 BED, 1 BATH	616	3
UNIT E4	1 BED, 1 BATH	618	1
UNIT F	STUDIO	396	76
UNIT F2	STUDIO	416	2
UNIT F4	STUDIO	449	1
UNIT G	1 BED, 1 BATH (JR)	495	2
UNIT G2	1 BED, 1 BATH (JR)	598	1
UNIT G3	1 BED, 1 BATH (JR)	507	1
UNIT G4	1 BED, 1 BATH (JR)	579	1

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2	20 08 26 27	FOR REVIEW BY SUBMISSION
3	20 08 26 18	FOR
4	20 08 26 26	FOR



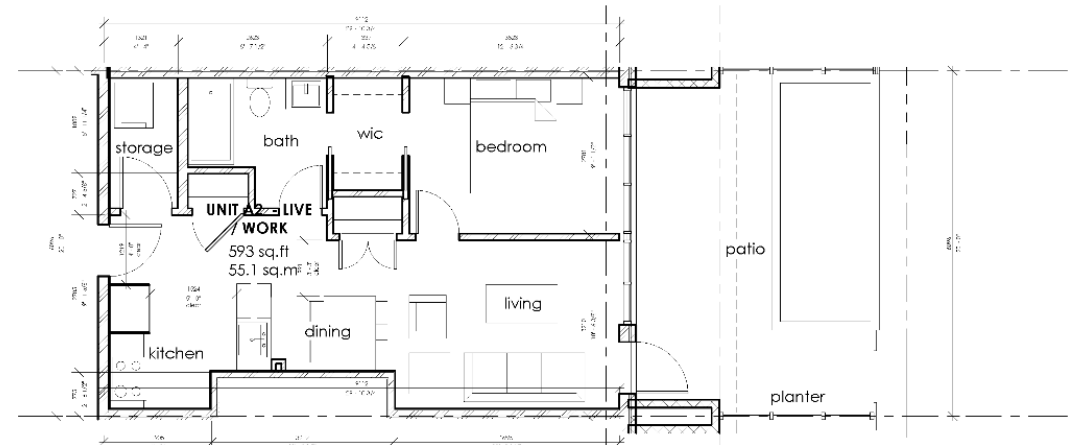
PROJECT NAME AND ADDRESS
DOMINION
1018 MacDougal Rd NE
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DRAWING
UNIT PLANS (TYPICAL)

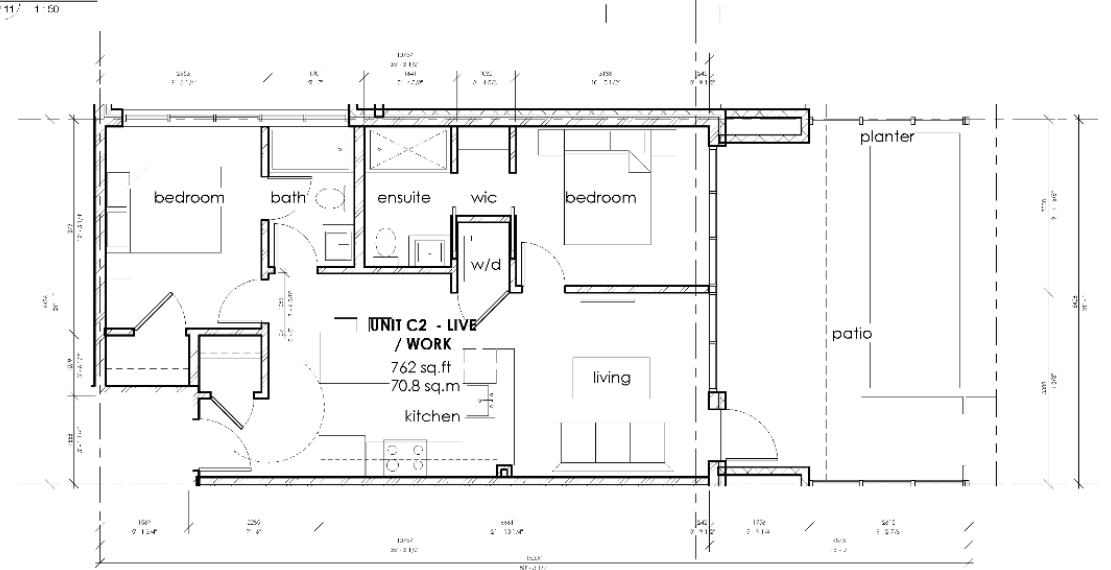
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SCALE As Indicated	

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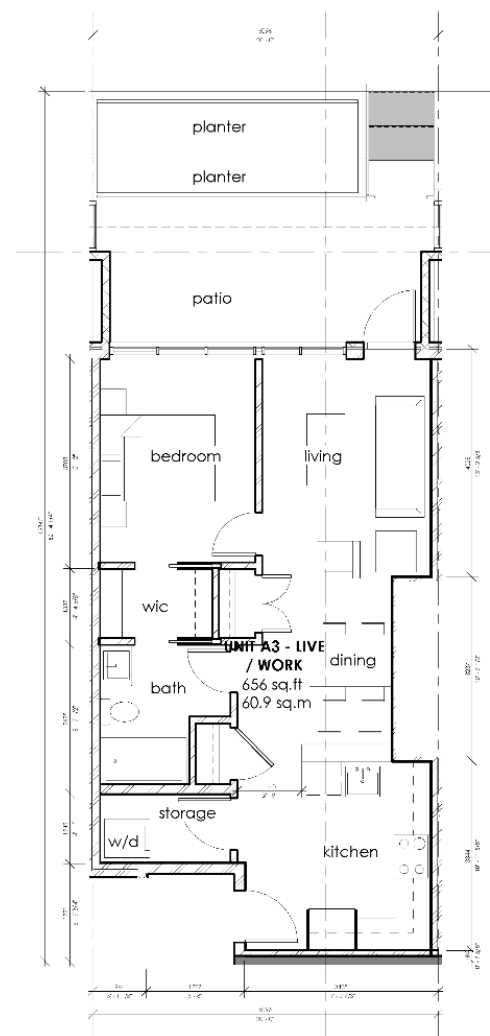
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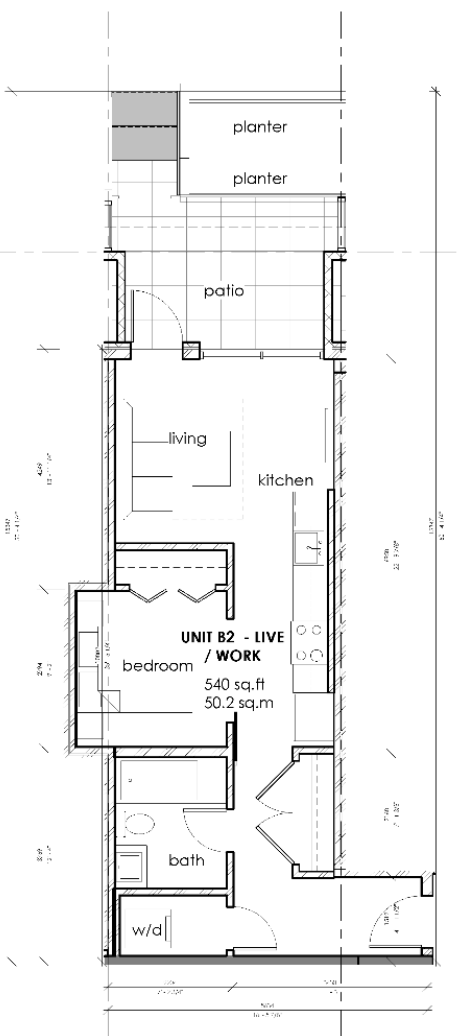
Unit A2
A2.11 1:50



Unit C2
A2.11 1:50



Unit A3
A2.11 1:50



Unit B2
A2.11 1:50

UNIT COUNTS

UNIT TYPE:	INFORMATION:	AREA (SQ.FT)	COUNT
UNIT A	1 BED, 1 BATH	630	26
UNIT A2	1 BED, 1 BATH	593	1
UNIT A3	1 BED, 1 BATH	656	1
UNIT B	1 BED, 1 BATH (LRI)	522	26
UNIT B2	1 BED, 1 BATH (LRI)	540	1
UNIT B3	1 BED, 1 BATH (LRI)	437	1
UNIT C	2 BED, 2 BATH	744	102
UNIT C2	2 BED, 2 BATH	762	1
UNIT C3	2 BED, 2 BATH	782	3
UNIT D	1 BED, 1 BATH	488	52
UNIT E	1 BED, 1 BATH	590	27
UNIT E2	1 BED, 1 BATH	620	3
UNIT E3	1 BED, 1 BATH	610	3
UNIT E4	1 BED, 1 BATH	618	1
UNIT F	STUDIO	396	78
UNIT F2	STUDIO	415	2
UNIT F4	STUDIO	449	1
UNIT G	1 BED, 1 BATH (LRI)	480	2
UNIT G2	1 BED, 1 BATH (LRI)	590	1
UNIT G3	1 BED, 1 BATH (LRI)	507	1
UNIT C4	1 BED, 1 BATH (LRI)	529	1

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DOMINION
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DRAWING
UNIT PLANS

DRAWN BY	MW	JOB NO.	1770
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SCALE	As Indicated		

A2.11

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3	20-05-18	REV
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1018 MacDougall Rd NE
[Lot 2, Block 10, Plan 08] 5793

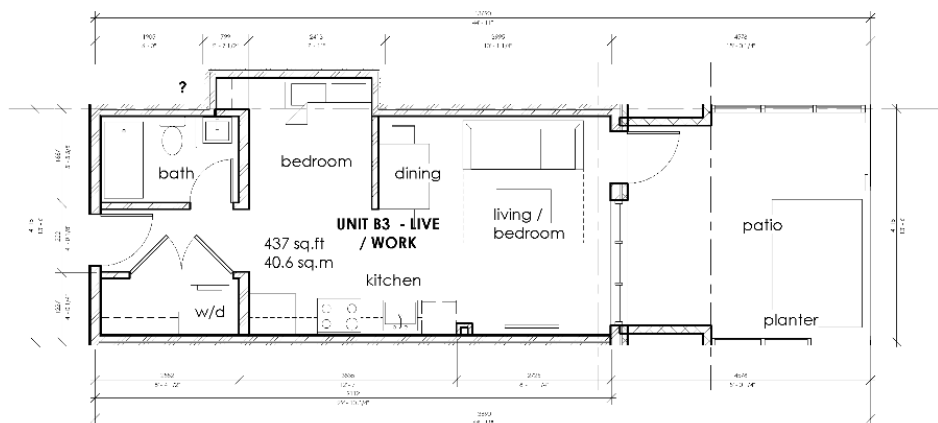
DRAWING
UNIT PLANS

UNIT TYPE:	INFORMATION:	AREA (SQ.FT)	COUNT
UNIT A	1 BED, 1 BATH	630	26
UNIT A2	1 BED, 1 BATH	593	1
UNIT A3	1 BED, 1 BATH	666	1
UNIT B	1 BED, 1 BATH (JRI)	522	26
UNIT B2	1 BED, 1 BATH (JRI)	540	1
UNIT B3	1 BED, 1 BATH (JRI)	437	1
UNIT C	2 BED, 2 BATH	744	102
UNIT C2	2 BED, 2 BATH	742	1
UNIT C3	2 BED, 2 BATH	782	3
UNIT D	1 BED, 1 BATH	488	52
UNIT E	1 BED, 1 BATH	590	27
UNIT E2	1 BED, 1 BATH	600	3
UNIT E3	1 BED, 1 BATH	615	3
UNIT E4	1 BED, 1 BATH	618	1
UNIT F	STUDIO	396	78
UNIT F2	STUDIO	415	2
UNIT F4	STUDIO	449	1
UNIT G	1 BED, 1 BATH (JRI)	485	2
UNIT G2	1 BED, 1 BATH (JRI)	598	1
UNIT G3	1 BED, 1 BATH (JRI)	507	1
UNIT G4	1 BED, 1 BATH (JRI)	529	1

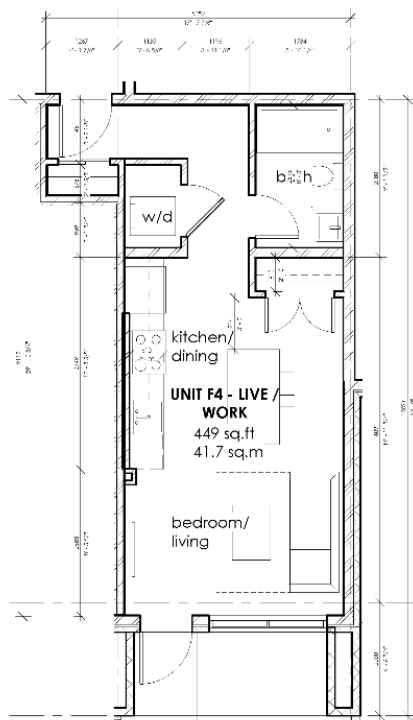
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CHECKED BY TC **SHEET**
DATE 20/05/20
SCALE As Indicated
A2.12

Proposed Development Permit Plans

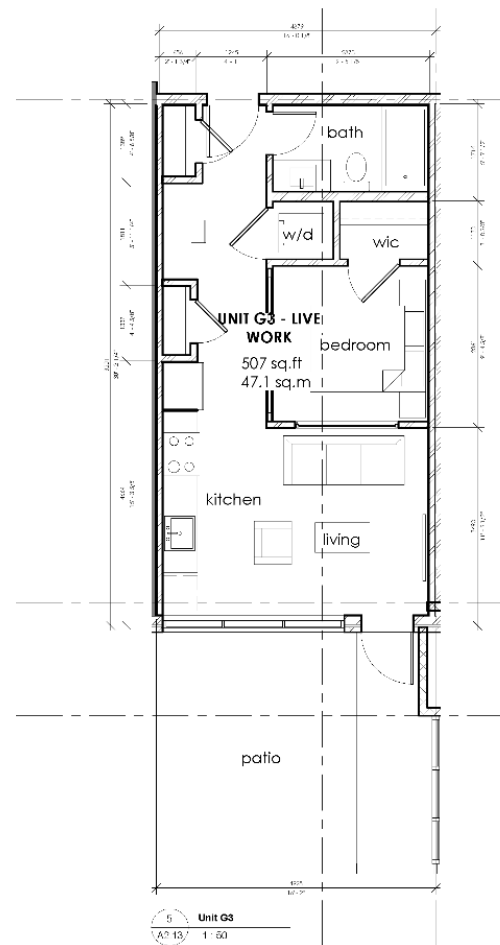
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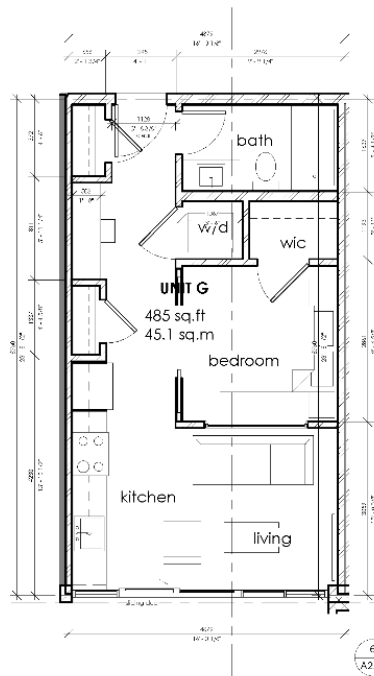
1 Unit B3
A2.13 1:50



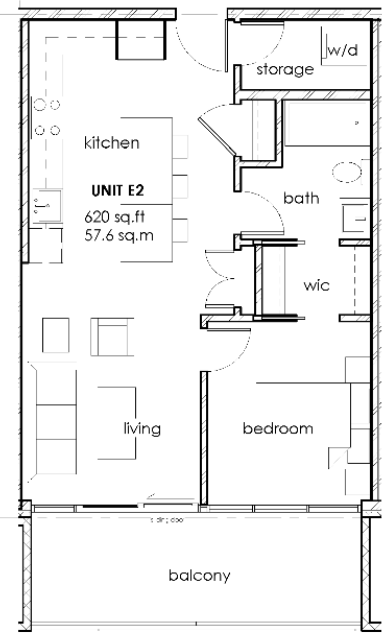
2 Unit F4
A2.13 1:50



5 Unit G3
A2.13 1:50



3 Unit G
A2.13 1:50



6 Unit E2
A2.13 1:50

UNIT TYPE:	INFORMATION:	AREA (SQ.FT)	COUNT
UNIT A	1 BTD, 1 BATH	630	26
UNIT A2	1 BTD, 1 BATH	593	1
UNIT A3	1 BTD, BATH	656	1
UNIT B	1 BTD, 1 BATH (JR)	622	26
UNIT B2	1 BTD, 1 BATH (JR)	640	1
UNIT B3	1 BTD, 1 BATH (JR)	437	1
UNIT C	2 BTD, 2 BATH	744	102
UNIT C2	2 BTD, 2 BATH	762	1
UNIT C2	2 BTD, 2 BATH	782	3
UNIT D	1 BTD, 1 BATH	488	52
UNIT E	1 BTD, 1 BATH	590	27
UNIT E2	1 BTD, 1 BATH	620	3
UNIT E3	1 BTD, 1 BATH	615	3
UNIT F4	1 BTD, 1 BATH	618	1
UNIT F	STUD O	376	78
UNIT F2	STUD O	415	2
UNIT F4	STUD O	449	1
UNIT G	1 BTD, 1 BATH (JR)	485	2
UNIT G2	1 BTD, 1 BATH (JR)	598	1
UNIT G3	1 BTD, BATH (JR)	507	1
UNIT G4	1 BTD, BATH (JR)	529	1

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REVISIONS

Number	Date (yy/mm/dd)	Description
1	20 08 26	FOR DEBAT
2	20 08 27	FOR SUBMISSION
3	20 08 28	JRF
4	20 08 28	JRF



PRODUCT NAME AND ADDRESS
DOMINION
1018 McDougall Rd NE
[Lot 2, Block 10, Plan 08] 5793

DRAWING

UNIT PLANS

DRAWN BY	JOB NO.
MW	1720
CHECKED BY	SHEET
TC	
DATE DATE (yy/mm/dd)	
20 8/26	
SCALE	
As Indicated	

A2.13

Proposed Development Permit Plans

9/28/2018 11:49:30 AM
C:\Users\Sarah-Mitchell\Desktop\120-1018-McDougal Rd. Schematic



South Elevation
A3.01 1:150

KEYNOTE LEGEND	
Key Value	Keynote Text
1	WINDOW WALL- SPANDREL PANEL- DARK GREY
2	WINDOW WALL- GLASS PANEL- CLEAR GLASS
3	WINDOW WALL MULLIONS- DARK GREY
4	MASONRY- IXL: COLOUR TBD
5	METAL LOUVRE SCREEN - WHITE/SILVER
6	SCREEN DIVIDER- FROSTED GREY GLASS- DARK GREY FRAME
7	METAL PANEL SYSTEM - WHITE/SILVER
8	NON METALIC SCREEN - COLOUR TBD
9	STOREFRONT SYSTEM AT COMMERCIAL FRONT- CLEAR GLASS & LIGHT GREY ANODIZED ALUMINIUM
10	SCREEN- ANODIZED ALUMINUM, CLEAR GLASS PANEL
11	STOREFRONT SYSTEM AT THE BUILDING ENTRIES - CLEAR GLASS
13	UNIT & BUILDING ENTRY CANOPIES - DARK GREY
14	UNIT SIGNAGE - ALUMINIUM
15	COMMERCIAL SIGNAGE - TBD
16	UNIT FRONT DOOR- CLEAR GLASS WITH DARK GREY TRIM
18	STEEL OVERHEAD DOOR- PAINTED BENJAMIN MOORE DARK GREY
19	METAL DOOR- PAINTED BENJAMIN MOORE DARK GREY
21	ELECTRICAL TRANSFORMER
22	CONCRETE
23	CONCRETE PLANTER- SANDBLASTED HEAVY ARCHITECTURAL FINISH WITH WET SEAL
26	PREFINISHED METAL FLASHING- DARK GREY
32	BALCONY GUARDRAIL- CLEAR GLASS & DARK GREY METAL
34	LOUVRE SCREEN - WOOD

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REVISIONS

Revision Schedule		
Number	Date (yy/mm/dd)	Description
1	20 8-29-18	FOR PERMIT
2	20 8-29-18	FOR PERMIT SUBMISSION
3	20 8-29-18	FOR PERMIT SUBMISSION
4	20 8-29-18	FOR PERMIT SUBMISSION



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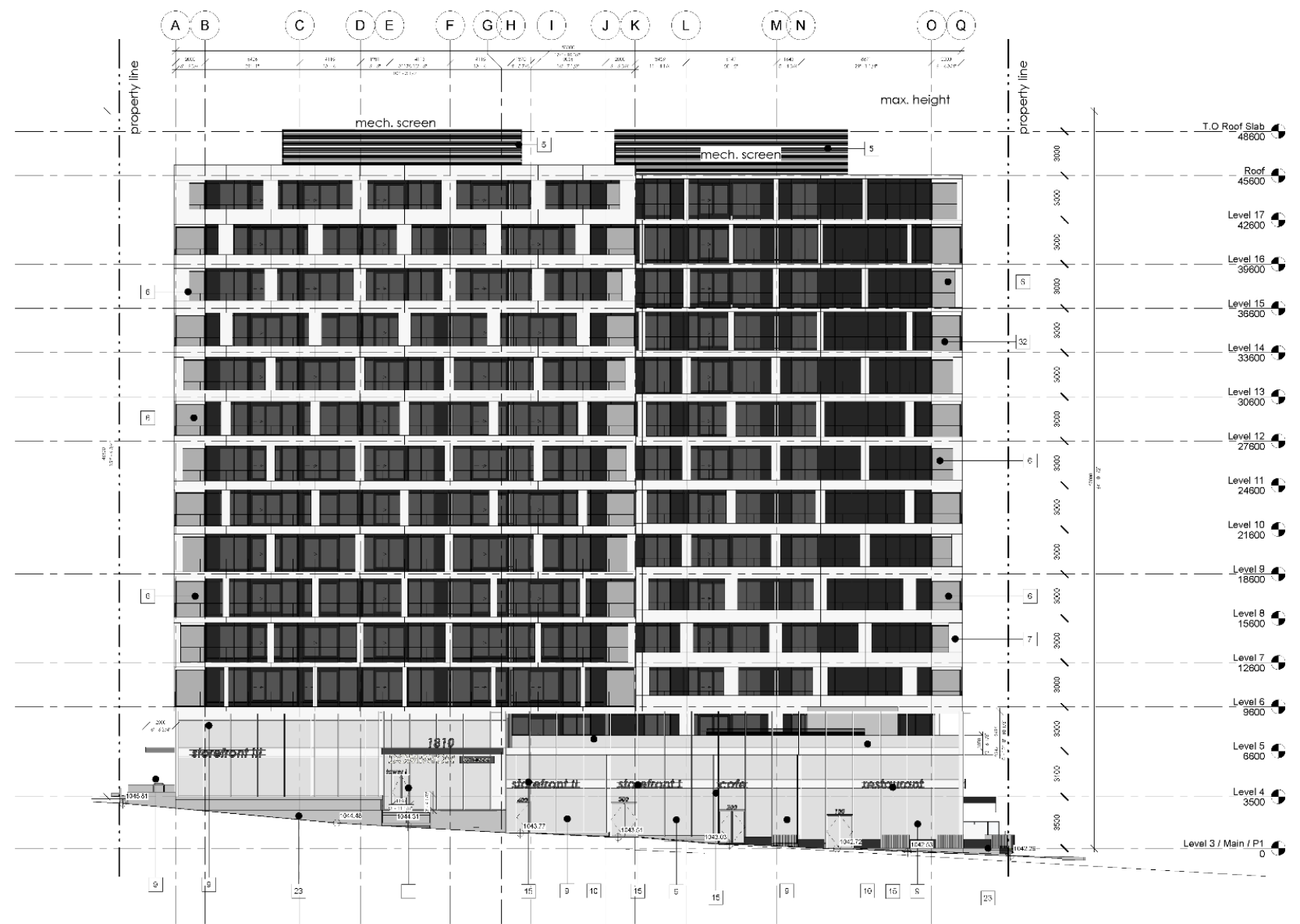
PRODUCT NAME AND ADDRESS
DOMINION
1018 McDougall Rd NE
[Lot 2, Block 10, Plan 08] 5793;

DRAWING
SOUTH ELEVATION

DRAWN BY MW	JOB NO. 1720	A3.01
CHECKED BY TC	SHEET 3/11	
DATE 20 8/26		
SCALE 1:150		

Proposed Development Permit Plans

9/28/2018 11:49:30 AM



KEYNOTE LEGEND	
Key Value	Keynote Text
1	WINDOW WALL- SPANDREL PANEL- DARK GREY
2	WINDOW WALL- GLASS PANEL- CLEAR GLASS
3	WINDOW WALL MULLIONS- DARK GREY
4	MASONRY- IXL- COLOUR TBD
5	METAL LOUVRE SCREEN - WHITE/SILVER
6	SCREEN DIVIDER- FROSTED GREY GLASS- DARK GREY FRAME
7	METAL PANEL SYSTEM - WHITE/SILVER
8	NON METALIC SCREEN - COLOUR TBD
9	STOREFRONT SYSTEM AT COMMERCIAL FRONT- CLEAR GLASS & LIGHT GREY ANODIZED ALUMINIUM
10	SCREEN- ANODIZED ALUMINIUM, CLEAR GLASS PANEL
11	STOREFRONT SYSTEM AT THE BUILDING ENTRIES - CLEAR GLASS
13	UNIT & BUILDING ENTRY CANOPIES - DARK GREY
14	UNIT SIGNAGE - ALUMINIUM
15	COMMERCIAL SIGNAGE - TBD
16	UNIT FRONT DOOR- CLEAR GLASS WITH DARK GREY TRIM
18	STEEL OVERHEAD DOOR- PAINTED BENJAMIN MOORE DARK GREY
19	METAL DOOR- PAINTED BENJAMIN MOORE DARK GREY
21	ELECTRICAL TRANSFORMER
22	CONCRETE
23	CONCRETE PLANTER- SANDBLASTED HEAVY ARCHITECTURAL FINISH WITH WET SEAL
26	PREFINISHED METAL FLASHING- DARK GREY
32	BALCONY GUARDRAIL- CLEAR GLASS & DARK GREY METAL
34	LOUVRE SCREEN - WOOD

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REVISIONS		
Revision Schedule		
Number	Date (yy/mm/dd)	Description
1	20 8-25-18	FOR DEPART
2	20 8-25-18	FOR SUBMIT BY SUBMISSION
3	20 8-25-18	FOR
4	20 8-25-18	FOR



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PRODUCT NAME AND ADDRESS
DOMINION
1018 MacDougal Rd NE
[Lot 2, Block 10, Plan 08] 5793;

DRAWING
WEST ELEVATION

DRAWN BY MW	JOB NO. 1770
CHECKED BY TC	SHEET 38/41
DATE 20 8/26	A3.02
SCALE 1 : 150	

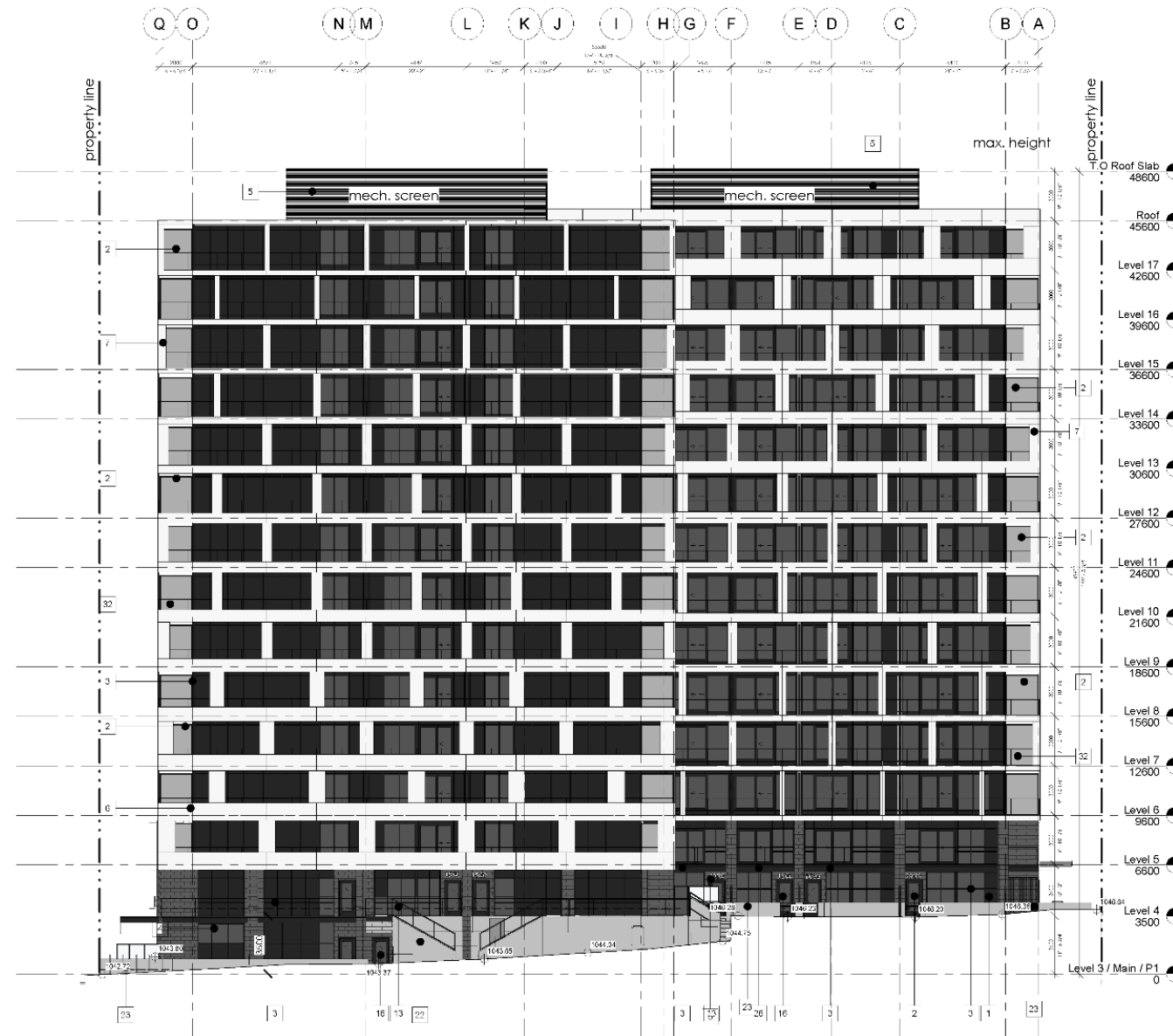
Proposed Development Permit Plans



Proposed Development Permit Plans

9/28/2018 11:50:28 AM

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KEYNOTE LEGEND	
Key Value	Keynote Text
1	WINDOW WALL- SPANDREL PANEL- DARK GREY
2	WINDOW WALL- GLASS PANEL- CLEAR GLASS
3	WINDOW WALL MULLIONS- DARK GREY
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5	METAL LOUVRE SCREEN - WHITE/SILVER
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34	LOUVRE SCREEN - WOOD

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REVISIONS		
Revision Schedule		
Number	Date (yy/mm/dd)	Description
1	20 8-26-18	FOR DEPART
2	20 8-26-18	FOR SUBMISSION
3	20 8-26-18	FOR
4	20 8-26-18	FOR



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BUS 403 287-9960 FAX 403 287-9962 info@casolark.com

PROJECT NAME AND ADDRESS
DOMINION
1018 McDougall Rd NE
{Lot 2, Block 10, Plan 081 5793}

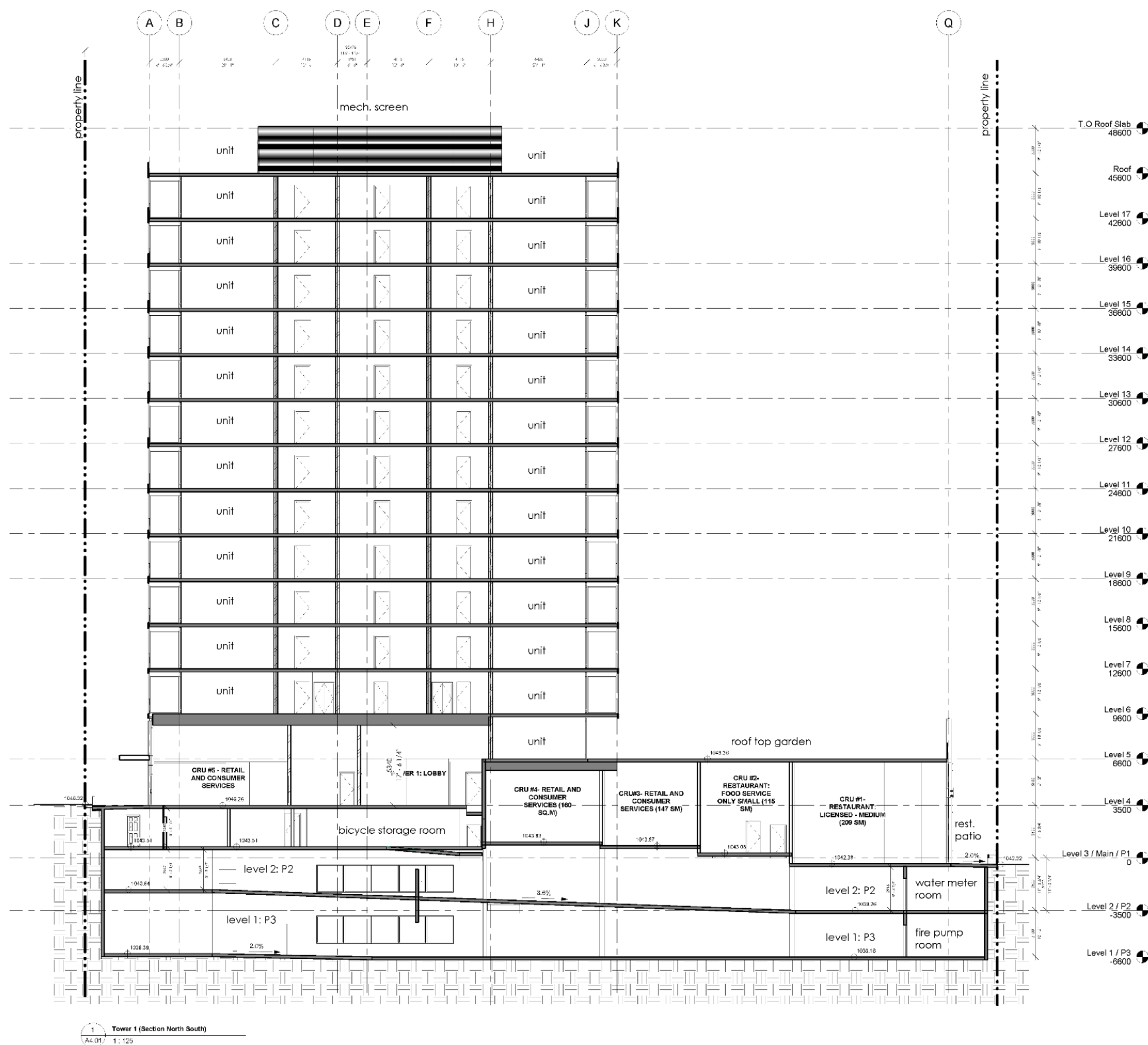
DRAWING
EAST ELEVATION

DRAWN BY MW	JOB NO. 1720	SHEET
CHECKED BY TC	DATE 20 8/06	
SCALE 1 : 150		A3.04

Proposed Development Permit Plans

9/28/2018 11:50:37 AM

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1 Tower 1 (Section North South)
A4.01 1:125

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REVISIONS

Revision Schedule		
Number	Date [yy/mm/dd]	Description
1	20 8-26-18	FOR DEPART
2	20 8-26-18	FOR SUBMITTAL
3	20 8-26-18	FOR SUBMISSION
4	20 8-26-18	FOR PERMIT

- T.O. Roof Slab 46600
- Roof 46600
- Level 17 42600
- Level 16 39600
- Level 15 36600
- Level 14 33600
- Level 13 30600
- Level 12 27600
- Level 11 24600
- Level 10 21600
- Level 9 18600
- Level 8 15600
- Level 7 12600
- Level 6 9600
- Level 5 6600
- Level 4 3500
- Level 3 / Main / P1 0
- Level 2 / P2 -3500
- Level 1 / P3 -6600



PROJECT NAME AND ADDRESS
DOMINION
1018 McDougall Rd NE
[Lot 2, Block 10, Plan 081 5793]

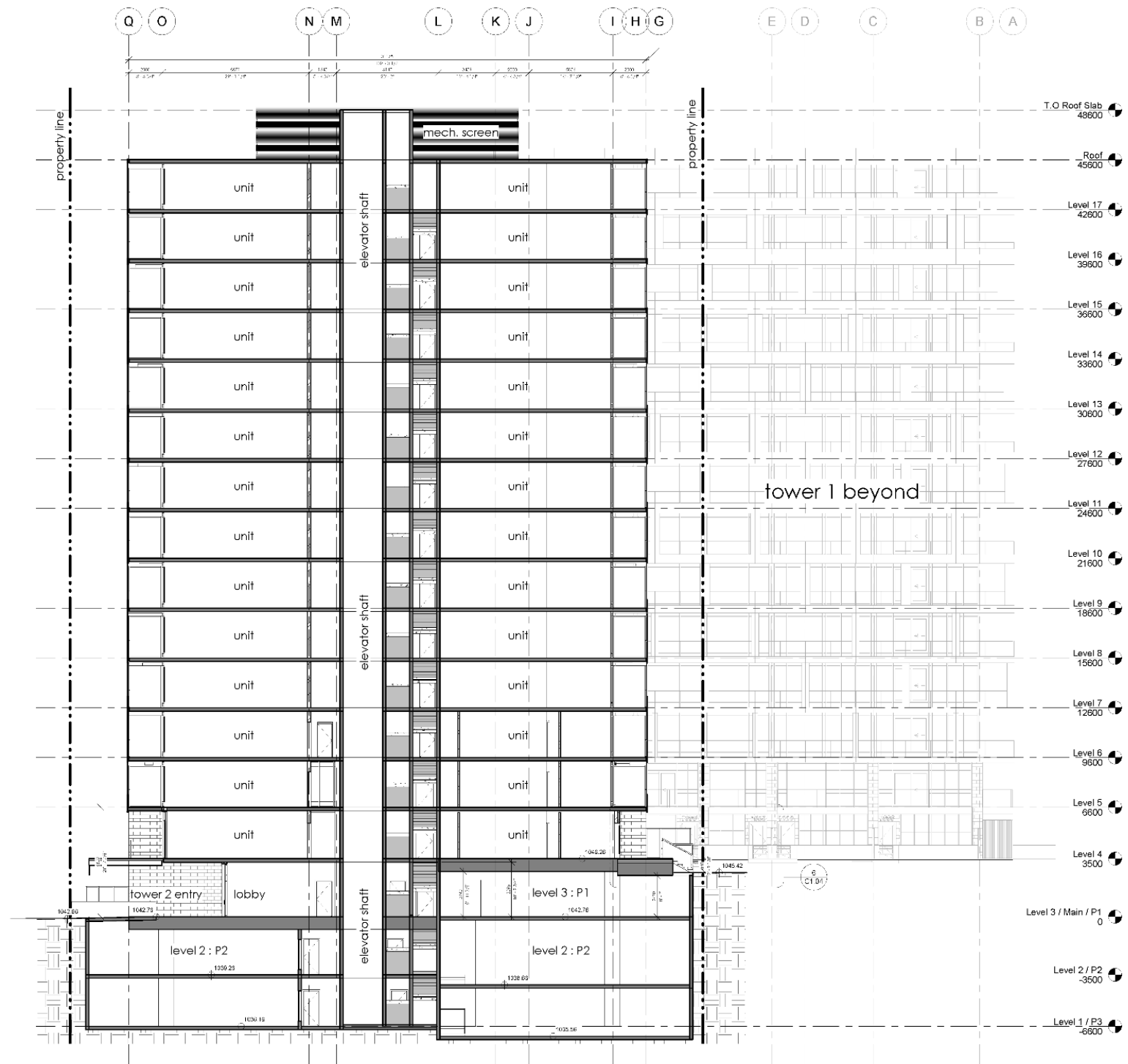
BUILDING SECTIONS (TOWER 1)

DRAWN BY MW	JOB NO. 1770
CHECKED BY TC	SHEET 3881
DATE DATE 20 8/26	A4.01
SCALE 1:125	

Proposed Development Permit Plans

9/28/2018 11:50:52 AM

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1 Tower 2 (Section North South)
A4.02 1:125

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REVISIONS

Revision Schedule		
Number	Date (yy/mm/dd)	Description
1	20-08-06	FOR DEBAT
2	20-08-27	FOR SUBMISSION
3	20-09-18	FOR
4	20-09-26	FOR



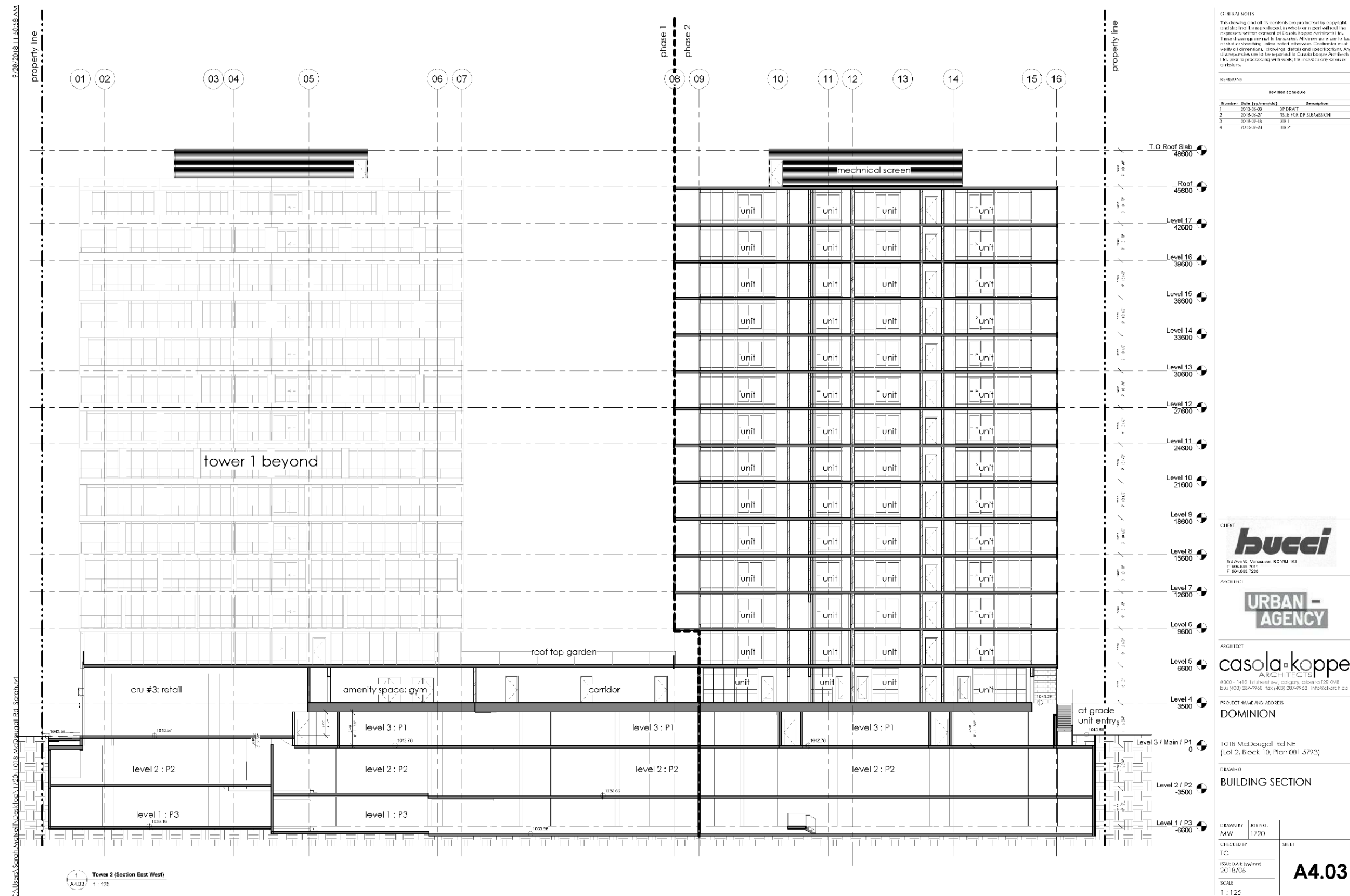
PRODUCT NAME AND ADDRESS
DOMINION

1018 McDougall Rd NE
[Lot 2, Block 10, Plan 08] 5793;

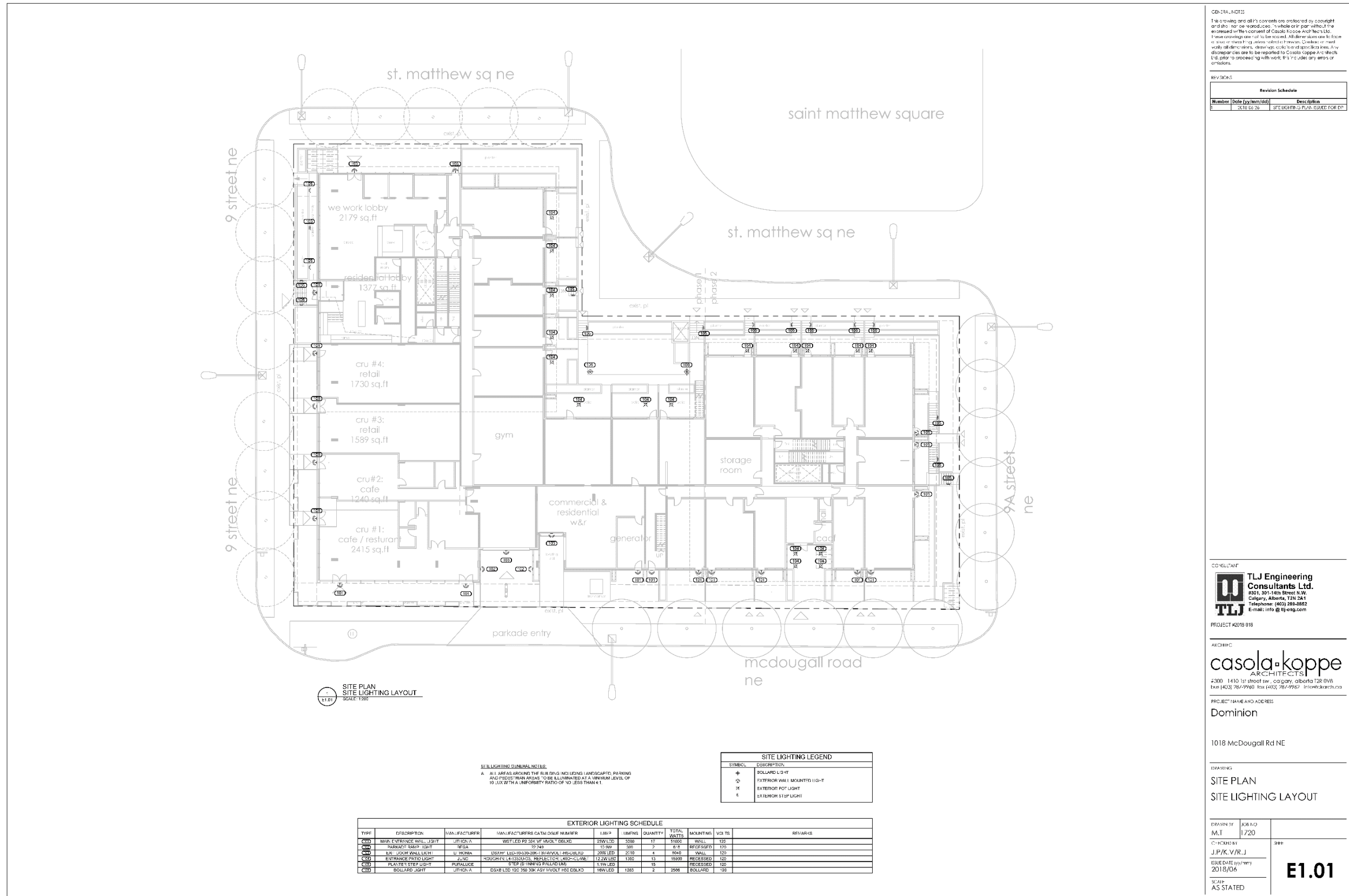
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BUILDING SECTION (TOWER 2)

DRAWN BY MW	JOB NO. 1720
CHECKED BY TC	SHEET 3/3
DATE DATE 20/8/20	
SCALE 1:125	A4.02

Proposed Development Permit Plans



Proposed Development Permit Plans



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REVISIONS

Number	Date (yy/mm/dd)	Description
01	2018/06/29	SITE LIGHTING PLAN - ISSUED FOR PERMIT

CONSULTANT
TLJ Engineering Consultants Ltd.
2501, 3015 14th Street N.W.
Calgary, Alberta, T2N 2A1
Telephone: (403) 299-8882
E-Mail: info@tlj-eng.com

PROJECT #2018 016

ARCHITECT
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2300 1410 1st street sw, calgary, alberta T2R 0V6
t: (403) 781-9960 f: (403) 781-9967 info@casolark.com

PROJECT NAME AND ADDRESS
Dominion

1018 McDougall Rd NE

DRAWING

**SITE PLAN
SITE LIGHTING LAYOUT**

DRAWN BY
M.T.
DATE
1/7/20
CHECKED BY
J.P.K./V.R.J.
ISSUE DATE
2018/06
SCALE
AS STATED

E1.01

Proposed Development Permit Plans

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Revision Schedule

Number	Date (yy/mm/dd)	Description
01	2018-06-28	FOR LIGHTING PLAN (ISSUED FOR DR)

CONSULTANT
TLJ Engineering Consultants Ltd.
 250, 390 14th Street NW
 Calgary, Alberta, T2N 2A1
 Telephone: (403) 288-5882
 E-mail: info@tlj-eng.com

PROJECT #2018-016

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 Bus: (403) 287-9960 Fax: (403) 287-9962 info@casolakuppe.ca

PROJECT NAME AND ADDRESS
 Dominion
 1018 McDougall Rd NE

DRAWN BY
 ELEVATION PLAN
 LIGHTING LAYOUT

DRAWN BY M.T.	JOB NO. 17/20
CHECKED BY J.P./K.V./R.J.	SHEET
ISSUE DATE (yy/mm) 2018/06	E1.02
SCALE AS STATED	

Proposed Development Permit Plans

D O M I N I O N



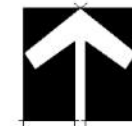
DRAWING LIST

- L-0 COVER PAGE
- L-1 SITE PLAN
- L-2 PLANTING PLAN
- L-3 PUBLIC PLAZA (ENLARGED)
- L-4 ST MATHEWS SQUARE ENLARGED
- L-5 WALLS GRADES AND HARDSCAPE
- L-6 SITE LIGHTING
- L-7 ROOF TOP GARDEN
- L-8 ROOF TOP PLANTING
- L-9 ROOF TOP LIGHTING
- L-10 ROOF TOP SEATING
- L-11 DETAILS
- L-12 SPECIFICATIONS

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V3M 3L7
Tel: 604.553.0044
Fax: 604.553.0045
Email: office@m2la.com



NO	DATE	REVISION DESCRIPTION	DR
1	2018.05.01	PROVISIONARY DESIGN	DY
2	2018.05.01	REVISED LAYOUT NEW ARCHITECTURE	TC
3	2018.05.01	IN PROGRESS FOR CLIENT CONFL	TC
4	2018.05.01	PROOF OF DESIGN AND LAYOUT	TC
5	2018.05.01	NEW PROOF OF DESIGN FOR NEW ARCH	TC
6	2018.05.01	BRUCE GARDEN LAYOUTS AND PROPOSALS	TC
7	2018.05.01	DP SUBMISSION	TC
8	2018.05.01	REVISED DP SUB-PACKAGE	TC



PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:
COVER PAGE

DATE: 07/24/18	DRAWING NUMBER:
SCALE: 1:300	L-0
DRAWN BY:	OF 12
DESIGN BY:	
CHECKED: MLH	

M2LA PROJECT NUMBER: **JOB NO. 18-03**

Proposed Development Permit Plans

KEY_QTY	BOTANICAL NAME	COMMON NAME	M2 JOB NUMBER: 18013	PLANTED SIZE / REMARKS
20	ACER TATARICUM	HOT HENS MANITOBA MAPLE SAHARAS		15CM GAL, 2 M STD, B&B
12	FRAXINUS PRINCEPS	PRINCEPS PRINCE ASH		15CM GAL, 1.8M STD, B&B
12	SYRINGA RETICULATA	IVORY SILK TREE		15CM GAL, 1.8 M STD, B&B
3	ULMUS AMERICANA	AMERICAN ELM		15CM GAL, PER MASTERPLAN
SHRUB				
62	HYDRANGEA ARBORESCENS 'ANNABELLE'	HYDRANGEA		40 POT
274	JUNIPERUS SCOPULORUM MOONBLOOM	MOONBLOOM JUNIPER HEDGE		40 POT
21	PRUNUS HISSO 'HIMELI'	CHERRY HISSO PINE		40 POT
30	ROSA COLLEGIAT HORDEN BLUSH	HORDEN BLUSH ROSE		42 POT
GRASS				
51	CALAMAGROSTIS ACUTIFLORA 'KARL FORSTER'	FOERSTER REED GRASS		41 POT
157	PENNESETUM ALOPECUROIDES 'HANELIN'	CHERRY FOUNTAIN GRASS		41 POT
PERENNIAL				
23	ANEMONE SYLVESTRIS	WINDFLOWER		41 POT
31	MATTEGGIA STRUTHEROPTERIS	OSTRICH FERN		41 POT
180	RUDEBECKIA FULVIDA LITTLE GOLDSTAR	CHERRY BLACK EYED SUSAN		41 POT
222	LANEUM MACULATUM PURPLE DRAGON	PURPLE DRAGON LAMNEM		41 POT

NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE ALBERTA LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER DATA STANDARDS. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REFER TO SPECIFICATIONS FOR LISTED CONTAINER HEADSIZES AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVEAL HAVE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND PRINCE GEORGE VALLEY. * SUBSTITUTIONS OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE SELECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO THE LANDSCAPE STANDARD - DETERMINATION OF CONDITIONS OF AVAILABILITY.

ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. PROVIDE CERTIFICATION UPON REQUEST.



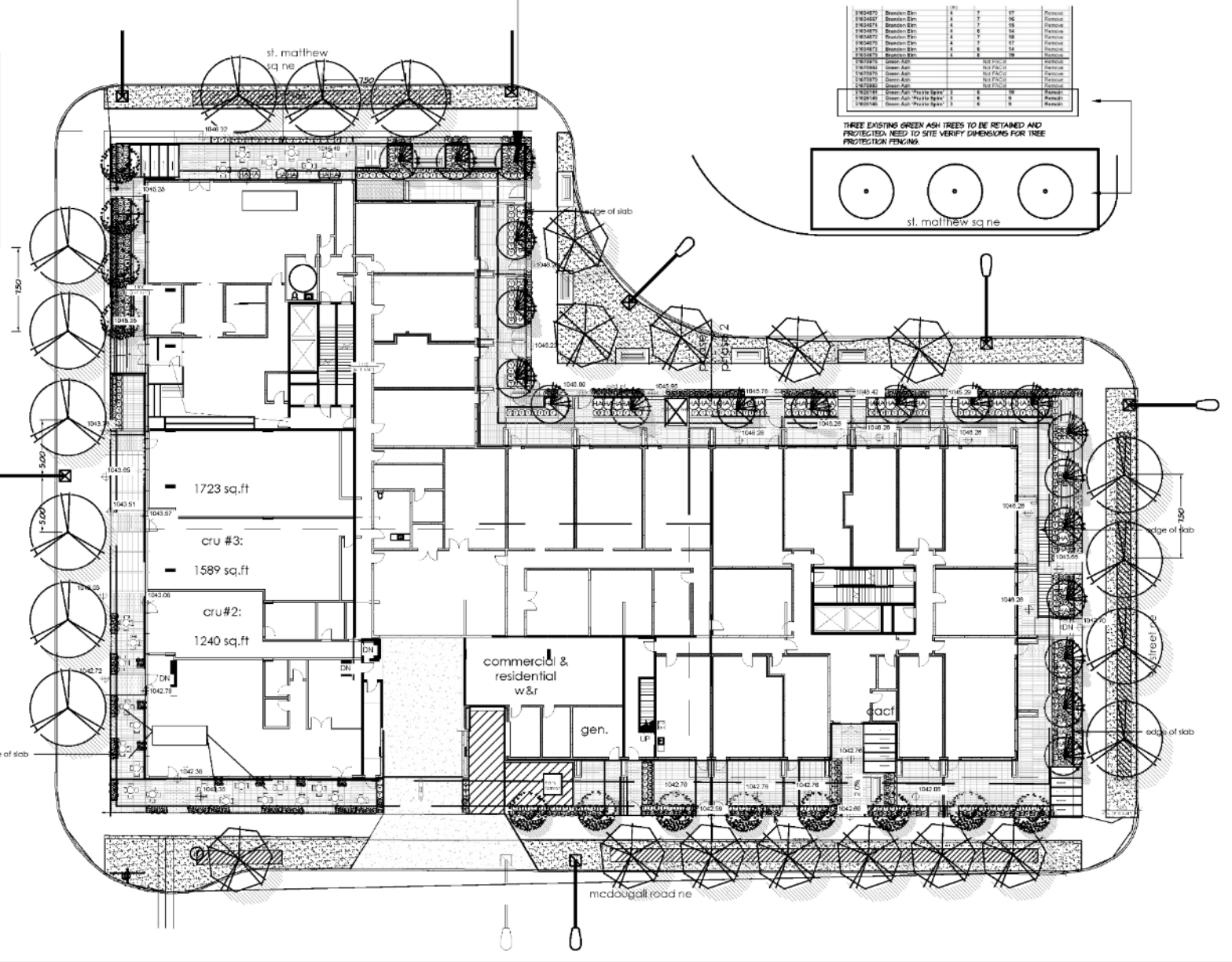
SPECIAL NOTE FOR BOULEVARD TREES:

CONTINGENCY FOR REPLACING ALL EXISTING BOULEVARD TREES IS IN EFFECT. REPLACEMENT TREES HAVE BEEN ADDED TO THE PLANTING SCHEDULE AND THE REPLACEMENT TREES HAVE BEEN DRAWN INTO THIS PLANTING PLAN.

THE SITE PLAN (L-1) IS SHOWING BOTH EXISTING AND REPLACEMENT TREES.

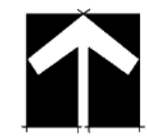
DIRECTION FROM CITY REQUIRED:

- ARE EXISTING TO BE REPLACED
- ARE EXISTING TO BE RETAINED AND PROTECTED WITH TREE PROTECTION FENCING



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NO.	DATE	REVISION DESCRIPTION	DR.
1	2018-01-10	PRELIMINARY DESIGN	EP
2	2018-01-26	REVISED LAYOUT NEW ARCHITECTURE	TK
3	2018-02-08	REVISED LAYOUT NEW ARCHITECTURE	TK
4	2018-02-08	ROOFTOP DESIGN AND LAYOUT	TK
5	2018-02-08	NEW ROOFTOP DESIGN PER NEW ARCH	TK
6	2018-02-08	DRIVE GARDEN LAYOUT/ARCHITECTURE	TK
7	2018-02-08	CP SUBMISSION	TK
8	2018-02-08	REVISED EIP SUBMISSION	TK

SEAL:

PROJECT:

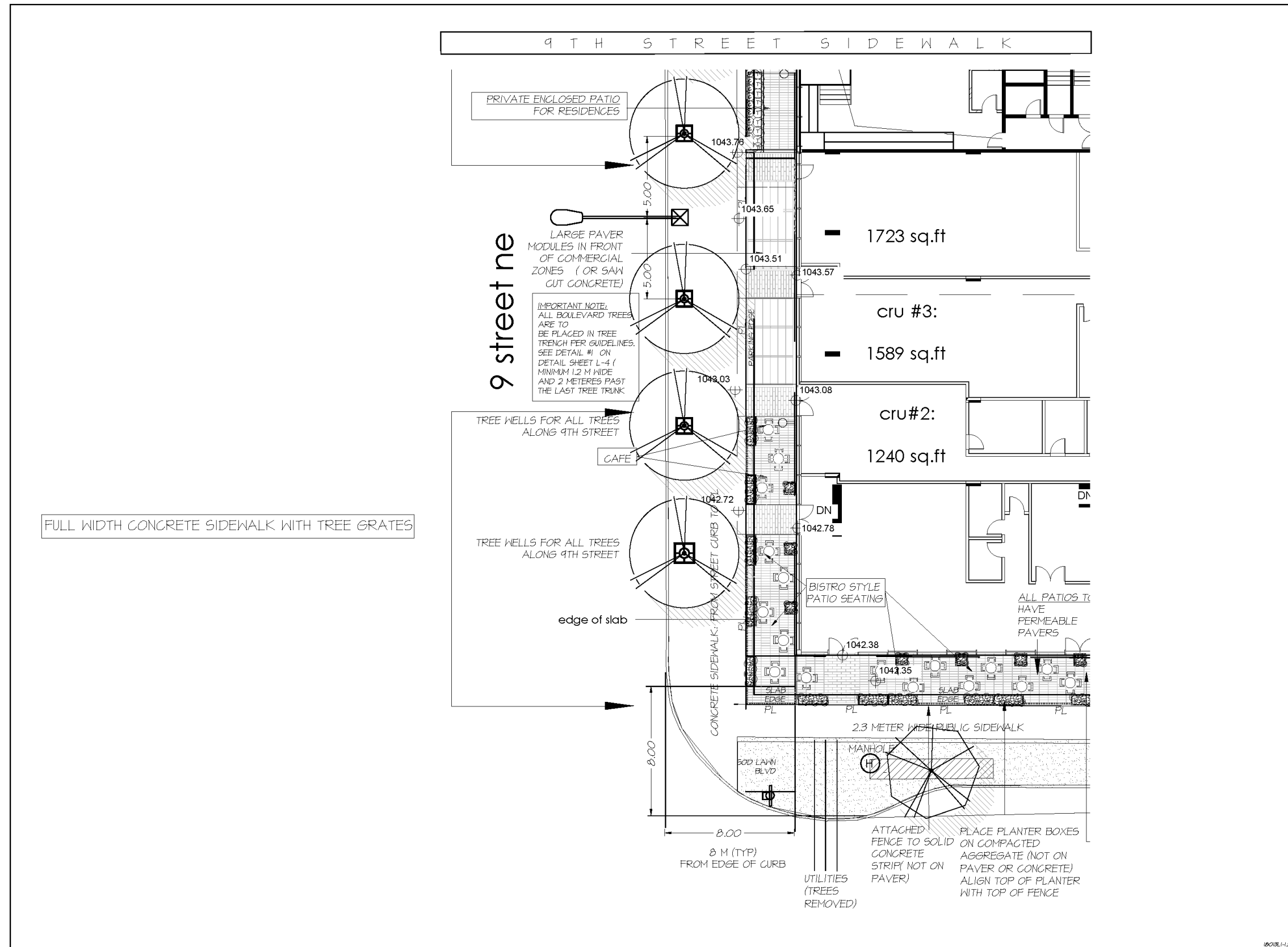
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:

PLANTING PLAN

DATE: 07MAR18	DRAWING NUMBER:
SCALE: A300	L2
DRAWN: TK	
DESIGN: TK	
CHK'D: HLM	OF 12
M2LA PROJECT NUMBER:	JOB NO. 18-013

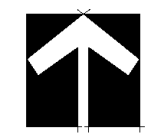
Proposed Development Permit Plans



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NO.	DATE	REVISION DESCRIPTION	DR.
1	2018-01-12	PRELIMINARY DESIGN	DT
2	2018-01-25	REVISED LAYOUT PER ARCHITECTURE	TK
3	2018-02-05	REVISED LAYOUT PER CLIENT CONSULT	TK
4	2018-02-15	ROOF TOP DESIGN AND LAYOUT	TK
5	2018-02-25	NEW PROPOSED DESIGN PER NEW ARCHITECT	TK
6	2018-03-05	DRIVE GARDEN LAYOUT/MOUNTING PROPORTIONS	TK
7	2018-03-15	DP SUBMISSION	TK
8	2018-03-25	REVISED DP SUBMISSION	TK

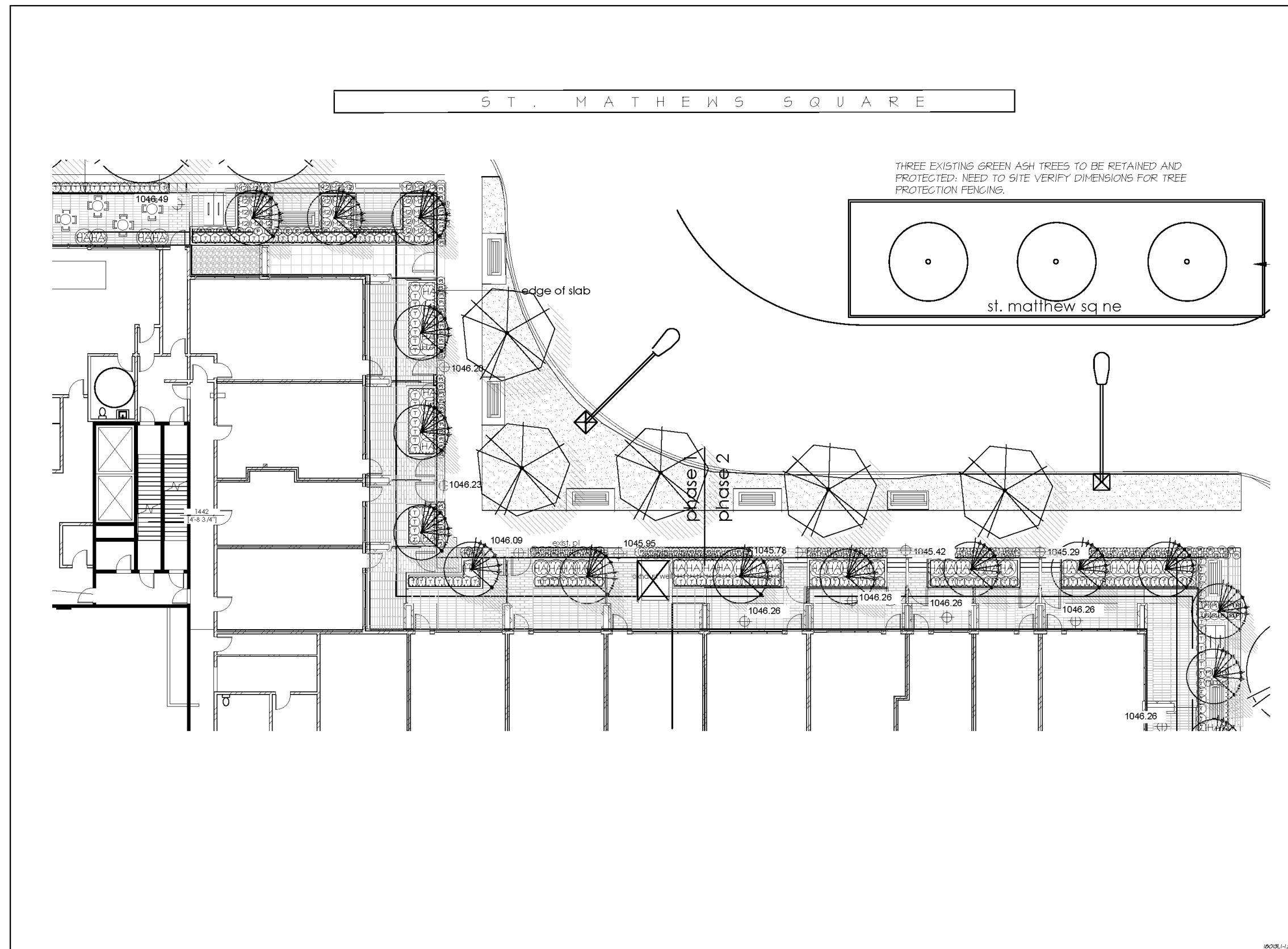


PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:
9TH STREET SIDEWALK

DATE: 07/18/18	DRAWING NUMBER:
SCALE: 1:100	L3
DRAWN: TK	
DESIGN: TK	
CHK'D: MTL	
M2LA PROJECT NUMBER: 18-018	JOB NO. 18-018

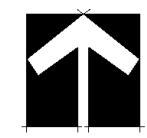
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NO.	DATE	REVISION DESCRIPTION	DR.
8	SEP 13/18	REVISED DFP SUBMISSION	TK
7	28 JUN 2018	DFP SUBMISSION	TK
6	5 JUL 2018	SHADE GARDEN LAYOUT/NOTES/FRONTAGES	TK
5	8 JUN 2018	NEW PROPOSED DESIGN PERM NEW ARCH	TK
4	27 MAY 2018	ROOF TOP DESIGN AND LAYOUT	TK
3	18 MAY 2018	NY FRONTAGES FOR CLIENT CONSULT	TK
2	11 MAY 2018	REVISED LAYOUT NEW ARCHITECTURE	TK
1	27 MAR 18	PRELIMINARY DESIGN	DT

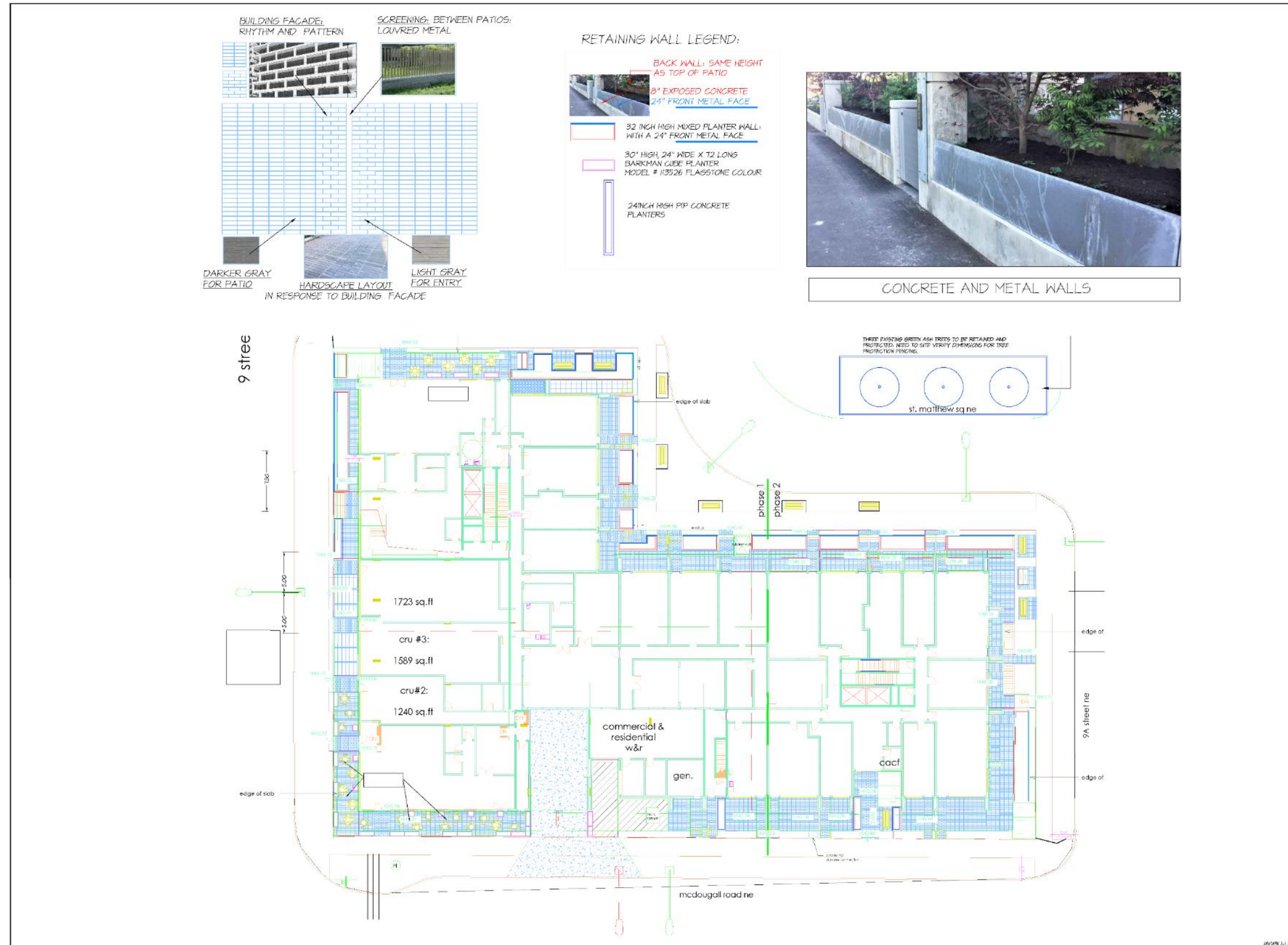


PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:
ST MATHEWS SQUARE

DATE: 07/18/18	DRAWING NUMBER:
SCALE: 1/8"=1'-0"	L4
DRAWN: TK	
DESIGN: TK	
CHK'D: MTL	OF 12
M2LA PROJECT NUMBER: 18-018	JOB NO. 18-018

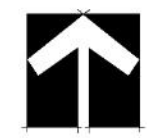
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NO.	DATE	REVISION DESCRIPTION	DR.
1	2018	PRELIMINARY DESIGN	ST
2	2018	REVISED LAYOUT AND ARCHITECTURE	TK
3	2018	REVISED LAYOUT AND ARCHITECTURE	TK
4	2018	REVISED LAYOUT AND ARCHITECTURE	TK
5	2018	NEW ROOFTOP DESIGN PER NEW APPRO	TK
6	2018	SPACE HARDER LAYOUT/FORMS	TK
7	2018	SP SUBMISSION	TK
8	2018	REVISED SP SUBMISSION	TK

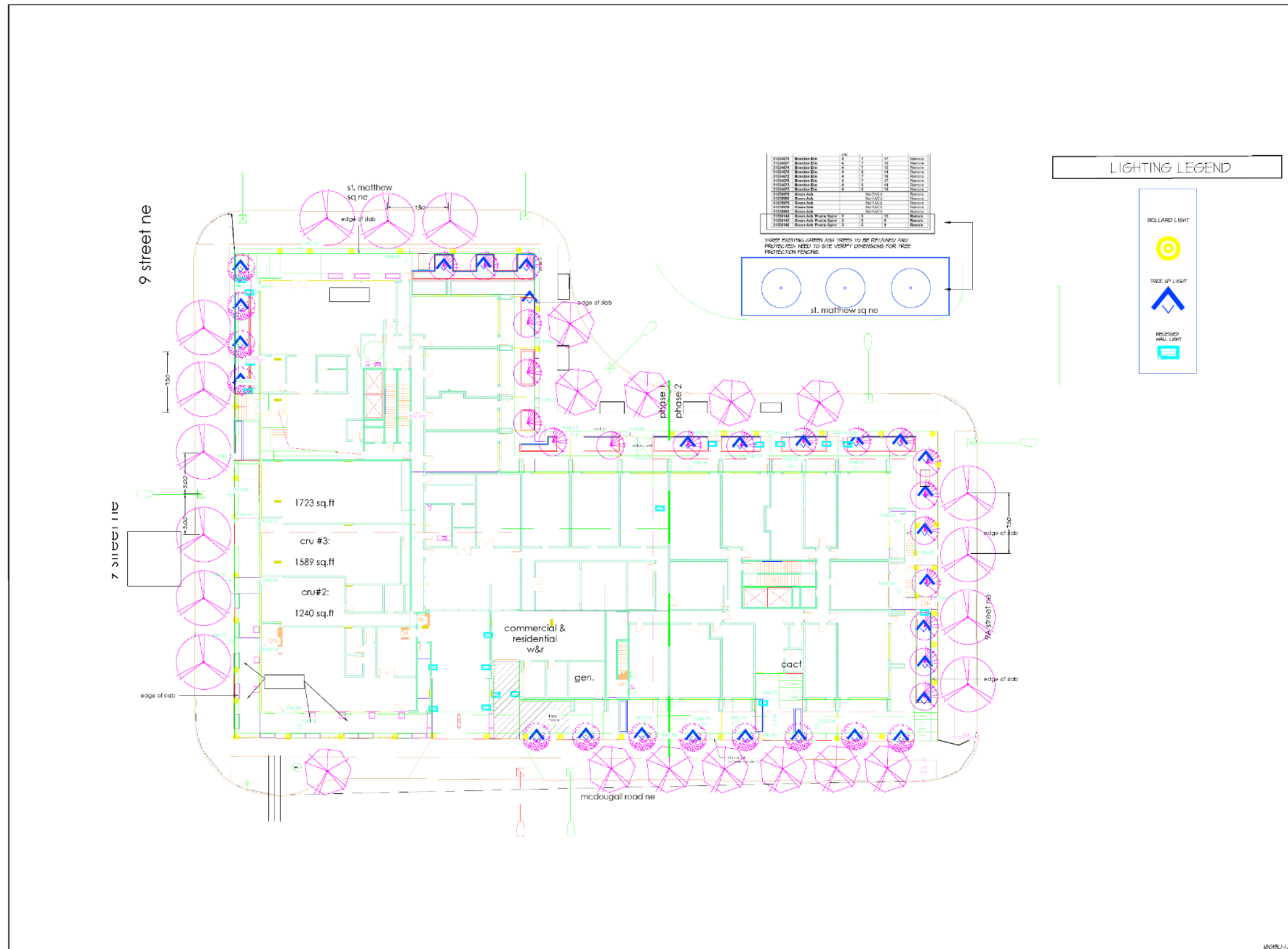


PROJECT:
THE DOMINION
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CALGARY, AB

DRAWING TITLE:
HARDSCAPE GRADES AND WALLS

DATE: 07/18/18 DRAWING NUMBER:
SCALE: 1/8"=1'-0" **L5**
DRAWN BY:
DESIGN: TROMBLY
CHK'D: HOLM **OF 12**
M2LA PROJECT NUMBER: **JOB NO. 18-013**

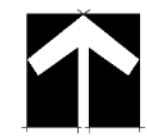
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NO.	DATE	REVISION DESCRIPTION	DR.
1	JUN 2018	PRELIMINARY DESIGN	DT
2	MAY 2018	REVISED LAYOUT AND ARCHITECTURE	TK
3	MAY 2018	REVISED LAYOUT AND ARCHITECTURE	TK
4	MAY 2018	REVISION DESIGN AND LAYOUT	TK
5	APR 2018	NEW ROOFTOP DESIGN PER NEW APPROVAL	TK
6	APR 2018	SHADE GARDEN LAYOUT AND PROPOSED	TK
7	JUN 2018	DP SUBMISSION	TK
8	JUN 2018	REVISED DP SUBMISSION	TK



PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:
SITE LIGHTING

DATE: 07-MAR-18 DRAWING NUMBER:
SCALE: 1:200
DRAWN: TK
DESIGN: TK
CHKD: HLM
L6
CF 12
MOLA PROJECT NUMBER: **JOB NO. 18-013**

Proposed Development Permit Plans



6" DEEP PLANTING TO PARAPET WALL.
ALSO SHOWING ORNAMENTAL GRASSES

BISTRO TABLES ON COMPOSITE WOOD DECKING
WITH DECORATIVE PEBBLES AND GRASSES

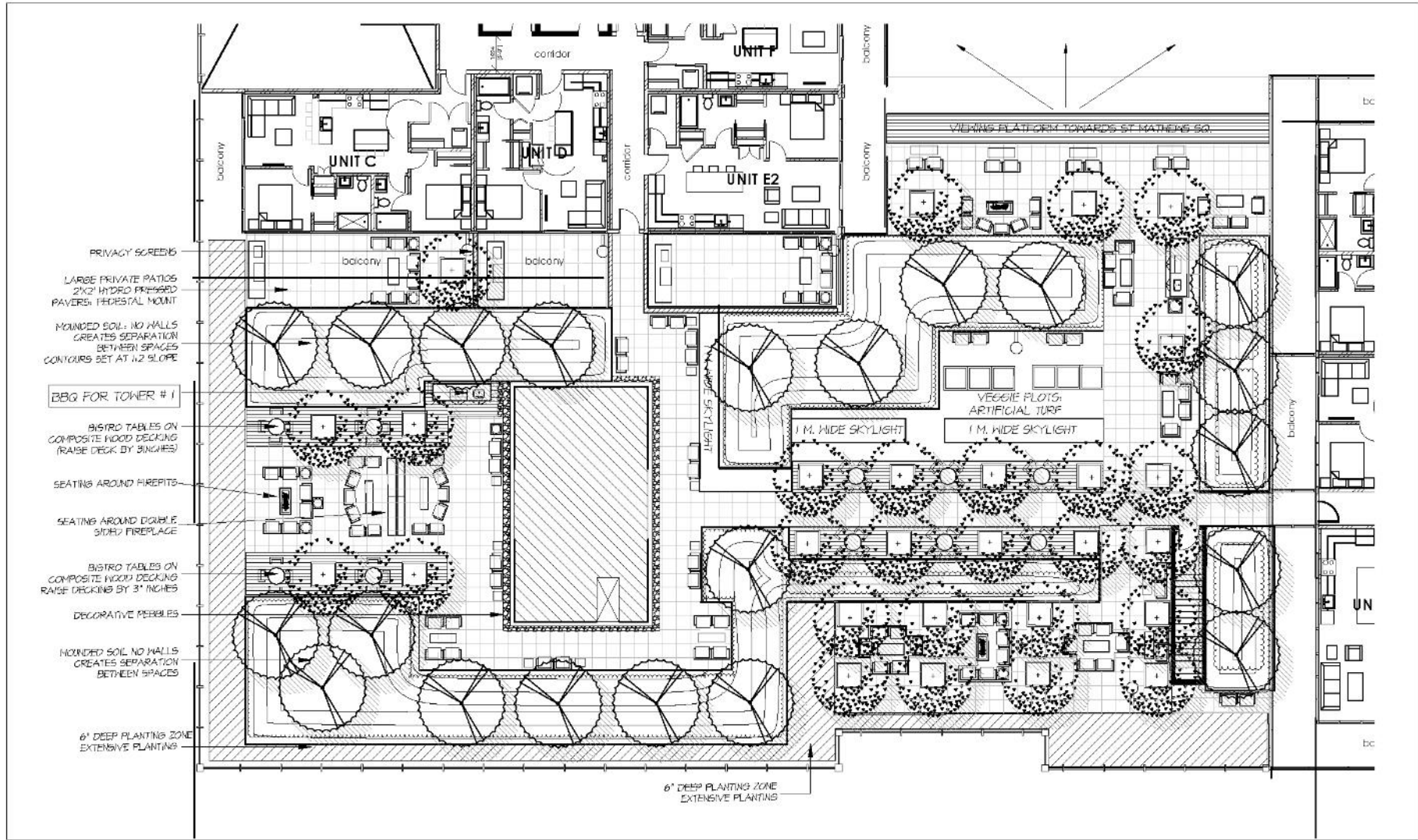
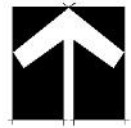
MOUND SOIL UP, NO WALLS

MATERIALS LEGEND	
	PLANTERS SUPPLIER: BABYBORN MODEL: B-WEST28 SIZE: 18" x 28" x 18" COLOUR: DARK BROWN
	OUTDOOR PAVING: PAVERS SUPPLIER: DDO MODEL: CONCRETE SIZE: 2302 X 121 COLOUR: NATURAL GRAY MOUNTED ON PEDESTALS
	WOOD COMPOSITE SUPPLIER: TREX MODEL: COMPOSITE 4000 SIZE: 6" THICK x LENGTH VARIABLE COLOUR: IRO
	BASE BED
	IRRIGIGATION SYSTEM NOT TO BE INCLUDED WITH ARCHITECT
	SOIL MOUND RAISE SOIL TO MAXIMUM 180° DEPTH USE LIGHT WEIGHT RAISE SOIL FOR THE FIRST 18" OF SOIL. MAY BE SUBSTITUTED WITH STRUCTURAL ENGINEER

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PRIVACY SCREENS

LARGE PRIVATE PATIOS
2"x2" HYDRO-FINISHED
PAVERS: THERMAL MOUNT

MOUND SOIL - NO WALLS
CREATES SEPARATION
BETWEEN SPACES
CONTOURS SET AT 1/2 SLOPE

BBG FOR TOWER # 1

BISTRO TABLES ON
COMPOSITE WOOD DECKING
(RAISE DECK BY 3" INCHES)

SEATING AROUND FIREPITS

SEATING AROUND DOUBLE
SIDED FIREPLACE

BISTRO TABLES ON
COMPOSITE WOOD DECKING
RAISE DECKING BY 3" INCHES

DECORATIVE PEBBLES

MOUND SOIL NO WALLS
CREATES SEPARATION
BETWEEN SPACES

6" DEEP PLANTING ZONE
EXTENSIVE PLANTING

NO.	DATE	REVISION DESCRIPTION	DR.
0	07/19/18	REVISED FOR SUBMISSION	TC
1	08/01/18	017 SUBMISSION	TC
2	08/02/18	SPACE BARRED LANDSCAPE PROPOSALS	TC
3	08/03/18	NEW HOOP OF DESIGN FOR HOOP	TC
4	08/03/18	ADJUST TOP DECKING AND LANDSC	TC
5	08/03/18	NO FRONTAGES PER CLIENT CORR	TC
6	08/03/18	REVISED LAYOUT WITH ARCHITECTURE	TC
1	08/08/18	PRELIMINARY DESIGN	DP



PROJECT:

THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:
ROOFTOP DESIGN

DATE: 07/19/18 DRAWING NUMBER:

SCALE: 1:100

DRAWN: TC

DESIGN: TC

CHECK: WLM

OF 12

L7

M2LA PROJECT NUMBER: **JOB NO. 18-018**

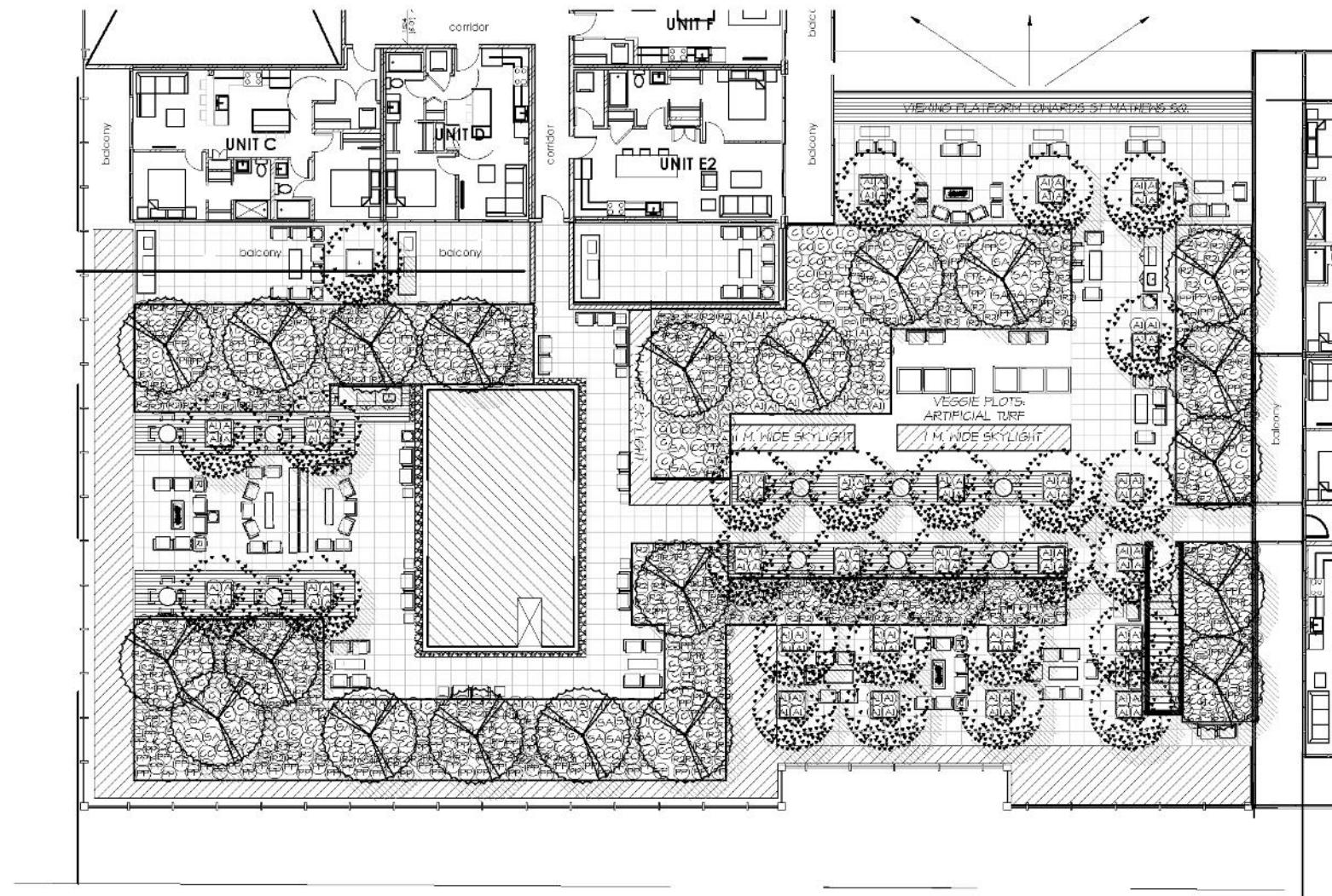
Proposed Development Permit Plans



PLANT SCHEDULE			M2 JOB NUMBER: 18019
KEY QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
TREE	25 SYRINGA RETICULATA	IVORY SILK TREE	75CM GALL. 5 Y STD, B1B
TREE	21 TILIA HONGOLICA HARVEST GOLD	HONGOLIAN LINDEN HARVEST GOLD	60H GAL, 1.8M STD, B4B
SHRUB	50 SALIX COMMUTATA	POWDERFACE HILLON	#2 POT
GRASS	185 CALAMAGROSTIS ACUTIFLORA KAEH. FOERSTER	FOERSTER REED GRASS	#1 POT
GRASS	144 PENISETUM ALOPECUROIDES HAVELIN'	DWARF FOUNTAIN GRASS	#1 POT
PERENNIAL	181 ARTEMESIA SCHMIDTIANA 'SILVER MOUND'	SILVER MOUND ARTEMESIA	#1 POT
PERENNIAL	281 MEKOVSKIA ATRIPLEXIFOLIA	RUSSIAN SAGE	#1 POT
PERENNIAL	390 RUDBECKIA FULGIDA LITTLE GOLDSTAR	DWARF BLACK EYED SUSAN	#1 POT
TREE	21 TILIA HONGOLICA HARVEST GOLD	HONGOLIAN LINDEN HARVEST GOLD	60H GAL, 1.8M STD, B4B

NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER CANA STANDARDS. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REFER TO SPECIFICATIONS FOR DEF-RED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVIEW: MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND TRAGER VALLEY. * SUBSTITUTIONS: OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS. TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD - DEFINITION OF CONDITIONS OF AVAILABILITY.

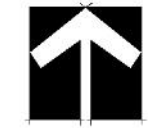
ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. PROVIDE CERTIFICATION UPON REQUEST.



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NO.	DATE	REVISION DESCRIPTION	DR.
0	07/18/18	REVISED FOR SUBMISSION	TK
1	08/01/18	FOR SUBMISSION	TK
2	08/01/18	SPACE SAVED LANDSCAPE FRONTAGES	TK
3	08/01/18	NEW ROOF TOP DESIGN FOR GARAGE	TK
4	08/01/18	ADOPTED DESIGN AND LANDSCAPE	TK
5	08/01/18	FOR FRONTAGES PER CLIENT CORP.	TK
6	08/01/18	REVISED LAYOUT FOR ARCHITECTURE	TK
1	08/01/18	PRELIMINARY DESIGN	DP



PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

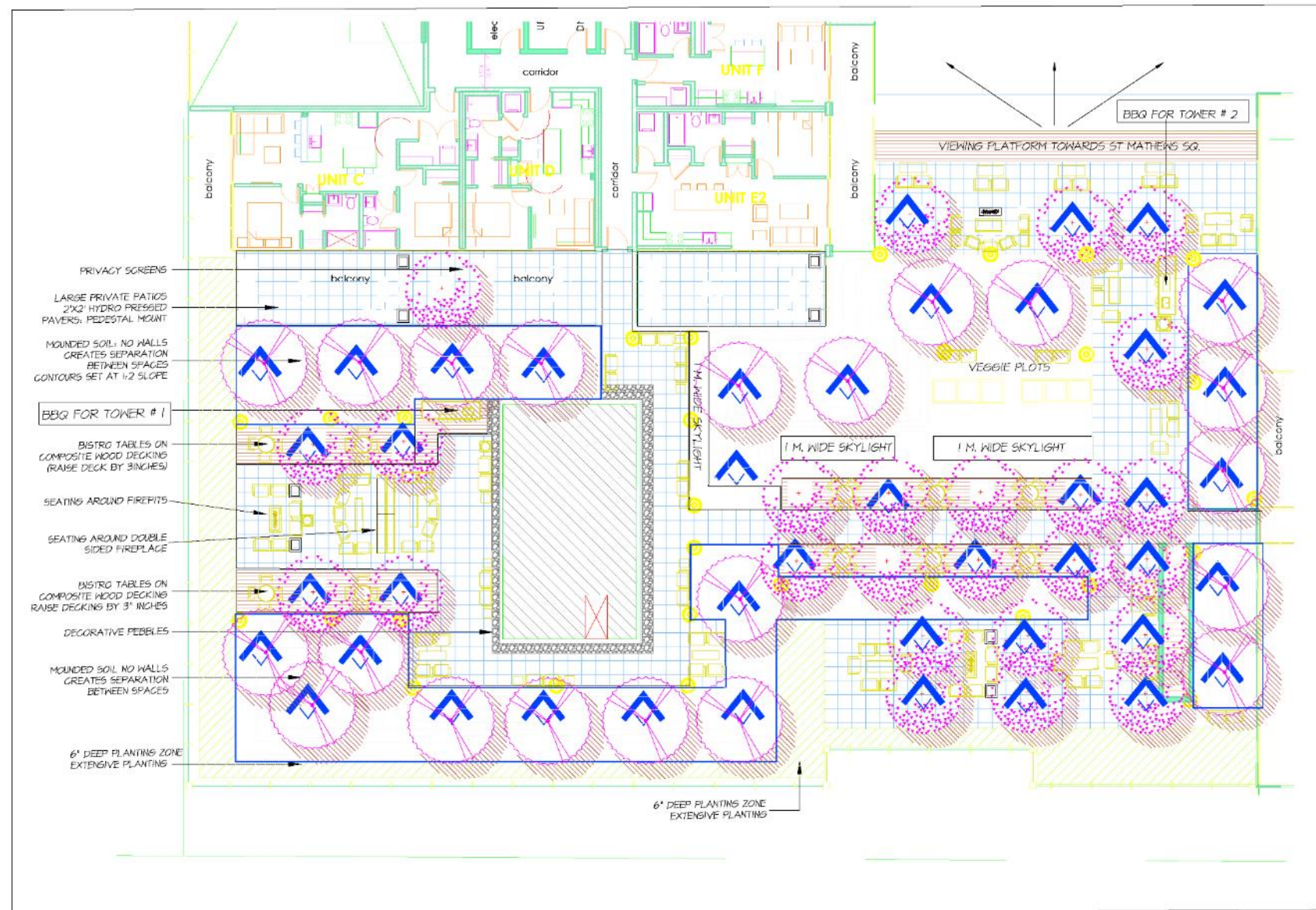
DRAWING TITLE:
ROOFTOP PLANTING

DATE: 07/18/18	DRAWING NUMBER:
SCALE: 1:100	L7
DRAWN: TK	
DESIGN: TK	
CHECK: M2LA	

OF 12

M2LA PROJECT NUMBER: **JOB NO. 18-018**

Proposed Development Permit Plans



LIGHTING LEGEND



BOLLARD LIGHTS

TREE UPLIGHTS

FIXTURES TO BE FINALIZED
ALL FIXTURES TO BE LED

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NO.	DATE	REVISION DESCRIPTION	DR.
1	07/2018	PRELIMINARY DESIGN	DM
2	08/2018	REVISED LAYOUT PER ARCHITECTURE	DM
3	09/2018	REVISED LAYOUT PER CLIENT COMMENT	DM
4	10/2018	ADJUSTED DESIGN AND LAYOUT	DM
5	11/2018	NEW ROOFTOP DESIGN PER NEW PLAN	DM
6	12/2018	SPACE GARDEN LANDSCAPE PROPOSALS	DM
7	01/2019	EPV SUBMISSION	DM
8	02/2019	REVISED DP SUBMISSION	DM



PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

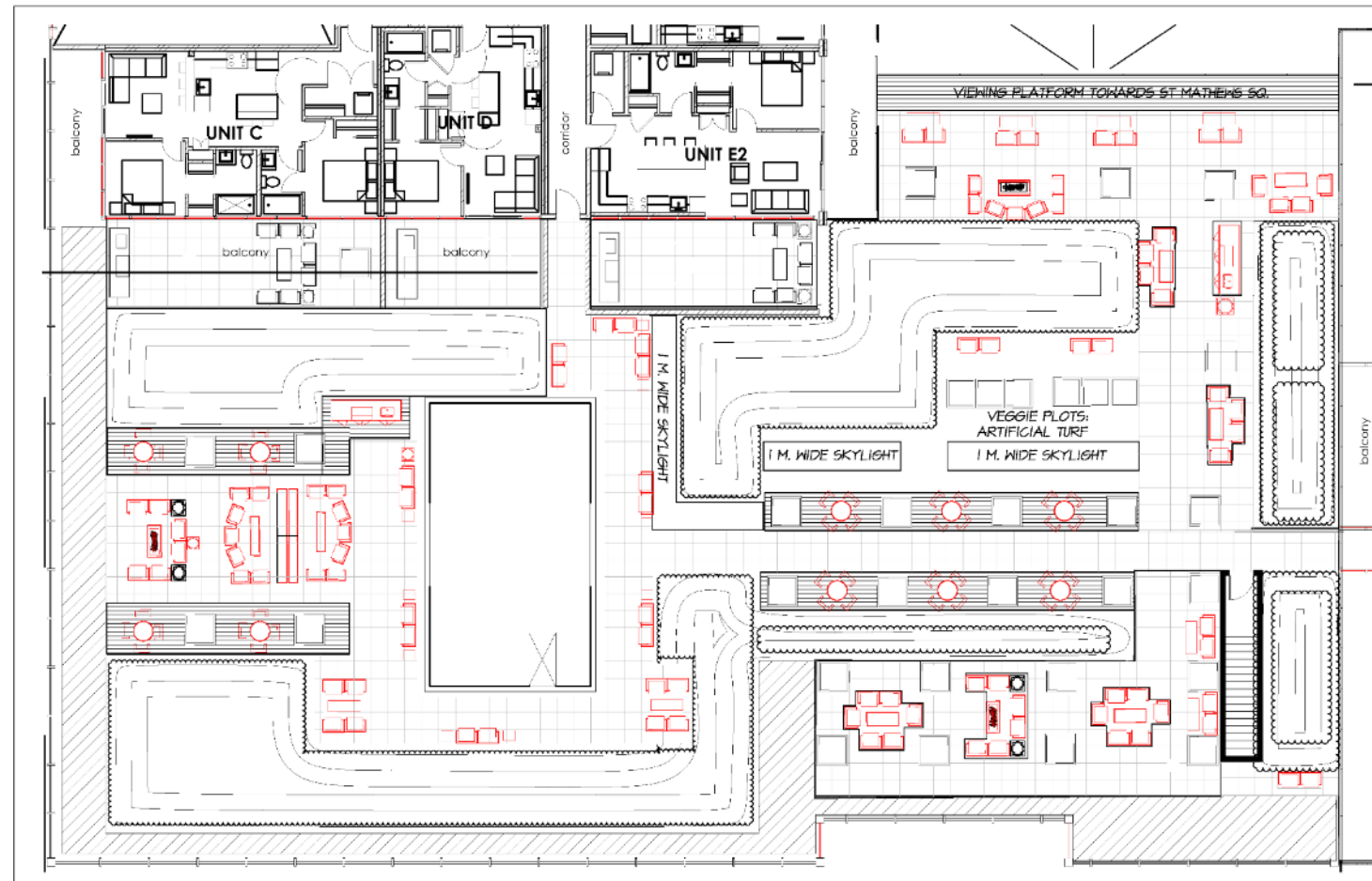
DRAWING TITLE:
ROOFTOP LIGHTING

DATE: 07/2018 DRAWING NUMBER:
SCALE: N100
DRAWING: TK
DESIGN: TK
CHECK: MILM
M2LA PROJECT NUMBER: **JOB NO. 18-013**

L9
OF 12

Proposed Development Permit Plans

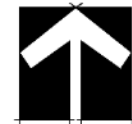
SEATING FOR 140 PEOPLE



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NO.	DATE	REVISION DESCRIPTION	DR.
0	07/11/20	REVISED OF SUBMISSION	TK
1	08/26/20	EP SUBMISSION	TK
2	08/26/20	SPRINKLER LAYOUT/FRONTAGES	TK
3	08/26/20	NEW ROOFTOP DESIGN PER NEW ARCH	TK
4	09/01/20	ADJUSTED DESIGN AND LAYOUT	TK
5	09/01/20	NEW FRONTAGES PER CLIENT CORREK	TK
2	09/01/20	REVISED LAYOUT PER ARCHITECTURE	TK
1	09/01/20	PRELIMINARY DESIGN	DY



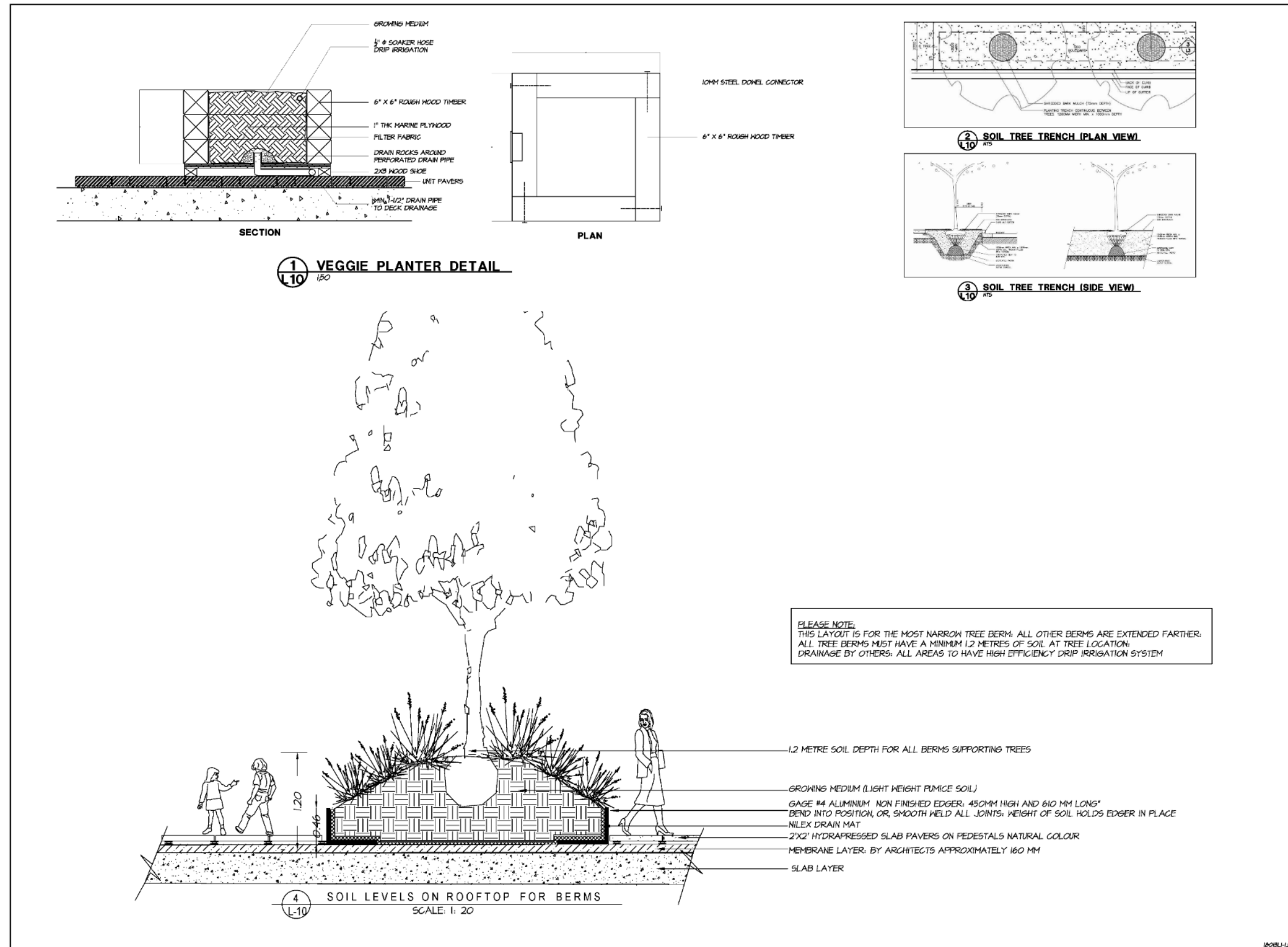
PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:
ROOFTOP SEATING

DATE: 07/14/20	DRAWING NUMBER:
SCALE: 1/8"=1'-0"	L-10
DRAWN: TK	OF 12
DESIGN: TK	
CHECK: MILM	

M2LA PROJECT NUMBER: **JOB NO. 18-018**

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NO.	DATE	REVISION DESCRIPTION	DR.
1		PRELIMINARY DESIGN	ET
2	11 MAY 2018	REVISED LAYOUT AND ARCHITECTURE	TC
3	14 MAY 2018	REVISED PRESENTATION FOR CLIENT CONSULTATION	TC
4	17 MAY 2018	ROOFTOP DESIGN AND LAYOUT	TC
5	17 MAY 2018	NEW ROOFTOP DESIGN FOR NEW AREA	TC
6	17 MAY 2018	SHAKE GARDEN LAYOUT/ARCHITECTURE	TC
7	22 JUN 2018	DP SUBMISSION	TC
8	27 JUN 2018	REVISED DP SUBMISSION	TC

SCALE:



PROJECT:

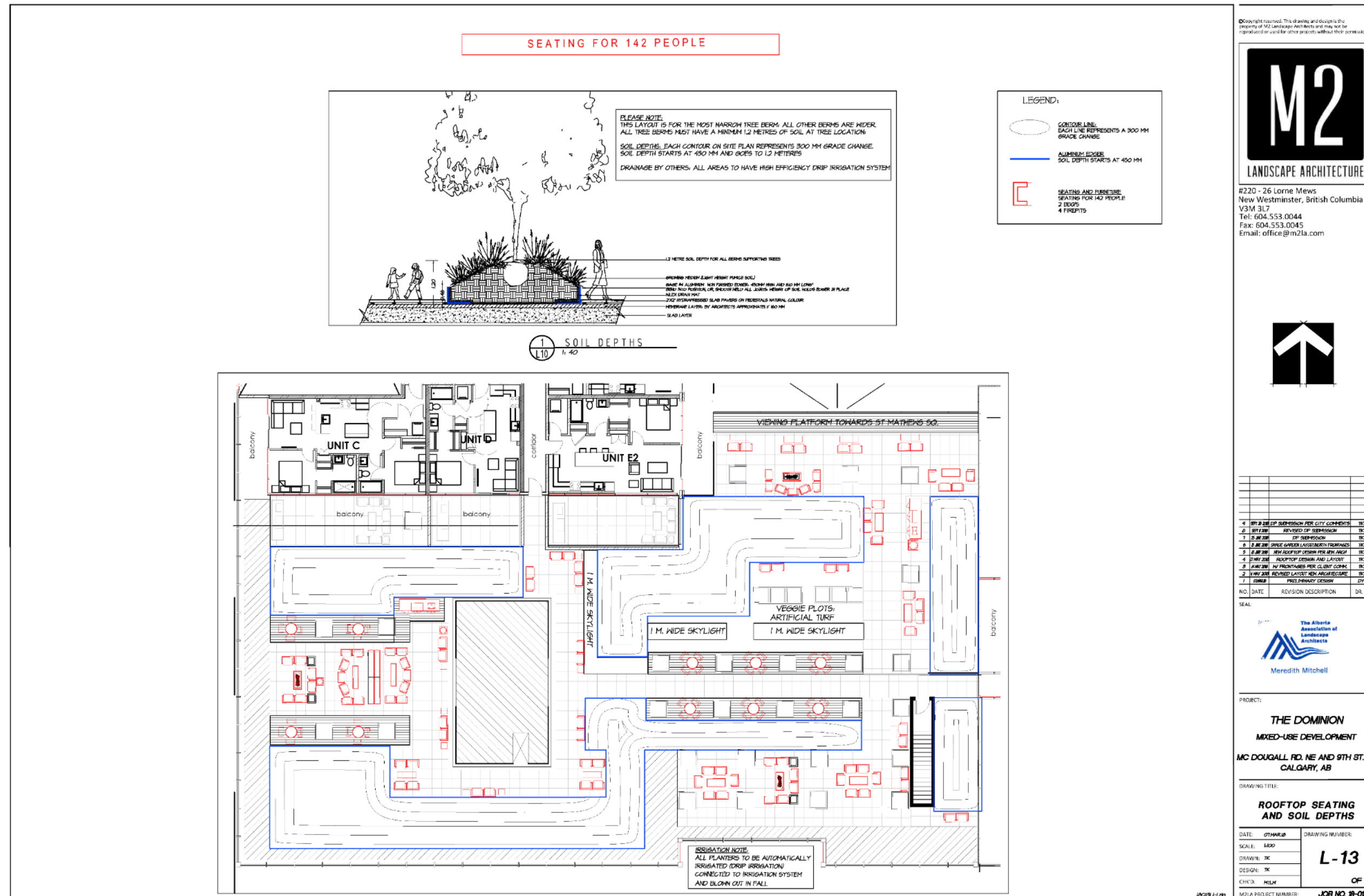
THE DOMINION
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MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:

DETAILS

DATE: 07 MAR 18	DRAWING NUMBER:
SCALE: 1:200	L-11
DRAWN BY: TC	
DESIGN BY: TC	
CHECKED BY: MELM	OF 12
M2LA PROJECT NUMBER:	JOB NO. 18-018

Proposed Development Permit Plans



Proposed Development Permit Plans

SECTION

PLAN

1 VEGGIE PLANTER DETAIL
L14 1/20

2 SOIL TREE TRENCH (PLAN VIEW)
L10

2 SOIL TREE TRENCH (SIDE VIEW)
L14

BENCH

BANCAL BENCH BY SANTA COLE (LANDSCAPE FORMS)
Size: 88" x 20" x 18"
Backed
Ipe unfinished wood
Powdercoated Metal in Stormcloud

Fig. 1 - Drilled hole size, board orientation

BIKE RACK

RIDE BIKE RACK BY LANDSCAPE FORMS
Capacity 2 bikes
Size: 3.3" x 28" x 26"
Surface mounted
Powdercoated Metal in Stormcloud colour
4 stainless steel leveling gildes within the base are provided for fine adjustment from top side of base.
Cover plate over bike rack base provides seamless appearance.
Must be spaced 30" apart and 24" from a wall to meet APBP guidelines

Fig. 3 - Recommended spacing, according to Association of Pedestrian and Bicycle Professionals (APBP)

BOLLARDS

STOP BOLLARD WITH LED LIGHT BY LANDSCAPE FORMS
Sleeve is cast aluminum with an integral ring at the base
Size: 9" x 7" x 32.5"
Surface-mounted bollard is stainless steel
Powdercoated Metal in Stormcloud colour
Post is 4" structural steel pipe. A stainless steel base plate welded to the steel pipe is anchored to the concrete in three locations.

LITTER BIN

COLLECT LITTER BY LANDSCAPE FORMS
23-gallon capacity
Size: 15.9" x 23.5" x 37.9"
Surface-mounted
Powdercoated matte black color to coordinate with the standard black bin.
Bin pivots open for emptying; bag is pulled out at a 35° angle

Side Opening Litter

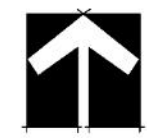
METAL PLANTER

30" HIGH, 24" WIDE X 72" LONG
FREE STANDING
MODEL # 113526 FLAGSTONE COLOUR BY BARKMAN CUBE PLANTER

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NO.	DATE	REVISION DESCRIPTION	DR.
1		PRELIMINARY DESIGN	DT
2			
3			
4			
5			
6			
7			
8			
9			
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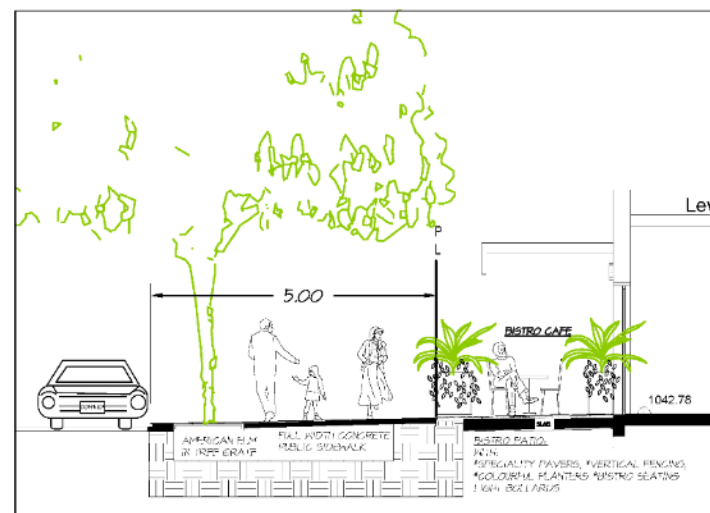


PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

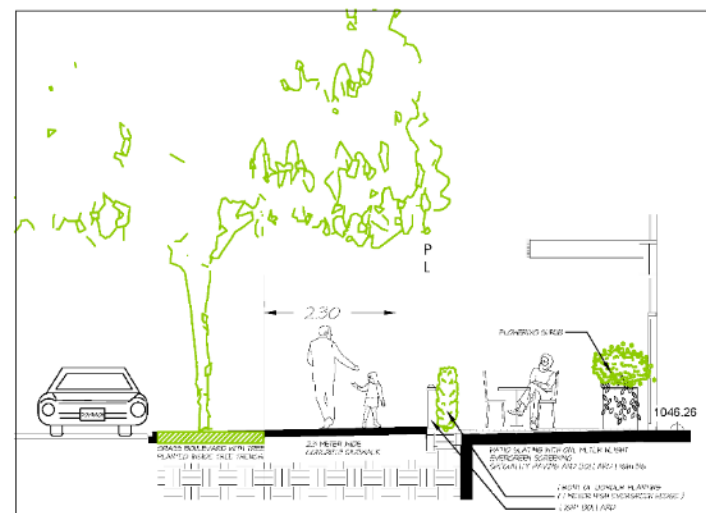
DRAWING TITLE:
DETAILS

DATE: 07/24/18	DRAWING NUMBER:
SCALE: 1/200	L-14
DRAWN BY: TR	
DESIGN: TR	
CHCKD: MELH	OF 15
M2LA PROJECT NUMBER: JOB NO. 18-03	

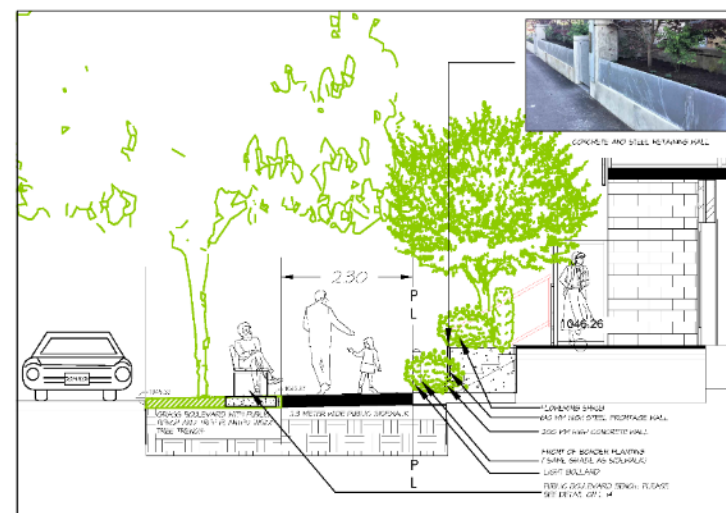
Proposed Development Permit Plans



1 SECTION # 1
1:15A
1:50



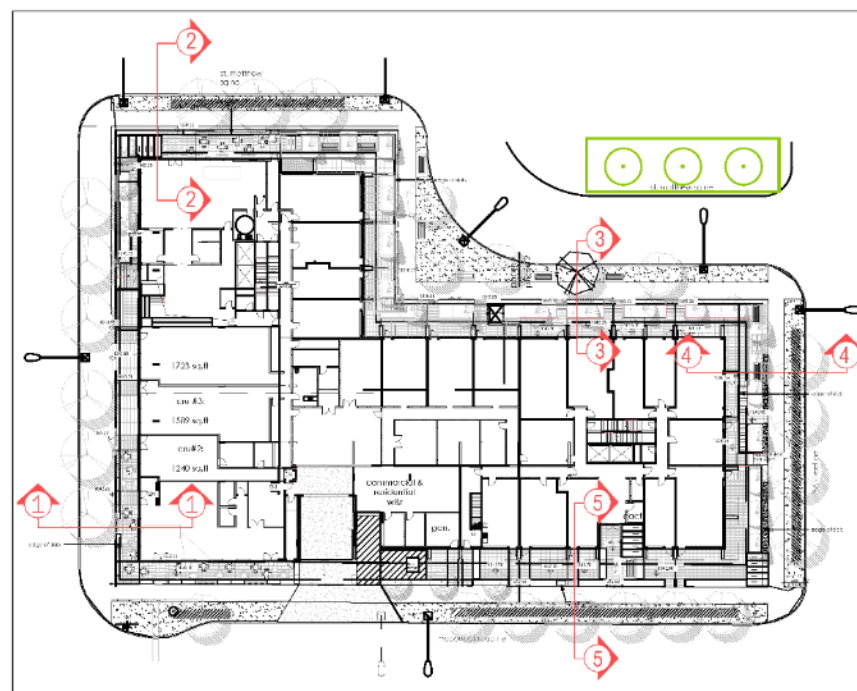
2 SECTION # 2
1:15A
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3 SECTION # 3
1:15A
1:50



TYPICAL PLANTINGS IN RAISED PLANTERS ALONG 9TH STREET



SECTION KEY PLAN



TYPICAL FLOWERING SHRUBS FOR NORTH AND EAST SHADY SIDES.

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V3M 3L7
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Fax: 604.553.0045
Email: office@m2la.com



NO.	DATE	REVISION DESCRIPTION	DR.
1	JAN 2018	PRELIMINARY DESIGN	DM
2	JUN 2018	REVISED LAYOUT PER ARCHITECT	DM
3	JUN 2018	REVISED PER CLIENT COMMENT	DM
4	JUN 2018	REDOOR DESIGN AND LAYOUT	TC
5	JUN 2018	NEW ROOFTOP DESIGN PER NEW ARCH	TC
6	JUN 2018	DRIVE GARDEN LAYOUT PER ARCHITECT	TC
7	JUN 2018	DP SUBMISSION	TC
8	JUN 2018	REVISED DP SUBMISSION	TC
9	JUN 2018	DP SUBMISSION PER CITY COMMENTS	TC

SEAL:



PROJECT:

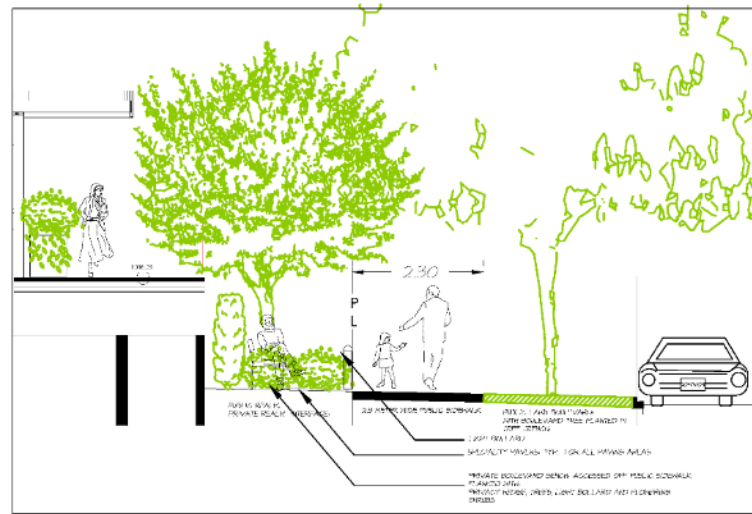
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:

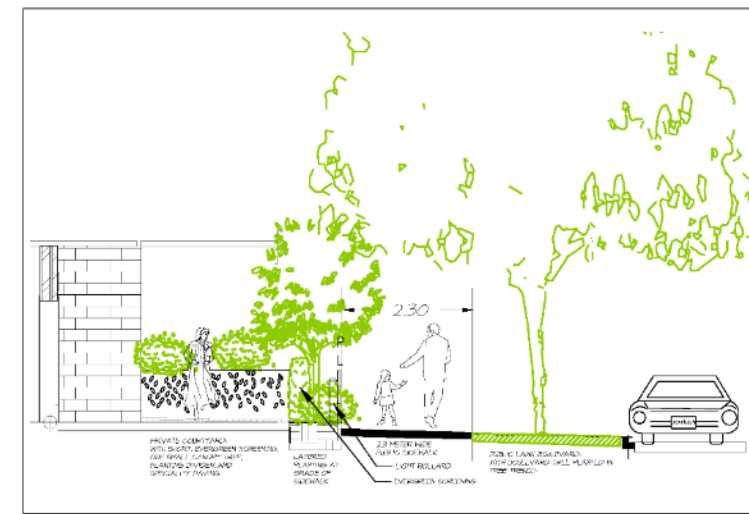
PUBLIC/PRIVATE REALM SECTIONS

DATE: 07/14/18	DRAWING NUMBER:
SCALE: ASB	L-15 (A)
DRAWN: TC	OF 15
DESIGN: TC	
CHECK: MLM	
MOLA PROJECT NUMBER:	JOB NO. 18-03

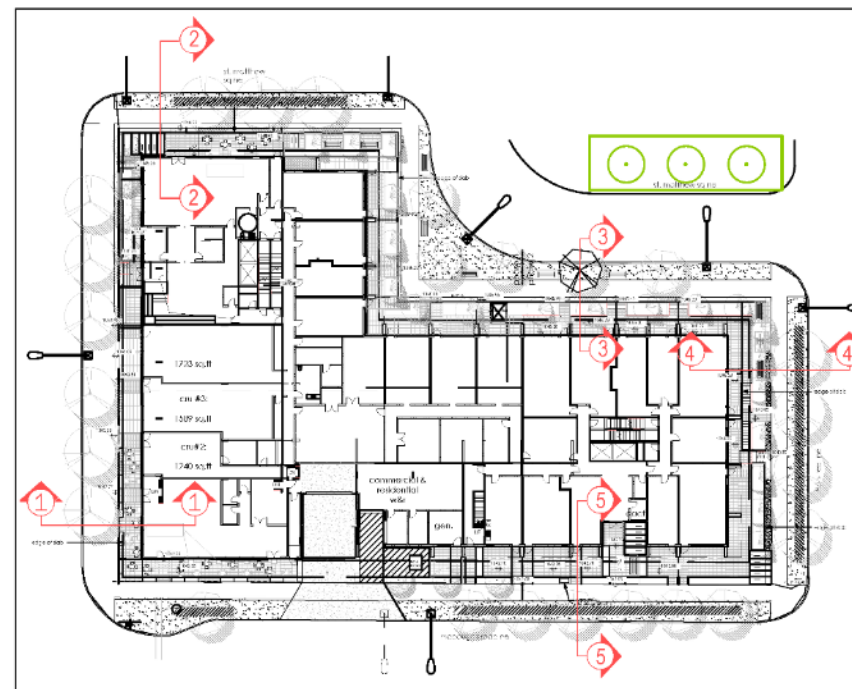
Proposed Development Permit Plans



SECTION # 4
1:50



SECTION # 5
1:50

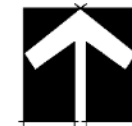


SECTION KEY PLAN

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NO.	DATE	REVISION DESCRIPTION	DR.
1		PRELIMINARY DESIGN	DT
2		REVISED LAYOUT AND ARCHITECTURE	TC
3		REVISED LAYOUT AND ARCHITECTURE	TC
4		REVISED LAYOUT AND ARCHITECTURE	TC
5		REVISED LAYOUT AND ARCHITECTURE	TC
6		REVISED LAYOUT AND ARCHITECTURE	TC
7		REVISED LAYOUT AND ARCHITECTURE	TC
8		REVISED LAYOUT AND ARCHITECTURE	TC
9		REVISED LAYOUT AND ARCHITECTURE	TC
10		REVISED LAYOUT AND ARCHITECTURE	TC

SEAL:
The Alberta Association of Landscape Architects
Meredith Mitchell

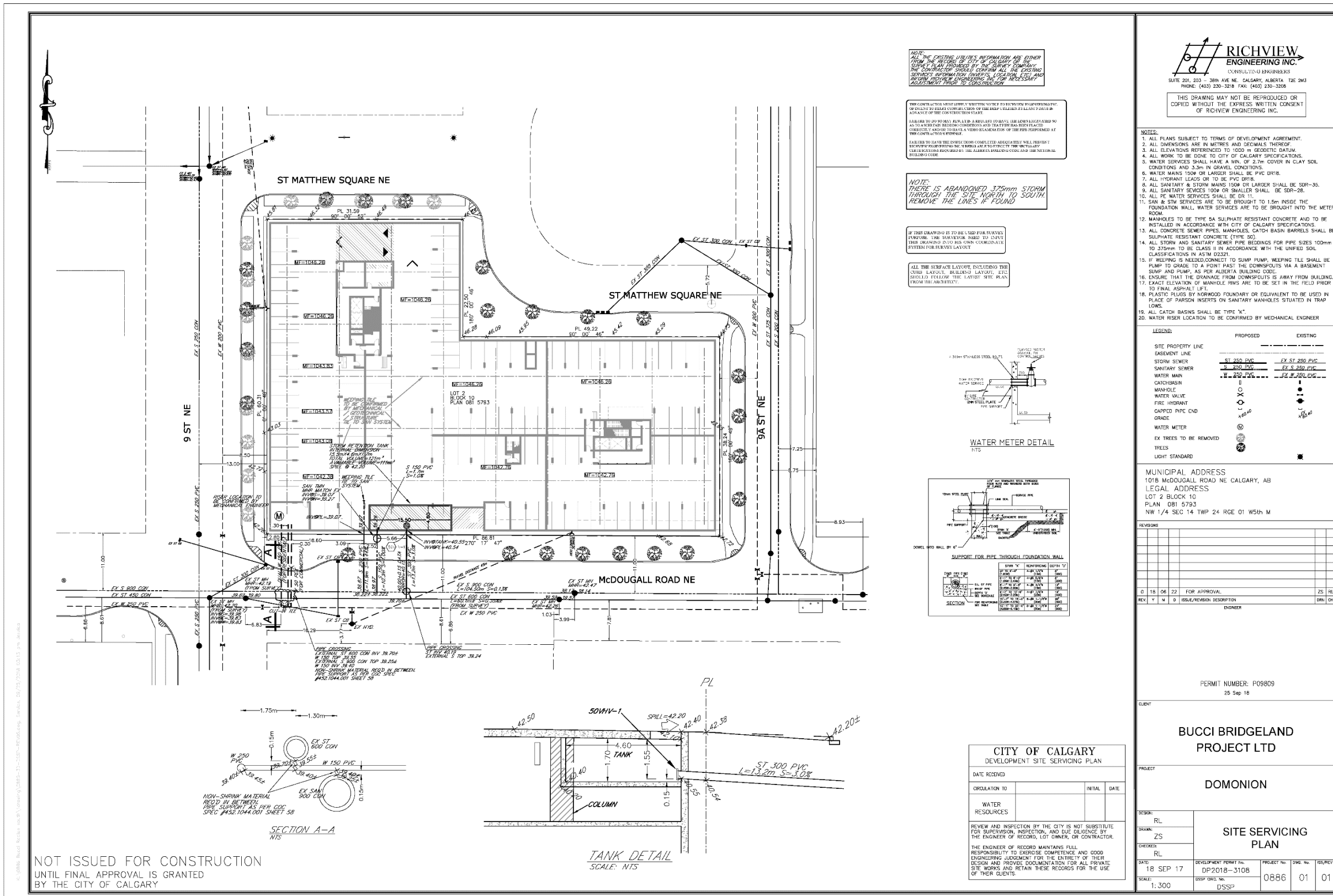
PROJECT:
THE DOMINION
MIXED-USE DEVELOPMENT
MC DOUGALL RD. NE AND 9TH ST. NE
CALGARY, AB

DRAWING TITLE:
PUBLIC/PRIVATE REALM SECTIONS

DATE: 07/14/18 DRAWING NUMBER:
SCALE: A30
DRAWN: TC
DESIGN: TC
CHKD: MLM
L-15B
OF 16

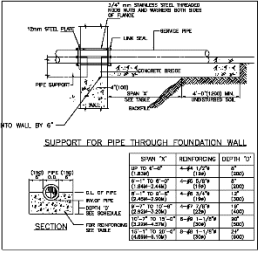
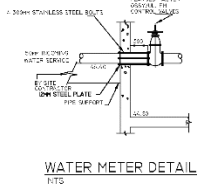
M2LA PROJECT NUMBER: JOB NO. 18-03

Proposed Development Permit Plans



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- NOTES:**
1. ALL PLANS SUBJECT TO TERMS OF DEVELOPMENT AGREEMENT.
 2. ALL DIMENSIONS ARE IN METRES AND DECIMALS THEREOF.
 3. ALL ELEVATIONS REFERENCED TO 1000 m GEODETIC DATUM.
 4. ALL WORK TO BE DONE TO CITY OF CALGARY SPECIFICATIONS.
 5. WATER SERVICES SHALL HAVE A MIN. OF 2.7m COVER IN CLAY SOIL CONDITIONS AND 3.3m IN GRAVEL CONDITIONS.
 6. WATER MAINS 1000 OR LARGER SHALL BE PVC DRIB.
 7. ALL HYDRANT LEADS OR TO BE PVC DRIB.
 8. ALL SANITARY & STORM MAINS 1500 OR LARGER SHALL BE SDR-35.
 9. ALL SANITARY SERVICES 1000 OR SMALLER SHALL BE SDR-26.
 10. ALL PE WATER SERVICES SHALL BE DR 11.
 11. SAN & STW SERVICES ARE TO BE BROUGHT TO 1.5m INSIDE THE FOUNDATION WALL. WATER SERVICES ARE TO BE BROUGHT INTO THE METER ROOM.
 12. MANHOLES TO BE TYPE SA SULPHATE RESISTANT CONCRETE AND TO BE INSTALLED IN ACCORDANCE WITH CITY OF CALGARY SPECIFICATIONS.
 13. ALL CONCRETE SEWER PIPES, MANHOLES, CATCH BASIN BARRELS SHALL BE SULPHATE RESISTANT CONCRETE (TYPE 80).
 14. ALL STORM AND SANITARY SEWER PIPE BEDDINGS FOR PIPE SIZES 100mm TO 375mm TO BE CLASS II IN ACCORDANCE WITH THE UNIFIED SOIL CLASSIFICATIONS IN ASTM D2221.
 15. IF WEEDING IS NEEDED, CONNECT TO SUMP PUMP. WEEDING TILE SHALL BE PUMP TO GRADE TO A POINT PAST THE DOWNSPOUTS OR A BASEMENT SUMP AND PUMP, AS PER ALBERTA BUILDING CODE.
 16. ENSURE THAT THE DRAINAGE FROM DOWNSPOUTS IS AWAY FROM BUILDING.
 17. EXACT ELEVATION OF MANHOLE RIMS ARE TO BE SET IN THE FIELD PRIOR TO FINAL ASPHALT LIFT.
 18. PLASTIC FLUES BY NORMCO FOUNDRY OR EQUIVALENT TO BE USED IN PLACE OF PARSON INSERTS ON SANITARY MANHOLES SITUATED IN TRAP LOWS.
 19. ALL CATCH BASINS SHALL BE TYPE "K".
 20. WATER RISER LOCATION TO BE CONFIRMED BY MECHANICAL ENGINEER.



LEGEND:

	PROPOSED	EXISTING
SITE PROPERTY LINE	---	---
EASEMENT LINE	---	---
STORM SEWER	ST 250 PVC	EX ST 250 PVC
SANITARY SEWER	SS 250 PVC	EX SS 250 PVC
WATER MAIN	W 250 PVC	EX W 250 PVC
CATCH BASIN	CB	CB
MANHOLE	M	M
WATER VALVE	WV	WV
FIRE HYDRANT	HY	HY
CAPPED PIPE END	CP	CP
GRADE	---	---
WATER METER	WTR	WTR
EX TREES TO BE REMOVED	---	---
TREES	---	---
LIGHT STANDARD	---	---

MUNICIPAL ADDRESS
1018 McDougall Road NE Calgary, AB

LEGAL ADDRESS
LOT 2 BLOCK 10
PLAN 081 5793
NW 1/4 SEC 14 TWP 24 RGE 01 W5th M

REV.	Y	M	D	ISSUE/REVISION DESCRIPTION	ENGR.
0	18	06	22	FOR APPROVAL	ZS RL

PERMIT NUMBER: P09809
25 Sep 18

BUCCI BRIDGELAND PROJECT LTD

DOMONION

CITY OF CALGARY
DEVELOPMENT SITE SERVICING PLAN

DATE RECEIVED	
CIRCULATION TO	INITIAL DATE
WATER RESOURCES	

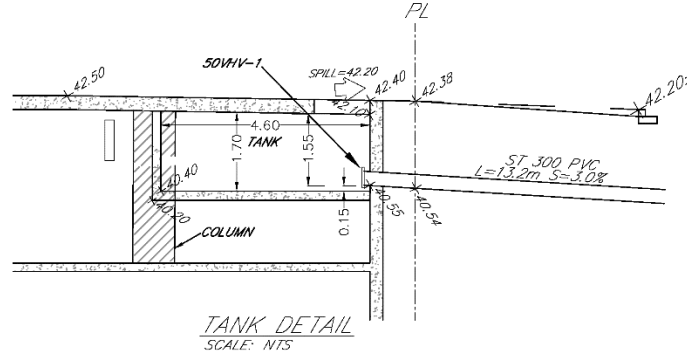
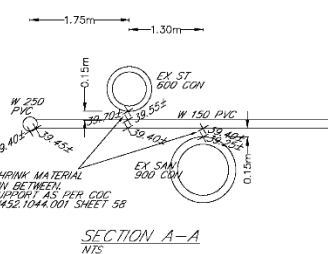
REVIEW AND INSPECTION BY THE CITY IS NOT SUBSTITUTE FOR SUPERVISION, INSPECTION, AND DUE DILIGENCE BY THE ENGINEER OF RECORD, LOT OWNER, OR CONTRACTOR.

THE ENGINEER OF RECORD MAINTAINS FULL RESPONSIBILITY TO EXERCISE COMPETENCE AND GOOD ENGINEERING JUDGMENT FOR THE ENTIRETY OF THEIR DESIGN AND PROVIDE DOCUMENTATION FOR ALL PRIVATE SITE WORKS AND RETAIN THESE RECORDS FOR THE USE OF THEIR CLIENTS.

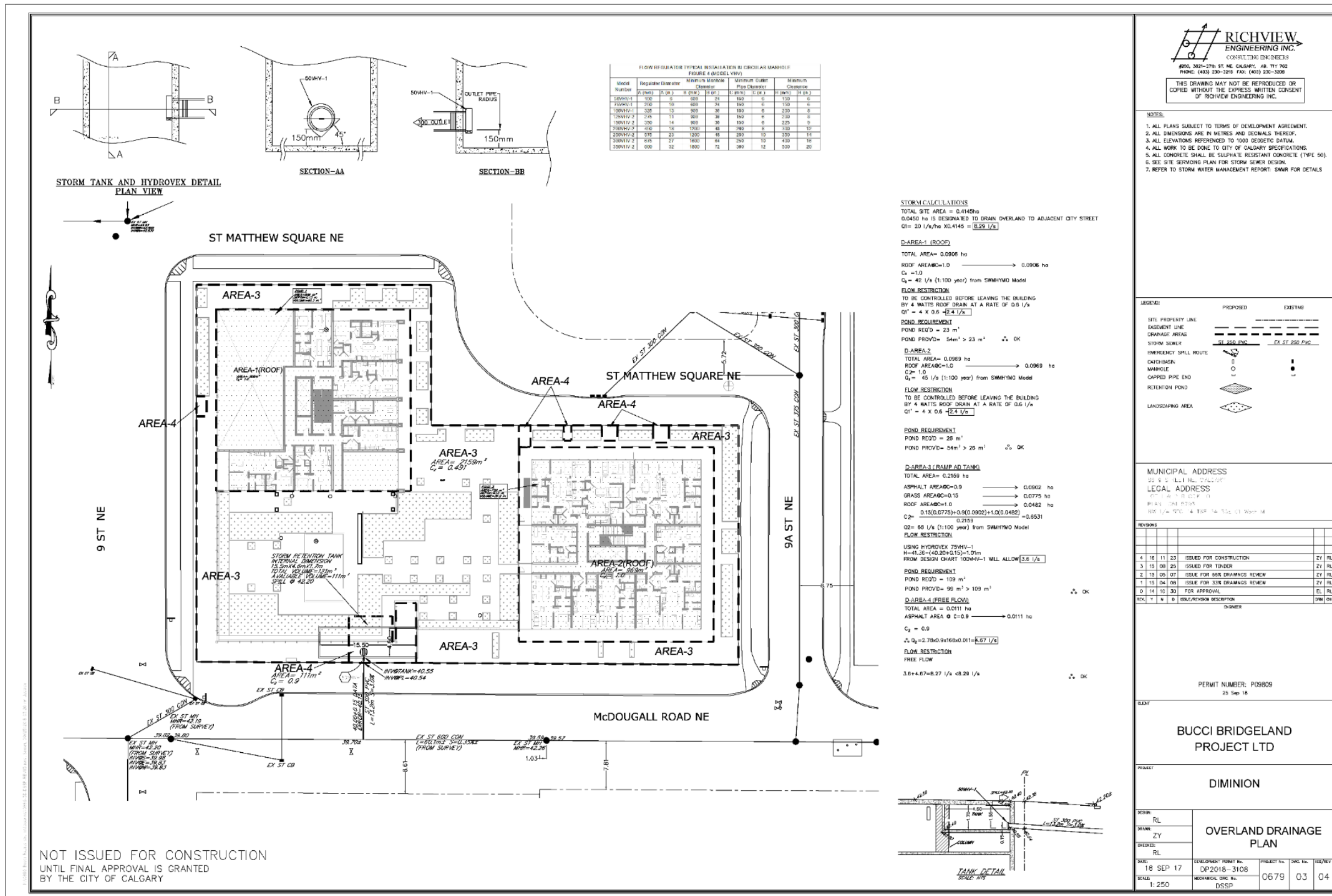
SITE SERVICING PLAN

DESIGNER	RL	DEVELOPMENT PERMIT No.	DP2018-3108	PROJECT No.	0886	DWG. No.	01	ISSUANCE	01
DRAWN	ZS								
CHECKED	RL								
DATE	18 SEP 17								
SCALE	1:300								

NOT ISSUED FOR CONSTRUCTION
UNTIL FINAL APPROVAL IS GRANTED
BY THE CITY OF CALGARY



Proposed Development Permit Plans



Conditions of Approval

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a cheque payable to the City of Calgary for a total of \$245.00 for the cost of one additional Development Completion Inspection that was not paid for at the time of the development permit submission.
2. Submit a letter from Enmax Corporation indicating that the following issues have been addressed. Please contact Rijad Tursunovic at rtursunovic@enmax.com or 403-514-1507 to resolve the issue.
 - Space for new transformer pad is insufficient.

Note: Enmax has been working with the applicant to resolve this conflict, as per correspondence provided by the application on 2018 October 1. A formal resolution letter is still required from Enmax

3. Submit a total of six (6) complete sets of Amended Plans (file folded and collated) to the Planning Generalist that comprehensively address the Prior To Release conditions of all Departments as specified below.
In order to expedite the review of the Amended Plans, please include the following in your submission:
 - a. Four (4) detailed written responses to the Conditions of Approval document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.

Please ensure that all plans affected by the revisions are amended accordingly.

4. For the roof top garden, amend plans to provide details as per Section 1346 of the Land Use Bylaw. Planting areas must have the following minimum soil depth:
 - (a) 0.6 metres for shrubs; and
 - (b) 0.3 metres for all other planting areas.

The soil depths must cover an area equal to the mature spread of the planting material.

Minimum depths for at grade landscaped areas are indicated on drawing L12. Depth for tree berms on rooftop is indicated on sheet L11, all other areas are not indicated.

5. On drawing L2, plant schedule indicate shrubs are POT and not minimum height of spread of 0.60 metres.

Amend plant schedule on drawing L2 to add a note to indicate proposed shrubs will be a minimum height or spread of 0.6 metres.

Conditions of Approval

6. Amend drawing A2.02, P2 parkade plan to indicate stalls 1-7 to be Visitor/Commercial stalls.
7. Amend drawing A2.03, P3 parkade plan to indicate stall 21 to be Visitor/Commercial stalls.
8. Amend the total proposed parking stall count in the "Commercial Area Information" table on A2.09 to reflect the additional commercial/visitor stall count (44 stalls) and to break up the parking requirements for Dwelling Units vs Live Work Units.
9. Amend the "Zoning Compliance Table" on A1.01B to reflect the changes to commercial/visitor parking stalls and parking requirements for Dwelling Units vs Live Work Units.

Development Engineering:

10. Submit three (3) sets of the Development Site Servicing Plan details to Development Servicing, Inspections and Permits, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact developmentservicing2@calgary.ca for additional details.

For further information, refer to the following:

Design Guidelines for Development Site Servicing Plans

http://www.calgary.ca/PDA/pd/Documents/urban_development/publications/DSSP2015.pdf

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/pd/Documents/development/development-site-servicing-plan.pdf>

11. The Applicant shall address the findings and recommendations in the following report: "Revised Final Report Phase II Environmental Site Assessment, 1018 McDougall Road NE, Calgary, Alberta" by Golder and Associates, dated March 22, 2016. All report(s) shall be prepared by a qualified professional and will be reviewed to the satisfaction of The City of Calgary (Environmental & Safety Management).

Transportation:

12. Amend the site plan to remove the north most bench in the triangular Right-of-way on St Matthew Square from landscape drawings.
13. All specialized boulevard treatments are to be submitted to materials and research for inspection and to meet safety requirements by that department for trips and falls and slip resistance. Provide details for review and approval.
14. Provide Easements, Covenants and restrictions for the proposed shared residential visitor and commercial parking stalls.

Conditions of Approval

15. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Construction of new driveway crossings on McDougall Road
 - b. Construction of new sidewalks adjacent to McDougall Road, 9A Street and St. Matthew Square
 - c. Construction of new wheelchair ramps
 - d. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel.
16. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Roads

- a. Street lighting upgrading adjacent to McDougall Road, 9 Street, 9A Street and St. Matthew Square.
17. Provide letter of acknowledgement that the application will contact the Traffic Engineer (403-268-4426) eight (8) weeks prior to occupancy to arrange for signage adjacent to the site to compliment operations.

This includes relocation of the on-street parking sign on the west side of 9 Street next to the wheelchair ramp at the northwest corner of McDougall Rd and 9 Street intersection. The parking sign needs to be moved 4-5m north of the existing location to provide better pedestrian sight lines for pedestrians crossing east-west. Signage will be invoiced at time of installation.

Parks:

18. Amend the Proposed Site Plan (A1.01), Site Plan (L1), and Planting Plan (L2) to show a continuous tree trench for the proposed boulevard trees along 9 Street NE, ensuring the trench extends a minimum of 2 metres past the tree grate to allow root growth away from the tree trunk. The trench design is to be approved by Calgary Roads.

Additionally, correct the annotation on Site Plan (L1) which refers to 'Sheet L-4' for the Tree Trench Detail; whereas, this detail is actually shown on Details (L11). Furthermore, ensure the Tree Trench Detail conforms to Roads' Standard (454.1001.039).

Conditions of Approval

Permanent Conditions

The following permanent conditions shall apply:

Planning:

19. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
20. No changes to the approved plans shall take place unless authorized by the Development Authority.
21. A Development Completion Permit shall be issued for each phase of the development; **before the use is commenced or the development occupied**. A Development Completion Permit is independent from the requirements of Building Permit occupancy. Call Development Inspection Services at 403-268-5311 to request a site inspection for the Development Completion Permit.
22. Upon completion of the main floor (storey) subfloor of the building proof of the geodetic elevation of the constructed main floor (storey) subfloor must be submitted to and approved by the Development Authority prior to any further construction proceeding. Email confirmation to geodetic.review@calgary.ca.
23. All areas of soft landscaping shall be provided with an underground sprinkler irrigation system as identified on the approved plans.
24. The walls, pillars and ceiling of the underground parkade shall be painted white or a comparable light colour.
25. The light fixtures in the parkade shall be positioned over the parking stalls (not the drive aisles).
26. All stairwell doors and elevator access areas shall be installed with a transparent panel for visibility.
27. Barrier free parking stalls shall be clearly designated, signed and located close to the elevators / exits with barrier-free accessibility.
28. All roof top mechanical equipment shall be screened as shown on the approved plans.
29. Loading and delivery shall take place in the designated loading stall as shown on the approved plans and shall, at no time, impede the safety of pedestrian movements and use of the parking lot.

Development Engineering:

30. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,

Conditions of Approval

- a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
31. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

32. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.
33. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
34. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.
35. The Fire Department connection (Siamese) location must face the street or access route, and not be blocked by columns, planters, bicycle racks, or landscaping etc.

Conditions of Approval

Transportation:

36. The development site is located within a residential parking zone “AAA” as defined by the City of Calgary Traffic By-law and, as such, no long term non-residential parking is permitted on-street. No residential parking permits shall be supplied to this development.
37. The approved driveway(s) required for this development must be constructed to the ramp grades as shown on the approved Development Permit plans. Negative sloping of the driveway within the City boulevard is not acceptable. If actual grades do not match the approved grades, the developer/owner shall be responsible for all costs to remove and reconstruct the entire driveway ramp in accordance with approved grades.
38. The developer shall be responsible for the cost of public work and any damage during construction in City road right-of-ways, as required by the Manager, Transportation Planning. All work performed on public property shall be done in accordance with City standards.
39. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the applicant's expense, upon completion of the foundation. Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.

Parks:

40. The submitted plans indicate that the removal of existing public trees along property frontage is necessary. As per the City of Calgary Tree Protection Bylaw, a letter of authorization to remove public trees is required from Parks Urban Forestry. The applicant is to contact Urban Forestry at 311 to make arrangements for the letter and compensation.
41. In order to ensure the integrity of existing public trees and roots, construction access is only permitted through the rear lane and outside the drip line of public trees, as per the approved Tree Protection Plan.
42. Public trees located on the boulevard adjacent to the development site shall be retained and protected during all phases of construction by installing a temporary fence around the extent of the branches ('drip line') and ensuring no construction materials are stored inside this fence unless otherwise authorised by Urban Forestry.
43. Install protective fence 3 metres from trunks of the existing public trees north of the subject site, along the south side of St. Matthew Square, and water once per week during construction. Contact Urban Forestry at 311 to arrange for Tree Protection Plan approval.
44. In order to ensure the integrity of existing public trees and roots, no grade changes are permitted in the boulevard within drip lines.

Conditions of Approval

45. In order to ensure the integrity of existing public trees and roots, there shall be a minimum 3 metre separation, ideally the full length of the canopy, between the trunk and any new/proposed structures, (i.e. driveways and walkways).
46. Tree protection information given as per the approved development permit does not constitute Tree Protection Plan approval. Tree Protection Plan approval must be obtained separately through Urban Forestry. Visit www.calgary.ca or call 311 for more information.
47. Applicant should indicate on the plans the location of new services in accordance with the Grade Slip or DSSP approved by the City. However, if the servicing trench will be located within the drip line of an existing adjacent public tree, the applicant should contact Urban Forestry through 311 or contact Development Site Servicing directly in attempt to avoid this conflict.
48. Any tree planting in the City boulevard shall be performed and inspected in accordance with Parks' *Development Guidelines and Standard Specifications – Landscape Construction (current edition)*. Applicant is to contact the Parks Development Inspector (403) 268-5325 to arrange an inspection.

Urban Design Review Panel Comments (PE2018-00310)

Date:	March 28, 2018	
Time:	2:45 pm	
Panel Members:	Present:	Absent:
	Janice Liebe	Chad Russill (chair)
	Chris Hardwicke	Bruce Nelligan
	Glen Pardoe	Gary Mundy
	Robert Leblond	Jack Vanstone
	Terry Klassen	Yogeshwar Navagrah
		Eric Toker
Advisor:	David Down, Chief Urban Designer	
Application number:	PE2018-00310	
Municipal address:	1018 McDougall Rd NE	
Community:	Bridgeland/Riverside	
Project description:	Radius II – mixed use development	
Review:	first	
File Manager:	Christine Leung	
City Wide Urban Design:	Lothar Wiwjorra	
Applicant:	Casola Koppe	
Architect:	Casola Koppe	
Owner:		
Ranking:	Endorse with comment	

Summary

The proposed project is a mixed-use midrise project that is comprised of two towers joined by a low podium. The Panel expressed support for the general urban strategy of the project including breaking the mass into two towers to provide a variegated elevation rather than a single slab. Recognizing that the project has no “back” as it is surrounded by parks or public streets on all sides the Panel supports the attempt to activate all frontages with commercial uses or live-work townhouses at grade. The Panel’s comments and concerns focused primarily on the challenges of providing active uses given the steep grades and the treatment of the servicing area. The landscape design is yet to be fully developed. In particular the boulevard condition in front of the retail units is currently shown as a grass boulevard typical of residential frontage. Opportunities to incorporate LID, resolution of universal accessibility and the specific detail of the stepped terraces and proposed retaining walls should be explored in future explorations. Similarly, the landscape in front of the townhouses is undeveloped showing only a simple walkway to the front door. Patio space, public/private separation and separation between units should be explored. The patio/playground courtyard on the north side of the project presents public/private interface issues and is also undeveloped. The edge condition along the blank wall to the south of the bike parking room does not show design mitigations to screen or articulate the blank wall. Finally, the applicant noted that the current design of the tower facades with a full building screen in front of deep balconies will likely change due to energy considerations and that future submissions may be different in aesthetic. For these reasons and others noted below the Panel endorses the project with comment.

Missing or incomplete information provided:

The applicant did not provide all the information required for a UDRP submission. Streetscape elevations, materials board, a written urban design response, and lighting design were missing. Although landscape drawings for at grade, patio levels and rooftop were provided they did not provide sufficient detail for evaluation. Some of the renderings in the document were not labelled as precedent images and thus did not represent the design. The applicant is asked to obtain the requirements from the assigned planner and to provide all missing information at the next meeting.

Urban Design Review Panel Comments (PE2018-00310)

Applicant Response

July 3, 2018

The proposed mixed-use development has been submitted for development permit and is currently showing that it will provide 347 residential units and approximately 9000 s.f. of commercial space to the site.

The proposed commercial retail frontage along 9th Street SE and western end of McDougall Road SE is a fundamentally important urban design element and is critical to the community, given that it can potentially provide the biggest public realm contribution to the immediate area. The shops cafes and restaurants will provide much needed pedestrian activation of this important location. The 9th Street frontage will consist of a conventional 3 zone public streetscape:

1. On site (setback area) uses such as outdoor seating, dining and merchandising. This area will be defined by a contrasting paving pattern and bordered in some areas by moveable fencing and landscaped planters (where licensed patios will be anticipated).
2. The main pedestrian path (uninterrupted city sidewalk).
3. Boulevard strip between city sidewalk and street curb. This area will provide space for public benches, trees, street and sidewalk lighting, waste and recycling receptacles and bicycle parking. This area will ensure that the city sidewalk remains unobstructed and supported at the same time.

The McDougall Road SE edge will start at its western end with the last portion of commercial frontage (it is anticipated that this will provide the greatest opportunity for a restaurant or pub patio given its solar exposure and potential to activate the 9 Street / McDougall Road SE corner. Moving eastward along McDougall Road SE, the parkade entrance, loading and waste/recycling and transformer will be contained to as small a frontage as possible to minimize its impact on the street. Landscape type screening will be relied upon to minimize its visual impact from the street. Beyond these important building service entry points the podium transitions to a grade oriented residential frontage that will incorporate private outdoor garden entrances defined by landscape and low fencing to effectively define the public/private edge. This type of interface will continue for the majority of the podium perimeter being interrupted only by the south tower lobby entrance that will have a large extending canopy highlighting its location from the street. The 9A Street edge, which has 2 residential addresses, will negotiate the steep grade change from McDougall to St. Matthew Square. Large landscaped planters will be employed to create the appropriate screening of the P1 foundation and help to make the grade transition along this challenging edge.

The St. Matthew Square edge will be lined with more street oriented residential entries that will then turn into the site to create the north courtyard which will provide access to the internal units of the podium. This courtyard will be treated as a semi-private garden that provides access to the residential units.

The north tower portion of the podium will provide access to the enlarged amenity lobby (labeled as the "We Work" lobby) that is envisioned as an opportunity for the residents of the site and surrounding community to have "as needed" collaborative office/meeting space available in a "hotel lobby" type setting. Spaces can be booked and paid for on demand with E-technology similar to that used for car sharing programs. The street entrance for this space will be entered from the St. Matthew Square / 9th Street corner. The north lobby private residential lobby entrance will be provided from a front stair off of 9th Street.

Casola Kopp Architects Ltd.

Urban Design Review Panel Comments (PE2018-00310)

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	Support
	UDRP Commentary		
	The project achieves retail street diversity by supplying retail units along the principal façade.		
	Applicant Response		
Greater development of the challenging grade and expression of the intended use of the retail streetscape space has been presented in the plans and images put forward.			
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	Support
	UDRP Commentary		
	The project appears to maximize glazing in the façade of the retail units.		
	Applicant Response		
Two storey tall transparent glazing dominates the retail frontage along 9th Street SE to maximize the visual connection from within the retail units looking out to the street and allows the best sight lines possible into the commercial units from the street.			
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	Further review recommended
	UDRP Commentary		
	The project meets the goals of pedestrian-first design although street furnishings are not shown on the plans.		
	Applicant response		
Further development of the streetscape defines 3 different zones within this critical frontage. The first 3m strip (setback area) of the property will be reserved for: CRU and residential lobby entry points, flexible outdoor seating and tables, moveable planters and low decorative fencing, potential outdoor merchandising or spill out of retail shop wares, and planters to negotiate sharp grade changes. The next zone will of course be the 2.0m wide city sidewalk which will remain uninterrupted and will be supported by 3rd zone: the boulevard strip between the street curb and sidewalk which will contain trees, public bench seating, bicycle parking, lamp standards, bollard lighting, waste/recycling receptacles.			
4	Entry definition / legibility	Entry points are clear and legible	Further review recommended
	UDRP Commentary		
	The documentation does not depict the entry points in either elevation or rendering.		
	Applicant Response		
Entry points to each CRU, residential lobby, and grade oriented unit are now confirmed and represented on the site plan, landscape plan, and elevations.			
5	Residential multi-level units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Further review recommended
	UDRP Commentary		
	Residential units are provided at grade. Private outdoor patios are not shown in the plans as the landscape design is undeveloped.		
	Applicant Response		
Further development of the grade oriented residential unit interface expresses the well-defined private outdoor garden entry points for each unit.			

Urban Design Review Panel Comments (PE2018-00310)

6	At grade parking	At grade parking is concealed behind building frontages along public streets.	NA
UDRP Commentary			
Parking is supplied underground.			
Applicant Response			
All of the parking (commercial/visitor/residential) is contained within the common parkade. Retail customers will be able to park on the first level of the parkade only and access the street front through a dedicated entrance beside the parkade driveway.			
7	Parking entrances	Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.	Support with comment.
UDRP Commentary			
Parking ramps are concealed and minimized. The effect of the blank wall along the bike storage façade should be mitigated.			
Applicant Response			
The parkade entrance has been recessed and located at a transitional point between commercial frontage and residential grade oriented units.			
8	Other		
Applicant Response			
Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i>			
Topic		Best Practice	Ranking
9	LRT station connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	NA
UDRP Commentary			
NA			
Applicant Response			
Proposed development will increase ridership of LRT. Development seeks to provide 0.65 stalls per residential unit and promote alternative modes of transport (car sharing, cycling, walking, transit)			
10	Regional pathway connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	NA
UDRP Commentary			
NA			
Applicant Response			
Proposed development will activated existing pedestrian connections making them more desirable to use.			
11	Cycle path connections	Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.	NA
UDRP Commentary			
NA			
Applicant Response			
Proposed development will attempt to provide at least one class one stall per unit within the first level of the building to promote cycling as an alternative mode of transport.			
12	Walkability - connection to adjacent neighbourhoods	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	NA

Urban Design Review Panel Comments (PE2018-00310)

	/ districts / key urban features		
	UDRP Commentary		
	NA		
	Applicant Response		
	Proposed development will activate existing pedestrian connections making them more desirable to use.		
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.	NA
	UDRP Commentary		
	NA		
	Applicant Response		
	Proposed development will activate existing pedestrian connections making them more desirable to use.		
14	Open space networks and park systems	Connects and extends existing systems and patterns.	NA
	UDRP Commentary		
	NA		
	Applicant Response		
	Proposed development will activate and support adjacent existing open spaces and parks to promote their use.		
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.	Support
	UDRP Commentary		
	The project massing and orientation appears to maximize views.		
	Applicant Response		
	Proposed towers are offset in an attempt to maximize views to south and west towards, skyline and mountains.		
16	Vehicular interface		NA
	UDRP Commentary		
	NA		
	Applicant Response		
	Although all of the required parking will be in the building, the 9th Street SE edge will provide some short term on street parallel parking which is already currently in place.		
17	Other		
	Applicant Response		
	Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i>		
	Topic	Best Practice	Ranking
18	Massing relationship to context	Relationship to adjacent properties is sympathetic	Support
	UDRP Commentary		
	Massing is sympathetic and improves on previous responses in the surrounding development.		
	Applicant Response		
	Two tower concept breaks up what would otherwise be a solid 8 to 10 storey full block massing.		
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites	Further review recommended
	UDRP Commentary		
	<i>No shadow studies were included in the package.</i>		
	Applicant Response		

Urban Design Review Panel Comments (PE2018-00310)

	Shadow studies have been included with the development permit application. Shadows cast will mostly affect the St. Matthew's park to the north east of the site. Ultimately, the surface area of shadow of the two tower massing will be close to the area of shadow cast by an uninterrupted solid 10 storey block massing.		
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
	UDRP Commentary		
	The project is successful in relating built-form to the surrounding streets.		
	Applicant Response		
	Proposed building engages the street edges at all possible opportunities.		
21	Massing distribution on site		Support
	UDRP Commentary		
	The massing is sensitively distributed on the site and breaks down built-form bulk into two distinct and complimentary towers.		
	Applicant Response		
	Two tower concept breaks up what would otherwise be a solid 8 to 10 storey full block massing.		
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Further review recommended
	UDRP Commentary		
	Although the towers do not step back from the street they have minimal street lengths and are joined by a one-story podium that minimizes the effect of a continuous slab alternative. The current resolution of the south west corner could use further study to create a more human scale at street level. The west retail face is flat to the street providing no cover or relief for pedestrians from the elements or cover at entry points.		
	Applicant Response		
	Entry points to residential tower lobbies have been provided large canopies to define these important locations and provide some cover for pedestrians. Outdoor seating, and dining areas will most likely incorporate free-standing trellis/pergola structures, and retractable umbrellas to provide some measure of solar cover during high exposure times of the year and day.		
23	Other		
	Applicant Response		
	Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i>		
	Topic	Best Practice	Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Support with comment
	UDRP Commentary		
	The north courtyard might present some view line issues.		
	Applicant Response		
	The north courtyard is now essentially a semi-private landscaped courtyard that will provide access to the half dozen residential units that front it. These units will provide the necessary passive visual surveillance of the area to make it a safe place to inhabit.		
25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	Further review recommended
	UDRP Commentary		
	No wind analysis or mitigation strategy was presented.		

Urban Design Review Panel Comments (PE2018-00310)

	Applicant Response		
	Landscaping will be utilized to mitigate and anticipated high wind areas.		
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.	Further review recommended
	UDRP Commentary		
	No snow drifting analysis or mitigation strategy was presented.		
	Applicant Response		
	Streetscape frontage will be sufficiently flat, smooth and uninterrupted to allow for efficient snow removal by shovel or motorized brush. Snow storage will be provided by the boulevard between the sidewalk and street curb.		
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.	Further review recommended
	UDRP Commentary		
	Not enough information in the package to comment.		
	Applicant Response		
	Entry points to residential tower lobbies have been provided large canopies to define these important locations and provide some cover for pedestrians.		
28	Night time design		Further review recommended
	UDRP Commentary		
	No lighting designs were included in the package.		
	Applicant Response		
	Lighting and landscape plans have been provided to show lighting types and quantities at the perimeter of the site and at the outdoor amenity space on the podium roof. A mix of bollard and building mounted down lighting will be utilized to provide a comfortable lighting level while avoiding and offsite spillage to neighbouring residences.		
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.	Further review recommended
	UDRP Commentary		
	<i>Slopes were not indicated but grades along the east and west of the project might present accessibility challenges. More information required to assess.</i>		
	Applicant Response		
	Special attention has been given to resolve grades along the 9th Street frontage to ensure all commercial and residential lobby entry points are barrier free and useable for outdoor seating and dining.		
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.	Further review recommended
	UDRP Commentary		
	No shadow studies were presented.		
	Applicant Response		
	Shadow studies have been included with the development permit application. The west and south faces of the commercial podium will provide the greatest opportunities for warm sunshine to make outdoor use desirable.		
31	Other		
	Applicant Response		
	Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i>		

Urban Design Review Panel Comments (PE2018-00310)

Topic	Commentary	Ranking
32	(specify) N/A	TBD
	Applicant Response	
	The parkade access driveway area attempts to consolidate the main service and utility functions of the development. The transformer, loading zone, waste and recycling all define a point of transition from the commercial retail frontage of 9th and McDougall and the street oriented residential entrances further east along McDougall. Other than these important elements, it is anticipated that there will be no other intrusive or unsightly building services that will interrupt the streetscape.	

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

Date: July 11, 2018
Time: 3:00 pm
Panel Members:

Present:	Absent:
Chad Russill (co-chair)	Janice Liebe (chair)
Amelia Hollingshurst	Glen Pardoe
Ben Bailey	Robert LeBlond
Yogeshwar Navagrah	Terry Klassen
Chris Hardwicke	Bruce Nelligan
	Jack Vanstone
	Gary Mundy
	Eric Toker

Advisor: David Down, Chief Urban Designer
Application number: LOC2018-0059/DP2018-3108
Municipal address: 1018 McDougall Rd NE
Community: Bridgeland
Project description: New: Multi-Residential Development, Retail and Consumer Service (2 buildings, 2 phases)
Review: Second (PE2018-00310)
File Manager: Christine Leung
City Wide Urban Design: Lothar Wiwjorra
Applicant: Casola Koppe
Architect: Casola Koppe
Owner: Bucci
Ranking: Endorse

Summary

July 11, 2018

The proposed project was previously presented to UDRP in March 2018. The panel noted that the new package did a strong job describing the project and the applicant has addressed the majority of the comments from the previous submission. Panel members stated that they were excited by the well-considered project that would bring inspired architectural diversity to the neighbourhood. The panel appreciated the exterior expression of the building but expressed some concern about the quality of the clear fritted glass screens as they will predominantly define the image of the building. The success of the project's thesis will largely hinge on the execution of this details.

The panel felt that weather protection along the street edge could be investigated further. The panel suggested that the residential entries are not as developed as other building elements and suggested highlighting the residential entrances by extending the tower language as a consideration. Ground floor residential entrances at grade present some grade issues along the north and east, as openly discussed with the applicant.

The applicant presented a recent sketch (not originally in the submission package) that explored moving the amenity area inside. The panel supported this move in principle as it resolves the inside corner security issues and provides a continuous streetwall condition.

The panel discussed the large footprint of the tower elements that visually appear larger due to the wrap-around balconies but appreciated the massing move of dividing the bulk of the building into two towers rather than a continuous slab.

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

Previous Comments (PE2018-00310)

March 28, 2018

The proposed project is a mixed-use midrise project that is comprised of two towers joined by a low podium. The Panel expressed support for the general urban strategy of the project including breaking the mass into two towers to provide a variegated elevation rather than a single slab. Recognizing that the project has no “back” as it is surrounded by parks or public streets on all sides the Panel supports the attempt to activate all frontages with commercial uses or live-work townhouses at grade. The Panel’s comments and concerns focused primarily on the challenges of providing active uses given the steep grades and the treatment of the servicing area. The landscape design is yet to be fully developed. In particular the boulevard condition in front of the retail units is currently shown as a grass boulevard typical of residential frontage. Opportunities to incorporate LID, resolution of universal accessibility and the specific detail of the stepped terraces and proposed retaining walls should be explored in future explorations. Similarly, the landscape in front of the townhouses is undeveloped showing only a simple walkway to the front door. Patio space, public/private separation and separation between units should be explored. The patio/playground courtyard on the north side of the project presents public/private interface issues and is also undeveloped. The edge condition along the blank wall to the south of the bike parking room does not show design mitigations to screen or articulate the blank wall. Finally, the applicant noted that the current design of the tower facades with a full building screen in front of deep balconies will likely change due to energy considerations and that future submissions may be different in aesthetic. For these reasons and others noted below the Panel endorses the project with comment.

Applicant Response

September 18, 2018

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	Support
	UDRP Commentary		
	The proposal successfully provides retail street diversity on a challenging site.		
	Applicant Response		
	In an effort to provide opportunities for public gathering, we have proposed several new areas around the site for benches and landscape enhancement. These are located both on site (see areas within the north setback facing St. Matthew’s Square and offsite also facing St. Matthew Square Park within the enlarged boulevard area at the curve of St. Matthew. Along 9 Street, it is anticipate that the setback areas fronting the retail spaces will provide opportunities for a wide range of seating possibilities such as bench/planting combinations for informal short term seating (similar to a an Ice cream shop scenario) or a more formal chairs and tables type seating in front of CRU’s that are expected to attract restaurant or licensed establishments. We believe it is merely important to provide enough room, proper grading and visual security to allow these types of outdoor gathering spaces to evolve and adapt to the uses and micro-climates that will define them over time.		
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	Support
	UDRP Commentary		
	Project maximizes retail glazing and transparency.		
	Applicant Response		
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop	Further review recommended


Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

		offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	
	UDRP Commentary		
	The applicant has attempted to mitigate slope issues to provide reasonable accessibility given its challenging site. The pedestrian surface treatment does not appear to span the vehicular entry to the parkade. Some difficult areas still exist and require further study to resolve fully.		
	Applicant response		
	Although continuous pedestrian surfaces are desirable, the change in material for the main access drive surface (complete with standard City of Calgary concrete apron) will provide a visual queue for pedestrians that vehicles may be present. Private patios and onsite commercial setback frontage areas are paved in contrasting pavers to provide a subtle definition of the pedestrian walk and areas of seating and/or layby potential.		
4	Entry definition / legibility	Entry points are clear and legible	Further review recommended
	UDRP Commentary		
	Residential entries are not as developed as other building elements; review of these entries is suggested to strengthen concept further. A possible expression may be to highlight entrances by extending the tower language or other avenues to differentiate these entries nodes from the retail.		
	Applicant Response		
	Although the entry canopies seem undeveloped, the intent here is to execute a simple slab type canopy form that both highlights the entry point while not competing with the simplicity of the rest of the design. A minimalist composite white metal panel clad canopy that appears to pierce the glass wall is the general appearance we are after. This canopy treatment will be repeated at the south tower entrance as well as that of the north tower on 9th Street NE.		
5	Residential multi-level units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Further review recommended
	UDRP Commentary		
	The proposal incorporates residential units at grade. The Panel encourages the applicant to consider the private/public realm transition, incorporating thoughtful buffering in the form of gating and/or landscaping features to promote usable private amenity space at grade.		
	Applicant Response		
	Grade oriented unit entrance areas are defined with careful placement of planters with both low lying vegetation and shading tree elements that define the private/public edge and provide (with mature growth) a bit of a green canopy to each entry. A low pedestrian gate provides secured access for resident's private outdoor amenity. All patio surfaces for these amenities will be finished in modular permeable concrete pavers to provide better drainage and residential scale.		
6	At grade parking	At grade parking is concealed behind building frontages along public streets.	NA
	UDRP Commentary		
	NA		
	Applicant Response		
7	Parking entrances	Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.	Support
	UDRP Commentary		
	The proposal appears to have minimized parking entrances as best as possible. See item #3.		
	Applicant Response		
8	Other		
	Applicant Response		

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i>		
Topic	Best Practice	Ranking
9	LRT station connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.
	UDRP Commentary	
	The Design includes a clear north-south pathway connection to nearby LRT station and activates with retail uses at grade.	
	Applicant Response	
	Inclusion of commercial uses will provide greater activation of 9th Street NE and thus provide greater natural security for this important north/south pedestrian route throughout the day.	
10	Regional pathway connections	Supports walkability via intentional urban design connections to pathway systems.
	UDRP Commentary	
	Applicant Response	
11	Cycle path connections	Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.
	UDRP Commentary	
	Outdoor public bicycle storage at grade has been provided.	
	Applicant Response	
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.
	UDRP Commentary	
	The proposal includes continuous sidewalks along all four sides and is mindful of the grade changes as it relates to adjacent uses.	
	Applicant Response	
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.
	UDRP Commentary	
	Due to the site size, pathways through the site are not deemed necessary to support successful urban design for this project given strength of public realm interface.	
	Applicant Response	
14	Open space networks and park systems	Connects and extends existing systems and patterns.
	UDRP Commentary	
	Retail/commercial facing 9 th Street and residential facing St. Matthew Sq. and McDougall Road support the current use patterns of adjacent sites.	
	Applicant Response	
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

UDRP Commentary		
The two-tower massing improves views to the city. The design also brings an inspired architectural diversity to the neighbourhood, enhancing the local streetscape.		
Applicant Response		
16	Vehicular interface	NA
UDRP Commentary		
Applicant Response		
17	Other	
Applicant Response		
Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i>		
Topic	Best Practice	Ranking
18	Massing relationship to context	Relationship to adjacent properties is sympathetic
		Further review recommended
UDRP Commentary		
Although visually dense the two-tower design breaks up the massing of the building. Continued study of the clear fritted glass screens is recommended as they will predominantly define the image of the building.		
Applicant Response		
During the past couple of months, the project design/engineering team has been exploring different strategies on executing the aesthetic look of the “patterned screen” that is the cornerstone of the application’s identity. Until a few weeks ago, we were leaning towards an “all glass” exterior skin. However, upon rendering under different lighting conditions, it became clear that the behavior of light/glare on the glass surface was going to compromise the reading of the vertical change of the pattern. Therefore, we have decided to move to an all composite metal panel solution that better matches the original renderings and strikes a better balance of constructability and aesthetics. See the latest rendering below:		
		
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites
		Support
UDRP Commentary		

Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

	The two-tower massing somewhat minimizes shadow impact on adjacent sidewalks.		
	Applicant Response		
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
	UDRP Commentary		
	The proposal successfully addresses adjacent streets with grade-related uses and form.		
	Applicant Response		
21	Massing distribution on site		Support
	UDRP Commentary		
	Although visually dense the two-tower massing breaks up the massing of the building. The staggered placement of the towers maximizes the separation between the towers and views.		
	Applicant Response		
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Further review recommended
	UDRP Commentary		
	The panel commends the clean lines and architectural expression of the glass box podium though weather protection along the street edge could be investigated further.		
	Applicant Response		
	As previously mentioned, we believe that commercial establishments should be allowed the ability to provide their own seating and canopy solutions. The glass podium provides a common backdrop for any/all solutions to occur. (ie. trellis, umbrella, private planter trees etc.) Furthermore it is our experience that built-in type free draining architectural canopies pose their own challenges during icy/snowy freeze thaw conditions.		
23	Other		
	Applicant Response		
Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i>			
	Topic	Best Practice	Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Further review recommended
	UDRP Commentary		
	Moving the amenity area inside would resolve the inside corner security issues and provide a continuous streetwall condition. This option should be further studied as it was generally supported as shown, though design was not completely resolved at time of review.		
	Applicant Response		
	The north "inset" court has been deleted and the north building edge now engages St. Matthew continuously with additional grade oriented residential units.		
25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	NA
	UDRP Commentary		
	Not reviewed.		
	Applicant Response		



Urban Design Review Panel Comments (LOC2018-3108 / DP2018-3108)

	The towers have significant articulation (continuous deep screen/balcony) that should prevent wind shear or down drafts to occur.	
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.
	UDRP Commentary	
	Not reviewed.	
	Applicant Response	
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.
	Further review recommended	
	UDRP Commentary	
	The panel recommends weather protection along the street edge could be investigated further.	
	Applicant Response	
	Majority of daily traffic in and out of the building will be through the tower lobby entrances. They are treated with their own special canopies. See note 22 for commercial retail units.	
28	Night time design	
	Support	
	UDRP Commentary	
	The proposal provides active uses along its retail frontage and passive surveillance opportunities along its residential edges.	
	Applicant Response	
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.
	Further review recommended	
	UDRP Commentary	
	The proposal appears to have attempted to mitigate slopes on a difficult site. The main residential entries provide ramps and there are some residential units with at grade barrier free entries.	
	Applicant Response	
	Barrier free access into and around the public areas of the building are code governed and conformed to throughout. Private residential units are barrier free accessible from within the building corridors if not from the street.	
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.
	NA	
	UDRP Commentary	
	Not reviewed.	
	Applicant Response	
31	Other	
	Applicant Response	
Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i>		
Topic	Commentary	Ranking
32	(specify)	

Applicant Led Engagement Summary

Dominion – Engagement Overview

The Community of Bridgeland-Riverside
LOC2018-0059 & DP2018-3108

<u>Summary of Engagement Events:</u>		
	Date	Event / Meeting
1	Feb. 6, 2018	Meeting held at City Hall with senior planning officials to discuss opportunities for the site. Present at the meeting was: Wallace Leung, Rollin Stanley, Lynn McKeown, Mike Bucci, Troy Abromaitis and Tony Casola
2	Feb. 20, 2018	Meeting held with Bridgeland-Riverside Community Association Board (BRCA) at the Bridgeland-Riverside Community Hall The majority of the board was present and Ali McMillan chaired the meeting.
3	Feb. 28, 2018	First Public Open House at Bridgeland_Riverside Community Hall Approximately 17 groups attended and 9 comment cards were collected. 
4	May 22, 2018	Second Public Open House at Bridgeland-Riverside Community Hall Approximately 50 groups attended and 31 comment cards were collected. 
5	July 18, 2018	Meeting held with Bridgeland-Riverside Community Association Board (BRCA) and approximately 3 invited non-board member community

Applicant Led Engagement Summary

		residents. This meeting was held at the Bridgeland-Riverside Community Hall.
6	September 24, 2018	An information session was held at the Bridgeland-Riverside Community Hall to provide updates on changes to the project. Comments and questions were directed to Planning officials and to Bucci's project website . Approximately 43 individuals attended the session.

Summary of <u>What was Heard</u> at Engagement Events:		
	Date	What was heard/discussed
1	Feb. 6, 2018	<p>At the meeting held with senior planning officials, initial thoughts on the potential for the site were shared. Some of the subjects discussed included:</p> <ol style="list-style-type: none"> 1. Existing land use and challenges with achieving maximum allowable density. 2. Initial concept diagrams and images were shared and distributed showing alternatives in massing and site development. 3. Desire of the applicant to get to construction by February of 2019. 4. Timelines were discussed specifically related to how the proposed site-specific land-use and the already contemplated Bridgeland-Riverside ARP amendment would need to align/coincide. 5. As an initial meeting it set very preliminary expectations and timelines to provide a guide to future conceptual development.
2	Feb. 20, 2018	<p>The first meeting held with Bridgeland-Riverside Community Association Board (BRCA)</p> <p>This meeting was intended as an introduction to the initial concepts and a chance for the applicant to hear initial thoughts and concerns from the members of the board. Some of the subjects discussed included:</p> <ol style="list-style-type: none"> 1. Support for the architectural expression of the initial images but concern about the ability to feasibly execute. 2. Challenges with on-street parking and the pilot project initiative that was being contemplated for the area given recent concerns voiced by residents. 3. Mix of unit types in light of a perceived need for a better mix of including suites suitable for families (3 bedroom units) 4. Strong support for commercial at 9th Street in order to increase pedestrian activity. Some uses discussed included, neighbourhood pub, grocer, fitness, cafe, general retail etc.

Applicant Led Engagement Summary

		<p>5. Strong concern that no medical uses be proposed. 6. Timing of project phases to buildout. Concerns about delayed projects given the economic climate and previous Bridges projects.</p> <p>The majority of the board was present and Ali McMillan chaired the meeting.</p>
<p>3</p>	<p>Feb. 28, 2018</p>	<p>First Public Open House at Bridgeland-Riverside Community Hall</p> <p>This was the public's first opportunity to see the initial concepts being proposed. Approximately 14 Information boards were presented showing the basic concepts for the site, buildings and project statistics. A comment box was made available and the following summarizes the common issues / themes that were put forward:</p> <ol style="list-style-type: none"> 1. Buildings look very attractive. 2. Nice to have retail/commercial as part of the project. 3. Concerns about the proposed height (one tower shown as 15 storeys) and that residents have always "been told" when they purchase their homes in the area that 8 storeys would be the highest level that any future buildings would be. Desire to keep height under at least 10 storeys. 4. Shadowing from buildings a concern. 5. Concerns about entrance to parkade on McDougall Road, given traffic anticipated as the area density increases. Would prefer it if entrance was off of 9A Street instead. 6. On street parking would be nice for retail shoppers. Street parking is already very limited. 7. Site lighting should be shrouded to avoid offsite spill onto neighbouring residence within close proximity such as The Crossings. 8. More eyes on the street will be a benefit to the community. 9. Preference for condos as opposed to rentals. 10. Restaurant, Café and Bakery type uses preferred for the CRU's. 11. Noise from construction a concern. Please follow bylaws and as a courtesy keep loud activities like jack hammering for later in the day. 12. Love the landscaped podium roof top amenity. 13. Additional traffic for the area a big concern. 14. Bicycle parking should be conveniently located at grade. 15. Desire at-grade units facing St. Matthew's Square. 16. Allow for increased height with a guarantee of mixed use in the land use amendment. 17. Limit retail space uses to those that activate the street (not office, fitness, medical, vet clinic) 18. Concern with amount of glass being used and whether that is appropriate for historic Bridgeland. 19. Size of units and number of small units will mean that the park areas and amenities nearby become even more important. 20. Consider CEPTED issues to minimize opportunities for crime.

Applicant Led Engagement Summary

		<p>21. Any allocation of affordable housing? Could changes be made to make it affordable?</p>
<p>4</p>	<p>May 22, 2018</p>	<p>Second Public Open House at Bridgeland-Riverside Community Hall</p> <p>This was the follow up to the initial open house to provide the community an update on the land use application that showed more detail and development of the design. Approximately 16 Information boards were presented showing the basic concepts for the site, buildings and project statistics. A comment box was made available and the following summarizes the common issues / themes that were put forward:</p> <ol style="list-style-type: none"> 1. Commercial space on 9th would be great. 2. Love the look of the building aesthetics. 3. Building height as proposed is out of context with the rest of the neighbourhood. 4. Increase in height would set precedent for increase on other sites in our historic neighbourhood. 5. Proposed buildings could shadow Radius solar panels and cut-off views from St. Matthew church area toward Tom Campbell Hill. 6. Lower podium height at the corner of the site is welcomed. Maybe some of the massing from the towers could be relocated above this lower massing. 7. Too much glass usage for envelope. It's not energy efficient. 8. Consider street benches for the 1000+ seniors who live further to the south east of the neighbourhood. 9. Please factor in Crime prevention, we are getting too many condo thefts. 10. Promote need for traffic light at intersection of 9th Street and McDougall. 11. Concerned about traffic with so many added units and less parking. Especially on McDougall. Maybe more crosswalks? 12. Don't think FAR of 5.3 for this area is appropriate. 13. Concerned about shading of park in the winter months. 14. Don't like the idea of a rental building for the community. 15. Concerned about whether retail space will be successful. 16. Buildings look to blocky/no curves and wouldn't it make sense to step back the upper storeys? 17. Parking not sufficient. Parking in the community is lacking. Hard to find parking on the street already and it could get worse in the future with more development. 18. Entrance to the parkade could be a problem at rush hour periods. It will add more congestion to McDougall. 19. Loading and waste and recycling could be noisy and the trucks always idle while parked. 20. Could the podium roof top garden could be accessible by the public?

Applicant Led Engagement Summary

		<ol style="list-style-type: none"> 21. Slender massing seems to be a good compromise to lessen impact of height & big box look. 22. Traffic study is a must to determine if traffic calming is required along McDougall Rd. McDougall Rd. is currently be used as an alternate shortcut from Memorial Drive congestion. 23. Size of units is not family friendly. 24. Bucci has created quality projects that are all interesting and well designed. 25. Perhaps have parking for Car2Go to encourage less use of personal vehicles and thus traffic. 26. Positive and exciting development for the area. 27. We need more rental units ---- Good! 28. Units are so small and they will attract almost only singles and not families. Consider proposing some 3 bedroom units. 29. Appreciate maximizing density while not impacting comfort. 30. Like the two towers instead of mass building. 31. Developer must commit to executing the look of the building. Lesser quality is not acceptable. 32. Nice modern design.
5	July 18, 2018	<p>Meeting with Bridgeland-Riverside Community Association Board (BRCA)</p> <p>In addition to the board members, 3 invited non-board member community residents. This meeting was held to answer some questions that BRCA Board had about the development permit application. Some of the topics discussed:</p> <ol style="list-style-type: none"> 1. General update on the development permit with discussion of latest developments of the proposed development. 2. Discussion of height and shadowing and impacts on the neighbouring development to the north (Radius). 3. Residents in attendance wanted better clarity on the shadow studies to better understand the impact on the adjacent properties/amenities. 4. Discussion surrounding method and timing of notification of future residents of the community (specifically buyers of Radius condo)

Summary of <u>Response and Actions</u> to What was Heard:		
	Date	Response/Action to what was heard
1	Feb. 6, 2018	<p>Meeting with Senior Planning Officials</p> <p>Although this meeting did not engage the CA or the public directly it did help set the path for the overall scope and timeline of the development. The main actions were:</p>

Applicant Led Engagement Summary

		<ol style="list-style-type: none"> 1. Aim to maximize density; 2. Work with ARP amendment team to align with key elements of their application; and 3. Set targets for March land use amendment application and late spring/early summer DP application.
2	Feb. 20, 2018	<p>The first meeting held with Bridgeland-Riverside Community Association Board (BRCA)</p> <ol style="list-style-type: none"> 1. Given the Board expressed concern about the ability to execute the architectural look of the concept, Bucci and its design team began exploring options for the skin or screen element of the buildings so that a feasible (buildable) solution could be found. Also, discussions regarding concurrent applications and approvals for both the land use amendment and the development permit were initiated with planning to provide better assurance to the CA that the design would be executed to the intent of the land-use amendment. 2. On-street parking concerns raised by the board, led Bucci to engage Watt Consulting Group (Traffic Engineers) to begin preparation of a parking demand study and a traffic impact assessment (TIA). 3. To allow for flexibility of unit plan layouts, structural systems for the typical floor plate were explored. This would hopefully allow for potential of unit combinations and potentially allow for some 3 bedroom units. 4. The board voiced strong support for the commercial component. In response to this, we continued to development the retail street front to create the start of a high street experience along 9th Street. 5. Given the concern with certain uses and support for others, we began compiling the permitted and discretionary uses that we would look to include for the land use amendment application. 6. As full build out timing and execution was raised as a concern, the design team began planning how phasing the project could be delineated to minimize the risk of over building.
3 & 4	Feb. 28, 2018 & May 22, 2018	<p>Public Open Houses at Bridgeland-Riverside Community Hall.</p> <p>The comments received at both open house events were compiled and organized into recurring themes. The following summarizes our responses and what actions and/or changes that occurred because of what was heard:</p>

Applicant Led Engagement Summary

		<ol style="list-style-type: none">1. Assurance of Design Commitment As described in the initial BRCA meeting, the CA and several open house attendees, voiced a concern for assurance that the design would be executed as proposed (quality of look and inclusion of mixed use commercial). The concurrent LOC and DP process was recommended by planning as the best way to provide this kind of assurance to the community. 2. Unique Architecture and Presence We were very pleased to hear so many positive comments about the look of the project. We are also very proud of the project and given that the success of project depends so much on fulfilling this look, the design team will do everything they can to execute the vision of the concept and make it a reality. 3. Materiality and Style Concerns around the use of glass as it relates to both the context of Bridgeland were raised. Some expressed contextual concerns they have about whether a building that has so much glass could be sensitive to what many consider a long established and historic community. We believe that even within the most historic of settings, unique and exciting things can and should be allowed to occur and still be sensitive to their surroundings. The proposed building will have a material palate consisting of glass, metal and masonry. Together they will combine to create an extraordinary sculptural presence that we hope will make this project as striking, memorable and "photo worthy" as other iconic landmarks in our city. At the same time the project stays true to fundamental urban design principals to ensure that the edges of the site are safe, pedestrian friendly and activated. 4. Height Concerns about height were raised by several residents with some understanding that some degree of height increase from the old DC zones was inevitable. Although, we feel that the height being proposed is appropriate for a transit-oriented site such as this, we can also appreciate that this may seem like a significant increase when compared to the 7 and 8 storey heights of the existing zones. Our message to the residents regarding this was:<ol style="list-style-type: none">a) amendments to the Bridgeland-Riverside ARP will account for the potential of increased heights on sites within a TOD context.b) although the proposed height is increasing, the allowable density would effectively stay the same (5.3 FAR). The goal is to redistribute massing to allow for more diversity and uniqueness to the community.
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Applicant Led Engagement Summary

		<p>c) this massing strategy also allows for height relief at the corner of 9th Street and McDougall with effectively only a 2 storey massing across from the 8 storeys of The Crossings development and a break up of an otherwise very long block into two slimmer towers.</p> <p>d) The site is not immediately adjacent to any low-rise properties that would be sensitive to this height increase and the towers will not cut-off views of the downtown skyline or mountains from Tom Campbell Hill given the grade differential.</p> <p>e) The north tower has been reduced in height by one floor.</p> <p>5. Shadowing Related to the height were concerns about shadowing or shading of the adjacent spaces and buildings by the proposed development. In response to these concerns, we prepared and submitted several shadow studies of the proposed development and of alternate massing arrangements within the context of the surrounding neighbourhood. Each study shows how the difference in massing translates at different times of the day and year for each scenario. The studies also show that, even if the massing changes, as long as the volume (FAR) of the building remains consistent than the surface area of the shadowing also remains similar.</p> <p>The studies also confirm that the proposed development has minimal impact on the most sensitive areas to the west, Murdoch Park soccer field and playground structures. The Radius rooftop garden would only be in shade for approx. 4 hours during the spring and fall and not all during the summer months when it can expect the most use. The Radius roof top solar panels are on the north portion of the roof and will not be affected by the proposed building. To illustrate this further, shadow animations will be made available on the project website that show the exact path, extent of the shadowing and corresponding time lapse throughout the day.</p> <p>The reduction of one floor from the north tower will help in minimizing the impact of shadowing on the community hall and park beyond.</p> <p>6. Light Pollution Any proposed exterior lighting for the development will be shrouded to ensure minimal spillage beyond the subject site. Fixtures such bollards and down lights will all be utilized to maintain safe yet comfortable lighting levels in and around the site.</p> <p>7. Parking Both on-street public parking and private parking were raised as concerns at both open house events. Realizing most often</p>
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Applicant Led Engagement Summary

		<p>that this issue stems from lack or inconvenience of visitor and commercial parking, we have focused on providing all of the visitor and commercial parking on the first level P1 which is accessed immediately off of McDougall with no need for a trip down stairs or elevators. This will ensure not only the convenience to visitors and shoppers, but also the best chance for success of the future businesses that will inhabit the retail units. Close to 40 stalls on this level will be shared between shoppers in the day and then visitors in the evening to make the most efficient use of these stalls throughout the day. The parkade continues 2 storeys below this level to provide close to 229 stalls for 334 apartment suites translating to a ratio of approximately 0.68 stalls per unit.</p> <p>8. Dedicated Car Sharing Stalls Car2Go or other similar car sharing will be explored as part of a comprehensive TOD strategy for the development.</p> <p>9. Traffic Concerns about increased traffic along the two main adjacent roads (9th and McDougall) were raised. Specifically, how the increase in density and additional vehicles would affect the community. And in greater detail, how the entrance to parkade on McDougall Rd. would affect traffic on McDougall Road.</p> <p>As described earlier in this report, Watt Consulting Group was engaged to prepare both a parking demand study and a TIA for the development. This was submitted to planning administration in support of the land-use amendment. The details of these studies were thus made available to the public for their review. Any potential conflicts or traffic related challenges will hopefully be highlighted so that they can be addressed appropriately.</p> <p>The access to the parkade was specifically located to take full advantage of the slope of the site and allow vehicles and pedestrians to access the P1 level (visitor and commercial parking) with full convenience and barrier free accessibility. Vehicles leaving the parkade, do so without having to negotiate a ramp and thus limit their visibility of pedestrians and/or other vehicles on the road. Vehicles entering the parkade will have sufficient queuing length before the high speed roll up door to ensure that cars waiting to enter will not spill out onto the road or straddle the sidewalk.</p> <p>10. Need for traffic light at 9th Street and McDougall Road As noted above Watt Consulting Group (Transportation Engineers) prepared a traffic impact assessment (TIA) for this development. This report did not highlight a need for signaling of this intersection.</p>
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Applicant Led Engagement Summary

		<p>11. Bicycle Parking The need to maximize and improve bicycle parking for this development was mentioned several times by the open house participants. It is for this reason, that both Class 1 and Class 2 parking stall quantities exceed the minimum bylaw amounts and are located in convenient areas (at or close to street level) to ensure they are utilized as much as possible.</p> <p>12. Commercial Uses Although the vast majority of the open house attendees supported the introduction of commercial space along 9th Street, there were many that went further to ask for specific uses that were needed or desired and those that were seen as potentially problematic. As a result of this input, we worked carefully with Planning and the CA to include the exact uses that both the community and CA considered a potential benefit to the area.</p> <p>13. Construction Noise and Disruption Disturbances during construction are unavoidable; however, we have made the commitment to the community that these will be minimized. Due to the site layout and road network, loading or hauling from the western side of the property (9th Street) cannot occur without appropriate permission from the City of Calgary. Almost all access will be from St. Matthews Square and 9A Street.</p> <p>Lane closures will also be required. The unique site conditions make this a challenging build but we have assured the CA that we will adhere to the City of Calgary street use and noise bylaws.</p> <p>14. Condo vs. Rental The Calgary multi-family residential sales are not at level that can allow a project such as this to be economically viable as a market (condo) development. Currently, the only way this development can move forward is if it is conceived as a rental building and maximizes the density that is allowed with an efficient design. With the recent changes in mortgage rules (2% stress tests on loans applicants) and anticipated increases in mortgage rates, it remains unclear as to how long it will take for the condo market to return to favorable levels.</p> <p>15. Size of units and desire to attract families The current proposed floor plans have been conceived with the idea that, if demand dictated, larger units could be developed by way of combining smaller units to create either a greater number of 2 bedroom units or, larger 3 bedroom units. Currently however, market studies continue show trends towards smaller, more price point driven units.</p>
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Applicant Led Engagement Summary

		<p>16. Affordable Housing Dominion will provide competitive affordable rentals because it has been designed and conceived with efficiency in mind. Smaller units that work better allow buyers access to the lifestyle they desire but could not previously afford in an areas as great as Bridgeland-Riverside.</p> <p>17. Safety and Crime Prevention Concerns about safety within and around the proposed development were raised. As with all of our projects, we make every effort within our design intentions to minimize potential undesired behavior and activity around the site. The best way to achieve this is of course to maximize visual surveillance of the street by utilizing as much clear glazing as possible. The grade-oriented units will provide much of this visual connection, while the commercial uses on 9th should provide more activation during typically quieter evening hours. Other CPTED measures will also be employed around and within the parkade, common areas and exits within the building. Visibility into exits and vestibules, high lighting levels, overall brightness, controlled access hardware and security systems /cameras will all contribute to a safer environment overall.</p> <p>18. Public seating around the site Opportunities for the public to gather and just spend some time around the site are important to foster a sense of community. We have proposed several areas within the setback of the site for benches and landscaping. Further opportunities for seating can be allowed to evolve as the use of the retail units becomes clear. For example, if a business such as a café or ice cream shop occupies one of the CRU's then it would make sense for seating to be introduced immediately outside of that shop. These seating areas can also provide areas for older pedestrians to rest as they walk from through the community.</p> <p>19. Lower towers by transferring some their massing to podium level Although it seems like a logical strategy to bring the height of the towers down, transferring massing from the towers would compromise the overall look of the two towers over a podium severely. In order to reduce the towers by even a few storeys each, the podium would most likely increase in height to about 5 storeys in height. The resulting look would be of a large block mass once again with two raised portions of that block on either end of the block. Currently, the sleek low profile podium enhances in contrast the slender nature of the towers.</p>
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Applicant Led Engagement Summary

5	July 18, 2018	<p>Meeting with Bridgeland-Riverside Community Association Board (BRCA)</p> <ol style="list-style-type: none">1. The concerns raised here were from two future residents of the Radius Condo and were aimed at a need for more clarity on the shadowing impact and questioned the massing in relation to the shadowing specifically on the south face of Radius, and its roof top garden. In response to this, we worked with planning to update the shadow studies to show greater context and additional times of the day for each time of the year. Further to that, we have created animations of the shadow studies to better illustrate the shadow path as it sweeps across the surrounding sites.2. With respect to method of contact to be used to reach future Radius residents, Bucci has confirmed additional contact information and will ensure that these stakeholders receive appropriate notification of future engagement and information sessions.
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Troy Abromaitis
Director of Development
Bucci Developments

Tony Casola, Architect, AAA, SAA, MAA, AIBC
LEED® AP, Principal
Casola Koppe Architects Ltd.

Community Association Letters



Planning Committee
917 Centre Avenue NE Calgary AB T2E0C6
brocalgary.org

2 October 2018

Circulation Control
Planning, Development & Assessment #8201
The City of Calgary
PO Box 2100 Station M
Calgary AB T2P2M5

Attn: CPAG.Ciro@calgary.ca
cc: Christine Leung, File Manager (christine.leung@calgary.ca)
Ali McMillan, BRCA Planning Director (planning@brocalgary.org)

To Whom It May Concern:

RE: DP2018-3108 (1018 McDougall Rd NE)

Thank you for the opportunity to comment with respect to development permit application DP2018-3108. These comments are updated from our previous letters; those letters should be read in conjunction with this one. Fresh comments here reflect only inputs received from those members of our Planning Committee who were present at our October 1 meeting. We were not able to give notice to adjacent neighbours of that meeting.

- We appreciate having received the Applicant's update regarding the building materials and are happy to see the developer move away from the "glass skin" option. We are still concerned about the certainty of the community achieving the "look" that has been shown in renderings.
- We would like to see a diversity of unit types in the building, including 3-bedroom options, as we feel that the suggested inventory offers too many small all-rental units, which may not appeal to evolving and broader population demographics ("complete communities").
- The overall heights proposed of 14 and 16 storeys are disproportionate to surrounding buildings and there are lingering and constant "over-height" concerns. We would still prefer to see a more significant stepping difference between the east and west towers (a minimum of two stories?). This would require looking at options like increasing the podium to 3 stories to allow for a 10 and 12 storey tower, or, other alternative ideas. With both towers being so close in height there is no significant "step down" in relation to contextual existing heights of surrounding buildings or the Community Association building and park across the street.
- Shadowing on the surrounding parks, the community hall, playground, rooftop garden, and skating rink is significant compared to that which would result from mid-rise options.
- Adjacent residents of Radius 1 are concerned about shadowing of their amenity spaces including private balconies and their rooftop garden.
- Shared parking as proposed between commercial- and visitor-designated stalls can easily result in a significant shortfall for one purpose or the other. We would prefer to see additional

Community Association Letters

parking allocated to these combined purposes, with the developer able to sell off some stalls after a stable period of full occupancy, and based upon parking studies.

- Enhanced landscaping and public realm improvements are always a great benefit to the community. We would look for certainty / assurances that the proposed levels of lighting, landscaping, benches, etc. actually get built.
- We appreciate the minimized parkade entry apron, recessed from street.
- The commercial uses at grade with active patio interface will, we hope, add vitality, "eyes on the street" to the area, and enhance the primary pedestrian corridor that 9th Street has already become to some degree already, and that has the potential to grow further.

Sincerely,

BRIDGELAND-RIVERSIDE COMMUNITY ASSOCIATION

Per: BRCA Board of Directors Planning Committee

Community Association Letters



Planning Committee
917 Centre Avenue NE Calgary AB T2E0C6
broacalgary.org

20 Aug 2018

Circulation Control
Planning, Development & Assessment #8201
The City of Calgary
PO Box 2100 Station M
Calgary AB T2P2M5

Attn: CPAG.Ciro@calgary.ca
cc: Christine Leung, File Manager (christine.leung@calgary.ca)
Ali McMillan, BRCA Planning Director (planning@broacalgary.org)

To Whom It May Concern:

RE: DP2018-3108 (1018 McDougall Rd NE)

Thank you for the opportunity to comment with respect to development permit application DP2018-3108. Notice of the Planning Committee meeting where this was reviewed was given to neighbours adjacent to the subject parcel via condo board emails and several attended. However since Buccì Radius (adjacent to the north) is not complete, we were not able to notify anyone who has bought into that building, and who would be directly impacted. Two 'pending' residents of Buccì 1 have contacted us and did attend the meeting. The comments below therefore reflect the feedback of the BRCA Planning Committee members and several residents of the surrounding area present at the Aug 7, 2018 meeting. The development team was not invited to attend as they came to present to us earlier, however an updated package of renders and rationales was provided by Buccì for the meeting which was appreciated.

What are the strengths and challenges of the proposed development?

- Strength –Ground oriented live work units accessed at grade from the main frontage along McDougall Rd. and wrapping around the rear of the building into St. Matthew Square with functional amenity spaces interacting with the pedestrian realm. High quality gate treatments for each unit that are visually permeable.
- Strength – substantially sized amenity spaces at grade at front and rear to provide "eyes on the street"
- Strength – adding density in an appropriate location near transit/ LRT
- Strength –unique and interesting architecture
- Strength – substantial permeable landscaping at grade shown including vibrant flower beds, specialty seating and unique pavers

Community Association Letters

- Strength -small retail spaces along 9th St with many entrances to add vibrancy to the street
 - Strength – patio space at grade wrapping important corner at McDougall Rd. NE and 9th St NE – providing activation, eyes on the street and a gathering space.
 - Strength – glass podium provides visual permeability at grade which increases safety/ interest and activates 9th St NE
 - Strength – wide sidewalks surrounding the building promote highly walkable nature of the Bridges
 - Strength – bike parking, benches and art sculptures specified to add to the public realm at grade around the building
 - Strength – screened transformer, loading stall and consideration given to hide parkade entrance with landscaping, etc.
 - Strength – well-lit perimeter with pedestrian-scaled lighting increases safety and uplit trees provide beauty at night
 - Strength – Podium rooftop space looks great – it would be even better if it were part of a public bar/ restaurant the whole community could enjoy?
 - Strength – substantial bike storage provided
 - Challenge – we question if this building can in fact be built to achieve the architectural style presented in renderings. The painted glass skin method seems like a departure from the look desired/ presented, and has questionable maintenance cost or longevity.
 - Challenge—Too many small all-rental units, not appealing to broader demographics
 - Challenge— no stepback on west side along 9th Street to 15 storey tower – not human scaled – could appear to “loom” over the park and street with massing.
 - Challenge – overall height of 15 stories is very disproportionate to surrounding buildings and there are over height concerns
 - Challenge – with both towers being 15 stories there is no step down/ relational element to existing heights of surrounding buildings
 - Challenge – Some concern for how trees will be planted – some in the first phase of the Bridges did not survive in the planters – we want to ensure survival with appropriate sized planters
 - Challenge – Shadowing on surrounding parks is significant compared to mid-rise option
 - Challenge – Shadowing on Community Association Hall lands including our playground, rooftop garden, skating rink is substantial compared to shadow of Bucci Radius building.
 - Challenge – shadowing on neighboring Bucci Radius amenity spaces including private balconies and rooftop garden is significant compared to mid-rise option due to two towers and height.
 - Challenge – Shared parking between commercial and visitor can easily leave a significant shortfall for one purpose or the other. We would prefer to see additional parking allocated to these combined purposes, with the developer able to sell off some stall after 12 months of full occupancy based on parking studies.
 - Challenge – engagement opportunities seem to be ill-timed. Concern that Bucci Radius residents are unaware of the change in plans from mid-rise to 15 storeys.
- 2) Are there changes that could be made to the proposed development to make it more compatible or beneficial to the area? YES.
- Reduce the height of the West tower to better fit with existing buildings, provide a transition and reduce shadowing on parks (max 10 storey). Reduce the height of the East tower as well.
 - Step back west tower from street at podium

Community Association Letters

- Explore ideas to improve articulation of west tower –, perhaps by turning it 45° or providing different articulation from base to reduce impact on pedestrian level.
 - We would like to see the apron at the parkade entrance reduced with visually appealing parkade door, plus the door size minimized as much as possible to reduce impact on the pedestrian realm
- 3) Provide comments on:
- 1) The use (if identified—not applicable for single detached houses, semi-detached dwellings or duplexes).
 - Appropriate use and density for this area, height issues aside.
 - 2) The site design
 - Well oriented
 - 3) The building design
 - See above comments
- 4) Has the applicant discussed the development permit application with the Community Association? If yes, what information was provided?
- Yes. The developer presented to the Bridgeland Riverside Community Association's Planning Committee
- 5.) Please provide any additional comments or concerns regarding the proposed development.
- None.

Sincerely,

BRIDGELAND-RIVERSIDE COMMUNITY ASSOCIATION

Per: BRCA Board of Directors Planning Committee

**Planning & Development Report to
Calgary Planning Commission
2018 October 18**

**ISC: UNRESTRICTED
CPC2018-1184
Page 1 of 14**

New Policy: International Avenue Area Redevelopment Plan

EXECUTIVE SUMMARY

The purpose of this report is to bring forward the International Avenue Area Redevelopment Plan (“The Plan”) that was drafted in collaboration with the Main Streets Initiative as the result of Council direction to amend the *Southeast 17 Corridor: Land Use and Urban Design Concept* (Corridor Concept Plan) to become a statutory document. Lands proposed for inclusion in The Plan are currently subject to *the Forest Lawn/Forest Heights/Hubalta Area Redevelopment Plan (ARP)* and the *Albert Park/Radisson Heights ARP*. Amendments to these policy plans will remove the lands that will be included in The Plan. City initiated land use amendments (CPC2018-1185) have also been submitted concurrent to this application, to be heard on the same Calgary Planning Commission and Council Meeting agendas.

Administration is recommending that Council rescind the existing non-statutory Corridor Concept Plan and replace it with The Plan. The Plan will realize Council’s direction to convert the existing document into a new statutory plan, address signage policy and regulations along the corridor, and clarify design guidelines, ensuring that development is better aligned with the *Developed Areas Guidebook (DAG)* of the *Municipal Development Plan (MDP)*. Furthermore, an updated policy and land use amendment will create certainty for local stakeholders, providing a clear vision that achieves the MDP goals for Main Streets, shortens approval time for redevelopment through appropriate land use districts, and reinforces The City’s investment in public realm improvements throughout the area.

This report contains the following components:

- Proposed International Avenue Area Redevelopment Plan (“The Plan”) (Attachment 1);
- Proposed amendments to *Forest Lawn/Forest Heights Hubalta Area Redevelopment Plan (ARP)* in support of the land use redesignation (Attachment 2); and
- Proposed amendments to *Albert Park/Radisson Heights Area Redevelopment Plan (ARP)* in support of the land use redesignation (Attachment 3).

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission receive the presentation and this report, and forward Calgary Planning Commission’s comments to the SPC on Planning and Urban Development for information.

PREVIOUS COUNCIL DIRECTION / POLICY

At the 2016 September 13 Combined Meeting of Council, Council decided to:

ADOPT, Moved by Councillor Chabot, Seconded by Councillor Carra, that Councillor Chabot’s Motion, NM2016-25 be adopted, **after amendment**, as follows:

NOW THEREFORE BE IT RESOLVED that the Planning & Development Department be directed to develop an area specific sign policy similar to the Stephen Avenue sign policy;

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2018 October 18

ISC: UNRESTRICTED
CPC2018-1184
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AND FURTHER BE IT RESOLVED that administration, in consultation with stakeholders, be directed to review and bring forward amendments to the 17th Avenue Corridor Study with the goal of having it approved as a statutory document and to return to Council at the Mid Cycle Adjustments deliberations with funding requests if required.

CARRIED

At the 2017 April 10 Combined Meeting of Council, Council decided to:

ADOPT, Moved by Councillor Chabot, Seconded by Councillor Woolley, that the SPC on Planning and Urban Development Recommendation contained in Report PUD2017-0241, be adopted, as follows:

That Council approve the Main Streets Implementation Plan.

BACKGROUND

The Corridor Concept Plan was originally adopted in 2010 as a non-statutory policy plan. While the Corridor Concept Plan established a long-term policy framework focused on urban design for International Avenue (17 Avenue SE), it only provided general direction. A statutory plan is a plan prepared and adopted in accordance with the procedures outlined in the *Municipal Government Act*, including requirements for public notification, engagement, and public hearing. While both non-statutory and statutory plans must be considered by the development authority when reviewing a permit application, on appeal, non-statutory documents have lesser standing and may be disregarded, as the *Municipal Government Act* only requires appeal boards to have regard to statutory plans. A statutory document would enable development and signage consistency, protect the architectural and historical elements of 17 Avenue SE and capitalize upon the investment in the Southeast Bus Rapid Transit (BRT) project which is anticipated to be completed by the end of 2018.

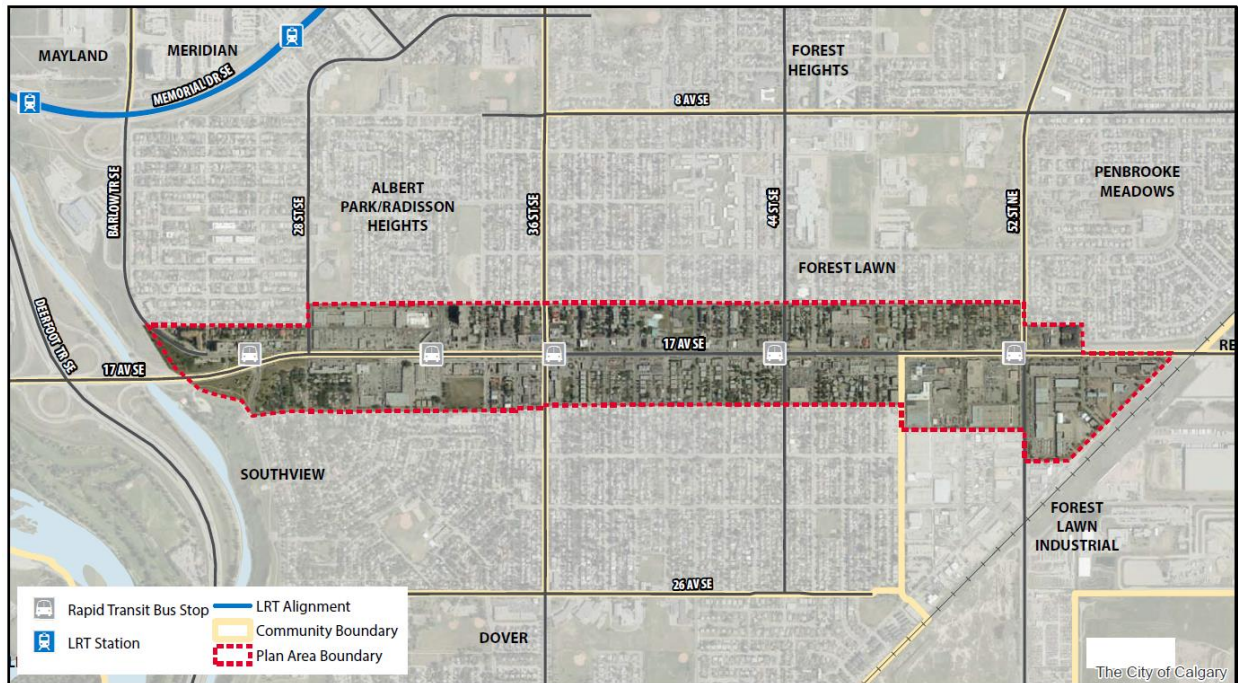
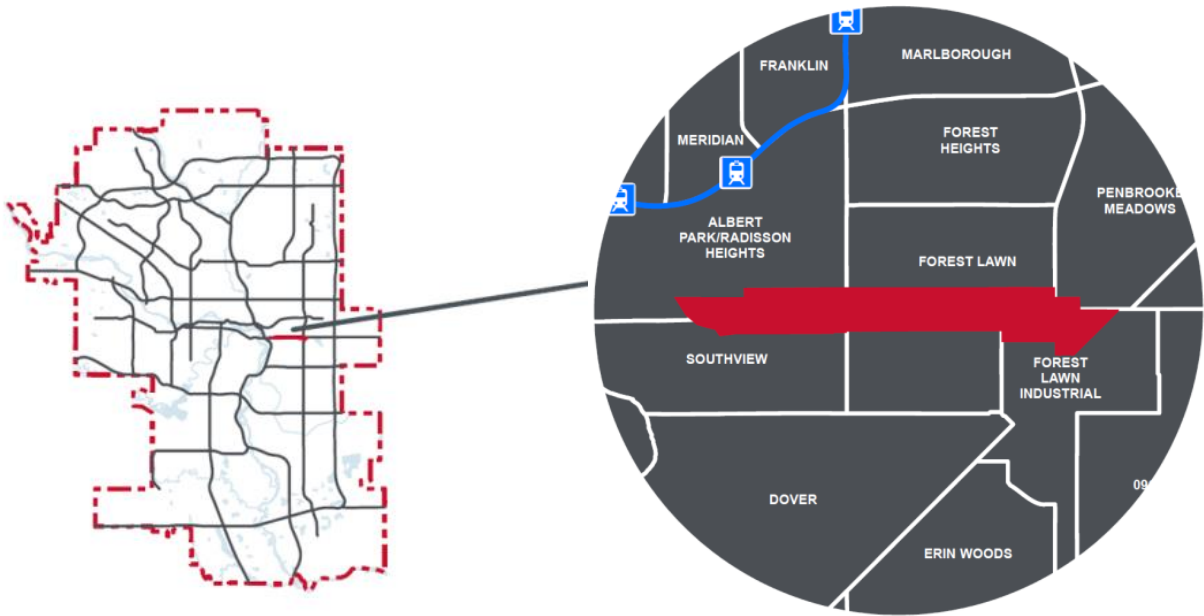
In 2016 September, Council directed Administration to investigate policy amendments to the Corridor Concept Plan to become a statutory document and to include signage policy specific to 17 Avenue SE (NM2016-25).

In 2017 April, Council approved the Main Streets Implementation Plan (PUD2017-0241 and PUD2018-0347) with a focus on the application of policies, goals and targets contained within the MDP.

As a result of the Council directions for 17 Avenue SE and the Main Street Initiative, the project teams combined the work into one project, the result of which is The Plan, amendments to *Forest Lawn/Forest Heights/Hubalta ARP*, the *Albert Park/Radisson Heights ARP*, and the City initiated land use amendments have also been submitted concurrent to this application (CPC2018-1185).

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Location Maps



New Policy: International Avenue Area Redevelopment Plan

Site Context

The Plan consists of approximately 117 hectares (289 acres), and is located in the southeast quadrant of Calgary. The Plan is approximately five kilometres east of downtown Calgary and includes one of the major corridors that provides for east/west commuting in and out of the city. Specifically, The Plan area is bounded by Barlow Trail SE and 26 Street SE to the west, 19 and 20 Avenue SE to the south, 16 Avenue SE to the north, and the Canadian National Railway line to the east. The area is characterized by a diverse population with rich cultural backgrounds. This has been a key contributing factor in the creation of the area known as “International Avenue”. The businesses in this area form part of the International Avenue Business Revitalization Zone (BRZ).

The existing development pattern generally features small-scale, local-oriented retail/commercial uses along most parts of the 17 Avenue SE, with large-scale commercial developments located in the east and west portions of The Plan area. This pattern reflects the historical development and function of 17 Avenue SE as both highway commercial and local retail. The street pattern in the area is based on a standard north/south, east/west rectangular grid. The north/south streets provide for excellent accessibility and interconnectivity between the commercial corridor and the adjacent residential communities. A typical block in the Plan area has one to three storey local retail and office uses with residential development across the lane. This pattern presents an early form of retail 'street wall'. However, the achievement of a more vibrant and walkable commercial street has been compromised by substantial areas of surface parking in front of many buildings as well as the concentration of auto-oriented uses along some parts of 17 Avenue SE. Although some multi-residential buildings have been developed over the past couple decades on the blocks behind the commercial corridor, low-density residential development is the predominant land use in these areas.

In general, based on the analysis of existing development and the construction of the new Southeast BRT Transit way, there is significant redevelopment potential in The Plan area.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The Plan

The Plan supports the enhancement of the established community with existing infrastructure into a more transit-focused, liveable and distinctive mixed-use community.

To respect the outcome of the public engagement held for the Corridor Concept Plan in 2008-2010, Administration maintained the vision and core ideas that the community supported. The Plan envisions 17 Avenue SE as a multi-modal Urban Boulevard with three distinctive character zones: a gateway (Western Gateway) to welcome visitors and mark the entrance, a vibrant central shopping street and activity hub (Central Boulevard), and a landscaped corridor (Eastern Parkway) extending to Stoney Trail at the eastern city limit. Different streetscapes, land uses, building heights and design criteria are applied to help achieve the specific objectives of each character zone.

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The Plan identifies the 36 Street SE and 52 Street SE intersections along 17 Avenue SE major transit hubs. These multi-functional hubs provide strategic locations for higher density developments and support connectivity to areas outside The Plan area. They also act as the transition between the three character zones. Given the distance between the two identified major transit hubs, secondary transit hubs are also identified at 28 Street, 33 Street and 44 Street SE. In addition to serving as transit hubs, these locations provide concentrated commercial services at a neighbourhood level.

The area will become a commercial destination that balances serving the local commercial needs while supporting the broader region outside The Plan area, with specialty services that will also attract people from the entire region. The Plan promotes the community character and cultural diversity of the area by providing for a wide range of housing choices and by its place-making approaches.

The proposed sign policy within The Plan has been developed to create a local vision and identify how signs should respond to the community character of 17 Avenue SE. This policy is specific to the local context. Since parcels along the Main Street have multiple land use districts that are used city wide, Administration determined that having these policies in The Plan would be the most strategic way to ensure that signage relates to this particular area. Specifically, signage policy ensures conformance of future signage with city-wide policies, as well as protecting heritage signage along 17 Avenue SE.

The proposed objectives of The Plan are to:

- a. replace and rewrite the existing non-statutory Corridor Concept Plan with a new statutory plan;
- b. address signage policy and regulations along the corridor;
- c. incorporate the polices of the newly adopted *Developed Areas Guidebook (DAG)*;
- d. reinforce the principles of the *MDP, Calgary Transportation Plan (CTP)*; and the *Southeast 17 Corridor Transportation Plan*;
- e. propose city-initiated land use amendments, which support growth and redevelopment aligned with local policies;
- f. provide land uses that support community development, economic vitality, and sustainable transportation modes including walking, cycling and transit;
- g. reinforce the positive character, quality and stability of existing neighbourhoods in the surrounding area;
- h. assist in achieving complete communities where people can live, work, learn, shop and find recreation;
- i. identify strategies for facilitating public improvement and infrastructure upgrading; and
- j. support the findings from various public engagements since 2003 until present.

Policy Overview

Recognizing that The Plan impacts many local communities with different contexts and physical patterns, and that there are existing characteristics that are highly valued by local communities, three distinct Character Zones are proposed based on planning analysis and community input.

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The Western Gateway Zone (from 26 Street SE to 36 Street SE) is a gateway district with strong visual impact that evokes a sense of arrival to 17 Avenue SE. The Western Gateway zone will welcome visitors, reduce traffic speed and visually define the entranceway through building design and a pedestrian-friendly streetscape. The Western Gateway zone contains the 28 Street SE, 33 Street SE and 36 Street SE BRT Stations, and includes the Main Street Village which is recognized as a distinct character area with unique building designs and streetscape features. The Main Street Village area features specialty shops, a linear park and a mixture of diverse but compatible land uses and building heights.

The Central Boulevard Zone (from 36 Street SE to 52 Street SE) is intended to create a retail hub and a core area for community activities. The Central Boulevard Zone provides for a vibrant, active and pedestrian-oriented retail environment, creating places for social interaction and community engagement, and building a compact, dynamic and mixed-use urban centre. The Central Boulevard Zone contains 36 Street SE, 44 Street SE and 52 Street SE BRT stations and envisions a continuous street wall development. Developments in this zone feature small-scale, active store frontages and clustered restaurants and outdoor cafes that are facing a tree-lined boulevard and many compact street parks (street corner parks or pocket parks between buildings).

The Eastern Parkway Zone (from 52 Street SE to Stoney Trail) represents the eastern terminus of the Plan Area that will combine residential and commercial uses to the north and a mix of commercial and light industrial uses to the south. The Eastern Parkway Zone will have a decrease in the intensity of the Main Street. Due to the connectivity of the lands that form the Eastern Parkway Zone, this area has a light industrial characteristic on parts of the land, while introducing the opportunity for more residential uses and a community centre adjacent to the Main Street.

Land Use Policy

Development in The Plan will support a variety of building types and vertical and horizontal mixed uses. Building blocks found in The Plan are based on those established in the DAG, Volume 2, Part 3 of the MDP, whereby each building block has a range of land use districts. Building blocks along 17 Avenue SE are intended to meet the minimum intensity of the Urban Main Street typology as defined in Volume 1 of the MDP. Policies within The Plan provide detailed guidance as to the appropriate land use and built form.

The following building blocks are found in the Plan:

Community – High Density	Is located around the main transit node of 36 Street SE and in a few sites where higher density is identified. This building block is intended to facilitate high intensity residential and mixed-use development with large scale residential and non-residential uses.
Community – Centre	Is located throughout The Plan fronting onto 17 Avenue SE It is characterized by a fine grain network of streets, wider sidewalks to encourage pedestrian activities, active streets and buildings set close to frontages to support higher levels of commercial and residential intensity.

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Community – Mid-Rise Is located throughout The Plan fronting 17 Avenue SE It is intended to accommodate a mix of mid-rise buildings that contain residential and commercial uses that create an active vibrant Main Street. Other uses that may be considered appropriate include schools, daycares, places of worship and civic uses.

Active Frontage	These policies apply specifically to the area classified as Main Street Village. Active Frontages are the portions of buildings where the ground floor commercial uses contribute to activity on the streets throughout the day and evening. This designation indicates the requirement for high-quality building and public realm design that will enhance the pedestrian experience and contribute to the vibrancy of the Main Street Village area.
Special Areas	These policies apply specifically to the areas classified as “Special Area” where auto-service uses or drive-through uses are allowed.
Future Comprehensive Development	These areas identify future land uses and require detailed site planning. It applies to the existing large parcels along the Main Street currently developed with one storey large format developments that have extensive surface parking areas abutting 17 Avenue SE and the flanking streets.
Neighbourhood – Low-Rise	This building block type is proposed outside much of the Main Street as a transitional use between the mixed use development fronting 17 Avenue SE and the low density residential areas in the communities to the north and south.
Employment – Industrial	Located in the eastern end of The Plan. It provides for a broad variety of industrial uses and intensities, protecting industrial land for primarily industrial uses, while allowing strategic non-industrial uses to support employees in the area.

Main Street Initiative

From 2015 September to 2016 June, the Main Streets Initiative focused on analysis and evaluation of a variety of factors that influence growth and change in the Main Streets areas. This included detailed analysis of feedback from stakeholders, review of the existing policy, land use districts and parking requirements. It also looked at current and future market demand and growth forecasts, infrastructure capacity within the Main Street areas and costs to redevelop streets and sidewalks that align with the *Calgary Transportation Plan* and the *Complete Streets Guide*.

Local Area Plan Amendments

The *Albert Park/Radisson Heights ARP* and the *Forest Lawn/Forest Heights Hubalta ARP* identify 17 Avenue SE as a growth and redevelopment area, but do not provide up-to-date guidance for mixed-use and commercial development due to the age of these plans. As such, the ARPs do not allow for complementary and supportive ground-oriented residential forms within their plan areas.

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The amendments proposed as part of this report align the ARPs with the MDP, and support the City's overall vision for Albert Park/Radisson Heights and Forest Lawn/Forest Heights Hubalta as urban and neighbourhood main streets. The amendments provide more appropriate guidance for mixed-use, commercial, and residential intensification along and nearby 17 Avenue SE. Specifically, the proposed amendments will remove the commercial lands along 17 Avenue SE from the ARPs and relocate them into The Plan area boundary. See Attachment 2 for *Forest Lawn/Forest Heights Hubalta* ARP Map and Attachment 3 for *Albert Park/Radisson Heights* ARP Map.

Implementation

The Main Streets implementation strategy includes streetscape master plan design work for public realm, street cross-section, traffic calming/management, options for all modes (if not within the cross-section, then potentially on nearby parallel routes), and parking management. This streetscape master plan design work for 17 Avenue SE is funded through the BRT construction budget.

Actual uptake of redevelopment opportunities enabled by the proposed redesignations and ARP amendments are likely to take place over multiple decades. Through the development permit process, Administration will have the opportunity to ensure that the details of each project (streetscape, walking and cycling access and facilities, as well as vehicle access, parking, and loading) are satisfactory. The proposed land use designations (LOC2017-0371 / CPC2018-1185) all include mandatory minimum requirements for vehicle and bicycle parking, as well as provisions for loading and visitor parking.

The proposed changes are expected to help 17 Avenue SE evolve into a vibrant destination with a mix of amenities serving the local neighbourhood and attracting Calgarians from across the city, supported by a wider range of housing options and investments in public space and streetscape. This in turn is expected to stimulate further redevelopment of the area.

Infrastructure

Transportation Networks

The Plan, along with the associated policy amendments help to achieve Calgary Transportation Plan (CTP) transportation goals relating to:

- alignment of infrastructure investment and land use direction;
- affordable mobility and universal access; and
- strengthening the value proposition for walking, cycling, and public transit.

The grid layout creates a mobility framework with many options for access to development sites for all modes of transportation. Redevelopment of sites along the Main Street will benefit from:

- a robust grid of streets, allowing vehicle access from defined intersections and dispersal of local access and on-street visitor parking across multiple streets;
- frequent transit service within walking distance along the Primary Transit Network; and

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- established walking and cycling infrastructure including complete sidewalk networks, on- and off-street cycling routes, and the city's pathway network.

Although these land use redesignations and ARP amendments are strategically located, the Main Streets program and other City initiatives, such as RouteAhead and the Pedestrian Strategy, have identified critical improvements to the infrastructure for all modes of transportation that will require further investment in order to support The City's long-term vision.

The Plan area benefits from proximity to the Centre City, as well as service on Routes #1 and BRT with Primary Transit Network status for service along 17 Avenue SE. Through the grid of neighbourhood streets and collector roads, redevelopment sites have easy access to arterial streets, urban boulevards, neighbourhood boulevards, and the skeletal road network.

Utilities and Servicing

All applicable policies regarding utility infrastructure will be located in the MDP Volume 2 Part 3 - DAG. Several identified utility infrastructure upgrades are already occurring along this street through the Southeast BRT project.

As the Main Streets program continues to develop, Administration is working on a number of initiatives aimed at viable and successful redevelopment. This work includes:

1. Sanitary, potable water, and fire flow capital upgrade analyses for 17 Avenue SE to ensure utility servicing can support the ultimate proposed land use and intensity increases. City budgets are informed with current and long-range assessments of major infrastructure improvements that could be triggered by increased Main Streets intensities.
2. Contribution to Calgary Growth Strategies' Industry/City Work Plan: Established Areas Initiative. This initiative undertakes cross-departmental actions to reduce barriers to redevelopment. This includes, but is not limited to:
 - Piloting communities and specific development sites to identify, plan, and prioritize technical requirements to enable redevelopment.
 - Review underutilized road rights-of-way on key corridors to increase flexibility of use of space.
 - Development of funding tools to support developed areas investment.
 - Review of the Centre City Levy program within updated legislative framework.
 - Work towards a strategic growth plan for the established areas.
3. Continued One Calgary alignment. This will ensure that any necessary capital investments have ample time to be budgeted for and aligned with other Business Unit needs, including streetscape master plan improvements, road surface improvements, potential Water Resources upgrades, etc.

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4. An ongoing commitment from Water Resources to support redevelopment where applicant-triggered infrastructure upgrades may create opportunities to improve utility networks in ways that benefit a larger set of redevelopment sites.

Stakeholder Engagement, Research and Communication

The communities that abut The Plan area (Forest Lawn, Albert Park, Radisson Heights, Southview and Penbrooke Meadows) were provided opportunities to comment on The Plan. A two-phase engagement and communications strategy was developed in order to be wide-reaching in the affected communities and to facilitate meaningful opportunities for engagement both in-person and online. The Engage Spectrum level for this project is Listen and Learn which is defined as “We will listen to stakeholders and learn about their plans, views, issues, concerns, expectations and ideas.”

Stakeholders were asked to review the proposed revisions to The Plan and provide input. They were also asked to review the proposed land use districts and consider the balance of commercial and residential uses. Additionally, stakeholders were able to share any other ideas or comments on the information provided that were taken in consideration in The Plan. See Attachment 4 for 2018 Final Engagement Summary Report.

Engagement and public meeting

Due to the continuous and iterative nature of The Plan work, community and stakeholder engagement happened over multiple years through multiple meetings. In the period between 2007 and 2010, the Corridor Concept Plan was completed with significant engagement.

As result of Council direction in 2016 September to update the Corridor Concept Plan to become a statutory document, several meetings and open houses were offered for landowners, stakeholders and residents to provide their feedback and input on The Plan and on the conceptual land use amendment. Two information sessions were held, the first one in 2017 November and the second in 2018 January, with online opportunities to provide input at the same time as these in-person opportunities, followed by additional discussions with community associations, International Avenue BRZ and landowners as requested.

The 2017 November information session presented the first draft of The Plan as well as a model for the proposed land use amendment reflecting the scale of development ranges in the DAG, supported by examples of building forms and types. This new land use district pattern was vetted with residents and stakeholders and then refined after input from the second information session in 2018 January. The finalized land use pattern determined the required amendments to the *Albert Park/Radisson Heights ARP* and the *Forest Lawn/Forest Heights Hubalta ARP*. This proposal was mailed to all impacted property owners with specific details on the recommended land use redesignations for the plan area.

The majority of comments received from community residents support The Plan and the idea of improving the street, which would in turn encourage more businesses to locate along 17 Avenue SE and improve the area. However, concerns regarding traffic have been expressed, including flow, and vehicular and pedestrian safety.

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A total of 127 people attended both information session events held at the Forest Lawn Community Centre. In addition, approximately 390 pieces of feedback from in person and online opportunities were received. A summary of engagement events is listed below:

- November 2017 – Information Café. Gather public input on draft policy and review of proposed Developed Areas Guidebook building blocks.
- January 2018 – Open House. Share Information Café outcomes, draft policy plan, and gather public input on proposed rezoning.
- March 2018 – Meeting with Albert Park CA (APRHCA) to share engagement outcomes and the relevant Albert Park ARP amendments.
- March 2018 – Meeting with Forest Lawn CA (FLCA) to share engagement outcomes and the relevant Forest Lawn ARP amendments. (No concerns were raised at the time of the meeting and the Open House. Administration received no comments)
- April 2018– Meeting with Albert Park CA (APRHCA) to share the draft for the Albert Park ARP amendments. Both, the current and the amended Albert Park ARP versions were shared with the CA’s members as hard copies and electronically.
- September 2018 – Last meeting with Albert Park CA (APRHCA) to share the final draft for the Albert Park ARP amendments. See Attachment 6 for Albert Park Response

The project team has met three times with the International Avenue BRZ to discuss signage policies and the development of the updated ARP. The BRZ is generally in support of The Plan and associated amendments. See Attachment 5 for International Avenue BRZ Supporting Letter.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the ‘City, Town’ area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this corridor, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The MDP identifies the Plan area as an Urban Main Street Typology with emphasis on a multi-modal boulevard fronted by a mix of higher intensity residential and business uses that foster a vibrant street environment. The MDP establishes a minimum intensity threshold of 200 jobs and population per gross developable hectare. The proposed Plan has established a series of objectives and policies to implement the MDP goals for Urban Main Streets. Additionally, realization of this Plan will achieve the MDP’s objective for area growth.

The building blocks proposed as part of The Plan are based on those found in the DAG, Volume 2, Part 3 of the MDP. The proposed Plan has been organized in order to maintain consistency with the layout of the DAG.

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Calgary Transportation Plan (Statutory, 2009)

The CTP identifies 17 Avenue SE as an Urban Boulevard where highest priority should be given to walking, cycling and transit and where the “Complete Street” policies should be implemented. This is the main objective of the proposed Southeast 17 Corridor Transportation Plan that is represented in the Streetscape Section in the Corridor Concept Plan. Moreover, the Corridor Concept Plan has demonstrated the focus on transit, pedestrian and cyclists by emphasizing pedestrian-oriented land uses and numerous urban design solutions.

Albert Park/Radisson Heights Area Redevelopment Plan (Statutory, 1989)

Adopted by Council in 1980, the Albert Park/Radisson Heights ARP identifies 17 Avenue SE as growth and redevelopment areas, but does not provide up-to-date guidance for mixed-use and commercial development, and does not allow for complementary and supportive ground-oriented residential forms nearby.

The amendments proposed as part of this application align the ARP with the MDP, support the City’s overall vision for Albert Park/Radisson Heights urban and neighbourhood main streets, and provide more appropriate guidance for mixed-use, commercial, and residential intensification along and nearby 17 Avenue SE.

Forest Lawn/Forest Heights Hubalta Area Redevelopment Plan (Statutory, 1995)

Adopted by Council in 1995, the Forest Lawn ARP identifies 17 Avenue SE as growth and redevelopment areas, but does not provide up-to-date guidance for mixed-use and commercial development, and does not allow for complementary and supportive ground-oriented residential forms nearby.

The amendments proposed as part of this application align the ARP with the MDP, support the City’s overall vision for Forest Lawn urban and neighbourhood main streets, and provide more appropriate guidance for mixed-use, commercial, and residential intensification along and nearby 17 Avenue SE.

Social, Environmental, Economic (External)

The proposed statutory policy, along with the proposed land use districts, will guide development in The Plan area towards the creation a vibrant, transit-focused, liveable and distinctive mixed use community.

Social

The Plan will provide opportunities for a broad range of housing and employment options in a mixed use context to allow those that live and work in the area to meet some of their daily needs without the use a personal vehicle.

Environmental

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Environmental Sustainability

An Environmental Site Assessment was not required. However, the original Corridor Concept Plan objectives and policies incorporate sustainable development principles and address the social, economic, environmental and smart growth impacts on the communities. Furthermore, efficient use of land and resources is a key aspect of working toward environmental sustainability. The approach to concentrate jobs and population on Main Streets that are well served by public transit, and where people can walk or cycle to meet many of their daily needs results in reduced dependence on the private automobile and greater mobility choice. Reduced automobile dependence also leads to corresponding reductions in greenhouse gas emissions, as 36 percent of all emissions in Calgary come from vehicular transportation. More efficient use of land for development also contributes to reducing impacts on the environment and natural systems both within the city and regionally.

Environmental Sustainability

Efficient use of land and resources is a key aspect of working toward environmental sustainability. The approach to concentrate jobs and population on Main Streets that are well served by public transit, and where people can walk or cycle to meet many of their daily needs results in reduced dependence on the private automobile and greater mobility choice. Reduced automobile dependence also leads to corresponding reductions in greenhouse gas emissions, as 36 percent of all emissions in Calgary come from vehicular transportation. More efficient use of land for development also contributes to reducing impacts on the environment and natural systems both within the city and regionally.

Economic

Long term land use policy and City initiated land use district amendments represent a strategic approach to ensure an adequate supply of housing and employment space to keep pace with demands in the established communities. Supporting such supply increases in locations with existing and planned infrastructure is a key component in reducing the fiscal impact of growth, while also acting to attenuate pressure on housing affordability.

Financial Capacity

Current and Future Operating Budget:

Calgary Transit and Roads are providing operating budgets for 17 Avenue SE BRT transit facilities and the upgraded 17 Avenue SE roadway. Calgary Transit, Roads, Parks and Recreation, as part of normal business operations, monitor and determine future needs for expanding services levels. As these departments identify these new citizen needs, those departments will determine future capital budget requirements. However, no needed improvements were identified at this time.

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Current and Future Capital Budget:

The construction of the 17 Avenue SE BRT project provides a complete street upgrade for the full width of the road right of way with new sidewalks, intersection signals and transit facilities. This construction project is fully funded and set to begin operation in the fall of 2018. The Main Streets program is not proposing any new funding for 17 Avenue SE as the new road cross section will support Council's policy objectives and new private developments for years to come.

As identified by other departments, any future transit, parks or road upgrades will be provided within the framework of the proposed Plan. Individual departments will determine future capital budget requirements and provide this infrastructure consistent with policies in the Plan.

Risk Assessment

Citizen and stakeholder interest and potential monetary investment in new development is very high for the Main Street areas. This current opportunity will be leveraged with City of Calgary actions to facilitate growth in identified strategic areas such as the 17 Avenue SE. Not proceeding with guiding policy and land use redesignations could slow momentum created with many stakeholders and potentially develop mistrust with citizens and the development industry.

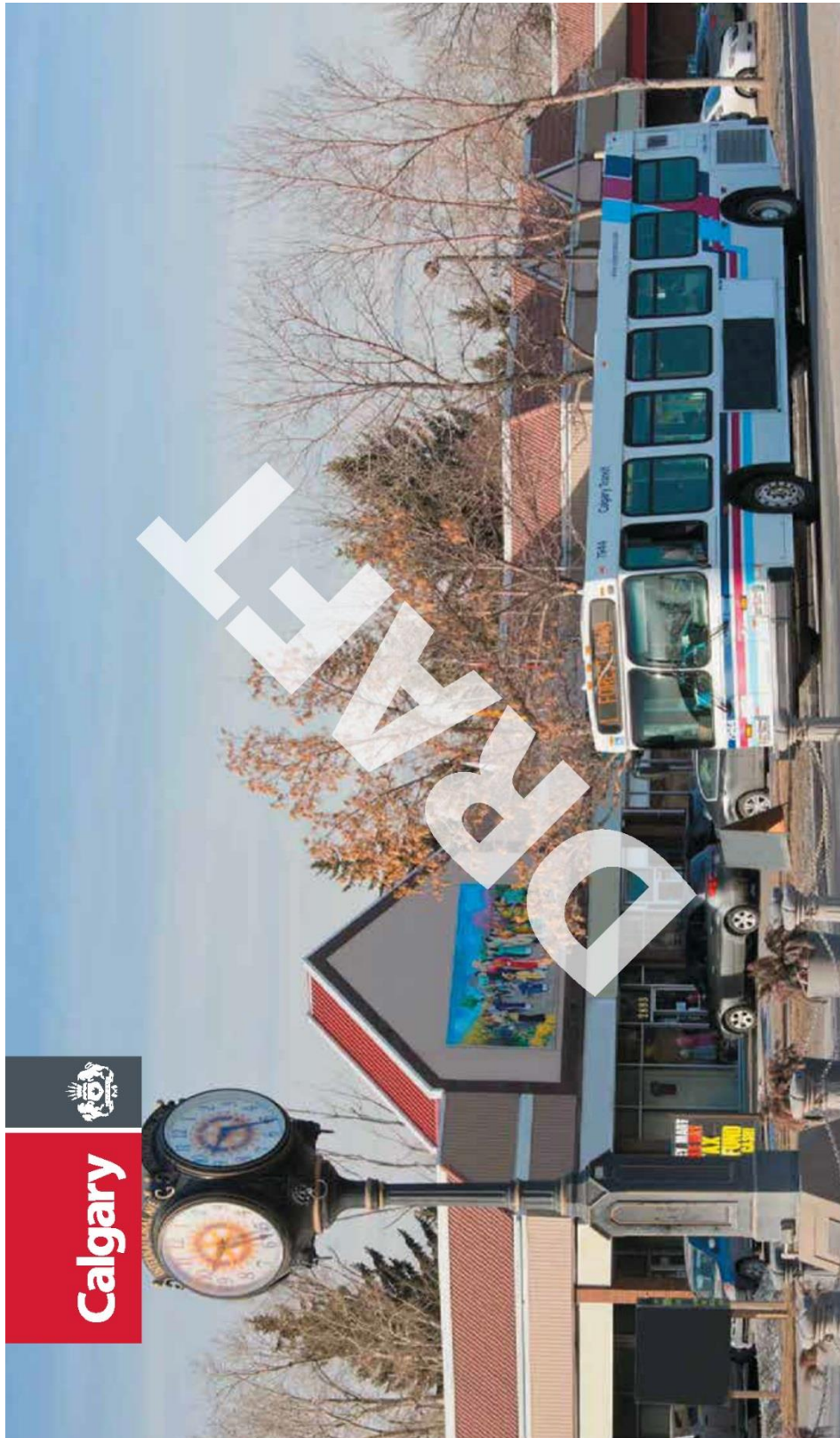
REASONS FOR RECOMMENDATIONS:

Administration recommends that Calgary Planning Commission receive this report for information as an opportunity for consultation. Administration will provide input from CPC as an attachment to the report going to Standing Policy Committee on Planning and Urban Development on 2018 November 05 for Council's consideration.

ATTACHMENTS

1. Proposed *International Avenue Area Redevelopment Plan*
2. *Forest Lawn/Forest Heights Hubalta Area Redevelopment Plan*
3. *Albert Park/Radisson Heights Area Redevelopment Plan*
4. Final Engagement Summary Report 2017/18
5. International Avenue BRZ Supporting Letter
6. Albert Park Community Association Supporting Letter

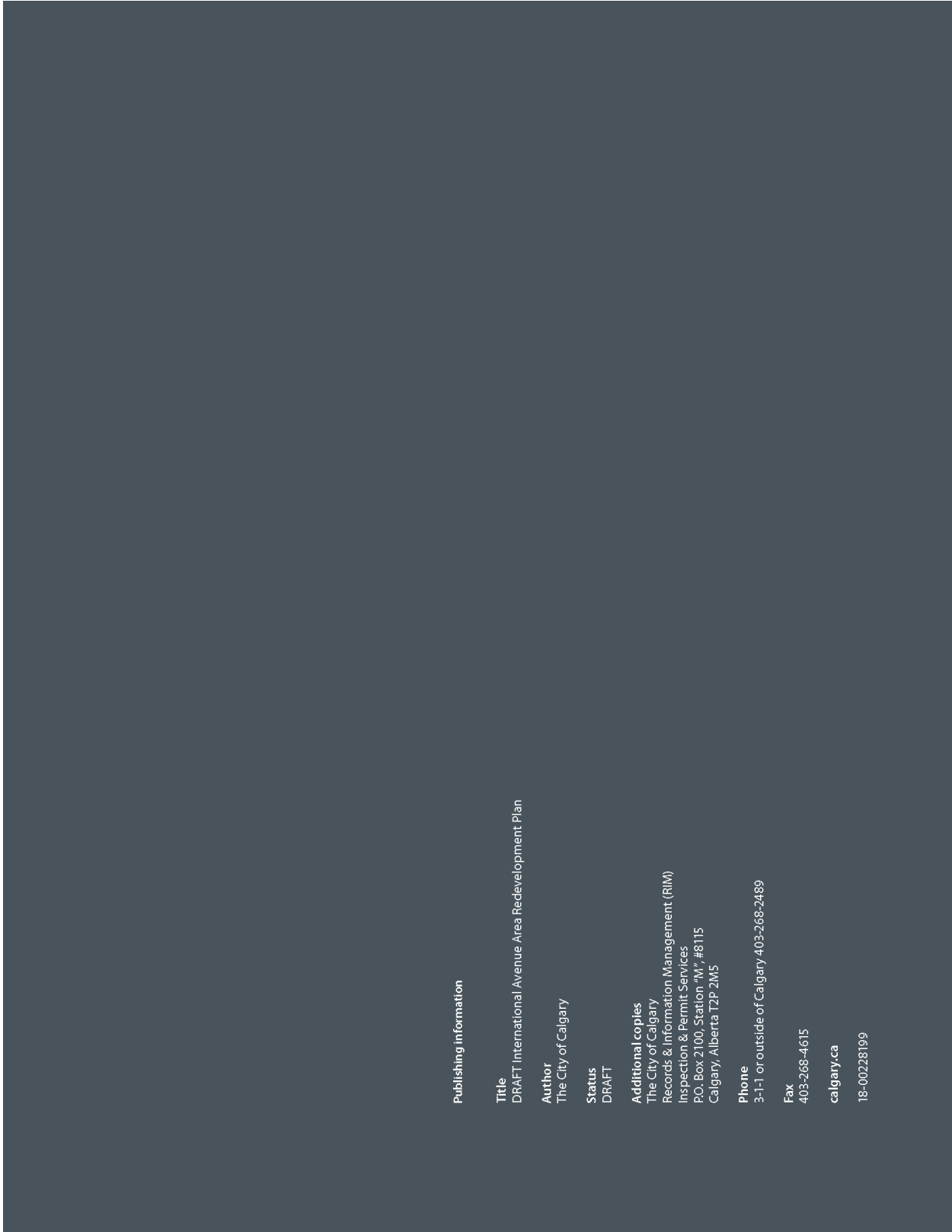
Proposed International Avenue Area Redevelopment Plan



International Avenue

Area Redevelopment Plan

Proposed International Avenue Area Redevelopment Plan



Publishing information

Title
DRAFT International Avenue Area Redevelopment Plan

Author
The City of Calgary

Status
DRAFT

Additional copies
The City of Calgary
Records & Information Management (RIM)
Inspection & Permit Services
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Proposed International Avenue Area Redevelopment Plan

**International Avenue
Area Redevelopment Plan**

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Proposed International Avenue Area Redevelopment Plan



1 International Avenue's past and present

The International Avenue Area Redevelopment Plan (ARP) is created to guide development in 17 Avenue S.E. focused around the Southeast Bus Rapid Transit (BRT) stations (Transit Hubs). The policies found within this document provide the framework to establish a Transit Oriented Development (TOD) Main Street that will actualize the core ideas and vision of this ARP.

Proposed International Avenue Area Redevelopment Plan

1.1 International Avenue and Greater Forest Lawn history

Calgary is located on the traditional territories of the people of Treaty 7, including: the Blackfoot Confederacy made up of the Siksika, Piikani, and Kainai First Nations; the Stoney Nakoda First Nations, comprised of the Chiniki, Bears paw, and Wesley First Nations; and the Tsuut'ina First Nation. The city of Calgary is also homeland to the historic Northwest Métis and to Métis Nation of Alberta, Region 3.

Many Indigenous names for the place we now call Calgary highlight the confluence of the Bow and Elbow rivers, where the two meet to flow as one. These names refer to the “elbow” shape of the confluence: the Blackfoot call Calgary Moh'kintstiss, the Stoney Nakoda use Wichispa, and the Tsuut'ina say Gut'sit'si.

For thousands of years the confluence has been a hub of activity where cultures converge like the two rivers; a gathering place for healing ceremonies and trading



Figure 1 | Built circa 1913–14, the Grand Trunk Pacific Railway station at Hubalta later became a Canadian National Railways station. Passenger service to Hubalta continued into the early 1950s. Glenbow Archives NA-2025-1

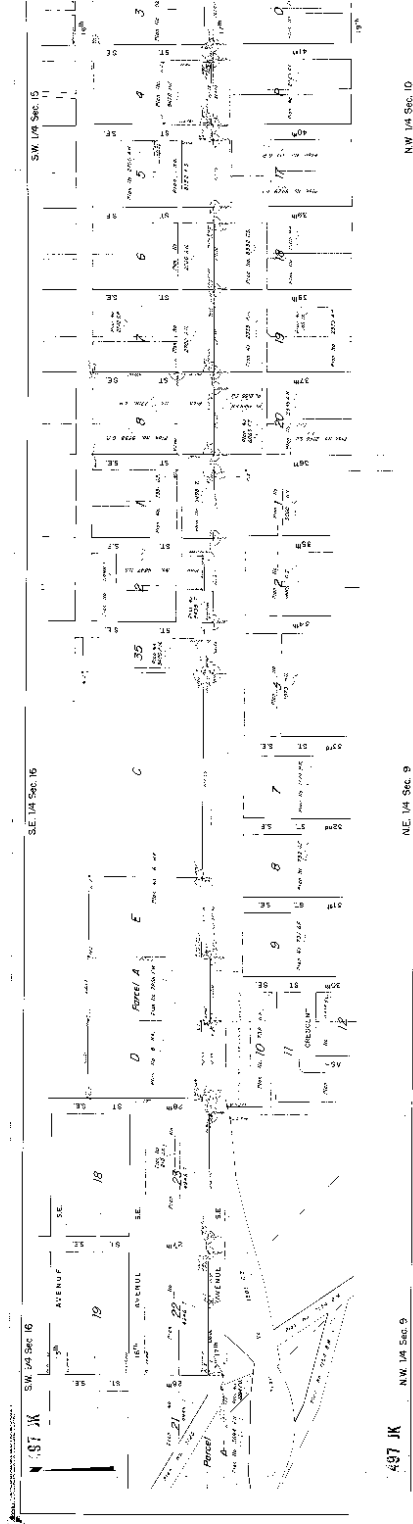


Figure 2 | This 1961 map of Plan 497JK shows the re-establishment of monuments on the north side of 17 Avenue SE. Courtesy Government of Alberta, Ministry of Service Alberta

Proposed International Avenue Area Redevelopment Plan

among nations. Indigenous peoples have been the stewards of this place since time immemorial.

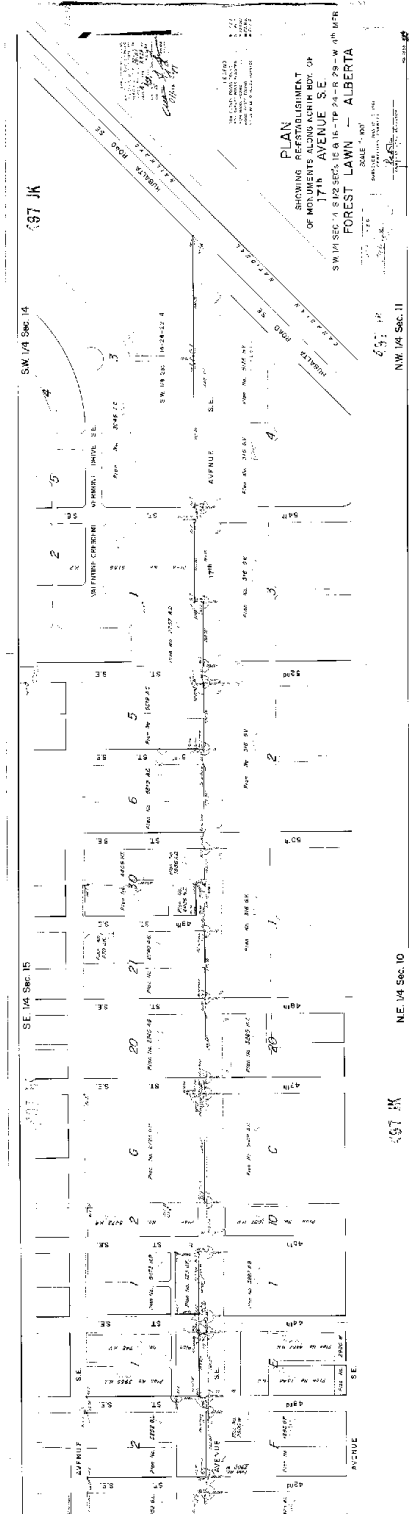
The confluence was chosen by the Northwest Mounted Police (NWMP) as the site for a new fort, which opened in 1875. Though the NWMP post was originally called "Fort Brisebois" after its captain, it was renamed Fort Calgary in 1876 after Calgary House, a castle on the Isle of Mull, Scotland. Calgary was incorporated as a town in 1884 and as a city in 1894. The plan area lay outside of the city limits until 1961.

The line traced by 17 Avenue SE originated as a section line when this area was surveyed in 1883. Farmers acquired land in the plan area as homesteads or by purchase from the CPR. Beginning in 1896, Ottawa expedited agricultural settlement in western Canada, largely through immigration from Europe and the United States.

This large population influx contributed to Calgary's rapid growth early in the 20th century and to a real estate boom that prompted land speculation. Additional local factors included

the creation of Chestermere Lake through irrigation activity and the promotion of Chestermere as a resort destination. In 1911, the Chestermere Calgary Suburban Railway Co. proposed building an inter-urban railway line, presumably through the plan area. The project advanced as far as placement of trolley poles and stockpiling of railway ties, but the line was never built. This evidently informed the myth that promoters of the plan area had tricked investors by placing railway ties and claiming, falsely, that a streetcar line would soon connect their developments with Calgary. Another local factor was the expected routing of the Grand Trunk Pacific Railway line into Calgary through Hubalta, as the proposed railway townsite was named in 1910. The line was constructed in 1913, and passenger service to Hubalta and Calgary stations began in 1914.

Between 1906 and 1912, promoters registered subdivisions like Albert Park (evidently named for its owner, Albert J. Smyth), Forest Lawn (named by owner C.T. Gilbert, reportedly for the Los Angeles cemetery), Gilbert Estates, Golden Rose Park, Hubalta Park, Mountain View, and Oughton Estates.



Proposed International Avenue Area Redevelopment Plan

Another subdivision, Victoria Square, emerged as an industrial area. Calgary's real estate boom ended in 1913, and the subdivisions evolved as the unincorporated hamlets of Albert Park, Forest Lawn, and Hubalta, all located within and administered by the Municipal District of Shepard No. 220. They were situated between Calgary's eastern limit and the CPR tracks to the east. Low taxation and living costs attracted residents, although the hamlets lacked most municipal infrastructure. By the early years of the Great Depression, a substantial portion of the municipality's population, and most of its relief recipients, lived in the hamlets. In 1934, in an apparent effort to ease the municipality's relief burden, the province withdrew the hamlets and reorganized them into two villages, Albert Park and Forest Lawn. A single appointed official administered both. The villages were reorganized in 1935 as the Village of Forest Lawn. An elected three-member council replaced the official administrator in 1946, and in 1952, Forest Lawn was re-incorporated as a town. Calgary annexed Forest Lawn in 1961. The plan area spans the city neighbourhoods of Albert Park, Forest Lawn, Forest Lawn Industrial, Penbrooke Meadows, and Southview. Seventeenth Avenue developed as part of Highway #1 and as the main street of Albert Park, Forest Lawn, and Hubalta. Each of these settlements had a post office and general store, presumably along 17 Avenue. In the 1950s, commercial, retail, and other development along 17 Avenue increased in tandem with municipal improvements and population increase. Beginning in 1953, residents organized an annual parade along 17 Avenue. In 1956, Highway #1 shifted northward as part of the emerging Trans-Canada Highway, and 17 Avenue became Highway #1A. That year, Forest Lawn adjusted its street numbers to align with Calgary's, and addresses along the avenues (including 17 Avenue) were consequently re-numbered. Town police started patrolling speed on 17 Avenue in 1957, and the first traffic light was installed at the 36 Street SE intersection in 1959. A horse-and-buggy service, followed by a motorized service, briefly provided public transportation to Calgary in the early 1920s. The privately-owned Forest Lawn Bus Service commenced operation in 1961, and the Calgary Transit System was extended to Forest Lawn following annexation.

The first non-Indigenous settlers were largely Anglo-Saxon, but the area soon attracted newcomers from a variety of European backgrounds, many of them Polish and Ukrainian. In 1930, a family from India settled in Forest Lawn. Calgary's first mosque opened in the former St. Luke's Anglican Church (3027-17 Avenue SE) in 1960. By the 1990s, the area's increased population comprised immigrants from countries around the world. Businesses and services on 17 Avenue began to reflect a wide cultural diversity.

In 1993, area businesses formed the International Avenue Business Revitalization Zone (BRZ) and fostered a new identity for 17 Avenue as International Avenue. Along with its advocacy for the business district, the BRZ has contributed to livability, vitality, and safety through special events (such as "Around the World in 35 Blocks" food tours), cultural murals, public art, site amenities and trees), and engagement and advocacy on planning and transportation issues. This advocacy and engagement contributed to reversing an approved plan from the 1970s (which called for the widening of 17 Avenue, closure of intersecting streets, and an increased speed limit) and the development and approval of the Southeast 17 Main Street Plan.



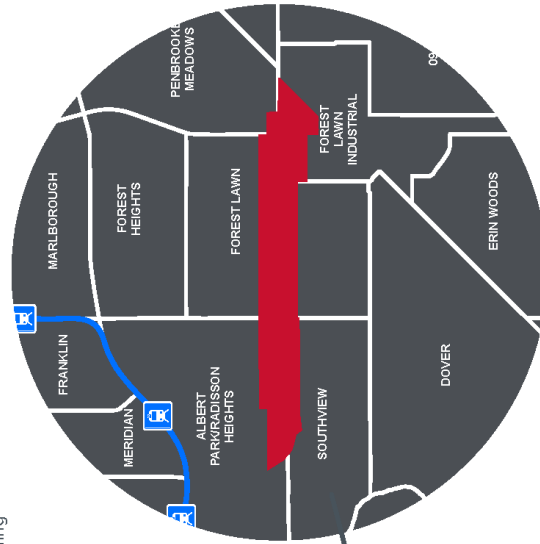
Figure 3 | Forest Lawn police and fire department buildings and vehicles, 17 Avenue SE, 1958. Glenbow Archives NA-5093-567

Proposed International Avenue Area Redevelopment Plan

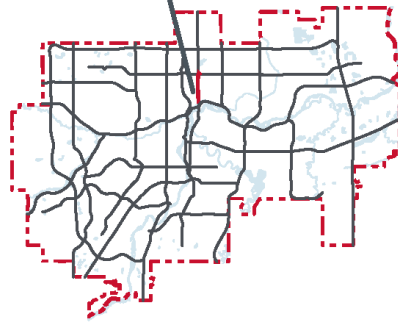
1.2 Main Street context

International Avenue is approximately 5 kilometres from downtown Calgary (Map 1: Plan context and Map 2: Plan location) and is one of the major corridors that provides for east/west commuting in and out of the city. The plan area is bounded by Barlow Trail/26 Street S.E. on the west and the Canadian National Railway line on the east, and generally includes one full city block on each side of 17 Avenue S.E.

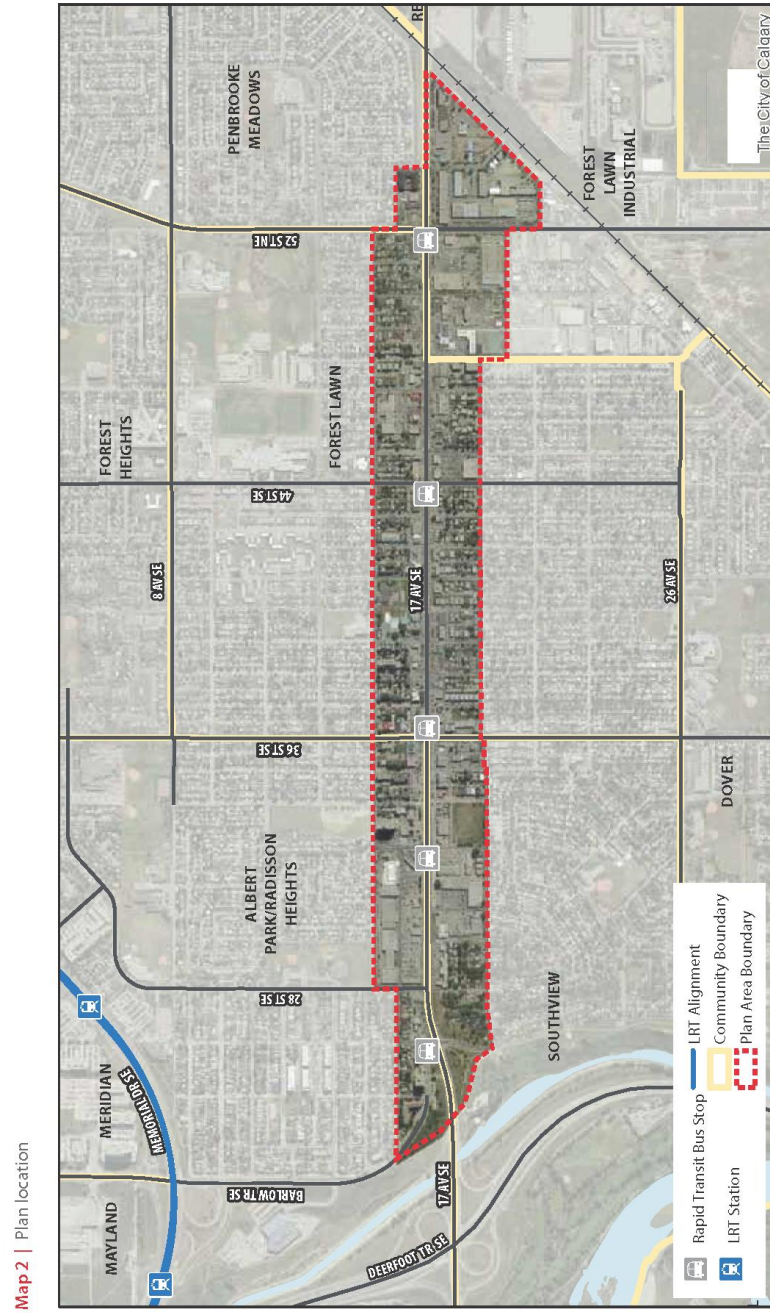
International Avenue serves Forest Lawn and the surrounding communities including Southview, Albert Park/Radisson Heights, Penbrooke Meadows, Forest Lawn Industrial, Red Carpet/Mountain View and Applewood Park. This region is known as the Greater Forest Lawn area.



Map 1 | Plan context



Proposed International Avenue Area Redevelopment Plan



Proposed International Avenue Area Redevelopment Plan



2 International Avenue's future

2.1 Vision and core ideas

The vision represents the community values as identified through various public engagements. The core ideas provide guidance for implementation planners, community representatives, local businesses, property developers and citizens when reviewing development permits and land use proposals. In evaluating the appropriateness of a land use or development permit application, the core ideas should be used in concert with the land use concept plan, the Municipal Development Plan, the Developed Areas Guidebook and the applicable Local Area Plans.

Proposed International Avenue Area Redevelopment Plan

Vision

International Avenue is the core of the greater Forest Lawn community that functions as a safe, green and walkable multi-modal urban boulevard, a shopping avenue, a job hub, a place to live and where the cultural diversity of the area is celebrated.



Before



After

Figure 4 | Example of the vision 17 Avenue S.E. at 52 Street S.E., looking west – From vehicle-oriented street today to multi-modal boulevard supporting a retail-based street (Source: Plan It! Calgary 17 Avenue S.E. case study charrette report)

Proposed International Avenue Area Redevelopment Plan

Core ideas

The following core ideas are essential for redevelopment across the plan area.

- 2.1.1 Create an attractive, distinctive and multicultural community**
 - a. Provide public gathering places that are centred on cultural activities, public art and building community.
 - b. Incorporate interesting, high quality architectural designs and public art that reflects the community values and identity as International Avenue.
- 2.1.2 Create memorable and inviting public spaces**
 - a. Celebrate local parks and improved connectivity between open spaces.
- 2.1.3 Create a multi-modal boulevard**
 - a. Support a variety of transportation options, with particular emphasis on walking, cycling and transit.
 - b. Comprehensively manage traffic and parking.
- 2.1.4 Promote a vibrant mixed use commercial Main Street**
 - a. Provide a framework for the ongoing redevelopment of neighbourhoods within the community to provide the services and amenities that meet the needs of current and future residents.
- 2.1.5 Respect and enhance the existing historical retail street character**
 - a. Identify and preserve the existing Main Street Village character streetscape, open spaces and buildings or building elements, which have historical value.
- 2.1.6 Increase in housing and promote housing diversity**
 - a. Promote retail/multi-residential mixed-use development along the International Avenue wherever practical to create a vibrant retail environment and to increase housing in close proximity to transit and other services.
 - b. Promote a range of housing types to suit all income levels, ages, lifestyles, and family needs.
- 2.1.7 Strategically directed intensification**
 - a. Facilitate redevelopment in strategic locations to provide a mix of housing choices and places for businesses to locate.
 - b. Auto-oriented uses and those that do not generate pedestrian traffic (such as self storage) should not be located in close proximity of transit hubs.
- 2.1.8 Promote safe and walkable neighborhoods.**
 - Improve the aesthetics, safety and public function of rear lanes.
- 2.1.9 Preserve and enhance biodiversity and green spaces**
 - a. Celebrate and improve the connectivity to the two major natural areas (Elliston Park and the Bow Valley).
 - b. Protect and improve existing planted boulevards along the Main Street and residential streets.
- 2.1.10 Promote sustainable building design and innovation**
 - a. Promote energy efficiency, water efficiency and waste minimization solutions through the use of sustainable building design

Proposed International Avenue Area Redevelopment Plan

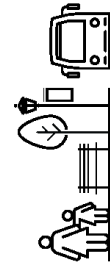
2.2 Land use concept

The land use concept is shown on Map 3: Land use concept, which depicts the general location of future types of land use throughout the plan area. It also illustrates the general location of the character zones and relationship between land use building blocks, the transportation network and open spaces. Refinements to the exact location may be made without an amendment to this ARP as part of a land use amendment application, provided they achieve the vision and core ideas of the plan.

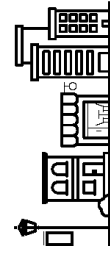
Building blocks found in this plan are based on those established in the Developed Areas Guidebook, Volume 2, Part 3 of the Municipal Development Plan. Each building block has a range of land use districts that can be applied during the land use amendment stage. Building blocks along the International Avenue are intended to meet the minimum intensity of the urban Main Street as defined in Volume 1 of the Municipal Development Plan.

Development in the plan area will include a variety of land uses, building types and vertical and horizontal mixed uses focused around 17 Avenue S.E. This plan envisions International Avenue as a multi-modal urban boulevard with three distinctive character zones: a gateway (Western Gateway) to welcome visitors and mark the entrance, a vibrant central shopping street and activity hub (Central Boulevard), and a landscaped corridor (Eastern Parkway) extending to Stoney Trail at the eastern city limit. Different streetscapes, land uses, building heights and design criteria are applied to help achieve the specific objectives of each character zone.

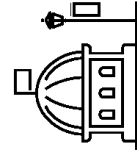
Furthermore, the plan identifies the 36 Street S.E. and 52 Street S.E. intersections as the communities' major transit hubs. These multi-functional hubs provide opportunities for higher density developments and connectivity beyond the boundary of the plan area. They also act as the transitions between the three character zones. Due to the far distance between the two identified major transit hubs, secondary transit hubs are also identified, (28 Street, 33 Street, and 44 Street S.E.). In addition to serving as transit hubs, they provide concentrated services at a neighbourhood level.



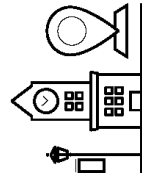
Vibrant public realm



Variety of retail and small business

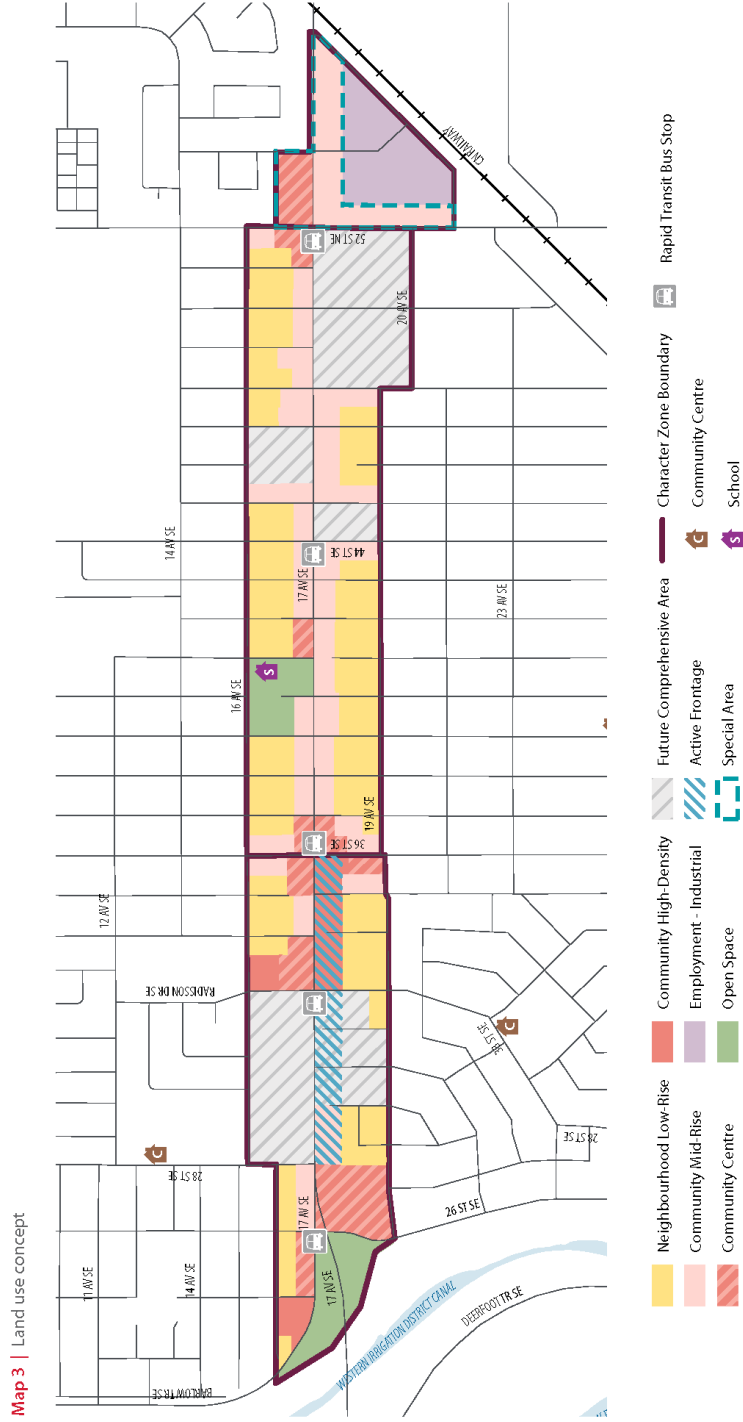


Unique character



Create a destination

Proposed International Avenue Area Redevelopment Plan



Map 3 | Land use concept

Proposed International Avenue Area Redevelopment Plan

2.3 Land use concept elements (building blocks)

The following list provides a quick reference to the land use building blocks and elements shown on Map 3: Land use concept. More extensive descriptions and policies pertaining to each building block are provided in the Developed Areas Guidebook. This ARP outlines specific ways in which these building blocks apply in the plan area.

Community – High-Density

The Community – High-Density building block is located around the main transit node of 36 Street S.E. and in a couple of sites where higher density exists. This block is intended to facilitate high intensity residential and mixed-use development with large scale residential and non-residential uses.

Community – Centre

Community – Centre building block is located throughout the plan area fronting 17 Avenue S.E. It is characterized by a fine grain network of streets, wider sidewalks to encourage pedestrians and active streets and buildings set close to frontages to support higher levels of commercial and residential intensity.

Community – Mid-Rise

The Community – Mid-Rise building block is located throughout the plan area fronting 17 Avenue S.E. It is intended to accommodate a mix of mid-rise buildings that contain residential and commercial uses, creating an active, vibrant main street. Other uses that may be considered appropriate within the Community – Mid-Rise area include schools, daycares, places of worship and civic uses.

Active frontage

These policies apply specifically to the area classified as Main Street Village it references the south portion of 17 Avenue S.E. between 28 Street S.E. and 35 Street S.E., which is recognized as a distinct character area. This designation indicates the requirement for high-quality building and public realm

design that will enhance the pedestrian experience and contribute to the vibrancy of the Main Street Village area. Buildings along these streets must provide commercial uses at grade, direct entries from ground floor units to the public sidewalk and enhanced transparency and other design elements outlined in the Developed Areas Guidebook.

Special areas

These policies apply specifically to the areas classified as Special Area where auto-service uses or drive-throughs are allowed. Auto-service use is a general term to describe those uses that primarily provide a service to automobiles, usually involve minimal pedestrian activity, and require that the site and building be designed to provide convenient access by vehicle. By including auto-service uses, the Special Area provides for a wider range of uses and acknowledge the existence of and continuing need for local auto-oriented services.

Neighbourhood – Low-Rise

Neighbourhood – Low-Rise areas are intended to provide compact, low-rise development (up to 4 storeys). They are proposed at the edge of much of the Main Street as a transitional use between the mixed use development fronting on 17 Avenue S.E. and the low density residential areas in the communities to the north and south.

Future Comprehensive Area

As shown on Map 3: Land use concept, Future Comprehensive Area apply to the existing large parcels along the Main Street currently developed with one storey large format developments that have extensive surface parking areas abutting 17 Avenue S.E. and the flanking streets.

Due to the size and prominent location of these sites within the Main Street, at the time of redevelopment, applications should include a comprehensive plan that meets and indicates the policies of the Developed Areas Guidebook to help achieve the vision and goals of this plan.

Proposed International Avenue Area Redevelopment Plan

Employment – Industrial

The Employment – Industrial building block is located in the eastern end of the plan area. It provides for a broad variety of industrial uses and intensities, protecting industrial land for primarily industrial uses, while allowing strategic non-industrial uses to support employees in the area.

Table 1 | International Avenue Snapshot

Area	116.5 hectares	113.5 hectares gross developable area	2.9 hectares open space ¹
2017 Civic Census data	2,726 residents	1,435 dwelling units	3,858 jobs (year 2013)
Projection²	9,295 residents (year 2041)	4,866 dwelling units (year 2041)	6,971 jobs (year 2041)
Public amenities	Cultural/civic spaces <ul style="list-style-type: none"> • One school, Holy Trinity • Forest Lawn Outdoor Pool 	Open space and parks <ul style="list-style-type: none"> • Irrigation Canal and Bow River to the west. • Smaller pocket park between 30 Street S.E. and 31 Street S.E. 	Improved connectivity <ul style="list-style-type: none"> • Southeast BRT • Regional pathways • Complete streets

¹ Open space area represents the combined total of local and regional open space within the plan area in hectares.

² Jobs and population are estimates only and represent a population focused development scenario which assumes 20% of the maximum allowable floor area ratio (FAR) as commercial development and residential development up to the total maximum.

Proposed International Avenue Area Redevelopment Plan



Proposed International Avenue Area Redevelopment Plan



3 Land use and built form

This section addresses the mix, location and intensity of land uses; their relationship to the public realm; and the amenities required to ensure that development is consistent with the overall vision and core ideas of the plan. The International Avenue ARP includes three unique policy areas, each contributing to the creation of a TOD Main Street on 17 Avenue S.E.

Proposed International Avenue Area Redevelopment Plan

3.1 General policies

This section outlines policies that apply to all areas within the International Avenue ARP and must be read in conjunction with the policies for each specific policy area.

Policy

- 3.1.1** Land use redesignations should be consistent with the land use classifications shown on Map 3: Land use concept. Policies associated with these categories are contained within this plan and the Developed Areas Guidebook.
- 3.1.2** Unless otherwise specified, auto-service uses, drive-throughs and large-scale regional development ("large format development") are prohibited.
- 3.1.3** Surface parking in front of buildings and vehicular access directly from 17 Avenue S.E., is prohibited, except where traditional service roads with angle or parallel parking are introduced as part of comprehensive redevelopment of a block.
- 3.1.4** Buildings should be designed to form a consistent and strong edge to streets and public places, define the spatial and visual quality and support the gathering functions at street corners.
- 3.1.5** The siting of buildings should respond and enhance the unique characteristics of the site to create unique architecture and take advantage of the location overlooking Calgary's city centre and the mountains.
- 3.1.6** The maximum street frontage for lobbies located on the ground floor is 7.5 metres.
- 3.1.7** Comprehensive site layouts and phasing for larger site developments shall be provided to ensure sufficient landscaping, at-grade level amenity areas and good pedestrian connectivity to sidewalks or other public realm.

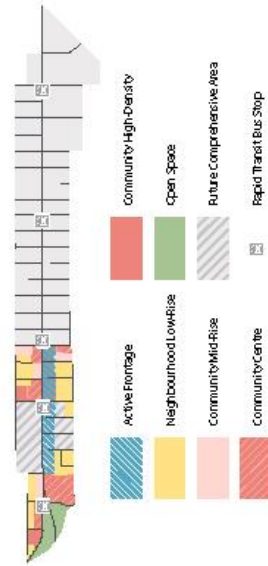
- 3.1.8** Consolidation of small parcels to help achieve height or density objectives is encouraged.
- 3.1.9** The following policies apply to the commercial development:
 - a. Large format retail/commercial establishments should not be allowed.
 - b. Commercial expansion into residentially-designated land should be designed in a manner which is responsive to the local context. Commercial development or expansion of commercial development should consider the following policies:
 - i. land for the sole purpose of additional standalone surface parking is prohibited;
 - ii. the required side and rear yard setbacks should not be relaxed; and
 - iii. where commercial uses are adjacent to residential uses without an intervening lane, a 5 metre landscaped buffer is required.
 - iv. Ensure that the design of the rear façade of commercial and mixed-use buildings is sympathetic to adjacent residential uses on the opposite side of the rear lane and uses materials and details of a standard similar to the front façade of the building.
 - c. Encourage consolidation of existing shallow parcels fronting 17 Avenue S.E. with the parcels at the rear to provide larger parcels that enable larger development to occur to achieve the recommended building heights and density targets in the Plan Area.
 - d. A mix of small-scale retail commercial development and residential uses are encouraged along the ground floor. Ground floor units should be flexibly designed to be able to accommodate both commercial and residential uses.

Proposed International Avenue Area Redevelopment Plan

- e. Historic industrial uses in commercial areas should be allowed to remain, but not to expand due to industrial impacts on residential uses (e.g., lighting, noise and traffic).
 - f. The required separation distance in the land use bylaw for uses such as liquor store, cannabis store, pawn shops and pay day loans, should not be relaxed and additional uses of this kind are discouraged. Any two of the uses in this policy are not allowed within the same development.
 - g. If existing gas station uses cease operations and not immediately redevelop, the demolition permit should require site reclamation and landscaping to the satisfaction of the approving authority.
 - h. Unless otherwise stated, development on the north side of 17 Avenue S.E. should not shadow abutting residential uses at 12:00 noon (M.S.T.) on September 21.
- 3.1.10** The following policies apply to the large development on the Future Comprehensive Plan Areas:
- a. Where large parcels are proposed for mixed-use redevelopment, the proposed development should include two or more buildings.
 - b. Where developments are in phases, ensure the small-scaled buildings along 17 Avenue S.E. are developed in the first phase or phases.
 - c. For lands fronting on 17 Avenue S.E., ensure any new subdivision is configured to accommodate future development that is oriented to 17 Avenue S.E. and provides for the continuity of the 'street wall' along the 17 Avenue S.E.
- d. In order to prevent the creation of a large surface parking lot adjacent to 17 Avenue S.E., development of the site will be in accordance with a comprehensive site plan. The proposed site plan should include a phased development permit to ensure the timely construction of the commercial/retail buildings. To create a pedestrian scaled street wall along the 17 Avenue S.E. right-of-way and, where feasible, pocket parks and pedestrian linkage oriented to 17 Avenue S.E. should be included in early phases of the development and should be following the construction of the primary building on the site.
 - e. Redevelopment for large-scale commercial uses will be allowed but only where the street frontage, particularly 17 Avenue S.E., is redeveloped for smaller scale buildings that are designed to accommodate more locally oriented uses. Creative concepts for site layout, such as a courtyard concept or pedestrian mall are encouraged.

Proposed International Avenue Area Redevelopment Plan

3.2 Western Gateway zone (from 26 Street S.E. to 36 Street S.E.)



The Western Gateway zone is a gateway district with strong visual impact that evokes a sense of arrival to International Avenue. Located between Barlow Trail and 26 Street S.E. at the west end of the 17 Avenue S.E. and 36 Street S.E., and from 15 to 19 Avenue S.E. the Western Gateway zone will welcomes visitors, reduces traffic speed and visually defines the entranceway through building design and a pedestrian-friendly streetscape.

The Western Gateway zone contains the 28 and 36 Street S.E. BRT Stations, and also includes the Main Street Village which is recognized as a distinct character area with distinctive building designs and streetscape features. The Main Street Village area features specialty shops, a linear park and a mixture of diverse but compatible land uses and building heights.

Objectives

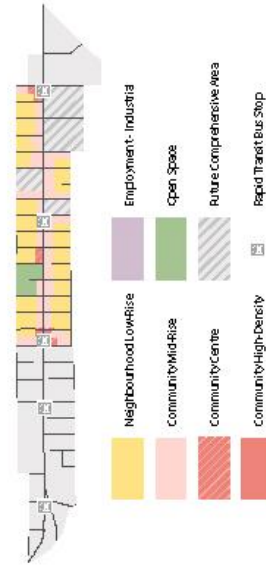
1. Create a strong gateway with landmark buildings.
2. An enhanced Main Street Village, preserving existing streetscape features (e.g., parallel parking and a service road) and reinforcing the village character by ensuring appropriate land uses and building designs.
3. Activate the commercial frontages to create a vibrant and safe pedestrian environment and contribute to activity on the street throughout the day and evening.
4. Provide good pedestrian connections within and through large parcel development.

Proposed International Avenue Area Redevelopment Plan

5. Encourage design and site layout that contributes to the character and function of International Avenue.
6. Improve pedestrian crossings of 17 Avenue S.E.
7. Create memorable and engaging public open spaces.
8. Encourage refurbishment or upgrading of existing retail buildings and façades.
- Composition**
- 3.2.1 As shown on Map 3: Land use concept, the Western Gateway zone consists of:
- Areas of Community – High-Density development
 - Areas of Community – Centre development
 - Areas of Community – Mid-Rise development
 - The Main Street Village Area identified as active frontage
 - Areas for Neighbourhood - Low-Rise development
 - Future comprehensive development area
 - Open space (see Section 4.2: Parks)
- 3.2.2 Site and building design in the Community – High-Density area shall conform to the policies of the Developed Areas Guidebook
- 3.2.3 Site and building design in the Community – Centre area shall conform to the policies of the Developed Areas Guidebook
- 3.2.4 Site and building design in the Community – Mid-Rise area shall conform to the policies of the Developed Areas Guidebook.
- 3.2.5 Buildings and sites located in the Main Street Village (Active Frontage) areas shall conform to the Developed Areas Guidebook, with the additional following policies:
- a. Redevelopment should maintain the character of the Main Street Village while revitalizing the sites.
 - b. Preserve the existing street park, parallel parking and service road only in the Main Street Village area.
 - c. Maintain continuous one to two storeys storefronts (podium style) with building façades that are designed to add scale, rhythm and character to the streetscape.
 - d. Portions of buildings higher than two storeys must be set back behind the retail frontages to maintain a sunlit, pedestrian-friendly environment and the low profile character along the Main Street.
 - e. Store frontages for individual use on the floor closest to grade is limited to 2 bays or 12 metres maximum width. If a larger area is needed the remainder of the commercial area should be located on the second floor or in the basement, or be wrapped behind.
- 3.2.6 Site and building design in the Neighbourhood – Low-Rise area of the Western Gateway zone shall conform to the policies of the Developed Areas Guidebook,
- 3.2.7 Site and building design in the Future Comprehensive Area of the Western Gateway zone shall conform to the policies of the Developed Areas Guidebook.

Proposed International Avenue Area Redevelopment Plan

3.3 Central Boulevard zone (from 36 Streets S.E. to 52 Street S.E.)



The Central Boulevard zone is intended to create a retail hub and a core area for community activities. Located between 36 Street S.E. and 52 Street S.E., and from 15 to 1920 Avenue S.E.

The Central Boulevard zone provides for a vibrant, active and pedestrian-oriented retail environment, creating places for social interaction and community engagement, and building a compact, dynamic and mixed-use urban centre.

The Central Boulevard zone contains 36 Street SE, 44 Street SE and 52 Street SE BRT stations and envisions a continuous street wall development. Developments in this zone features multiple small-scale, active storefronts and clustered restaurants with outdoor cafes that are facing a tree-lined boulevard and a number of compact street parks (street corner parks or pocket parks between buildings).

Objectives

1. Encourage land uses that support transit use, social interaction and activity in pedestrian areas.
2. Promote a multi-functional and pedestrian-oriented public realm.
3. Design development on large parcels in a manner that ensures pedestrian connectivity and is compatible with adjacent smaller scale development.
4. Design development to achieve a 4 to 6 storey continuous street wall.
5. Reflect the diversity and richness of the cultural background of the adjacent communities in the design of buildings and public spaces.

Proposed International Avenue Area Redevelopment Plan

Composition

3.3.1 As shown on Map 3: Land use concept, the Central Boulevard zone consists of:

- Community – High-Density development
- Community – Centre development
- Community – Mid-Rise development
- Neighbourhood – Low-Rise development
- Future comprehensive development area
- Open space (see Section 4.2: Parks)

3.3.2 Site and building design in the Community – High-Density area shall conform to the policies of the Developed Areas Guidebook.

3.3.3 Site and building design in the Community – Centre area shall conform to the policies of the Developed Areas Guidebook.

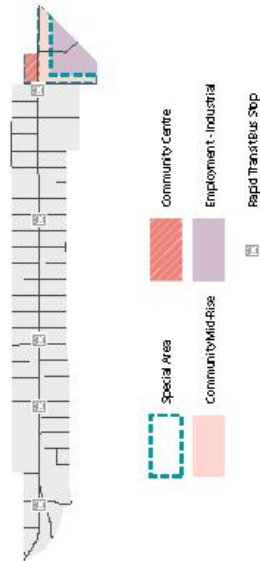
3.3.4 Site and building design in the Community – Mid-Rise area shall conform to the policies of the Developed Areas Guidebook with the additional following policy that requires a minimum of 1.8 metres setback from the property line for commercial buildings along 44 Street S.E. and 45 Street S.E.

3.3.5 Site and building design in the Neighbourhood – Low-Rise area shall conform to the policies of the Developed Areas Guidebook.

3.3.6 Site and building design in the future comprehensive area shall conform to the policies of the Developed Areas Guidebook.

Proposed International Avenue Area Redevelopment Plan

3.4 Eastern Parkway zone (from 52 Streets S.E. to Stoney Trail)



to Elliston Park and beyond. The area signalizes a decrease in the intensity of the main street.

The Eastern Parkway zone has the benefit of allowing for efficient regional commuting, providing good connectivity to Elliston Park, and accommodating variety of compatible land extensive uses (e.g., horticultural, storage buildings, community garden, urban agriculture, industrial parks).

Objectives

1. Maintain existing landscaped areas.
2. Explore opportunities to increase landscaped areas.
3. Establish good pedestrian and cyclist connections to Elliston Park.
4. Emphasize uses that contribute to improving energy efficiency, improving air quality, minimizing water and soil pollution and minimizing the ecological footprint.

The Eastern Parkway Zone represents the eastern terminus of the plan area with residential and commercial uses to the north and a mix of commercial and light industrial uses to the south and is located between 52 Street S.E. and the CN Rail line to the east, and from 16 to 21 Avenue S.E.

The Eastern Parkway Zone is well connected both east and west via 17 Avenue S.E., and through a linear park that connects to a regional pathway

Proposed International Avenue Area Redevelopment Plan

Composition

3.4.1 As shown on Map 3: Land Use Concept, the Eastern Parkway Zone consists of:

- Community – Centre development
- Community - Mid-Rise development
- Special area
- Employment – Industrial development

3.4.2 Site and building design in the Community – Centre area shall conform to the policies of the Developed Areas Guidebook.

3.4.3 Site and building design in the Community – Mid-Rise area shall conform to the policies of the Developed Areas Guidebook.

3.4.4 Site and building design in the Special Area shall conform to the policies of the Developed Areas Guidebook, with the additional following policies:

- a. Reduce the presence and amount of surface parking visible from 17 Avenue S.E.
- b. Reduce the number of vehicular access points to 17 Avenue S.E.
- c. Where driveways cannot be avoided, use of screening, alternative or shared vehicular access, or separating the parking area into smaller, enclosed compartments is encouraged.
- d. The development of a site for a single use, if allowed by the development authority, should be undertaken in a manner that permits or facilitates future conversion or redevelopment for a multi-use development.

3.4.5 Site and building design in the Employment – Industrial area shall conform to the policies of the Developed Areas Guidebook, with the additional following policies:

- a. Public amenity spaces for employees should be located in close proximity to the street to encourage activity.
- b. Parking shall be located to the rear of buildings, underground or away from the street with screening.
- c. Articulated façades and varying setbacks are encouraged to create outdoor amenity space for employees.
- d. New industrial development should incorporate building forms and features that will allow for flexibility and future transition to greener industries.

Proposed International Avenue Area Redevelopment Plan



4 Urban design

The urban design section provides a series of development strategies and design criteria to address the street/building interface and to create distinctive public gathering spaces. To complement the urban design policies (see: Appendix #1) a series of development strategies and design criteria have been created to demonstrate to landowners and developers possible solutions on how to address the street, to provide building interfaces and to create distinctive public gathering spaces. The plan has identified a variety of development scenarios, particularly related to lot or property sizes.

Proposed International Avenue Area Redevelopment Plan

4.1 Streetscape character and public realm

A well-defined and well-designed roadside area can reduce pedestrian-vehicle conflicts, promote pedestrian safety, prioritize pedestrian access to the BRT stops along the corridor, and create interesting public places and contribute to the success of businesses.

As future development occurs along 17 Avenue S.E., the goals of improved walkability and a pedestrian-friendly environment should be supported by signs oriented and scaled towards pedestrians. Careful attention should be paid to how signage impacts the appearance and feel of the corridor for all modes of transportation, while retaining the area's characteristic diversity and visual interest.

4.1.1 Boulevard/street furniture

- a. A flex zone on each side of the street is encouraged for a continuous tree-lined boulevard area and a range of street furniture. Wherever the right-of-way has sufficient width, linear green strips and pocket parks should be provided in strategic locations.
- b. As a multi-functional flex zone, the area could be designed to incorporate low impact development features and function as a utility corridor, if applicable. Also, the design should enhance the appearance of adjacent buildings and integrate pedestrian way-finding elements such as signage, banners, directional kiosks and pedestrian-scaled lighting.

4.1.2 Signage

Business signs along 17 Avenue S.E. play a significant role in defining a recognizable identity for the area, and are an important part of its visual character. Throughout the district's history, various signs have been created for many independently-run stores, restaurants and services – representing the abundance of small business, and the diverse range of cultural backgrounds in the area.

The growth of automobile commuters in the mid-twentieth century saw an increase in large, eye-catching signs to attract drivers, and this type of signage continues to be popular along 17 Avenue S.E. Certain historic freestanding and rooftop signs have become visual landmarks in their own right, and have particular significance to the area's heritage (see 3.6.7.2 Character signage).

4.1.2.1 General policies

- a. All commercial signage should be an appropriate height, size, location, orientation, illumination and be scaled for legibility by pedestrians.
- b. Signage should maintain visual interest within the context of a small-town feel; canopy or projecting signs scaled and oriented to pedestrians are deemed appropriate and encouraged.
- c. Signage should be integrated into building design.
- d. Third party advertising signs and billboards, are not allowed within the area plan (i.e., Sign – Class F (Third party advertising sign) and/or Sign – Class G (Digital third party advertising sign)).
- e. All commercial signage facing residential uses are discouraged.
- f. External finishing materials for proposed commercial signage and developments should be compatible with the best examples of local development.
- g. Windows must be free of clutter to maintain clear glazing.
- h. The use of film or any visually blocking material on doors and display windows at street level to cover more than 20% of the coverage area is prohibited.

Proposed International Avenue Area Redevelopment Plan

4.1.2.3 Policies

- i. Wayfinding and signage for cyclists and pedestrians should be provided throughout the plan area to destinations within and beyond the plan area. This signage may be required as part of a Development Permit application where the site location is ideal for such signage.
- j. Fencing along 17 Avenue S.E. is generally discouraged but may be permitted for screening or security purposes to a maximum of 1.8 metres tall and should not include the use of barbed wire, razor wire, or chain link. Acceptable materials include wrought iron, galvanized painted fencing, brick, stucco and wood.

4.1.2.2 Character signage policies

Prominent, often-illuminated signs popular in the 1950s, 60s and 70s have a strong association with the visual identity of 17 Avenue S.E., and important significance to the area's character. Although many of these signs have been removed from their original locations, five remaining examples have been identified as particularly representative of this style, and are shown below (Figure 1) with specific policy to incentivize their retention.

- a. Encourage owners of property containing identified character signage (Figure 1) or signs on the Inventory of Evaluated Historic Resources to designate these sites as Municipal Historic Resources, protecting them under the Alberta Historical Resources Act and making them eligible for conservation incentives.
- b. Support relaxations that increase the viability of retaining identified character signage (Figure 1) or signs on the Inventory of Evaluated Historic Resources.
- c. Encourage the retention of character signage or signs on the Inventory of Evaluated Historic Resources in existing locations as redevelopment occurs, or if impossible, encourage incorporation of such signage into new development according to conservation best practice.

Figure 1 | Character signage



Far East Foods
2830 17 Avenue S.E.



4 Seas Restaurant
3600 Block 17 Avenue S.E.



Paradise Lanes Bowl
3411 17 Avenue S.E.



Dragon's Gate Restaurant & Bar
4408 17 Avenue S.E.



Best World Travel LTD
3500 Block 17 Avenue S.E.

Proposed International Avenue Area Redevelopment Plan

4.2 Building interface

Building interface includes the setback areas from the property line to the buildings (building setback), as well as the actual building façade. These elements play an important role in the overall character of the streetscape and usually complement the characteristics of other street design elements. The building setback areas may accommodate landscaping, outdoor seating, public art, decorations, signs and bike parking, and provide additional publicly-accessible places. This area, although on privately-owned land, does provide a degree of public amenity and is usually functionally integrated with the public sidewalk area. A well designed and proportioned building façade will animate the street and contribute to a comfortable, human-scaled environment.

4.2.1 Building setback policies

- a. One metre front setbacks are encouraged to allow buildings to be built close to the front property lines and to form a continuous, well transitioned street wall. Variations to the setback are allowed for the purpose of façade articulation (e.g., projections).
- b. In order to help ensure a consistent and continuous street wall along the corridor, large building setbacks (more than 3 metres) are generally discouraged, except where:
 - i. It has been determined that the site is in a strategic location for creating an urban plaza or a more sizable pocket park with the intent of diversifying the streetscape and providing visible and comfortable spaces for pedestrians to gather and linger. To maximize sun exposure, plazas/parks are encouraged to be located on the north side of the street.
 - ii. The larger building setback is required to meet the minimum right-of-way setback as set out in 17 Avenue S.E. Transportation Planning Study.

4.2.2 Building façade policies

- a. Buildings greater than 30 metres in width are encouraged to provide more than one entrance, and should use architectural articulation to reduce the massing of the building by dividing the façade into smaller modules.
- b. Maximize the use of transparent windows, doors, and display windows at street level (clear glass over a minimum of 75 per cent over the length of the façade) to provide eyes on the street.

Proposed International Avenue Area Redevelopment Plan



5 Community amenities and facilities

Community services, amenities and facilities provide care, culture, education, recreation and protection to people who live, work and play in the community. The private sector, public sector, non-profit agencies, charities and partnerships can all play a role in the ownership and operation of community facilities and services. In addition to the policies of the Developed Areas Guidebook, community services, amenities and facilities should meet the following criteria:

Proposed International Avenue Area Redevelopment Plan

5.1 Community association sites

There are three existing Community Association sites within the contextual area of the plan: Forest Lawn Community Association, Southview Community Association, and Alberta Park/Radisson Heights Community Association. Access between these sites and Southeast 17 Main Street should be facilitated via direct multi-modal routes between the locations, reducing or eliminating physical barriers along the way.

5.2 Recreation and cultural facilities

There are several recreational and cultural facilities within or nearby to the contextual area of the plan; the Forest Lawn Outdoor Pool, Forest Lawn Public Library, Bob Bahan Aquatic and Fitness Centre and the Ernie Starr Arena. Access to these facilities should be enhanced and facilitated from 17 Avenue S.E. via direct, high quality pedestrian routes with enhanced wayfinding.

5.3 Care facilities

A broad range of care facilities is an important element of a complete and inclusive community. Care facilities are encouraged pursuant to the policies of the Developed Areas Guidebook.

5.4 Housing affordability

Complete communities include a broad range of housing forms, tenures, and unit sizes to meet the needs of Calgarians at all income levels and stages of life. Housing diversity and affordable housing are encouraged pursuant to the policies of the Municipal Development Plan.

5.5 School sites

Schools sites have traditionally been the institutional, recreational and social focus of a community. One school site, Holy Trinity, is located within the plan boundary. In the event that this school site is deemed surplus by the school board the City should consider its acquisition in order to ensure an adequate supply of open space in Forest Lawn.

5.6 Emergency services safe communities

Emergency services Safe Community Design Principles should be applied throughout the plan area. An emergency services safe community promotes and maintains safe and healthy behaviours, supports effective emergency responses and offers protection to people and their property. An emergency response station may be located within the plan area as shown on Map 3: Land use concept.

Proposed International Avenue Area Redevelopment Plan

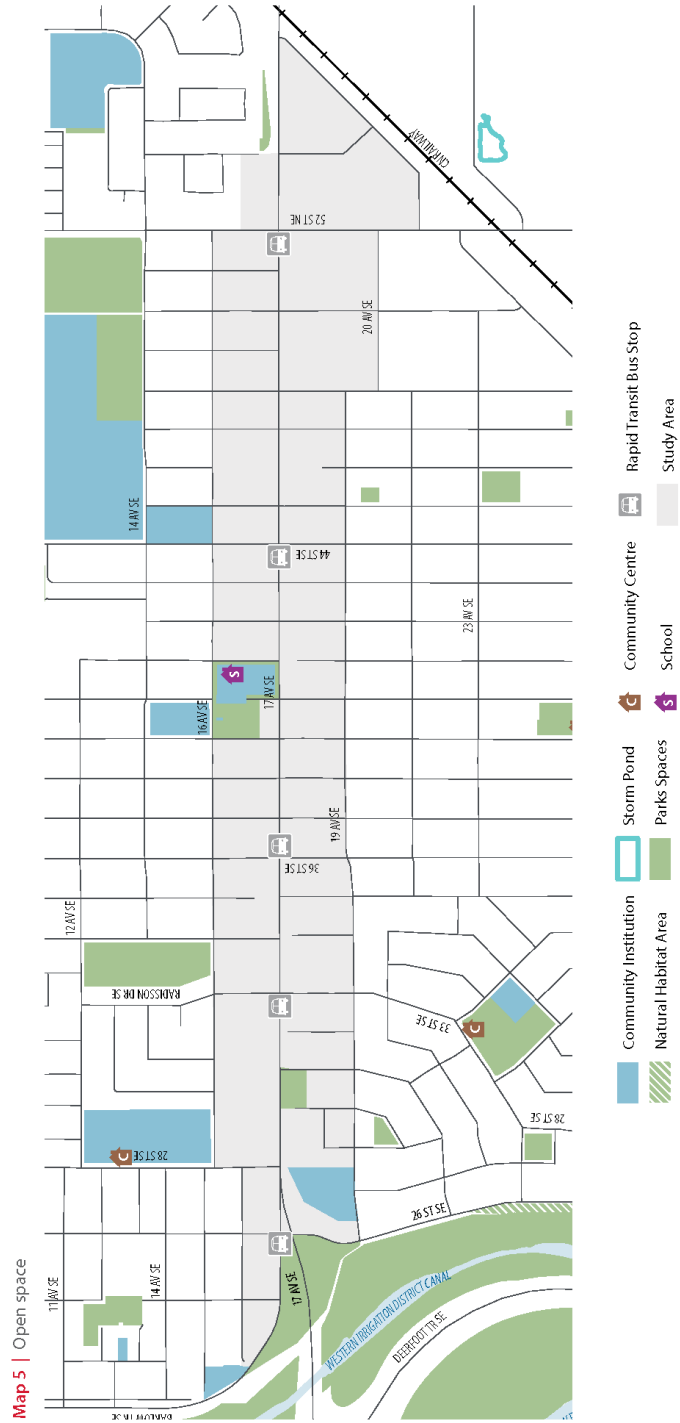


6 Open space

Open spaces are essential in ensuring a community has places to gather, participate in recreational activities, enjoy the outdoors, and relax. Open spaces can contribute to healthy lifestyles and can contribute to a healthier environment.

Public open spaces within the plan area includes the Holy Trinity elementary site, the Forest Lawn Outdoor Pool and a smaller pocket park between 30 Street S.E. and 31 Street S.E. in the vicinity of the plan area there are several school sites and a variety of smaller open spaces to the north and the south of 17 Avenue S.E., Elliston Park to the east, and the Irrigation Canal and Bow River to the west. Parks within the plan area and in close proximity are shown on Map 4: Open space.

Proposed International Avenue Area Redevelopment Plan



Proposed International Avenue Area Redevelopment Plan

6.1 General policy

In addition to the policies contained within the Developed Areas Guidebook, the following open space policies will apply:

- 6.1.1 In accordance with the Joint Use Agreement, if the Holy Trinity school site is declared surplus, The City should consider the acquisition of the site in order to maintain adequate open space provision in the community.
- 6.1.2 In the event of closure or relocation, the land where the Forest lawn Outdoor pool is currently located should remain as open space.
- 6.1.3 Public and publicly accessible private open spaces should;
 - i. be designed to be universally accessible,
 - ii. be designed to maximize sunlight exposure,
 - iii. be well connected to adjacent pedestrian facilities such as sidewalks and pathways, and,
 - iv. be designed in accordance with CPTED principles; to ensure natural surveillance ("eyes on the park"), clear sightlines, lighting and where possible several points of access/egress.

6.2 Open space typology

Five types of open spaces are identified in the plan area for future place-making improvement and they can be categorized under the following: Transit Hub, Urban Plaza, Pocket Park, Linear Park and Street Park

Provision of open spaces in accordance with these typologies is encouraged on private development sites, on existing City lands, and within City road right of way and may be achieved by utilizing the following methods; registration of access easements on private land, formal road or lane closures, re-purposing of existing City lands, or other methods deemed acceptable by the City.

6.2.1 Transit hub

Section 7.3 indicates provisions for two transit-only lanes as well as BRT Stations in the median of 17 Avenue S.E. The transit hubs are the intersection areas where major stations are being provided. This includes the intersections at 28 Street S.E., 33 Street S.E., 36 Street S.E., 44 Street S.E. and 52 Street S.E.

Of these station areas, the 36 Street S.E. and 52 Street S.E. intersections are the major hubs due to their function within the hierarchy of the regional transportation network. Transit hubs should be designed to provide comprehensive services and to become key nodes for redevelopment and community activity.

6.2.1.1 Design policies

- a. Transit hubs shall incorporate pedestrian-focused elements such as transit lanes, waiting platforms, seating areas, shelters, street lighting, wayfinding signage, hard and soft landscaping and pedestrian crossings into the design.
- b. Surrounding developments and the public realm within a transit hub should be integrated with the BRT station.
- c. Urban plazas should be incorporated into the site and parcel layout at transit hubs where possible.

Proposed International Avenue Area Redevelopment Plan

Figure 2 | An example of transit hub concept



6.2.2 Urban plaza

An urban plaza should be fully integrated with surrounding buildings and spaces, and not be merely leftover areas between buildings. Activities accommodated by an urban plaza, such as socializing, resting, eating, bus waiting, exhibitions and open air markets, add to the quality of city life, enhance diversity and increase the educational and cultural opportunities that define the positive experience of urban living (source: Urban Plaza Design Guidelines, Vancouver, 1992).

6.2.2.1 Design policies

Urban plazas should:

- Encourage activity in both daytime and evening hours;
- Include furniture and landscaping that is appropriate for purpose of the plaza, such as seating, lighting, information kiosks, flower beds, trees, tables, signage, etc.; and
- Be located at gateways, major intersections, street corners, transit hubs and areas classified as Special Mixed-Use areas.

6.2.3 Pocket park

A pocket park is a small park accessible to the general public. Pocket parks are often created on a single vacant building lot, a closed road right-of-way, or on small, irregular pieces of land. Because pocket parks are generally compact in size and are generally the “unused land” between buildings or at street corners, they act as convenient, accessible, inexpensive outdoor amenity space and serve the immediate adjacent population.

Proposed International Avenue Area Redevelopment Plan

6.2.3.1 Design policies

Pocket parks should be:

- a. well-defined by landscaping;
- b. furnished with comfortable seating for all ages;
- c. uniquely designed and decorated through the use of special pavers and art features; and
- d. located at a street corner, adjacent to a sidewalk or in a location with connectivity to major activity destinations

6.2.4 Linear Park

A linear park is an elongated area, which features treed landscaping and open space that provides pedestrian movement and for activities that are generally unstructured in nature. The width of a linear park varies depending on the available space. The purpose of designing linear parks in the Corridor is to frame and reinforce the area's character and enable pedestrian movement. A linear park is may be suitable for applications of Low Impact Development solutions.

6.2.4.1 Design Policies

Linear parks should:

- a. incorporate landscaping, space for passive use, and a sidewalk or pathway to facilitate movement through the space;
- b. include Low Impact Development features only where those features would not diminish the overall function of the linear park;
- c. connect to or act as anchors for pedestrian pathways and sidewalks; and
- d. have a minimum width of 10.0m.

6.2.5 Street Park

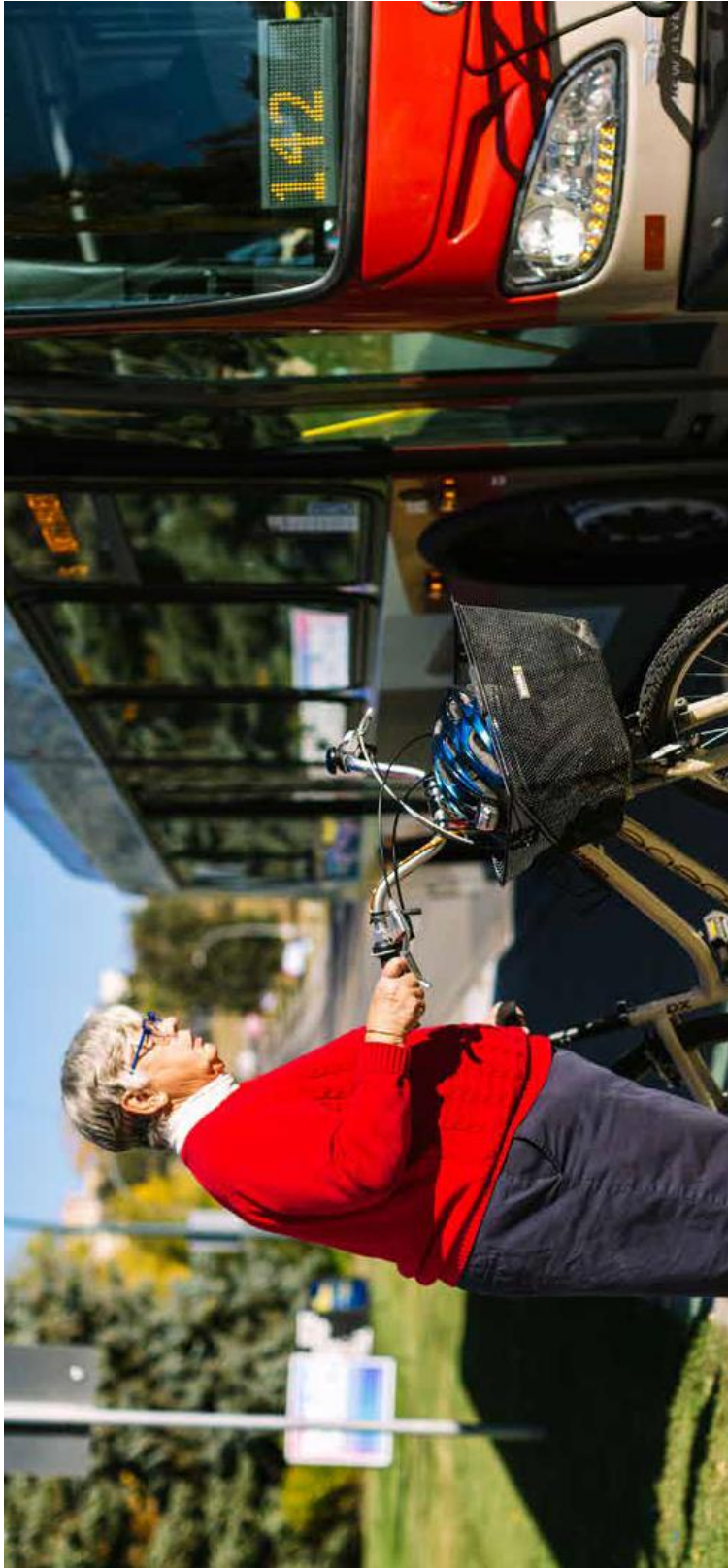
A street park is a park on a street or portion of a street right-of-way. It is typically located on a south-north street where there is generally ample access to sunlight and the traffic volume is fairly low. A street park can be accessible to vehicles year-round with occasional road closures (or partial closures) for special events, or it may be accessible to vehicles only during the winter months, while in summer, it provides a pedestrian-only space, which accommodates hospitality, special events, recreational, commercial and artistic activities.

6.2.5.1 Design Policies

Street parks should:

- a. be located on low traffic volume streets, preferably running north-south, with temporary or seasonal road closure opportunities; and
- b. allow maximum integration with the adjacent public realm system and connectivity to major pedestrian destinations such as transit hubs

Proposed International Avenue Area Redevelopment Plan



7 Mobility

The mobility system envisioned for the plan area will encourage sustainable modes of transportation and provide a highly connected network of paths, streets and transit routes.

Proposed International Avenue Area Redevelopment Plan

7.1 Pedestrian circulation

The intent of these policies is to provide a safe and accessible pedestrian environment for people of all ages and abilities.

7.1.1 Location and features

- a. The pedestrian circulation network should adhere to Map 6: Pedestrian and cycling network.
- b. Redevelopment of the area along 17 Avenue S.E. linking Forest Lawn and other communities in the east to the downtown will require continuity of the pedestrian and cycling networks across Deerfoot Trail. This pedestrian connection will be aligned with the BRT Busway structure and should be designed to provide a pleasant and safe experience for users at all times of day and in all weather conditions.

7.1.2 Public realm

The crossings of 17 Avenue S.E. shown on Map 6: Pedestrian and Cycling Network shall incorporate the use of distinctive pavement treatments that may include enhanced paint markings and differentiated paving materials to enhance crossing visibility.

7.2 Cyclist circulation

The intent of these policies is to provide cycling infrastructure and connections that will encourage local trips by bicycle and link to the larger pathway network and destinations throughout Calgary.

7.2.1 Location and features

Cycling facilities (e.g. parking, showers) should be provided at destinations in Forest Lawn, along 17 Avenue S.E., at intersecting collector and arterial Streets, bus stops and entrances to parks and open spaces, and other key locations connecting to the existing cycling network.

7.2.2 Public realm

Wayfinding and signage for cyclists shall be provided throughout the International Avenue area to destinations within and beyond the plan area. This signage may be required as part of a Development Permit application where the site location is ideal for such signage.

7.3 Transit network

The intent of the transit network is to provide safe, accessible and convenient connections to its user's destinations. It should be a logical choice for people wanting to move through, to and out the community.

7.3.1 Location and features

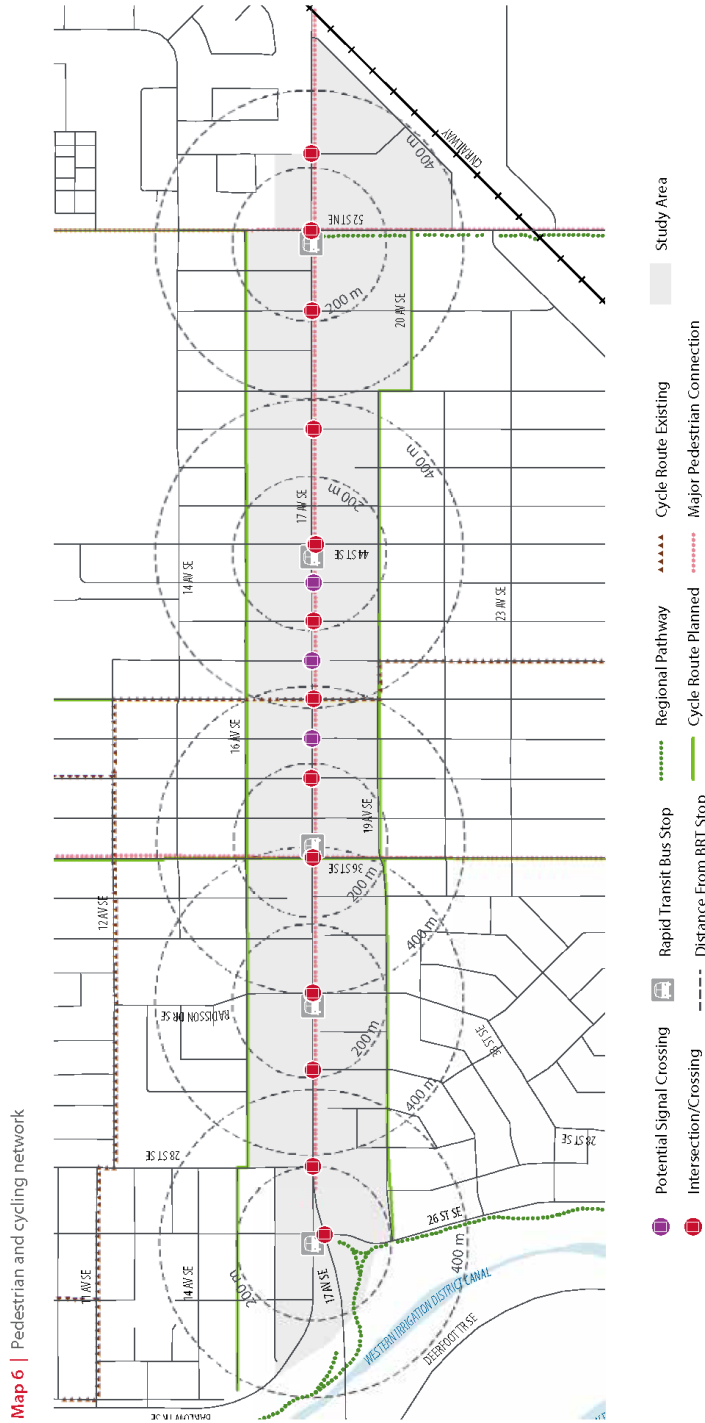
- a. Transit routes will be located as shown on Map 7: Transit network.
- b. BRT station and Transit only lanes are planned to be integrated in the 17 Avenue S.E. corridor, internal to the right-of-way (in the median) with accompanying curb side local transit provisions.

7.4 Street network

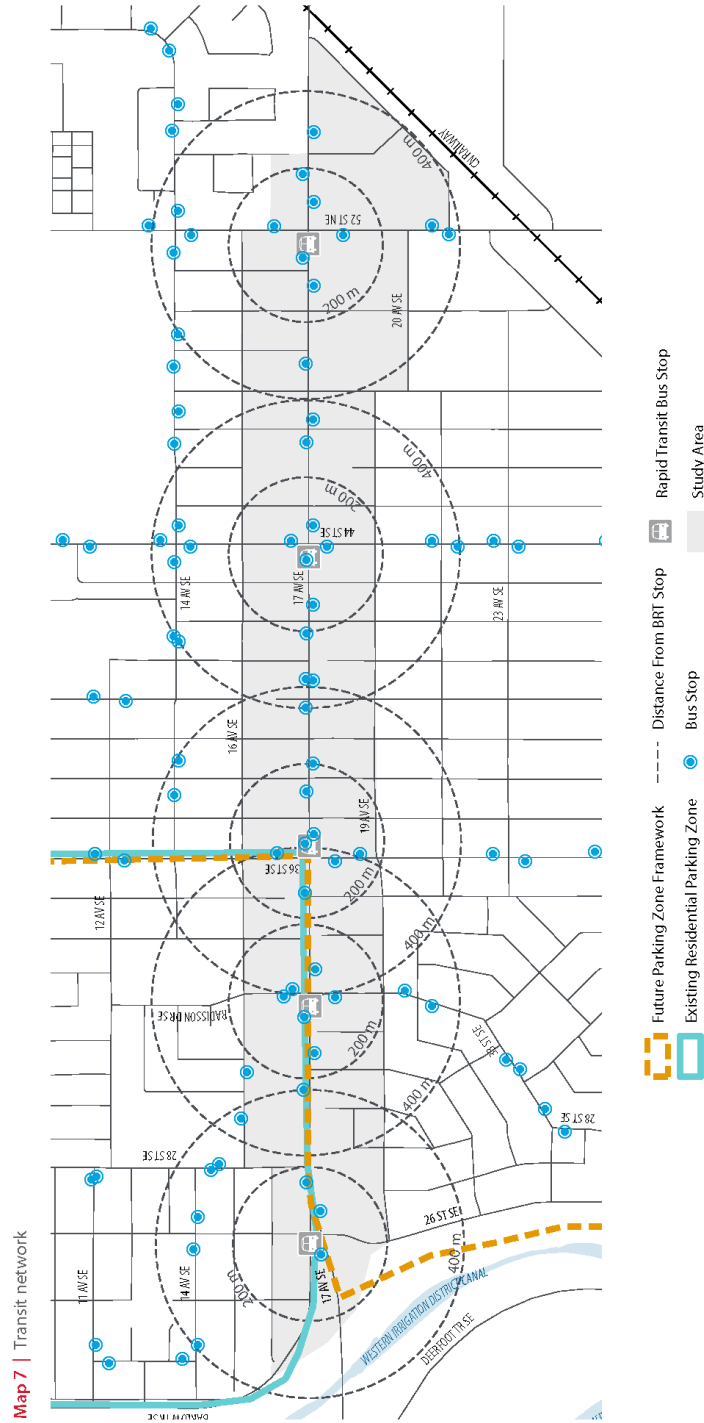
The intent of the street network policies is to establish a transit-oriented and development-supportive street network. The street network for the plan area is well integrated with regional network connections and balances the needs of pedestrians, cyclists, transit and automobiles, using street cross-sections compliant with the Complete Streets Policy. While the street network is already mostly established, there are some changes that are anticipated over the time frame of the plan, including the following:

- changes to the regional system that may affect traffic volumes on streets within the plan area; and
- changes to the function and components of higher-capacity streets.

Proposed International Avenue Area Redevelopment Plan



Proposed International Avenue Area Redevelopment Plan



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7.5 Parking framework

Established communities such as Forest Lawn were developed long before the population and vehicle ownership levels of today. As the priority in all established communities is to encourage new residential development, there will likely be situations in which the traffic generated by proposed developments would exceed street capacity guidelines, but 17 Avenue S.E. is designed to provide a balance between modes of travel and to accommodate both local and regional movements in these modes.

Access to and organization of parking and internal drive aisles affect the efficiency and safety of pedestrian, cyclist and vehicle traffic. These parking policies have as their primary goal the stimulation and facilitation of new retail development. Stimulating new development by allowing off-site parking and/or LUB relaxations has the potential to create overspill parking.

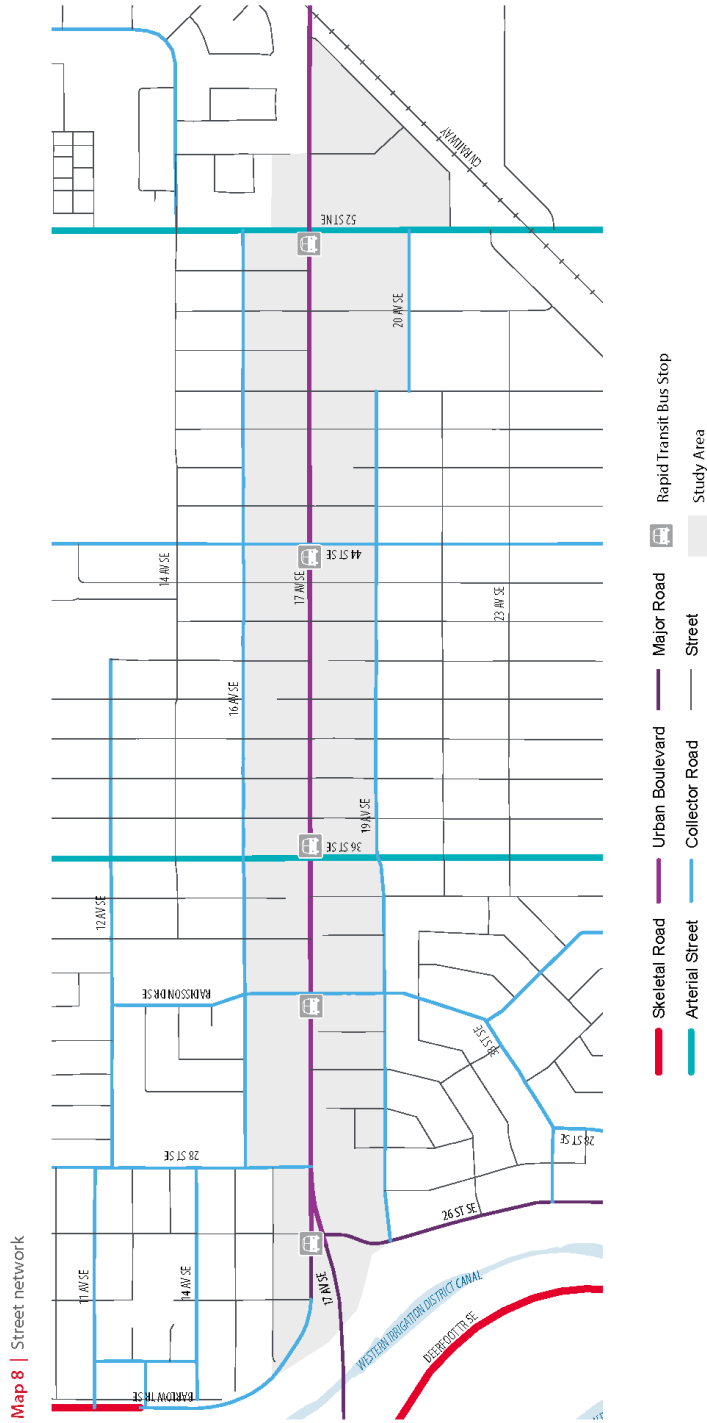
7.4.1 Location and features

- a. The street network should be located as shown on Map 8: Street network.
- b. Any new streets established as a part of a redevelopment application should be functionally connected to the existing network.
- c. New local streets may be established as part of a land use amendment and/or outline plan process without requiring an amendment to this plan.
- d. Principles of the Complete Streets Policy shall be applied when reconfiguring existing streets and designing new ones.
- e. Direct pedestrian, cycling, vehicular and transit connections should be provided between Forest Lawn and destinations throughout the adjoining communities.

7.5.1 Location and features

- a. Unless otherwise specified, front yard parking and outdoor storage is prohibited within the plan area.
- b. Outdoor garbage and loading areas should be accommodated within the site and should be visibly and permanently screened and integrated with the building design.
- c. Parking relaxations should be granted where historic buildings are being protected.

Proposed International Avenue Area Redevelopment Plan



Proposed International Avenue Area Redevelopment Plan



8 Plan area attributes

8.1 Attributes

The plan area has a number of unique features that should be considered when making planning and development decisions (see Map 9: Attributes). These are listed on the next page.

Proposed International Avenue Area Redevelopment Plan

8.1.1 Airport Vicinity Protection Area

The Airport Vicinity Protection Area (AVPA) regulation governs development within the flight path of planes to the Calgary International Airport to ensure that land uses are compatible. A portion of the International Avenue area plan lies within areas regulated by the AVPA regulation. Properties lying within these areas may be subject to certain development restrictions or conditions, including the condition that all buildings constructed in the Noise Exposure Forecast (NEF) contour must comply with the acoustical requirements of the Alberta Building Code, as well as the prohibition of residences, schools, medical clinics and other uses. The AVPA regulation's list of prohibited uses in NEF areas should be consulted prior to the application for any development or redevelopment of any property falling within the NEF contour.

8.1.2 Topography

An escarpment exists on the west side of the plan area, leading down to the Bow River and wildlife can occasionally be spotted in the vicinity. At the time of development, impacts to the escarpment will be reviewed in greater detail.

8.1.3 Western irrigation district canal

An irrigation canal runs parallel to the escarpment at the west border of the plan area. Connections to the pathways along this canal should be maintained.

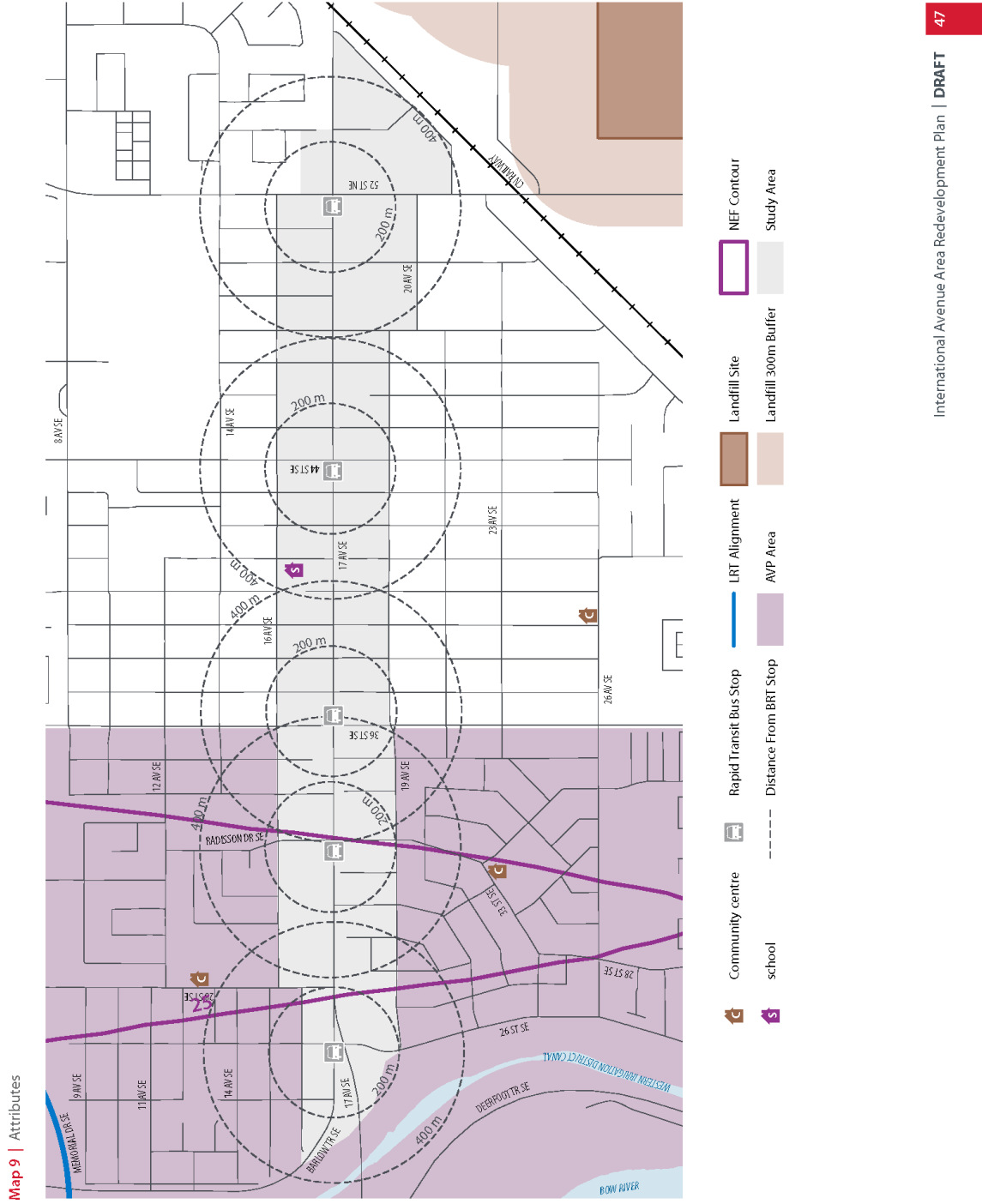
8.1.4 Freight corridor

The International Avenue plan area includes a freight corridor. Any development in proximity to a freight railway property must conform to The City's requirements at the time of application due to the proximity to the railway right-of-way.

8.1.5 Landfill setback

Although there are no landfills within the plan area, East Calgary Landfill is in relatively close proximity to the International Avenue boundary. The subdivision and Development Regulations prohibits specific uses, including hospitals, schools, residences and food establishments.

Proposed International Avenue Area Redevelopment Plan



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9 Implementation and interpretation

Accurate interpretation is paramount to achieving the goals of this plan. The intent of this section is to provide policy necessary for plan implementation, such as interpretation, limitations, amendments, monitoring and growth management.

Proposed International Avenue Area Redevelopment Plan

9.1 Policy framework

The Municipal Government Act (MGA) outlines the purpose and scope of powers for municipalities. International Avenue Redevelopment Plan (ARP or the plan) is a statutory document that establishes a long-range framework for land use, urban design and mobility for the Southeast 17 Main Street. This Plan has considered and is in alignment with the South Saskatchewan Regional Plan. The ARP must be read in conjunction with the Municipal Development Plan (MDP) Volume 1 and Volume 2 Part 3: Developed Areas Guidebook; the Calgary Transportation Plan (CTP); and other City of Calgary policy and guiding documents, unless otherwise indicated. In the event of a discrepancy between this plan and the Developed Areas Guidebook, the policy of the plan will prevail.

accordance with applicable policies. Any major changes may require an amendment to this plan.

9.2.4 Any change to the text or maps within this plan shall require an amendment to the plan that includes a Public Hearing of Council.

Policy interpretation

9.2.5 The South Saskatchewan Regional Plan (SSRP) establishes a long-term vision for the region using a cumulative effects management approach to guide local decision-makers in land use management to achieve Alberta's economic, environmental and social goals. This plan allows The City to encourage and incentivize more progressive policies related to sustainability and the environment.

9.2 Area Redevelopment Plan interpretation

Map interpretation

9.2.1 Unless otherwise specified in this plan, the boundaries or locations of any symbols or areas shown on a map are approximate only, not absolute, and will be interpreted as such. The maps are not intended to define exact locations except where they coincide with clearly recognizable physical features or fixed boundaries such as property lines or road or utility rights-of-way. The precise location of these boundaries, for the purpose of evaluating development proposals, will be determined by the approving authority at the time of application.

9.2.6 Where an intent statement accompanies a policy, it is provided as information only to illustrate the intent and enhance the understanding of the subsequent policies. If an inconsistency arises between the intent statement and a policy, the policy will take precedence.

9.2.7 The word "should" is explicitly used to further clarify the directional nature of the statement. Policies that use active tense or "should" are to be applied in all situations, unless it can be clearly demonstrated to the satisfaction of The City that the policy is not reasonable, practical or feasible in a given situation. Proposed alternatives will comply with MDP and CTP policies, intent and guidelines to the satisfaction of The City with regard to design and performance standards.

9.2.8 Policies that use the words "shall," "will," "must" or "require" apply to all situations, without exception, usually in relation to a statement of action, legislative direction or situations where a desired result is required.

9.2.2 No measurements of distances or areas should be taken from the maps in this plan.

9.2.3 All proposed land use areas, road and utility alignments and classifications may be subject to further study and may be further delineated at the outline plan or land use amendment stage in

Proposed International Avenue Area Redevelopment Plan

Order of plans



9.2.9 **Illustration and photo interpretation**
All illustrations and photos are intended to illustrate concepts included in the ARP and are not exact representations of an actual intended development. They are included solely as examples of what might occur after implementation of this plan's policies and guidelines.

9.2.10 **Figure interpretation**
Unless otherwise specified within this plan, the boundaries or locations of any symbols or areas shown on a figure are approximate only, not absolute, and shall be interpreted as such. Figures are not intended to define exact locations except where they coincide with clearly recognizable physical features or fixed boundaries such as property lines or road or utility rights-of-way.

9.2.11 **Appendix interpretation**
Unless otherwise specified within this plan, where actual quantities or numerical standards are contained within the figure, these quantities or standards shall be interpreted as conceptual only and will be determined at the detailed design stage.

9.2.12 **Plan limitations**
The appendices do not form part of the statutory portion of this plan. The intent of the appendices is to provide information and guidelines to support the policies of this plan.

9.2.13 **Plan limitations**
Policies and guidelines in this plan are not to be interpreted as an approval for a use on a specific site. No representation is made herein that any particular site is suitable for a particular purpose. Detailed site conditions or constraints must be assessed on a case-by-case basis as part of an outline plan, land use amendment, subdivision or development permit application.

Proposed International Avenue Area Redevelopment Plan

9.3 Area Redevelopment Plan implementation

Monitoring, review and amendments

9.3.1 New concepts and ideas may arise that are constrained by or contradictory to certain policies within this plan. Where such new concepts and ideas respond to and meet the intent of the vision and core ideas of the plan found in Section 2, or offer a creative solution to a particular problem, amendments may be supported. To make any change to the text or maps within this plan, an amendment that includes a Public Hearing of Council shall be required.

9.3.2 The policies within this plan shall be monitored over time in relation to development in order to ensure they remain current and relevant. Where determined necessary by Administration, these policies shall be updated through the plan amendment process either generally or in response to a specific issue in accordance with the Municipal Government Act. Where an amendment to the ARP is requested, the applicant shall submit the supporting information necessary to evaluate and justify the potential amendment and ensure its consistency with the MDP and other relevant policy documents.

Freight corridor

9.3.3 Any development in proximity to a freight railway property must conform to all requirements of The City at the time of application due to the proximity to the rail right-of-way.

Airport Vicinity Protection Area (AVPA)

9.3.4 The Airport Vicinity Protection Area Regulation (AVPA) governs land use development close to the Calgary International Airport to ensure that compatible land uses are adopted. Some areas within the Southeast 17 Main Street Area are designated by the AVPA regulation as Noise Exposure Forecast (NEF) 25–30 areas. Properties lying within these areas may be subject to certain development restrictions or conditions. This may include a requirement for all buildings constructed in a NEF area to comply with the acoustical requirements of the Alberta Building Code.

Proposed International Avenue Area Redevelopment Plan

Appendix

A. Urban design direction

The objectives in each character zone will be achieved over a long-term timeframe and should be phased. The following plan (Map 10: International Avenue urban design concept) provides users with a better understanding of the key recommendations of the land use and urban design concept.

Within this plan distinct areas have been identified by incorporating public/quasi-public places into areas with significant redevelopment potential (such as large block parcels or parcels located at major intersections). Many most of these distinct areas are ideal for fostering community activities. The design and implementation of a distinct areas will likely be a joint effort among the municipality, the individual landowners and the community.

This plan also directs The City of Calgary to determine future actions and initiatives required to implement the International Avenue land use concept plan and to provide a useful reference for the review of development proposals. Therefore this plan explores and recommends a variety of possible site conversion solutions based on these factors:

A.1 Parcel Development Typologies 1-6;

A.2 Distinct Area Typology A to G;

Proposed International Avenue Area Redevelopment Plan

A.1 Parcel development typologies

A key aspect to determining parcel development typologies is the parcel size for any location adjacent to 17 Avenue S.E. as the size influences built-form, density, massing and transition. There are several parcel size typologies along 17 Avenue S.E. available which allow for development opportunities of various size, use and intensity. The table below lists typical parcel types.

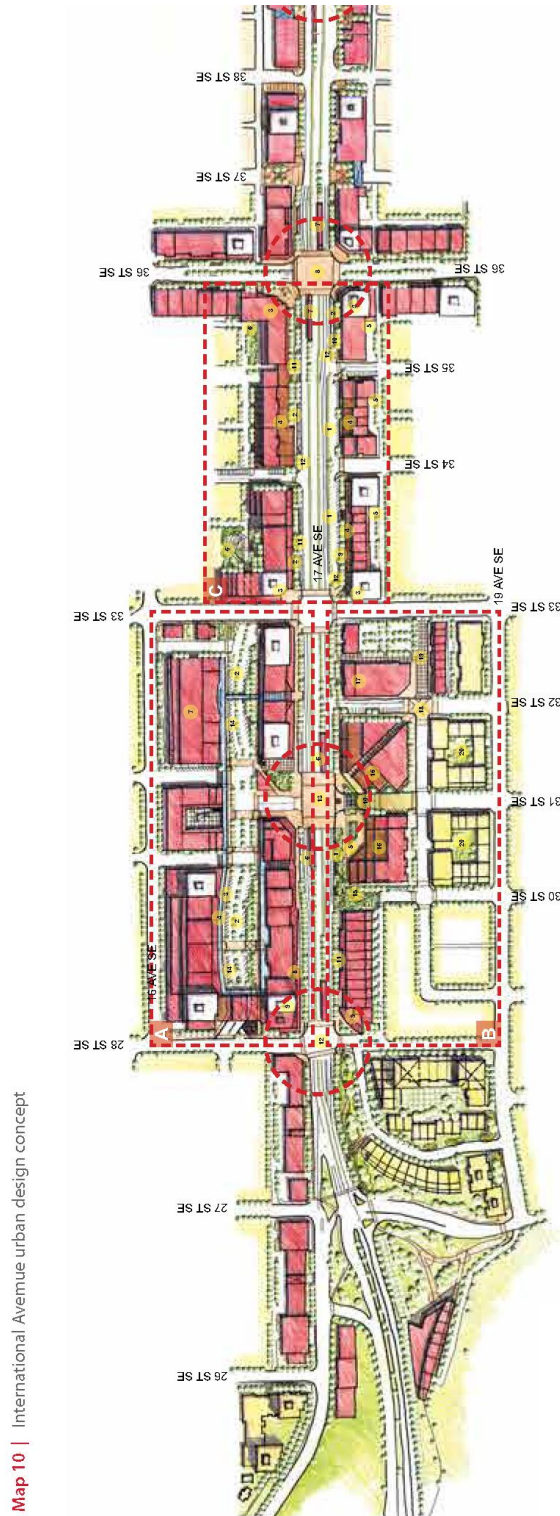
Commonalities of development typologies

- Combination of two or more adjacent parcels into one development;
- On-site parking along back-lane;
- Shared publicly accessible space between two buildings for outdoor use (plaza); and
- Fine-grain retail frontage with outdoor use capability facing the public boulevard of 17 Avenue S.E.

Development typology type 1-5

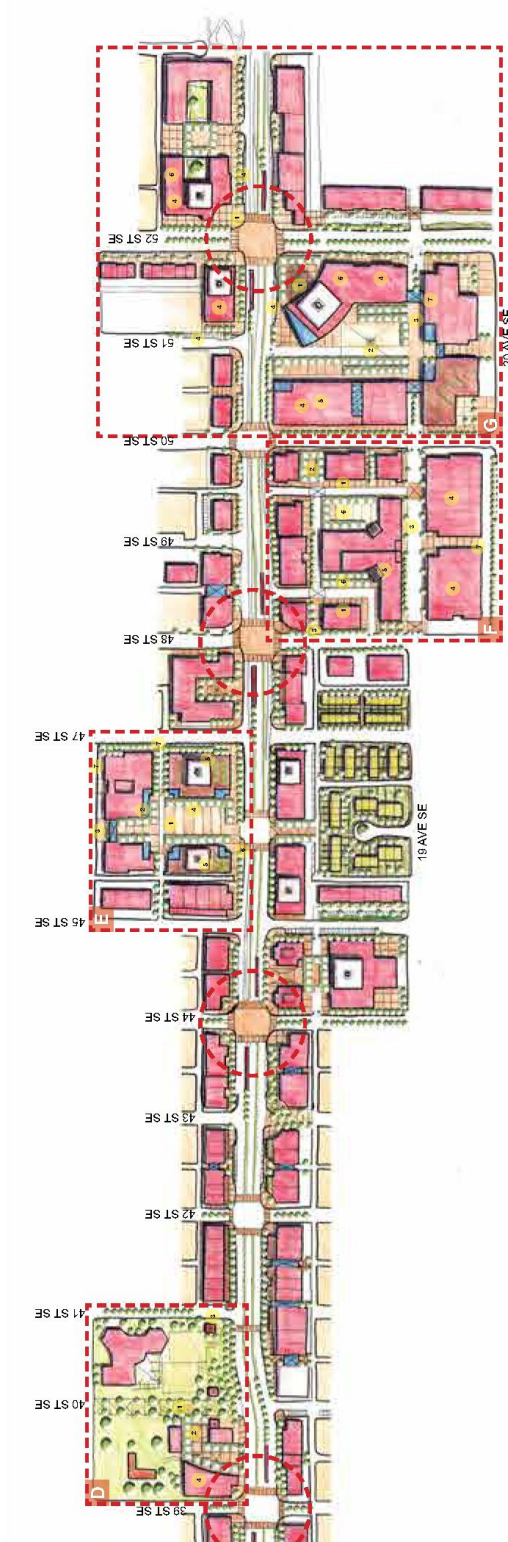
Parcel development typology	Frontage /width in meters	Depth in meters	Characteristics
Type 1	Up to 45 m	Up to 26 m	Frontage parking
Type 2	45 m	30 m	Back lane access
Type 3	90 m	45 m	Half street-block frontage with back lane access
Type 4	180 m	45 m	Full street-block frontage with partial street- block depth
Type 5	180 m	300 m	Full street-block frontage with full street block depth

Proposed International Avenue Area Redevelopment Plan



Map 10 | International Avenue urban design concept

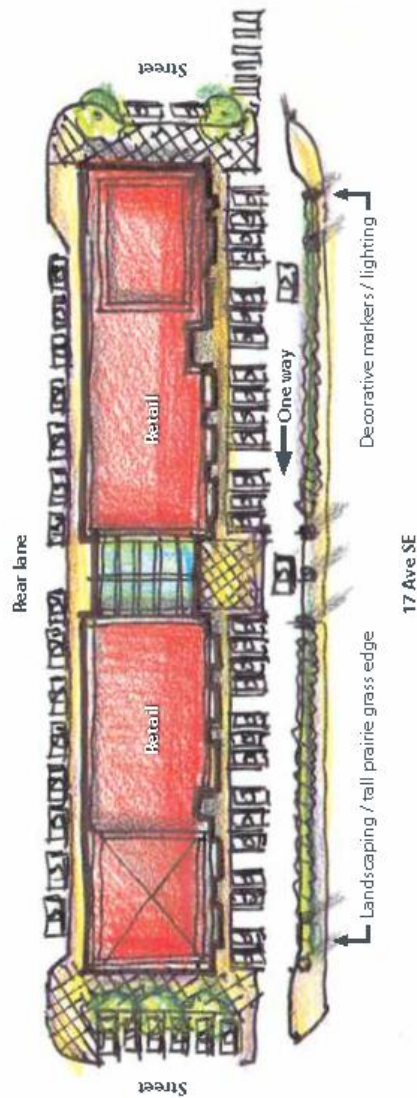
Proposed International Avenue Area Redevelopment Plan



Proposed International Avenue Area Redevelopment Plan

- Type 1** offers the smallest parcel configuration along the 17 Avenue S.E. corridor. It is available in multiple locations within the Special Typology areas A to G. Historically these small parcels defined the character of 17 Avenue S.E. with their 1-story buildings, frontage parking, narrow building format and several small commercial units within one building. A redevelopment of these parcels is very limited due to the lack of available space for on-site surface parking or no existing back-of-house parking with lane access. These parcels can maintain their current built-form and use while leaving their on-site frontage parking unchanged until a redevelopment occurs - see Figures 1, 2 and 3.
- Scenario 1 and 2**
This development scenarios apply to parcels with existing frontage parking and rear lane:
- Street parking may be provided in the service lane.
 - A landscaped edge should be provided between the service lane and the 17 Avenue S.E. Decorative markers and lighting can be located within the landscape edge; and
 - Activate the elevations facing side streets.
- see Figure 1 and Figure 2.

Figure 1: Parcel development type 1: Scenario 1



Proposed International Avenue Area Redevelopment Plan

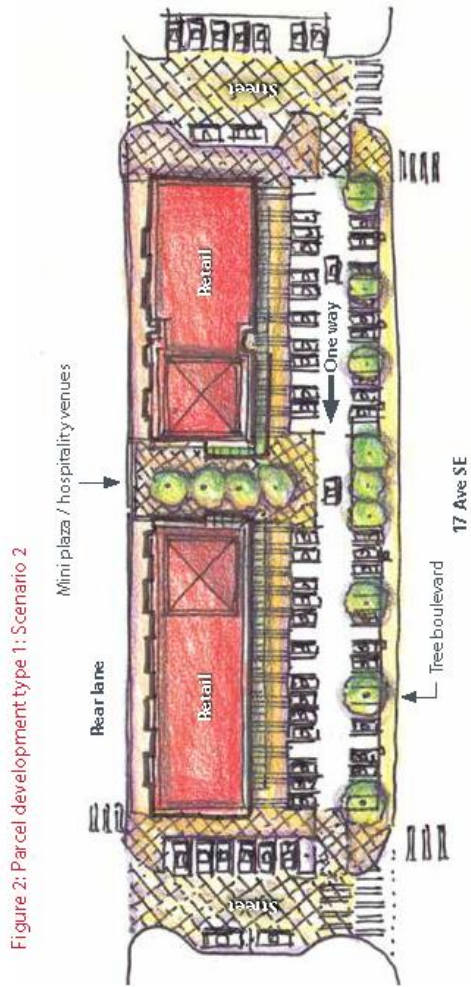


Figure 2: Parcel development type 1: Scenario 2

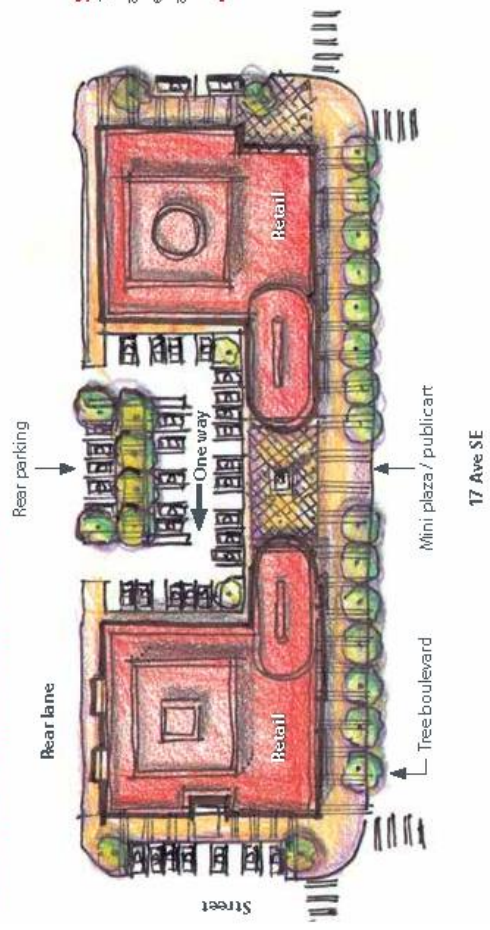


Figure 3: Parcel development type 1: Scenario 3

Scenario 3
This development Scenario applies to parcels without existing frontage parking and with back-lane access:

- Provide parking area with back-lane access.- see Figure 3.

Proposed International Avenue Area Redevelopment Plan

Figure 4: Parcel development type 2



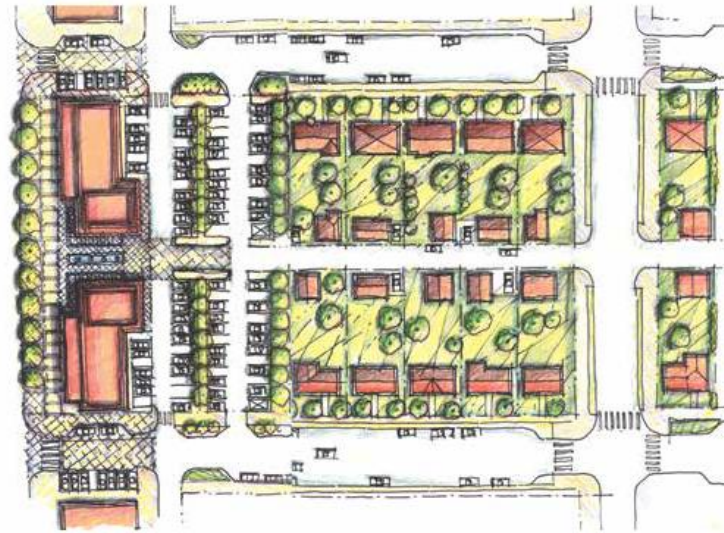
Type 2 A redevelopment of these parcels above 1-storey is limited as the available parking space on site is limited and at-grade. However, these parcels allow for a variety of redevelopment opportunities – see Figure 4.

In addition to the commonalities of typologies 1-6, type 2 provides:

- on-street public parking where available;
- underground parking where feasible; and
- smooth integration into community context with landscaped parking, shared lane configuration and step-backs of upper floors to transition towards residential parcels.

Proposed International Avenue Area Redevelopment Plan

Figure 5: Parcel development type 3



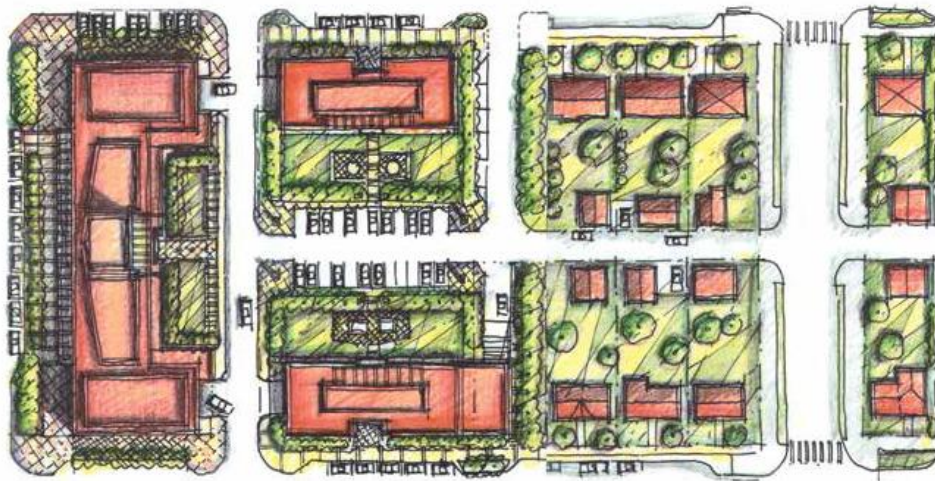
Type 3 offers a parcel width and depth combined with back-lane access which allows the construction of underground parking and an efficient parcel redevelopment. This parcel type is available in multiple locations within the District Areas of Topology A to G. These parcels allow for significant redevelopments and densifications – see Figure 5.

In addition to the commonalities of typologies 1-6, type 3 provides:

- On-street public parking where available;
- underground parking where feasible;
- option 1: Shifting of back-lane to accommodate rear parcel access, combined with additional at-grade parking by integration of adjacent existing single-family lots into the development transformation;
- option 2: Underground parking when combining two adjacent parcels; and
- smooth integration into community context with landscaped parking, shared lane configuration and step-backs of upper floors to transition towards residential parcels.

Proposed International Avenue Area Redevelopment Plan

Figure 6: Parcel development type 4



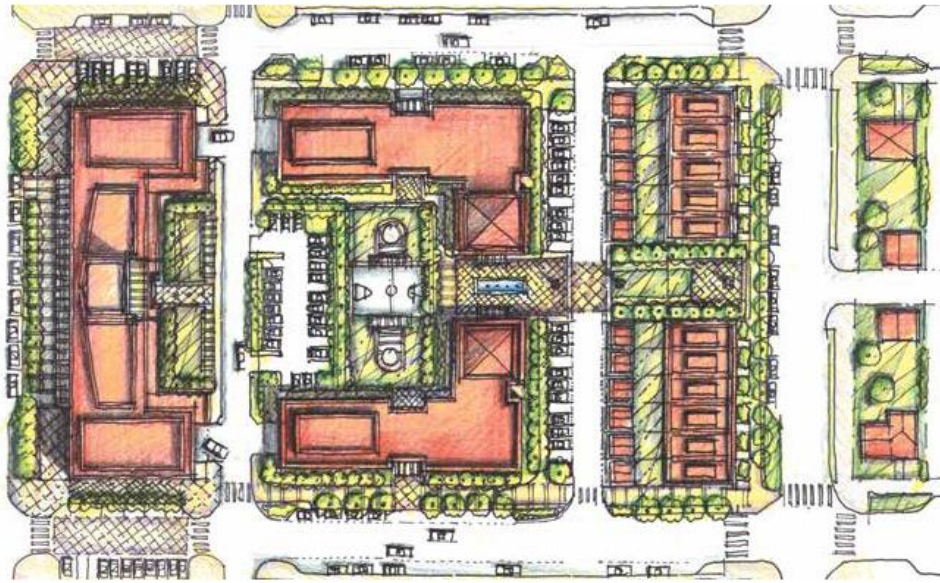
Types 4 offers parcel widths and depths which could allow the integration of a half or a full street block into a comprehensive corridor redevelopment. This parcel type is available at most public transit stops within the District Area Typology A to G. These parcels allow for strategic and key redevelopments and densifications – see Figure 5.

In addition to the commonalities of typologies 1-6, type 4 provides:

- utilization of existing back-lane to accommodate rear parcel access, combined with additional transformation of adjacent residential parcels into the redevelopment;
- underground parking;
- smooth integration into community context with landscaped parking, shared lane configuration and step-backs of upper floors to transition towards residential parcels; and
- integration of street-block internal north-south street into a comprehensive development.

Proposed International Avenue Area Redevelopment Plan

Figure 7: Parcel development type 5

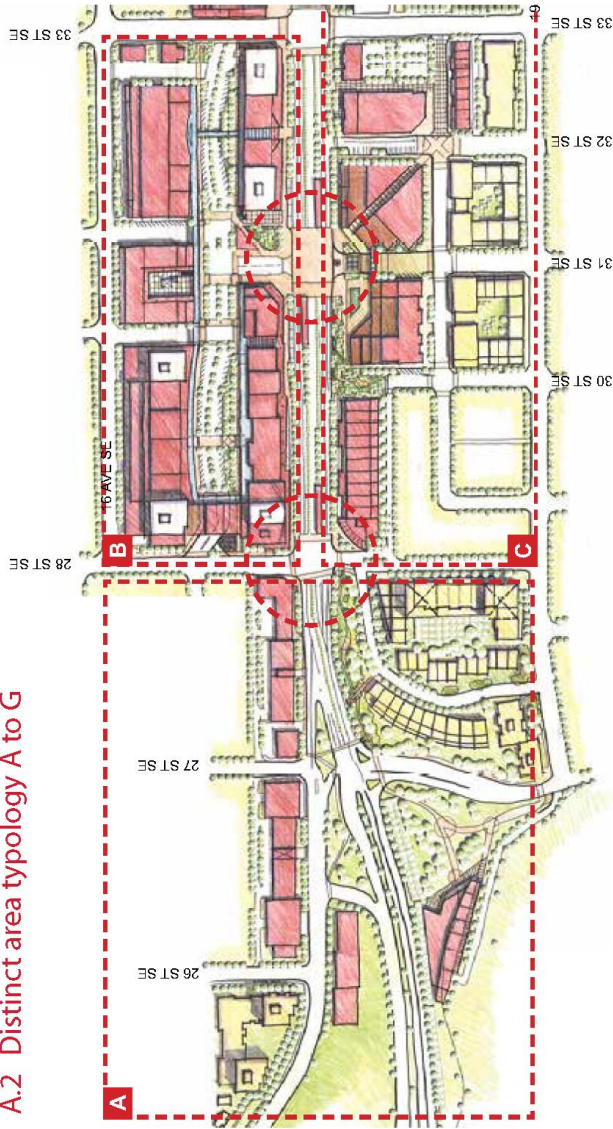


In addition to the commonalities of typologies 1-6, type 5 provides:

- Utilization of existing back-lane to accommodate rear parcel access, combined with additional transformation of adjacent residential parcels into the redevelopment.
- Underground parking.
- Integration of partial or full street block into development and densification of residential into multi-family building locations.
- Comprehensive integration of public streets into the block redevelopment with green courtyard configurations and shared street design – see Figure 9.

Proposed International Avenue Area Redevelopment Plan

A.2 Distinct area typology A to G



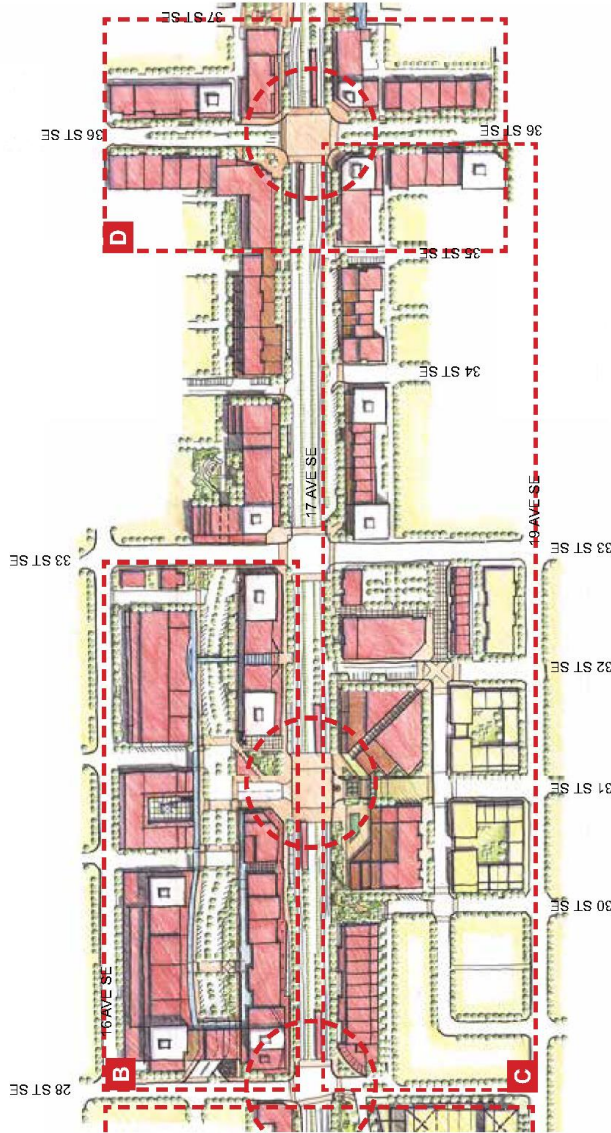
A – Gateway area

- Landmark buildings
- Improved pedestrian connectivity

B – Potential for innovative redesign – commercial/activity node concept

- Retrofitting existing shopping mall
- Open air mall with focus on pedestrian connectivity
- Internal street increasing retail frontages
- Continuous street wall
- Angled parking along internal street of the ‘open air mall’
- Future transit station
- Publicly accessible plaza for community gathering
- Concentrated parking structure (above grade with retail shops at grade)
- Comprehensive pedestrian walkway system (at grade and at upper level)

Proposed International Avenue Area Redevelopment Plan



C – Main Street Village

- Extension of the existing community park
- Character buildings defining public realm
- Grocery store with green parking lot
- Residential courtyard development
- Service road with angled parking
- Specialty shops between 28 Street S.E. and 36 Street S.E.
- Linear park
- Trees between angled parking and sidewalk
- Street park for community events

D – 36 Street transit hub

- High density mixed use development
- Buildings fronting on 17 Avenue S.E. and 36 Street S.E.
- Street corner to facilitate pedestrian activities
- Boulevards on 17 Avenue S.E. and 36 Street S.E.

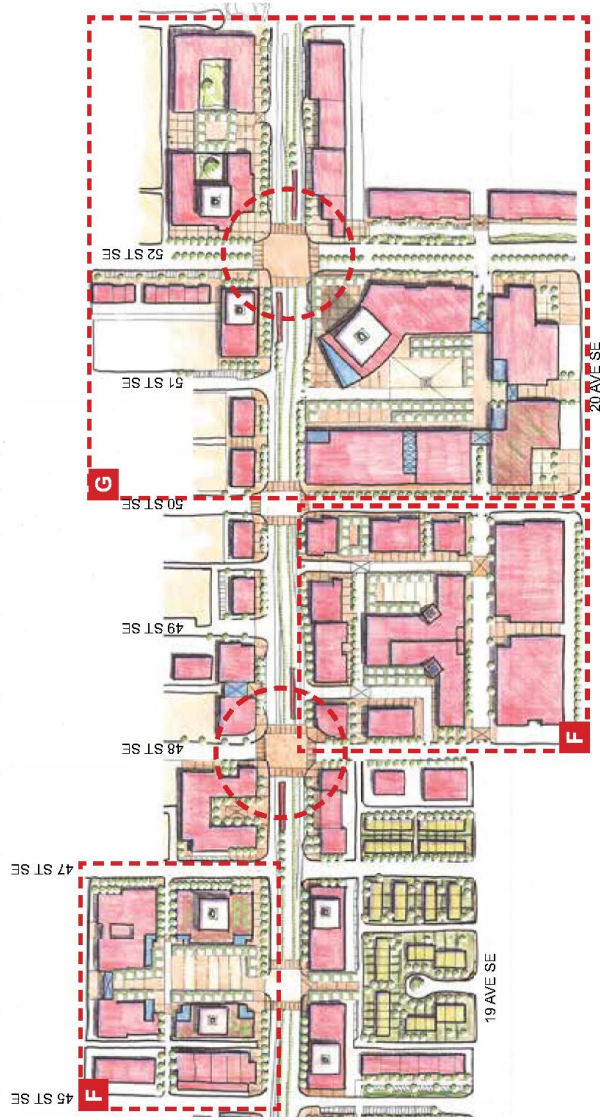
Proposed International Avenue Area Redevelopment Plan



E – Potential for innovative redesign – Community Recreation Centre concept

- Street Park on 40 Street S.E. (north of 17 Avenue S.E.)
- Public square with social facilities and playgrounds
- Tree-lined landscaping buffering playgrounds from street
- Additional organization buildings providing community services

Proposed International Avenue Area Redevelopment Plan



F – Potential for innovative redesign – Special Mixed-Use concept

- Open air mall concept with double sided retail frontage
- Retrofitting existing shopping mall
- Connectivity to residential areas
- New lane for access
- Squares or plazas for community activities
- Mix of parking solutions: on-street, podium parking, roof-top, underground
- Extension of 19 Avenue S.E.

G – Transit hub

- High density development/ landmark building design
- Retrofitting existing shopping mall
- Corner plaza
- Extension of 19 Avenue S.E.
- Mix of parking solutions: on-street, podium parking, roof-top, underground

Proposed International Avenue Area Redevelopment Plan



Proposed Amendments to the Forest Lawn/Forest Heights Hubalta Area Redevelopment Plan

WHEREAS it is desirable to amend the Forest Lawn/Forest Heights/Hubalta Area Redevelopment Plan Bylaw 27P94, as amended;

AND WHEREAS Council has held a public hearing as required by Section 692 of the Municipal Government Act, R.S.A. 2000, c.M-26, as amended:

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

1. The Forest Lawn/Forest Heights/Hubalta Area Redevelopment Plan attached to and forming part of Bylaw 27P94, as amended, is hereby further amended as follows:
 - (a) In Section 1.1, after the last paragraph insert a new paragraph as follows:

“The Municipal Government Act (MGA) outlines the purpose and scope of powers for municipalities. The Forest Lawn Area Redevelopment Plan is a statutory document that designates an area within the city for redevelopment. The Forest Lawn Area Redevelopment Plan (referred to as ‘this Plan’) must be read in conjunction with the Municipal Development Plan (MDP) Volume 1 and Volume 2 Part 3: The Developed Areas Guidebook (the Guidebook) (see Map 1 for the area that is subject to the Guidebook), the Calgary Transportation Plan (CTP) and other City of Calgary policy and guiding documents, unless otherwise indicated. In the event of a discrepancy between this Plan and the Guidebook, the Guidebook will prevail.”
 - (b) Delete Section 1.5 Planning Process in its entirety and renumber the remaining section accordingly.
 - (c) Delete the existing Map 1 entitled “Vicinity Map”.
 - (d) In Section 2.2 Study Area Boundaries delete text in its entirety and replace as follows:

“The Forest Lawn-Forest Heights/Hubalta Study area is located on Map 1.”
 - (e) Delete the existing Map 2 entitled “Location Map” and replace with the revised Map 1 entitled “Study Area” attached here to as Schedule A.
 - (f) In Section 3.1.2 Specific Issues delete 3.1.2.d in its entirety.
 - (g) In Section 3.1.4 Policies delete 3.1.4.b and 3.1.4.c in their entirety, and renumber all subsequent policies.
 - (h) Delete the existing Map 3 entitled “Land Use Policy Areas” and replace with the revised Map 2 entitled Land Use Policy Areas attached hereto as Schedule B.
 - (i) Delete the existing Map 3A entitled “Land Use Policy Areas (Detail Area)”.

**Proposed Amendments to the Forest Lawn/Forest Heights Hubalta
Area Redevelopment Plan**

- (j) In Section 3.1.5 implementation delete 3.1.5.1 and 3.1.5.10 in their entirety, and renumber all subsequent policies.
- (k) Delete the existing Table 1 entitled "Residential Sites to be Redesignated".
- (l) Delete the existing Map 3B entitled "Sites for Implementation Action".
- (m) In Section 3.2 entitled "Commercial Land Use" delete 3.2.1, 3.2.2, 3.2.3 in their entirety.
- (n) Delete the existing Map 4 entitled "Commercial Policy Areas".
- (o) Delete the existing Figure 1 entitled "Suggested Commercial Development Options".
- (p) Delete the existing Figure 2 entitled "Suggested Commercial Shadowing Guideline".
- (q) Delete the existing Table 2 entitled "Public Right-of-Way Improvements".
- (r) Delete the existing Table 3 entitled "Commercial Policy Areas and Sites Requiring Action".
- (s) In Section 3.2.4 entitled "Implementation", renumber the section as 3.2.1, delete policies 3.2.4.1, 3.2.4.2, 3.2.4.3, 3.2.4.4, 3.2.4.5, 3.2.4.7 and 3.2.4.8 and renumber policy 3.2.4.6 as 3.2.1.1.
- (t) Delete the existing Map 6 entitled "Industrial Sites for Implementation".
- (u) In Section 3.4 entitled "Open Space, Recreation and Community Facilities" delete 3.4.1.b and 3.4.1.d and renumber remaining policies accordingly.
- (v) In Section 3.4 entitled "Open Space, Recreation and Community Facilities" delete 3.4.3.a, 3.4.3.b, 3.4.3.e and renumber remaining policies accordingly.
- (w) In Section 3.4 entitled "Open Space, Recreation and Community Facilities" delete 3.4.4.a, 3.4.4.c, 3.4.4.d and renumber remaining policies accordingly.
- (x) Delete the existing Map 7 entitled "Open Space/Schools" and replace with revised Map 3 entitled "Open/Space Schools" attached hereto as Schedule C.
- (y) In Section 4.1 Context delete the second last paragraph in its entirety.
- (z) In Section 4.3 Transportation Policies delete 4.3.b, 4.3.c, 4.3.d, 4.3.e, and 4.3.g and renumber remaining policies accordingly.

**Proposed Amendments to the Forest Lawn/Forest Heights Hubalta
Area Redevelopment Plan**

- (aa) Delete the existing Map 8 entitled "Existing Transportation System".
- (bb) Delete the existing Map 9A entitled "36 Street S.E Road Widening".
- (cc) Delete the existing Map 9B entitled "36 Street S.E Road Widening".
- (dd) Delete Section 4.4 Implementation in its entirety.

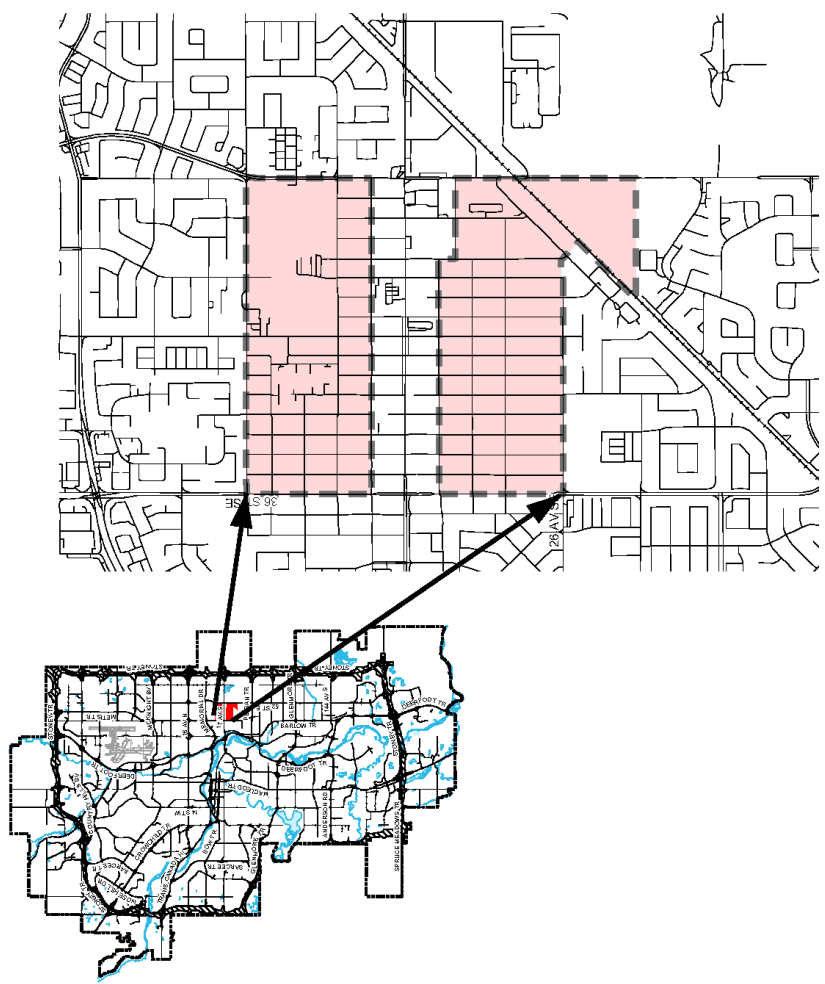
TEXT FOR DISCUSSION
ONLY

Proposed Amendments to the Forest Lawn/Forest Heights Hubalta Area Redevelopment Plan

SCHEDULE A

Map 1: Study Area

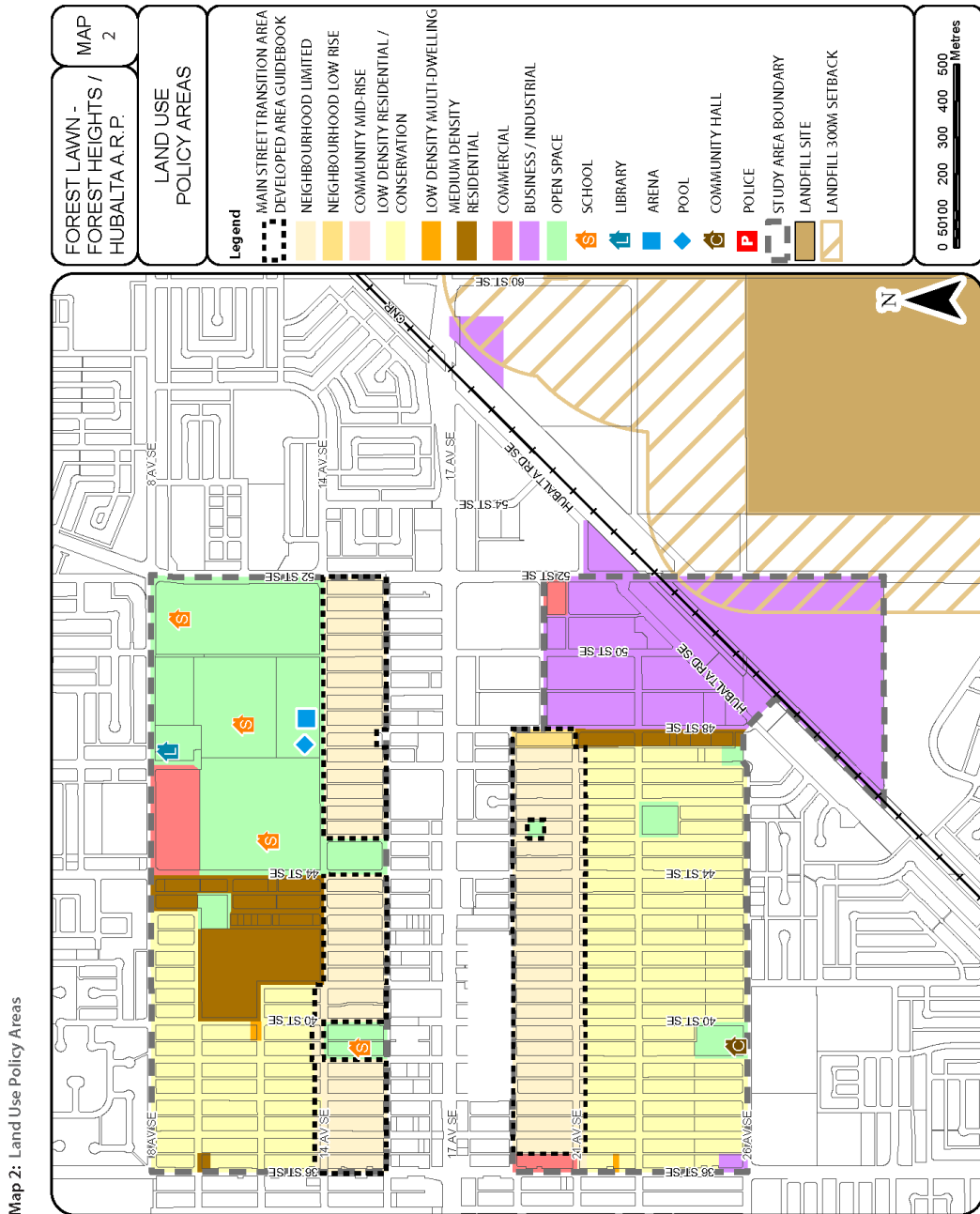
FOREST LAWN - FOREST HEIGHTS / HUBALTA A.R.P.	MAP 1
Study Area	
Legend STUDY AREA	
Approved: 27F94 Amended:	
THE CITY OF CALGARY LAND USE PLANNING & POLICY	
0 250 500 1,000 Metres	



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Proposed Amendments to the Forest Lawn/Forest Heights Hubalta Area Redevelopment Plan

SCHEDULE B



Proposed Amendments to the Forest Lawn/Forest Heights Hubalta Area Redevelopment Plan

SCHEDULE C



**Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan**

WHEREAS it is desirable to amend the Albert Park/Radisson Heights Area Redevelopment Plan Bylaw 15P88, as amended;

AND WHEREAS Council has held a public hearing as required by Section 692 of the Municipal Government Act, R.S.A. 2000, c.M-26, as amended:

**NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS
FOLLOWS:**

1. The Albert Park/Radisson Heights Area Redevelopment Plan attached to and forming part of Bylaw 15P88, as amended, is hereby further amended as follows:
 - (a) In the Preface, after the last paragraph insert a new paragraph as follow.

“The Municipal Government Act (MGA) outlines the purpose and scope of powers for municipalities. The Forest Lawn Area Redevelopment Plan is a statutory document that designates an area within the city for redevelopment. The Forest Lawn Area Redevelopment Plan (referred to as ‘this Plan’) must be read in conjunction with the Municipal Development Plan (MDP) Volume 1 and Volume 2 Part 3: The Developed Areas Guidebook (the Guidebook) (see Map 1 for the area that is subject to the Guidebook), the Calgary Transportation Plan (CTP) and other City of Calgary policy and guiding documents, unless otherwise indicated. In the event of a discrepancy between this Plan and the Guidebook, the policy of the Guidebook will prevail.”
 - (b) Delete Section 1.1 Study Boundaries in its entirety and replace with the following:

“The boundaries of the Albert Park/Radisson Heights Area Redevelopment Plan (A.R.P.) are illustrated on Map 1.
 - (c) Delete the existing Map 1 entitled “Action Plan” and the associated Action Plan boxes and text.
 - (d) Delete the existing Map 2 entitled “Study Area” and replace with the revised Map 1 entitled “Study Area” attached hereto as Schedule A.
 - (e) Delete the existing Map 3 entitled “Residential Character Areas” and replace with the revised Map 2 entitled “Residential Character Areas” attached hereto as Schedule B.
 - (f) Delete Section 2.1.1.2 Medium Density Residential in its entirety
 - (g) Delete the existing Map 4 entitled “Commercial/Residential Boundary”.
 - (h) In Section 2.1 Delete the existing Map 5 entitled “Residential Sites Requiring Implementation Action” and replace with the revised Map 3 entitled “Land Use” attached hereto as Schedule C.

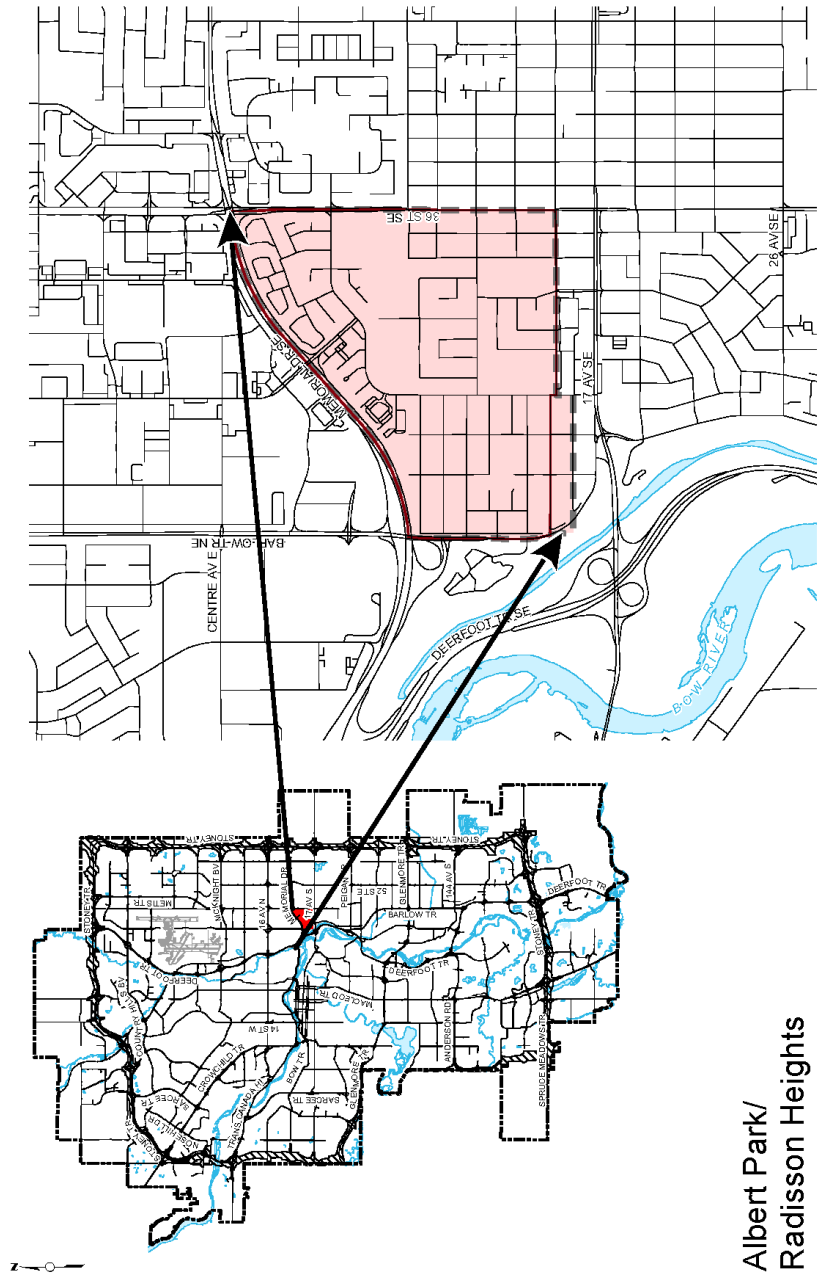
**Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan**

- (i) In Section 2.1.5 Implementation, following the Implementation Chart add a new Map 4 entitled “Residential Sites Requiring Implementation Action” attached hereto as Schedule D.
- (j) In Section 2.1.5 Implementation delete the words “Map 5” and replace with “Map 4”.
- (k) In Section 2.1.5 Implementation Chart delete the text for sites 4 and 5.
- (l) Delete section 2.2 Commercial and all subsections in their entirety.
- (m) Delete the existing Map 6 entitled “Commercial Sites Requiring Implementation Action”.
- (n) Delete existing Map 7 entitled “Traffic Shortcutting Route” and replace with the revised Map 5 entitled “Traffic Shortcutting Route” attached hereto as Schedule E.
- (o) Delete existing Map 8 entitled “Transportation System” and replace with the revised Map 6 entitled “Transportation System” attached hereto as Schedule F.
- (p) Delete existing Map 9 entitled “Sites Requiring Transportation Improvements” and replace with the revised Map 7 entitled “Sites Requiring Transportation Improvements” attached hereto as Schedule G.
- (q) Delete existing Map 10 entitled “Open Space Sites Requiring Implementation Action” and replace with the revised Map 8 entitled “Open Space Sites Requiring Implementation Action” attached hereto as Schedule H.

Proposed Amendments to the Albert Park/Radisson Heights Area Redevelopment Plan

SCHEDULE A

Map 1: Study Area

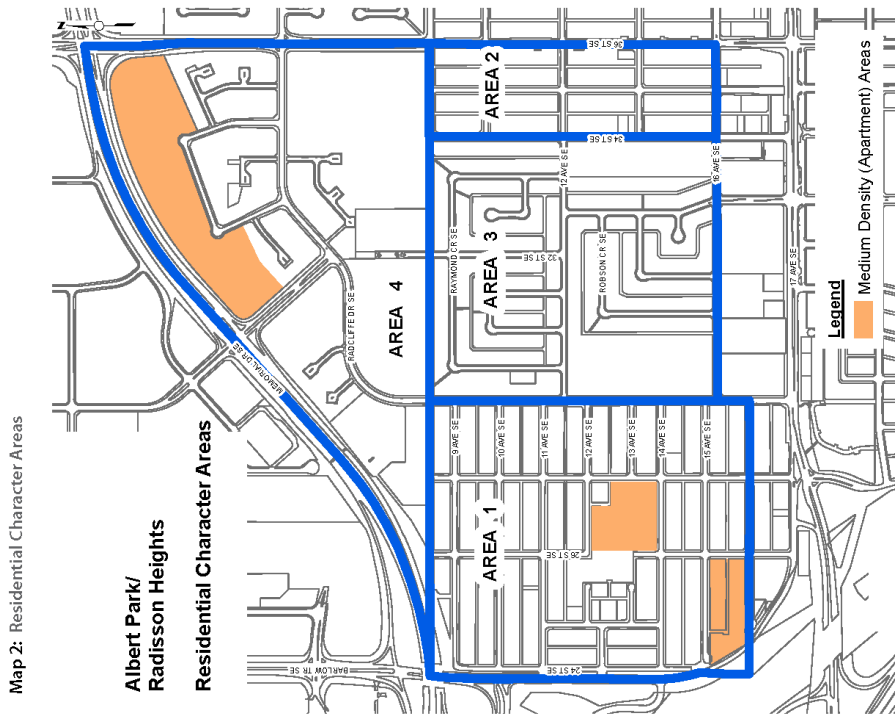


Albert Park/
Radisson Heights
Study Area

This map is conceptual only. No measurements of distances or areas should be taken from this map.

Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan

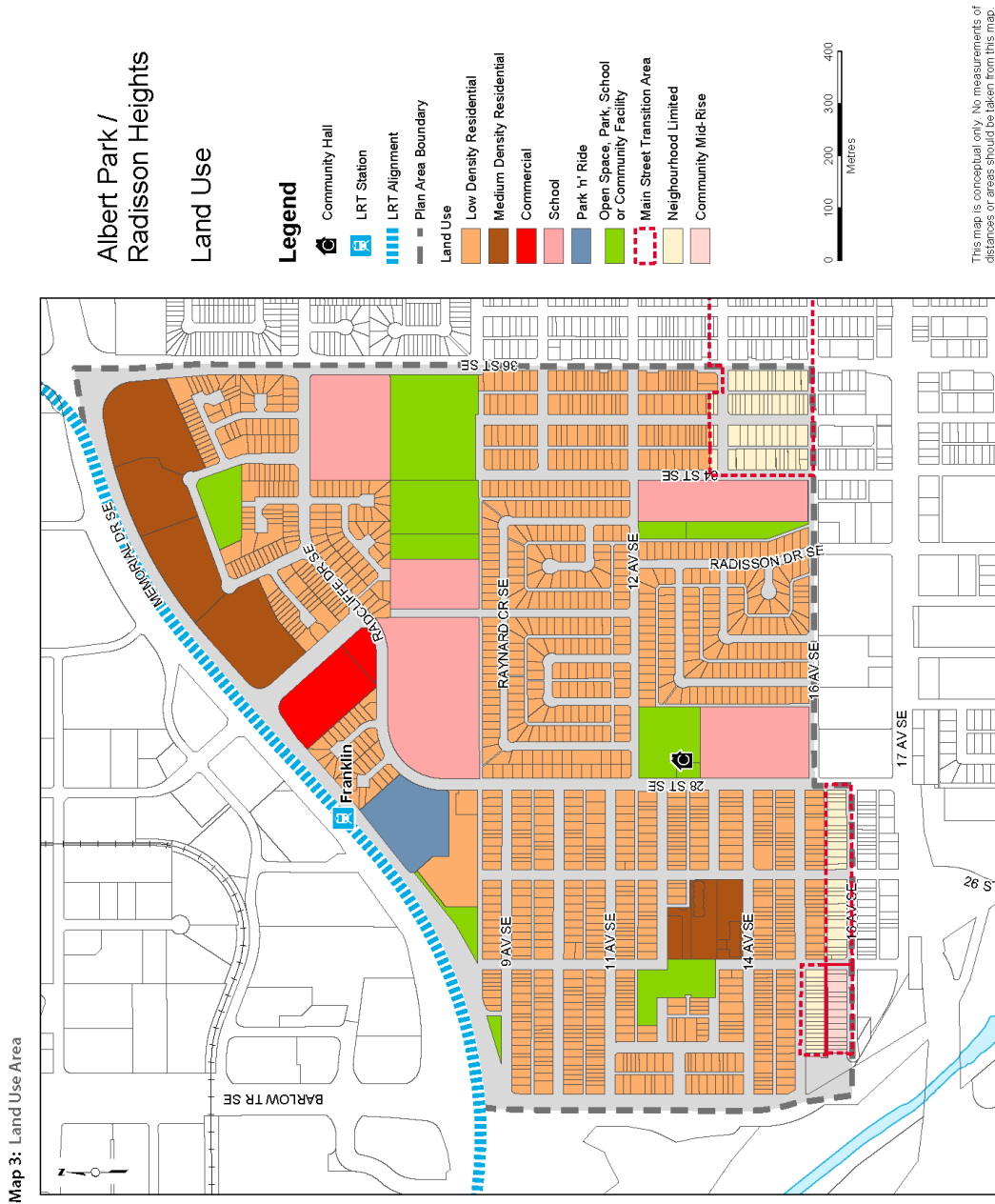
SCHEDULE B



TEXT FOR

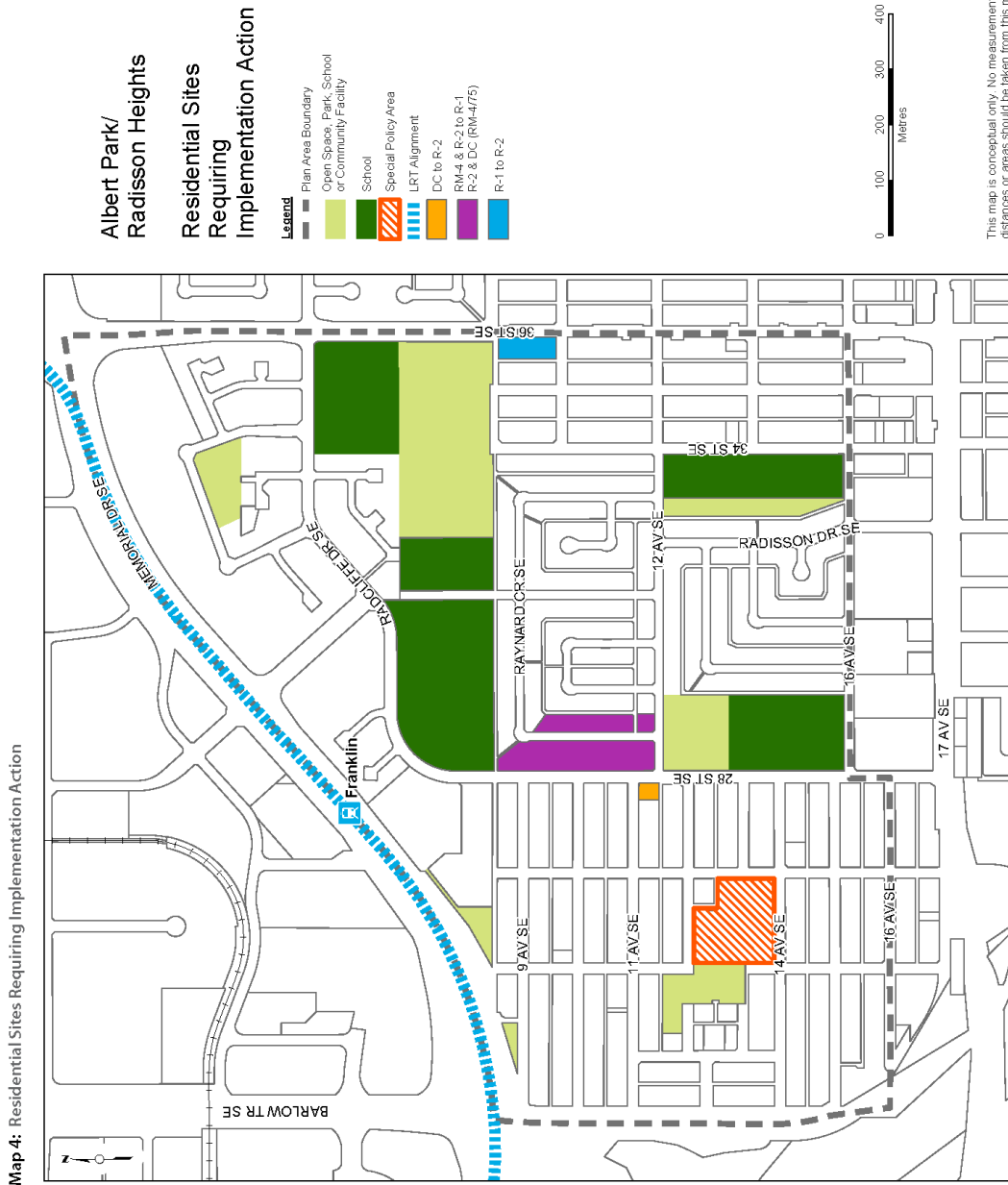
Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan

SCHEDULE C



**Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan**

SCHEDULE D



Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan

SCHEDULE E

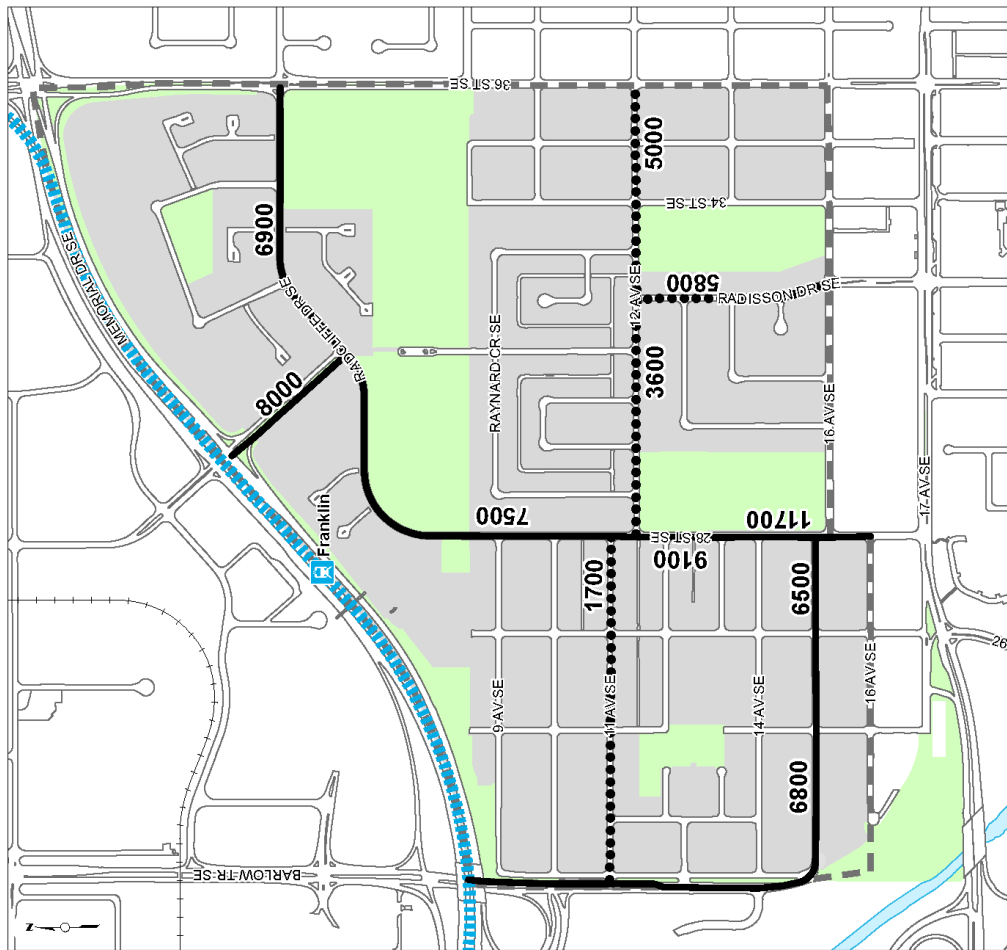
Map 5: Traffic Shortcutting Route

Albert Park /
Radisson Heights
Traffic Shortcutting
Route

- Legend**
- Plan Area Boundary
 - Park or Open Space
 - Primary Shortcutting Route
 - Secondary Shortcutting Route
 - Vehicle Trips Per Day (1986)
Traffic Volumes



This map is conceptual only. No measurements of distances or areas should be taken from this map.



Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan

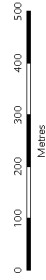
SCHEDULE F

Map 6: Transportation System

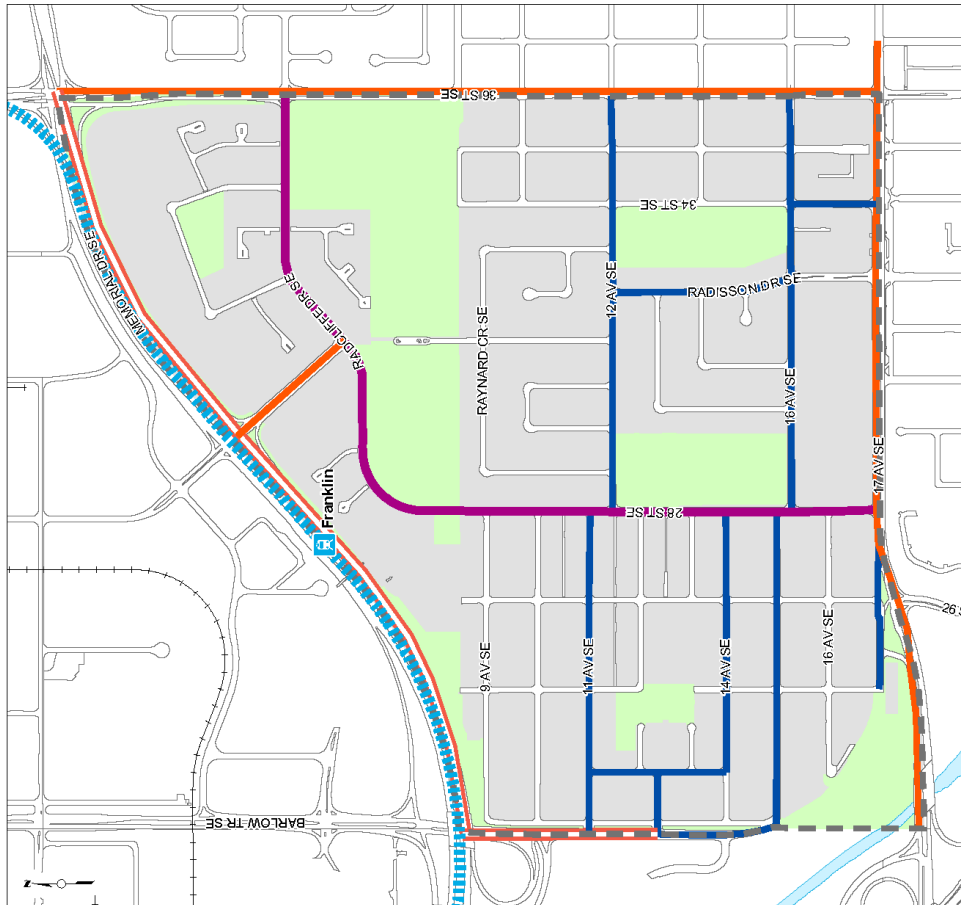
Albert Park/
Radisson Heights

Transportation
System

- Legend**
- Plan Area Boundary
 - LRT Alignment
 - Freeway/ Expressway
 - Major Road
 - Primary Collector
 - Collector
 - Park or Open Space



This map is conceptual only. No measurements of distance or areas should be taken from this map.

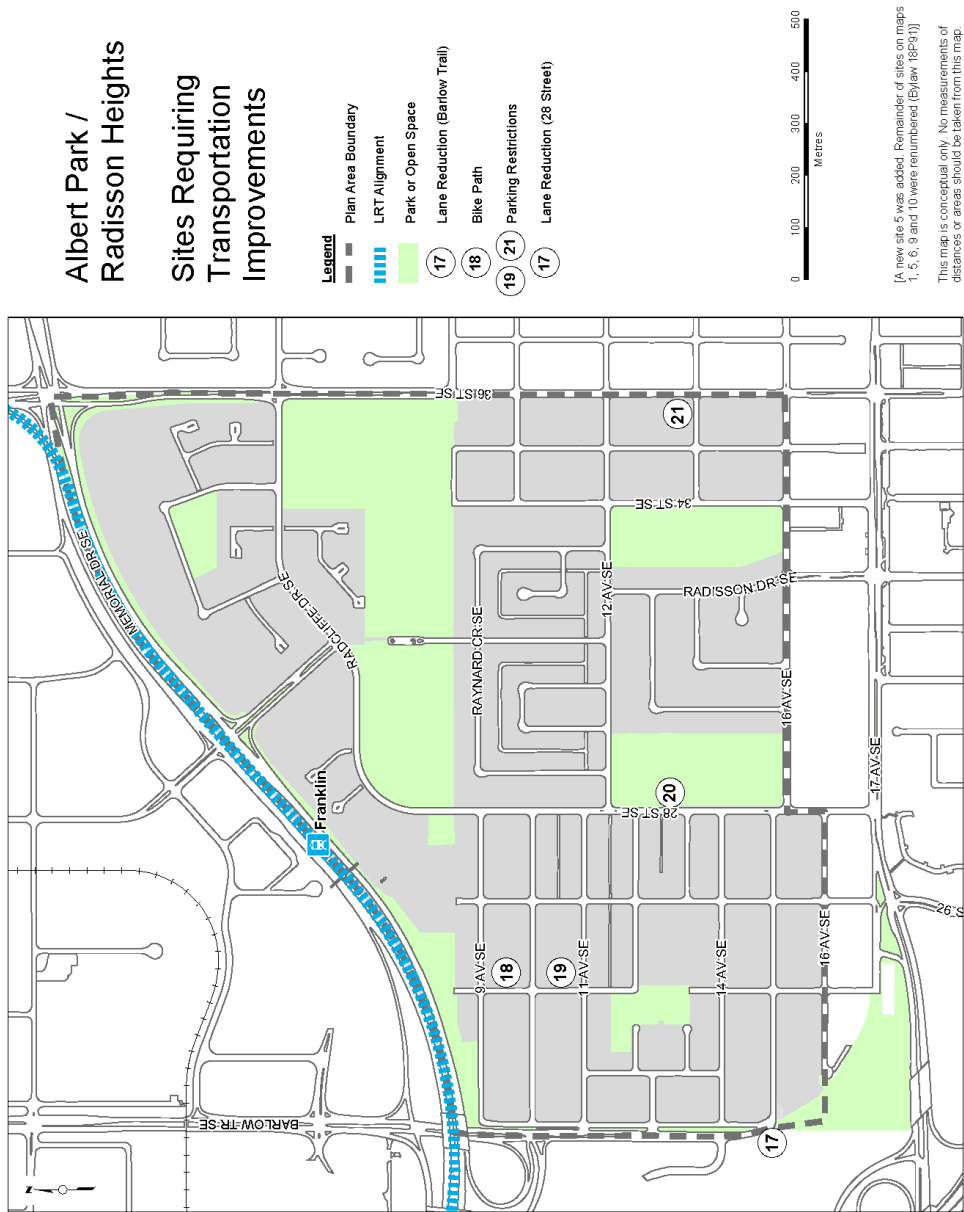


Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan

SCHEDULE G

Map 7: Sites Requiring Transportation Improvements

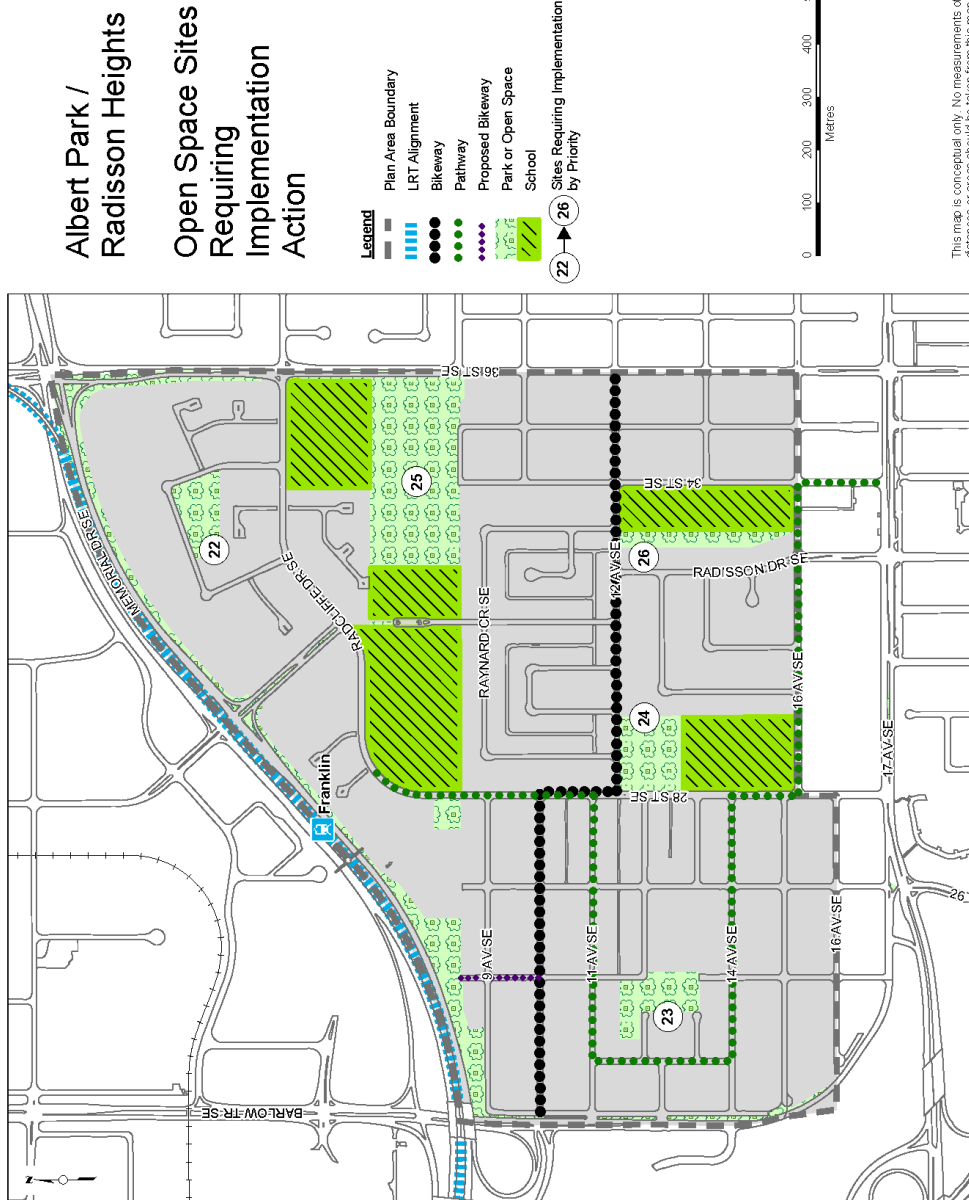
Albert Park /
Radisson Heights
Sites Requiring
Transportation
Improvements



Proposed Amendments to the Albert Park/Radisson Heights
Area Redevelopment Plan

SCHEDULE H

Map 8: Open Space Sites Requiring Implementation Action



Engagement Summary

International Avenue Final Engagement Summary Report for Calgary Planning Commission

Stakeholder Engagement, Research and Communication

Following Calgary Planning Commission, notifications for Public Hearing of Council will be mailed out to land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

City-led engagement

Engagement Overview

A 2-phase engagement and communications strategy was developed to facilitate meaningful opportunity for engagement with in-person and online opportunities. The Engage Spectrum level for this project is Listen and Learn which is defined as "We will listen to stakeholders and learn about their plans, views, issues, concerns, expectations and ideas."

Stakeholders were asked to review the proposed revisions to the Southeast 17 Main Street ARP and share their thoughts. The public was also asked to review the proposed land use districts and consider the balance of Commercial and Residential uses. Stakeholders were also able to share any other ideas or comments on the information provided.

Between November 2017 and February 2018, we offered the following engagement opportunities:

- Online Survey, November 16 to December 11, 2017
- Information Café, November 29, 2017
- Online Survey, January 31 – February 14, 2018
- Information Session, January 31, 2018

Over the course of The City's phased engagement program we collected over 400 comments and ideas from approximately 195 citizens.

What we heard/ what we did

Citizens provided a diversity of comments through all phases of engagement and the main themes identified by citizens across all phases have been highlighted in the chart below including a response in how this feedback was used by The City to inform the draft policy plan.

What we heard (Issue or opportunity identified)	What changes were made
Areas for increased density	The proposed increase in density in the Plan fully aligned with the outcome from several public engagement sessions with the community in 2010.

Engagement Summary

Bus Rapid Transit on 17 Ave SE	BRT's construction implication is outside the scope of the policy work. The BRT construction work is expected to be finalised by the end of 2018.
Density is too restrictive	The density is proposed in specific locations, mainly around the major transit hubs as per the public engagement and charrettes to avoid any negative implication to the existing residential urban fabric. The Plan also proposed transition areas.
Concern regarding pedestrian safety	The Plan includes several policies that enforce the outcome of safe pedestrian realm.
Concerns regarding traffic congestion	The transportation modeling for the Plan Area is built to accommodate the proposed density. The Mobility section in the Plan includes policies that are designed to prevent congestion and it includes specific parking framework to avoid future implications on the Main Street.
Concerns regarding general safety within the plan area	The plan promote the use of Crime Prevention Through Environmental Design (CPTED) policies in all developments in the area aiming to help promoting public safety.
Desire for increased clarity in the information provided and the use of plain language in the plan	The plan has been reviewed and edited to improve clarity.
Concerns regarding transition from existing development to future redevelopment envisioned in the plan	The proposed land use districts are fully aligned with the outcome of all public engagements.

Communications overview

A communications plan was developed to inform the community about the project and the engagement opportunities. The tactics used to inform the public on an ongoing basis included:

- Project specific engage portal page (engage.calgary.ca/17AveSE)
- A postcard was sent advising of a two-part input and information session for your main street area to discuss options for growth with City planners.
- A letter was sent to advise residents of the two events and surveys.
- Social media advertising was used to promote the events and surveys.

The following communications tactics were employed to promote participation in each phase of our engagement program:

- Communication with the local area Councillor
- Facebook advertisement to area residents

Engagement Summary

Full engagement reports

The full What We Heard Reports from each phase with verbatim comments can be found here:

- Phase one engagement: [What We Heard Report](#)
- Phase two engagement: [What We Heard Report](#)

International Avenue BRZ Supporting Letter



September 25, 2018

Jihad Bitar, PhD,
Planning Lead
City of Calgary.
P.O. Box 2100, Stn. M,
Calgary, AB, T2P 2M5

Dear Mr: Bitar

We are writing to express our gratitude and support for the proposed International Avenue Street Area Redevelopment Plan. The International Avenue Business Revitalization Zone (BRZ) is a non-profit organization formed to promote, improve and create a more pleasant community in which to shop and live. Currently, we have over 425 businesses and services that operate within the 35 blocks of 17 Avenue SE from 26 to 61 Streets and area.

The 17th Avenue SE business district is a significant employer and provider of commercial goods for the local community and the city overall. It is a complex mix of businesses and services many of which have been in business for a decade or more, and comes with its own unique local challenges. The proposed ARP does an excellent job in setting forth clear policy to provide positive direction of the main street. Furthermore, the document best represents the key necessary policy framework and deals with some issues that have been outstanding for decades. The core ideas outlined within the ARP we wholeheartedly agree with in order to continue moving International Avenue on a positive path forward.

We met numerous times with administration to discuss issues that have posed significant problem towards our revitalization efforts over the years. The comprehensive document produced does an outstanding job to ensure our concerns are addressed in this document. We are particularly pleased with the document as a whole and believe it provides the best way forward to ensure a bright future for one of Calgary's longest and most complicated avenues. Issues that are specific to our area have been adequately addressed. Overall, we are supportive of the proposed Area Redevelopment Plan and anxiously await it's approval. Thank you very much for this essential document.

Regards,



Alison Karim-McSwiney, MiPM(UK), BA, BREC
Executive Director

250, 3515 17 Avenue SE, Calgary, AB, T2A 0R5
Telephone: 403 248-7288 internationalavenue.ca

Albert Park Community Associations Supporting Letter

From: APRH Community Association [mailto:aprh@shaw.ca]
Sent: September 27, 2018 4:18 PM
To: Bitar, Jihad <Jihad.Bitar@calgary.ca>
Cc: aprh@shaw.ca
Subject: [EXT] Re: Final Albert Park Radisson Heights ARP

Hello Jihad,

Thank you for your presentation at our Board Meeting. We accept and agree with your proposed amendments of the Albert Park Radisson Heights ARP. We, the Board and the community, will support these amendments when they are presented to CPC and City Council.

Thank you for keeping us in the loop with the many changes.

Nancy Kearney
President
Albert Park Radisson Heights Community Association

**Planning & Development Report to
Calgary Planning Commission
2018 October 18**

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CPC2018-1185
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**City Initiated Land Use Amendment - International Avenue (17 Avenue SE) -
LOC2017-0371**

EXECUTIVE SUMMARY

International Avenue (17 Avenue SE) is identified as a main street and is located in the communities of Albert Park, Forest Lawn, Southview and Penbrooke Meadows. The purpose of this report is to bring forward city-initiated land use amendments to various properties within the Albert Park, Forest Lawn and Southview areas in accordance with Council's direction.

The proposed *International Avenue Area Redevelopment Plan* (ARP) and the amendments to existing Area Redevelopment Plans (CPC2018-1184) have been reviewed concurrently with this application and to be heard on the same Calgary Planning Commission and Council Meeting agendas.

The Council approved Main Streets Implementation Plan (PUD2017-0241 and PUD2018-0347) provides direction and a process to support the achievement of goals and targets contained within the Municipal Development Plan (MDP). The initiative is creating more opportunity for private investment to build a more complete community, along with infrastructure investment by The City of Calgary in the public realm for priority MDP corridor areas.

Administration is recommending that Council approve the land use amendments in this report that are associated with the proposed International Avenue ARP and the associated policy amendments (CPC2018-1184) to create certainty for local stakeholders, achieve the MDP goals for Main Streets, shorten approval time for redevelopment with the appropriate land use districts, and complement the recent investments in public realm improvements throughout the area.

Specifically,

- Redesignate portions of the main street to Mixed Use General (MU-1) District and Mixed Use Active Frontage (MU-2) District (Attachment 3);
- Redesignate portions of the main street to Multi-Residential Contextual Low Profile (M-C1) District (Attachment 3); and
- Redesignate the main street's adjacent transition areas to Residential - Grade-Oriented Infill (R-CG) District (Attachment 3).

**City Initiated Land Use Amendment - International Avenue (17 Avenue SE) -
 LOC2017-0371**

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 31.726 hectares \pm (78.395 acres \pm) located at various addresses (See Attachment 1) from Residential-One Dwelling (R-C1) District and Residential – Contextual One / Two Dwelling (R-C2) District **to** Residential – Grade-Oriented Infill (R-CG) District; and
2. Give three readings to the proposed Bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 22.808 hectares \pm (56.358 acres \pm) located at various addresses (See Attachment 1) from Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District and DC Direct Control District **to** Residential – Grade-Oriented Infill (R-CG) District and Multi-Residential – Contextual Low Profile (M-C1) District; and
4. Give three readings to the proposed Bylaw.
5. **ADOPT**, by bylaw, the proposed redesignation of 13.618 hectares \pm (33.651 acres \pm) located at various addresses (See Attachment 1) from Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District, DC Direct Control District, Multi-Residential – Contextual-Oriented (M-CG) District, Commercial – Corridor 2 (C-COR2) District and Special Purpose – Community Institution District (S-C1) District **to** Residential – Grade-Oriented Infill (R-CG) District, Multi-Residential – Contextual-Oriented (M-CG) District, Multi-Residential – Contextual Low Profile (M-C1) District, Multi-Residential – High Density Low Rise (M-H1) District, Mixed Use – Active Frontage (MU-2f2.0h10) District, Mixed Use – Active Frontage (MU-2f3.0h23) District, Mixed Use - Active Frontage (MU-2f3.0h27) District and Mixed Use - Active Frontage (MU-2f4.5h20) District; and
6. Give three readings to the proposed Bylaw.
7. **ADOPT**, by bylaw, the proposed redesignation of 13.184 hectares \pm (32.576 acres \pm) located at various addresses (See Attachment 1) from Residential – Contextual One / Two Dwelling (R-C2) District, DC Direct Control District, Multi-Residential – Contextual-Oriented (M-CG) District, Multi-Residential – Contextual Low Profile (M-C1) District, Commercial – Corridor 1 (C-COR1) District and Commercial – Corridor 2 (C-COR2) District **to** Multi-Residential – Contextual Low Profile (M-C1) District, Multi-Residential – Contextual Medium Profile (M-C2) District, Mixed Use - General (MU-1f2.0h12) District, Mixed Use - General (MU-1f3.0h46) District, Mixed Use - General (MU-1f5.0h37) District, Mixed Use – Active Frontage (MU-2f3.0h46) District, Mixed Use - Active Frontage (MU-2f4.5h20) District, Mixed Use - Active Frontage (MU-2f4.5h23) District, Mixed Use - Active Frontage (MU-2f5.0h26) District and Mixed Use - General (MU-1f3.0h16) District; and

**City Initiated Land Use Amendment - International Avenue (17 Avenue SE) -
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8. Give three readings to the proposed Bylaw.
9. **ADOPT**, by bylaw, the proposed redesignation of 17.135 hectares \pm (42.339 acres \pm) located at various addresses (See Attachment 1) from Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District, Multi-Residential – Contextual-Oriented (M-CG) District, Multi-Residential – Contextual Low Profile (M-C1) District, DC Direct Control District, Commercial – Corridor 1 (C-COR1) District and Commercial – Corridor 2 (C-COR2) District **to** Multi-Residential – Contextual Low Profile (M-C1) District, Mixed Use - General (MU-1f3.5h22) District, Mixed Use - General (MU-1f4.5h20) District and Mixed Use - Active Frontage (MU-2f4.5h20) District; and
10. Give three readings to the proposed Bylaw.
11. **ADOPT**, by bylaw, the proposed redesignation of 14.952 hectares \pm (36.946 acres \pm) located at various addresses (See Attachment 1) from Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District, Multi-Residential – Contextual-Oriented (M-CG) District, DC Direct Control District, Commercial – Neighbourhood 2 (C-N2) District and Commercial – Corridor 2 (C-COR2) District **to** Multi-Residential – Contextual Low Profile (M-C1) District, Mixed Use - General (MU-1f3.0h16) District, Mixed Use - General (MU-1f4.5h20) District and Mixed Use - Active Frontage (MU-2f3.0h16) District; and
12. Give three readings to the proposed Bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

At the 2016 September 13 Combined Meeting of Council, Council decided to:

ADOPT, Moved by Councillor Chabot, Seconded by Councillor Carra, that Councillor Chabot's Motion, NM2016-25 be adopted, **after amendment**, as follows:

NOW THEREFORE BE IT RESOLVED that the Planning & Development Department be directed to develop an area specific sign policy similar to the Stephen Avenue sign policy;

AND FURTHER BE IT RESOLVED that administration, in consultation with stakeholders, be directed to review and bring forward amendments to the 17th Avenue Corridor Study with the goal of having it approved as a statutory document and to return to Council at the Mid Cycle Adjustments deliberations with funding requests if required.

CARRIED

Planning & Development Report to
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At the 2017 April 10 Combined Meeting of Council, Council decided to:

ADOPT, Moved by Councillor Chabot, Seconded by Councillor Woolley, that the SPC on Planning and Urban Development Recommendation contained in Report PUD2017-0241, be adopted, as follows:

That Council approve the Main Streets Implementation Plan.

BACKGROUND

In 2016 September, Council directed Administration to investigate policy amendments to the *Southeast 17 Corridor: Land Use and Urban Design Concept* (Corridor Concept Plan) to become a statutory document and to include signage policy specific to 17 Avenue SE (NM2016-25).

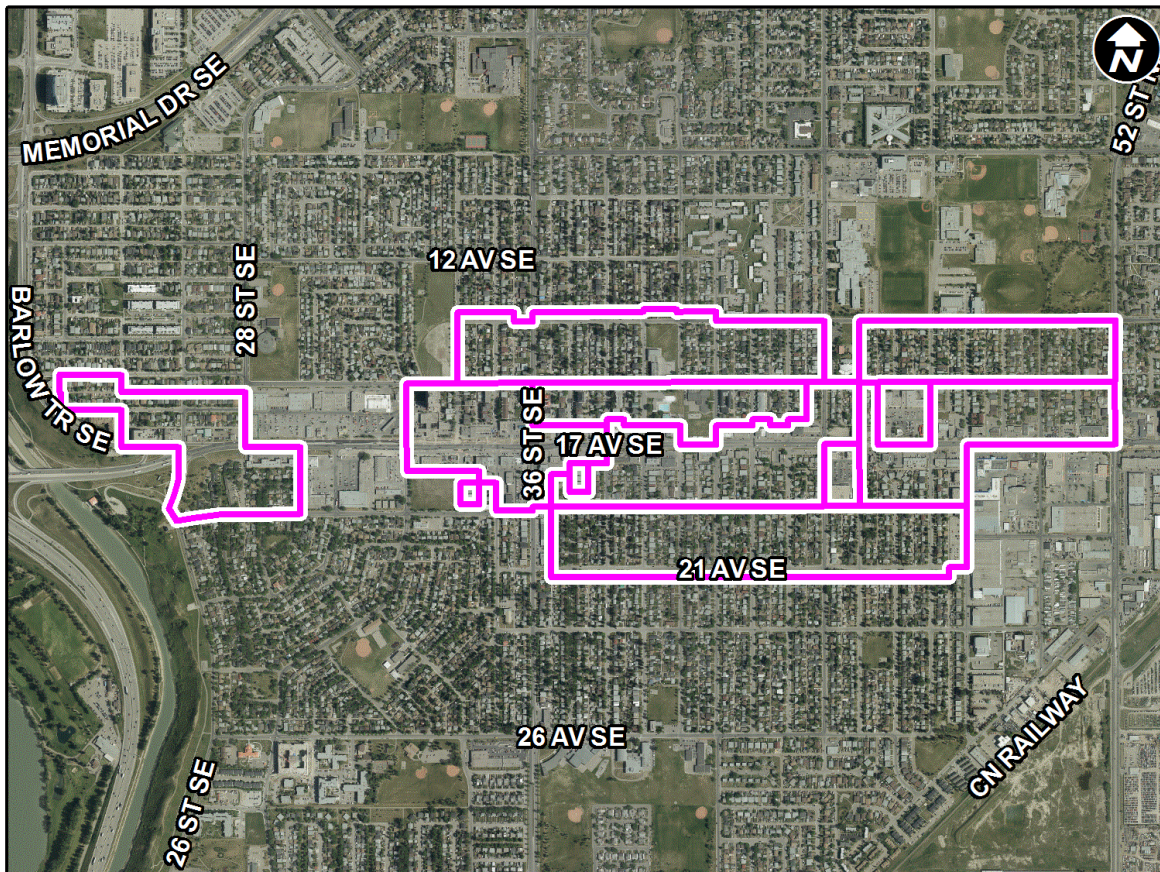
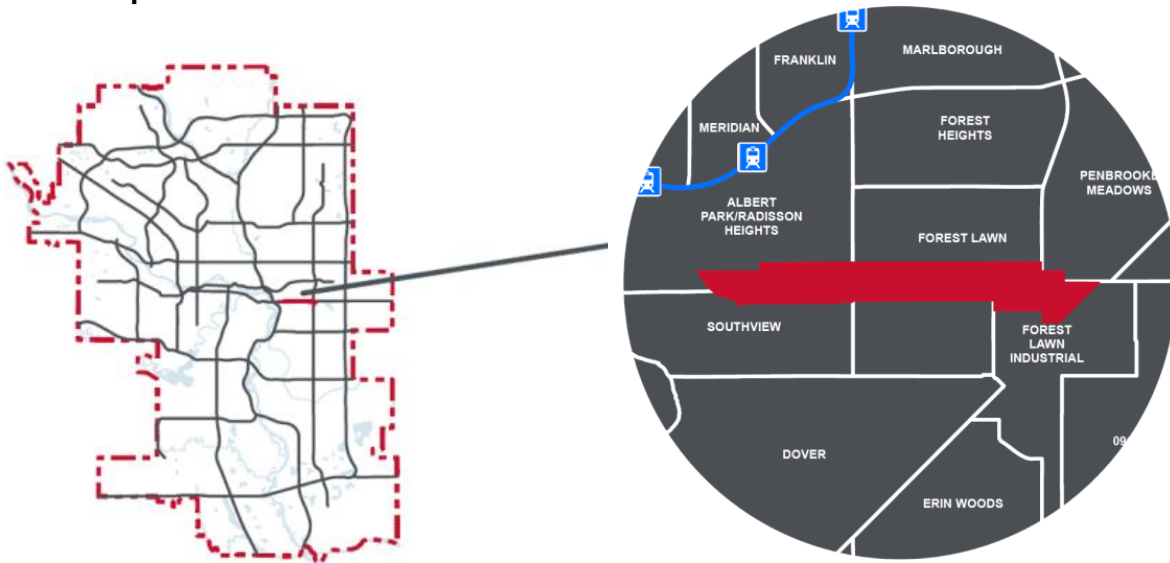
In 2017 April, Council approved the Main Streets Implementation Plan (PUD2017-0241 and PUD2018-0347) with a focus on the application of policies, goals and targets contained within the MDP.

As a result of the Council directions for 17 Avenue SE and the Main Street Initiative, this project combined the efforts of the Main Street team with the Area Redevelopment Plan project to develop the policy and land use framework that includes the policies of the Developed Areas Guidebook (MDP, Volume 2, Part 3) and provided a comprehensive public engagement process.

The result is the proposed International Avenue Area Redevelopment, amendments to the *Forest Lawn/Forest Heights/Hubalta* and the *Albert Park/Radisson Heights ARPs* as per recommendations in report CPC2018-1184 and the City-initiated land use amendments that are covered in this report.

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Location Maps



**City Initiated Land Use Amendment - International Avenue (17 Avenue SE) -
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Site Context

The existing development pattern generally features small-scale, local-oriented retail/commercial uses along most parts of 17 Avenue SE, with large-scale commercial developments located in the east and west portions of the area. This pattern reflects the historical development and function of 17 Avenue SE as both highway commercial and local retail. The street pattern in the area is based on a standard north/south, east/west rectangular grid. The north/south streets provide for excellent accessibility and interconnectivity between the commercial corridor and the adjacent residential communities.

Although some multi-residential buildings have been developed over the past couple decades on the blocks behind the commercial corridor, low-density residential development is still the predominant land use in these areas. Generally, the local population has been declining from the peak population over twenty years ago. Based on the analysis of existing development and the construction of the new Southeast BRT Transit way, there is significant redevelopment potential in the area.

Albert Park/ Radisson Heights	
Peak Population Year	2017
Peak Population	6,745
2017 Current Population	6,745
Difference in Population (Number)	0
Difference in Population (Percent)	0

Forest Lawn	
Peak Population Year	1982
Peak Population	9,088
2017 Current Population	7,772
Difference in Population (Number)	-605
Difference in Population (Percent)	-14%

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Southview	
Peak Population Year	1970
Peak Population	3,464
2017 Current Population	1,841
Difference in Population (Number)	-1,623
Difference in Population (Percent)	-47%

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Main Street Initiative – Land Use

From 2015 September to 2016 June, the Main Streets Initiative focused on analysis and evaluation of a variety of factors that influence growth and change in the Main Streets areas. This included detailed analysis of feedback from stakeholders, review of the existing policy, land use districts and parking requirements. It also looked at current and future market demand and growth forecasts, infrastructure capacity within the Main Street areas and costs to redevelop streets and sidewalks that align with the *Calgary Transportation Plan (CTP)* and the *Complete Streets Guide*.

Existing Land Use

17 Avenue SE is intended to be an urban main street, serving the local neighbourhood as well as being a city-wide destination. The current land use districts do not align with Council approved policy. The existing districts allow for a variety of mixed and commercial uses along 17 Avenue SE, with some transitional apartment uses largely to the north, but pose challenges for row house and townhouse development. Redesignations would allow for greater flexibility for mixed use development along the main street as well as more housing options for the surrounding community and population to support 17 Avenue SE businesses.

Proposed Land Use

The proposed land use districts for 17 Avenue SE are largely based on feedback received from stakeholder groups. When community stakeholders provided input about the future of this area, they shared that they would like to see their main streets continue to evolve into destinations with a mix of amenities serving the local neighbourhood and attract Calgarians from across the city, supported by a wider range of housing options and investments in public space and streetscape. In the spring of 2018, local residents provided detailed feedback at public input sessions on a proposed land use framework to enable this vision. These comments were considered when refining this proposal.

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The area where the three typologies of the Community building blocks are applied will consist of mixed use, mid to high-rise building types that accommodate a range of retail, service, office and residential uses that may be arranged vertically within a building or horizontally across an area in multiple buildings. This area is proposed to be designated Mixed Use - General (MU-1) and Mixed Use – Active Frontage (MU-2.) with a variety of building heights set by a “h” modifier.

Neighbourhood - Low Rise areas are proposed to be Multi-Residential – Contextual Grade-Oriented M-CG and Multi-Residential – Contextual Low Profile M-C1 while the Neighbourhood Residential Limited areas are proposed to have Residential – Contextual Grade-Oriented R-CG designation.

Policy 3.4.3 c of the MDP indicates an Urban Main Street should provide a land use framework of a minimum of 200 jobs and population per gross developable hectare. This policy serves as a measurable unit that achieves multiple-objectives, and is easy to measure and monitor over time.

Table 1 indicates that the intensity (people + jobs per gross developable hectare) achievable under the existing land use districts is 169, however actual existing intensity of the area is approximately 51. Through the proposed land use amendments, the area will be able to achieve an intensity of 234, which exceeds the requirements of the MDP.

Table 1: 17 Avenue SE Density/Intensity

17 Avenue SE (Deerfoot Trail to 52 Street SE)			
	EXISTING	POTENTIAL BY CURRENT LAND USE	POTENTIAL BY NEW LAND USE DISTRICTS
Number of jobs	3,087	7,665	7,554
Number of people	2,726	11,580	19,149
Number of dwelling units	1,435	5,338	10,013
Intensity (Job + Population/ gross developable hectare) (Target 200)	51	169	234

Table 2 identifies a maximum potential of up to 1,230 dwelling units under the present land use districts in the residential lands surrounding the Main Street, with a current housing stock of 1,003 units. The proposed land use redesignation will allow for a maximum of 1,407 dwelling units.

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Table 2: Surrounding Area Residential Density/Intensity

Surrounding area (R-CG)			
	EXISTING	ALLOWED BY CURRENT LAND USE	POTENTIAL ALLOWED BY NEW LAND USE DISTRICTS
Number of jobs	86	150	171
Number of people	2,250	3,936	4,502
Number of dwelling units	1,003	1,230	1,407

To achieve goals and objectives of both the MDP and the proposed Plan, the proposed land use designations are structured to support the necessary range of uses and appropriate scale of buildings along International Avenue and the transition into the surrounding community. Table 3 indicates the extent of the proposed land use districts, specifically area and number of parcels.

Table 3: The Proposed Land Use District

District	Hectares	Acres	# parcels
Residential – Grade-Oriented Infill (R-CG)	36.7	90.6	677
Multi-Residential – Contextual Grade-Oriented (M-CG)	1.22	3.015	19
Multi-Residential – Contextual Low Profile (M-C1)	20.61	50.9	325
Multi-Residential – Contextual Medium Profile (M-C2)	1.52	3.76	17
Multi-Residential – High Density Low Rise District (M-H1)	2.26	5.58	2
Mixed Use - General f2.0h10 (MU-1 f2h10)	0.68	1.68	8
Mixed Use - General f3.0h16 (MU-1 f3h16)	1.70	4.2	14
Mixed Use - General f4.5h20 (MU-1 f4.5h20)	2.07	5.12	26
Mixed Use - General f4.5h23 (MU-1 f4.5h23)	0.24	0.59	4
Mixed Use - General f5.0h37 (MU-1 f5h37)	0.56	1.38	2
Mixed Use - Active Frontage f2.0h10 (MU-2 f2h10)	0.71	1.75	4
Mixed Use - Active Frontage f3.0h16 (MU-2 f3h16)	1.95	4.8	12
Mixed Use - Active Frontage f3.0h23 (MU-2 f3h23)	0.22	0.54	2

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Mixed Use - Active Frontage f3.0h27 (MU-2 f3h27)	0.33	0.82	6
Mixed Use - Active Frontage f3.0h46 (MU-2 f3h46)	0.8	1.98	55
Mixed Use - Active Frontage f4.5h20 (MU-2 f4.5h20)	3.96	9.79	6
Mixed Use - Active Frontage f4.5h23 (MU-2 f4.5h23)	0.34	0.84	3
Mixed Use - Active Frontage f5.0h26 (MU-2 f5h26)	1.31	3.24	9
Totals	77.2	190.8	1191

Implementation

The Main Streets implementation strategy includes streetscape master plan design work for public realm, street cross-section, traffic calming/management, options for all modes (if not within the cross-section, then potentially on nearby parallel routes), and parking management. This streetscape master plan design work for 17 Avenue SE is funded through the BRT construction budget.

Actual uptake of redevelopment opportunities enabled by the proposed redesignations are likely to take place over multiple decades. Through the development permit process, Administration will have the opportunity to ensure that the details of each project (streetscape, walking and cycling access and facilities, as well as vehicle access, parking, and loading) are satisfactory. The proposed land use designations all include mandatory minimum requirements for vehicle and bicycle parking, as well as provisions for loading and visitor parking.

The proposed changes are expected to help 17 Avenue SE evolve into a vibrant destination with a mix of amenities serving the local neighbourhood and attracting Calgarians from across the city, supported by a wider range of housing options and investments in public space and streetscape. This in turn is expected to stimulate further redevelopment of the area.

Infrastructure

Transportation Networks

The land use redesignations help to achieve Calgary Transportation Plan (CTP) transportation goals relating to:

- alignment of infrastructure investment and land use direction;
- affordable mobility and universal access; and
- strengthening the value proposition for walking, cycling, and public transit.

The grid layout creates a mobility framework with many options for access to development sites for all modes of transportation. Redevelopment of sites along the Main Street will benefit from:

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- a robust grid of streets, allowing vehicle access from defined intersections and dispersal of local access and on-street visitor parking across multiple streets;
- frequent transit service within walking distance along the Primary Transit Network; and
- established walking and cycling infrastructure including complete sidewalk networks, on- and off-street cycling routes, and the city's pathway network.

Although these land use redesignations are strategically located, the Main Streets program and other City initiatives, such as RouteAhead and the Pedestrian Strategy, have identified critical improvements to the infrastructure for all modes of transportation that will require further investment in order to support The City's long-term vision.

The proposed land use redesignations benefit from proximity to the Centre City, as well as service on Routes 1 and BRT with Primary Transit Network status for service along 17 Avenue SE. Through the grid of neighbourhood streets and collector roads, redevelopment sites have easy access to arterial streets, urban boulevards, neighbourhood boulevards, and the skeletal road network.

Utilities and Servicing

Several identified utility infrastructure upgrades are already occurring along this street through the Southeast BRT project.

As the Main Streets program continues to develop, Administration is working on a number of initiatives aimed at viable and successful redevelopment. This work includes:

1. Sanitary, potable water, and fire flow capital upgrade analyses for 17 Avenue SE have been performed to ensure that utility servicing can support the ultimate proposed land use and intensity increases. City budgets are informed with current and long-range assessments of major infrastructure improvements that could be triggered by increased Main Streets intensities.
2. Contribution to Calgary Growth Strategies' Industry/City Work Plan: Established Areas Initiative. This initiative undertakes cross-departmental actions to reduce barriers to redevelopment. This includes, but is not limited to:
 - Piloting communities and specific development sites to identify, plan, and prioritize technical requirements to enable redevelopment.
 - Review underutilized road rights-of-way on key corridors to increase flexibility of use of space.
 - Development of funding tools to support developed areas investment.
 - Review of the Centre City Levy program within updated legislative framework.
 - Work towards a strategic growth plan for the established areas.
3. Continued OneCalgary alignment. This will ensure that any necessary capital investments have ample time to be budgeted for and aligned with other Business Unit

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needs, including streetscape master plan improvements, road surface improvements, potential Water Resources upgrades, etc.

4. An ongoing commitment from Water Resources to support redevelopment where applicant-triggered infrastructure upgrades may create opportunities to improve utility networks in ways that benefit a larger set of redevelopment sites.

Stakeholder Engagement, Research and Communication

The communities adjacent to this main street area (Forest Lawn, Albert Park, Radisson Heights, Southview and Penbrooke Meadows) were provided opportunities to comment on the Main Street Initiative. A two-phase engagement and communications strategy was developed in order to be wide-reaching in the affected communities and to facilitate meaningful opportunities for engagement both in-person and online. The Engage Spectrum level for this project is Listen and Learn which is defined as “We will listen to stakeholders and learn about their plans, views, issues, concerns, expectations and ideas.”

Stakeholders were asked to review and comment on the proposed land use districts and consider the balance of commercial and residential uses. Additionally, stakeholders were able to share any other ideas or comments on the information provided that were taken in consideration in The Plan.

See Attachment 4 for 2018 Final Engagement Summary Report

Engagement and public meeting

Citizen Comments

The focus of Phase 1 of the Main Streets initiative was stakeholder discussion and public engagement. From November 2014 to May 2015 there were five public engagement opportunities for each of the 24 MDP corridors or main street areas. This resulted in 1,335 hours of discussion and 4,112 comments provided by over 2,200 Calgarians at events and online. As well, the Main Streets team presented project details at 20 meetings to community associations, business revitalization zones and other groups during this same time. Following this public engagement phase, Local Viewpoint Maps were created for most main street areas including the 17 Avenue SE.

Public Meetings

As result of Council direction in 2016 September to update the Corridor Concept Plan to become a statutory document, several meetings and open houses were offered for landowners, stakeholders and residents to provide their feedback and input on the ARP and on the conceptual land use amendment. Two information sessions were held, the first one in 2017 November and the second in 2018 January, with online opportunities to provide input at the same time as these in-person opportunities, followed by additional discussions with landowners and community associations as requested.

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The first session presented a model for new land use framework using the scale of development ranges proposed in the Developed Areas Guidebook, supported by examples of building forms and types. Worksheets asked participants about the range of building types, heights and locations of land uses to determine an appropriate land use district pattern. This new land use district pattern was vetted with residents and stakeholders and then refined after input from the second workshop.

Additional revisions occurred where community associations or residents requested more discussion. The finalized land use pattern determined the required local area plan amendments and was aligned to the vision of the new ARP. This proposal was mailed to all impacted property owners with specific details on the recommended changes for the main street area.

A total of 127 people attended both information session events held at the Forest Lawn Community Centre. In addition, approximately 390 pieces of feedback from in person and online opportunities were received. A summary of engagement events is listed below:

- November 2017 – Information Café. Gather public input on draft policy and review of proposed Developed Areas Guidebook building blocks.
- January 2018 – Open House. Share Information Café outcomes, draft policy plan, and gather public input on proposed rezoning.
- March 2018 – Meeting with Albert Park CA to share engagement outcomes and the relevant Albert Park ARP amendments.
- March 2018 – Meeting with Forest Lawn CA to share engagement outcomes and the relevant Forest Lawn ARP amendments. (No concerns were raised at the time of the meeting and the Open House. Administration received no comments)
- April 2018– Meeting with Albert Park CA to share the draft for the Albert Park ARP amendments. Both, the current and the amended Albert Park ARP versions were shared with the CA's members as hard copies and electronically.
- September 2018 – Last meeting with Albert Park CA to share the final draft for the Albert Park ARP amendments. (Administration received a support email, Attachment 6)

The project team has met three times with the International Avenue Business Revitalization Zone (BRZ) to discuss signage policies and the development of the updated ARP. The BRZ is generally in support of The Plan and associated amendments. (Administration received a letter of support, Attachment 5)

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Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014) (SSRP)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this corridor, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009) (MDP)

The *Municipal Development Plan (MDP)* proposes a compact urban form for Calgary by encouraging increased population and job growth to occur in the existing, built-up areas of the city. In particular, activity centres, corridors, and other areas that are well connected to the existing and future Primary Transit Network are identified as the primary areas to accommodate significant residential and employment growth. The MDP identifies this area as an Urban Main Street typology with emphasis as a multi-modal environment fronted by a mix of higher intensity residential and business uses that foster a vibrant street environment. The MDP also establishes a minimum intensity threshold of 200 jobs and population per gross developable hectare for an Urban Main Street.

In order to recognize the local context and create an urban environment that supports and integrates new development with existing communities, the specific density target for each corridor must be established through a Local Area Plan. The MDP identifies the remainder of the community of Forest Lawn within the Contextual Area as Inner City and Established typologies. These land use typologies are intended to experience gradual intensification over the next 60 years.

Calgary Transportation Plan (Statutory, 2009) (CPT)

The CTP identifies 17 Avenue SE as an Urban Boulevard where highest priority should be given to walking, cycling and transit and where the "Complete Street" policies should be implemented.

Albert Park/Radisson Heights Area Redevelopment Plan (Statutory, 1989)

Adopted by Council in 1980, the *Albert Park/Radisson Heights ARP* identifies 17 Avenues SE as growth and redevelopment areas, but does not provide up-to-date guidance for mixed-use and commercial development, and does not allow for complementary and supportive ground-oriented residential forms nearby.

The land use amendments supporting the amended policy (*International Avenue ARP*) in CPC2018-1184 align the ARP with the MDP, support the City's overall vision for Albert Park/Radisson Heights urban and neighbourhood main streets, and provide more appropriate guidance for mixed-use, commercial, and residential intensification along and nearby 17 Avenue SE.

**City Initiated Land Use Amendment - International Avenue (17 Avenue SE) -
LOC2017-0371**

Forest Lawn/Forest Heights Hubalta Area Redevelopment Plan (Statutory, 1995)

Adopted by Council in 1995, the *Forest Lawn/Forest Heights Hubalta ARP* identifies 17 Avenues SE as growth and redevelopment areas, but does not provide up-to-date guidance for mixed-use and commercial development, and does not allow for complementary and supportive ground-oriented residential forms nearby.

The land use amendments supporting the amended policy (*International Avenue ARP*) in CPC2018-1184 align the ARP with the MDP, support the City's overall vision for Forest Lawn urban and neighbourhood main streets, and provide more appropriate guidance for mixed-use, commercial, and residential intensification along and nearby 17 Avenue SE.

Social, Environmental, Economic (External)

The proposed land use districts will guide development towards the creation a vibrant, transit-focused, liveable and distinctive mixed use community.

Social

The proposed land use districts will provide opportunities for a broad range of housing and employment options.

Environmental

An Environmental Site Assessment was not required. Efficient use of land and resources is a key aspect of working toward environmental sustainability. The approach to concentrate jobs and population on Main Streets that are well served by public transit, and where people can walk or cycle to meet many of their daily needs results in reduced dependence on the private automobile and greater mobility choice. Reduced automobile dependence also leads to corresponding reductions in greenhouse gas emissions, as 36 percent of all emissions in Calgary come from vehicular transportation. More efficient use of land for development also contributes to reducing impacts on the environment and natural systems both within the city and regionally.

Economic

Long term land use policy and City initiated land use district amendments represent a strategic approach to ensure an adequate supply of housing and employment space to keep pace with demands in the established communities. Supporting such supply increases in locations with existing and planned infrastructure is a key component in reducing the fiscal impact of growth, while also acting to attenuate pressure on housing affordability.

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Financial Capacity

Current and Future Operating Budget:

Calgary Transit and Roads are providing operating budgets for 17 Avenue SE BRT transit facilities and the upgraded 17 Avenue SE roadway. Calgary Transit, Roads, Parks and Recreation, as part of normal business operations, monitor and determine future needs for expanding services levels. As these departments identify these new citizen needs, those departments will determine future capital budget requirements.

Current and Future Capital Budget:

The construction of the 17 Avenue SE BRT project provides a complete street upgrade for the full width of the road right of way with new sidewalks, intersection signals and transit facilities. This construction project is fully funded and set to begin operation in the fall of 2018. The Main Streets program is not proposing any new funding for 17 Avenue SE as the new road cross section will support Council's policy objectives and new private developments for years to come.

Risk Assessment

Citizen and stakeholder interest and potential monetary investment in new development is very high for the Main Street areas. This current opportunity will be leveraged with City of Calgary actions to facilitate growth in identified strategic areas such as 17 Avenue SE. Not proceeding with guiding policy and land use redesignations could slow momentum created with many stakeholders and potentially develop mistrust with citizens and the development industry.

REASONS FOR RECOMMENDATIONS:

Administration is recommending approval of the proposed land use amendments for the following reasons.

The proposed amendments:

- Respond to Council direction to action the Main Streets Implementation Plan,
- Provide additional direction and certainty for area residents, landowners and applicants; and
- The proposed city initiated land use resignations will allow for an appropriate scale of development and intensity within the context of the communities of Forest Lawn and Albert Park/Radisson Heights.

ATTACHMENT(S)

1. Municipal Address for City-Initiated Redesignation
2. Proposed Land Use – Colour Graphics
3. Proposed Land Use Maps (All bylaws)
4. Final Engagement Summary Report 2017/18
5. International Avenue BRZ Supporting Letter
6. Albert Park Community Associations Supporting Letter

Municipal Address for Redesignation

Bylaw #1

14 Avenue			
3502 14 AV SE	PLAN 5498T BLOCK 6 LOTS 1 AND 2	R-C1	R-CG
3611 14 AV SE	PLAN 5498T BLOCK 4 LOTS 21 AND 22	R-C2	R-CG
3818 AND 3820 14 AV SE	PLAN 7039GGT BLOCK 23 LOTS 20 AND 21	R-C2	R-CG
4004 AND 4008 14 AV SE	PLAN 5111HH BLOCK 21 LOT 1	R-C2	R-CG
4111 AND 4113 14 AV SE	PLAN 7113GW BLOCK 13 LOT 6	R-C2	R-CG
4311 14 AV SE	PLAN 1273JK BLOCK 15 LOT 11	R-C2	R-CG
4411 14 AV SE	PLAN 127LK BLOCK 16 LOT 1	R-C1	R-CG
4709 AND 4711 14 AV SE	PLAN 1681GS BLOCK 12 LOT 11	R-C2	R-CG
4909 14 AVE SE	PLAN 6115HS BLOCK 22 LOT 20	R-C2	R-CG
16 Avenue			
3506 16 AV SE	PLAN 5498T BLOCK 3 LOTS 1 AND 2	R-C1	R-CG
3602 16 AV SE	PLAN 5498T BLOCK 4 LOTS 39 AND 40	R-C2	R-CG
3730 16 AV SE	PLAN 2700AH BLOCK 9 LOT 40	R-C2	R-CG
4102 16 AV SE	PLAN 7282HK BLOCK 13 LOT 15	R-C2	R-CG
4114 16 AV SE	PLAN 7282HK BLOCK 13 LOT 14	R-C2	R-CG
4208 16 AV SE	PLAN 5637HR BLOCK 15 LOT 1	R-C2	R-CG
4920 16 AV SE	PLAN 727HW BLOCK 29 LOT 11	R-C2	R-CG
4412 16 AV SE	PLAN 127LK BLOCK 16 LOT 12	R-C1	R-CG
5204 16 AV SE	PLAN 5519AC BLOCK 4 LOTS 21 TO 24	R-C2	R-CG
34 Street			
1502 34 ST SE	PLAN 5498T BLOCK 3 LOTS 19 AND 20	R-C1	R-CG
1506 TO 1524 34 ST SE	PLAN 1383HI BLOCK 3 LOTS 14 TO 17	R-C1	R-CG
1524 34 ST SE	PLAN 5498T BLOCK 3 LOT 8	R-C2	R-CG
1528 TO 1538 34 ST SE	PLAN 5498T BLOCK 3 LOTS 1 TO 6	R-C2	R-CG
35 Street			
1337 AND 1339 35 ST SE	PLAN 5498T BLOCK 6 LOTS 39 AND 40	R-C2	R-CG
1340 AND 1344 35 ST SE	PLAN 5498T BLOCK 5 LOTS 1 AND 2	R-C2	R-CG
1501 TO 1527 35 ST SE	PLAN 4755HA BLOCK 3 LOTS 1 TO 7	R-C2	R-CG
1502 TO 1542 35 ST SE	PLAN 5498 T BLOCK 4 LOTS 1 TO 20	R-C2	R-CG
1531 AND 1539 35 ST SE	PLAN 5498T BLOCK 3 LOTS 36 TO 40	R-C2	R-CG
36 Street			
1340 36 ST SE	PLAN 2700AH BLOCK 24 LOTS 1 AND 2	R-C2	R-CG
1504 TO 1540 36 ST SE	PLAN 2700AH BLOCK 9 LOTS 1 TO 20	R-C2	R-CG
1505 TO 1535 36 ST SE	PLAN 5498T BLOCK 4 LOTS 23 TO 39	R-C2	R-CG
37 Street			
1337 AND 1339 37 ST SE	PLAN 6422HA BLOCK 24 LOT 35	R-C2	R-CG
1340 37 ST SE	PLAN 7039GG BLOCK 23 LOT 1	R-C2	R-CG
1501 TO 1517 37 ST SE	PLAN 5428GG BLOCK 9 LOTS 1 TO 5	R-C2	R-CG

Municipal Address for Redesignation

1521 TO 1541 37 ST SE	PLAN 2700AH BLOCK 9 LOTS 31 TO 39	R-C2	R-CG
1502 37 ST SE	PLAN 328GM BLOCK 10 LOT 1	R-C2	R-CG
1506 AND 1508 37 ST SE	PLAN 1210286 BLOCK 10 LOTS 15 AND 16	R-C2	R-CG
1510 AND 1512 37 ST SE	PLAN 328GM BLOCK 10 LOT 3	R-C2	R-CG
1514 37 ST SE	PLAN 2700AH BLOCK 10 LOTS 13 AND 14	R-C2	R-CG
1520 TO 1540 37 ST SE	PLAN 6906HJ BLOCK 10 LOTS 1 TO 6	R-C2	R-CG
38 Street			
1340 38 ST SE	PLAN 2628HJ BLOCK 22 LOT 19	R-C2	R-CG
1501 TO 1539 38 ST SE	PLAN 4095HI BLOCK 10 LOTS 1 TO 10	R-C2	R-CG
1502 TO 1540 38 ST SE	PLAN 2187HI BLOCK 11 LOTS 11 TO 20	R-C2	R-CG
39 Street			
1337 39 ST SE	PLAN 2628HJ BLOCK 22 LOT 1	R-C2	R-CG
1501 TO 1525 39 ST SE	PLAN 2187HI BLOCK 11 LOTS 4 TO 10	R-C2	R-CG
1529 AND 1531 39 ST SE	PLAN 0511329 BLOCK 11 LOTS 21 AND 22	R-C2	R-CG
1535 39 ST SE	PLAN 8496GW BLOCK 11 LOT 1	R-C2	R-CG
1539 39 ST SE	PLAN 2700AH BLOCK 11 LOTS 39 AND 40	R-C2	R-CG
40 Street			
1339 40 ST SE	PLAN 5111HH BLOCK 21 LOT 6	R-C2	R-CG
1340 40 ST SE	PLAN 5997HE BLOCK 20 LOT 10	R-C2	R-CG
1502 TO 1518 40 ST SE	PLAN 7113GW BLOCK 13 LOTS 1 TO 5	R-C2	R-CG
1520 TO 1536 40 ST SE	PLAN 7282HK BLOCK 13 LOTS 16 TO 18	R-C2	R-CG
41 Street			
1339 41 ST SE	PLAN 5997HE BLOCK 20 LOT 11	R-C2	R-CG
1501 TO 1535 41 ST SE	PLAN 7113GW BLOCK 13 LOTS 7 TO 13	R-C2	R-CG
1504 TO 1540 41 ST SE	PLAN 5252GL BLOCK 14 LOTS 1 TO 10	R-C2	R-CG
42 Street			
1501 TO 1539 42 ST SE	PLAN 5252GL BLOCK 14 LOTS 11 TO 20	R-C2	R-CG
1502 TO 1536 42 ST SE	PLAN 5637HR BLOCK 15 LOTS 2 TO 10	R-C2	R-CG
43 Street			
1504 TO 1522 43 ST SE	PLAN 127LK BLOCK 16 LOTS 2 TO 11	R-C1	R-CG
1507 TO 1539 43 ST SE	PLAN 1273JK BLOCK 15 LOTS 12 TO 19	R-C2	R-CG
44 Street			
1501 TO 1523 44 ST SE	PLAN 127LK BLOCK 16 LOTS 13 TO 24	R-C1	R-CG
45 Street			
1504 TO 1540 45 ST SE	PLAN 2542HT BLOCK 9 LOTS 9 TO 19	R-C2	R-CG
46 Street			
1502 TO 1520 46 ST SE	PLAN 1681GS BLOCK 12 LOTS 30 TO 34	R-C2	R-CG
1501 TO 1537 46 ST SE	PLAN 452GJ BLOCK 9 LOTS 1 TO 9	R-C2	R-CG

Municipal Address for Redesignation

1522 AND 1524 46 ST SE	PLAN9912542 BLOCK 12 LOTS 41 AND 42	R-C2	R-CG
1528 TO 1540 46 ST SE	PLAN 452GJ BLOCK 12 LOTS 37 TO 40	R-C2	R-CG
47 Street			
1505 TO 1519 47 ST SE	PLAN 1681GS BLOCK 12 LOTS 7 TO 10	R-C2	R-CG
1523 TO 1539 47 ST SE	PLAN 452GJ BLOCK 12 LOTS 1 TO 5	R-C2	R-CG
1506 TO 1516 47 ST SE	PLAN 6933GL BLOCK 19 LOTS 25 TO 28	R-C2	R-CG
1518 47 ST SE	PLAN 2740AG BLOCK 19 LOTS 29 AND 30	R-C2	R-CG
1522 TO 1526 47 ST SE	PLAN 6933GL BLOCK 19 LOTS 31 AND 32	R-C2	R-CG
1532 TO 1534 47 ST SE	PLAN 2740AG BLOCK 19 LOTS 37 AND 38	R-C2	R-CG
1540 47 ST SE	PLAN 2740AG BLOCK 19 LOT D	R-C2	R-CG
48 Street			
1503 TO 1531 48 ST SE	PLAN 1256JK BLOCK 19 LOT 3 TO 10	R-C2	R-CG
1535 AND 1537 48 ST SE	PLAN 1213253 BLOCK 19 LOTS 39 AND 40	R-C2	R-CG
1502 TO 1540 48 ST SE	PLAN 4406HT BLOCK 22 LOT 1 TO 10	R-C2	R-CG
49 Street			
1507 TO 1539 49 ST SE	PLAN 6115HS BLOCK 22 LOTS 11 TO 19	R-C2	R-CG
1502 AND 1508 49 ST SE	PLAN 2740AG BLOCK 29 LOTS 21 TO 24	R-C2	R-CG
1512 TO 1540 49 ST SE	PLAN 4406HT BLOCK 29 LOTS 3 TO 10	R-C2	R-CG
50 Street			
1501 TO 1535 50 ST SE	PLAN 727HW BLOCK 29 LOTS 12 TO 20	R-C2	R-CG
1502 TO 1540 50 ST SE	PLAN 4406HT BLOCK 7 LOTS 1 TO 10	R-C2	R-CG
51 Street			
1501 TO 1539 51 ST SE	PLAN 4406HT BLOCK 7 LOTS 11 TO 20	R-C2	R-CG
1502 TO 1512 51 ST SE	PLAN 5519AC BLOCK 4 LOTS 35 TO 40	R-C2	R-CG
1516 TO 1532 51 ST SE	PLAN 3836JK BLOCK 4 LOTS 1 TO 5	R-C2	R-CG
52 Street			
1503 TO 1539 52 ST SE	PLAN 5519AC BLOCK 4 LOTS 1 TO 20	R-C2	R-CG

Municipal Address for Redesignation

Bylaw #2

37 Street			
2001 AND 2007 37 ST SE	PLAN 2335AH BLOCK 13 LOTS 17 TO 20	R-C2	R-CG
2011 TO 2017 37 ST SE	PLAN 6101GW BLOCK 13 LOTS 11 TO 13	R-C2	R-CG
2021 TO 2039 37 ST SE	PLAN 2335AH BLOCK 13 LOTS 1 TO 10	R-C2	R-CG
2004 TO 2040 37 ST SE	PLAN 2335AH BLOCK 14 LOTS 21 TO 40	R-C2	R-CG
2201 37 ST SE	PLAN 3457GT BLOCK 12 LOT 10	R-C2	R-CG
2202 37 ST SE	PLAN 3877HJ BLOCK 11 LOT 11	R-C2	R-CG
38 Street			
2001 38 ST SE	PLAN 2335AH BLOCK 14 LOTS 19 AND 20	R-C2	R-CG
2007 TO 2027 38 ST SE	PLAN 6101GW BLOCK 14 LOTS 7 TO 12	R-C2	R-CG
2029 TO 2039 38 ST SE	PLAN 2335AH BLOCK 14 LOTS 1 TO 6	R-C2	R-CG
2002 AND 2006 38 ST SE	PLAN 1512952 BLOCK 15 LOTS 21 AND 22	R-C2	R-CG
2008 TO 2020 38 ST SE	PLAN 3720IA BLOCK 15 LOTS 15 TO 18	R-C2	R-CG
2024 TO 2040 38 ST SE	PLAN 2376HM BLOCK 15 LOTS 10 TO 14	R-C2	R-CG
2201 AND 2202 38 ST SE	PLAN 3877HJ BLOCK 11 LOTS 10 AND 11	R-C2	R-CG
39 Street			
2001 39 ST SE	PLAN 2335AH BLOCK 15 LOTS 19 AND 20	R-C2	R-CG
2009 TO 2039 39 ST SE	PLAN 2376HM BLOCK 15 LOTS 1 TO 9	R-C2	R-CG
2004 TO 2036 39 ST SE	PLAN 4984JK BLOCK 16 LOTS 11 TO 19	R-C2	R-CG
2202 39 ST SE	PLAN 3877HJ BLOCK 9 LOT 11	R-C2	R-CG
40 Street			
2003 TO 2039 40 ST SE	PLAN 4984JK BLOCK 16 LOTS 1 TO 10	R-C2	R-CG
2002 TO 2018 6 40 ST SE	PLAN 2425AC BLOCK Q LOTS 21 TO 30	R-C2	R-CG
2022 TO 2040 40 ST SE	PLAN 2425AC BLOCK Q LOTS 31 TO 40	R-C2	R-CG
2201 40 ST SE	PLAN 3586HF BLOCK 9 LOT 10	R-C2	R-CG
41 Street			
2001 41 ST SE	PLAN 2425AC BLOCK Q LOTS 19 AND 20	R-C2	R-CG
2007 TO 2031 41 ST SE	PLAN 7460FU BLOCK Q LOTS 5 TO 11	R-C2	R-CG
2025 AND 2027 41 ST SE	PLAN 0715882 BLOCK Q LOTS 41 AND 42	R-C2	R-CG
2033 TO 2039 41 ST SE	PLAN 2425AC BLOCK Q LOTS 1 TO 4	R-C2	R-CG
2006 AND 2008 41 ST SE	PLAN 2425AC BLOCK LOT C	R-C2	R-CG
2010 TO 2016 41 ST SE	PLAN 7460FU BLOCK LOTS 24 AND 25	R-C2	R-CG
2018 TO 2040 41 ST SE	PLAN 2425AC BLOCK N LOTS 29 TO 40	R-C2	R-CG
2202 41 ST SE	PLAN 2425AC BLOCK M LOTS 21 AND 22	R-C2	R-CG
2203 41 ST SE	PLAN 3586HF BLOCK R LOT 10	R-C2	R-CG
42 Street			
2001 42 ST SE	PLAN 2425AC BLOCK N LOT A	R-C2	R-CG
2005 42 ST SE	PLAN 2425AC BLOCK N LOTS 17 AND 18	R-C2	R-CG
2007 TO 2039 42 ST SE	PLAN 2425AC BLOCK N LOTS 1 TO 16	R-C2	R-CG

Municipal Address for Redesignation

2002 TO 2044 42 ST SE	PLAN 2900W BLOCK G LOTS 21 TO 40	R-C2	R-CG
2201 42 ST SE	PLAN 2425AC BLOCK M LOTS 19 AND 20	R-C2	R-CG
2202 42 ST SE	PLAN 2900W BLOCK H LOTS 21 AND 22	R-C2	R-CG
43 Street			
2001 TO 2011 43 ST SE	PLAN 2900W BLOCK G LOTS 15 TO 20	R-C2	R-CG
2013 AND 2015 43 ST SE	PLAN 9112366 BLOCK G LOTS 41 AND 42	R-C2	R-CG
2017 TO 2039 43 ST SE	PLAN 2900W BLOCK G LOTS 1 TO 12	R-C2	R-CG
2002 TO 2036 43 ST SE	PLAN 9027GU BLOCK D LOTS 2 TO 9	R-C2	R-CG
2201 43 ST SE	PLAN 2900W BLOCK H LOTS 19 AND 20	R-C2	R-CG
2202 43 ST SE	PLAN 2900W BLOCK C LOTS 21 AND 22	R-C2	R-CG
44 Street			
2001 44 ST SE	PLAN 2900W BLOCK D LOTS 18 TO 20	R-C1	R-CG
2009 44 ST SE	PLAN 9210940 BLOCK D LOT 11	R-C1	R-CG
2015 TO 2027 44 ST SE	PLAN 4632HK BLOCK D LOTS 7 TO 10	R-C1	R-CG
2031 TO 2039 44 ST SE	PLAN 2900W BLOCK D LOTS 1 TO 6	R-C1	R-CG
2002 44 ST SE	PLAN 5882AB BLOCK 2 LOT A	R-C1	R-CG
2010 AND 2016 44 ST SE	PLAN 5882AB BLOCK 2 LOTS 5 TO 8	R-C1	R-CG
2020 TO 2032 44 ST SE	PLAN 4632HK BLOCK 2 LOTS 9 TO 12	R-C1	R-CG
2036 AND 2040 44 ST SE	PLAN 4632HK BLOCK 2 LOTS 13 AND 14	R-C2	R-CG
2201 44 ST SE	PLAN 2900W BLOCK C LOTS 19 AND 20	R-C1	R-CG
2202 44 ST SE	PLAN 1529HP BLOCK 3 LOT 1	R-C1	R-CG
45 Street			
2002 TO 2008 45 ST SE	PLAN 5882AB BLOCK 9 LOTS 1 TO 4	R-C1	R-CG
2024 45 ST SE	PLAN 5882AB BLOCK 9 LOTS 11 AND 12	R-C1s	R-CG
2028 TO 2040 45 ST SE	PLAN 5882AB BLOCK 9 LOTS 13 TO 20	R-C1	R-CG
2001 TO 2039 45 ST SE	PLAN 7562HK BLOCK 2 LOTS 1 TO 10	R-C1	R-CG
2201 45 ST SE	PLAN 5882AB BLOCK 3 LOTS 39 AND 40	R-C1	R-CG
2202 45 ST SE	PLAN 6587HM BLOCK 8 LOT 1	R-C1s	R-CG
46 Street			
2001 46 ST SE	PLAN5882AB BLOCK 9 LOT A	R-C1	R-CG
2007 46 ST SE	PLAN5882AB BLOCK 9 LOT A	R-C1s	R-CG
2011 46 ST SE	PLAN5882AB BLOCK 9 LOT A	R-C1	R-CG
2015 TO 2039 46 ST SE	PLAN6302HO BLOCK 9 LOTS 1 TO 7	R-C2	R-CG
2002 AND 2008 46 ST SE	PLAN8271HN BLOCK 12 LOTS 1 AND 2	R-C1	R-CG
2012 TO 2032 46 ST SE	PLAN8271HN BLOCK 12 LOTS 3 TO 8	R-C2	R-CG
2036 AND 2040 46 ST SE	PLAN8271HN BLOCK 12 LOTS 9 AND 10	R-C1	R-CG
2201 46 ST SE	PLAN6587HM BLOCK 13 LOT 1	R-C1	R-CG
2202 46 ST SE	PLAN6587HM BLOCK 8 LOT 20	R-C1	R-CG
47 Street			
2002 TO 2040 47 ST SE	PLAN 8779HP BLOCK 19 LOTS 1 TO 10	R-C1	R-CG
2007 47 ST SE	PLAN 5882AB BLOCK 12	R-C1	R-CG

Municipal Address for Redesignation

2011 TO 2039 47 ST SE	PLAN 8271HN BLOCK 12 LOTS 11 TO 18	R-C1	R-CG
2201 47 ST SE	PLAN 2009HP BLOCK 13 LOT 20	R-C1	R-CG
2202 47 ST SE	PLAN 8779HP BLOCK 18 LOT 1	R-C1	R-CG
48 Street			
2011 48 ST SE	PLAN 5998JK BLOCK 19 LOTS 16 AND 17	DC	M-C1
2015 48 ST SE	PLAN 5998JK BLOCK 19 LOT 15	DC	M-C1
2035 48 ST SE	PLAN 5882AB BLOCK 19 LOT C	DC	M-C1
19 Avenue			
4727 AND 4729 19 AV SE	PLAN 8010997 BLOCK 12 LOTS A AND B	R-C1	R-CG
4201 AND 4203 19 AV SE	PLAN 2425AC BLOCK N LOTS 21 AND 22	R-C2	R-CG
21 Avenue			
3955 21 AV SE	PLAN 3877HJ BLOCK 10 LOT 10	R-C2	R-CG
4404 21 AV SE	PLAN 9027GU BLOCK D LOT 1	R-C2	R-CG
4101 21 AV SE	PLAN 3586HF BLOCK R LOT 11	R-C2	R-CG
4004 21 AV SE	PLAN 4984JK BLOCK 16 LOT 20	R-C2	R-CG

Municipal Address for Redesignation

Bylaw #3

Ash			
1901 TO 1941 ASH CR SE	PLAN 4346GL BLOCK 11 LOTS 1 TO 11	R-C1	M-CG
1902 TO 1914 ASH CR SE	PLAN 4346GL BLOCK 12 LOTS 1 TO 4	R-C1	M-CG
26 Street			
1605 26 ST SE	PLAN 4946T BLOCK 20 LOTS 24 AND 25	R-C2	R-CG
30 Street			
1903 TO 1913 30 ST SE	PLAN 4346GL BLOCK 12 LOTS 5 TO 8	R-C1	M-CG
15 Avenue			
2613 TO 2645 15 AV SE	PLAN 4946T BLOCK 20 LOTS 5 TO 23	R-C2	R-CG
16 Avenue			
2604 TO 2624 16 AV SE	PLAN 4946T BLOCK 20 LOTS 35 TO 45	R-C2	M-C1
2632 AND 2634 16 AV SE	PLAN 9010332 BLOCK 20 LOTS 48 AND 49	R-C2	M-C1
2638 TO 2644 16 AV SE	PLAN 4946T BLOCK 20 LOTS 26 TO LOT 31	R-C2	M-C1
2701 TO 2739 16 AV SE	PLAN 4946T BLOCK 22 LOTS 1 TO 20	R-C2	M-C1
2702 TO 2738 16 AV SE	PLAN 4946T BLOCK 19 LOTS 21 TO 40	R-C2	R-CG
2802 TO 2842 16 AV SE	PLAN 4946T BLOCK 18 LOTS 21 TO 40	R-C2	R-CG
2805 TO 2817 16 AV SE	PLAN 4946T BLOCK 23 LOTS 3 TO 9	R-C2	M-C1
2819 AND 2821 16 AV SE	PLAN 0214300 BLOCK 23 LOTS 41 AND 42	R-C2	M-C1
2823 16 AV SE	PLAN 218JK BLOCK 23 LOTS A AND B	R-C2	M-C1
2825 AND 2827 16 AV SE	PLAN 0913951 BLOCK 23 LOTS 43 AND 44	R-C2	M-C1
2831 TO 2837 16 AV SE	PLAN 4946T BLOCK 23 LOTS 16 TO 20	R-C2	M-C1
17 Avenue			
2710 17 AV SE	PLAN 4946T BLOCK 22 LOTS 33 TO 40	C-COR2 f3.0h27	M-U2 f3.0h27
2718 AND 2718A 17 AV SE	PLAN 4946T BLOCK 22 LOTS 31 AND 32	C-COR2 f3.0h23	M-U2 f3.0h23
2720 17 AV SE	PLAN 7014FW PARCEL B PLAN 22	C-COR2 f3.0h23	M-U2 f3.0h23
2728 AND 2740 17 AV SE	PLAN 4946T BLOCK 22 LOT A	C-COR2 f3.0h23	M-U2 f3.0h23
2805 17 AV SE	MERIDIAN 4 RANGE 29 TOWNSHIP 24 SECTION 9	C-COR2 f1.0h10	M-U2 f2.0h10
2806 TO 2830 17 AV SE	PLAN 4946T BLOCK 23 LOTS 23 TO 40	DC	M-U2 f4.5h20
2825 17 AV SE	PLAN 732GF BLOCK 10	C-COR2 f1.0h10	M-U2 f2.0h10
2825R 17 AV SE	PLAN 4346GL BLOCK 11	C-COR2 f1.0h10	M-U2 f2.0h10

Municipal Address for Redesignation

2875 17 AV SE	MERIDIAN 4 RANGE 29 TOWNSHIP 24 SECTION 9	C-COR2 f1.0h10	M-U2 f2.0h10
2919 17 AV SE	MERIDIAN 4 RANGE 29 TOWNSHIP 24 SECTION 9	C-COR2 f1.0h10	M-U2 f2.0h10
19 Avenue			
2730 19 AV SE	PLAN 7811669 BLOCK 1 LOT 2	S-CI	M-H1
2736 19 AV SE	PLAN 7811669 BLOCK 1 LOT 1	S-CI	M-H1
3720 19 AV SE	PLAN 1210859 BLOCK 20 LOT 17	M-CG d72	M-C1

Municipal Address for Redesignation

Bylaw #4

Radisson Dr SE			
1702 AND 1710 RADISSON DR SE	PLAN 8311202 LOTS 1 AND 2	C-COR2 f3.0h23	M-U1 f5.0h37
1720 RADISSON DR SE	PLAN 0212474 BLOCK B LOT 4	C-COR2 f3.0h23	M-U2 f5.0h26
33 Street			
1814 33 ST SE	PLAN1079HU BLOCK 3 LOT 1	C-COR1 f3.0h46	M-U2 f3.0h46
34 Street			
1812 34 ST SE	PLAN 5682GI BLOCK 2 LOT 17	M-CG d111	M-C1
1816 34 ST SE	PLAN 5682GI BLOCK 2 LOT 16	M-C1	M-C1
1820 TO 1834 34 ST SE	PLAN 5682GI BLOCK 2 LOTS 14 AND 15	R-C2	M-C1
1702 TO 1710 34 ST SE	PLAN 5498T BLOCK 2 LOTS 26 TO 30	M-C1	M-C2
1703 TO 1723 34 ST SE	PLAN 3400AM BLOCK 35 LOTS 1 TO 12	M-C1	M-C2
1712 AND 1714 34 ST SE	CONDOMINIUM PLAN 1512518 UNITS 1 TO 4	M-C1	M-C2
1718 34 ST SE	PLAN 5498T BLOCK 2 LOTS 33 AND 34	DC	M-C2
1720 34 ST SE	PLAN 5498T BLOCK 2 LOT 37	M-C1	M-C2
1728 34 ST SE	CONDOMINIUM PLAN 0113277 UNITS 1 TO 6	M-C1	M-C2
35 Street			
1701 35 ST SE	DESCRIPTIVE PLAN 0212920 BLOCK 2 LOT 40	M-C1	MC-2
1702 35 ST SE	CONDOMINIUM PLAN 9412670 UNITS 1 TO 4	M-C1	MC-2
1704 AND 1712 35 ST SE	PLAN 3551GR BLOCK 1 LOTS 2 TO 6	M-C1	MC-2
1723 35 ST SE	PLAN46T47HS BLOCK 2 LOT 9	M-C1	MC-2
1804 35 ST SE	PLAN 5660GG BLOCK 1 LOTS 1, 5 AND 10	C-COR2 f1.0h12	MU-1 f 2.0h12
1813 35 ST SE	PLAN 5682GI BLOCK 2 LOT 7	R-C2	M-C1
1818 35 ST SE	PLAN 5660GG BLOCK 1 LOT A	C-COR2 f1.0h12	MU-1 f 2.0h12
36 Street			
1701 36 ST SE	PLAN 3551GR LOT 14	M-C1	MU-2 f4.5h20
1705 AND 1711 36 ST SE	PLAN 3551GR LOTS 12 AND 13	M-C1	MU-2 f4.5h20

Municipal Address for Redesignation

1713 TO 1721 36 ST SE	PLAN 3551GR LOTS 9 TO 11	C-COR2 f1.0h10	MU-2 f4.5h20
1803 36 ST SE	PLAN 5660GG BLOCK 1 LOT 6	DC	M-U2 f4.5h23
1815 36 ST SE	PLAN 5660GG BLOCK 1 LOTS 7 TO 9	C-COR2 f2.0h23	M-U2 f4.5h23
1827 36 ST SE	PLAN 5660GG BLOCK 1 LOT B	C-COR2 f2.0h23	M-U2 f4.5h23
1814 36 ST SE	PLAN 2335AH BLOCK 20 LOT C	C-COR2 f1.0h10	M-U2f4.5h20
1826 TO 1840 36 ST SE	PLAN 2146GJ BLOCK 20 LOTS 1 TO 5	C-COR2 f1.0h10	M-U2f4.5h20
37 Street			
1813 37 ST SE	PLAN 2335AH BLOCK 20 LOTS 12 TO 14	C-COR2 f1.0h10	M-U2 f4.5h20
1823 37 ST SE	PLAN 1210859 BLOCK 20 LOT 16	M-CG d72	MU-1f3.0h16
1836 AND 1840 37 ST SE	PLAN 2335AH BLOCK 19 LOTS 36 TO 39	M-CG d72	M-C1
38 Street			
1728 38 ST SE	PLAN 2700AH BLOCK 6 LOTS 11 AND 12	DC	M-C1
1812 TO 1832 38 ST SE	PLAN 1505HB BLOCK 18 LOTS 15 TO 19	R-C2	M-C1
1813 TO 1831 38 ST SE	PLAN 345JK BLOCK 19 LOTS 5 TO 9	R-C2	M-C1
1835 AND 1839 38 ST SE	PLAN 2335AH BLOCK 19 LOTS 1 TO 4	R-C2	M-C1
1840 38 ST SE	PLAN 1505HB BLOCK 18 LOT 14	R-C2	M-C1
17 Avenue			
3411 AND 3419 17 AV SE	PLAN 1079HU BLOCK 3 LOTS 2 AND 3	C-COR1 f3.0h46	M-U2 f3.0h46
3420 17 AV SE	PLAN 0212474 BLOCK B LOT 3	C-COR2 f3.0h23	M-U2 f5.0h26
3460 17 AV SE	PLAN 4065JK BLOCK 35 LOT A	C-COR2 f1.0h10	M-U2 f5.0h26
3501 17 AV SE	PLAN 5682GI BLOCK 2 LOT 1	C-COR1 f3.0h46	M-U1 f3.0h46
3502 AND 3504 17 AV SE	PLAN 7661JK BLOCK 2 LOTS 1 AND 2	C-COR2 f3.0h23	M-U2 f4.5h23
3505 TO 3517 17 AV SE	PLAN 5682GI BLOCK 2 LOTS 2 TO 5	C-COR1 f3.0h46	M-U1 f3.0h46
3520 17 AV SE	PLAN 4647HS BLOCK 2 LOT 6	C-COR2 f3.0h23	M-U2 f4.5h23
3601 TO 3619 17 AV SE	PLAN 5660GG BLOCK 1 LOTS 1 TO 4	C-COR1 f3.0h46	M-U2 f3.0h46

Municipal Address for Redesignation

3620 17 AV SE	PLAN 0212141 BLOCK 1 LOT 41 E	C-COR2 f3.0h23	M-U2 f5.0h26
3703 TO 3709 17 AV SE	PLAN 6263FT LOTS 1 TO 4	C-COR2 f2.0h12	M-U2 f5.0h26
3710 17 AV SE	PLAN 2700AH BLOCK 8 LOT A	C-COR2 f2.0h12	M-U2 f4.5h20
3711 AND 3713 17 AV SE	PLAN 6263FT LOTS 5 AND 6	C-COR2 f2.0h12	M-U2 f5.0h26
3717 17 AV SE	PLAN 3929FR PARCEL B	C-COR2 f2.0h12	M-U2 f5.0h26
3719 17 AV SE	PLAN 2335AH BLOCK 20 LOT A	C-COR2 f2.0h12	M-U2 f5.0h26
3712 17 AV SE	PLAN 2700AH BLOCK 8 LOT A	C-COR2 f2.0h12	M-U2 f4.5h20
3720 17 AV SE	PLAN 2700AH BLOCK 8 LOTS 1 AND 2	C-COR2 f2.0h12	M-U2 f4.5h20

Municipal Address for Redesignation

Bylaw #5

39 Street			
1740 39 ST SE	PLAN 8132FS BLOCK 5 LOT 1	C-COR2 f2.0h12	MU-2 f4.5h20
1811 TO 1813 39 ST SE	PLAN 1611977 BLOCK 18 LOTS 20 TO 21	R-C2	M-C1
1817 TO 1839 39 ST SE	PLAN 1505HB BLOCK 18 LOTS 8 TO 13	R-C2	M-C1
1822 TO 1840 39 ST SE	PLAN 6429GT BLOCK 17 LOTS 11 TO 15	R-C2	M-C1
40 Street			
1808 TO 1812 40 ST SE	PLAN 2425AC BLOCK P LOTS 23 TO 26	DC	M-U2 f4.5h20
1816 TO 1832 40 ST SE	PLAN 2425AC BLOCK P LOTS 27 TO 34	M-CG d72	M-C1
1832 40 ST SE	PLAN 2425AC BLOCK P LOTS 35 AND 36	R-C2	M-C1
1836 40 ST SE	PLAN 2425AC BLOCK P LOTS 37 AND 38	M-CG d72	M-C1
1840 40 ST SE	PLAN 2425AC BLOCK P LOTS 39 AND 40	R-C2	M-C1
1821 TO 1839 40 ST SE	PLAN 111GR BLOCK 17 LOTS 6 TO 10	R-C2	M-C1
41 Street			
1809 41 ST SE	PLAN 2425AC BLOCK P LOTS 15 AND 16	C-COR2 f2.0h12	MU-2 f4.5h20
1813 41 ST SE	PLAN 2425AC BLOCK P LOTS 13 AND 14	R-C2	M-C1
1819 41 ST SE	PLAN 2425AC BLOCK P LOTS 11 AND 12	M-CG d72	M-C1
1821 AND 1825 41 ST SE	PLAN 2425AC BLOCK P LOTS 7 TO 10	R-C2	M-C1
1833 AND 1835 41 ST SE	PLAN 2425AC BLOCK P LOTS 1 TO 6	M-C1 d100	M-C1
1812 41 ST SE	PLAN 2425AC BLOCK O LOTS 25 AND 26	DC	MU-2 f4.5h20
1818 41 ST SE	PLAN 2425AC BLOCK O LOTS 27 TO 29	M-CG d72	M-C1
1820 TO 1836 41 ST SE	PLAN 2425AC BLOCK O LOTS 30 TO 38	R-C2	M-C1
1840 41 ST SE	PLAN 2425AC BLOCK O LOTS 39 AND 40	M-C1 d55	M-C1

Municipal Address for Redesignation

42 Street			
1726 42 ST SE	PLAN 5252GL BLOCK 2 LOT 6	DC	M-C1
1807 42 ST SE	PLAN 2425AC BLOCK O LOTS 17 TO 20	C-COR2 f2.0h12	M-U2 f4.5h20
1811 42 ST SE	PLAN 2425AC BLOCK O LOTS 15 AND 16	DC	M-U2 f4.5h20
1810 42 ST SE	PLAN 1883GR BLOCK F LOT 2	DC	M-U2 f4.5h20
1814 42 ST SE	PLAN 1883GR BLOCK F LOT 3	M-CG	M-C1
1816 AND 1818 42 ST SE	PLAN 1883GR BLOCK F LOTS 4 AND 5	R-C2	M-C1
1828 42 ST SE	PLAN 1883GR BLOCK F LOT 6	M-CG d72	M-C1
1832 TO 1840 42 ST SE	PLAN 1883GR BLOCK F LOTS 7 AND 8	R-C2	M-C1
1815 AND 1819 42 ST SE	PLAN 2425AC BLOCK O LOTS 11 TO 14	M-CG d72	M-C1
1823 TO 1839 42 ST SE	PLAN 2425AC BLOCK O LOTS 1 TO 10	R-C2	M-C1
43 Street			
1726 43 ST SE	PLAN 3969HJ BLOCK 1 LOT 7	DC	M-C1
1812 TO 1836 43 ST SE	PLAN 1348GJ BLOCK E LOTS 1 TO 5	R-C2	M-C1
1840 43 ST SE	PLAN 2900W BLOCK E LOTS 39 AND 40	R-C2	M-C1
1817 43 ST SE	PLAN 1883GR BLOCK F LOT 14	R-C2	M-C1
1821 AND 1823 43 ST SE	PLAN 1883GR BLOCK F LOT 13	M-CG d72	M-C1
1827 TO 1839 43 ST SE	PLAN 1883GR BLOCK F LOTS 9 TO 12	DC	M-C1
44 Street			
1701 TO 1723 44 ST SE	PLAN 748HV BLOCK 1 LOTS 10 TO 15	R-C2	M-C1
1727 44 ST SE	PLAN 748HV BLOCK 1 LOT 9	DC	M-C1
1702 TO 1714 44 ST SE	PLAN 5472HP BLOCK 1 LOTS 9 TO 12	R-C2	M-C1
1718 TO 1726 44 ST SE	PLAN 5472 HP BLOCK 1 LOT A	R-C2	M-C1
1817 44 ST SE	PLAN 4467HR BLOCK E LOT 11	M-CG d72	M-C1
1823 TO 1835 44 ST SE	PLAN 4467HR BLOCK E LOTS 12 TO 15	R-C1	M-C1
1839 44 ST SE	PLAN 2900W BLOCK E LOTS 1 AND 2	R-C1	M-C1
45 Street			
1701 TO 1725 45 ST SE	PLAN 5472HP BLOCK 1 LOTS 2 TO 8	R-C2	M-C1

Municipal Address for Redesignation

17 Avenue			
3800 17 AV SE	PLAN 2700AH THE SOUTH 130 FEET OF 37 STREET SE	C-COR2 f2.0h12	M-U1 f4.5h20
3801 17 AV SE	PLAN 2335AH BLOCK 19 LOTS 15 TO 25	C-COR2 f2.0h12	M-U1 f4.5h20
3810 17 AV SE	PLAN 2700AH BLOCK 7 LOTS 5 TO 10	C-COR2 f2.0h12	M-U1 f4.5h20
3820 17 AV SE	PLAN 2700AH BLOCK 7 LOTS 1 TO 4	C-COR2 f2.0h12	M-U1 f4.5h20
3904 17 AV SE	PLAN 2700AH BLOCK 6 LOTS 8 TO 10	C-COR2 f2.0h12	M-U2 f4.5h20
3908 17 AV SE	PLAN 2700AH BLOCK 6 LOTS 3 TO 7	C-COR2 f2.0h12	M-U2 f4.5h20
3909 17 AV SE	PLAN 8552 FS LOTS 1 TO 4	C-COR2 f2.0h12	M-U2 f4.5h20
3912 17 AV SE	PLAN 2700AH BLOCK 6 LOTS 1 AND 2	C-COR2 f2.0h12	M-U2 f4.5h20
3913 AND 3917 17 AV SE	PLAN CALGARY 8552FS BLOCK 18 LOTS 4 TO 6	C-COR2 f2.0h12	M-U2 f4.5h20
4002 17 AV SE	PLAN 8132FS BLOCK 5 LOT 2	C-COR2 f2.0h12	M-U2 f4.5h20
4010 17 AV SE	PLAN 8132FS BLOCK 5 LOTS 3 AND 4	C-COR2 f2.0h12	M-U2 f4.5h20
4015 AND 4019 17 AV SE	PLAN 140LK BLOCK 17 LOTS 1 AND 4	C-COR2 f2.0h12	M-U2 f4.5h20
4020 17 AV SE	PLAN 8132FS BLOCK 5 LOTS 5 AND 6	C-COR2 f2.0h12	M-U2 f4.5h20
4117 17 AV SE	PLAN 7510805 BLOCK A	C-COR2 f2.0h12	M-U2 f4.5h20
4204 TO 4210 17 AV SE	PLAN 5252GL BLOCK 3 LOTS 5 TO 8	C-COR1 f2.0h12	M-U2 f4.5h20
4205 17 AV SE	PLAN 2425AC BLOCK O LOTS 21 TO 24	C-COR2 f2.0h12	M-U2 f4.5h20
4214 17 AV SE	PLAN 5252GL BLOCK 3 LOTS 2, 3 AND 4	C-COR1 f3.5h22	M-U1 f3.5h22
4220 17 AV SE	PLAN 5252GL BLOCK 3 LOT 1	C-COR1 f3.5h22	M-U1 f3.5h22
4301 17 AV SE	PLAN 2900W BLOCK F LOTS 22 TO 24	C-COR2 f2.0h12	M-U2 f4.5h20
4303 17 AV SE	PLAN 1883GR BLOCK F LOT 1	C-COR2 f2.0h12	M-U2 f4.5h20
4315 17 AV SE	PLAN 9111393 BLOCK F LOT 25	C-COR2 f2.0h12	M-U2 f4.5h20
4302 TO 4314 17 AV SE	PLAN 5252GL BLOCK 2 LOTS 1 TO 5	C-COR1 f2.0h12	M-U2 f4.5h20
4401 TO 4417 17 AV SE	PLAN 1348GJ BLOCK E LOTS 6 TO 10	C-COR2 f2.0h12	M-U1 f4.5h20
4402 17 AV SE	PLAN 748HV BLOCK 1 LOT 8	DC	M-U1 f4.5h20

Municipal Address for Redesignation

4420 17 AV SE	PLAN 748HV BLOCK 1 LOT 8	DC,C-COR2 f2.0h12	M-U1 f4.5h20
4504 TO 4518 17 AV SE	PLAN 123JK BLOCK 1 LOTS 13 TO 16	C-COR2 f2.0h12	M-U1 f4.5h20

Municipal Address for Redesignation

Bylaw #6

45 Street			
1702 TO 1726 45 ST SE	PLAN 5472HP BLOCK 2 LOTS 2 TO 8	R-C2	M-U1 f4.5h20
1816 45 ST SE	PLAN 1687HP BLOCK 10 LOT 2	DC	M-U1 f3.0h16
1820 TO 1840 45 ST SE	PLAN 1687HP BLOCK 10 LOTS 3 TO 8	R-C1	M-U1 f3.0h16
46 Street			
1821 TO 1839 46 ST SE	PLAN7410810 BLOCK 10 LOTS 15 TO 19	R-C2	M-C1
1822 TO 1840 46 ST SE	PLAN7410810 BLOCK 10 LOTS 10 TO 14	R-C2	M-C1
47 Street			
1700 47 ST SE	CONDOMINIUM PLAN 7610849 UNITS 1 TO 5	M-CG d75	M-C1
1704 47 ST SE	CONDOMINIUM PLAN 7611112 UNITS 1 TO 4	M-CG d75	M-C1
1708 47 ST SE	PLAN 7410131 BLOCK 20 LOT 3	M-CG d75	M-C1
1817 TO 1835 47 ST SE	PLAN 7410810 BLOCK 10 LOTS 5 TO 9	R-C2	M-C1
1818 TO 1840 47 ST SE	PLAN 3265HV BLOCK 20 LOTS 5 TO 11	R-C1	M-C1
48 Street			
1701 48 ST SE	CONDOMINIUM PLAN 7610116 UNITS 1 TO 4	M-CG d75	M-C1
1705 48 ST SE	CONDOMINIUM PLAN 7610115 UNITS 1 TO 4	M-CG d75	M-C1
1706 48 ST SE	PLAN 2740AG BLOCK 21 LOTS 23 AND 24	R-C2	M-C1
1709 48 ST SE	CONDOMINIUM PLAN 7610114 UNITS 1 TO 4	M-CG d75	M-C1
1712 48 ST SE	PLAN 2740AG BLOCK 21 LOTS 25 AND 26	R-C2	M-C1
1713 48 ST SE	CONDOMINIUM PLAN 7610113 UNITS 1 TO 4	M-CG d75	M-C1
1714 AND 1716 48 ST SE	PLAN 2740AG BLOCK 21 LOTS 27 AND 28	R-C2	M-C1
1718 48 ST SE	PLAN 2740AG BLOCK 21 LOTS 29 AND 30	M-CG d68	M-C1
1817 AND 1847 48 ST SE	PLAN 3265HV BLOCK 20 LOTS 12 AND 13	C-COR2 f2.0h12	MU-1 f3.0h16

Municipal Address for Redesignation

49 Street			
1702 TO 1728 49 ST SE	PLAN 4406HT BLOCK 30 LOTS 1 TO 12	R-C2	M-C1
1707 To 1719 49 ST SE	PLAN 270JK BLOCK 21 LOTS 2 TO 5	M-CG d72	M-C1
50 Street AW			
1701 TO 1719 50 ST SE	PLAN 4406HT BLOCK 30 LOTS 6 TO 10	R-C2	M-C1
1702 AND 1732 50 ST SE	PLAN 5519AC BLOCK 6 LOTS 26 TO 40	R-C2	M-C1
1734 AND 1740 50 ST SE	PLAN 5519AC BLOCK 6 LOTS 22 TO 25	C-COR2 f2.0 h12	M-U2 f3.0h16
51 Street			
1703 51 ST SE	PLAN 5519AC BLOCK 6 LOTS 1 TO 3	R-C2	M-C1
1707 AND 1709 51 ST SE	PLAN 9611221 BLOCK 6 LOTS 41 AND 42	R-C2	M-C1
1713 51 ST SE	PLAN 5519AC BLOCK 6 LOTS 6 AND 7	R-C2	M-C1
1715 AND 1717 51 ST SE	PLAN 9912631 BLOCK 6 LOTS 43 AND 43	R-C2	M-C1
1719 51 ST SE	PLAN 5519AC BLOCK 6 LOTS 10 AND 11	R-C2	M-C1
1725 51 ST SE	PLAN 5519AC BLOCK 6 LOTS 12 AND 13	R-C2	M-C1
1733 51 ST SE	PLAN 5519AC BLOCK 6 LOTS 14 TO 17	C-COR2 f2.0h12	M-U2 f3.0h16
1739 51 ST SE	PLAN 5519 A.C. BLOCK 6 LOTS 5, 18 AND 19	C-COR2 f2.0h12	M-U2 f3.0h16
1704 51 ST SE	PLAN 5519AC BLOCK 5 LOTS 37 AND 38	M-CG d68	M-C1
1710 51 ST SE	PLAN 5519AC BLOCK 5 LOTS 34 TO 36	R-C2	M-C1
1718 AND 1720 51 ST SE	PLAN 0213701 BLOCK 5 LOTS 43 AND 44	R-C2	M-C1
1724 51 ST SE	PLAN 5519AC BLOCK 5 LOTS 29 TO 32	R-C2	M-C1
1728 51 ST SE	PLAN 5519AC BLOCK 5 LOTS 27 TO 29	R-C2	M-C1
52 Street			
1701 TO 1721 52 ST SE	PLAN 5519AC BLOCK 5 LOTS 1 TO 12	C-N2	M-U1 f4.5h20

Municipal Address for Redesignation

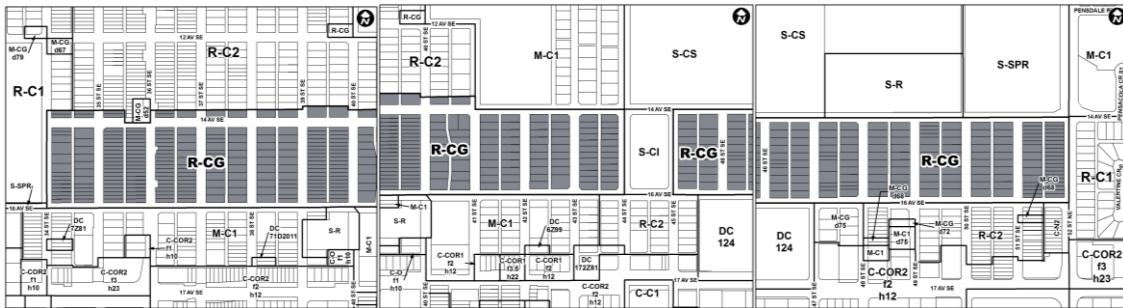
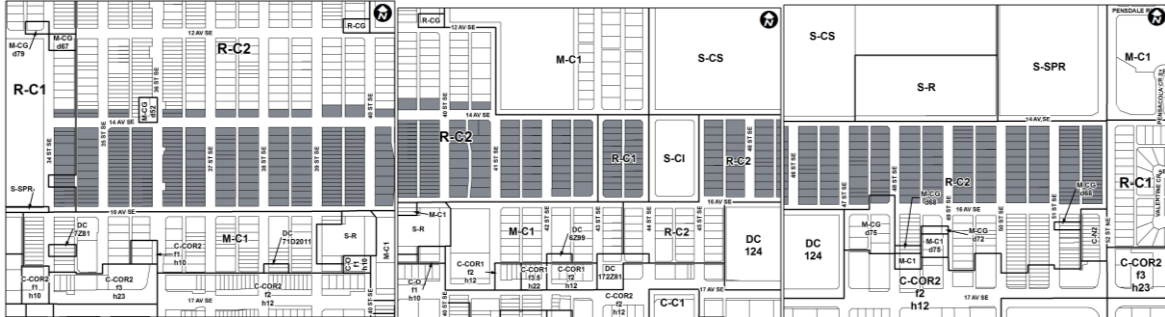
16 Avenue			
4907 16 AV SE	PLAN 2740AG BLOCK 21 LOTS 21 AND 22	R-C2	M-C1
4913 16 AV SE	PLAN 270JK BLOCK 21 LOT 1	R-C2	M-C1
5203 16 AV SE	PLAN 5519AC BLOCK 5 LOTS 39 AND 40	R-C2	M-C1
17 Avenue			
4601 17 AV SE	PLAN 1687HP BLOCK 10 LOT 1	C-COR2 f2.0h12	M-U1 f3.0h16
4610 17 AV SE	PLAN 0310111 BLOCK 2 LOT 9	C-COR2 f2.0h12	M-U1 f4.5h20
4615 17 AV SE	PLAN 616LK BLOCK 10 LOTS 1 AND 2	C-COR2 f2.0h12	M-U1 f3.0h16
4707 17 AV SE	PLAN 616 LK BLOCK 10 LOTS 2 AND 3	C-COR2 f2.0h12	M-U1 f3.0h16
4802 17 AV SE	PLAN 5754JK BLOCK 20 LOT C	C-COR2 f2.0h12	M-U2 f3.0h16
4805 17 AV SE	PLAN 3265HV BLOCK 20 LOTS 4 AND 5	C-COR2 f2.0h12	M-U2 f3.0h16
4818 17 AV SE	PLAN 1262JK BLOCK 20 LOT A	C-COR2 f2.0h12	M-U2 f3.0h16
4821 17 AV SE	PLAN 3265HV BLOCK 20 LOTS 1 TO 3	C-COR2 f2.0h12	M-U2 f3.0h16
4908 17 AV SE	CONDOMINIUM PLAN 9911551 UNITS 1 TO 14	C-COR2 f2.0h12	M-U2 f3.0h16
4920 17 AV SE	PLAN 560LK BLOCK 21 LOTS 6 AND 7	C-COR2 f2.0h12	M-U2 f3.0h16
5016 17 AV SE	PLAN 4406HT BLOCK 30 LOT 11	C-COR2 f2.0h12	M-U2 f3.0h16
5020 17 AV SE	PLAN 1856AD BLOCK 30	C-COR2 f2.0h12	M-U2 f3.0h16
5260 17 AV SE	PLAN 8211086 BLOCK 5 LOT 42	C-COR2 f2.0h12	M-U2 f3.0h16

Proposed Land Use Maps

Bylaw 1

From: Residential-One Dwelling (R-C1) District and Residential – Contextual One / Two Dwelling (R-C2) District

To: Residential – Grade-Oriented Infill (R-CG) District

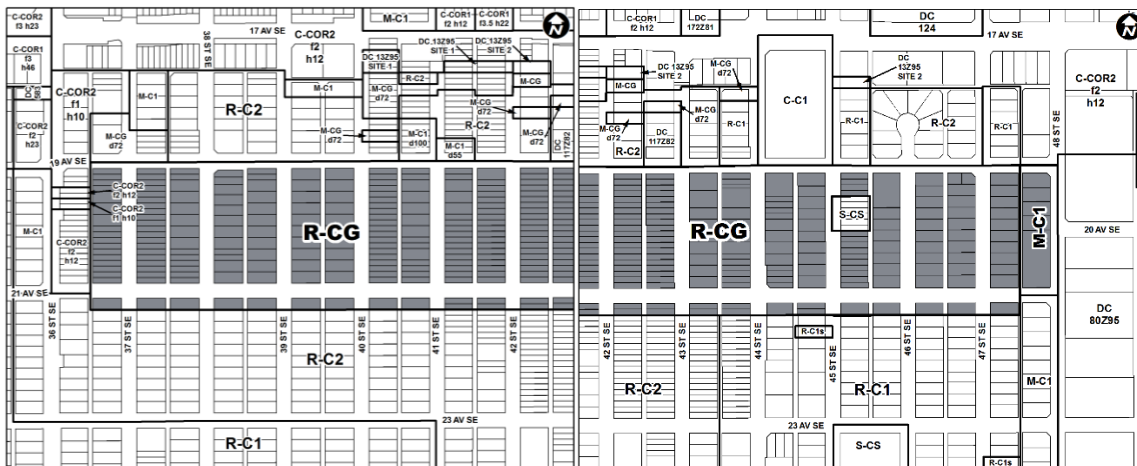
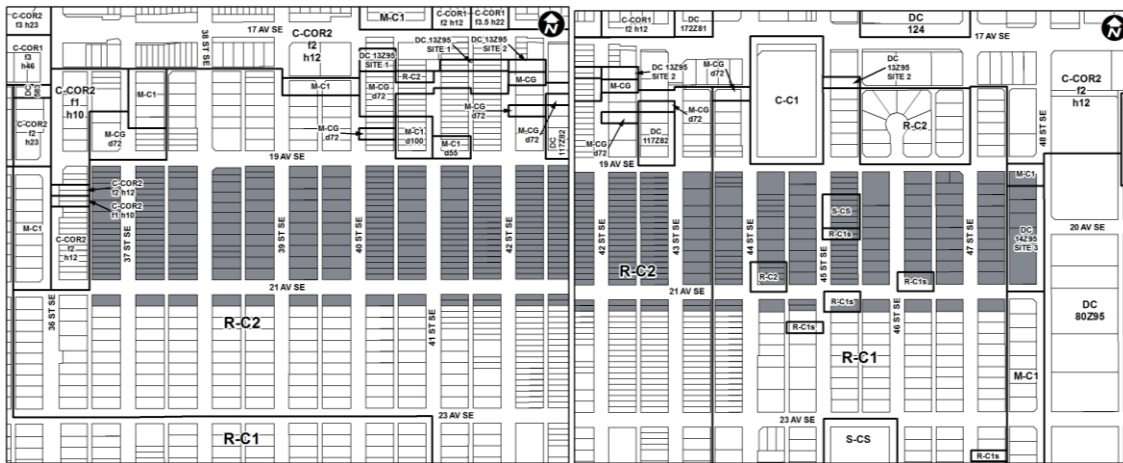


Proposed Land Use Maps

Bylaw 2

From: Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District and DC Direct Control District

To: Residential – Grade-Oriented Infill (R-CG) District and Multi-Residential – Contextual Low Profile (M-C1) District

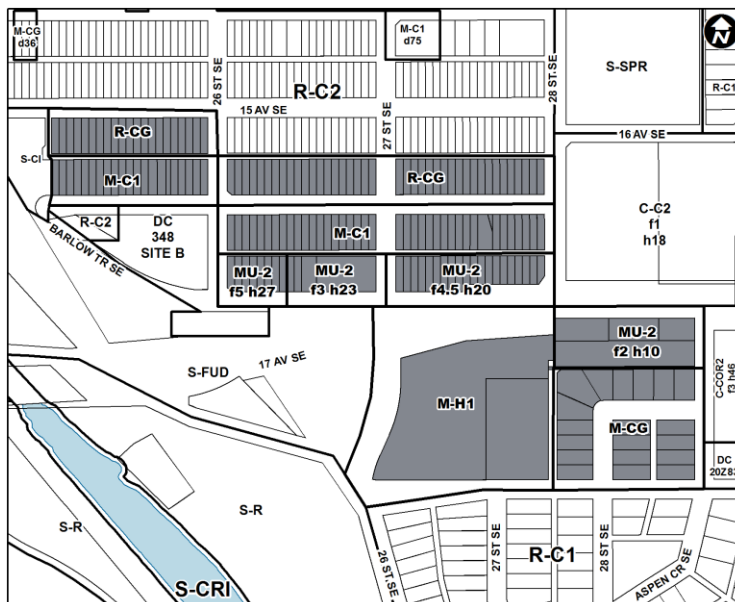
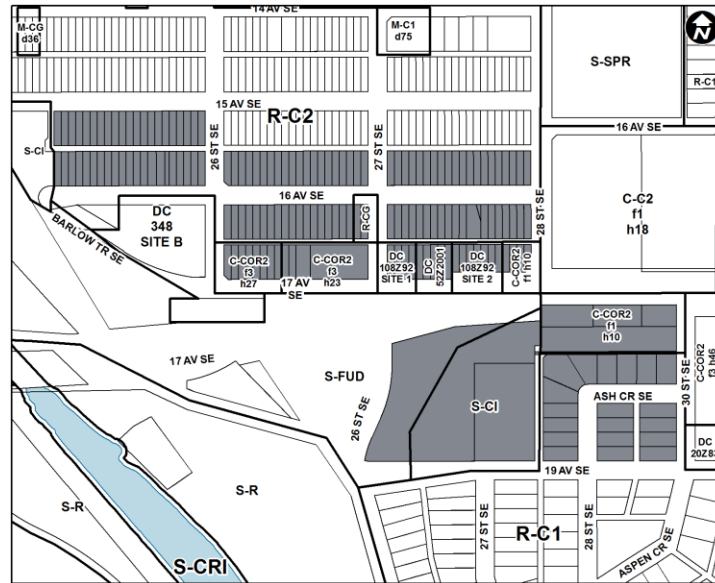


Proposed Land Use Maps

Bylaw 3

From: Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District, DC Direct Control District, Multi-Residential – Contextual-Oriented (M-CG) District, Commercial – Corridor 2 (C-COR2) District and Special Purpose – Community Institution District (S-C1) District

To: Residential – Grade-Oriented Infill (R-CG) District, Multi-Residential – Contextual-Oriented (M-CG) District, Multi-Residential – Contextual Low Profile (M-C1) District, Multi-Residential – High Density Low Rise (M-H1) District, Mixed Use – Active Frontage (MU-2f2.0h10) District, Mixed Use – Active Frontage (MU-2f3.0h23) District, Mixed Use – Active Frontage (MU-2f3.0h27) District and Mixed Use - Active Frontage (MU-2f4.5h20) District



Proposed Land Use Maps

Bylaw 4

From: Residential – Contextual One / Two Dwelling (R-C2) District, DC Direct Control District, Multi-Residential – Contextual-Oriented (M-CG) District, Multi-Residential – Contextual Low Profile (M-C1) District, Commercial – Corridor 1 (C-COR1) District and Commercial – Corridor 2 (C-COR2) District

To: Multi-Residential – Contextual Low Profile (M-C1) District, Multi-Residential – Contextual Medium Profile (M-C2) District, Mixed Use - General (MU-1f2.0h12) District, Mixed Use - General (MU-1f3.0h46) District, Mixed Use - General (MU-1f5.0h37) District, Mixed Use – Active Frontage (MU-2f3.0h46) District, Mixed Use - Active Frontage (MU-2f4.5h20) District, Mixed Use - Active Frontage (MU-2f4.5h23) District, Mixed Use - Active Frontage (MU-2f5.0h26) District and Mixed Use - General (MU-1f3.0h16) District

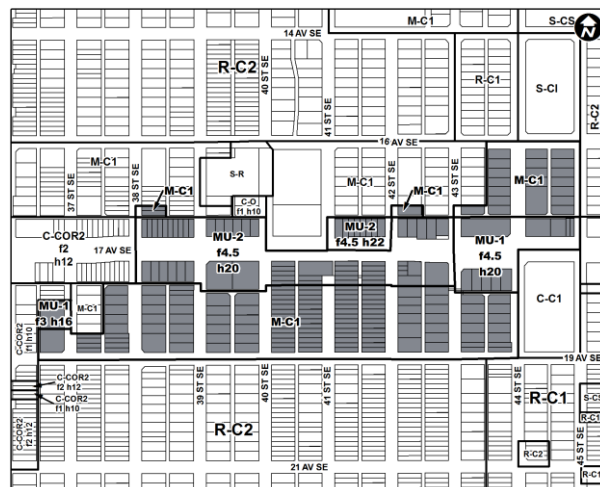
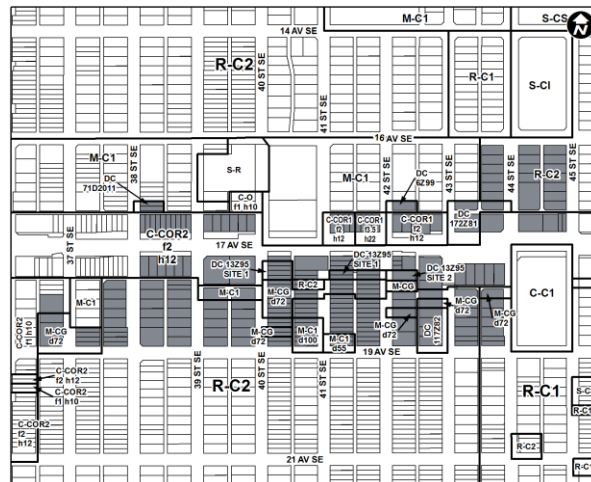


Proposed Land Use Maps

Bylaw 5

From: Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District, Multi-Residential – Contextual-Oriented (M-CG) District, Multi-Residential – Contextual Low Profile (M-C1) District, DC Direct Control District, Commercial – Corridor 1 (C-COR1) District and Commercial – Corridor 2 (C-COR2) District

To: Multi-Residential – Contextual Low Profile (M-C1) District, Mixed Use - General (MU-1f3.5h22) District, Mixed Use - General (MU-1f4.5h20) District and Mixed Use - Active Frontage (MU-2f4.5h20) District



Proposed Land Use Maps

Bylaw 6

From: Residential – One Dwelling (R-C1) District, Residential – Contextual One / Two Dwelling (R-C2) District, Multi-Residential – Contextual-Oriented (M-CG) District, DC Direct Control District, Commercial – Neighbourhood 2 (C-N2) District and Commercial – Corridor 2 (C-COR2) District

To: Multi-Residential – Contextual Low Profile (M-C1) District, Mixed Use - General (MU-1f3.0h16) District, Mixed Use - General (MU-1f4.5h20) District and Mixed Use - Active Frontage (MU-2f3.0h16) District



Engagement Summary

International Avenue Final Engagement Summary Report for Calgary Planning Commission

Stakeholder Engagement, Research and Communication

Following Calgary Planning Commission, notifications for Public Hearing of Council will be mailed out to land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

City-led engagement

Engagement Overview

A 2-phase engagement and communications strategy was developed to facilitate meaningful opportunity for engagement with in-person and online opportunities. The Engage Spectrum level for this project is Listen and Learn which is defined as "We will listen to stakeholders and learn about their plans, views, issues, concerns, expectations and ideas."

Stakeholders were asked to review the proposed revisions to the Southeast 17 Main Street ARP and share their thoughts. The public was also asked to review the proposed land use districts and consider the balance of Commercial and Residential uses. Stakeholders were also able to share any other ideas or comments on the information provided.

Between November 2017 and February 2018, we offered the following engagement opportunities:

- Online Survey, November 16 to December 11, 2017
- Information Café, November 29, 2017
- Online Survey, January 31 – February 14, 2018
- Information Session, January 31, 2018

Over the course of The City's phased engagement program we collected over 400 comments and ideas from approximately 195 citizens.

What we heard/ what we did

Citizens provided a diversity of comments through all phases of engagement and the main themes identified by citizens across all phases have been highlighted in the chart below including a response in how this feedback was used by The City to inform the draft policy plan.

What we heard (Issue or opportunity identified)	What changes were made
Areas for increased density	The proposed increase in density in the Plan fully aligned with the outcome from several public engagement sessions with the community in 2010.

Engagement Summary

Bus Rapid Transit on 17 Ave SE	BRT's construction implication is outside the scope of the policy work. The BRT construction work is expected to be finalised by the end of 2018.
Density is too restrictive	The density is proposed in specific locations, mainly around the major transit hubs as per the public engagement and charrettes to avoid any negative implication to the existing residential urban fabric. The Plan also proposed transition areas.
Concern regarding pedestrian safety	The Plan includes several policies that enforce the outcome of safe pedestrian realm.
Concerns regarding traffic congestion	The transportation modeling for the Plan Area is built to accommodate the proposed density. The Mobility section in the Plan includes policies that are designed to prevent congestion and it includes specific parking framework to avoid future implications on the Main Street.
Concerns regarding general safety within the plan area	The plan promote the use of Crime Prevention Through Environmental Design (CPTED) policies in all developments in the area aiming to help promoting public safety.
Desire for increased clarity in the information provided and the use of plain language in the plan	The plan has been reviewed and edited to improve clarity.
Concerns regarding transition from existing development to future redevelopment envisioned in the plan	The proposed land use districts are fully aligned with the outcome of all public engagements.

Communications overview

A communications plan was developed to inform the community about the project and the engagement opportunities. The tactics used to inform the public on an ongoing basis included:

- Project specific engage portal page (engage.calgary.ca/17AveSE)
- A postcard was sent advising of a two-part input and information session for your main street area to discuss options for growth with City planners.
- A letter was sent to advise residents of the two events and surveys.
- Social media advertising was used to promote the events and surveys.

The following communications tactics were employed to promote participation in each phase of our engagement program:

- Communication with the local area Councillor
- Facebook advertisement to area residents

Engagement Summary

Full engagement reports

The full What We Heard Reports from each phase with verbatim comments can be found here:

- Phase one engagement: [What We Heard Report](#)
- Phase two engagement: [What We Heard Report](#)

International Avenue BRZ Supporting Letter



September 25, 2018

Jihad Bitar, PhD,
Planning Lead
City of Calgary.
P.O. Box 2100, Stn. M,
Calgary, AB, T2P 2M5

Dear Mr: Bitar

We are writing to express our gratitude and support for the proposed International Avenue Street Area Redevelopment Plan. The International Avenue Business Revitalization Zone (BRZ) is a non-profit organization formed to promote, improve and create a more pleasant community in which to shop and live. Currently, we have over 425 businesses and services that operate within the 35 blocks of 17 Avenue SE from 26 to 61 Streets and area.

The 17th Avenue SE business district is a significant employer and provider of commercial goods for the local community and the city overall. It is a complex mix of businesses and services many of which have been in business for a decade or more, and comes with its own unique local challenges. The proposed ARP does an excellent job in setting forth clear policy to provide positive direction of the main street. Furthermore, the document best represents the key necessary policy framework and deals with some issues that have been outstanding for decades. The core ideas outlined within the ARP we wholeheartedly agree with in order to continue moving International Avenue on a positive path forward.

We met numerous times with administration to discuss issues that have posed significant problem towards our revitalization efforts over the years. The comprehensive document produced does an outstanding job to ensure our concerns are addressed in this document. We are particularly pleased with the document as a whole and believe it provides the best way forward to ensure a bright future for one of Calgary's longest and most complicated avenues. Issues that are specific to our area have been adequately addressed. Overall, we are supportive of the proposed Area Redevelopment Plan and anxiously await it's approval. Thank you very much for this essential document.

Regards,



Alison Karim-McSwiney, MiPM(UK), BA, BREC
Executive Director

250, 3515 17 Avenue SE, Calgary, AB, T2A 0R5
Telephone: 403 248-7288 internationalavenue.ca

Albert Park Community Associations Supporting Letter

From: APRH Community Association [mailto:aprh@shaw.ca]
Sent: September 27, 2018 4:18 PM
To: Bitar, Jihad <Jihad.Bitar@calgary.ca>
Cc: aprh@shaw.ca
Subject: [EXT] Re: Final Albert Park Radisson Heights ARP

Hello Jihad,

Thank you for your presentation at our Board Meeting. We accept and agree with your proposed amendments of the Albert Park Radisson Heights ARP.

We, the Board and the community, will support these amendments when they are presented to CPC and City Council.

Thank you for keeping us in the loop with the many changes.

Nancy Kearney

President

Albert Park Radisson Heights Community Association

Planning & Development Report to
Calgary Planning Commission
2018 October 18

ISC: UNRESTRICTED
CPC2018-1147
Page 1 of 6

Land Use Amendment in Beddington Heights (Ward 4) at 1310 and 1312 Berkley Drive NW, LOC2018-0105

EXECUTIVE SUMMARY

This land use redesignation application was submitted by Rick Balbi Architect on 2018 May 08 on behalf of the landowners Little Scholars Daycare Inc and Gargi Devi Saini. The application proposes to change the designation of two adjacent properties from Residential – Contextual One/Two Dwelling (R-C2) District to a DC Direct Control District based on the Residential – Contextual One / Two Dwelling (R-C2) District to allow for the additional use of child care service.

The proposal is in keeping with the *Municipal Development Plan* and aligns with the *Child Care Service Policy and Development Guidelines*.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares ± (0.14 acres ±) located at 1310 and 1312 Berkley Drive NW (Plan 7910490, Block 28, Lot 33) from Residential – Contextual One / Two Dwelling (R-C2) District **to** DC Direct Control District based on the Residential – Contextual One / Two Dwelling (R-C2) District to accommodate a Child Care Service, with guidelines (Attachment 3); and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

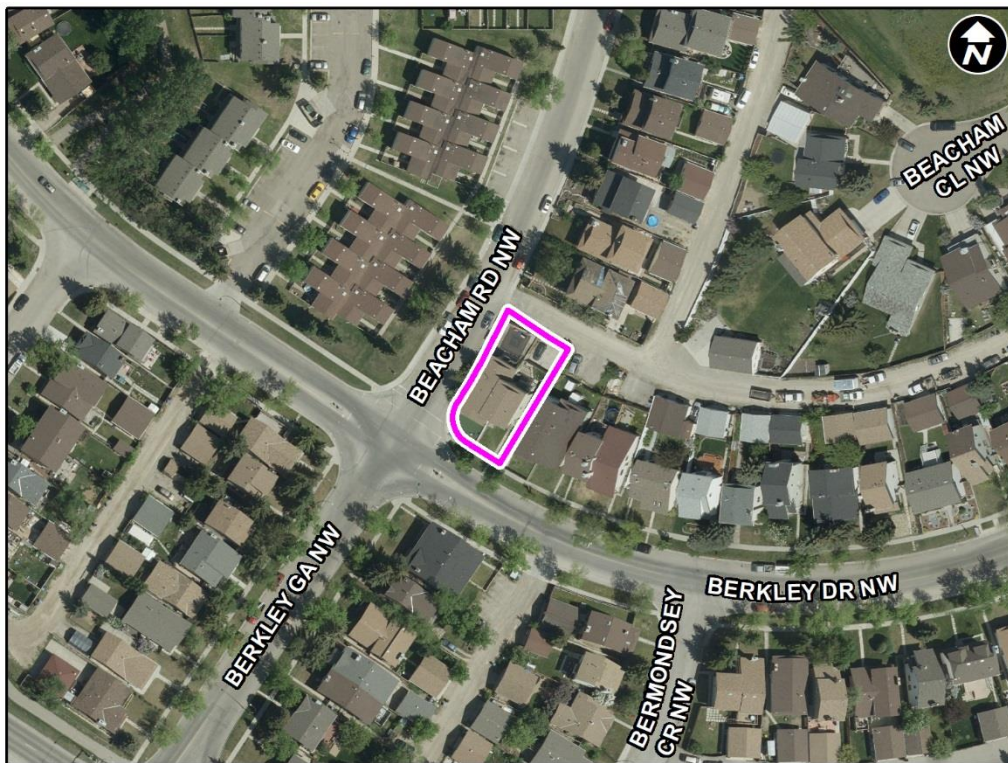
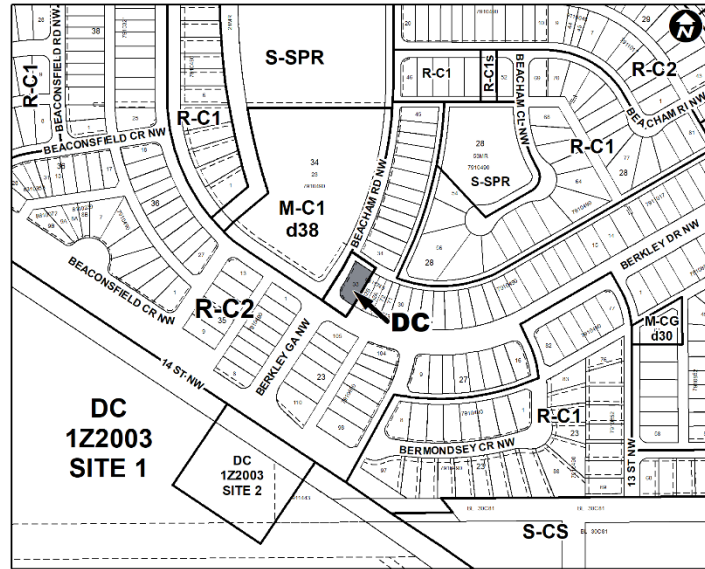
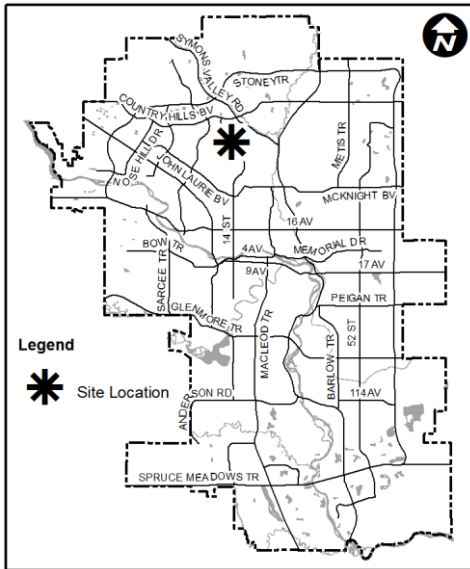
BACKGROUND

This land use amendment was submitted on 2018 May 8 by Rick Balbi Architect on behalf of the landowners, Little Scholars Daycare Inc. and Gargi Saini. No development permit has been submitted at this time. As noted in the Applicant's Submission (Attachment 1), the applicant is intending on expanding an existing child care service into the eastern half of the existing semi-detached dwelling.

At the time of construction of the semi-detached building in 1979, a development permit was approved to allow a child care facility in the west unit as this was a listed use in the district at the time. This child care facility has been operating with service to 35 to 40 children since that time. The east side of the building is currently operating as a day home with service to six children.

Land Use Amendment in Beddington Heights (Ward 4) at 1310 and 1312 Berkley Drive NW, LOC2018-0105

Location Maps



Land Use Amendment in Beddington Heights (Ward 4) at 1310 and 1312 Berkley Drive NW, LOC2018-0105

Site Context

The subject site is located on a corner parcel in the community of Beddington Heights on the corner of Beacham Road NW and Berkley Drive NW, which is one of the primary access points to the Beddington Heights community.

Surrounding development includes semi-detached dwellings to the east, single detached dwellings to the south and north, and multi-residential development to the west. Nose Hill Park is located one block southwest of the site, across 14 Street NW (a controlled intersection with crosswalks). Two other local parks are located within 300 metres of the site.

The site is currently developed with a semi-detached dwelling bridging two parcels (these parcels were originally one lot when first developed, but they underwent subdivision sometime since then). The corner parcel has an area of approximately 330 square metres and the east parcel has an area of approximately 240 square metres. The west unit is currently used as a child care service, and in related operations, the east side is currently a dwelling unit operating a day home with six children. Parking for the east unit is provided in a parking pad accessed from the rear lane on the east parcel. No on-site parking is currently provided on the west parcel.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This proposal allows for a child care service facility to operate within the existing semi-detached dwelling and is in keeping with the applicable legislation as identified in the Strategic Alignment section of this report.

Planning Considerations

The primary planning consideration in evaluation of this application constituted determining whether a child care service use could be accommodated on the subject site and whether it fits within the policies of the *Municipal Development Plan* and the *Child Care Service Policy and Development Guidelines*.

Land Use

The subject site is currently designated Residential – Contextual One/Two Dwelling (R-C2) District. The R-C2 designation allows for low density residential development in the form of single detached dwellings, semi-detached dwellings, and duplex dwellings. The district also provides for two forms of home based child care for up to ten children (home based child care – classes 1 and 2).

This application proposes redesignating the two parcels from the existing Residential – Contextual One/Two Dwelling (R-C2) District to a Direct Control (DC) District based on the Residential – Contextual One/Two Dwelling (R-C2) District.

The proposed Direct Control District is intended to:

Land Use Amendment in Beddington Heights (Ward 4) at 1310 and 1312 Berkley Drive NW, LOC2018-0105

- allow the additional discretionary use of child care service;
- allow for a residential use consistent with the surrounding development if a child care service is discontinued; and
- be based on the rules of the Residential – Contextual One/Two Dwelling (R-C2) District.

The review of the application considered as an alternative the stock land use district Special Purpose – Community Institution (S-CI) District as suggested in the *Child Care Service Policy and Development Guidelines*. However, this land use district provides for a number of uses that would not be compatible in this location and context (for example, columbarium, conference and event facility, post-secondary learning institution). Thus, a Direct Control (DC) District with the Residential – Contextual One/Two Dwelling (R-C2) District as the base district is proposed.

Development and Site Design

A discretionary use development permit application will be required to enable the expansion of the child care service use on these parcels. The ultimate number of children, on-site parking stalls, location of pick-up and drop-off stalls, and outdoor play areas will be determined through that process. The child care service operators will also require Provincial licensing and thus will be further evaluated by the Calgary Region Child and Family Services Authority.

Environmental

There are no existing environmental conditions on these residential parcels. An environmental site assessment was not required for this application.

Transportation

The site is located on a collector street (Berkley Drive NW) and has rear lane access. Berkley Drive NW serves as one of the primary access points to the community of Beddington Heights. The site is within 150 metres of a transit stop. A transportation impact assessment was not required.

Utilities and Servicing

Water, sanitary and storm mains are available to this site. Further details for servicing and waste collection facilities will be reviewed at the development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application has been advertised online. No public meetings were held by the applicant or Administration.

Land Use Amendment in Beddington Heights (Ward 4) at 1310 and 1312 Berkley Drive NW, LOC2018-0105

A letter from the Beddington Heights Community Association was submitted by the applicant (Attachment 2). The community association letter indicates general support for the use but raised concerns about traffic and the ultimate provision of parking on the site. These items will be reviewed at the development permit stage.

No letters were received from the public as a result of the notice posting. The applicant provided 52 signed form letters within their submission indicating support for the proposed land use. The signatories were from various locations within and nearby the community.

Following Calgary Planning Commission, notifications for the Public Hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSRP) which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Municipal Development Plan (Statutory, 2009)

These parcels are located in the Established Area of the Developed Communities as identified on Map 1: Urban Structures in the *Municipal Development Plan*. Modest redevelopment in these areas is encouraged in a form of development that respects the scale and character of the neighbourhood.

The *Municipal Development Plan* recognizes that child care services are an integral part of 'complete communities' and should be accommodated within neighbourhoods as appropriate. This application proposes expanding the child care service into the east parcel and is intended to retain the existing semi-detached building form and thus fits within the established context of the surrounding community. Additionally, the location of the site, along a primary entrance to the community of Beddington Heights, adds to the convenience this service provides for parents in the area. The letters of support provided by the applicant further show that the use is valued by the community.

Child Care Service Policy and Development Guidelines (Non-statutory, 2009)

The application conforms to a number of the site selection criteria listed in the *Child Care Service Policy and Development Guidelines*. The following location criteria were consistent with the guidelines:

- the site is located on a corner site;
- the site is on a collector street;
- the site is large enough to accommodate outdoor play areas; and

Land Use Amendment in Beddington Heights (Ward 4) at 1310 and 1312 Berkley Drive NW, LOC2018-0105

- the site can provide parking on-site (there is a slight grade difference between the lane and the rear yard of the corner parcel, however, this could be modified to accommodate access).

The site is not in an activity focused area such as a school or commercial area. However, this site is situated at one of the main entrances to Beddington Heights off 14 Street NW and thus already experiences higher levels of traffic and activity as would be associated with an activity focused area.

Social, Environmental, Economic (External)

The proposed land use allows the continuation and expansion of a needed service for families and contributes to the goal of complete communities.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed land use amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal and operational risks which have been identified will be managed at the time of development permit.

REASONS FOR RECOMMENDATIONS:

The proposal is consistent with the applicable policies of the *Municipal Development Plan* and the *Child Care Policy and Development Guidelines*. The redesignation will support the continuation of an existing child care service and contributes to the goal of establishing complete communities through providing important related child care services.

ATTACHMENT(S)

1. Applicant's Submission
2. Community Association Letter
3. Proposed DC Direct Control District Bylaw

Applicant's Submission

The intent of this application is to redesignate two parcels located at 1312 and 1310 Berkley Drive NW from Residential - Contextual One / Two Dwelling (R-C2) District, to a Direct Control (DC) District, based on the existing R-C2, with the additional Use of "Child Care Service". The reason for the application is twofold. First, in order to provide legal and conforming land use status to the existing Daycare located at 1312 Berkley Drive NW, which has been operating as Daycare for over 30 years, prior to the current Land Use Bylaw. Secondly, to allow the Daycare to expand into the eastern half of the existing semi-detached "residence" which currently operates as a Day Home. The redesignation of these parcels would not materially change the nature of the existing uses, but would increase the number of children able to be cared for and provide certainty for the owners in the event something happened to the existing facility. The parcels would be consolidated and the facility would operate under a single licence regulated by the Province of Alberta.

The application is being made as a Direct Control as there are no districts that are low-density residential in nature that allow for the Use of "Child Care Service". Since the intent of the application is strictly to allow for this Use that is in part already operating in this location, we feel that adding this Use to the existing base district provides for a predictable outcome that maintains stability for the neighbourhood and for the owners.

Originally, the Daycare facility (1312 Berkley Drive NW) occupied the entire building, but was divided into two units around 1979, where the eastern unit (1310 Berkley Drive NW) became a single family residence. The Daycare is now under operation of "Little Scholars Daycare and Preschool", an accredited facility with a history of exceptional care that has spots for 37 children. When the owners of "Little Scholars" purchased the eastern half of the building in 2011, they opened a Day Home that operates separately. This existing Day Home has six (6) children, which is permitted as a home occupation within the existing R-C2 district. By renovating and combining the facility, including a small addition at the back of the the east unit, the total number of children proposed to be in care (day care, preschool and before/after school care) is approximately 73.

The parcels are located on the corner of Berkley Drive and Berkley Gate NW, just one block to the east of 14th Street NW. The Calgary Transportation plan identifies 14th Street as an arterial road, which is directly connected to downtown. There are also transit routes within 100m of the site along Berkley Drive. Furthermore, the parcel is just two (2) km west of a future LRT Station at Centre Street and Beddington Blvd North. While the parcel has good access to transportation networks, the overwhelming majority of children come from the immediate surrounding neighbourhood and walk. It has been thus far been an ideal location. The site is adjacent to RC-2 properties to the north, south and east, but borders on a Multi-residential Contextual (MC-1 d38) complex to the west.

Currently the majority of the drop off and pickup is done curb-side, as there are ample street parking spots adjacent to the property on both Berkley Drive and Berkley Gate NW. Drop-off and pick-ups generally take less than 5 minutes. The property does have lane access, with parking at the rear. As mentioned, many of the existing children coming to the facility come from within walking distance, particularly the multi-residential complex across the street and the Calgary Housing Company complex, just 450m to the east. Although there is abundant street parking and access from all directions, we acknowledge that an increase in the number of children may necessitate more off-street parking. Additional on-site parking could be provided off the lane, if required through the development permit application process.

Prior to making this application, the owners contacted the Community Association and neighbours who would be potentially impacted. Although no negative impacts or drastic changes in existing operations are anticipated, communication with the community has been sought to ensure transparency. The support has been overwhelming and we are pleased to include 68 letters endorsing this application. Childcare spots in affordable facilities with are in high demand. Approving this land use application would increase a valuable and essential service.

The owners of "Little Scholars Daycare and Preschool" pride themselves providing the highest quality care that prepares children for their future. They have a proven track record in their existing location. An expansion of their operation will allow for further benefits and opportunities for families. We request the support and approval from the City of Calgary Administration and Council for this Land Use Amendment that will add value to the both the community and the city as a whole.

Community Association Letter



Little Scholars Daycare and Preschool
1312 Berkley Drive N.W.
Calgary, AB T3K 1S9

April 27, 2018

Re: Little Scholars Daycare and Preschool (File 17-248)

Hi Gargi

This letter is to express support from the Board of Directors of the Beddington Heights Community Association for Little Scholars Daycare and Preschool increasing its capacity by an additional 10-15 spots as you redevelop the second side of your duplex to an accredited childcare facility. We have an ongoing need for preschool and daycare spots in our community that your facility can help service.

Please note our only concern with the increased daycare capacity is an increase in traffic in the area and we want to ensure the completion of the additional parking spots at the rear of the building as well as communication with parents about abiding by the traffic and parking rules around this already busy 4-way intersection.

Best of luck with your project.

Sincerely,

The Board of Directors
Beddington Heights Community Association

Proposed Direct Control Guidelines

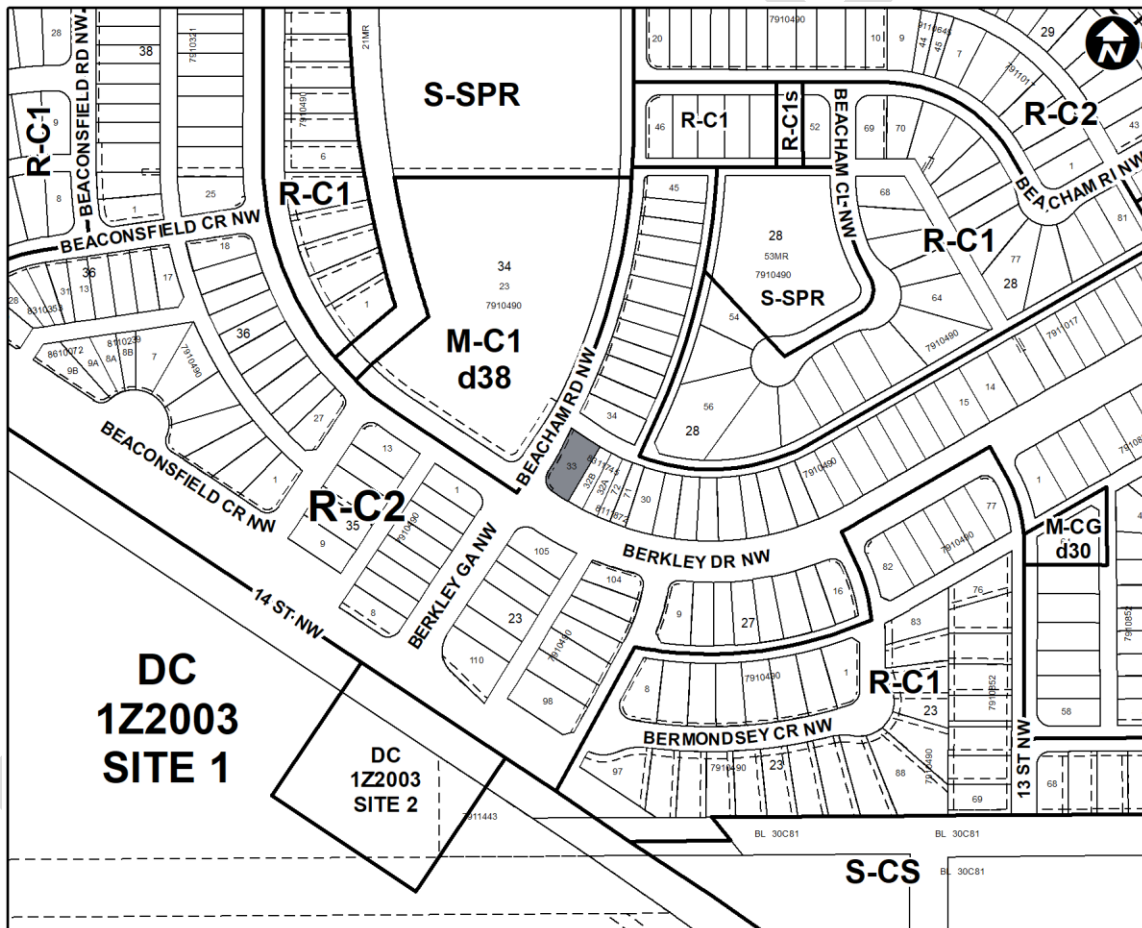
WHEREAS it is desirable to amend the Land Use Bylaw Number 1P2007 to change the land use designation of certain lands within the City of Calgary;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26 as amended;

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

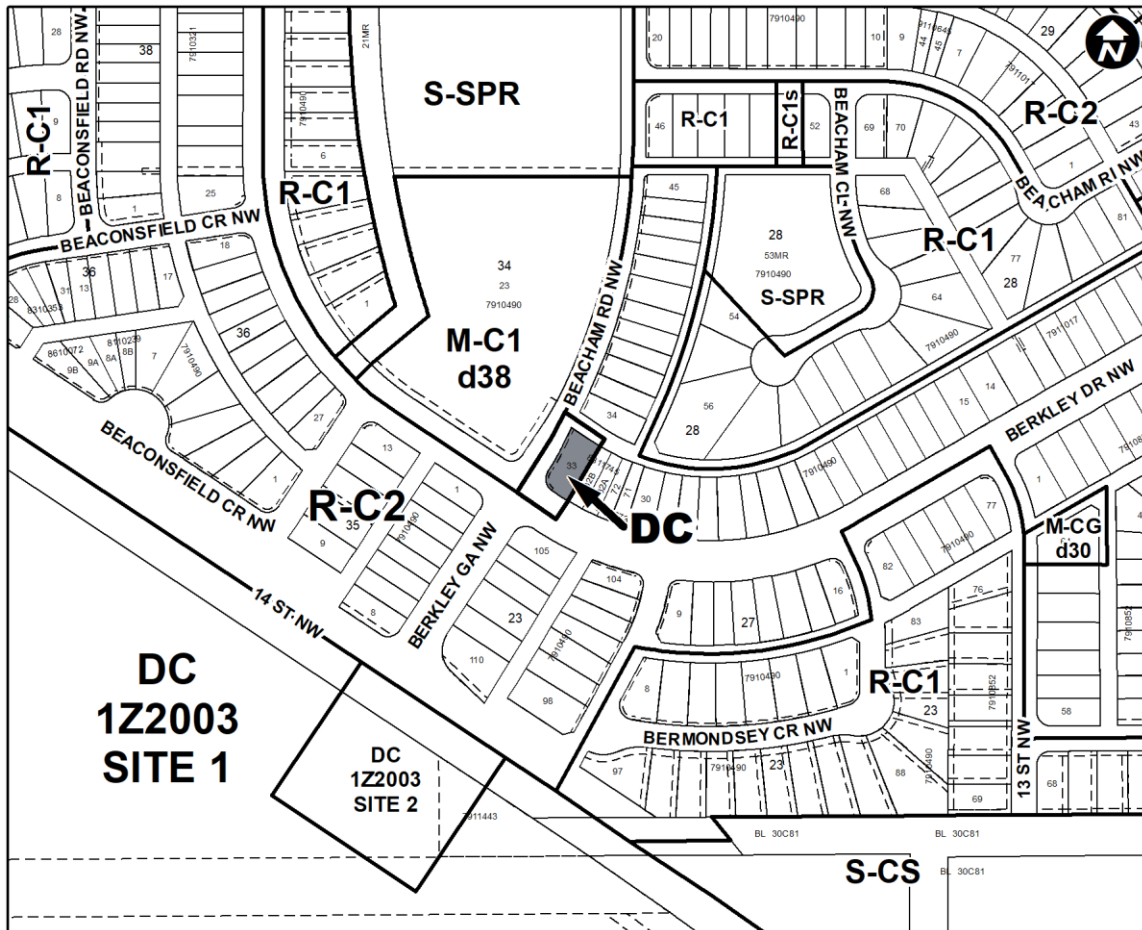
1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".

SCHEDULE A



Proposed Direct Control Guidelines

SCHEDULE B



DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control District is intended to:

- (a) accommodate a **Child Care Service**.

Compliance with Bylaw 1P2007

2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

4 The **permitted uses** of the Residential – Contextual One / Two Dwelling (R-C2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Proposed Direct Control Guidelines

Discretionary Uses

5 The ***discretionary uses*** of the Residential – Contextual One / Two Dwelling (R-C2) District of Bylaw 1P2007 are the ***discretionary uses*** in this Direct Control District with the addition of:

- (a) **Child Care Service.**

Bylaw 1P2007 District Rules

6 Unless otherwise specified, the rules of the Residential – Contextual One / Two Dwelling (R-C2) District of Bylaw 1P2007 apply in this Direct Control District.

TEXT FOR DISCUSSION ONLY

Planning & Development Report to
Calgary Planning Commission
2018 October 18

ISC: UNRESTRICTED
CPC2018-1101
Page 1 of 10

Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

EXECUTIVE SUMMARY

This application was submitted by Battistella Developments on 2018 May 18 on behalf of the landowners Churchstone Enterprises Limited, Elaine Lai-King Wong, David Wong, and Midelco Holdings Limited. This application proposes to designate the former Hillhurst Baptist Church as a Municipal Historic Resource and to redesignate the subject parcels from Commercial – Corridor 1 (C-COR1f2.8h13) District and Multi-Residential – Contextual Grade-Oriented (M-CGd72) District to DC Direct Control District. The proposed Direct Control District includes two site areas, and is based on the Multi-Residential – Contextual Medium Profile (M-C2) District, Multi-Residential – High Density Medium Rise (M-H2) District, and Commercial – Corridor 1 (C-COR1) District. The proposed DC Direct Control District is intended to:

- implement a site specific heritage density incentive for the legal protection of a heritage building;
- allow for medium and high density medium-rise multi-residential development in alignment with the policies of the *Hillhurst/Sunnyside Area Redevelopment Plan*;
- allow a maximum building height of 32 metres for Site 1 and a maximum building height of 16 metres for Site 2 (an increase from the current maximums of 13 and 12 metres respectively); and
- implement the density bonus provisions of the *Hillhurst/Sunnyside Area Redevelopment Plan*.

The proposal is in keeping with the applicable policies of the *Municipal Development Plan*. The proposal is in keeping with the intent of the applicable policies of the *Hillhurst/Sunnyside Area Redevelopment Plan*, with amendments required to accommodate the proposed land use redesignation. The amendments required include changes to maps indicating maximum heights, floor area ratios, and land use policy areas, and policy changes to facilitate density bonuses in exchange for designation of historic resources and provision of urban design improvements.

No development permit application has been received at this time.

Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to the Hillhurst/Sunnyside Area Redevelopment Plan (Attachment 3); and
2. Give three readings to the proposed bylaw; and
3. **ADOPT**, by bylaw, the proposed redesignation of 0.36 hectares \pm (0.91 acres \pm) located at 1110, 1114, 1120, 1124, 1126 Gladstone Road NW (Plan 5609J, Block Q, Lots 19 to 28) from Commercial – Corridor 1 (C-COR1f2.8h13) District and Multi-Residential – Contextual Grade-Oriented (M-CGd72) District to DC Direct Control District to accommodate multi-residential development and the preservation of a heritage building, with guideline (Attachment 4); and
4. Give first reading to the proposed bylaw; and
5. **WITHHOLD** second and third reading pending Municipal Historic Designation of the site or until any other mechanism to ensure such designation is in place.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

This land use amendment application was submitted by Battistella Developments on behalf of the landowners Churchstone Enterprises Limited, Elaine Lai-King Wong, David Wong, and Midelco Holdings Limited, on 2018 May 18. As noted in the Applicant's Submission (Attachment 1), the applicant is intending to accommodate high density medium rise multi-residential development, designate the Hillhurst Baptist Church as a Municipal Historic Resource and rehabilitate the building for future commercial uses, and implement the density bonus provisions of the *Hillhurst/Sunnyside Area Redevelopment Plan*.

The Hillhurst Baptist Church was constructed by parish volunteers in 1907 during the early development of Hillhurst. It was enlarged to its present size in 1914 and is a good example of Gothic Revival architecture. The building no longer serves as a church and in recent years has housed a number of commercial uses.

The Hillhurst Baptist Church is identified as a Historic Resource on The Calgary Heritage Authority's Inventory of Evaluated Historic Resources. As such, it merits designation as a Municipal Historic Resource. The applicant has agreed to allow this designation and a legal agreement has been prepared that will be required to be in place prior to Council's second and third reading of the Area Redevelopment Plan amendment and land use redesignation bylaws.

Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

Site Context

The subject lands are located along the north side of Gladstone Road NW in the community of Hillhurst. To the north of the sites across a lane are a gas station, and several single detached dwellings that are designated as Multi-Residential – Contextual Grade-Oriented District. To the west are single detached dwellings that are also designated as Multi-Residential – Contextual Grade-Oriented Districts. To the south across Gladstone Road NW are low-rise multi-residential developments and a commercial development fronting onto 10 Street NW. The parcel containing the commercial development was recently redesignated (DC 152D2018) to allow for multi-residential development with a maximum height of 32 metres and maximum floor area ratio of 5.0. To the east across 10 Street NW is a mixed-use 11-storey multi-residential building with commercial uses at grade.

The subject lands are located within approximately 200 metres walking distance of the Sunnyside LRT Station. The eastern portion of the site is developed with the former Hillhurst Baptist Church, which currently houses a commercial use. Additional buildings and uses on the subject lands include a low rise commercial building, and three low-rise multi-residential buildings. Vehicular access to the site is available via a rear lane, and there is on-street parking on Gladstone Road NW.

According to data from The City of Calgary 2018 census, the population of Hillhurst peaked in 2015 with a decline of 1.8 percent since that time. Figure 1 below summarizes the population trends in the community of Hillhurst.

Figure 1: Community Peak Population

Hillhurst	
Peak Population Year	2015
Peak Population	6,737
2018 Current Population	6,616
Difference in Population (Number)	-121
Difference in Population (Percent)	-1.8%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained on the [Hillhurst Profile](#) online page.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal is to allow for medium and high density medium-rise multi-residential development, the adaptive re-use and designation of the Hillhurst Baptist Church as a Municipal Historic Resource, and to implement the density bonus provisions of the *Hillhurst/Sunnyside Area Redevelopment Plan*.

Although amendments to the *Hillhurst/Sunnyside Area Redevelopment Plan* are required, the proposal is generally aligned with the objectives of the applicable policies, as discussed in the Strategic Alignment section of this report.

Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

Planning Considerations

The primary planning considerations in evaluation of this application included determining appropriate base districts for the proposed DC Direct Control District, ensuring the preservation and restoration of the Hillhurst Baptist Church, and implementing the density bonus provisions of the *Hillhurst/Sunnyside Area Redevelopment Plan*.

Land Use

This application is to redesignate the site from the existing Commercial – Corridor 1 (C-COR1f2.8h13) District and Multi-Residential – Contextual Grade-Oriented (M-CGd72) District to DC Direct Control District. The existing Commercial – Corridor 1 (C-COR1f2.8h13) allows for a broad range of commercial uses and opportunities for residential and office uses, and has a maximum floor area ratio of 2.8 and a maximum height of 13 metres. The Multi-Residential – Contextual Grade-Oriented (M-CGd72) District allows for a variety of multi-residential development forms with a maximum height of 12 metres and a maximum density of 72 units per hectare.

The proposed DC Direct Control District is intended to:

- implement a site specific heritage density incentive for the legal protection of a heritage building;
- provide for medium and high density medium rise multi-residential development in compliance with the policies of the *Hillhurst/Sunnyside Area Redevelopment Plan*;
- allow a maximum building height of 32 metres for Site 1 and a maximum building height of 16 metres for Site 2;
- set a maximum cumulative floor area ratio to be allocated between sites at the development permit stage;
- implement the density bonus provisions of the *Hillhurst/Sunnyside Area Redevelopment Plan*; and
- locate the tallest building heights closest to 10 Street NW and reduce building height in proximity to lower density residential areas to the west.

Site 1 of the proposed Direct Control District incorporates 1110, 1114, and 1120 Gladstone Road NW and includes the Hillhurst Baptist Church. The rules of Site 1 are based on the Multi-Residential – High Density Medium Rise (M-H2) District with a maximum height of 32 metres. A bonus density provision will allow a maximum floor area ratio of 5.0 within Site 1 with the designation of the Hillhurst Baptist Church as a Municipal Historic Resource. The DC Direct Control District will also allow commercial uses within the Hillhurst Baptist Church as per the existing Commercial – Corridor 1 (C-COR1) District, with the addition of the use Temporary Residential Sales Centre to facilitate the sales of future residential units on the subject sites.

Site 2 of the proposed Direct Control District incorporates 1124 and 1126 Gladstone Road NW is based on the Multi-Residential – Contextual Medium Profile (M-C2) District and allows a maximum height of 16 metres, and a floor area ratio of up to 2.5 in accordance with the density bonus provisions of the *Hillhurst/Sunnyside Area Redevelopment Plan*.

Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

The proposed DC Direct Control District provides the potential opportunity for a significant reduction in minimum motor vehicle parking requirements. The location is appropriate for consideration of such a reduction based on the close proximity to the Sunnyside LRT Station, high quality transit service along 10 Street NW, and a broad range of services and amenities in the immediate vicinity, including a grocery store. At the development permit stage the applicant will be required to provide a parking study and include transportation demand management measures to the satisfaction of the development authority in order to qualify for the potential parking reduction.

Development and Site Design

The site is currently developed with the Hillhurst Baptist Church which contains a commercial use, a low-rise commercial building, and three low rise multi-residential buildings with surface parking lots accessed from the rear lane.

The proposed DC Direct Control District is intended to allow the potential development of two multi-residential buildings with access provided via the rear lane. It is also intended to facilitate the Municipal Historical Designation and refurbishment of the Hillhurst Baptist Church, and possible development of a new public plaza located at the corner of Gladstone Road NW and 10 Street NW.

A development permit application has not been submitted at this time. Considerations to be evaluated at the development permit stage include site layout, building designs, and appropriate interface with lower density residential uses.

Environmental

There are no noteworthy environmental features on this site, and an Environmental Site Assessment was not required for this application.

Transportation Networks

A Transportation Impact Assessment (TIA) and parking study was not required as part of this land use amendment. The proposed DC Direct Control District includes provisions for a parking study which will inform the motor vehicle parking stall requirements of future developments. Vehicular access, parking, and loading will be from the rear lane with specifics to be determined at the development permit stage. A transit stop served by routes 4 and 104 is located on 10 Street NW approximately 120 metres from the subject site. The site is also located within approximately 200 metres walking distance to the Sunnyside LRT Station.

Utilities and Servicing

Water, sanitary, and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time.

Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application has been advertised online. A webpage on the City of Calgary website provides information on the application and contact information for providing comments.

In addition, a public open house was held by the applicant with City staff in attendance at the Hillhurst/Sunnyside Community Association building on 2018 June 13 and was attended by approximately 30 people.

The Hillhurst/Sunnyside Community Association submitted a letter regarding the proposed land use redesignation on 2018 June 25 (Attachment 2). Administration met directly with representatives of the Hillhurst/Sunnyside Community Association on 2018 July 5 to discuss the application and gain a better perspective on their concerns.

Administration also received 26 letters from the public in response to the application. Three letters indicated support, including one from the Kensington Business Revitalization Zone/Business Improvement Area, and are summarized as follows:

- The proposal honours the intent of the *Hillhurst/Sunnyside Area Redevelopment Plan*.
- Designation of the Hillhurst Baptist Church is a worthwhile undertaking.
- Location close to the Sunnyside LRT Station and other amenities is appropriate for the proposed scale of development.
- Reduced parking rates would allow for more affordable housing in the community.
- Buildings of similar height and scale to those allowable under the proposal exist nearby.
- Proposal would bring vitality to the area.

Twenty letters indicated opposition, and are summarized as follows:

- Designation of the Hillhurst Baptist Church is not supported.
- Concern regarding infrastructure capacity.
- Perception that the proposal is not aligned with the policies of the *Hillhurst/Sunnyside Area Redevelopment Plan*.
- Proposed height and density are excessive.
- Concerns over increased traffic and demand on street parking.
- Concerns over increased noise associated with future development.
- Preference to retain low-rise and low-density built forms.
- Objection to any new redesignations or development until a Mobility Assessment and Plan is completed.
- Objection to redesignation without a concurrent development permit application.
- Lack of architectural controls and concern about fit with existing character.
- Location is inappropriate for high density development.
- Uncertainty about the number of possible units if density is defined by floor area ratio.
- Potential for privacy and overlooking issues.
- Proposal would change the ambiance and character of Hillhurst.

Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

Some public submissions expressed conditional support if certain criteria could be achieved such as high quality design or reduction in maximum building height.

Following Calgary Planning Commission, notifications for the Public hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the South Saskatchewan Regional Plan (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns, as it proposes an increase in development intensity on a site within an adequately serviced and appropriately planned inner city mixed-use neighbourhood.

Municipal Development Plan (Statutory, 2009)

The site is located in the 'Inner City' as identified on Map 1: Urban Structure Map in the *Municipal Development Plan*. The proposed redesignation would support the revitalization of the local community by adding population and a mix of uses, and facilitating moderate intensification in a form and nature that respects the scale and character of the neighbourhood.

The *Municipal Development Plan* Heritage policies recognize that historic preservation is part of good city building and fostering community identity and pride. As such, these policies encourage the identification, protection and management of Calgary's historic resources by encouraging owners to conserve and/or enhance Calgary's historic resources and The City to be a leader in preserving and enlivening these resources using all tools and mechanisms that are available to a municipality (subsection 2.3.3).

The proposal is consistent with the policies of the *Municipal Development Plan*.

Hillhurst/Sunnyside Area Redevelopment Plan (Statutory, 1988)

The proposal is located within the Transit Oriented Development Area of the *Hillhurst/Sunnyside Area Redevelopment Plan*. The Transit Oriented Development policies indicate that land use redesignations must be consistent with the general land use classifications identified on Map 3.1: Land Use Policy Areas Map. 1110 and 1114 Gladstone Road NW are located in the 'Urban Mixed-Use' Area. 1120, 1124, and 1126 Gladstone Road NW are located in the 'Medium Density Residential' Area. A proposed amendment to Map 3.1 will change the classification of 1114 and 1120 Gladstone Road NW to the 'Medium-density Mid-rise' area.

Heritage conservation policies of the *Hillhurst/Sunnyside Area Redevelopment Plan* encourage the preservation of historic resources and are meant to provide economic incentive for preservation by allowing the transfer of density from historic sites to other properties. This proposal includes the provision of bonus density from the site of the Hillhurst Baptist Church to

Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

be applied on another property, and does not entail a net increase in density above what is currently allowed in the policy.

All sites within the Transit Oriented Development Area have been assigned a maximum density through the use of Floor Area Ratios. A minor amendment to Map 3.1 of the *Hillhurst/Sunnyside Area Redevelopment Plan* is required to facilitate the Floor Area Ratio of 5.0 allowable in Site 1 of the proposed Direct Control District. Proposed Floor Area Ratios within Site 2 of the Direct Control District are consistent with Map 3.1. The slight adjustments to the allowable maximum Floor Area Ratios do not represent a density increase, but rather would redistribute density among the subject sites. The maximum height is proposed to increase, but does not exceed the maximum height allowed elsewhere in the plan area and is consistent with nearby parcels. Other minor amendments to the policy have been proposed in order to allow greater flexibility in implementing the bonusing policy in the future.

This proposal is consistent with the intent of the policies of the *Hillhurst/Sunnyside Area Redevelopment Plan*.

Transit Oriented Development Policy Guidelines (Non-statutory, 2005)

Transit oriented development guidelines apply to development in proximity to Transit Stations and encourage higher density, walkable, mixed-use environments that optimize the use of infrastructure. This application is in alignment with the key policy objectives of the *Transit Oriented Development Policy Guidelines*, including ensuring transit supportive land uses, increasing density around Transit Stations, making station areas a “place”, and planning in context with local communities.

Social, Environmental, Economic (External)

The recommended land use district will help to implement policy goals of providing appropriate densification of the inner city and meeting Transit Oriented Development objectives.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

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CPC2018-1101
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Policy Amendment and Land Use Amendment in Hillhurst (Ward 7) at multiple properties, Gladstone Road NW, LOC2018-0114

REASON(S) FOR RECOMMENDATION(S):

The proposal is consistent with the applicable policies of the *Municipal Development Plan* and the *Hillhurst/Sunnyside Area Redevelopment Plan* as amended. The proposal meets The City's objectives and policies for preservation, protection, and adaptive reuse of historic resources through the official designation of the Hillhurst Baptist Church as a Municipal Historic Resource. The DC Direct Control District includes intensification of sites within approximately 200 metres walking distance of the Sunnyside LRT Station, and is compatible with surrounding areas which have been established by policy as future medium-density residential development within the Residential Developed – Inner City area.

ATTACHMENT(S)

1. Applicant's Submission
2. Community Association Letter
3. Proposed Amendments to the Hillhurst/Sunnyside Area Redevelopment Plan
4. Proposed Direct Control Guidelines

Applicant Submission

Applicants Submission

Introduction

Land use redesignation – **FROM** Commercial – Corridor 1 (C-COR1) and Multi-Residential - Contextual Grade Oriented (M-CG) Districts **TO** Direct Control District based on Multi-Residential – High Density Medium Rise (M-H2), Multi-Residential – Contextual Medium Profile (M-C2) and Commercial – Corridor 1 (C-COR1) Districts

The subject site is located in the community of Hillhurst/Sunnyside at 1110-1126 Gladstone Road NW between 10th and 11th St. NW. The site is comprised of five properties to be consolidated into one parcel measuring .34 hectares in total area, approximately 300 meters to the Sunnyside LRT station and adjacent to the 10 Street commercial corridor.

The development proposal for this site is to fully restore, repurpose and municipally designate the Hillhurst Baptist Church as well as create a public plaza on the current site. All other structures will be removed or demolished and replaced with two (2) multi-storey apartment style multi-residential buildings with a centre courtyard.

The need for a DC land use is required to modestly increase the overall site density and increase the allowable height on a portion of the site in order to preserve, restore, repurpose and municipally designate the Hillhurst Baptist Church. This is accomplished by the following:

- creating a comprehensive development site that would result in two new buildings with one shared parkade
- to moderately increase the overall site density from 3.3 FAR (on a total site basis) to 3.5 FAR
- to increase the allowable heights from 26 metres on site 1114 Gladstone and 16 metres on site 1120 Gladstone, to 32 metres over both of those sites.
- to modify the rules of M-H2 for 1114 and 1120 Gladstone to allow for greater site coverage in order to keep the height at or below 32 metres.
- to reduce the overall parking ratio to .55 stalls per unit (including visitor)
- to eliminate the need for a loading stall

Legislation and Policy

The subject site falls within the Transit Oriented Development Area as defined in the Hillhurst/Sunnyside Area Redevelopment Plan (ARP). This application supports the broad strategic objectives as outlined in the Visions and Guiding Principles presented in Section 2.0 including increasing housing, promoting a vibrant and vital commercial corridor, emphasize walking and creating memorable and inviting public places.

Section 3.2 of the policy states that buildings that are 8 storeys or higher should be designed with landmark features that provide reference to important destinations within the community. The

Applicant Submission

church site in its repurposed renovated format combined with the provision of publicly accessible courtyard at the front end would achieve those goals.

The proposed development respects the intentions of the policy and keeps the highest densities and heights closest to 10th Street NW. More moderate densities and heights are offered as it transitions to the low density residential community to the west of the site.

Site Development

The application consists of three different built forms.

Site 1 - 1110 Gladstone Road NW - is proposed to preserve, restore, repurpose and municipally designate the Hillhurst Baptist Church. The height and density would remain as they are, which would be significantly lower than allowed for under the ARP (5 FAR and 26m in height). The east end of the site would become a publicly accessible courtyard space. In conjunction with the repurposed church it is envisioned that this area would become a significant landmark and public gathering space in the community

Site 2 - 1114 and 1120 Gladstone Road NW - is proposed for a 32 metre Multi-Residential – High Density Medium Rise (M-H2) building. This would effectively transfer the density and height from 1110 Gladstone Road NW to 1114 and 1120 Gladstone Road NW in exchange for the municipal designation of the Hillhurst Baptist Church, thereby protecting it in perpetuity. In order to achieve the overall FAR and preserve the church the height of the building on this site would need to reach 32 metres.

Site 3 - 1124 and 1126 Gladstone Road NW - is proposed for a 16 metre Multi-Residential – Contextual Medium Profile (M-C2) building. This does not require any amendments to the policy and complies with the Area Redevelopment Plan with regards to height and density.

Urban Design

By shifting the ARP allowable massing density for this site to the west, it maintains the pedestrian scale of the church building and activates the triangular corner of the site that fronts onto 10th Street NW marking it as a focal point for the community. The unique shape of the site, the historical form of the church itself combined with renovating and re-purposing framed by higher multi-family and mixed use development will allow it to become a piece of public art in the community.

The new M-H2 building will fit within the context of the surrounding buildings that are planned in the policy. A generous landscaped amenity space between the proposed M-H2 building and the M-C2 building provides a break along the pedestrian walkway and at the intersection of 10a St and the site.

Community Association Letter



June 25, 2018

Shane Gagnon
City of Calgary File Manager
Community Planning (North Team)
The City of Calgary

Emailed to: shane.gagnon@calgary.ca

**RE: LOC2018-0114 | 1110, 1114, 1120, 1124 & 1126 Gladstone Road NW | Land Use
Amendment from Existing C-COR1 f2.8h13, M-CGd72, M-CGd72 to DC/C-COR1, DC/M-
H2, DC/MC-2 (The Site)**

Dear Mr. Shane Gagnon,

In this letter, the Hillhurst Sunnyside Planning Committee (HSPC) presents the community comments regarding the above Land Use Application made by Battistella Developments (Applicant). We use our expert knowledge of the community, within the spirit and content of the Hillhurst Sunnyside Area Redevelopment Plan (ARP).

This is an unusual application in that the Applicant proposes height and density above the ARP, intruding into the interior of the low density residential community. The Applicant presents the preservation of the Hillhurst Baptist Church (Church) as an opportunity to justify a density transfer plus additional Floor Area Ratio (FAR), when there is no clear direction from the community regarding the retention of the Church and its historical value.

Our Comments:

1. The land use and development approval process for this Site, due to its interface with the interior low-density residential community must be done with great care. We ask that The City require that the land use includes guarantees that the interface will be respectful and consistent with the current context. ARP recommendations such as streetscape, setbacks and step-backs should be reflected in the Land Use District.
2. Saving the Church is not a clear priority for the community, particularly as an equivalent trade-off resulting in pushing a significant increase in density and height into the interior of the residential community, away from the Main Streets. We have not heard from any community members in support of this density transfer at all costs. (Refer to Appendix 1 – 1. Heritage)
3. The developer's application operates under the assumption that maximum heights and densities as defined by the Transit Oriented Development (TOD) section of the ARP are a given. This TOD-justified height and density are not guaranteed entitlements. (Refer to Appendix 1 – 2. ARP Height and Density)
4. The Applicant's economic challenges should not drive a planning decision. The application states that cost of the preservation of the Church is its main justification for the increased FAR, beyond

Community Association Letter

the combined TOD-justified FAR of the entire site. However, the Applicant has stated that it will retain ownership of the Church which means that any investment is directly to their benefit. If the Applicant chooses to sell, rent or lease it will benefit accordingly. (Refer to Appendix 1 – 3. Economics)

5. The application also requests reduction in parking in TOD areas from .9 to .55 stalls per unit to increase the Applicant's financial ability to invest in the restoration of the Church. The reduction of parking solely for profitability does not consider the impact of this reduced parking on the community at large or the parking enforcement cost to The City.
6. The Applicant's illustrations show the surrounding properties in its future vision, consistent with its view of the community and not within the context of the community as it is today. We find this approach to be misleading and not consistent with how residential development applications are required to be rendered. Again, this is another example of the proposal's insensitivity regarding the context of the community to be impacted and intruded upon. (Refer to Appendix 2 – Comparative Illustrations)
7. If concessions are granted to allow the Church to be saved, we need assurances that the Church will be saved at whatever cost to the Applicant. To this point, we request to be informed regarding environmental testing on The Site, particularly due to the Church proximity to the gas station. We have a concern that the outcome of this testing may have an impact on the Church restoration. Without this work being done before Application reviewed, there is a risk that a density transfer will be awarded, and the Church will not be salvageable on a contaminated site.
8. Per the ARP Section 4.3.8 Traffic Management Monitoring, "The City shall prepare a Mobility Assessment & Plan (MAP) in consultation with the Community Association to review the transportation impacts of the intensified land use on adjacent roadways, as well as pedestrian, bicycle and transit connections." We request that this Application should not be approved or debated until this study is completed. (Refer to Appendix 1 – 4. Mobility Study)
9. We request assurances that there is future capacity within the water, sewer, flood mitigation, electrical and natural gas infrastructure and that these services as provided to the rest of the community will not be negatively affected by this and other future developments. If not done, The City will have no opportunity to require developers contribute to the costs of overall community infrastructure upgrades needed due to the continued densification of our community.

Please contact the undersigned should you have any questions.

Sincerely,

Hillhurst Sunnyside Planning Committee
Hillhurst Sunnyside Community Association

Cc: Robert McKercher, Chair, Hillhurst Sunnyside Planning Committee
Members, Hillhurst Sunnyside Planning Committee
Lisa Chong, Community Planning Coordinator, HSCA
Paul Battistella and Chris Pollen, Battistella Developments / Applicant's Team
Dale Calkins, Senior Policy & Planning Advisor, Ward 7 Councillor's Office
Circulation Control, Planning & Development, City of Calgary

Attachments:

Appendix 1 – Further Information
Appendix 2 – Comparative Illustrations
Application Notice LOC2018-0114 dated June 01, 2008

Community Association Letter

Appendix 1 – Further Information

1. Heritage

The Church is listed on the City of Calgary Inventory of Historic Resources, but it is not formally designated nor protected from future redevelopment. Within The City there is strong concern regarding the loss of heritage buildings in Calgary, which the community understands and supports. However, within the ARP, The City classified the Church under “Other Historical Sites”, “whose significance has been diminished by poor maintenance, insensitive alteration or some other factor”. While the community supports heritage preservation in general, this requires equitable and creative solutions and should not be done at all costs.

The Applicant states “The need for a DC land use is required to modestly increase the overall site density and increase the allowable height on a portion of the site in order to preserve, restore, repurpose and municipally designate the Hillhurst Baptist Church”. This is an inaccurate statement because the DC land use is required to get approval for the ARP recommended land use, regardless of the Church.

The lack of clarity and certainty regarding the Applicant’s commitment and intention regarding the Church further weakens our support of the density transfer. The Church may be sold, leased or rented. It could be a revenue source rather than an expense for the Applicant. We note that the historic designation of the Church makes the owner eligible for municipal and provincial grants.

2. ARP Height and Density

ARP Section 3.1.5, Policy 3 states “*The maximum densities Table 3.1 (or on Map 3.2) are not guaranteed entitlements. In order to achieve these maximums, projects will need to meet high standards of architectural and urban design quality that ensure projects make positive contributions to the public realm based on conformance to the design policies and guidelines of Section 3.0 of the Plan.*”

ARP Section 3.2.1, Policy 2 states “*The maximum heights shown in Table 3.2 (or on Map 3.3) are not guaranteed entitlements. In order to achieve these maximums, projects will need to meet high standards of architectural and urban design quality that ensure projects make positive contributions to the public realm.*”

The following chart is included to highlight and clarify the additional height and FAR that the Applicant is assuming is its right:

Existing Parcels	Existing Land Use	Existing ARP	Applicant’s Proposal
1110 (Church/retail)	13 metres / 2.8 FAR	26 metres / 5.0 FAR	Restoration & Historical Designation
1114 (gym)	13 metres / 2.8 FAR	26 metres / 5.0 FAR	32 metres
1120 (apartment)	12 metres / .72 FAR	16 metres / 2.5 FAR	32 metres
1124 (apartment)	12 metres / .72 FAR	16 metres / 2.5 FAR	16 metres
1126 (apartment)	12 metres / .72 FAR	16 metres / 2.5 FAR	16 metres
Average FAR	1.4 FAR	3.3 FAR	3.5 FAR (increase of .2 over ARP and 2.1 over existing)

Community Association Letter

We are not opposed to height in the right areas. The only site in the TOD Study area where the 32m height was granted is the isolated RBC Site (LOC2017-0393), a standalone parcel situated on the Main Street of 10th Street NW and away from the lower-density/lower-height residential interior of the community. The RBC parcel is considered a gateway into the Hillhurst Sunnyside community and Main Streets. Accordingly, the application to bring the zoning of this RBC parcel to the ARP recommendation was recently approved by City Council with support from the community. The Site (LOC2018-0114) does not meet these criteria.

3. Economics

The Applicant's plan (and required relaxations) to concentrate density and height in the center of the site also appear more economically efficient for the Applicant. If the Applicant's economics are challenging due to the shape of the site these challenges should be reflected in the land cost rather than in zoning concessions.

When asked about the potential to build height over the Church, the Applicant stated that it has no interest because of the cost.

4. Mobility Study

The ARP indicates that *“Upon completion of six significant redevelopments in the [TOD] study area, The City shall prepare a Mobility Assessment & Plan (MAP) in consultation with the Community Association to review the transportation impacts of the intensified land use on adjacent roadways, as well as pedestrian, bicycle and transit connections”* (Section 4.3.8).

To validate the urgent need for the City to act, the following lists the requisite six significant developments that have been completed since the TOD section of the ARP was approved:

1. Ven
2. Pixel
3. Lido
4. The Kensington
5. Ezra
6. St. Johns on Tenth

This application should not be approved until the Mobility Study is complete and we understand the impacts on congestion and safety of all the existing, proposed and potential applications. The land use taking the RBC site to 32 metres has recently been approved, without the completion of the mobility study per The City commitment. HSCA has repeatedly requested that this work be done.

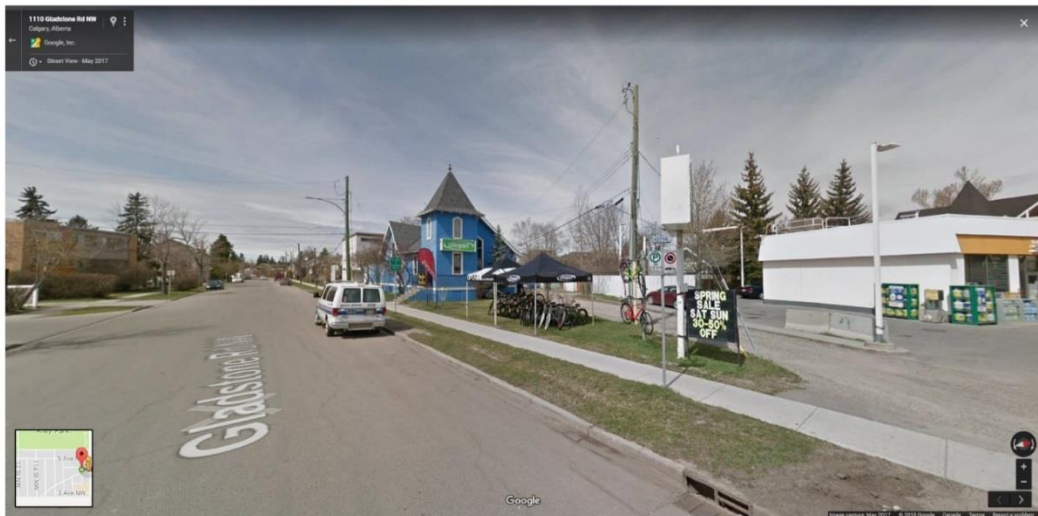
Community Association Letter

Appendix 2 – Comparative Illustrations

Applicant Rendering (from Applicant website June 23, 2018)



Google Maps Camera View (May 2017 and reflects current street view)



Proposed Amendments to the Hillhurst/Sunnyside Area Redevelopment Plan

WHEREAS it is desirable to amend the Hillhurst/Sunnyside Area Redevelopment Plan Bylaw 19P87, as amended;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26, as amended:

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

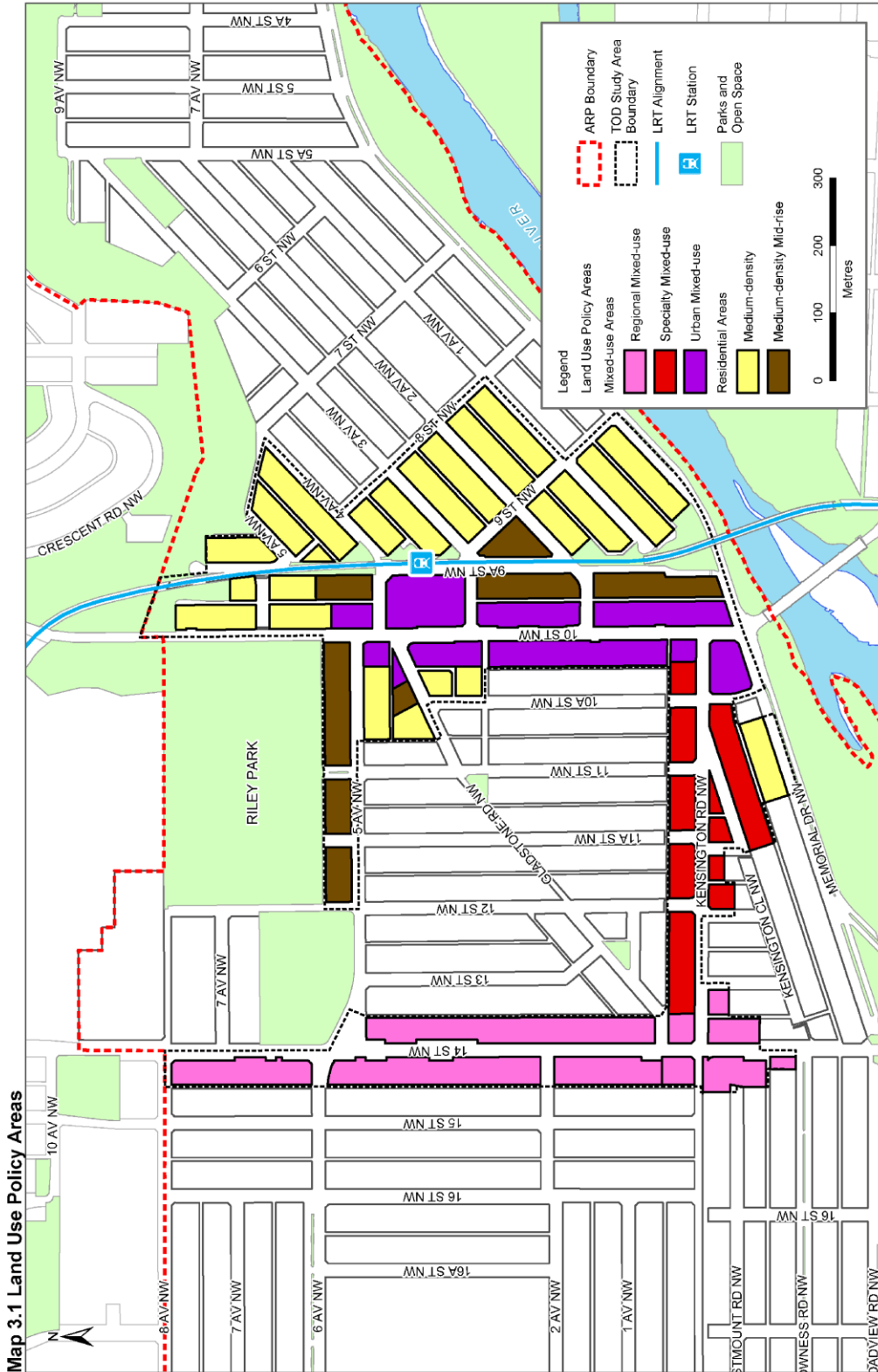
1. The Hillhurst/Sunnyside Area Redevelopment Plan attached to and forming part of Bylaw 19P87, as amended, is hereby further amended as follows:
 - (a) Delete the existing Map 3.1 entitled “Land Use Policy Areas” and replace with the revised Map 3.1 entitled “Land Use Policy Areas”, as attached as Schedule A.
 - (b) Delete the existing Map 3.2 entitled “Maximum Densities” and replace with the revised Map 3.2 entitled “Maximum Densities”, as attached as Schedule B.
 - (c) Under Section 3.1.5, Policy 4, delete ii. and replace with the following:
 - “ii. Provision of Urban Design Improvements

A developer may provide specific urban design improvements related to the Urban Design Initiatives described in Section 3.3 Urban Design Initiatives and Public Realm, identified on Map 3.4 Urban Design Initiatives or otherwise negotiated with the Development Authority. The allowable bonus floor area in square meters is equal to the total construction cost of the improvement divided by the contribution rate referred to in Section 3.1.5.4.i. Total Construction cost shall not include any construction costs necessary to fulfill the infrastructure requirements of a development permit for a development equal to or less than the base density. Specific details of design and costing shall be determined through the development permit process.”
 - (d) Delete the existing Map 3.3 entitled “Building Heights” and replace with the revised Map 3.3 entitled “Building Heights”, as attached as Schedule C.
 - (e) Under Section 3.1.5, Policy 4, delete iii. and replace with the following:
 - “iii. Designation of Historic Resources

For a density bonus a developer may designate a building(s) as a Municipal Historic Resource under the Historical Resources Act. Specific details of repair, restoration, or improvement of the historic building will be determined through a Municipal Historic Resource Designation and Compensation Agreement.”

Proposed Amendments to the Hillhurst/Sunnyside Area Redevelopment Plan

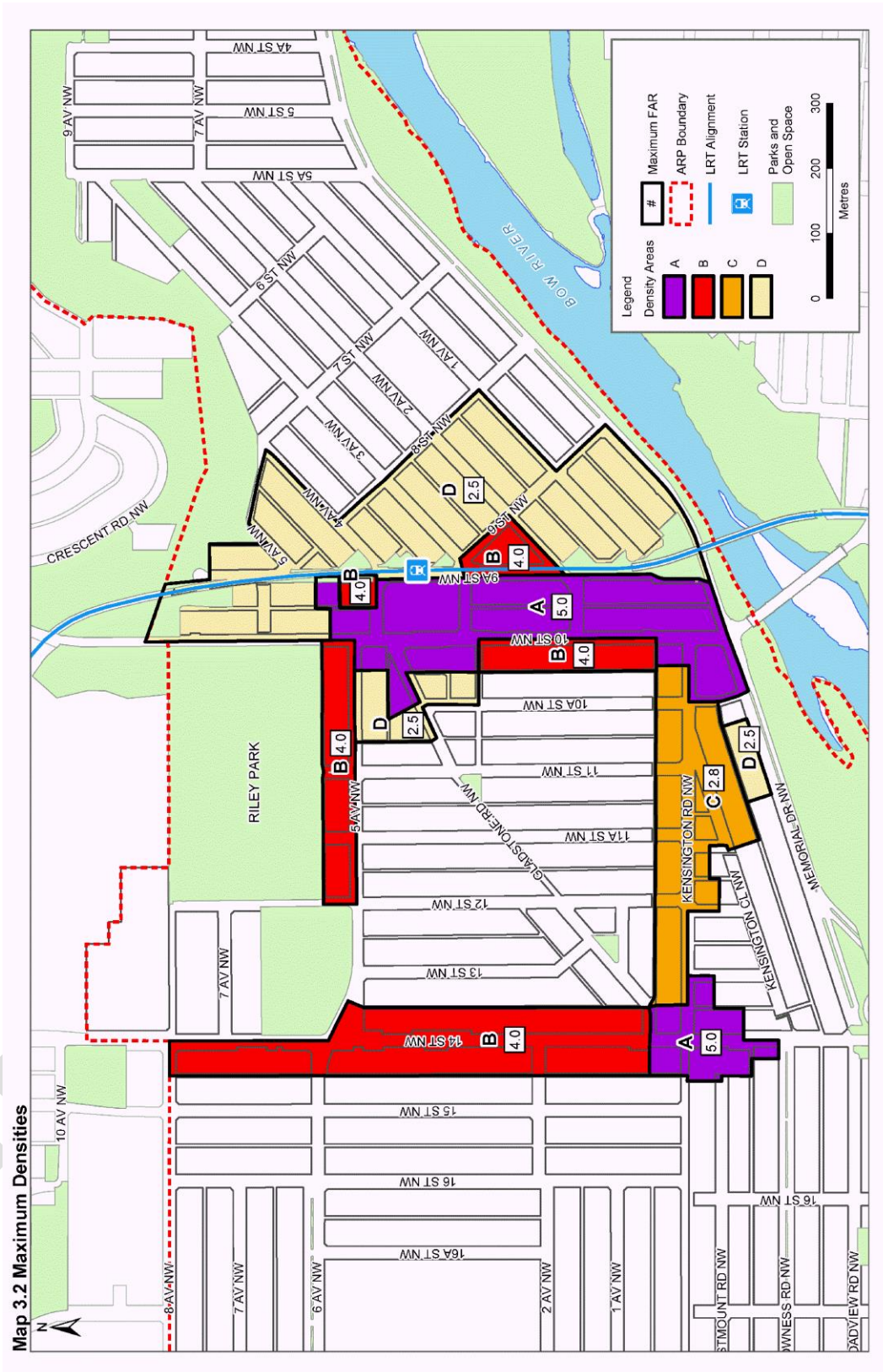
SCHEDULE A



Map 3.1 Land Use Policy Areas

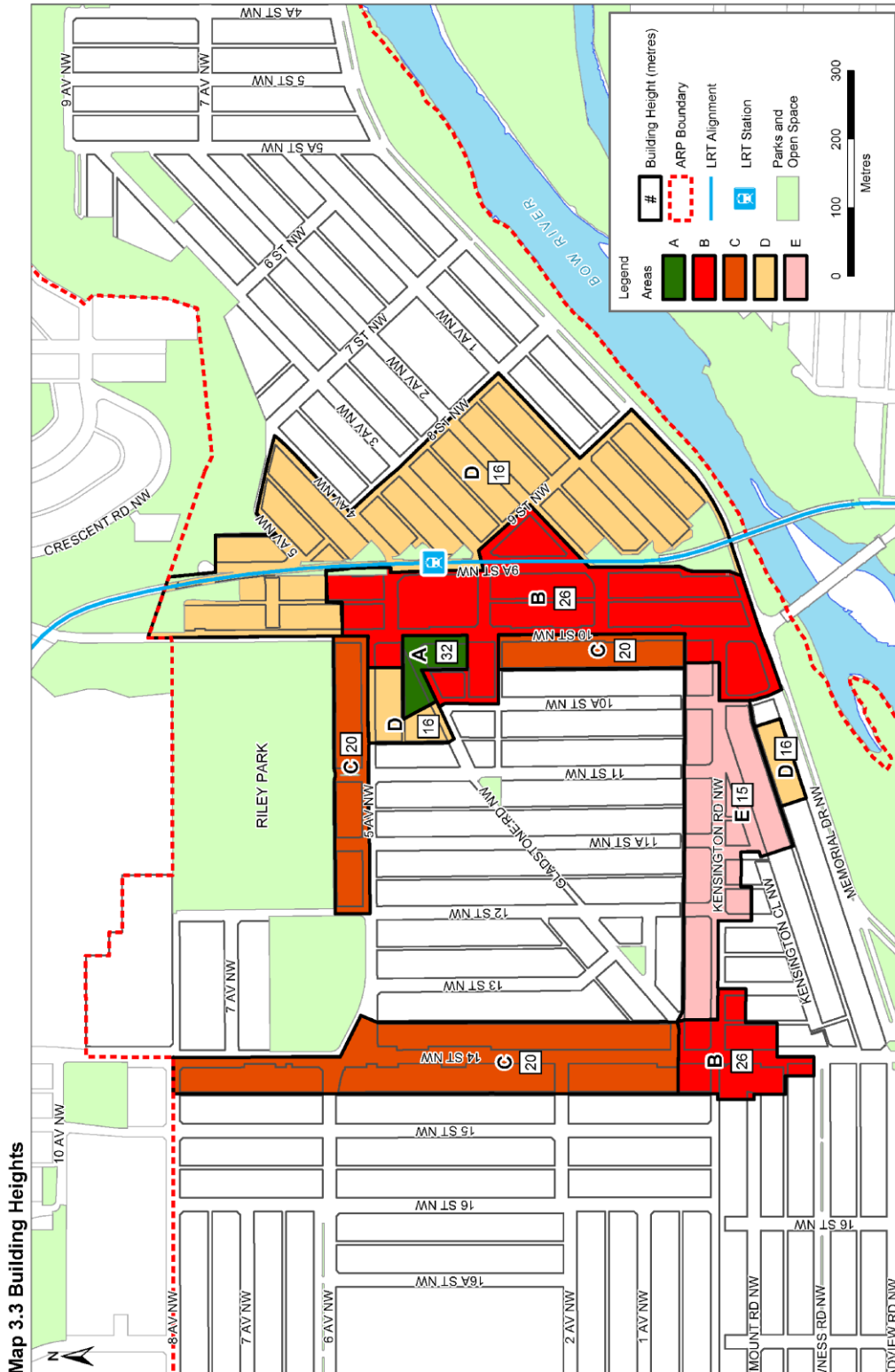
Proposed Amendments to the Hillhurst/Sunnyside Area Redevelopment Plan

SCHEDULE B



Proposed Amendments to the Hillhurst/Sunnyside Area Redevelopment Plan

SCHEDULE C



Proposed Direct Control Guidelines

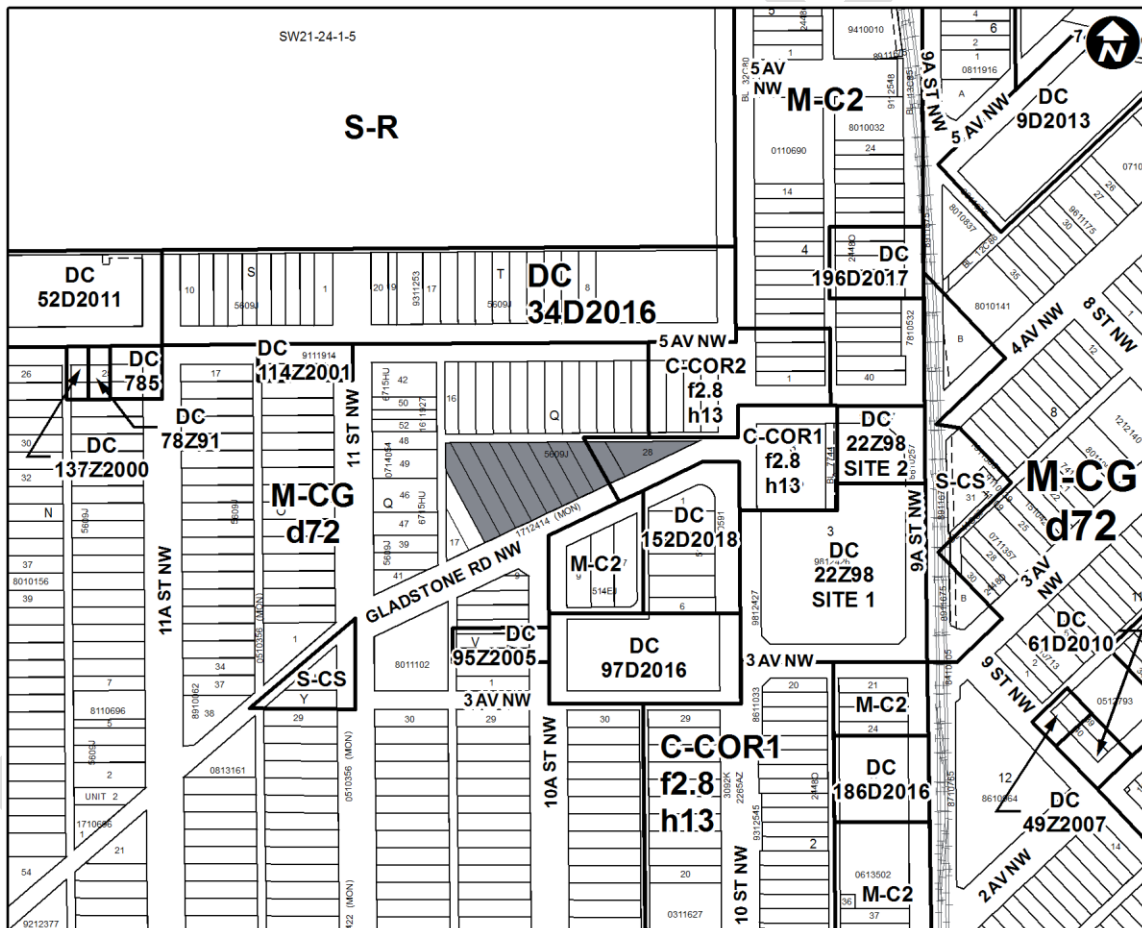
WHEREAS it is desirable to amend the Land Use Bylaw Number 1P2007 to change the land use designation of certain lands within the City of Calgary;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26 as amended;

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

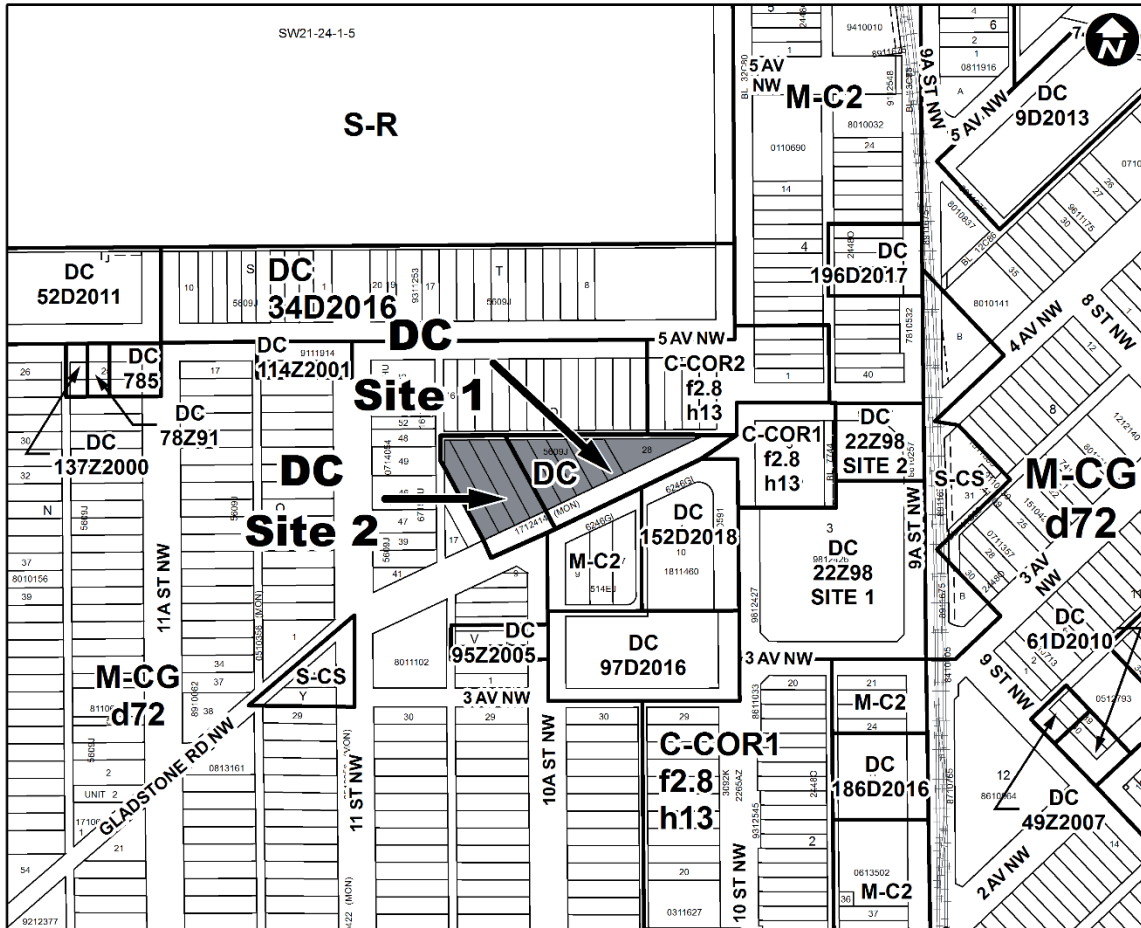
1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".

SCHEDULE A



Proposed Direct Control Guidelines

SCHEDULE B



DIRECT CONTROL DISTRICT

Purpose

- 1 This Direct Control District is intended to:
 - (a) Allow for medium and high **density** medium rise **Multi-Residential Development** opportunities taking into account the applicable local area redevelopment plan;
 - (b) Implement a site specific heritage **density** incentive for the legal protection of a **Municipal Historic Resource**; and
 - (c) Implement the **density** bonus provisions of the Hillhurst/Sunnyside Area Redevelopment Plan.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Proposed Direct Control Guidelines

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

General Definitions

- 4 In this Direct Control District:

- (a) “**Municipal Historic Resource**” means the **building** existing on the date of passage of this Direct Control District located on the **parcels** legally described as Lots 27 and 28, Block Q, Plan 5609J with the municipal address 1110 Gladstone Road NW.

Landscaped Areas

- 5 A minimum of 40.0 per cent of the area of this Direct Control District must be a **landscaped area**.

Motor Vehicle Parking Stall Requirements

- 6 (1) Unless otherwise referenced in subsection (2) the minimum **motor vehicle parking stall** requirements are as per section 558 of Bylaw 1P2007.
- (2) The minimum **motor vehicle parking stall** requirement for each **Dwelling Unit** and **Live Work Unit** may be reduced to 0.55 stalls for residential and visitor parking combined if:
- (a) A parking study is provided to the satisfaction of the **Development Authority**; and
- (b) Transportation demand management measures are approved by the **Development Authority** and are required to be implemented as a condition of a **development permit**.

Floor Area Ratio

- 7 The maximum cumulative **floor area ratio** in this Direct Control District is 3.29.

Relaxation Rule

- 8 The **Development Authority** may relax the provisions of sections 5, 6, 15, and 20 of this Direct Control District provided the relaxation test in Bylaw 1P2007 is met.

Site 1

0.18 hectares (± 0.44 acres)

Application

- 9 The provisions in sections 10 through 15 apply only to Site 1.

Bylaw 1P2007 District Rules

- 10 Unless otherwise specified, the rules of the Multi-Residential – High Density Medium Rise (M-H2) District of Bylaw 1P2007 apply in this Direct Control District.

Proposed Direct Control Guidelines

Permitted Uses

- 11 (1) Unless otherwise specified in subsection (2), the **permitted uses** of the Multi-Residential – High Density Medium Rise (M-H2) District of Bylaw 1P2007 are the **permitted uses** of this Direct Control District.
- (2) The **permitted uses** of the Commercial – Corridor 1 (C-COR1) District are the **permitted uses** in the **Municipal Historic Resource** in this Direct Control District.

Discretionary Uses

- 12 (1) Unless otherwise specified in subsection (2), the **discretionary uses** of the Multi-Residential – High Density Medium Rise (M-H2) District of Bylaw 1P2007 are the **discretionary uses** in Site 1 of this Direct Control District.
- (2) The **discretionary uses** of the Commercial – Corridor 1 (C-COR1) District are the **discretionary uses** in the **Municipal Historic Resource** in this Direct Control District with the addition of:
- (a) **Temporary Residential Sales Centre.**

Floor Area Ratio

- 13 (1) Unless otherwise referenced in subsection (2) and subject to section 7, the maximum **floor area ratio** is 2.0.
- (2) The **floor area ratio** may be increased to a maximum of 5.0 through the municipal historic resource designation by **Council** of the **Municipal Historic Resource**, and the repair, restoration, and improvement of the **Municipal Historic Resource** as approved by the **Development Authority**.

Density

- 14 There is no minimum **density** expressed in **units** per hectare. **Density** is expressed in **floor area ratio** pursuant to section 12 of this Direct Control District.

Building Height

- 15 The maximum **building height** is 32.0 metres.

Site 2

0.18 hectares (± 0.44 acres)

Application

- 16 The provisions in sections 17 through 20 apply only to Site 2.

Bylaw 1P2007 District Rules

- 17 Unless otherwise specified, the rules of the Multi-Residential – Contextual Medium Profile (M-C2) District of Bylaw 1P2007 apply in this Direct Control District.

Floor Area Ratio

- 18 (1) Unless otherwise referenced in subsection (2) and subject to section 7, the maximum **floor area ratio** is 0.72.

Proposed Direct Control Guidelines

- (2) The **floor area ratio** may be increased to a maximum of 2.5 in accordance with the **density** bonus provisions contained in section 19 of this Direct Control District.

Density Bonus

- 19 (1) For the purposes of this section: "Cash Contribution Rate" means \$17.85 per square metre for the year 2018. The Cash Contribution Rate will be adjusted annually on January 1 by the **Development Authority**, based on the Statistics Canada Consumer Price Index for Calgary.
- (2) A **density** bonus may be earned by a contribution to the Hillhurst/Sunnyside Community Amenity Fund, such that: Cash Contribution Amount = Cash Contribution Rate x total floor area in square metres above the **floor area ratio** of 0.72.
- (3) A **density** bonus may be earned by the provision of an urban design improvement in accordance with section 3.1.5.4 of the Hillhurst/Sunnyside Area Redevelopment Plan, where the allowable bonus floor area in square metres is equal to the cost of construction of the improvement divided by the Cash Contribution Rate, such that:

Allowable bonus floor area = total construction cost of the improvement / Cash Contribution Rate.

Total construction cost will not include any construction costs necessary to fulfill the infrastructure requirements of a **development permit** for a **development** equal to or less than a **floor area ratio** of 0.72. Details of the construction cost will be determined through the **development permit** process.

Building Height

- 20 The maximum **building height** is 16.0 metres.

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Calgary Planning Commission
2018 October 18

ISC: UNRESTRICTED
CPC2018-1152
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Land Use Amendment in Arbour Lake (Ward 2) at 20 Crowfoot Crescent NW, LOC2018-0133

EXECUTIVE SUMMARY

This application was submitted by Bart Carswell on 2018 June 11 on behalf of the landowner Artis Crowfoot Village Ltd. This application proposes to redesignate the subject parcel from DC Direct Control District to Commercial – Community 1 (C-C1) District to accommodate a Cannabis Store.

The proposal is in keeping with the applicable policies of the *Municipal Development Plan* and the proposed Cannabis Store use is appropriate for the Major Activity Centre context that the site is within.

A change of use development permit application for a Cannabis Store use was submitted and is currently on hold pending this application.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 2.11 hectares ± (5.22 acres ±) located at 20 Crowfoot Crescent NW (Plan 8610321, Block 6, Lot 3) from DC Direct Control District to Commercial – Community 1 (C-C1) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

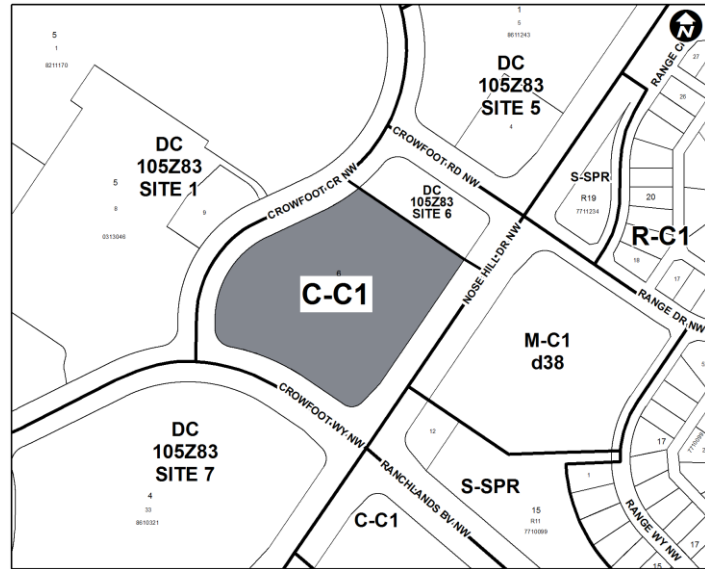
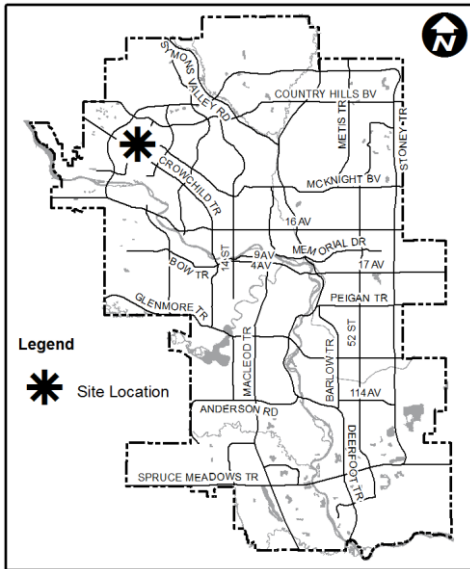
This land use amendment application was submitted to The City of Calgary by Bart Carswell on behalf of the landowner Artis Crowfoot Village Ltd. on 2018 June 11. As noted in the applicant's submission (Attachment 1), the applicant is intending to accommodate a proposed Cannabis Store use.

A change of use development permit application (DP2018-1714) for a Cannabis Store use was submitted on 2018 April 24 and is currently on hold pending this application. At this time the applicant is not considering redevelopment of this site.

On 2018 April 05, Council approved a new definition for Cannabis Store in Land Use Bylaw 1P2007. This new use is not contemplated in any pre-1P2007 Bylaw DC Districts. The existing Direct Control District is based on Land Use Bylaw 2P80 and no new uses can be added to it, as Bylaw 2P80 is not being updated.

Land Use Amendment in Arbour Lake (Ward 2) at 20 Crowfoot Crescent NW,
LOC2018-0133

Location maps



**Land Use Amendment in Arbour Lake (Ward 2) at 20 Crowfoot Crescent NW,
LOC2018-0133**

Site Context

The subject site is located northwest of the junction of Crowfoot Way NW and Nose Hill Drive NW, within the Crowfoot Crossing Major Activity Centre. Vehicular access is from Crowfoot Crescent NW and Crowfoot Way NW, and existing commercial buildings are oriented towards an internal circular drive aisle. The Cannabis Store use is proposed within an existing commercial bay.

To the north, west, and south are commercial developments that are part of the existing DC Direct Control District. To the east across Nose Hill Drive NW are multi-residential townhouse style developments, designated as Multi-Residential – Contextual Low Profile (M-C1) District. The subject site is located within approximately 1 kilometer walking distance of Crowfoot LRT Station.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal is to allow for a Cannabis Store in the existing commercial development. The Commercial – Community 1 (C-C1) District has been proposed by the applicant as it is most closely aligned with the current DC Direct Control District's C-1A Local Commercial District base, and best reflects the existing site conditions, context, and intent.

Planning Considerations

The primary planning consideration in evaluation of this application constituted determining the most appropriate new Land Use District. The proposed district is considered to be in alignment with the existing site conditions and policy context.

Land Use

This application is to redesignate the site from the existing DC Direct Control District (Bylaw 105Z83). The existing 1983 DC Direct Control District is based on Land Use Bylaw 2P80 C-1A Local Commercial District. The DC sets minimum yard (setback) dimensions of 6 metres, and stipulates that a development permit is required for development of the site.

The proposed Commercial – Community 1 (C-C1) District is the closest equivalent to the Land Use Bylaw 2P80 C-1A base district of the current DC Direct Control District, reflects the existing site conditions, and will allow for the Cannabis Store use. The Commercial – Community 1 (C-C1) District is intended to be characterized by small to mid-scale commercial developments on sites less than 3.2 hectares within a community or along a commercial street. The proposed redesignation would not create any non-conforming uses or buildings.

Development and Site Design

A discretionary change of use development permit application for a Cannabis Store at the subject site is currently on hold, subject to Council's decision on this land use redesignation. No redevelopment of the site is being contemplated at this time.

**Land Use Amendment in Arbour Lake (Ward 2) at 20 Crowfoot Crescent NW,
LOC2018-0133**

Environmental

There are no noteworthy environmental features on this site, and an Environmental Site Assessment was not required for this application.

Transportation

A Transportation Impact Assessment (TIA) and parking study were not required as part of this land use amendment. At the development permit stage, site access, parking, and loading are intended to continue as per the existing site conditions. The subject site is located approximately 850 metres from the Crowfoot LRT Station. A transit stop served by routes 40 and 113 is located directly adjacent to the site on Crowfoot Crescent NW.

Utilities and Servicing

Water, sanitary, and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application has been advertised online. No public meetings were held by the applicant or Administration.

The Arbour Lake Community Association did not provide comments on this application. Administration received four letters in response to the notice posting. The objections are summarized as follows:

- The shopping centre is 'family friendly' and not an appropriate location for a Cannabis Store;
- Concern about second hand smoke;
- Concern about the social and health impacts of cannabis use; and
- General unspecified objection.

Following Calgary Planning Commission, notifications for the Public Hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns as this application proposes a more flexible land use designation in a built out

**Land Use Amendment in Arbour Lake (Ward 2) at 20 Crowfoot Crescent NW,
 LOC2018-0133**

Major Activity Centre that serves as a commercial focal point for the adjacent communities and for residents living in northwest Calgary and the region.

Municipal Development Plan (Statutory, 2009)

The site is located in the 'Developed - Established' area as identified on Map 1: Urban Structure Map in the *Municipal Development Plan* (MDP). The proposed redesignation would allow the maintenance of local commercial development that provides retail and service uses in close proximity to residents and City transit infrastructure in keeping with MDP policy. The subject site is located within a Major Activity Centre, which are strategically located mixed-use destinations with broad ranges of land uses, central to a larger residential or business catchment area and along one or more Primary Transit Network routes. This proposal is consistent with the policies of the *Municipal Development Plan*.

Social, Environmental, Economic (External)

The recommended land use district will help to implement policy goals of providing more complete communities with a variety of shops and services that meet daily needs. The proposed district allows for an appropriate range of uses, supporting a variety of neighbourhood shops and services and increasing the market viability of the commercial space.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

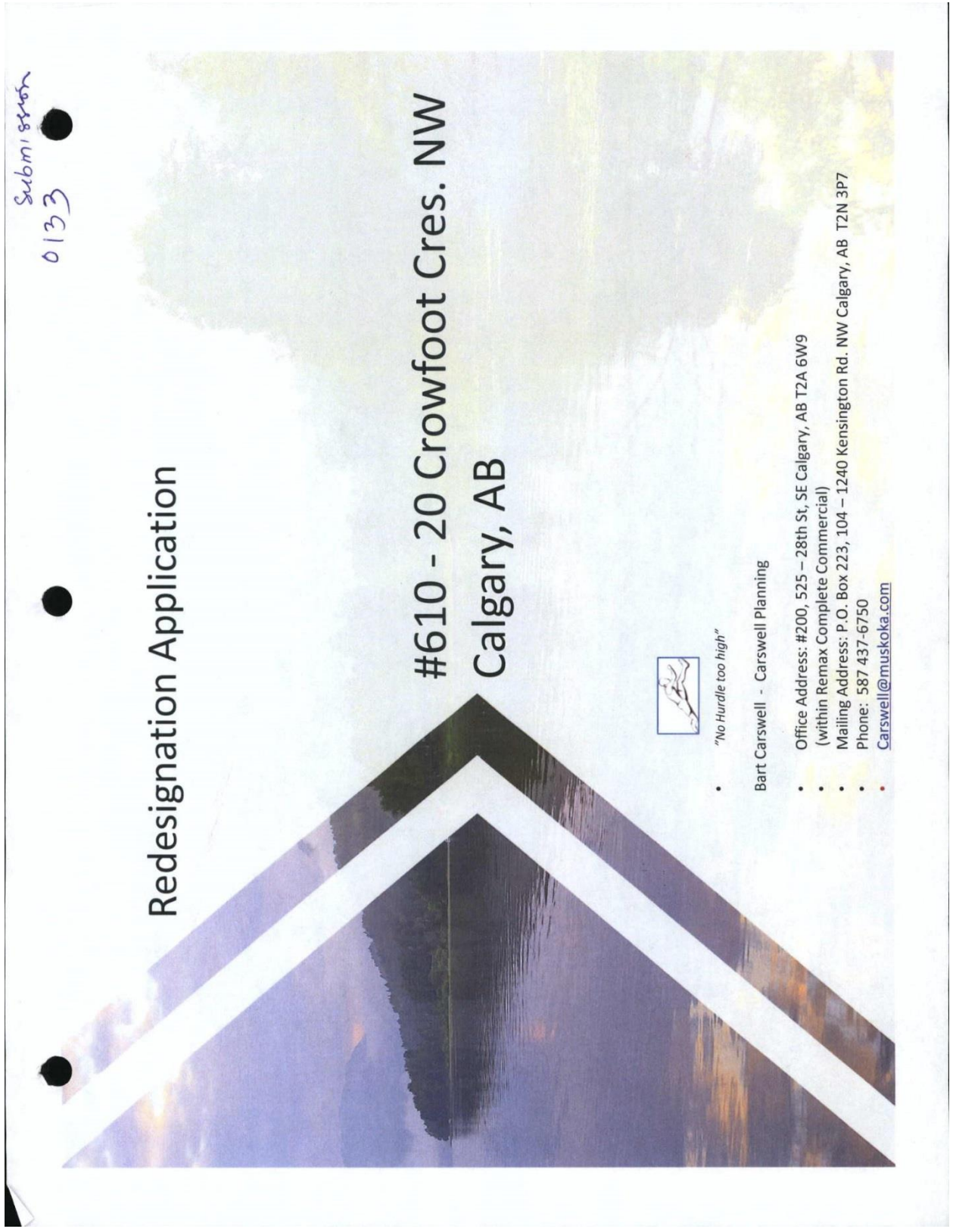
REASON(S) FOR RECOMMENDATION(S):

The redesignation will bring the subject site into alignment with Land Use Bylaw 1P2007 and allow a broader range of uses in response to changing community needs. The proposal is consistent with the applicable policies of the *Municipal Development Plan* and will support the future viability of a Major Activity Centre.

ATTACHMENT(S)

1. Applicant's Submission

Applicant Submission



Submission
0133

Redesignation Application

#610 - 20 Crowfoot Cres. NW
Calgary, AB



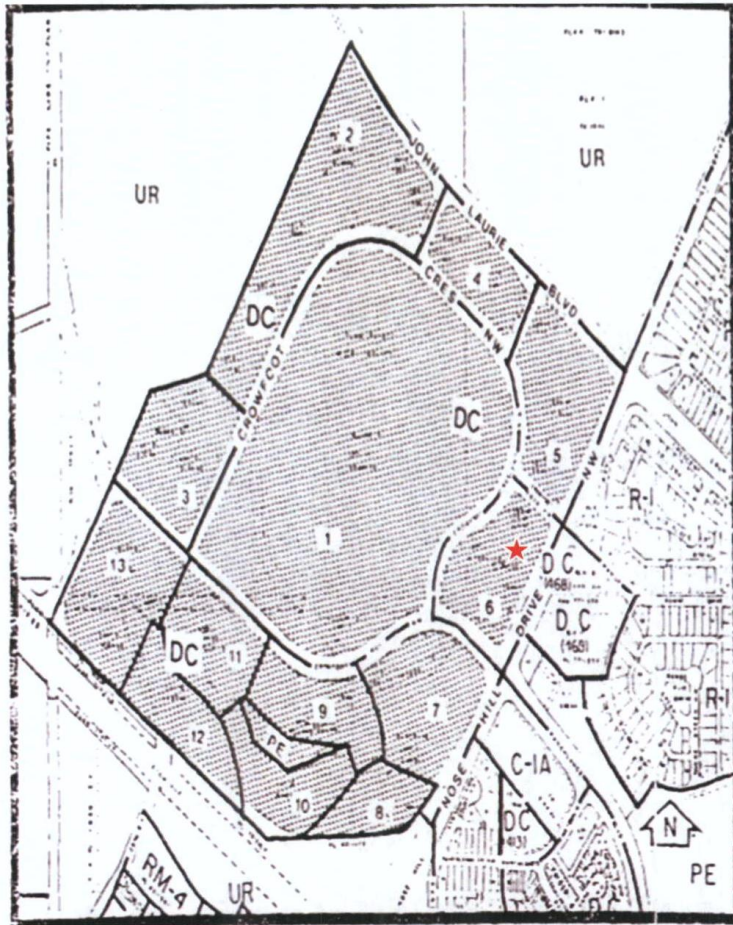
"No Hurdle too high"

Bart Carswell - Carswell Planning

- Office Address: #200, 525 - 28th St, SE Calgary, AB T2A 6W9
(within Remax Complete Commercial)
- Mailing Address: P.O. Box 223, 104 - 1240 Kensington Rd. NW Calgary, AB T2N 3P7
- Phone: 587 437-6750
- Carswell@muskoka.com

Applicant Submission

DC 105Z83, Schedule B



Applicant Submission



"Carswell Planning, "No hurdle too high."

April 20, 2018

Planning Justification Brief

Cannabis Store

610, 20 Crowfoot Crescent NW, Calgary AB

Carswell Planning, on behalf of *Canna Cabana Inc.*, is seeking approval for an amendment to the DC bylaw which would allow for the discretionary use of operating a cannabis store. The lease is for a unit in the Crowfoot Centre located near the corner of Crowfoot Cr NW and Nose Hill Dr NW. *Canna Cabana Inc.* is associated with *RGR Canada Inc.* and owner Harkirat (Raj) Grover, as confirmed by the lease and title provided. Other matters related to the site have been dealt with by the developer.

It was zoned DC 105Z83, Site 6 by Amendment No. 83/045 to the Land Use Bylaw Sept. 6, 1983. Specifically, the Bylaw states that discretionary uses for the C-1A District apply.

"SITE 6 (6.2 acres±)

1. Land Use

The land use shall be the permitted and discretionary uses for the C-1A (Local Commercial District) with the addition of drinking establishments.

2. Development Guidelines

The General Rules for Commercial Districts contained in Section 33 of By-law 2P80 and the Permitted and Discretionary Use Rules of the C-1 (Local Commercial District) shall apply unless otherwise noted below.

a) Yards

Minimum yard dimensions shall be 6 metres (20 feet).

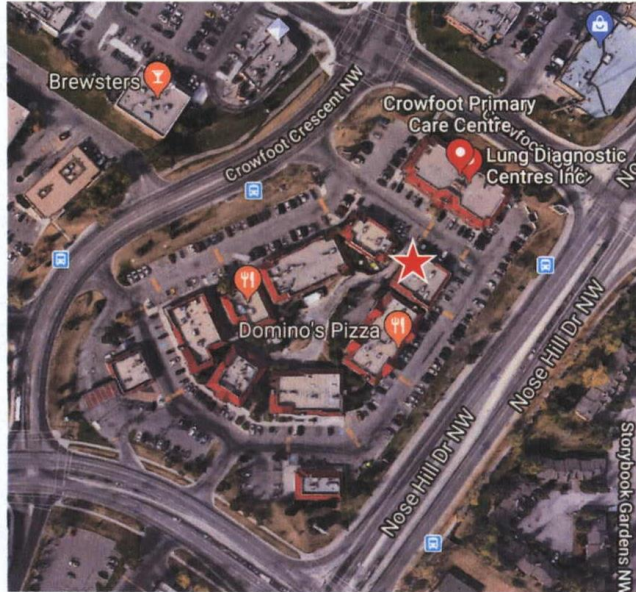
b) Development Plans

Approval of this application does not constitute approval of a development permit. Comprehensive plans, including building design, site layout, exterior finishes and colour, landscaping, parking and accesses shall subsequently be submitted to the Approving Authorities as part of a development permit application."

Applicant Submission

Stores at Site 6 are commercial in nature and would be in keeping with the rationale behind locating cannabis stores.

Aerial Image of 610, 20 Crowfoot Cr NW



Land Use Districts

On the Calgary.ca FAQs site, the question was asked,

“Can I open a cannabis store in a mall (does it have to be a stand-alone space)?”

Cannabis stores can be located within a parcel that meets the land use requirements and the setback distances. A Change of Use Development Permit (DP) application is required for an existing space. When listed in a Land Use District, Cannabis Store will be a discretionary use and will be managed through the development permit process, including provisions for notice posting and opportunity for appeals. Applicants should make themselves familiar with the Development Permit process. You should check Our analysis of separation distances show that the site would meet the requirements for a cannabis store.” This site is in a mall.

Applicant Submission

On the Calgary.ca Development Rules site for Land Use Districts, the following were identified where a cannabis store may be located,

District	Use
<ul style="list-style-type: none">• C-N1• C-N2• C-C1• C-C2• C-COR1• C-COR2• C-COR3• C-R1• C-R2• C-R3• I-G*	<ul style="list-style-type: none">• I-E• I-C• CC-X• CC-COR• CR20-C20/R20• CC-ET• CC-EIR• CC-EMU• CC-EPR• MU-1• MU-2 <p>A <u>cannabis store</u> may be located in this district.</p> <p>* Only when in conjunction with a cannabis facility.</p>

Note that C-C1 is included. Let me reiterate that the Land Use Bylaw recognizes Site 6 where, "The land use shall be the permitted and discretionary uses for the C-1A (Local Commercial District) with the addition of drinking establishments."

By this logic, cannabis stores should be considered at this location as having the land use which supports it.

Should you have any questions and wish clarification, please do not hesitate to contact me.

Sincerely,



Bart Carswell, RPP
Carswell Planning
Suite 223, 104 – 1240 Kensington Rd. NW
Calgary, Alberta, T2N 3P7
p. (587) 437-6750
e. carswell@muskoka.com

Land Use Amendment in West Hillhurst (Ward 7) at multiple properties, LOC2018-0159

EXECUTIVE SUMMARY

This application was submitted by Mediated Solutions on 2018 July 10 on behalf of the landowner Kensington Village Holdings Ltd. This application proposes to redesignate the subject parcel from a DC Direct Control District to Mixed Use – General (MU-1f2.0h10) District to accommodate a Cannabis Store.

The proposal is in keeping with the applicable policies of the *Municipal Development Plan* and will support the viability of local commercial development in close proximity to residents and City transit infrastructure.

A change of use development permit application for a Cannabis Store use was submitted and is currently on hold pending this application.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.18 hectares ± (0.44 acres ±) located at 106 and 110 - 19 Street NW and 1982 Kensington Road NW (Plan 8942GB, Block 19, Lots 13, 14, and 15) from DC Direct Control District to Mixed Use – General (MU-1f2.0h10) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

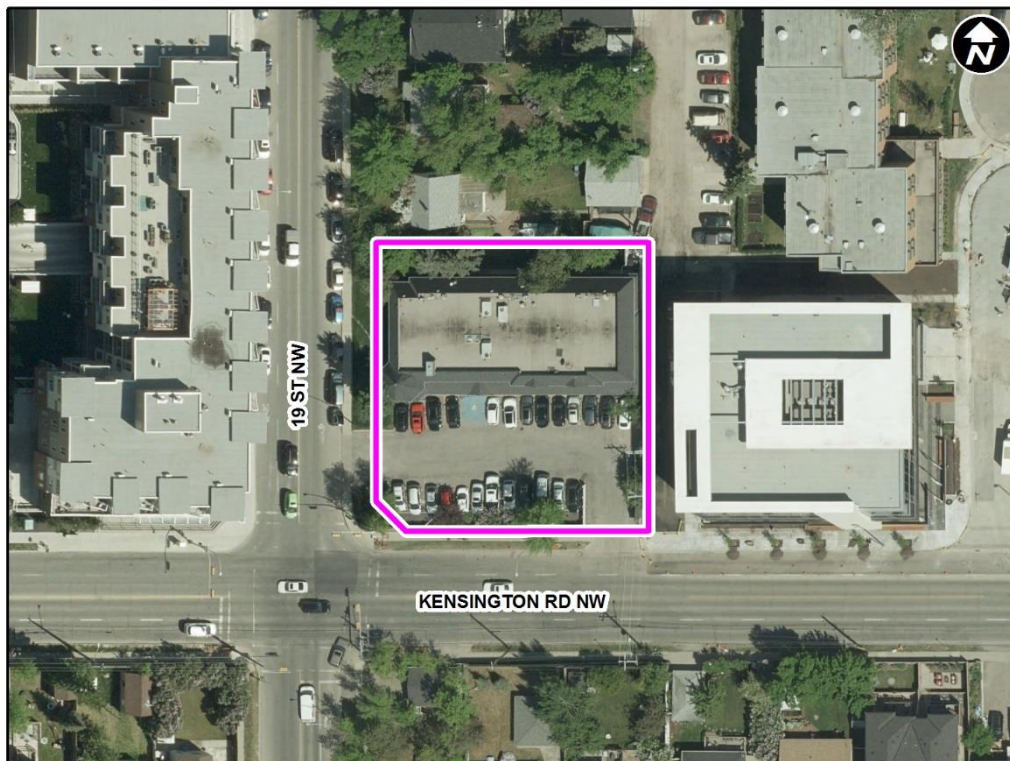
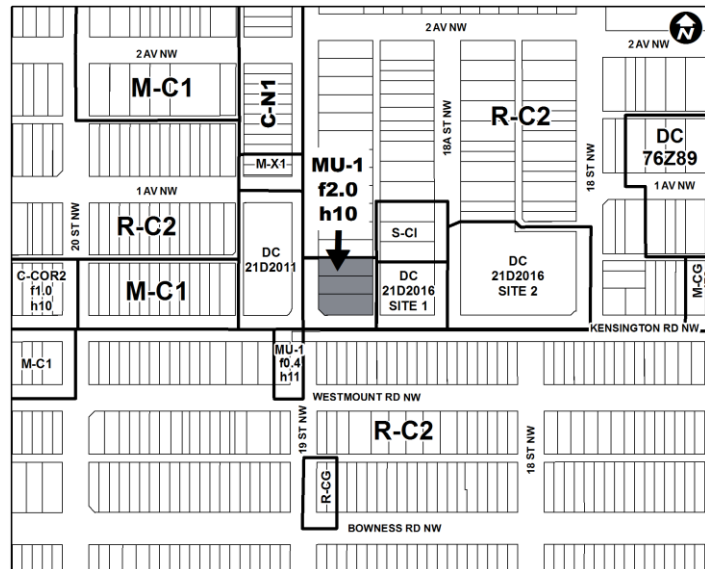
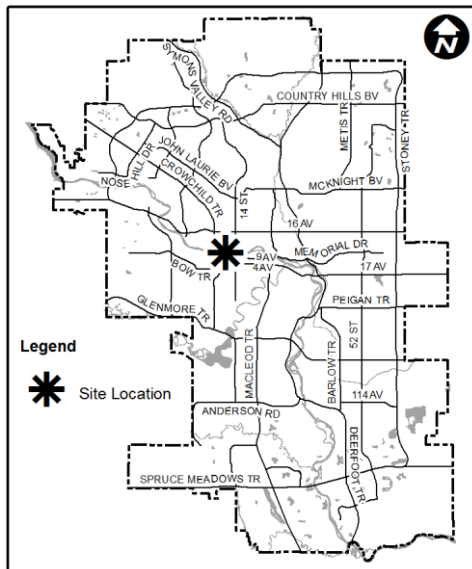
This land use amendment application was submitted to The City by Mediated Solutions Ltd. on behalf of the landowner Kensington Village Holdings Ltd. on 2018 July 10. As noted in the applicant's submission (Attachment 1), the applicant is intending to accommodate a proposed Cannabis Store use.

A change of use development permit application (DP2018-3500) for a Cannabis Store use was submitted on 2018 July 20 and is currently on hold pending this application. At this time the applicant is not considering redevelopment of this site.

On 2018 April 05, Council approved a new definition for Cannabis Store in Land Use Bylaw 1P2007. This new use is not contemplated in any pre-1P2007 Bylaw DC Districts. The existing Direct Control District is based on Land Use Bylaw 2P80 and no new uses can be added to it, as Bylaw 2P80 is not being updated.

Land Use Amendment in West Hillhurst (Ward 7) at multiple properties, LOC2018-0159

Location Maps



Land Use Amendment in West Hillhurst (Ward 7) at multiple properties, LOC2018-0159

Site Context

The subject site is located along Kensington Road NW in the northwest community of West Hillhurst. To the north of the site are low density residential uses. To the west across 19 Street NW is a mixed-use multi-residential building with commercial uses at grade. To the south across Kensington Road NW are low density residential uses. To the east is a mixed use commercial and office development.

Kensington Road NW is a Main Street and a well-established shopping and business destination for local residents and visitors. Local engagement was conducted in 2014-2015 and the proposal is aligned with preferences expressed by residents for small cohesive commercial areas and redevelopment along the north side of the street.

The site is currently developed with an auto-oriented strip mall with parking in front of the building. Local commercial uses are located at grade and offices are located on the second floor. The building was constructed in the early 1990s. It remains in good condition and no redevelopment is contemplated in the foreseeable future. Vehicular access is from Kensington Road NW and 19 Street NW. The site is located along a Primary Transit Network Bus Rapid Transit route, with a transit stop located within approximately 60 metres walking distance.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal is to allow for a Cannabis Store in the existing commercial development. The Mixed Use – General (MU-1f2.0h10) District has been proposed by the applicant in order to accommodate the desired use at present, and to allow for potential future redevelopment of the subject site that is appropriate for the mixed-use context of the immediate area.

Planning Considerations

The primary planning consideration of this application consisted of determining the most appropriate land use district. The applicant originally proposed the Commercial – Neighbourhood 1 (C-N1) District; however, this is not considered to be an optimal district for the subject site nor to be in alignment with the Main Streets engagement undertaken with local residents. A range of districts were discussed with the applicant and the Mixed Use – General (MU-1) District was selected to provide maximum flexibility in a future redevelopment scenario.

Land Use

This application is to redesignate the site from the existing DC Direct Control District (Bylaw 21Z91). The existing 1991 DC Direct Control District is based on Land Use Bylaw 2P80 C-1 Local Commercial District. The Direct Control District sets customized rules for setbacks, floor area, parking, landscaping, and lighting.

The proposed Mixed Use – General (MU-1f2.0h10) District will allow for the Cannabis Store use, and establishes the most appropriate land use District for the site along a Main Street, and its immediate context. The floor area ratio and height modifiers are based on the approximate site conditions at this time.

Land Use Amendment in West Hillhurst (Ward 7) at multiple properties, LOC2018-0159

Although suggested by Administration, increases to the density and height modifiers are not proposed at this time, as the applicant is not considering redevelopment in the foreseeable future, and is not prepared to engage in discussions about the potential intensification of the site. Utilization of the Mixed Use – General (MU-1f2.0h10) District does, however, establish a land use district which is far more flexible than the existing DC Direct Control District, and establishes a more appropriate land use district to base future modifier increases on. It is expected that in a future redevelopment scenario the modifiers will be increased as appropriate based on a range of factors including market conditions, local engagement, technical studies as necessary, and policy framework

Development and Site Design

A discretionary change of use development permit application for a Cannabis Store at the subject site is currently on hold, subject to Council's decision on this land use redesignation. No redevelopment of the site is being contemplated at this time.

Environmental

There are no noteworthy environmental conditions on the subject site, and an Environmental Site Assessment was not required for this application.

Transportation

A Transportation Impact Assessment (TIA) and parking study were not required as part of this land use amendment. At the development permit stage, site access, parking, and loading are intended to continue as per the existing site conditions. A transit stop served by Bus Rapid Transit is located approximately 60 metres from the subject site on Kensington Road NW.

Utilities and Servicing

Water, sanitary, and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application has been advertised online. No public meetings were held by the applicant or Administration.

The West Hillhurst Community Association submitted comments (Attachment 2) and has no objection to redesignation of the subject site to allow a Cannabis Store. The Community Association expressed hope that future redevelopment will take a more appropriate form. In separate correspondence the Community Association indicated that it does not support increases of allowable height and density on the site until appropriate public engagement has taken place.

Land Use Amendment in West Hillhurst (Ward 7) at multiple properties, LOC2018-0159

No public comments were received at the time of report submission.

Following Calgary Planning Commission, notifications for the Public Hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns as this application proposes a more flexible land use designation along a Main Street that serves as a commercial destination for local residents and visitors.

Municipal Development Plan (Statutory, 2009)

The site is located in the 'Inner City' as identified on Map 1: Urban Structure Map in the *Municipal Development Plan (MDP)*. The proposed redesignation would allow the maintenance of local commercial development that provides retail and service uses in close proximity to residents and City transit infrastructure in keeping with MDP policy. The subject site is located on the Kensington Road NW Main Street, which is identified as a location for urban intensification along the Primary Transit Network. Main Streets are intended to incorporate a greater share of new growth in form of compact, mixed-use, high-quality urban developments, optimizing use of existing transit and other infrastructure. The proposal is in alignment with the applicable policies of the *Municipal Development Plan*.

There is no local area plan for the community of West Hillhurst.

Social, Environmental, Economic (External)

The recommended land use district will help to implement policy goals of providing more complete communities with a variety of shops and services that meet daily needs. The district will allow for an appropriate range of uses, supporting a variety of neighbourhood shops and services and increasing the market viability of the commercial space.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Planning & Development Report to
Calgary Planning Commission
2018 October 18

ISC: UNRESTRICTED
CPC2018-1153
Page 6 of 6

Land Use Amendment in West Hillhurst (Ward 7) at multiple properties, LOC2018-0159

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The redesignation will bring the subject site into alignment with Land Use Bylaw 1P2007 and allow a broader range of uses in response to changing community needs. The proposal is consistent with the applicable policies of the *Municipal Development Plan* and will support the future viability of the Kensington Road NW Main Street.

ATTACHMENT(S)

1. Applicant's Submission
2. West Hillhurst Community Association's Comments

Applicant Submission

APPLICANTS SUBMISSION 1982 Kensington Rd. NW

We propose that the existing Direct Control (Bylaw No. 1991z21) for 1982 Kensington Rd. NW be amended to **Mixed - Use General (MU-1f2.0h10)**

The existing DC does not allow 'Cannabis Store', our proposed use, on the site. The redesignation would allow a cannabis store as a discretionary use.

After discussion with the City and landowners we conclude that the MU-1 f2.0h10 would best meet the landowners/applicants' goals for the site and would reflect the City's planning objectives for the Kensington Rd. major corridor.

MU1 designation better reflects the future planning intention for Kensington Rd as enunciated by the Main Streets study. It will allow a building height similar to the adjacent commercial office building fronting on Kensington Rd. and will require retail at-grade to create a future pedestrian amenity and the desired character along Kensington Rd. NW.

The proposed MU zoning will include certain uses as discretionary uses that the existing DC does not include; however, given that the site is currently fully developed there is no expectation that redevelopment will occur within the foreseeable future which could impact the community. In addition, the allowable discretionary MU uses will be subject to the development permit process.

The West Hillhurst Planning Committee has no objections to the cannabis store use on this site or the rezoning of the site for future pedestrian oriented mixed-use corridor development.

Thank you for your consideration.

Community Association Comments

Hello Shane,

The WHCA reviewed LOC2018-0159 and offers the following comment:

The WHCA Planning Committee has no objections to the re-zoning of this location to allow for a Cannabis Store. It is our understanding that the application may be changed from C-N1 to MU with height and density restrictions consistent with the existing building. As the building will remain the same, we are not concerned with the designation at this time. We do, however, sincerely hope that future redevelopment will be more appropriate for this prominent corner of our neighbourhood than the current development is.

Thank you,

Karen Dahl
Planning Committee Coordinator
West Hillhurst Community Association
planning@westhillhurst.com

**Land Use Amendment in Temple (Ward 10) at 3 Templemont Way NE,
 LOC2018-0132**

EXECUTIVE SUMMARY

This application was submitted by Sarbjit Singh Virdi on 2018 June 11 on behalf of the landowners Sarbjit Singh Virdi and Gurmohan Singh Braich. The application proposes to change the designation of this property from Residential – Contextual One Dwelling (R-C1) District to Residential – Contextual One / Two Dwelling (R-C2) District to allow for:

- contextual semi-detached dwellings; as well duplex dwellings and single detached dwellings;
- a maximum building height of 10 metres (no change from R-C1 district);
- a maximum of one main residential building per parcel (no change from R-C1 district); and
- the uses listed in the Residential - Contextual One / Two Dwelling (R-C2) District.

The proposal is in keeping with applicable policies of the *Municipal Development Plan*. As noted in the Applicant's Submission (Attachment 1), the applicant intends to pursue a development permit application for a semi-detached development in the future.

No development permit has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommends that Council a hold Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares ± (0.12 acres ±) located at 3 Templemont Way NE (Plan 7910737, Block 44, Lot 96) from Residential – Contextual One Dwelling (R-C1) District to Residential – Contextual One / Two Dwelling (R-C2) District; and
2. Give three reading to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

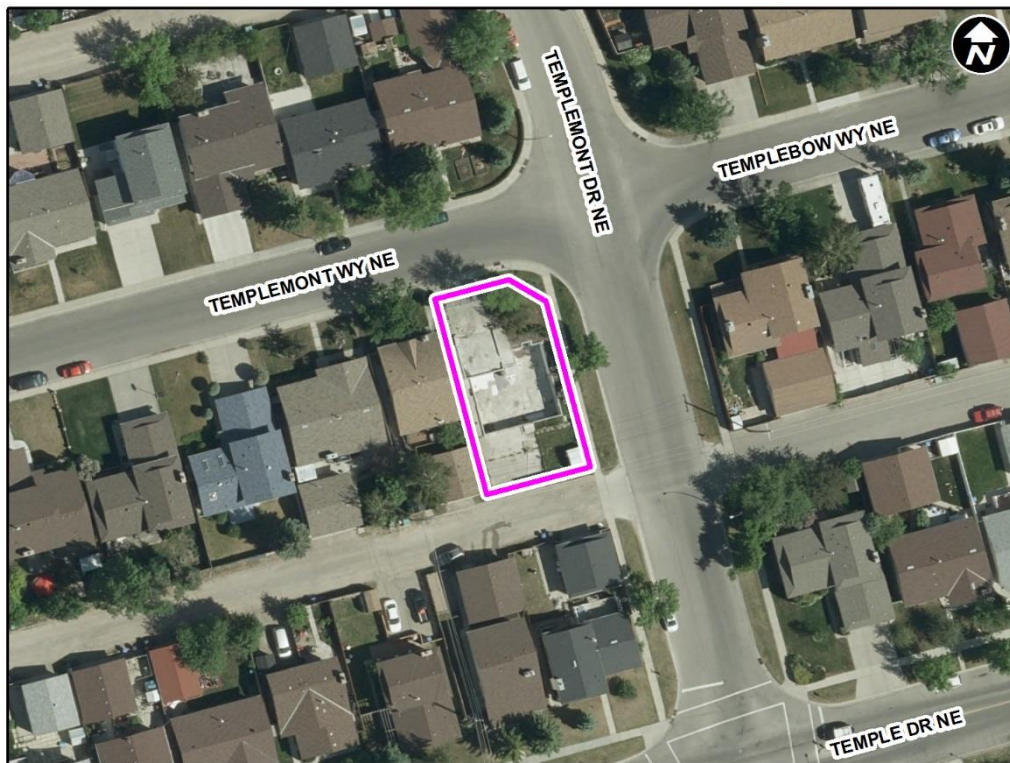
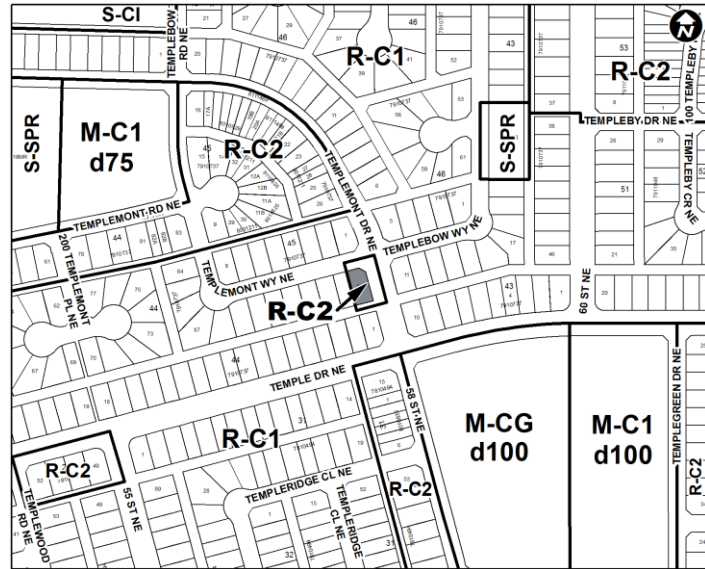
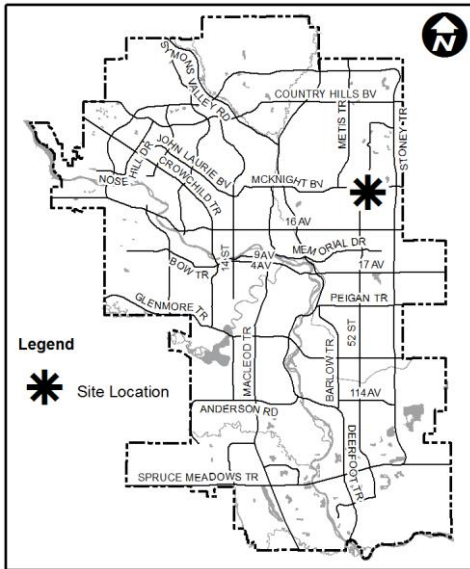
BACKGROUND

This application was submitted by Sarbjit Singh Virdi on 2018 June 11 on behalf of the landowners Sarbjit Singh Virdi and Gurmohan Singh Braich. As noted in the Applicant's Submission (Attachment 1), the landowner is intending to build a semi-detached development.

A development permit was approved in 2014 for a single detached dwelling but lapsed in 2017. The site was discovered to be a grow-op so an environmental restoration permit was also applied for; however, it was later cancelled as the owner decided to demolish the site instead. The site is currently vacant and the building demolished.

Land Use Amendment in Temple (Ward 10) at 3 Templemont Way NE, LOC2018-0132

Location Maps



Land Use Amendment in Temple (Ward 10) at 3 Templemont Way NE, LOC2018-0132

Site Context

The subject site is located in the northeast community of Temple, at the southwest corner of Templemont Way NE and Templemont Drive NE. The surrounding development consists of low-density residential to the north, west and south of the site. A multi-residential development exists one block to the south of the site. The parcel is approximately 16 metres by 30 metres in size. The site is currently vacant and the building has been demolished. The site has front drive access from Templemont Way NE as well as a rear lane from Templemont Drive NE.

As identified in *Figure 1*, Temple has experienced a population decrease from its peak in 1998.

Figure 1: Community Peak Population

Temple	
Peak Population Year	1998
Peak Population	12,059
2017 Current Population	11,162
Difference in Population (Number)	-897
Difference in Population (Percent)	-7%

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Temple](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for a range of building types that have the ability to be compatible with the established building form of the existing neighbourhood.

Planning Considerations

Land Use

The existing Residential – Contextual One Dwelling (R-C1) District is a residential designation in developed areas that is primarily for single detached homes. The current R-C1 district allows for a maximum building height of 10 metres and a maximum of one dwelling unit.

The proposed Residential – Contextual One/Two Dwelling (R-C2) District is a residential designation in developed areas that is primarily for single detached, semi-detached and duplex homes. Single detached homes may include a secondary suite. The R-C2 District allows for a maximum building height of 10 metres and a maximum of two dwelling units.

Development and Site Design

Building design, number of units and site layout details such as parking, landscaping and site access will be evaluated at development permit stage.

Land Use Amendment in Temple (Ward 10) at 3 Templemont Way NE, LOC2018-0132

Given the specific context of this corner site, additional items that will be considered through the development permit process include, but are not limited to:

- ensuring an engaging facade along Templemont Way NE and Templemont Drive NE
- emphasizing individual at-grade entrances;
- minimizing vehicle interaction with pedestrians by utilizing the rear lane; and
- applying an appropriate front setback.

Environmental

An Environmental Site Assessment was not required for this application.

Transportation

The subject site is located on the corner of Templemont Way NE and Templemont Drive NE. Both are classified as a residential street. The site is approximately 65 metres from a bus stop on Temple Drive NE. Vehicular access is available from the existing rear lane as well as front driveway. A Traffic Impact Assessment was not required as part of this application.

Utilities and Servicing

Water, sanitary and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. Individual servicing connections as well as appropriate stormwater management will be considered and reviewed at development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised on-line.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

The Temple Community Association was circulated as part of this application and no response was received. A second request was sent to the Community Association with no response received at the time of this report.

A petition opposing the land use redesignation was received from neighbours with a total of 14 signatures. The petition identified the following 3 reasons for opposing the application: decrease in neighbouring property value, further constraints to on-street parking, and providing an opportunity for illegal secondary suites. Administration also received 1 letters from adjacent neighbours concerned about aesthetics, traffic and parking.

No public meetings were held by Administration for this application.

Land Use Amendment in Temple (Ward 10) at 3 Templemont Way NE, LOC2018-0132

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to the site, the proposal meets the policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The subject parcel is located within the Residential - Developed - Established area of the *Municipal Development Plan*. The applicable policies encourage modest redevelopment of established areas that is similar in scale and built form to existing development, including a mix of housing types. The *Municipal Development Plan* also calls for a modest intensification of the established area, an area serviced by existing infrastructure, public amenities and transit.

The proposal is in keeping with relevant *Municipal Development Plan* policies as the rules of the R-C2 District provide for a development form that may be sensitive to existing residential development in terms of height, built form and density.

Local Area Plan

There is no local area plan for the community of Temple. It is within the Airport Phase 2 Design Brief (non statutory from 1972).

Social, Environmental, Economic (External)

The recommended land use allows for a slightly wider range of housing types than the existing R-C1 District and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

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Land Use Amendment in Temple (Ward 10) at 3 Templemont Way NE, LOC2018-0132

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the *Municipal Development Plan*. The proposed R-C2 District is intended for low density residential development. The proposal represents a modest increase in density for this established parcel of land and allows for a development that can be compatible with the character of the existing neighbourhood. In addition, the subject parcel is a corner site, is located within walking distance of a bus transit stop, and has direct lane access.

ATTACHMENT(S)

1. Applicant Submission

Applicant Submission

City of Calgary
Planning and Building
800 MacLeod Trail SW
Calgary AB T2P 2M5

RE: Applicant Submission Statement

The subject parcel is located in the community of Temple in NE quadrant of Calgary and consists of approximately 0.0485 ha of land. The current zoning for the land use is RC1 (residential designation in developed area for single detached home). An application is submitted requesting for redesignation from RC1 to RC2 (residential designation in developed area for side-by-side homes). The proposed use is well-suited to the site, given its surrounding context, lot characteristics and location. The current lot size meets all the requirements stipulated by City of Calgary for RC2 zoning. As can be seen in Figures 1 & 2, the site is within few hundred meters to existing RC2 zones.

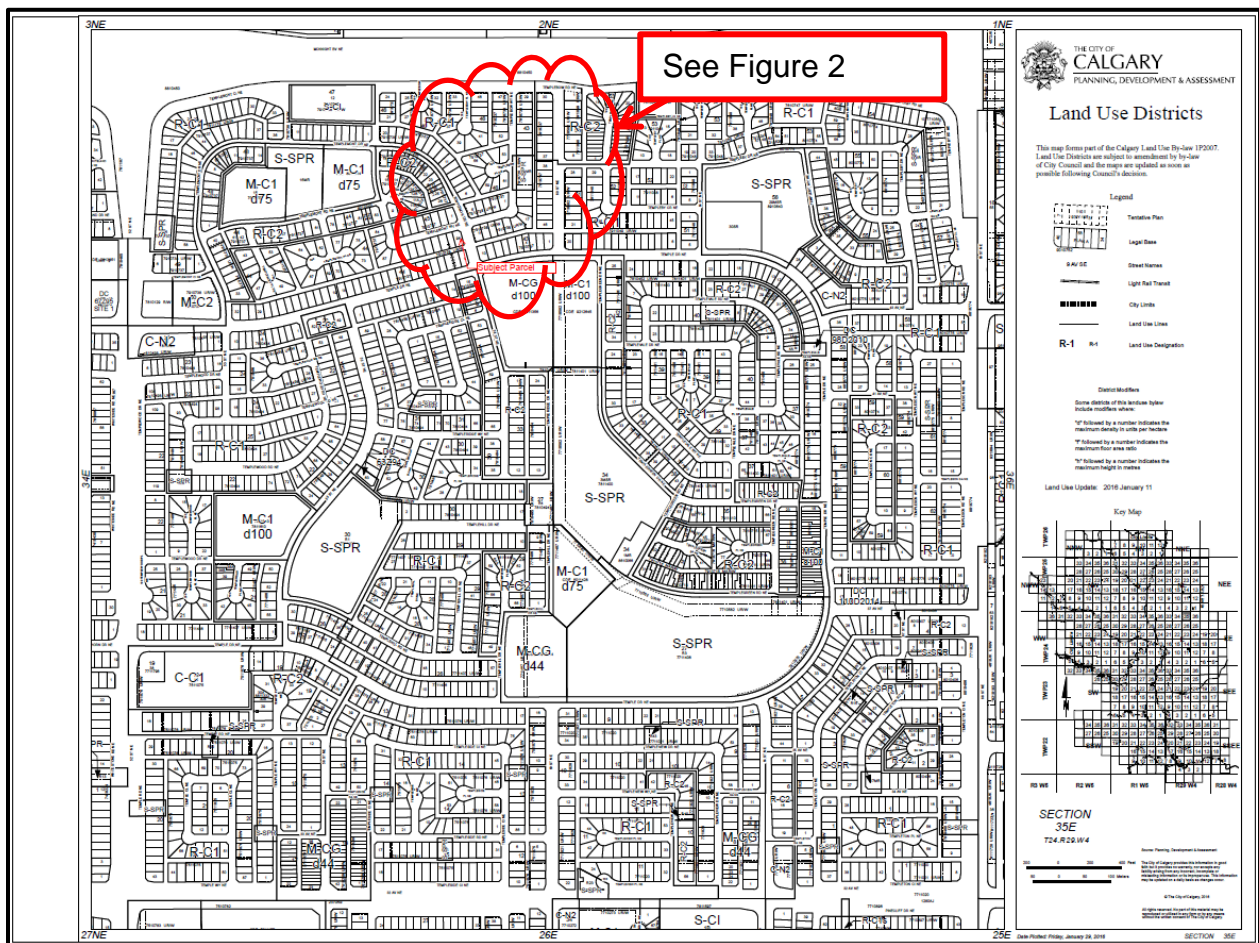


Figure 1: Community of Temple Zoning Map

(source: <http://www.calgary.ca/PDA/pd/Pages/Calgary-Land-Use-bylaw-1P2007/Land-Use-bylaw-1P2007-maps.aspx>)

Applicant Submission

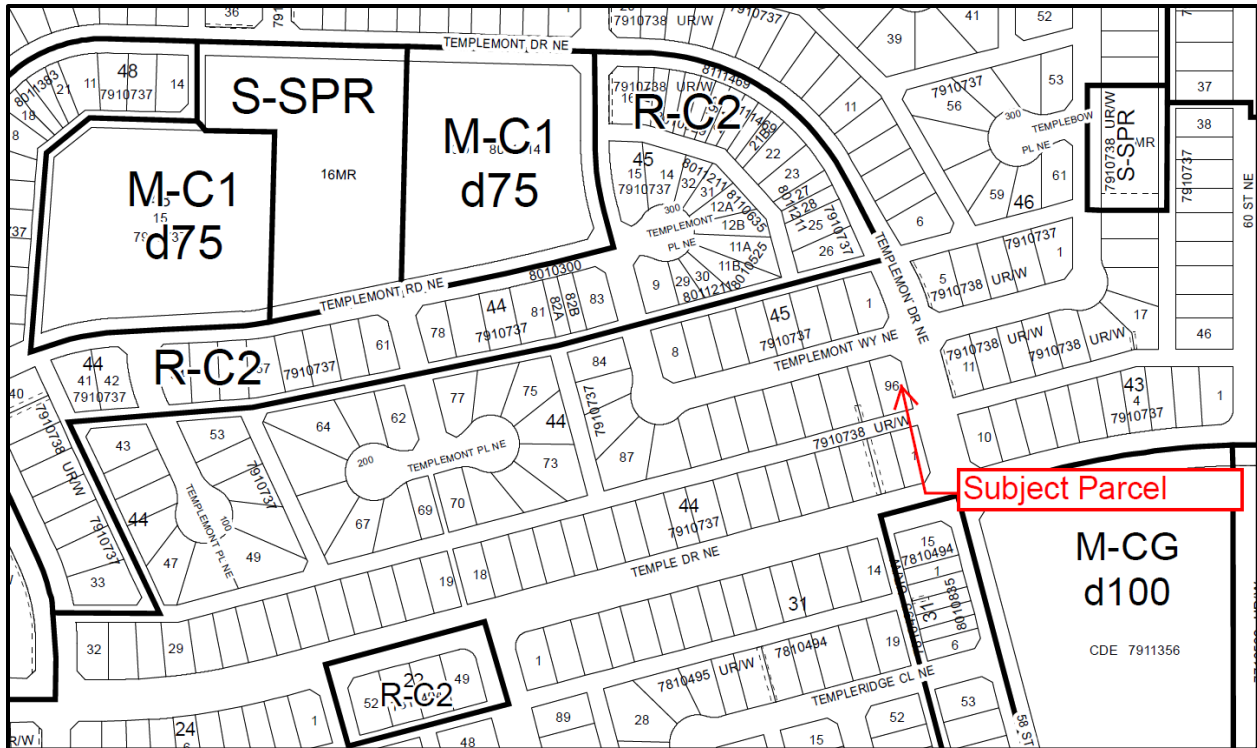


Figure 2: Zoomed Zoning Map Near Subject Parcel

(source: <http://www.calgary.ca/PDA/pd/Pages/Calgary-Land-Use-bylaw-1P2007/Land-Use-bylaw-1P2007-maps.aspx>)

The plan to build new homes with front doors facing Templemont Way NE and garages in the back alley. Since this is the corner lot, once the construction begins it will not impact the neighbors significantly. In the end, there will be two beautiful new homes that will add value to the neighborhood appeal and rid the area of the current undeveloped/demolished grow-up house.

As can be seen in Figure 3, the subject site is less than 400m away from Calgary Transit route 25 and 38. Calgary's Primary Transit Network provides community with daily reliable public transit service, with a frequency of every 20 minutes or less, 7 days a week. The subject site is also within walking distance to Coral Springs Childcare Society Day Care.

Applicant Submission

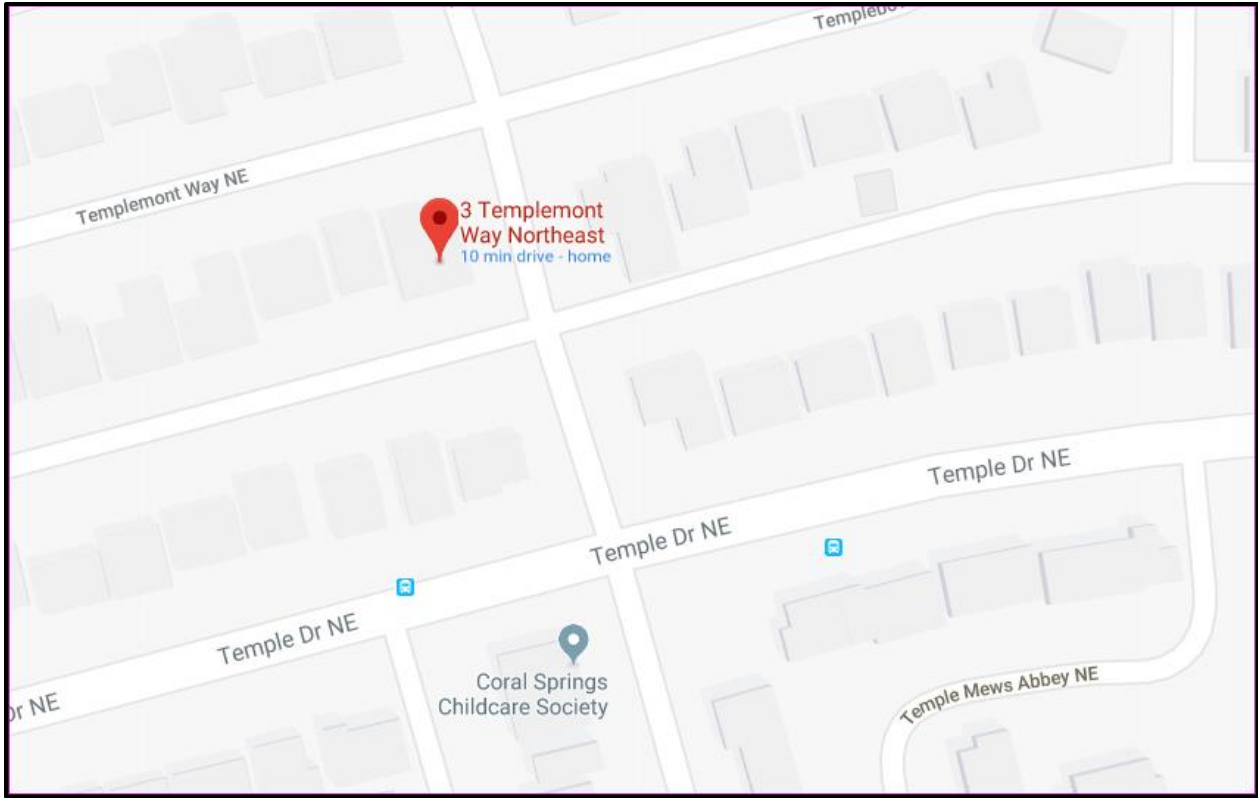


Figure 3: Nearest Bus Stops and Day Care from Subject Site

(source: <https://www.google.ca/maps/place/3+Templemont+Way+NE,+Calgary,+AB+T1Y+5C1/@51.0928076,-113.9510466,16.45z/data=!4m5!3m4!1s0x5371636059383749:0x4592b109fceb8aa7!8m2!3d51.0928929!4d-113.949358>)

This proposed land use redesignation and associated development vision is consistent with the city-wide goals which encourages affordable housing options in established communities; more efficient use of infrastructure; and more compact built forms in locations with direct and easy access to transit, shopping, schools and other community services.

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 CPC2018-1064
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**Land Use Amendment in Arbour Lake (Ward 2) at 35 Crowfoot Way NW,
 LOC2018-0142**

EXECUTIVE SUMMARY

This application was submitted by BCW Architects on 2018 June 15 on behalf of the landowner Calgary Co-operative Association Limited. The application proposes to change the land use of the subject site from DC Direct Control District to Commercial – Community 2 f1.0h18 (C-C2f1.0h18) District (1P2007) to allow for:

- the permitted and discretionary uses listed in the proposed C-C2 designation;
- a greater variety of commercial uses, such as Cannabis Store;
- a maximum building height of 18 metres (no change proposed); and
- a maximum building floor area of 36,440 square metres (no change proposed).

This land use amendment has been requested to allow for the additional commercial use of Cannabis Store. No additional changes to the existing development are being contemplated at this time

The proposal is compatible with surrounding land uses and in alignment with the applicable policies of the *Municipal Development Plan*. A change of use development permit application for a Cannabis Store was submitted on 2018 April 24, and is under review.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 3.65 hectares \pm (9.01 acres \pm) located at 35 Crowfoot Way NW (Plan 8610321, Block 4, Lot 33) from DC Direct Control District to Commercial – Community 2 f1.0h18 (C-C2f1.0h18); and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

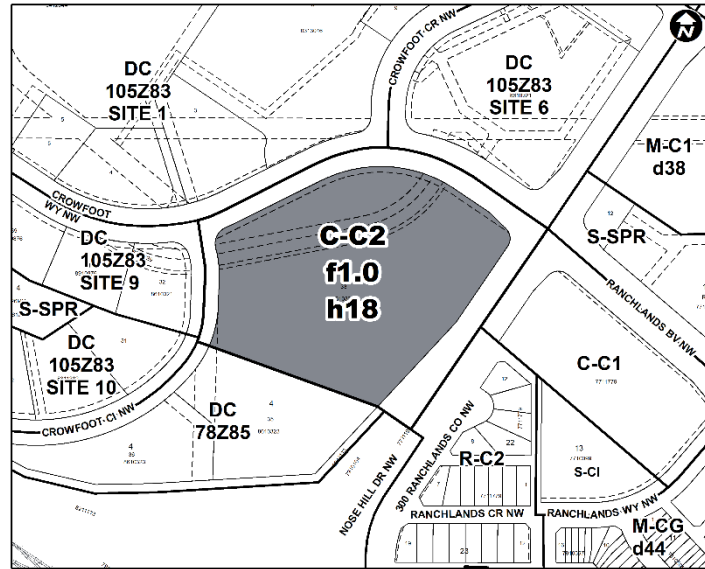
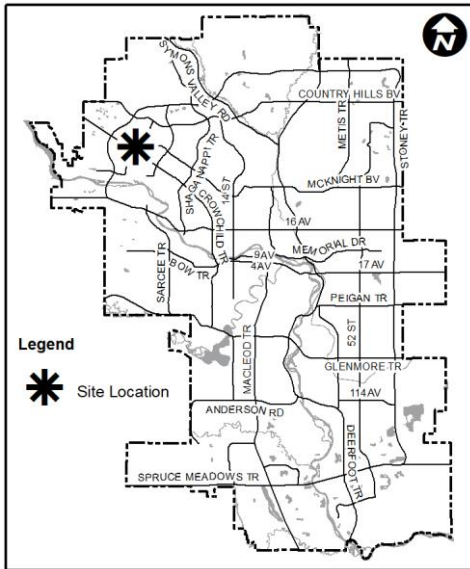
BACKGROUND

This application was submitted by BCW Architects on 2018 June 15 on behalf of the landowner Calgary Co-operative Association Limited.

No redevelopment of the site has been proposed, however, a change of use development permit application (DP2018-1833) for a Cannabis Store was submitted by Calgary Co-op Cannabis Crowfoot on 2018 April 24, and is under review as outlined in the Applicant's Submission (Attachment 1).

Land Use Amendment in Arbour Lake (Ward 2) at 35 Crowfoot Way NW, LOC2018-0142

Location Maps



Land Use Amendment in Arbour Lake (Ward 2) at 35 Crowfoot Way NW, LOC2018-0142

Site Context

The parcel is developed with a large scale commercial development and is located in the community of Arbour Lake, north of Crowchild Trail NW and immediately west of Nose Hill Drive NW.

The site is approximately 3.65 hectares (9.01 acres) in size, and is developed with three single-storey buildings: a liquor store, a gas bar, and a grocery store. The gross floor area for all existing buildings is approximately 9,100 square metres (97,952 square feet). A 440-stall surface parking area serves the development.

Lands to the north, south and west are commercially designated. The lands located to the east across Nose Hill Drive contain a mix of low density residential and commercial parcels. A large park is located to the east, just north of Ranchlands Boulevard.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal is to allow for the conversion to a Commercial District of Land Use Bylaw 1P2007 that allows for a range of commercial uses, including the Cannabis Store use. The proposal is consistent with the applicable policies, as discussed in the Strategic Alignment section of this report.

Planning Considerations

Land Use

On 2018 April 05, Council approved a new definition for cannabis retail in Land Use Bylaw 1P2007. This new use is called Cannabis Store and is proposed within specific land use districts, including the C-C2 District. Cannabis Store is not contemplated in any pre-1P2007 Bylaw DC Districts.

The subject site is currently designated as DC Direct Control District based on Land Use Bylaw 2P80's C-5 Shopping Centre Commercial District. The purpose of the existing district is to allow for the development of sector (community) shopping centres. The existing DC from 1983 is based on Bylaw 2P80 and is 'frozen in time', which means no new uses can be added to the existing DC, as Bylaw 2P80 is not being updated.

The proposed Commercial – Community 2 f1.0h18 (C-C2f1.0h18) is characterized by large commercial developments that are on the boundary of several communities. These developments have a wide range of use area sizes and types and have a slightly higher maximum building height than nearby low density residential areas. A maximum floor area ratio of 1.0 and a maximum height of 18 metres is proposed with which the existing development complies.

The uses listed under the proposed C-C2f1.0h18 District are similar in scope and scale those within the existing DC Direct Control District.

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Land Use Amendment in Arbour Lake (Ward 2) at 35 Crowfoot Way NW, LOC2018-0142

Development and Site Design

The applicant's submission indicated they are interested in using a portion of the existing building for a cannabis retail use. No redevelopment is being contemplated as part of this application.

Any future redevelopment of the site will be evaluated against the guidelines of the proposed DC District subject to Council's decision on this land use redesignation application.

Environmental

No environmental issues have been identified. An Environmental Site Assessment was not required for this application.

Transportation

The subject site is located adjacent to transit stops for several bus routes on Crowfoot Crescent NW as well as 600 metres walking distance from the Crowfoot LRT Station. Vehicular access is available from Crowfoot Way NW and Crowfoot Circle NW. A Traffic Impact Assessment was not required as part of this application.

Utilities and Servicing

Water, sanitary, and storm sewer mains are available and can accommodate the potential new uses and use area changes on the subject site without the need for off-site improvements at this time.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised on-line.

Administration did not receive comments from the Arbour Lake Community Association by the Calgary Planning Commission report submission date. No citizens' comments were received by the Calgary Planning Commission report submission date, and no public meetings were held by the applicant or Administration.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Land Use Amendment in Arbour Lake (Ward 2) at 35 Crowfoot Way NW, LOC2018-0142

Strategic Alignment

South Saskatchewan Regional Plan (Statutory 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The site is identified as a Major Activity Centre according to Map 1 – Urban Structure of the *Municipal Development Plan (MDP)* and is in close proximity to primary transit. Major Activity Centres (MACs) provide for the highest concentration of jobs and population outside of the Centre City area. The MDP states that MACs should be developed to function as an “urban centre” for a sub-region of the city and provide opportunities for people to work, live, shop, recreate, be entertained and meet their daily needs.

Social, Environmental, Economic (External)

The proposal continues to allow for and further supports a mix of commercial uses in Arbour Lake and provides local amenities for community residents

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposed land use redesignation is aligned with applicable policies identified in the *Municipal Development Plan*. The proposed Commercial – Community 2 f1.0h18 (C-C2f1.0h18) District integrates well with the existing commercial development and allows for a range of uses that are compatible with adjacent uses.

ATTACHMENT(S)

1. Applicant's Submission

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Land Use Amendment in Arbour Lake (Ward 2) at 35 Crowfoot Way NW, LOC2018-0142

Applicant's Submission



Land Use Redesignation Applicant's Submission

Not Including Secondary Suites

PL 1263 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this form to ensure it will fit the space requirements of the report. Supplementary information can be provided separately in your application if required.

A Change of Use application (DP2018-1833) was submitted on May 9, 2018 for a 'Cannabis Store'. The current land use zoning of the site does not permit a 'Cannabis Store' as a discretionary use. Therefore, we propose that the current Land Use designation of Direct Control DC 105Z83, to be amended to Commercial Community 2 (C-C2f##). The proposed Land Use Designation has been discussed with the City of Calgary and aligns with the existing conditions found on the site.

**Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05)
at multiple properties, LOC2017-0111**

EXECUTIVE SUMMARY

This application was submitted by Urban Systems Survey on behalf of Pacific Calgary GP Ltd on 2017 April 06. The land use redesignation consists of 96.15 hectares (237.59 acres) of undeveloped greenfield land in the northeast Residual Sub Area 5G. The proposed land use area is within the *East Stoney Area Structure Plan*.

The subject lands are currently designated Special Purpose – Future Urban Development (S-FUD) District and Special Purpose – Transportation and Utility Corridor (S-TUC) District. The proposal includes a number of land uses to accommodate a range of residential development, neighbourhood commercial, limited industrial and open space, including:

- The development of a residential neighbourhood on lands approximately 41.63 hectares (102.87 acres) with a mix of single detached dwellings, semi-detached dwellings, duplex dwelling and rowhouse dwellings (Residential – Low Density Mixed Housing (R-G) (R-Gm) District);
- Approximately 10.09 hectares (24.93 acres) for a mobile home park comprised of manufactured housing that will facilitate the development of adequate and affordable housing, creating conditions to enable citizens from a wide economic spectrum to live within a neighbourhood and provide quick access to Stoney Trail and adjacent existing/future employment areas (Residential – Manufactured Home (R-MH) District);
- A neighbourhood activity centre consisting of a smaller commercial site providing local retail services, community facilities and integrated transit stops. The neighbourhood activity centre also includes medium density housing further defined below;
- Three multi-residential sites totalling 6.03 hectares (14.90 acres) for comprehensive townhouse or at-grade multi-residential development (Multi-Residential – At Grade Housing (M-G) District);
- A 1.60 hectare (3.95 acre) local commercial site within the neighbourhood activity centre (Commercial – Community 1 (C-C1) District);
- Approximately 0.68 hectares (1.68 acres) for a light industrial parcel to facilitate production of the manufactured and modular housing units proposed for the neighbourhood;
- Approximately 9.84 hectares (24.31 acres) of Municipal Reserve, in the form of public open space and a K-9 school site (Special Purpose – School, Park and Community Reserve (S-SPR) District);
- Preserving 1.91 hectares (4.72 acres) of land to be dedicated as Environmental Reserve (Special Purpose – Urban Nature (S-UN) District); and
- 5.26 hectares (13.00 acres) for a stormpond and supporting public infrastructure (Special Purpose – City and Regional Infrastructure (S-CRI) District).

This application has been applied for with the support of a related outline plan application (CPC2018-1104) to provide the future subdivision layout for the site's development. Conditions have been incorporated in the outline plan to effectively address the site's development given the unique conditions and site constraints.

**Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05)
 at multiple properties, LOC2017-0111**

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommends that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to the East Stoney Area Structure Plan (Attachment 4); and
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 96.15 hectares \pm (237.59 acres \pm) located at 7055, 7111, 7697, 8393 - 84 Street NE (Portion of Lot 1, Block 1, Plan 1612484; Portion of SE1/4 Section 12-25-29-4; Portion of NE1/4 Section 12-25-29-4; Portion of SE1/4 Section 13-25-29-4) from Special Purpose – Future Urban Development (S-FUD) District and Special Purpose – Transportation and Utility Corridor (S-TUC) District to Residential – Low Density Mixed Housing (R-G) (R-Gm) District, Residential Manufactured Home (R-MH) District, Multi-Residential – At Grade Housing (M-G) District, Commercial Community 1 (C-C1) District, Industrial (I-G) District, Special Purpose – Community Service (S-CRI) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, and Special Purpose – Urban Nature (S-UN) District; and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

The direction to commence the East Stoney Area Structure Plan was provided through a Notice of Motion (NM2016-09) from Councillor Stevenson on that was approved by Council on 7 March 2016:

WHEREAS there is an EXPLORE application under review by Administration for the development of a comprehensively designed affordable housing community;

AND WHEREAS there is a need to facilitate conditions to enable citizens from a wide economic and demographic spectrum to live within a community;

AND WHEREAS access to adequate and affordable housing in the City of Calgary is a fundamental component of the quality of life in a city, as stated in Part 2, Section 2.3.1 of the Calgary Municipal Development Plan;

AND WHEREAS the subject lands are currently intended to accommodate temporary uses within residual parcels prior to comprehensive development as stated in Section 7.1 of the Rocky View/Calgary Intermunicipal Development Plan;

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**Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05)
at multiple properties, LOC2017-0111**

AND WHEREAS there is presently no local area policy for the lands in question;
AND WHEREAS Section 7.1.2 of the Rocky View/Calgary Intermunicipal Development Plan states that Residual Long-Term Growth Areas, as identified in Map 3, should be planned comprehensively through an Area Structure Plan and/or Regional Context Study with adjacent land within Rocky View County;

AND WHEREAS this Area Structure Plan will be under the developer funded model;

NOW THEREFORE BE IT RESOLVED that Council direct Administration to enter into an agreement with the landowner to fund the cost of producing an Area Structure Plan for the lands municipally and legally described as 7111, 7697, 8393 84 ST NE (SE12-25-29- W4M, NE12-25-12-W4M, SE13-25-29-W4M) and to review concurrently an application for Outline Plan and Land Use Amendment with the development of the Area Structure Plan.

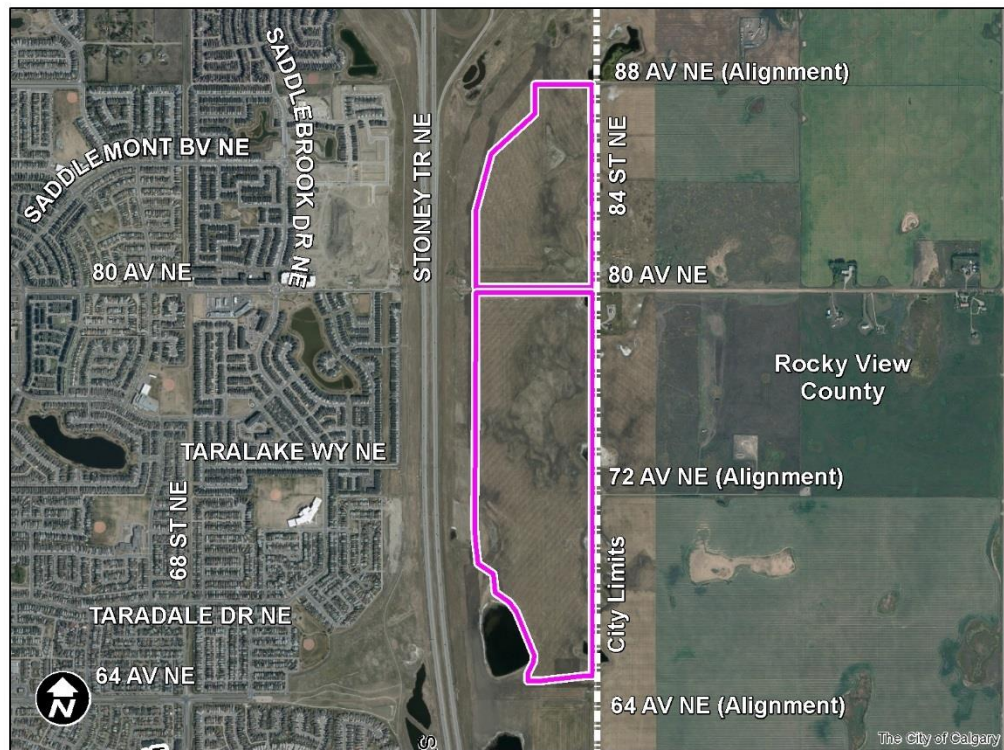
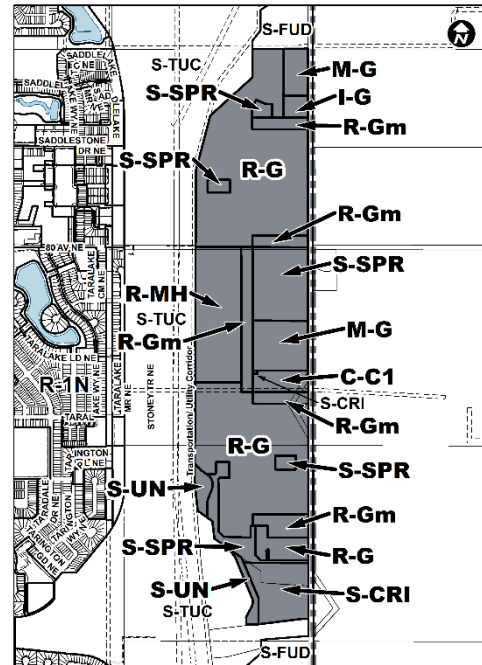
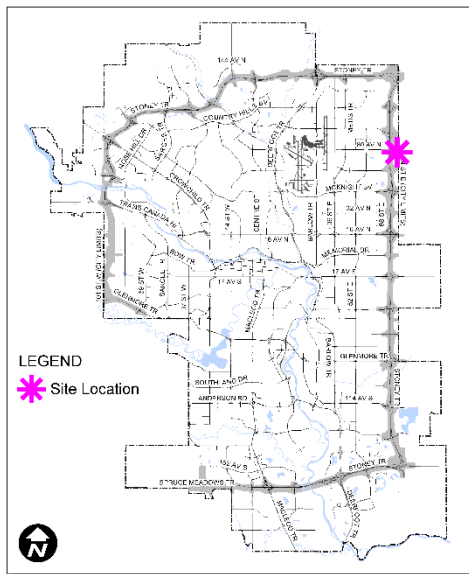
BACKGROUND

Urban Systems Survey on behalf of Pacific Calgary GP Ltd submitted the subject application to on 2017 April 06 and have provided a summary of their proposal in the Applicant's Submission (Attachment 1).

On 2017 May 08, Council adopted the *East Stoney Area Structure Plan* which outlined the development vision for the subject lands of this application. On 2018 July 30, Council approved a total of 14 new communities for the 2018 New Community Growth Strategy. On 2018 September 11, Council approved the first reading of the East Stoney bylaw amendment (69P2018) for Growth Management Overlay removal, and as requested by Rocky View County, withheld second and third readings to allow time for The County to review the amendment in detail and to meet with City staff to discuss the Growth Management Overlay removal and potential ancillary issues. City staff met with Rocky View County representatives to discuss The City's Growth Management Overlay process and Rocky View County's potential planning and technical issues. On 2018 October 09, Council amended the *East Stoney Area Structure Plan* to remove the Growth Management Overlay for the majority of plan area providing greater certainty to proponents, and allow for planning application reviews underway in these communities to continue moving forward.

**Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05)
 at multiple properties, LOC2017-0111**

Location Maps



Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05) at multiple properties, LOC2017-0111

Site Context

The subject lands 7055, 7111, 7697 and 8393 - 84 Street, are situated in the northeast quadrant of the city. Located immediately east of Stoney Trail NE, the subject lands are bound on the south by 64 Avenue NE, on the north at 96 Avenue NE (Airport Trail NE), and the east by 84 Street NE. The existing communities of Saddleridge, Taradale, Martindale, and Coral Springs are located to the west.

Adjacent to the east are existing country residential and agricultural uses located in Rocky View County. The Conrich Area Structure Plan, consisting primarily of industrial uses, is located to the southeast while the OMNI Area Structure Plan is located to the northeast, consisting primarily of highway commercial and light industrial uses.

Historically, the subject lands were utilized for agricultural purposes.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This land use amendment (Attachment 2), outline plan application (Attachment 3) and minor Area Structure Plan mapping amendment (Attachment 4) will facilitate the development of a residential neighbourhood with a mix of uses that will provide adequate and affordable housing, creating conditions to enable citizens from a wide economic spectrum to live within a neighbourhood and have quick access to Stoney Trail and adjacent existing/future employment areas in the northeast quadrant of the City, as envisioned by the policies of the *Municipal Development Plan* and *East Stoney Area Structure Plan*.

Planning Considerations

As part of the review of this application, several key factors were considered by Administration including implementing the direction of the *East Stoney Area Structure Plan*, providing a condition framework that ensures key infrastructure is built and including the basic design elements that contribute to a complete neighbourhood.

Subdivision Design

The outline plan informs a future proposed subdivision that is approximately 96.15 hectares (237.59 acres) in size. Generally, the subdivision consists of residential lands and support commercial and local employment uses. The street network and design of the plan area is a modified grid network due to the challenging linear shape of the subject lands. Local residential streets and various walkways, pathways and trails provide further connectivity within the neighbourhood and the surrounding existing and proposed communities in the City and County. A unique feature of the proposed plan area is that the industrial lot on the north end is intended to house a production facility for the modular and manufactured housing in the neighbourhood and will ultimately be phased out once development of all phases in the subdivision is complete.

The plan area contains one Neighbourhood Activity Centre (NAC), that is centrally located within the neighbourhood. The NAC is intended to be a focal point for the neighbourhood that creates gathering space and accommodates a mix of transit-supportive residential and non-

**Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05)
at multiple properties, LOC2017-0111**

residential uses. The NAC is connected to nearby focal points by a network of active transportation modes that is designed to provide a positive pedestrian environment promoting activity in the public realm. Aligned with the policies, the proposed NAC is located adjacent to a transit stop and provides a mix of grade-oriented residential sites, neighbourhood commercial development, and a school site with playfields.

The design provides a framework to accommodate a diversity of housing forms with a combination of laned and non-laned product types. Laned products are located along the collector roadways to improve the residential street interface and access conditions with lane-accessed parking. This also reduces pedestrian-vehicular disruptions along the collectors. A mix of unit types are proposed in the plan area, including single detached homes, semi-detached units, duplex dwelling units, rowhouse units, townhouse units and apartments.

The area will contain an extensive network of regional pathways and various local walkways that connect residents to the neighbourhood activity centre, joint use site (which will be a future Kindergarten to Grade 9 school site) and a series of planned park spaces. These connections create a local network, but also tie to the greater regional network through the neighbouring communities to the west. Future connections to the north, south and east may be designed in collaboration with Rocky View County when the Airport Trail NE interchange and 64 Avenue NE flyover are constructed.

Municipal Reserve/City public park dedications will be provided in various shapes and types of uses incorporating the aforementioned school site and active and passive uses throughout the plan area. The majority of the Municipal Reserve parks are connected by the regional pathway system.

A single stormpond is proposed at the south end of the plan area located in a portion of surplus provincial TUC lands to serve the development.

Land Use

The subject site is currently designated Special Purpose – Future Urban Development (S-FUD) District and Special Purpose – Transportation and Utility Corridor (S-TUC) District. To implement the proposed outline plan's neighbourhood design, the following uses are proposed:

- Residential – Low Density Mixed Housing (R-G) (R-Gm) District;
- Residential – Manufactured Home (R-MH) District;
- Multi-Residential – At Grade Housing (M-G) District;
- Commercial – Community 1 (C-C1) District;
- Industrial – General (I-G) District;
- Special Purpose – Community Service (S-CRI) District;
- Special Purpose – School, Park and Community Reserve (S-SPR) District; and
- Special Purpose – Urban Nature (S-UN) District.

Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05) at multiple properties, LOC2017-0111

The mix of residential districts including Residential – Low Density Mixed Housing (R-G) (R-Gm) District and Multi-Residential – At Grade Housing (M-G) District will provide for a diversity of both low-density housing forms and multi-residential developments. The R-G and R-Gm Districts are both intended to allow a greater diversity of housing types in the form of single detached, semi-detached, duplexes and rowhouses. The R-Gm District is identical to the R-G District with the exception of excluding single detached dwellings as a permitted use.

Multi-residential developments are accommodated by the proposed Multi-Residential – At Grade Housing (M-G) Districts on three distinct sites.

The Special Purpose – Urban Nature (S-UN) District is proposed to preserve an existing drainage course and provide a buffer between the proposed stormpond and the existing wetland southwest of the plan area in the TUC. The Special Purpose – School, Park and Community Reserve (S-SPR) Districts will accommodate a K-9 school, community association site, a variety of parks and open spaces incorporating both active and passive uses to further enhance and provide access to the creek valley, and provide additional amenities to residents. The Special Purpose – City and Regional Infrastructure (S-CRI) District will accommodate a large stormpond, and associated infrastructure (e.g. inlets, outlets, and lift stations).

Density

The land uses proposed provides for development that achieves both the *Municipal Development Plan* and the *East Stoney Area Structure Plan* minimum density and intensity targets (population and jobs).

Development for the overall community is required by the *Municipal Development Plan* and the *East Stoney Area Structure Plan* to achieve an intensity of 60 to 70 people and jobs per hectare. The subject site is anticipating an intensity of approximately 66 people and jobs per hectare, which exceeds this intensity target.

The application proposes to accommodate a residential unit range between 1771 and 2042 units. This equates to an anticipated residential density of 18.79 units per hectare (7.6 units per acre) with a maximum residential density of 21.7 uph (8.8 upa). This density range achieves the overall intent of the minimum residential density of 20 uph (8 upa) suggested by the *Municipal Development Plan* for the neighbourhood.

Environmental

The proposed application is located within an undeveloped ‘greenfield’ area. Historically, the lands have been used for agricultural purposes as identified through an Environmental Site Assessment report, in which no environmental concerns were identified. No other environmental issues have been identified for the subject lands.

**Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05)
at multiple properties, LOC2017-0111**

An existing ephemeral water body is located in the southwest corner of the outline plan area carries the proposed dedication as Environmental Reserve (Special Purpose – Urban Nature (S-UN) District) and is surrounded by a linear park that features a green corridor, naturalized planting and native grasses. The southern portion of the Environmental Reserve also includes a 30-metre buffer from the provincially classified wetland outside the plan area.

Transportation

Streets and Access

The Plan Area will benefit from proximity to the provincial highway network. Primary access to the plan area is provided via 84 Street NE to McKnight Boulevard NE connecting to the regional transportation network at Stoney Trail. A future eastward extension of Airport Trail NE (96 Avenue NE) and 64 Avenue NE flyover of Stoney Trail NE will provide secondary access.

As part of the Growth Management Overlay removal process separate to this application on 2018 September 11, a crossing over Stoney Trail NE at 80 Avenue NE to accommodate access for emergency services, transit, pedestrian and cyclist connectivity to the plan area was approved by Council. This is a necessary piece of infrastructure to support this proposed development. This project will be added to the existing Offsite Levy Bylaw, with City portion of funding approved with the One Calgary 2019 – 2022 budget plan in 2018 November (PFC2018-0973). The connection of the flyover within the plan area is along Homestead Avenue NE, which is an extension of the 80 Avenue NE alignment. The City and the Developer will coordinate to complete the connection of Homestead Avenue NE to 84 Street NE in conjunction with commissioning of the flyover.

Construction and opening of Homestead Road NE, as well as the Developer's boundary half of 84 Street NE is intended to align with construction and opening of the 80 Avenue NE flyover crossing of Stoney Trail NE.

Currently, 84 Street NE is a two-lane, rural cross-section gravel road between McKnight Boulevard NE and Country Hills Boulevard NE, formerly under the jurisdiction of Rocky View County. To accommodate additional traffic on 84 Street NE, it will be necessary to upgrade to a paved road consistent with City standards. Portions of the 84 Street NE alignment are in Rocky View County, where the alignment diverts at its intersections with McKnight Boulevard NE and Country Hills Boulevard NE.

Eighty-fourth Street NE is subject to a joint study between the City and Rocky View County to determine road classification, long term alignment at intersections with major east-west roads, right-of-way and access management. At the time of writing of this report, the 84 Street NE study is not approved by all parties. However, the outline plan conditions of approval (CPC2018-1104) are structured to support the continued collaboration between the developer, The City, and Rocky View County, allowing Administration to bring forward a recommendation of approval for the proposed applications. Conditions around development and infrastructure staging necessary to support development and associated infrastructure are reflective of this approach.

Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05) at multiple properties, LOC2017-0111

Upgrades to boundary roads are typically funded and constructed by adjacent landowners in conjunction with their development. However, if other development has not occurred along this section of 84 Street NE, it may be necessary to advance developer-funded upgrades of contiguous sections of the road to support development in the *East Stoney Area Structure Plan* area. Coordination between area developers, the City, and Rocky View County is required to complete construction and servicing of new developments within the City and the County; this includes coordination by this Developer, the City, and Rocky View County in order to complete the required upgrades of 84 Street NE to service the community.

The long-term transportation plans for this area include upgrades of the interchange of Stoney Trail and Airport Trail NE (96 Avenue NE) to accommodate all movements, including an eastward extension of Airport Trail into Rocky View County. 64 Avenue NE will also be extended eastward over Stoney Trail as a flyover. These projects are not funded at this time, and are not included in the City's current budget or 10-year capital plan. These future projects provide opportunities for better connections for active modes, transit and vehicle travel.

Transit

This area is currently not served by any transit routes. Transit service introduction to new communities is balanced with service increases in existing communities including addressing ridership growth or introducing evening and weekend service. The 80 Avenue NE flyover will facilitate a direct connection to the Blue Line LRT located at Saddletowne. Alternate transit routing options may include a connection to the Blue Line station located at Whitehorn, or local transit service connecting to other regional bus routes.

Utilities and Servicing

Water Servicing

Water servicing of the lands within the Plan Area will be achieved through an extension of the existing water distribution network west of the Transportation Utility Corridor (TUC.) A minimum of three developer-funded water distribution mains crossing the TUC will be required to service the Plan Area.

Sanitary Servicing

To service all developable lands within the Plan Area, a developer-funded lift station will be required and will be designed to City of Calgary standards. Sanitary servicing of the lands within the Plan Area will be achieved through extensions of the existing sanitary collection network west of the Transportation Utility Corridor. A developer-funded sanitary forcemain from the proposed lift station will be connected into the existing sanitary network between Saddlebrook Common NE and Airport Trail NE (96 Avenue NE).

Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05) at multiple properties, LOC2017-0111

Stormwater Servicing

The plan area includes a proposed storm pond to contain all onsite stormwater. The pond will temporarily drain into the Coral Springs system in The City of Calgary through a pump system during off-season periods. Ultimately, the facility will be redirected to a regional facility under the Cooperative Stormwater Management Initiative (CSMI), once available. The CSMI is a collective involving The City of Calgary, The City of Chestermere, Rocky View County, The Town of Strathmore, Wheatland County and the Western Irrigation District.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

There is no Community Association for the subject area and no letters from adjacent landowners or the general public were received.

No public meetings were conducted by the applicant or Administration in direct relation to this outline plan application. However, as part of the previous Area Structure Plan work two public information sessions were held in 2016. The first information session was a "listen and learn" format, where the public had the opportunity to review and provide feedback on two possible land use concepts. The second information session was an "inform and communicate" format, where the public reviewed the revised land use concept, supporting maps, graphics, and discussed with City staff.

A project webpage was launched in 2016 April, with over 1200 page views. The webpage was used to explain the process, timelines, and engagement opportunities available for this project. It also served as a hub for supporting information and resource links.

Due to the proximity of the subject lands to the neighboring municipality – Rocky View County, an important part of the stakeholder engagement with this application was ongoing consultation with that municipal authority. As part of the initial application process, Rocky View County was circulated the land use and outline plan. The County initially cited concerns about the transportation network and proposed stormwater solution. The County was circulated an amended land use and outline plan package once a technical review by Administration was complete. City staff met with Rocky View County representatives in 2018 September to discuss The City's Growth Management Overlay process and address Rocky View County's transportation and engineering concerns. On 2018 October 5, Administration received a letter from Rocky View County (Attachment 5) stating that they are satisfied their concerns with the *East Stoney Area Structure Plan* will be appropriately addressed through subsequent development stages as part of tentative plan and subdivision approval. Conditions have been included within the conditions of approval of the outline plan (CPC2018-1104) to address their concerns related to transportation impacts and stormwater management.

Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05) at multiple properties, LOC2017-0111

Following Calgary Planning Commission, notifications for Public Hearing of Council, in relation to this land use amendment, will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)*. The site is located within the "City, Town" area as identified on Schedule C: *South Saskatchewan Regional Plan Map* in the *SSRP*. While the *SSRP* makes no specific reference to this site, the supporting application is consistent with the *SSRP* policies including the Land Use Patterns strategies (subsection 8.14) within the Implementation Plan part of the document by featuring innovative housing designs with a range of densities and housing types such as mixed-use, row-housing and low-end market manufactured and modular housing product.

Rocky View County / City of Calgary Intermunicipal Development Plan (Statutory – 2012)

The *Intermunicipal Development Plan (IDP)* identified the subject lands as "Residual Long-Term Growth Areas", until the *East Stoney Area Structure Plan* was approved on 2018 May 8. The proposed land use and outline plan comply with the general policies of interface planning, specifically the "Industrial and Residential Interface Policies".

Municipal Development Plan (Statutory – 2009)

The MDP, Map 1 'Urban Structure' identifies the subject lands as 'Residential – Developing - Future Greenfield'. The *Municipal Development Plan* provides guidance for the development of new communities through the policies of the *East Stoney Area Structure Plan*. The proposed application meets the following *Municipal Development Plan* objectives (Section 3.6.2):

- Meeting minimum intensity targets;
- Providing a diversity of housing types;
- Protecting the key natural features;
- Including parks located throughout the community in walkable proximity to all residents;
- and
- Creating a connected, multi-modal street network.

East Stoney Area Structure Plan (Statutory – 2017)

The *East Stoney Area Structure Plan* provides more detailed direction with detailed policies and guidelines for development. The proposed application is consistent with the applicable policies of this plan. This land use amendment is supported by a mapping amendment to all ten maps of the *East Stoney Area Structure Plan* (Attachment 4) to enlarge the plan area boundary to include the surplus Transportation Utility Corridor lands on the south end of the plan area for stormwater management.

Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05) at multiple properties, LOC2017-0111

Social, Environmental, Economic (External)

The land use enables and outline plan for a neighbourhood that provides a future framework for a mix of housing types, various densities and preservation of natural drainage courses in the area. The development of these lands will enable citizens from a wide economic spectrum to live within a neighbourhood with adequate and affordable housing and provide quick access to Stoney Trail NE and adjacent existing/future employment areas in the northeast quadrant of the City.

Financial Capacity

Current and Future Operating Budget:

A large portion of operating costs required to service the proposed development are included within the current operating budget. As development proceeds, the provision of other City services such as roads, transit, parks and waste and recycling would have an operating budget impact at such time as they are provided. Projected operating costs for this development during 2019-2022 have been included in the One Calgary service plan and budget.

Current and Future Capital Budget:

There is no impact to the current capital budget as a result of this report. The capital investment required to construct and upgrade the required local infrastructure will be funded entirely by the developer. The proposed development will leverage existing City funded capital investment, and will require additional transportation infrastructure that has been included in the 2019-2022 budget cycle. This additional infrastructure consists of the 80 Avenue NE and Stoney Trail flyover and is to be added to the Off-site Levy Bylaw through report PFC2018-0973. The flyover will be funded by The City and through off-site levies paid by developers.

Risk Assessment

This project lies on the periphery of The City of Calgary in an area which is currently unserved and has a still developing transportation network. The associated outline plan conditions of approval (CPC2018-1104) adequately deal with the infrastructure costs associated with development of a new neighborhood in this area. At the time of writing this report, the amended Off-site Levy Bylaw and One Calgary service plan and budget has not been approved by Council and therefore, there is not a current funding source for the full cost of the 80 Avenue NE flyover at Stoney Trail NE. If a funding source has not been approved prior to land use approval, the timing of infrastructure may not align with the timing of development. It is anticipated that the funding sources for the 80 Avenue NE flyover at Stoney Trail NE will have been approved by the end of 2018 November prior to the 2018 December 10 public hearing of Council for the land use redesignation of the subject lands.

**Policy Amendment and Land Use Amendment in Residual Sub-Area 5G (Ward 05)
at multiple properties, LOC2017-0111**

REASON(S) FOR RECOMMENDATION(S):

The proposed land use amendment has been developed in accordance with the objectives of the *Municipal Development Plan*, and more specifically is in accordance with the policies specified in the *East Stoney Area Structure Plan*. The application also follows through on the intent of the previous Council direction for these lands.

The proposed land uses and their distribution facilitate the development of adequate and affordable housing, creating conditions to enable citizens from a wide economic spectrum to live within a neighbourhood and provide quick access to Stoney Trail and adjacent existing/future employment areas in the northeast quadrant of the City.

These land uses will be implemented through the supporting outline plan application that provides the subdivision layout and conditions to realize the site's development.

ATTACHMENT(S)

1. Applicant Submission
2. Proposed Land Use District Map
3. Proposed Outline Plan
4. Proposed Mapping Amendments to the East Stoney Area Structure Plan
5. Letter from Rocky View County

Applicant Submission

This application for land use redesignation and outline plan approval is located within the East Stoney Area Structure Plan (ASP). The East Stoney ASP was presented to Calgary Planning Commission on March 9, 2017 and will be presented to Council for public hearing on May 8, 2017.

It was prepared to guide the development of +/- 96 ha (238 acres) of land in NE Calgary. The document sets the framework for future development of the community that provides a mix of uses consistent with the City of Calgary's growth objectives.

The subject lands are located along the east boundary of Stoney Trail NE and bordering Rocky View County to the east at 84th Street NE. The property consists of +/- 96.19 hectares (+/- 237.69 acres) and is owned by Pacific Investments and Development Ltd.

The current land uses within the outline plan area are Special Purpose (S-FUD) District and Transportation and Utility Corridor (S-TUC) District. The lands have historically been utilized for farming and have never been developed. The lands are generally flat with surface drainage currently flowing from the west to the east draining to an existing wetland bordering the subject lands to the south. There is a small strip of environmentally sensitive natural grass lands and creek that border the south west boundary of the site. There are no significant historical features within the plan boundary.

The East Stoney community will be developed as a residential area in the northeast limits of Calgary, consistent with the ASP policies. Density and intensity targets specified within the Municipal Development Plan (and New Community Planning Guidebook) are achieved.

The site will be developed in four phases starting at the southeast corner of the site. Development phasing will be based on market demand and logical extensions of infrastructure to the site.

Low density residential will be composed of R-G uses in the form of either single family residential, villas brownstones or townhomes. Semi-detached and "brownstone" homes will be located along the Collector Street for enhanced accessibility and walkability within the community.

One Manufactured home parcel is proposed for a manufactured home park.

Three medium density M-G parcels are proposed. The M-G parcels will be developed a comprehensively designed at grade homes which compliment both the NAC and the manufactured home development.

A small industrial I-G parcel has been located at the north access of the development to provide for local industrial services within the plan area. It is proposed that a beverage recycling depot will be on this site. Proper licenses for this facility have already been secured by the developer.

At the NAC is a local commercial site which will serve the convenience needs of the neighbourhood. This NAC is located along 84th Street NE yet still internal to the community to serve residents and to meet the criteria for NAC's within City policies.

Open spaces are provided throughout the East Stoney community. Green space is linked by the regional pathway system that has excellent connectivity within the community, and meet Municipal Reserve dedication requirements.

Applicant Submission

A joint use site (JUS) of 4.86 ha (12 acres) is provided in the north central area of East Stoney in accordance with the East Stoney ASP. The JUS site was relocated north to allow for better distribution of open space, closer proximity to medium density residential, and efficiency of transit service and for development staging purposes.

All streets within the outline plan are sized to meet forecast transportation capacities. Complete Streets Guidelines have been applied to the plan. Transit service is shown to be along north/south collector with transit stops adequately spaced. Transit will service the area through a looped route that ultimately will connect to the future LRT station to the northeast.

A Sanitary Servicing Study for East Stoney was completed by Kellam Berg Engineering & Surveys Ltd. as part of the Area Structure Plan approvals. At the time of study, existing downstream sanitary sewer had sufficient capacity for full build out of the ASP lands and no upgrades were required.

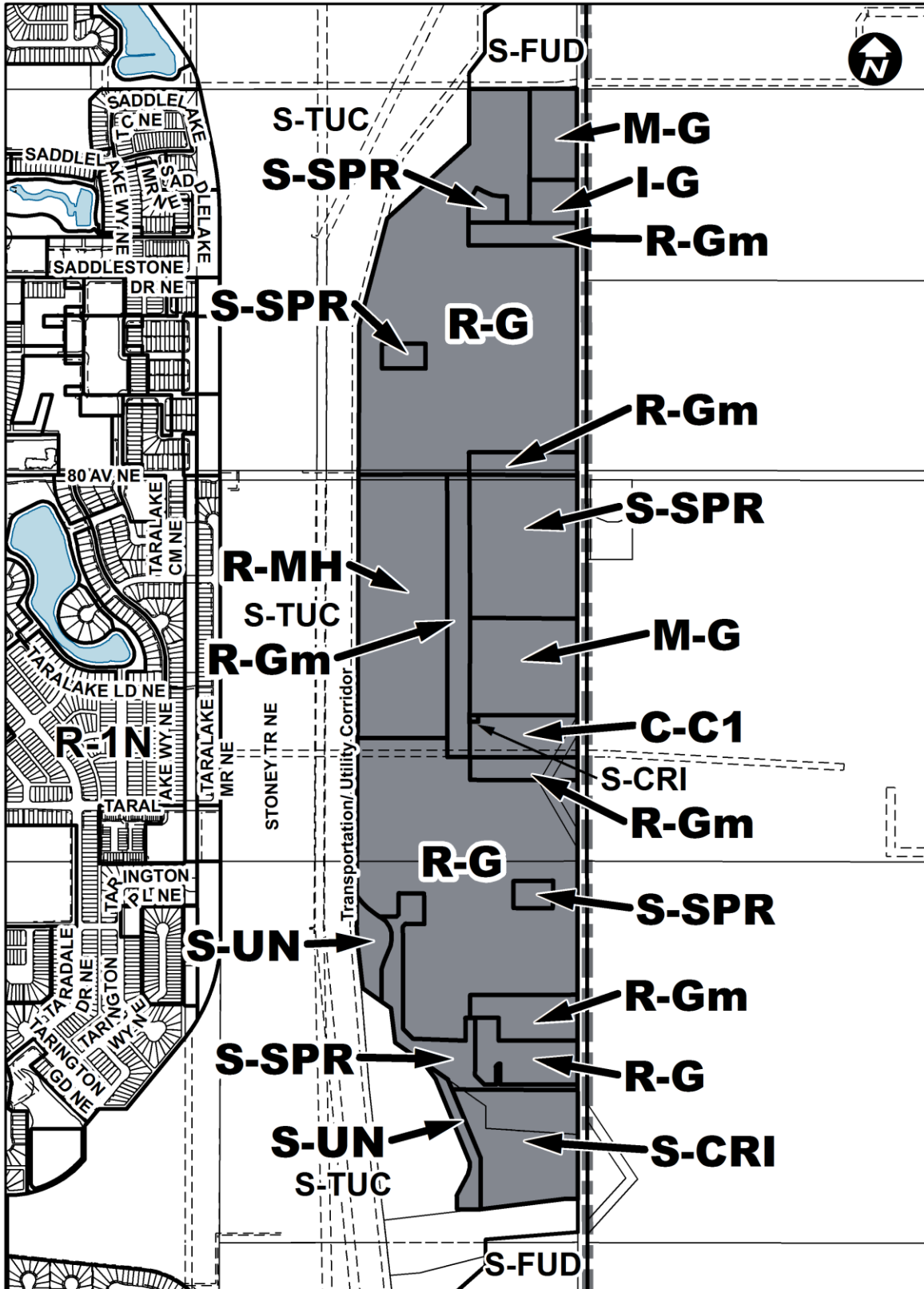
Stormwater servicing for the Outline Plan area will be provided by construction of a stormwater management facility at the south end of the plan and will be sized to handle runoff from all lands within the ASP boundary. There will be two manufactured housing parcels created within the development, each of which will be required to construct a private stormwater management facility. The entire development will be serviced with gravity storm sewer mains that drain towards the main stormwater facility at the south end of the plan.

A formal application for community and street names will follow under a separate application.

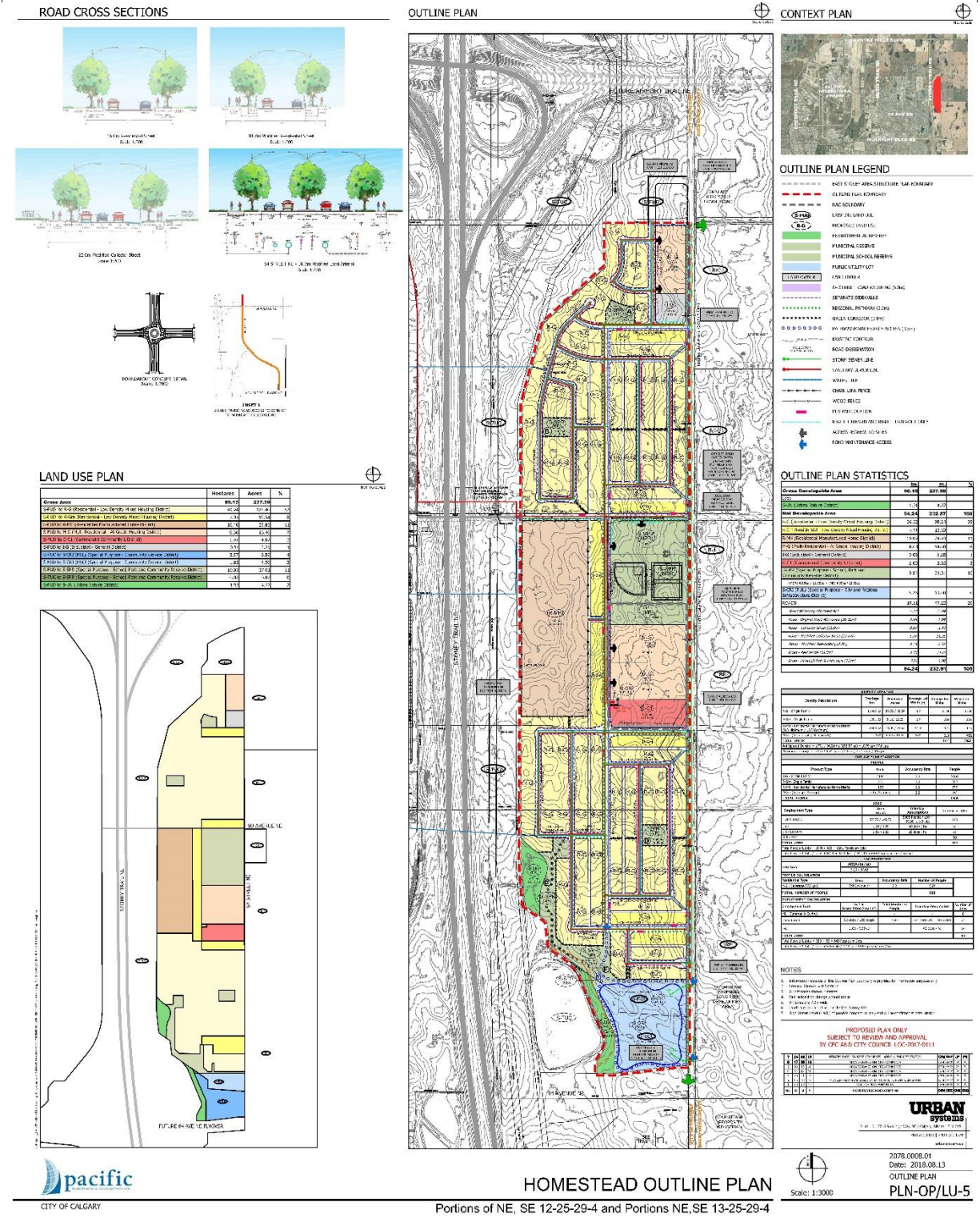
On behalf of Pacific Investments & Developments Ltd., Urban Systems respectfully requests approval of the outline plan from the Calgary Planning Commission and approval for the land use to be redesignated to single family, multi-family, multi-residential, commercial, industrial and special purpose districts by City Council.

- Urban Systems

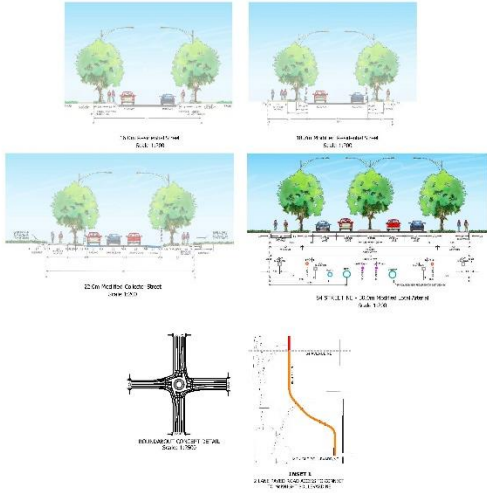
Proposed Land Use District Map



Proposed Outline Plan

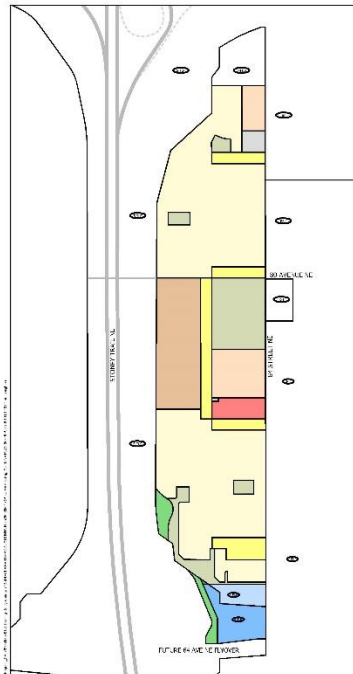


ROAD CROSS SECTIONS

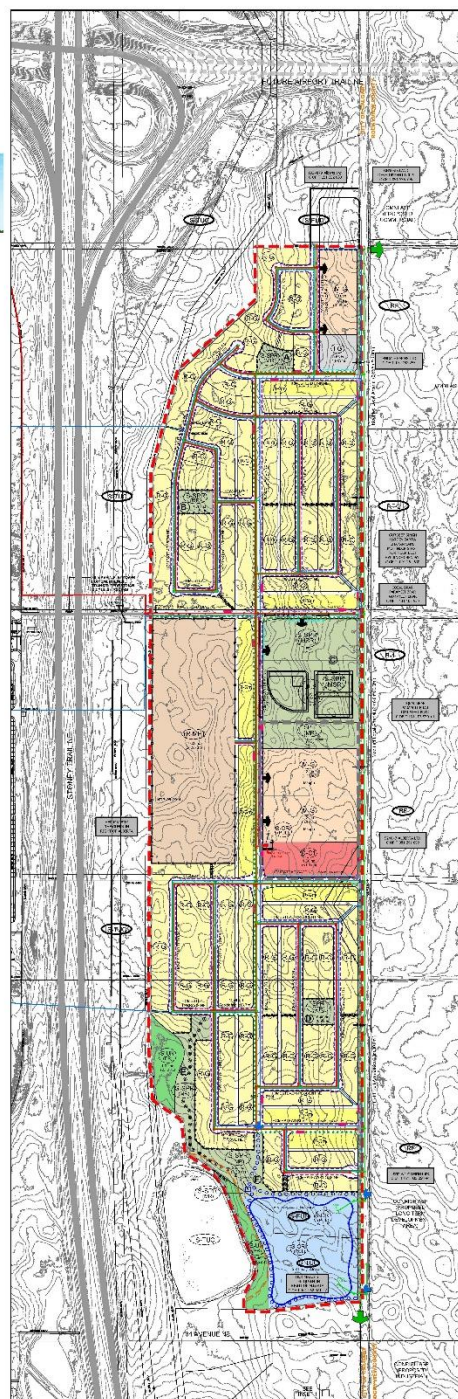


LAND USE PLAN

	Hectares	Acres	%
Grass Area	85.17	207.26	
SP100 - Single Detached Residential	46.16	113.84	53.5
SP100 - Single Detached Residential (Low Density Residential)	21.14	52.54	24.7
SP100 - Single Detached Residential (Medium Density Residential)	18.87	46.88	21.9
SP100 - Single Detached Residential (High Density Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Very High Density Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Ultra High Density Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Community Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Other Residential)	0.00	0.00	0.0



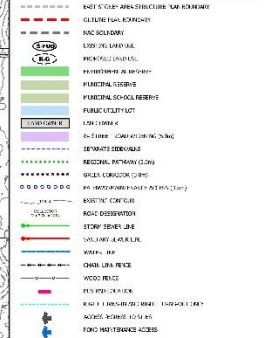
OUTLINE PLAN



CONTEXT PLAN



OUTLINE PLAN LEGEND



OUTLINE PLAN STATISTICS

Category	Area (Hectares)	Area (Acres)	Percentage (%)
Grass Development Area	85.17	207.26	53.5
SP100 - Single Detached Residential	46.16	113.84	24.7
SP100 - Single Detached Residential (Low Density Residential)	21.14	52.54	21.9
SP100 - Single Detached Residential (Medium Density Residential)	18.87	46.88	21.9
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SP100 - Single Detached Residential (Ultra High Density Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Community Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Other Residential)	0.00	0.00	0.0
Total	161.34	404.52	100.0

Category	Area (Hectares)	Area (Acres)	Percentage (%)
Grass Area	85.17	207.26	53.5
SP100 - Single Detached Residential	46.16	113.84	24.7
SP100 - Single Detached Residential (Low Density Residential)	21.14	52.54	21.9
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SP100 - Single Detached Residential (Ultra High Density Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Community Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Other Residential)	0.00	0.00	0.0
Total	161.34	404.52	100.0

NOTES

1. UNRESTRICTED AREA - The City Council has approved the outline plan for the project area.
2. THE CITY OF CALGARY HAS APPROVED THE OUTLINE PLAN FOR THE PROJECT AREA.
3. THE CITY OF CALGARY HAS APPROVED THE OUTLINE PLAN FOR THE PROJECT AREA.
4. THE CITY OF CALGARY HAS APPROVED THE OUTLINE PLAN FOR THE PROJECT AREA.
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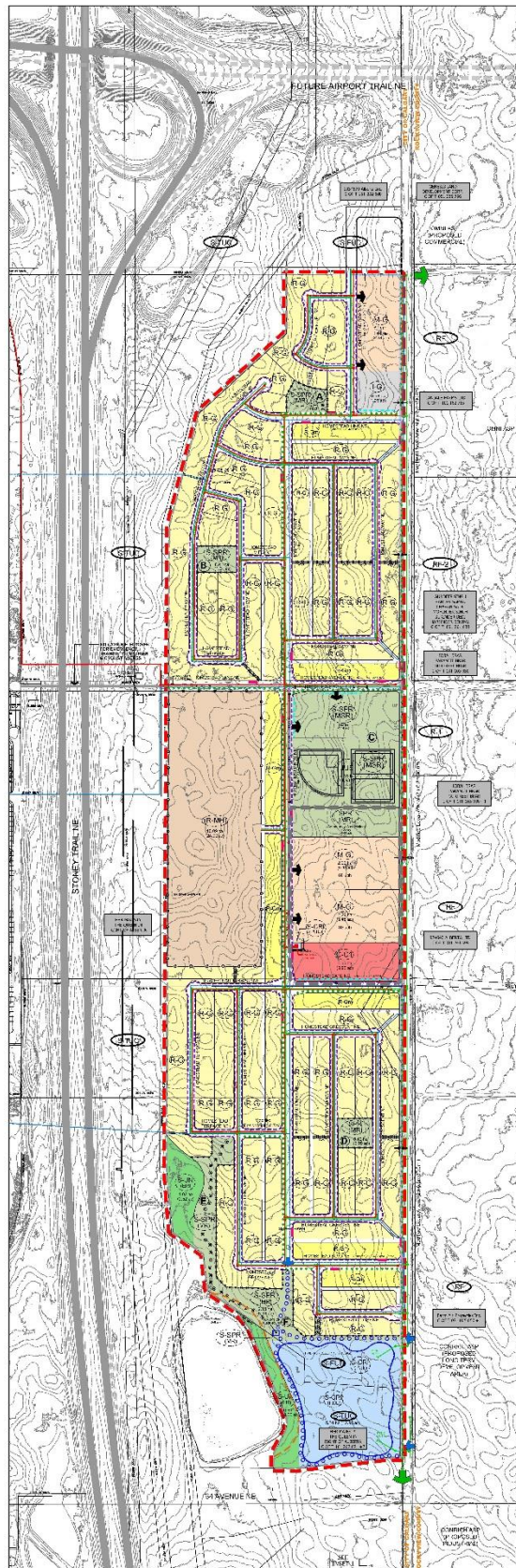
Category	Area (Hectares)	Area (Acres)	Percentage (%)
Grass Area	85.17	207.26	53.5
SP100 - Single Detached Residential	46.16	113.84	24.7
SP100 - Single Detached Residential (Low Density Residential)	21.14	52.54	21.9
SP100 - Single Detached Residential (Medium Density Residential)	18.87	46.88	21.9
SP100 - Single Detached Residential (High Density Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Very High Density Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Ultra High Density Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Community Residential)	0.00	0.00	0.0
SP100 - Single Detached Residential (Special Purpose - Other Residential)	0.00	0.00	0.0
Total	161.34	404.52	100.0



HOMESTEAD OUTLINE PLAN
Portions of NE, SE 12-25-29-4 and Portions NE, SE 13-25-29-4
















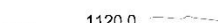










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Date: 2018.06.13
OUTLINE PLAN
PLN-OP/LU-5

Proposed Outline Plan



Proposed Outline Plan

OUTLINE PLAN LEGEND

	EAST STONEY AREA STRUCTURE PLAN BOUNDARY
	OUTLINE PLAN BOUNDARY
	NAC BOUNDARY
	EXISTING LAND USE
	PROPOSED LAND USE
	ENVIRONMENTAL RESERVE
	MUNICIPAL RESERVE
	MUNICIPAL SCHOOL RESERVE
	PUBLIC UTILITY LOT
	LAND OWNER
	84 STREET ROAD WIDENING (5.0m)
	SEPARATE SIDEWALKS
	REGIONAL PATHWAY (3.0m)
	GREEN CORRIDOR (3.0m)
	PATHWAY/MAINTENANCE ACCESS (3.5m)
	EXISTING CONTOUR
	ROAD DESIGNATION
	STORM SEWER LINE
	SANITARY SEWER LINE
	WATER LINE
	CHAIN LINK FENCE
	WOOD FENCE
	BUS PAD LOCATION
	RIGHT TURNS-IN AND RIGHT TURNS-OUT ONLY
	ACCESS /EGRESS TO SITES
	POND MAINTENANCE ACCESS

Proposed Outline Plan

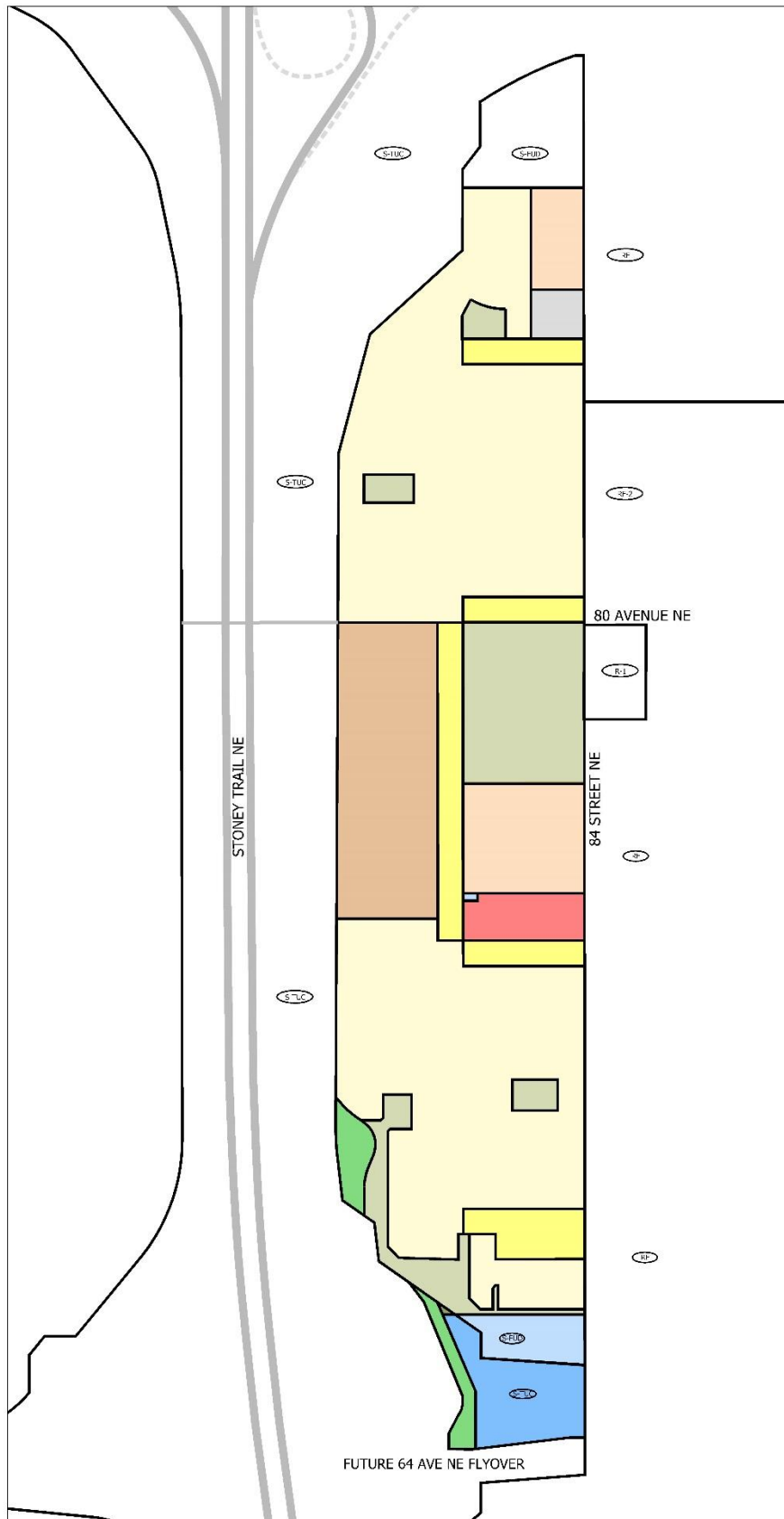
OUTLINE PLAN STATISTICS

	ha.	ac.	%
Gross Developable Area	96.15	237.59	
Less			
S-UN (Urban Nature District)	1.91	4.72	
Net Developable Area	94.24	232.87	100
R-G (Residential - Low Density Mixed Housing District)	36.52	90.24	39
R-Gm (Residential - Low Density Mixed Housing District)	5.11	12.63	5
R-MH (Residential Manufactured Home District)	10.09	24.93	11
M-G (Multi-Residential - At Grade Housing District)	6.03	14.90	6
I-G (Industrial - General District)	0.68	1.68	1
C-C1 (Commercial Community 1 District)	1.60	3.95	2
S-SPR (Special Purpose - School, Park and Community Reserve District)	9.84	24.31	10
(MSR) 4.85ha / 11.98ac + (MR) 4.99ha / 12.33ac			
S-CRI (PUL) (Special Purpose - City and Regional Infrastructure District)	5.26	13.00	6
ROADS	19.11	47.22	20
<i>Road Widening (84 Street NE)</i>	<i>1.17</i>	<i>2.89</i>	
<i>Road - Original Road Allowance (20.12m)</i>	<i>0.44</i>	<i>1.09</i>	
<i>Road - Collector Street (21.0m)</i>	<i>0.57</i>	<i>1.41</i>	
<i>Road - Modified Collector Street (22.0m)</i>	<i>5.77</i>	<i>14.26</i>	
<i>Road - Modified Residential (18.7m)</i>	<i>0.15</i>	<i>0.37</i>	
<i>Road - Residential (16.0m)</i>	<i>8.50</i>	<i>21.00</i>	
<i>Road - Lanes (7.0m) & Walkways (3.0m)</i>	<i>2.51</i>	<i>6.20</i>	
	94.24	232.91	100

Proposed Outline Plan

DENSITY ANALYSIS					
Density Calculations	Frontage (m)	Hectares/ Acres	Average Lot Width (m)	Anticipated Units	Maximum Units
R-G - Single Family	10047.62	36.52 / 90.24	8.7	1154	1154
R-Gm - Single Family	1551.52	5.11/ 12.63	6.7	231	231
R-MH - Residential Manufactured Home District (9.8 Minimum / 13.7 Maximum)	2068.72	10.09 / 24.93	11.8	175	175
M-G - (35 min uph / 80 max uph)	N/A	6.03 / 14.90	N/A	211	482
TOTAL UNITS				1771	2042
Anticipated Density = 1771 / 94.24 ha (232.87 ac) = 18.79 uph / 7.6 upa					
Maximum Density = 2042/ 94.24 ha (232.87 ac) = 21.7 uph / 8.8 upa					
OUTLINE PLAN STATISTICS					
PEOPLE					
Product Type	Units	Occupancy Rate	People		
R-G - Single Family	1154	3.3	3808		
R-Gm- Single Family	231	3.3	762		
R-MH - Residential Manufactured Home District	175	3.3	577		
M-G - (57.5 uph Average)	346 (Average)	2.2	761		
TOTAL PEOPLE				5908	
JOBS					
Employment Type	Area ha / ac	Intensity Assumptions	Number of Jobs		
HOME BASED	57.75 / 142.70	5908 People / 100 = 59.08 x 3.8 jobs	225		
C-C1	1.60 / 3.95	40 Jobs / ha	64		
I-G (1.0 FAR)	0.54 / 1.33	25 Jobs / ha	14		
SPR (MSR)			50		
TOTAL JOBS				353	
Total People & Jobs = 5908+ 353 = 6261 People and Jobs					
Total People & Jobs / ha = 6261 People & Jobs / 94.24 ha = 66 People and Jobs / hectare					
NAC STATISTICS					
NAC Area	AREA (ha / ac)				
	7.13 / 17.62				
PEOPLE CALCULATION					
Residential Type	Units	Occupancy Rate	Number of People		
M-G - (average 57.5 uph)	254 (average)	2.2	560		
TOTAL NUMBER OF PEOPLE				560	
EMPLOYMENT CALCULATION					
Employment Type	ha / ac Gross Floor Area (m ²)	Total Number of People	Intensity Assumption	Number of Jobs	
MR - Community Centre)	-	-	-	0	
Home Based	3.8 Jobs / 100 people	560	737 People/100 = 7.4 x 3.8 jobs	21	
C-C1	1.60 / 3.95 ac		40 Jobs / ha	64	
TOTAL JOBS				85	
Total People & Jobs = 560 + 85 = 645 People and Jobs					
Total People & Jobs / ha = 645 People / 7.13 ha = 91 People and Jobs / ha					

Proposed Outline Plan



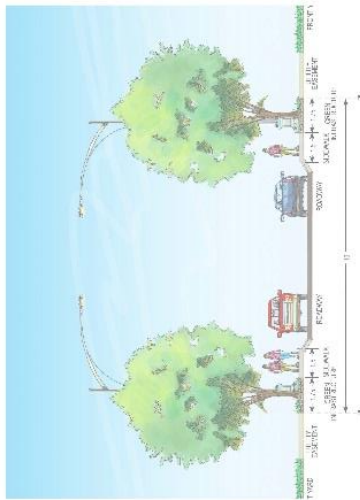
Proposed Outline Plan

LAND USE PLAN

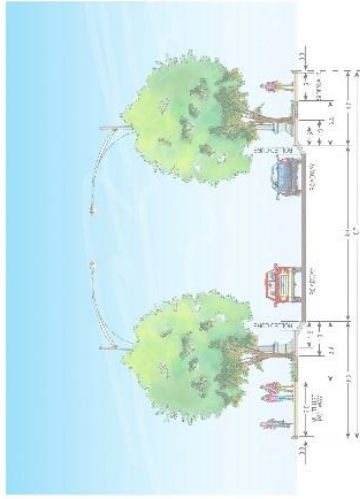
	Hectares	Acres	%
Gross Area	96.15	237.59	
S-FUD to R-G (Residential - Low Density Mixed Housing District)	49.94	123.40	52
S-FUD to R-Gm (Residential - Low Density Mixed Housing District)	7.95	19.64	8
S-FUD to R-MH (Residential Manufactured Home District)	10.46	25.85	11
S-FUD to M-G (Multi-Residential - At Grade Housing District)	6.66	16.46	7
S-FUD to C-C1 (Commercial Community 1 District)	1.97	4.87	2
S-FUD to I-G (Industrial - General District)	0.91	2.25	1
S-TUC to S-CRI (PUL) (Special Purpose - Community Service District)	3.57	8.82	4
S-FUD to S-CRI (PUL) (Special Purpose - Community Service District)	1.82	4.50	2
S-FUD to S-SPR (Special Purpose - School, Park and Community Reserve District)	10.93	27.01	11
S-TUC to S-SPR (Special Purpose - School, Park and Community Reserve District)	0.03	0.07	0
S-FUD to S-UN (Urban Nature District)	1.91	4.72	2

Proposed Outline Plan

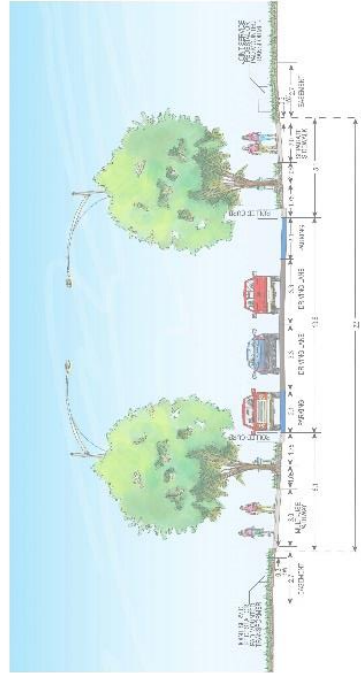
ROAD CROSS SECTIONS



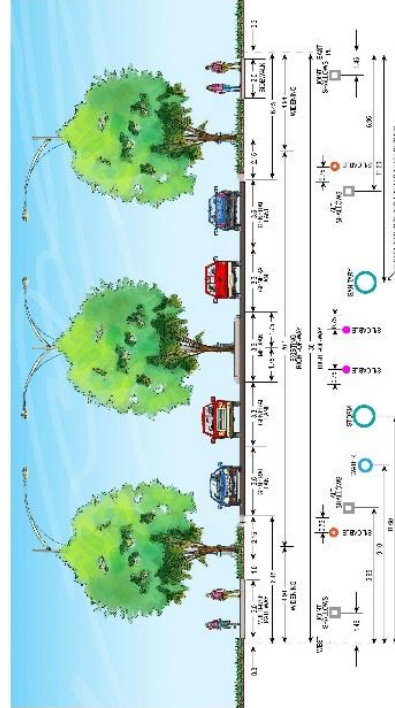
16.0m Residential Street
Scale 1:200



18.7m Modified Residential Street
Scale 1:200



22.0m Modified Collector Street
Scale 1:200



84 STREET NE - 30.0m Modified Local Arterial
Scale 1:200

Proposed Amendments to the East Stoney Area Structure Plan

WHEREAS it is desirable to amend the East Stoney Area Structure Plan Bylaw 23P2017, as amended;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26, as amended:

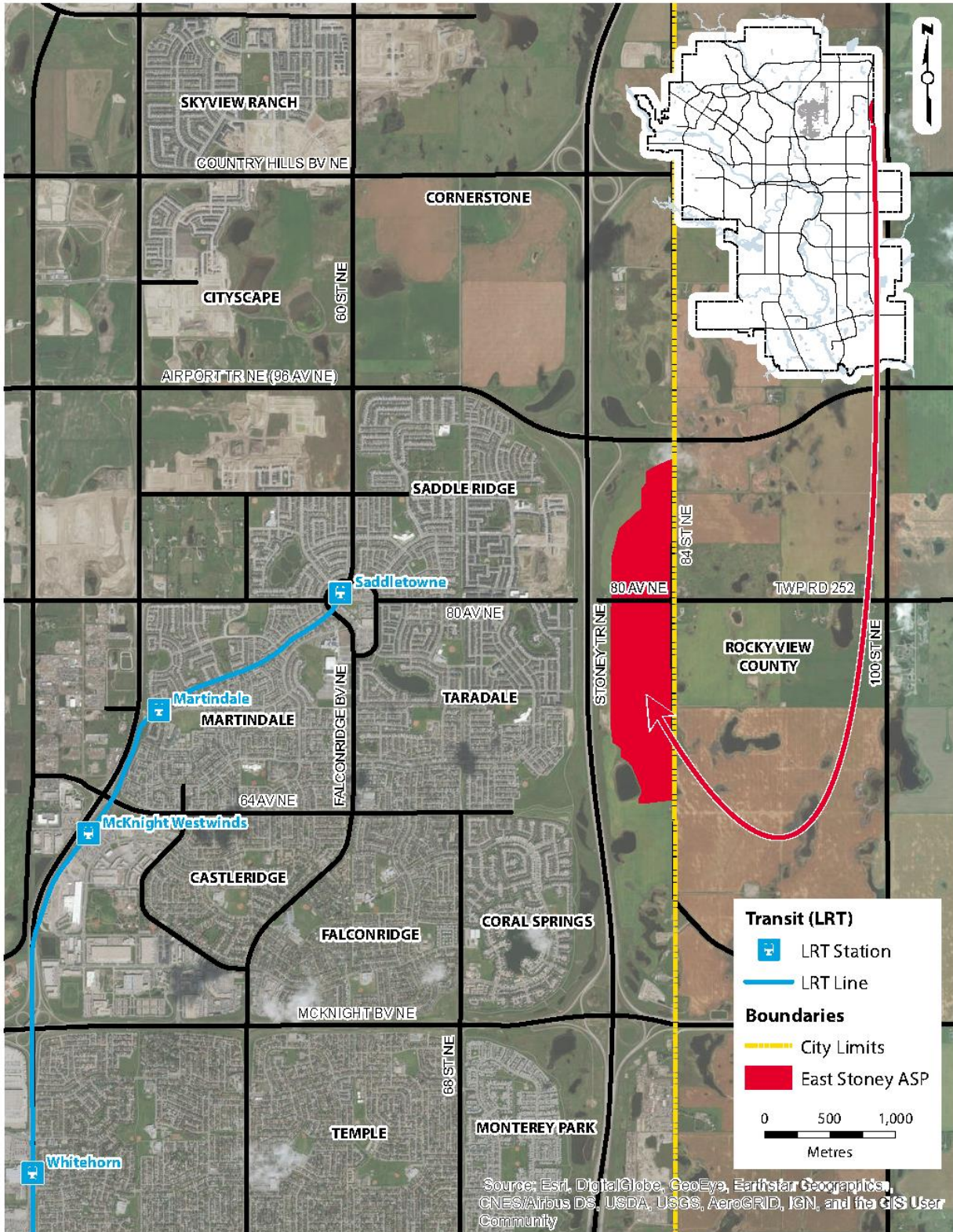
NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

1. The East Stoney Area Structure Plan attached to and forming part of Bylaw 23P2017, as amended, is hereby further amended as follows:
 - (a) Delete the existing Map 1 entitled "Plan Area Location" and replace with revised Map 1 entitled "Plan Area Location", as attached as Schedule A.
 - (b) Delete the existing Map 2 entitled "Land Use Concept" and replace with revised Map 2 entitled "Land Use Concept", as attached as Schedule B.
 - (c) Delete the existing Map 3 entitled "Regional Pathways and Green Corridors" and replace with revised Map 3 entitled "Regional Pathways and Green Corridors", as attached as Schedule C.
 - (d) Delete the existing Map 4 entitled "Environmental Open Space (EOS) Study Area" and replace with revised Map 4 entitled "Environmental Open Space (EOS) Study Area", as attached as Schedule D.
 - (e) Delete the existing Map 5 entitled "Transit Network" and replace with revised Map 5 entitled "Transit Network", as attached as Schedule E.
 - (f) Delete the existing Map 6 entitled "Street Network" and replace with revised Map 6 entitled "Street Network", as attached as Schedule F.
 - (g) Delete the existing Map 7 entitled "Growth Management Overlay" and replace with revised Map 7 entitled "Growth Management Overlay", as attached as Schedule G.
 - (h) Delete the existing Map 8 entitled "Biophysical Features" and replace with revised Map 8 entitled "Biophysical Features", as attached as Schedule H.
 - (i) Delete the existing Map 9 entitled "Development Staging" and replace with revised Map 9 entitled "Development Staging", as attached as Schedule I.
 - (j) Delete the existing Map 10 entitled "Plan Area Constraints" and replace with revised Map 10 entitled "Plan Area Constraints", as attached as Schedule J.

Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE A

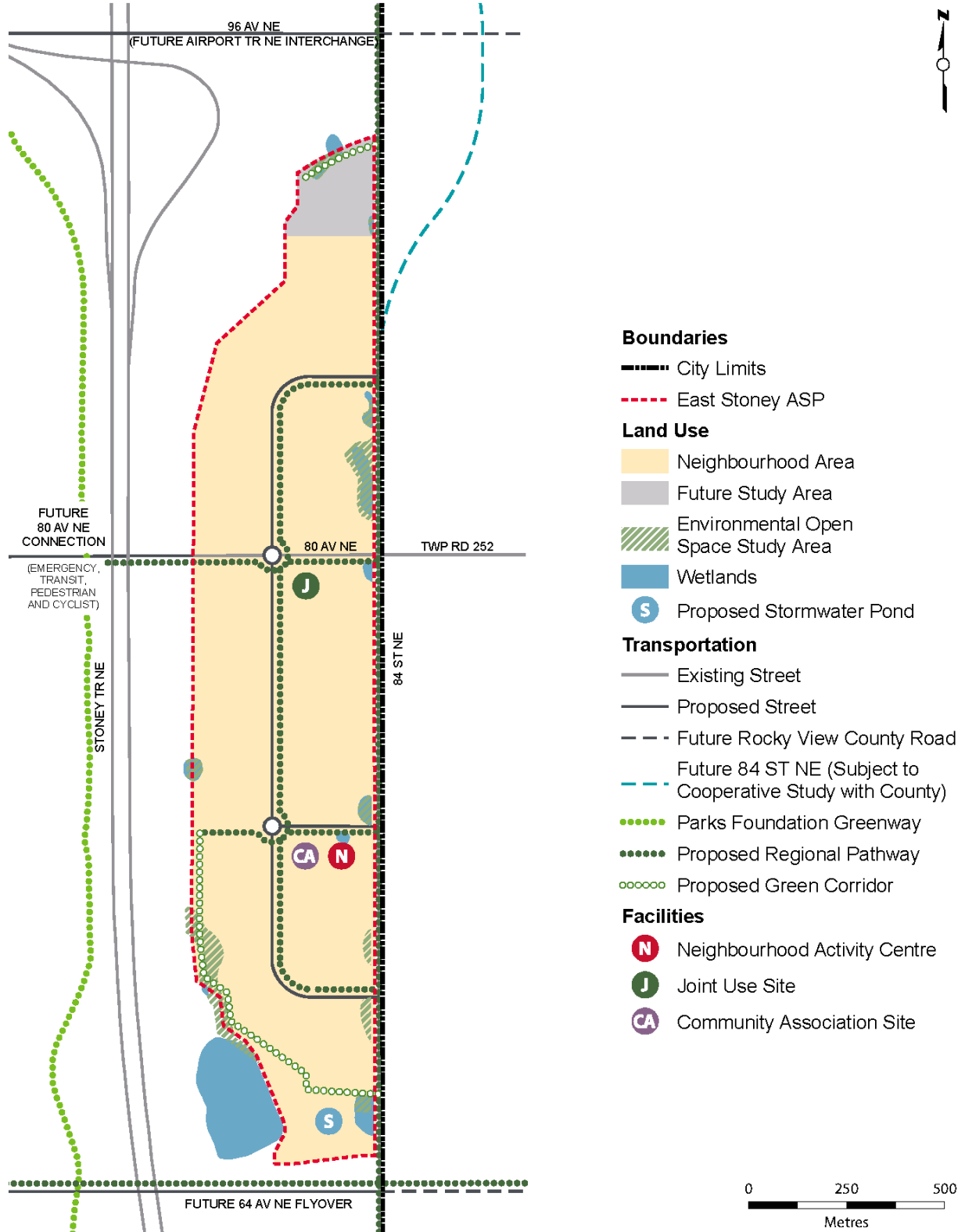
Map 1: Plan Area Location



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE B

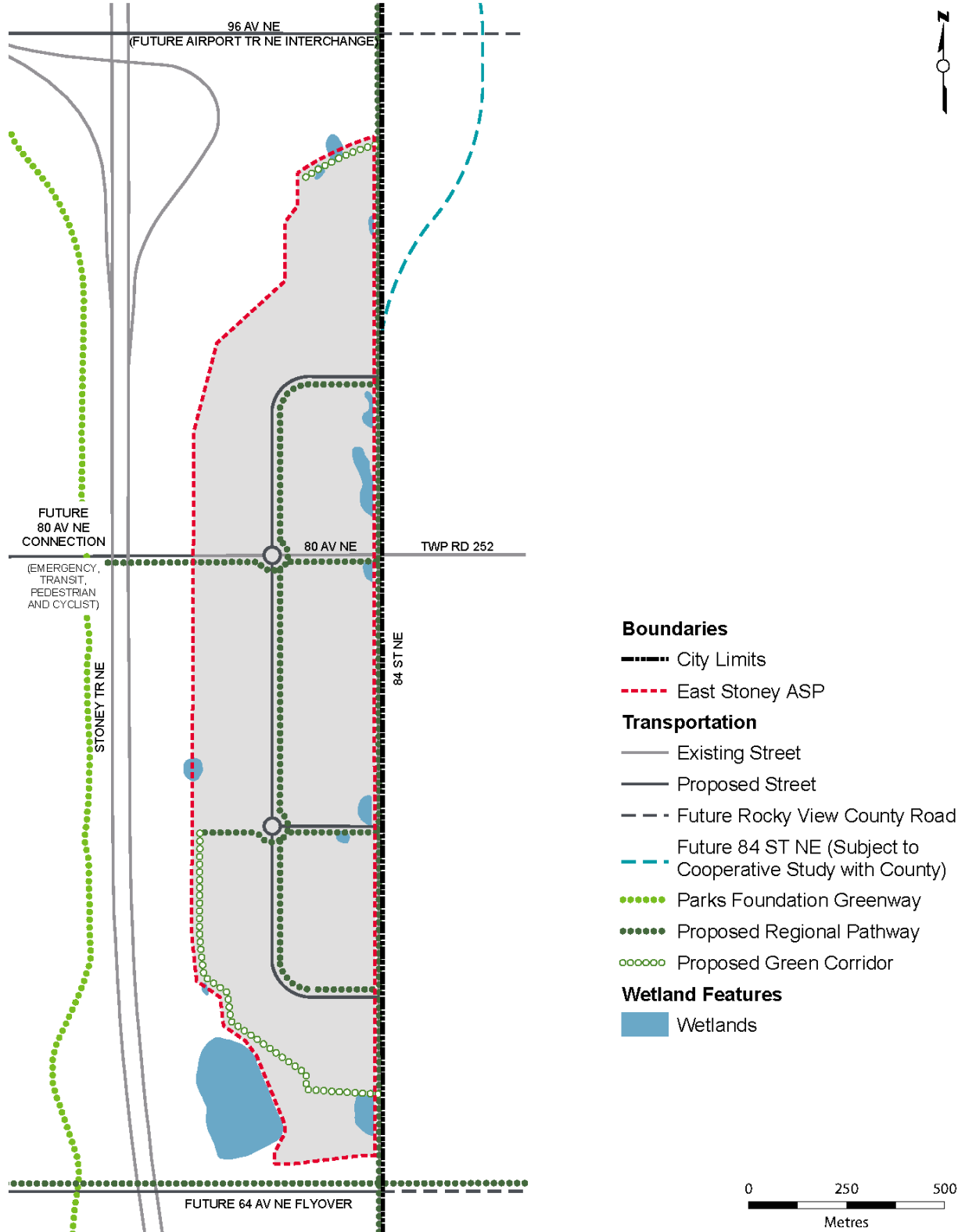
Map 2: Land Use Concept



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE C

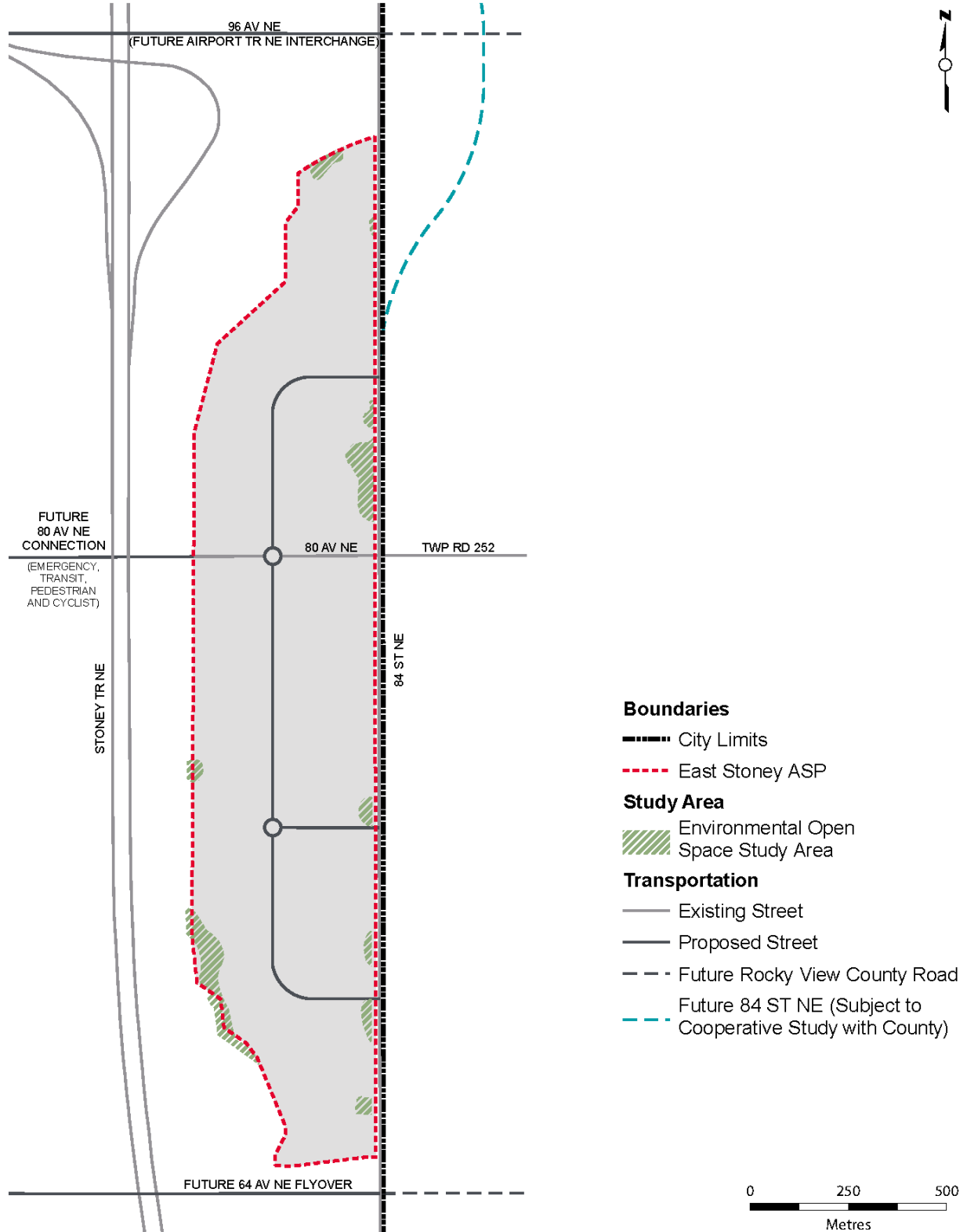
Map 3: Regional Pathways and Green Corridors



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE D

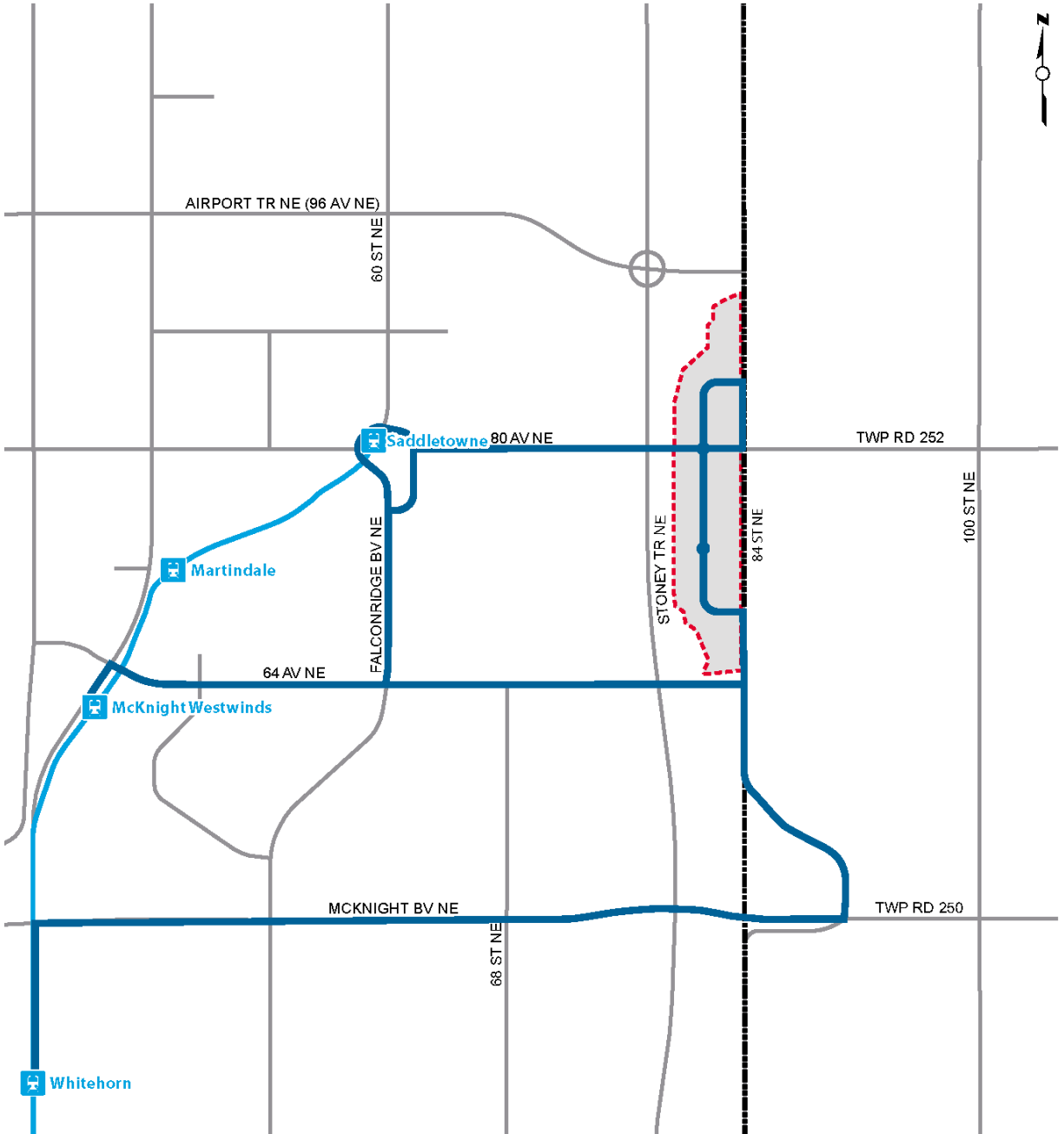
Map 4: Environmental Open Space (EOS) Study Area



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE E

Map 5: Transit Network



Boundaries

--- City Limits

- - - East Stoney ASP

Transit

— Potential Routing

— CTrain Line

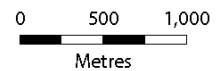
🚊 CTrain Station

Transportation

— Existing Street

- - - Future Rocky View County Road

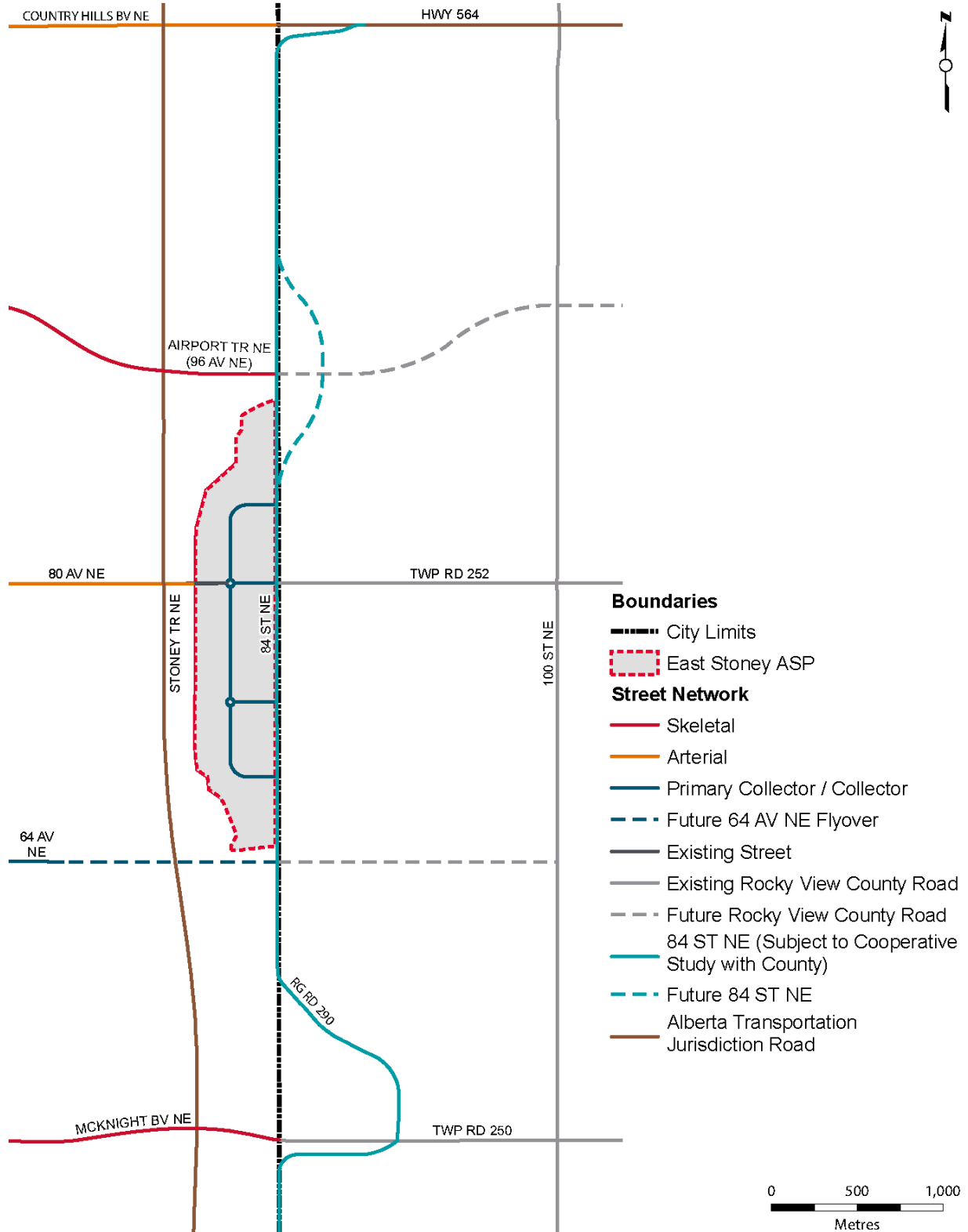
○ Future Full Interchange



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE F

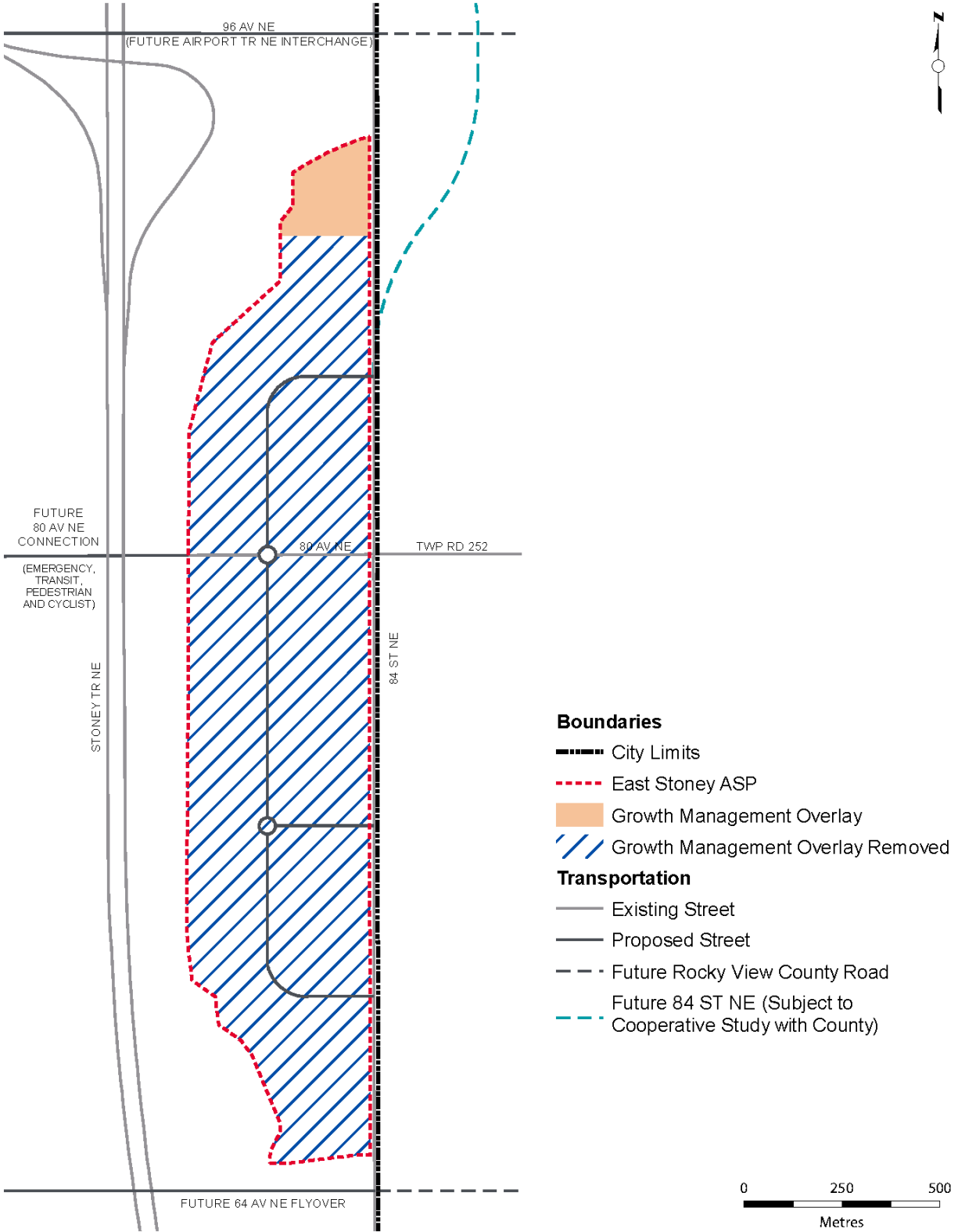
Map 6: Street Network



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE G

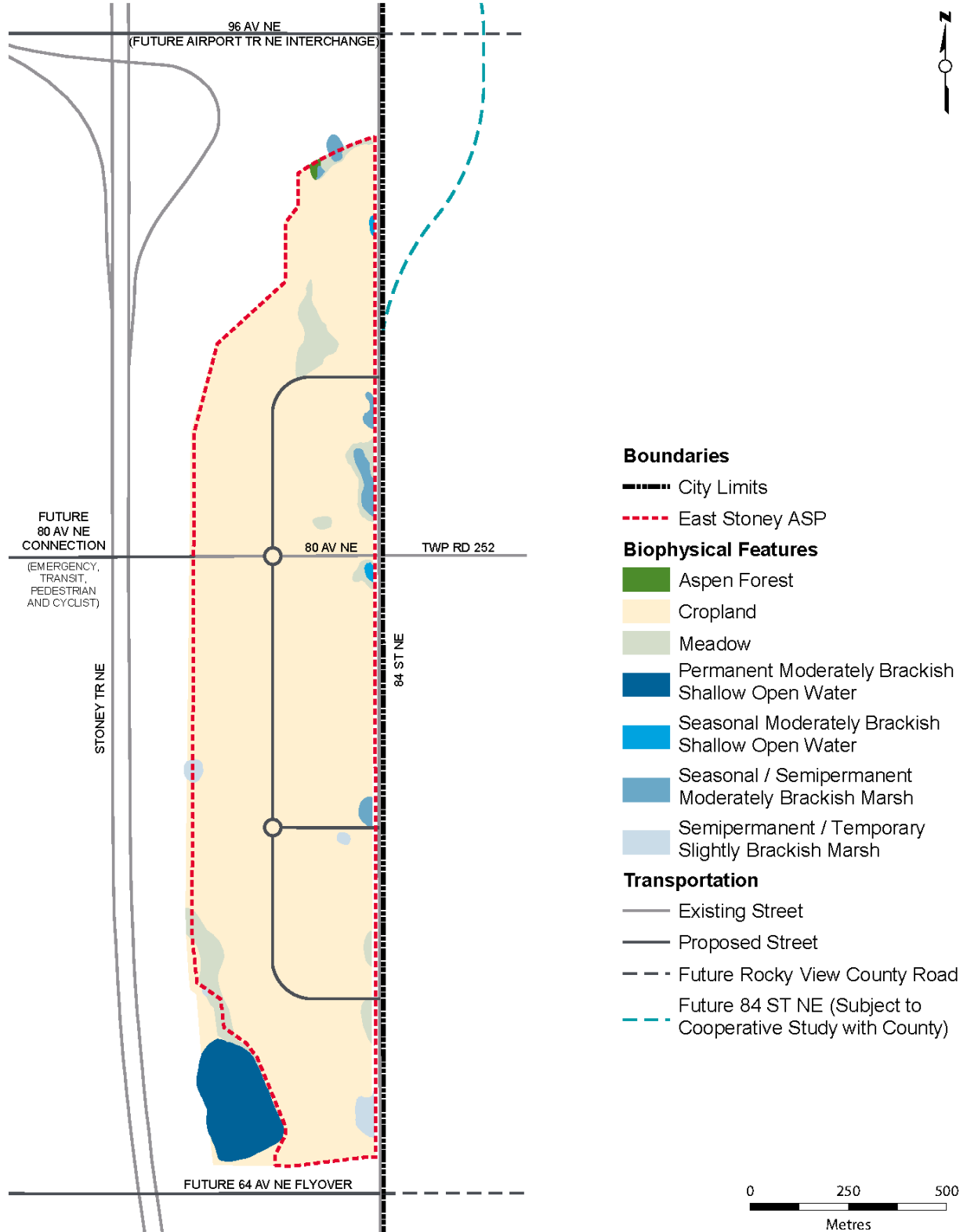
Map 7: Growth Management Overlay



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE H

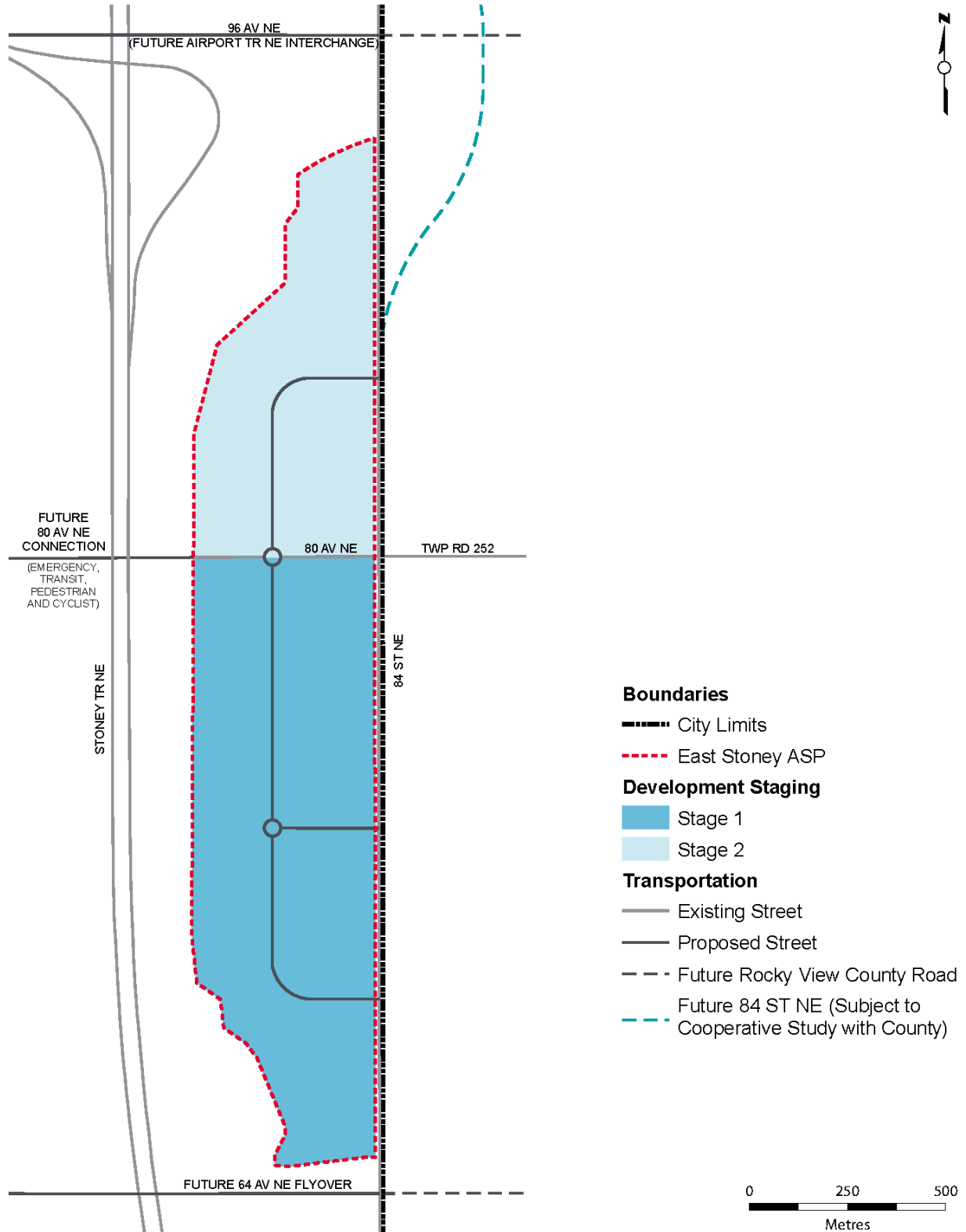
Map 8: Biophysical Features



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE I

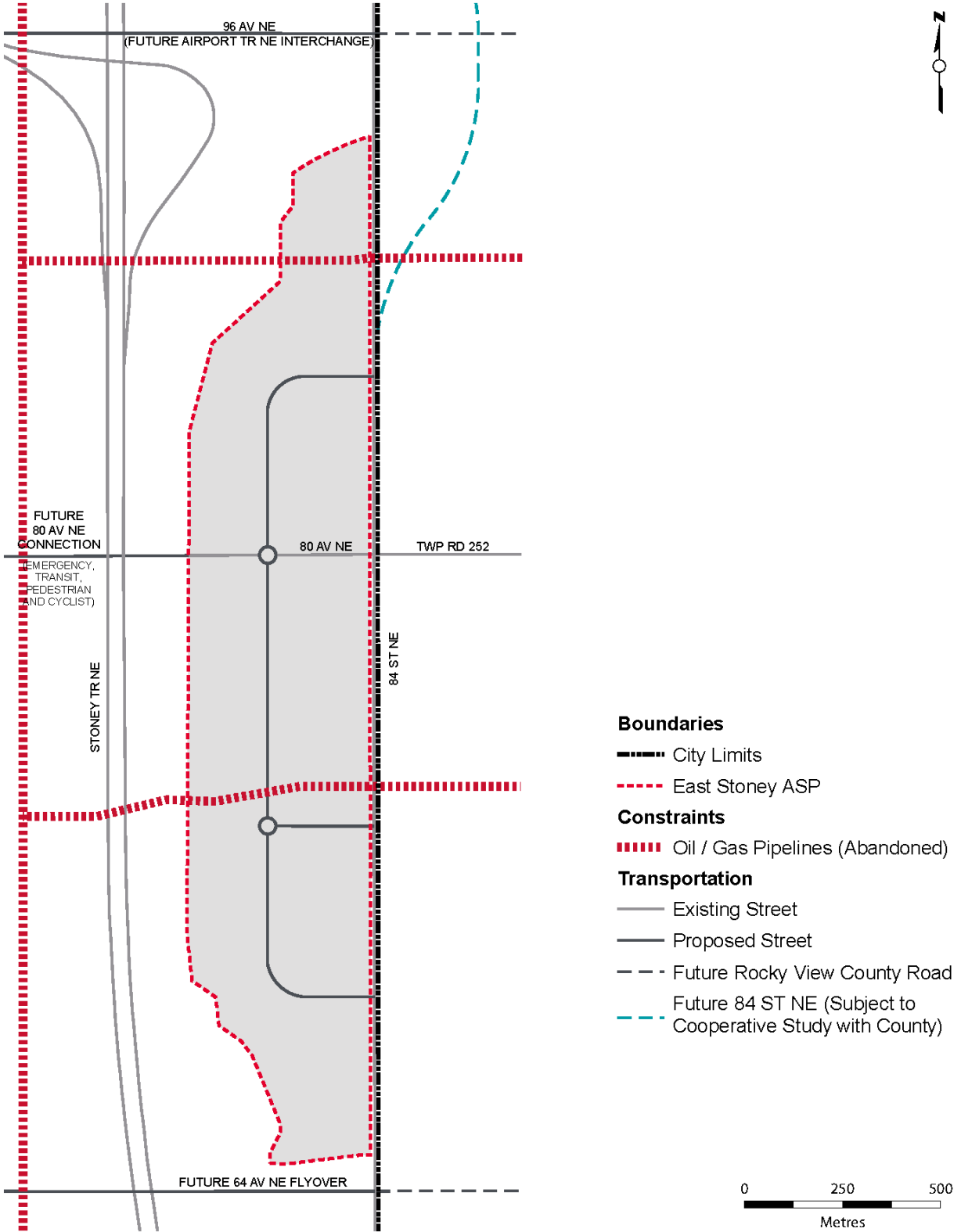
Map 9: Development Staging



Proposed Amendments to the East Stoney Area Structure Plan

SCHEDULE J

Map 10: Plan Area Constraints



Letter from Rocky View County



911 - 32 Avenue NE | Calgary, AB | T2E 6X6
Phone: 403-230-1401 | Fax: 403-277-5977
www.rockyview.ca

October 5, 2018

Morgan Huber
City of Calgary
Calgary Municipal Building
12th Floor, 800 MacLeod Trail SE
Calgary, Alberta T2G 2M3

Via email: Morgan.Huber@calgary.ca

Dear Mr. Huber,

RE: Proposed Homestead Outline Plan and Land Use Redesignation LOC2017-0111 and East Stoney ASP Amendments Circulation Response

Thank you for providing the County time to review the Homestead Outline Plan and Land Use Redesignation circulation package, received on September 5, 2018, along with the proposed East Stoney ASP amendments as provided October 1, 2018.

The County identified concerns during the initial preparation and adoption of the East Stoney ASP, primarily with respect to stormwater and traffic impacts on County infrastructure. The County was circulated on the first draft of the Homestead Outline Plan in June of 2017, and comments were provided at that time.

The County understands that the Homestead Outline Plan approval will include conditions to be satisfied prior to tentative plan approval including specific requirements for stormwater and traffic accommodation. These items will be circulated to the County for review and comment and that time.

Based on the information provided with the revised outline plan and in meetings between both Administrations, the County is satisfied that our concerns with the East Stoney ASP will be appropriately addressed through subsequent development stages.

Thank you for the opportunity to comment, and we look forward to continued cooperation with respect to the East Stoney ASP.

Regards,

ROCKY VIEW COUNTY

A handwritten signature in blue ink that reads "Jessica Anderson".

Jessica Anderson
Planning Services

/ja

CC: Neil Younger, Intergovernmental and Corporate Strategy, City of Calgary
Richard Barss, Intergovernmental Affairs, Rocky View County
Sherry Baers, Manager of Planning, Rocky View County

Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP)

EXECUTIVE SUMMARY

This application was submitted by Urban Systems Survey on behalf of Pacific Calgary GP Ltd on 2017 April 06. The application proposes a framework for the future subdivision and development of 96.15 hectares (237.59 acres) of undeveloped greenfield land in the northeast Residual Sub Area 5G. The proposed land use area is within the *East Stoney Area Structure Plan*. The application provides for:

- The development of a residential neighbourhood on lands approximately 41.63 hectares (102.87 acres) with a mix of single detached dwellings, semi-detached dwellings, duplex dwelling and rowhouse dwellings (Residential – Low Density Mixed Housing (R-G) (R-Gm) District);
- Approximately 10.09 hectares (24.93 acres) for a mobile home park comprised of manufactured housing that will facilitate the development of adequate and affordable housing, creating conditions to enable citizens from a wide economic spectrum to live within a neighbourhood and provide quick access to Stoney Trail and adjacent existing/future employment areas (Residential – Manufactured Home (R-MH) District);
- A neighbourhood activity centre consisting of a smaller commercial site providing local retail services, community facilities and integrated transit stops. The neighbourhood activity centre also includes medium density housing further defined below;
- Three multi-residential sites totalling 6.03 hectares (14.90 acres) for comprehensive townhouse or at-grade multi-residential development (Multi-Residential – At Grade Housing (M-G) District);
- A 1.60 hectare (3.95 acre) local commercial site within the neighbourhood activity centre (Commercial – Community 1 (C-C1) District);
- Approximately 0.68 hectares (1.68 acres) for a light industrial parcel to facilitate production of the manufactured and modular housing units proposed for the neighbourhood;
- Approximately 9.84 hectares (24.31 acres) of Municipal Reserve, in the form of public open space and a K-9 school site (Special Purpose – School, Park and Community Reserve (S-SPR) District);
- Preserving 1.91 hectares (4.72 acres) of land to be dedicated as Environmental Reserve (Special Purpose – Urban Nature (S-UN) District);
- 5.26 hectares (13.00 acres) for a stormpond and supporting public infrastructure (Special Purpose – City and Regional Infrastructure (S-CRI) District); and
- The location of future local and major roadways, utilities and services.

The proposed outline plan serves to implement the policies of the *Municipal Development Plan* and the *East Stoney Area Structure Plan* supporting the efficient utilization of land and infrastructure by providing a framework for the future subdivision of a new residential neighbourhood, open space and pathway network, local commercial and employment recommended in the associated land use and policy amendment application on today's agenda (CPC2018-1103).

Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP)

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission **APPROVE** the proposed outline plan located at 7055, 7111, 7697, 8393 - 84 Street (Portion of Lot 1, block 1, Plan 1612484; Portion of SE1/4 Section 12-25-29-4; Portion of NE1/4 Section 12-25-29-4; Portion of SE1/4 Section 13-25-29-4) to subdivide 96.15 hectares \pm (237.59 acres \pm), with conditions (Attachment 1).

PREVIOUS COUNCIL DIRECTION / POLICY

The direction to commence the East Stoney Area Structure Plan was provided through a Notice of Motion (NM2016-09) from Councillor Stevenson on that was approved by Council on 7 March 2016:

WHEREAS there is an EXPLORE application under review by Administration for the development of a comprehensively designed affordable housing community;

AND WHEREAS there is a need to facilitate conditions to enable citizens from a wide economic and demographic spectrum to live within a community;

AND WHEREAS access to adequate and affordable housing in the City of Calgary is a fundamental component of the quality of life in a city, as stated in Part 2, Section 2.3.1 of the Calgary Municipal Development Plan;

AND WHEREAS the subject lands are currently intended to accommodate temporary uses within residual parcels prior to comprehensive development as stated in Section 7.1 of the Rocky View/Calgary Intermunicipal Development Plan;

AND WHEREAS there is presently no local area policy for the lands in question;

AND WHEREAS Section 7.1.2 of the Rocky View/Calgary Intermunicipal Development Plan states that Residual Long-Term Growth Areas, as identified in Map 3, should be planned comprehensively through an Area Structure Plan and/or Regional Context Study with adjacent land within Rocky View County;

AND WHEREAS this Area Structure Plan will be under the developer funded model;

NOW THEREFORE BE IT RESOLVED that Council direct Administration to enter into an agreement with the landowner to fund the cost of producing an Area Structure Plan for the lands municipally and legally described as 7111, 7697, 8393 84 ST NE (SE12-25-29- W4M, NE12-25-12-W4M, SE13-25-29-W4M) and to review concurrently an application for Outline Plan and Land Use Amendment with the development of the Area Structure Plan.

**Planning & Development Report to
Calgary Planning Commission
2018 October 18**

**ISC: UNRESTRICTED
CPC2018-1104
Page 3 of 12**

Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP)

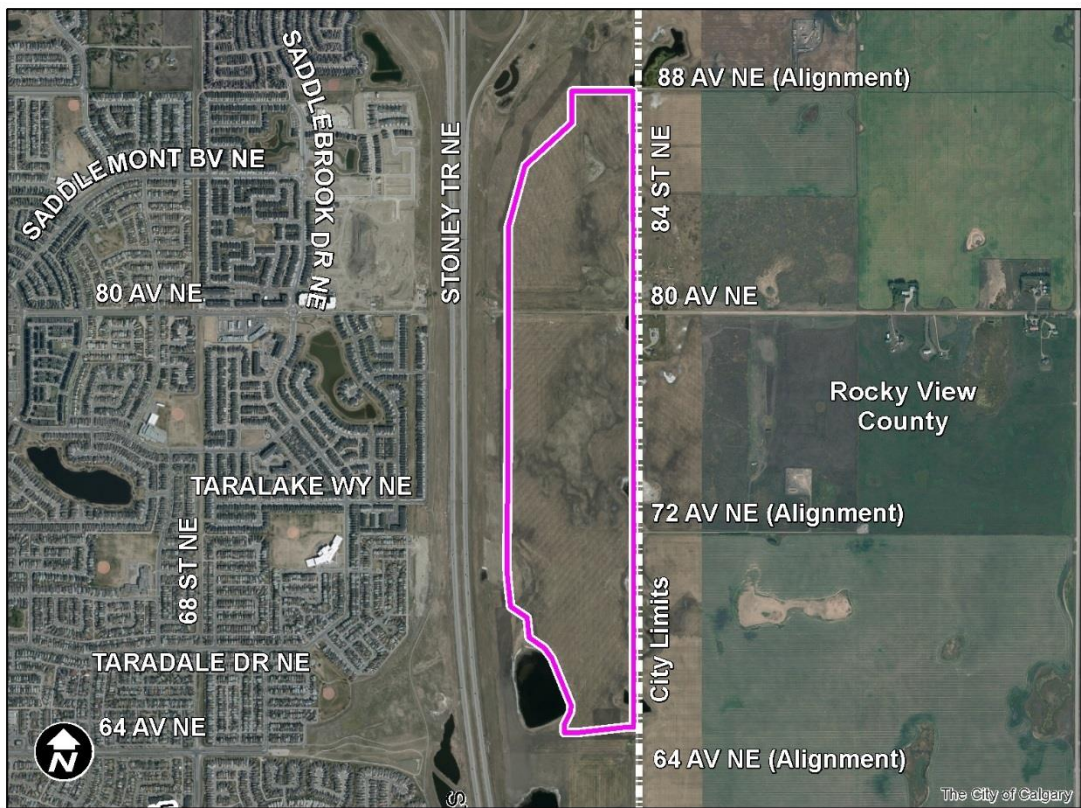
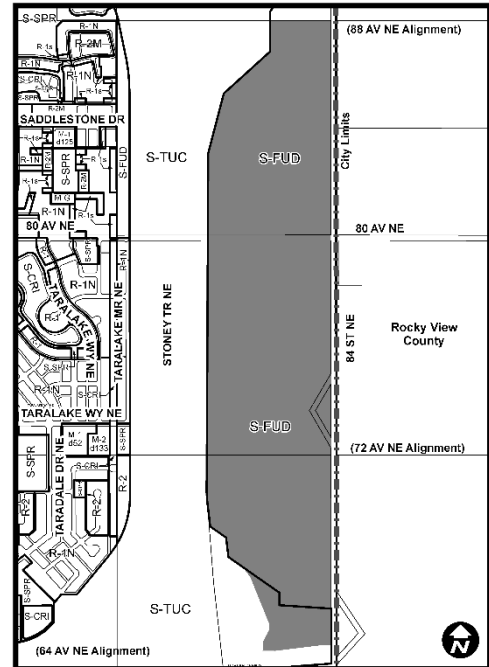
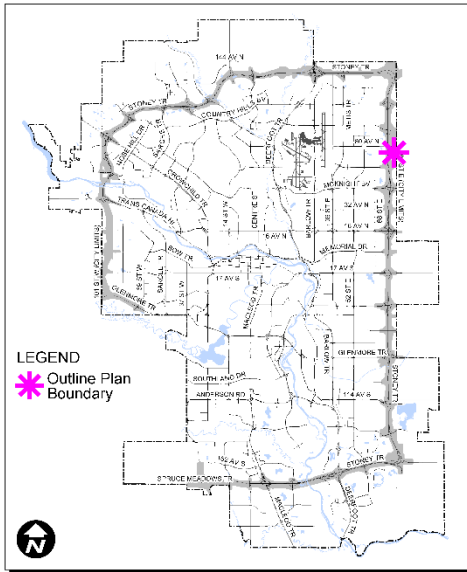
BACKGROUND

Urban Systems Survey on behalf of Pacific Calgary GP Ltd submitted the subject application to on 2017 April 06 and have provided a summary of their proposal in the Applicant's Submission (Attachment 1).

On 2017 May 08, Council adopted the East Stoney Area Structure Plan which outlined the development vision for the subject lands of this application. On 2018 July 30, Council approved a total of 14 new communities for the 2018 New Community Growth Strategy. On 2018 September 11, Council approved the first reading of the East Stoney bylaw amendment (69P2018) for Growth Management Overlay removal, and as requested by Rocky View County, withheld second and third readings to allow time for The County to review the amendment in detail and to meet with City staff to discuss the Growth Management Overlay removal and potential ancillary issues. City staff met with Rocky View County representatives to discuss The City's Growth Management Overlay process and Rocky View County's potential planning and technical issues. On 2018 October 9, Council amended the *East Stoney Area Structure Plan* to remove the Growth Management Overlay for the majority of plan area providing greater certainty to proponents, and allow for planning application reviews underway in these communities to continue moving forward.

Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP)

Location Maps



Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP)

Site Context

The subject lands 7055, 7111, 7697, and 8393 84 Street NE are situated in the northeast quadrant of the city. Located immediately east of Stoney Trail NE, the subject lands are bound on the south by 64 Avenue NE, on the north at 96 Avenue NE (Airport Trail NE), and the east by 84 Street NE. The existing communities of Saddleridge, Taradale, Martindale, and Coral Springs are located to the west.

Adjacent to the east are existing country residential and agricultural uses located in Rocky View County. The Conrich Area Structure Plan, consisting primarily of industrial uses, is located to the southeast while the OMNI Area Structure Plan is located to the northeast, consisting primarily of highway commercial and light industrial uses.

Historically, the subject lands were utilized for agricultural purposes.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This outline plan application (Attachment 3), along with the associated land use and Area Structure Plan amendment application will facilitate the development of a residential neighbourhood with a mix of uses that will provide adequate and affordable housing, creating conditions to enable citizens from a wide economic spectrum to live within a neighbourhood and have quick access to Stoney Trail and adjacent existing/future employment areas in the northeast quadrant of the City, as envisioned by the policies of the *Municipal Development Plan* and *East Stoney Area Structure Plan*.

Planning Considerations

As part of the review of this application, several key factors were considered by Administration including implementing the direction of the East Stoney Area Structure Plan, providing a condition framework that ensures key infrastructure is built and including the basic design elements that contribute to a complete neighbourhood.

Subdivision Design

The outline plan informs a future proposed subdivision that is approximately 96.15 hectares (237.59 acres) in size. Generally, the subdivision consists of residential lands and support commercial and local employment uses. The street network and design of the plan area is a modified grid network due to the challenging linear shape of the subject lands. Local residential streets and various walkways, pathways and trails provide further connectivity within the neighbourhood and the surrounding existing and proposed communities in the City and County. A unique feature of the proposed plan area is that the industrial lot on the north end is intended to house a production facility for the modular and manufactured housing in the neighbourhood and will ultimately be phased out once development of all phases in the subdivision is complete.

The plan area contains one Neighbourhood Activity Centre (NAC) that is centrally located within the neighbourhood. The NAC is intended to be a focal point for the neighbourhood that creates gathering space and accommodates a mix of transit-supportive residential and non-residential uses. The NAC is connected to nearby focal points by a network of active transportation modes

Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP)

that is designed to provide a positive pedestrian environment promoting activity in the public realm. Aligned with the policies, the proposed NAC is located adjacent to a transit stop and provides a mix of grade-oriented residential sites, neighbourhood commercial development, and a school site with playfields.

The design provides a framework to accommodate a diversity of housing forms with a combination of laned and non-laned product types. Laned products are located along the collector roadways to improve the residential street interface and access conditions with lane-accessed parking. This also reduces pedestrian-vehicular disruptions along the collectors. A mix of unit types are proposed in the plan area, including single detached homes, semi-detached units, duplex dwelling units, rowhouse units, townhouse units and apartments.

The area will contain an extensive network of regional pathways and various local walkways that connect residents to the neighbourhood activity centre, joint use site (which will be a future Kindergarten to Grade 9 school site) and a series of planned park spaces. These connections create a local network, but also tie to the greater regional network through the neighbouring communities to the west. Future connections to the north, south and east may be designed in collaboration with Rocky View County when the Airport Trail NE interchange and 64 Avenue NE flyover are constructed.

Municipal Reserve/City public park dedications will be provided in various shapes and types of uses incorporating the aforementioned school site and active and passive uses throughout the plan area. The majority of the Municipal Reserve parks are connected by the regional pathway system.

A single stormpond is proposed at the south end of the plan area located in a portion of surplus provincial TUC lands to serve the development.

Land Use

Concurrent with the outline plan is a land use amendment application (Attachment 5) that proposes to redesignate lands from Special Purpose – Future Urban Development (S-FUD) District and Special Purpose – Transportation and Utility Corridor (S-TUC) District to a mix of Residential – Low Density Mixed Housing (R-G) (R-Gm) District, Residential Manufactured Home (R-MH) District, Multi-Residential – At Grade Housing (M-G) District, Commercial Community 1 (C-C1) District, Industrial (I-G) District, Special Purpose – Community Service (S-CRI) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, and Special Purpose – Urban Nature (S-UN) District. Overall, these districts will provide for a variety of forms of residential development, a supporting parks and open space network, complementary small-scale neighbourhood commercial uses and opportunities for local employment and supporting public infrastructure.

The outline plan provides the framework for community and infrastructure build out, to be implemented largely at the subdivision stage. The land uses are complimentary to this plan and provide more certainty on densities, and unit types and forms. The final decision for the outline plan lies with Calgary Planning Commission.

Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP)

Density

The land uses proposed provides for development that achieves both the *Municipal Development Plan* and the *East Stoney Area Structure Plan* minimum density and intensity targets (population and jobs).

Development intensity for the overall community is required by the *Municipal Development Plan* and the *East Stoney Area Structure Plan* to achieve an intensity of 60 to 70 people and jobs per hectare. The subject site is anticipating an intensity of approximately 66 people and jobs per hectare, which exceeds these intensity targets.

The application proposes to accommodate a residential unit range between 1771 and 2042 units. This equates to an anticipated residential density of 18.79 units per hectare (7.6 units per acre) with a maximum residential density of 21.7 uph (8.8 upa). This density range achieves the overall intent of the minimum residential density of 20 uph (8 upa) suggested by the *Municipal Development Plan* for the neighbourhood.

Environmental

The proposed application is located within an undeveloped 'greenfield' area. Historically, the lands have been used for agricultural purposes as identified through an Environmental Site Assessment report, in which no environmental concerns were identified. No other environmental issues have been identified for the subject lands.

An existing ephemeral water body is located in the southwest corner of the outline plan area carries the proposed dedication as Environmental Reserve (Special Purpose – Urban Nature (S-UN) District) and is surrounded by a linear park that features a Green Corridor, naturalized planting and native grasses. The southern portion of the Environmental Reserve also includes a 30-metre buffer from the Provincially classified wetland outside the plan area.

Transportation

Streets and Access

The Plan Area will benefit from proximity to the provincial highway network. Primary access to the plan area is provided via 84 Street NE to McKnight Boulevard NE connecting to the regional transportation network at Stoney Trail. A future eastward extension of Airport Trail NE (96 Avenue NE) and 64 Avenue NE flyover of Stoney Trail NE will provide secondary access.

As part of the Growth Management Overlay removal process separate to this application on 2018 September 11, a crossing over Stoney Trail NE at 80 Avenue NE to accommodate access for emergency services, transit, pedestrian and cyclist connectivity to the plan area was approved by Council. This is a necessary piece of infrastructure to support this proposed development. This project will be added to the existing Offsite Levy Bylaw, with City portion of funding approved with the One Calgary 2019 – 2022 budget plan in 2018 November (PFC2018-0973). The connection of the flyover within the plan area is along Homestead Avenue NE, which is an extension of the 80 Avenue NE alignment. The City and the Developer will coordinate to

Outline Plan in Residual Sub Area 5G at multiple addresses, LOC2017-0111(OP)

complete the connection of Homestead Avenue NE to 84 Street NE in conjunction with commissioning of the flyover.

Construction and opening of Homestead Road NE, as well as the Developer's boundary half of 84 Street NE is intended to align with construction and opening of the 80 Avenue NE flyover crossing of Stoney Trail NE.

Currently, 84 Street NE is a two-lane, rural cross-section gravel road between McKnight Boulevard NE and Country Hills Boulevard NE, formerly under the jurisdiction of Rocky View County. To accommodate additional traffic on 84 Street NE, it will be necessary to upgrade to a paved road consistent with City standards. Portions of the 84 Street NE alignment are in Rocky View County, where the alignment diverts at its intersections with McKnight Boulevard NE and Country Hills Boulevard NE.

84 Street NE is subject to a joint study between the City and Rocky View County to determine road classification, long term alignment at intersections with major east-west roads, right-of-way and access management. At the time of writing of this report, the 84 Street NE study is not approved by all parties. However, the outline plan conditions of approval (CPC2018-1104) are structured to support the continued collaboration between the developer, The City, and Rocky View County, allowing Administration to bring forward a recommendation of approval for the proposed applications. Conditions around development and infrastructure staging necessary to support development and associated infrastructure are reflective of this approach.

Upgrades to boundary roads are typically funded and constructed by adjacent landowners in conjunction with their development. However, if other development has not occurred along this section of 84 Street NE, it may be necessary to advance developer-funded upgrades of contiguous sections of the road to support development in the East Stoney Area Structure Plan area. Coordination between area developers, the City, and Rocky View County is required to complete construction and servicing of new developments within the City and the County; this includes coordination by this Developer, the City, and Rocky View County in order to complete the required upgrades of 84 Street NE to service the community.

The long-term transportation plans for this area include upgrades of the interchange of Stoney Trail and Airport Trail NE (96 Avenue NE) to accommodate all movements, including an eastward extension of Airport Trail into Rocky View County. Sixty-fourth Avenue NE will also be extended eastward over Stoney Trail as a flyover. These projects are not funded at this time, and are not included in the City's current budget or 10-year capital plan. These future projects provide opportunities for better connections for active modes, transit and vehicle travel.

Transit

This area is currently not served by any transit routes. Transit service introduction to new communities is balanced with service increases in existing communities including addressing ridership growth or introducing evening and weekend service. The 80 Avenue NE flyover will facilitate a direct connection to the Blue Line LRT located at Saddletowne. Alternate transit routing options may include a connection to the Blue Line station located at Whitehorn, or local transit service connecting to other regional bus routes.

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Utilities and Servicing

Water Servicing

Water servicing of the lands within the Plan Area will be achieved through an extension of the existing water distribution network west of the Transportation Utility Corridor (TUC.) A minimum of three developer-funded water distribution mains crossing the TUC will be required to service the Plan Area.

Sanitary Servicing

To service all developable lands within the Plan Area, a developer-funded lift station will be required and will be designed to City of Calgary standards. Sanitary servicing of the lands within the Plan Area will be achieved through extensions of the existing sanitary collection network west of the Transportation Utility Corridor. A developer-funded sanitary forcemain from the proposed lift station will be connected into the existing sanitary network between Saddlebrook Common NE and Airport Trail NE (96 Avenue NE).

Stormwater Servicing

The plan area includes a proposed storm pond to contain all onsite stormwater. The pond will temporarily drain into the Coral Springs system in The City of Calgary through a pump system during off-season periods. Ultimately, the facility will be redirected to a regional facility under the Cooperative Stormwater Management Initiative (CSMI), once available. The CSMI is a collective involving The City of Calgary, The City of Chestermere, Rocky View County, The Town of Strathmore, Wheatland County and the Western Irrigation District.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

There is no Community Association for the subject area and no letters from adjacent landowners or the general public were received.

No public meetings were conducted by the applicant or Administration in direct relation to this outline plan application. However, as part of the previous Area Structure Plan work two public information sessions were held in 2016. The first information session was a "listen and learn" format, where the public had the opportunity to review and provide feedback on two possible land use concepts. The second information session was an "inform and communicate" format, where the public reviewed the revised land use concept, supporting maps, graphics, and discussed with City staff.

A project webpage was launched in 2016 April, with over 1200 page views. The webpage was used to explain the process, timelines, and engagement opportunities available for this project. It also served as a hub for supporting information and resource links.

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Due to the proximity of the subject lands to the neighboring municipality – Rocky View County, an important part of the stakeholder engagement with this application was ongoing consultation with that municipal authority. As part of the initial application process, Rocky View County was circulated the land use and outline plan. The County initially cited concerns about the transportation network and proposed stormwater solution. The County was circulated an amended land use and outline plan package once a technical review by Administration was complete. City staff met with Rocky View County representatives in 2018 September to discuss The City's Growth Management Overlay process and address Rocky View County's transportation and engineering concerns. On 2018 October 05, Administration received a letter from Rocky View County (Attachment 6) stating that they are satisfied their concerns with the *East Stoney Area Structure Plan* will be appropriately addressed through subsequent development stages as part of tentative plan and subdivision approval. Specific conditions have been included within the conditions of approval (Attachment 1) to address their concerns related to transportation impacts.

Following Calgary Planning Commission, notifications for Public Hearing of Council, in relation to the associated land use and Area Structure Plan amendments, will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)*. The site is located within the "City, Town" area as identified on Schedule C: *South Saskatchewan Regional Plan Map* in the *SSRP*. While the *SSRP* makes no specific reference to this site, the supporting application is consistent with the *SSRP* policies including the Land Use Patterns strategies (subsection 8.14) within the Implementation Plan part of the document by featuring innovative housing designs with a range of densities and housing types such as mixed-use, row-housing and low-end market manufactured and modular housing product.

Rocky View County / City of Calgary Intermunicipal Development Plan (Statutory – 2012)

The *Intermunicipal Development Plan (IDP)* identified the subject lands as "Residual Long-Term Growth Areas", until the *East Stoney Area Structure Plan* was approved on 2018 May 8. The proposed land use and outline plan comply with the general policies of interface planning, specifically the "Industrial and Residential Interface Policies".

Municipal Development Plan (Statutory – 2009)

The *Municipal Development Plan*, Map 1 "Urban Structure" identifies the subject lands as "Residential – Developing - Future Greenfield". The *Municipal Development Plan* provides guidance for the development of new communities through the policies of the *East Stoney Area Structure Plan*. The proposed application meets the following *Municipal Development Plan* objectives (Section 3.6.2):

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- Meeting minimum intensity targets;
- Providing a diversity of housing types;
- Protecting the key natural features;
- Including parks located throughout the community in walkable proximity to all residents;
and
- Creating a connected, multi-modal street network.

East Stoney Area Structure Plan (Statutory – 2017)

The *East Stoney Area Structure Plan* provides more detailed direction with detailed policies and guidelines for development. The proposed application is consistent with the applicable policies of this plan. A minor mapping amendment as part of the land use application to adjust the plan boundary is required to facilitate the development of a stormpond on the south end of the plan. This outline plan application is supported by a mapping amendment to all ten maps of the *East Stoney Area Structure Plan (CPC2018-1103)* to enlarge the plan area boundary to include the surplus Transportation Utility Corridor lands on the south end of the plan area for stormwater management.

Social, Environmental, Economic (External)

The proposed outline plan enables development of a neighbourhood that provides a future framework for a mix of housing types, various densities and preservation of natural drainage courses in the area. The development of these lands will enable citizens from a wide economic spectrum to live within a neighbourhood with adequate and affordable housing and provide quick access to Stoney Trail and adjacent existing/future employment areas in the northeast quadrant of the City.

Financial Capacity

Current and Future Operating Budget:

A large portion of operating costs required to service the proposed development are included within the current operating budget. As development proceeds, the provision of other City services such as roads, transit, parks and waste and recycling would have an operating budget impact at such time as they are provided. Projected operating costs for this development during 2019-2022 have been included in the draft One Calgary service plan and budget.

Current and Future Capital Budget:

There is no impact to the current capital budget as a result of this report. The capital investment required to construct and upgrade the required local infrastructure will be funded entirely by the developer. The proposed development will leverage existing City funded capital investment, and will require additional transportation infrastructure that has been included in the 2019-2022 budget cycle. This additional infrastructure consists of the 80 Avenue NE and Stoney Trail flyover and is to be added to the Off-site Levy Bylaw through report PFC2018-0973. The flyover will be funded by The City and through off-site levies paid by developers.

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Risk Assessment

This project lies on the periphery of The City of Calgary in an area which is currently unserved and has a still developing transportation network. The associated outline plan conditions of approval (CPC2018-1104) adequately deal with the infrastructure costs associated with development of a new neighborhood in this area. At the time of writing this report, the amended Off-site Levy Bylaw and One Calgary service plan and budget has not been approved by Council and therefore, there is not a current funding source for the full cost of the 80 Avenue NE flyover at Stoney Trail NE. If a funding source has not been approved prior to land use approval, the timing of infrastructure may not align with the timing of development. It is anticipated that the funding sources for the 80 Avenue NE flyover at Stoney Trail NE will have been approved by the end of 2018 November prior to the 2018 December 10 public hearing of Council for the land use redesignation of the subject lands.

REASON(S) FOR RECOMMENDATION(S):

The proposed outline plan serves to implement the objectives of the *Municipal Development Plan* which supports the efficient utilization of land and infrastructure by providing a framework for the future subdivision of a new residential neighbourhood, open space and pathway network, local commercial and employment recommended in the associated land use application. The proposal is also in accordance with the applicable policies of the *East Stoney Area Structure Plan*. The application also follows through on the intent of the previous Council direction for these lands.

The proposal allows for the development of adequate and affordable housing, creating conditions to enable citizens from a wide economic spectrum to live within a neighbourhood and provide quick access to Stoney Trail and adjacent existing/future employment areas in the northeast quadrant of the City.

The conditions of approval (Attachment 1) effectively implement the technical aspects of the plan area's future development.

ATTACHMENT(S)

1. Conditions of Approval
2. Applicant Submission
3. Proposed Outline Plan
4. Subdivision Data Sheet
5. Proposed Land Use District Map
6. Letter from Rocky View County

Conditions of Approval

The following Conditions of Approval shall apply:

Planning:

1. The Developer shall submit a density phasing plan with each Tentative Plan submission, showing the proposed phasing within the Outline Plan area and the projected number of dwelling units within each phase to demonstrate compliance with the minimum required densities of the Area Structure/Community Plan and Municipal Development Plan.
2. If the total area for Roads & Public Utility Lot dedication is over 30%, note that compensation in the order of \$1 for over dedication is deemed to be provided.
3. If the total area for Municipal Reserve dedication is over 10%, note that this is considered a voluntary Municipal Reserve contribution, and compensation in the order of \$1 for over dedication is deemed to be provided.
4. The Standard City of Calgary Party Wall Agreement regarding the creation of separate parcels for (semi-detached / townhouses / rowhouse) units shall be executed and registered against the titles concurrently with the registration of the final instrument.
5. Prior to approval of any affected Tentative Plan and/or submission of construction drawings, the proposed street names shall be submitted and approved, to the satisfaction of the Subdivision Authority. The new street name will need Council approval prior to endorsement of the legal plan.

Development Engineering:

6. The parcels shall be developed in accordance with the development restriction recommendations outlined in the following report(s):
 - Geotechnical Report, prepared by McIntosh-Lalani (File No ML7977), dated January 24, 2017
7. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.
8. The offsite levy for Shepard stormwater catchment will be assessed for the development area.
9. In conjunction with the applicable tentative plan, the developer will be required to enter into an agreement with The City of Calgary to pay for the capital cost and operation/maintenance costs for pumping until the 'ultimate' stormwater solution becomes available. Such agreement may include a requirement for a letter of credit to be provided to the City in an amount to be determined by Development Engineering. Additionally, the developer will be responsible for paying for the decommissioning of the temporary infrastructure and the tying into the 'ultimate' solution.

Conditions of Approval

10. Prior to endorsement of any Tentative Plan/prior to release of a Development Permit, execute a Development Agreement. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
11. Off-site levies, charges and fees are applicable. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
12. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:
 - a) Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.
 - b) Construct the onsite and offsite storm water management facilities (wet pond, wetlands, etc) to service the plan area according to the most current City of Calgary Standard Specifications Sewer Construction, Stormwater Management and Design Manual and Design Guidelines for Subdivision Servicing.
 - c) Construct a wood screening fence, chain link fence, sound attenuation fence, whichever may be required, inside the property line of the residential lots/lane/walkway/roadway/(other) where they abut the boundary of the plan area.
 - d) Construct the regional pathway within and along the boundaries of the plan area, to the satisfaction of the Director of Parks Development.
13. The developer, at its expense, subject to normal oversize, but with no other recovery from The City of Calgary, shall be required to enter into an agreement to:
 - a) Construct the underground utilities within 84 Street NE which is along the east boundary of the plan area.
 - b) Construct the surface improvements within the westerly half of 84 Street NE, which is along the east boundary of the plan area.
14. Concurrent with the registration of the final instrument, execute and register on all parcels with double frontage lots that are adjacent to a collector road, a neighbourhood boulevard, an urban boulevard, an arterial road, a skeletal road, or a Transportation Utility Corridor, a Screening Fence Access Easement Agreement with the City of Calgary. The agreement and registerable access right of way plan shall be approved by the Manager, Infrastructure Planning and the City Solicitor prior to endorsement of the final instrument. A standard template for the agreement will be provided by the Development Engineering Generalist. Prepare and submit three (3) copies of the agreement for the City's signature.

Conditions of Approval

Transportation:

15. In conjunction with the initial Tentative Plan, the Developer shall register a road plan to the satisfaction of the Director, Transportation Planning for the boundary half of 84 Street NE from Homestead Avenue NE (80 Avenue NE) to the south boundary of the Outline Plan.
16. In conjunction with the initial Tentative Plan, the Developer shall register a road plan to the satisfaction of the Director, Transportation Planning for Homestead Avenue NE (80 Avenue NE), from the TUC boundary to 84 Street NE, inclusive.
17. In conjunction with the applicable Tentative Plan, the Developer shall register a road plan to the satisfaction of the Director, Transportation Planning for the boundary half of 84 Street NE from the north boundary of the Outline Plan to Homestead Avenue NE (80 Avenue NE).
18. In conjunction with the initial Tentative Plan, the Developer shall construct the boundary half of 84 Street NE (southbound two lanes plus boulevard, sidewalk, lighting, to the ultimate condition) along the east boundary of the Outline Plan, from Homestead Avenue NE (80 Avenue NE) to 64 Avenue NE (south boundary of the Outline Plan), inclusive. 84 Street NE and ancillary works to support the roadway shall be designed and constructed at the Developer's sole expense, subject to normal oversize but no other recoveries will be provided.
19. In conjunction with the initial Tentative Plan, the Developer shall construct interim surface improvements (asphalt surface overlay) of 84 Street NE, from 64 Avenue NE (south boundary of Outline Plan) to the City of Calgary boundary (approximately 54 Avenue alignment). 84 Street NE and ancillary works to support the roadway shall be designed and constructed at the Developer's sole expense.
20. In conjunction with the initial Tentative Plan, the developer shall coordinate with the relevant stakeholders (including but not limited to Rocky View County, the City of Calgary, and adjacent land-owners in the City and County, if required) regarding the improvements in Rocky View County that will be required for 84 Street NE from The City of Calgary Boundary to McKnight Boulevard NE, inclusive, including intersection upgrades at McKnight Boulevard/Range Road 290 (84 Street NE) in accordance with the applicable policy and technical studies.
21. In conjunction with the applicable Tentative Plan, the Developer shall construct the boundary half of 84 Street NE (southbound two lanes plus boulevard, sidewalk, lighting, to the ultimate condition), from Homestead Avenue NE (80 Avenue NE) to the north boundary of the Outline Plan to the satisfaction of the Director, Transportation Planning. 84 Street NE and ancillary works to support the roadway shall be designed and constructed at the Developer's sole expense, subject to normal oversize but no other recoveries will be provided.
22. In conjunction with the initial Tentative Plan, the Developer shall construct Homestead Avenue NE (80 Avenue NE), from the TUC boundary to 84 Avenue NE to the satisfaction of the Director, Transportation Planning.

Conditions of Approval

23. In conjunction with the applicable Tentative plan or Development Permit, the Developer shall register public access easement agreements for pedestrian access through the R-MH lands, to the satisfaction of the Director, Transportation Planning.
24. In conjunction with the applicable Tentative Plan, the regional pathway (3m regional pathway) along Homestead Link, shall be designed to the satisfaction of the Director, Transportation Planning.
25. Prior to the Endorsement of the applicable Tentative Plan, the Developer shall provide a Letter of Credit for pedestrian-activated crossing signals at the intersection of Homestead Avenue and Homestead Drive, and the intersection of Homestead Drive/R-MH access. road Note that the Developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit.
26. In conjunction with the applicable tentative plan, the following bus zones (not shown on the Outline Plan drawings) must be provided to the satisfaction of Transportation:
 - Westbound 80 Av NE/Homestead Ln @ Homestead Dr NE
 - Eastbound Homestead Li @ Homestead Dr NE
 - Northbound and southbound Homestead Dr NE @ entrance to R-MH site
 - Northbound Homestead Dr @ Homestead Bv NE, and Eastbound Homestead Bv @ Homestead Dr NE (relocation of the pair shown at the south leg of Homestead Cr NE)
27. In conjunction with the applicable Tentative Plan or Development Permit for the staged construction of the road network, transit stops shall be provided to the satisfaction of the Director, Transportation Planning. All bus zones shall be located:
 - Where commercial areas are concentrated;
 - Where the grades and site lines are compatible to install bus zones; and
 - Where pedestrian walkways, pathways, and roadway crossing opportunities are provided.
28. A restrictive covenant shall be registered against the specific lot(s) identified by the Director, Transportation Planning concurrent with the final instrument prohibiting the construction of front driveways over the bus loading area(s).
29. No direct vehicular access shall be permitted to or from Stoney Trail (TUC) or 84 Street NE; and a restrictive covenant shall be registered on all applicable titles concurrent with the registration of the final instrument to that effect at the Tentative Plan stage.
30. In conjunction with the applicable Tentative Plan, no direct vehicular access shall be permitted to collector roadways for all R-G residential lots. Vehicular access shall be provided via rear lanes. Restrictive covenant shall be registered on all applicable titles concurrent with the registration of the final instrument to that effect at the Tentative Plan stage.

Conditions of Approval

31. No direct vehicular access shall be permitted to residential streets containing a regional pathway within the boulevard. Restrictive covenant shall be registered on all applicable titles concurrent with the registration of the final instrument to that effect at the Tentative Plan stage.
32. In conjunction with the applicable Tentative Plan or Development Permit, a noise attenuation study is required for the residential adjacent to 84 Street and Stoney Trail NE, certified by a Professional Engineer with expertise in the subject of acoustics related to land use planning, shall be submitted to Transportation Planning for approval.

If a screening fence is a required, concurrent with the registration of the final instrument, execute and register on all parcels with double frontage lots that are adjacent to a collector road, a neighbourhood boulevard, an urban boulevard, an arterial road, a skeletal road, or a Transportation Utility Corridor, a Screening Fence Access Easement Agreement with the City of Calgary. The agreement and registerable access right of way plan shall be approved by the Manager, Infrastructure Planning and the City Solicitor prior to endorsement of the final instrument. A standard template for the agreement will be provided by Development Engineering.

Note that where sound attenuation is not required adjacent to Arterial roadways, a uniform screening fence shall be provided, in accordance with the 2014 Design Guidelines for Subdivision Servicing.

33. All noise attenuation features (noise walls, berms, etc.), screening fence, and ancillary facilities required in support of the development shall be constructed entirely within the development boundary (location of noise walls, berms, screening fence, etc) and associated ancillary works shall not infringe onto the road right-of-ways. Noise attenuation features and screening fences shall be at the Developer's sole expense.
34. In conjunction with the applicable Tentative Plan, sidewalks along the school site frontages shall be designed and constructed as mono-walks, with a minimum width of 2.0 meters.
35. In conjunction with the applicable Tentative Plan, collector standard roads (and below) shall be built to their full width to the satisfaction of the Director, Transportation Planning.
36. In conjunction with each Tentative Plan, the Developer shall register road plans for Collector standard roadways within the subject lands to the satisfaction of the Director, Transportation Planning that provides continuous active modes and vehicle routing through the community with at least two points of public access around the Tentative Plan boundary to the arterial road network.

The continuous collector road network is required to ensure that efficient Transit routing through the plan area can be accommodated, while the tow points of access to the plan area ensures residents will have two routes into and out of the area, in the event of emergency or road closures, and the ensure availability of capacity at the plan area access points.

Conditions of Approval

37. In conjunction with the applicable Tentative Plan, detailed engineering drawings and turning templates shall be submitted and approved to the satisfaction of the Director, Transportation Planning for all roadways within the plan area, as well as boundary roads. Construction drawing review may require changes to proposed right-of-way to meet the approved design.
38. In conjunction with the applicable Tentative Plan, all roads and intersections within the plan area shall be located, designed, and constructed at the Developer's sole expense to the satisfaction of the Director, Transportation Planning.
39. In conjunction with the applicable Tentative Plan or Development Permit, access to multi-family and commercial sites shall be located and designed to the satisfaction of the Director, Transportation Planning.
40. Temporary oil and gravel bus turnaround / cul-de-sac with a minimum radius of 15.25 meters is required at the terminus of each construction phase. Where the developer intends to fence the turnaround, the minimum radius shall be increased to 16.25 meters. If road construction and/or construction phasing affects the operations of transit service, the Developer is required to provide an interim transit route replacement, to the satisfaction of the Director, Transit and the Director, Transportation Planning.
41. In conjunction with the applicable Tentative Plan, the Developer shall provide signage within the road right-of-way or on city public land, indicating the future road extension of 84 Street NE and Homestead Avenue NE into adjacent lands. Signage shall be designed and located to the satisfaction of the Director, Transportation Planning. All work associated with the supply and installation of the signage will be at the Developer's sole expense.
42. In conjunction with the applicable Tentative Plan or Development Permit, the Developer shall enter into a Construction Access Roads Agreement with Roads Maintenance.
43. In conjunction with the applicable Tentative Plan or Development Permit, all community entrance features must be located outside the public right-of-way.
44. Prior to approval of construction drawings and permission to construct surface improvements, the Developer shall provide signed copies of back sloping agreements (and Ministerial Consent, if applicable) for any back sloping that is to take place on adjacent lands.
45. The Transportation/Utility Corridor (TUC) shall be permanently and prominently signed in accordance with City Councils' policy and it shall also be clearly identified on the Land Use Sign for the area.

Parks:

46. All landscape construction shall be in accordance with the City of Calgary Parks Development Guidelines and Standards Specifications: Landscape Construction (current version).

Conditions of Approval

47. The developer shall submit Landscape Construction Drawings for all reserve lands (MR/ER/MSR) to Parks for review. For further information, contact Parks Development Coordinator at 403-268-2367.

With the submission of Landscape Construction Drawings, the developer shall include a detailed Restoration Plan including a maintenance schedule for each Environmental Reserve proposed to be affected by any construction. The Plan should indicate how it will be rehabilitated and restored. The restored area(s) shall be maintained by the developer until it is established and approved by Parks prior to Final Acceptance Certificate.

48. **Prior to the approval of the affected tentative plan**, the developer shall submit Landscape Concepts for all reserve lands (MR/ER/MSR) to Parks for review. Refer to the City of Calgary Parks Development Guidelines and Standards Specifications for Landscape Construction (current version), Section 2.1 Concept Plan Requirements for more information.

OR

Prior to the approval of the affected tentative plan, the developer shall submit Landscape Construction Drawings for all reserve lands (MR/ER/MSR) to Parks for review.

49. **Prior to the approval of the affected tentative plan**, the developer shall confirm fencing requirements adjacent to reserve lands (MR/ER/MSR) to the satisfaction of Parks.
50. **Prior to approval of the tentative plan or stripping and grading permit** (whichever comes first), it shall be confirmed that grading of the development site will match the existing grades of adjacent Environmental Reserve lands, with all grading confined to the private property, unless otherwise authorized by Parks.
51. **Prior to approval of the tentative plan or stripping and grading permit** (whichever comes first), an onsite meeting shall be arranged to confirm that the surveyed boundaries of the Environmental Reserve lands, to the satisfaction of Parks. A plan illustrating the surveyed Environmental Reserve boundaries must be provided to Calgary Parks in advance of the onsite meeting.
52. **Prior to the approval of the affected tentative plan**, any proposed disturbance within the Environmental Reserve, including that for roadways, utilities, and storm water management infrastructure, shall be identified and approved by Parks.
53. **Prior to release of the stripping and grading permit**, the developer shall install protection fencing completely within private property along the shared property line with adjacent Environmental Reserve lands. An onsite meeting shall be arranged to confirm that the fencing has been installed to the satisfaction of Parks Development Inspector. The protection fencing shall be maintained along shared property line until construction activity of the subject site has been completed. Contact the Parks Development Inspector at 403-268-1358.

Conditions of Approval

54. In alignment with the Environmental Reserve Setback Guidelines, the crown-claimed Class V wetland located along the SW border of the subject site shall have a 30m Environmental Reserve (ER) setback applied, with no encroachments permitted.
55. Pursuant to Part 4 of the *Water Act* (Alberta), the applicant shall promptly provide Parks with a copy of the *Water Act* approval, issued by Alberta Environment, for the proposed wetland disturbance.
56. Until receipt of the *Water Act* approval by the applicant from Alberta Environment, the wetland(s) affected by the development boundaries shall not be developed or disturbed in any way and shall be protected in place.
57. Construct all regional pathway routes within and along the boundaries of the plan area according to Parks' Development Guidelines and Standard Specifications – Landscape Construction (current version), including setback requirements, to the satisfaction of Parks.
58. Plant all public trees in compliance with the approved Public Landscaping Plan.
59. The developer shall restore, to a natural state, any portions of the Environmental Reserve lands along the boundaries of the plan area that are damaged in any way as a result of this development. The restored area is to be maintained until established and approved by the Park Development Inspector.
60. Any development or grading related to permanent disturbance which results from storm water infrastructure within lands designated as Environmental Reserve, requires approval from Parks.
61. Drainage from the development site onto reserve lands (MR/ER/MSR) and is not permitted, unless otherwise authorized by Parks.
62. Construction access through Environmental Reserve lands is not permitted, unless otherwise authorized by Parks.
63. Stockpiling and/or dumping of construction materials on Environmental Reserve lands is not permitted.
64. Retaining walls placed within reserve lands (MR/ER/MSR) is not permitted, unless otherwise authorized by Parks.
65. Backsloping of private lots into reserve lands (MR/ER/MSR) is not permitted, unless otherwise authorized by Parks.

Applicant Submission

This application for land use redesignation and outline plan approval is located within the East Stoney Area Structure Plan (ASP). The East Stoney ASP was presented to Calgary Planning Commission on March 9, 2017 and will be presented to Council for public hearing on May 8, 2017.

It was prepared to guide the development of +/- 96 ha (238 acres) of land in NE Calgary. The document sets the framework for future development of the community that provides a mix of uses consistent with the City of Calgary's growth objectives.

The subject lands are located along the east boundary of Stoney Trail NE and bordering Rocky View County to the east at 84th Street NE. The property consists of +/- 96.19 hectares (+/- 237.69 acres) and is owned by Pacific Investments and Development Ltd.

The current land uses within the outline plan area are Special Purpose (S-FUD) District and Transportation and Utility Corridor (S-TUC) District. The lands have historically been utilized for farming and have never been developed. The lands are generally flat with surface drainage currently flowing from the west to the east draining to an existing wetland bordering the subject lands to the south. There is a small strip of environmentally sensitive natural grass lands and creek that border the south west boundary of the site. There are no significant historical features within the plan boundary.

The East Stoney community will be developed as a residential area in the northeast limits of Calgary, consistent with the ASP policies. Density and intensity targets specified within the Municipal Development Plan (and New Community Planning Guidebook) are achieved.

The site will be developed in four phases starting at the southeast corner of the site. Development phasing will be based on market demand and logical extensions of infrastructure to the site.

Low density residential will be composed of R-G uses in the form of either single family residential, villas brownstones or townhomes. Semi-detached and "brownstone" homes will be located along the Collector Street for enhanced accessibility and walkability within the community.

One Manufactured home parcel is proposed for a manufactured home park.

Three medium density M-G parcels are proposed. The M-G parcels will be developed a comprehensively designed at grade homes which compliment both the NAC and the manufactured home development.

A small industrial I-G parcel has been located at the north access of the development to provide for local industrial services within the plan area. It is proposed that a beverage recycling depot will be on this site. Proper licenses for this facility have already been secured by the developer.

At the NAC is a local commercial site which will serve the convenience needs of the neighbourhood. This NAC is located along 84th Street NE yet still internal to the community to serve residents and to meet the criteria for NAC's within City policies.

Open spaces are provided throughout the East Stoney community. Green space is linked by the regional pathway system that has excellent connectivity within the community, and meet Municipal Reserve dedication requirements.

A joint use site (JUS) of 4.86 ha (12 acres) is provided in the north central area of East Stoney in accordance with the East Stoney ASP. The JUS site was relocated north to allow for better

Applicant Submission

distribution of open space, closer proximity to medium density residential, and efficiency of transit service and for development staging purposes.

All streets within the outline plan are sized to meet forecast transportation capacities. Complete Streets Guidelines have been applied to the plan. Transit service is shown to be along north/south collector with transit stops adequately spaced. Transit will service the area through a looped route that ultimately will connect to the future LRT station to the northeast.

A Sanitary Servicing Study for East Stoney was completed by Kellam Berg Engineering & Surveys Ltd. as part of the Area Structure Plan approvals. At the time of study, existing downstream sanitary sewer had sufficient capacity for full build out of the ASP lands and no upgrades were required.

Stormwater servicing for the Outline Plan area will be provided by construction of a stormwater management facility at the south end of the plan and will be sized to handle runoff from all lands within the ASP boundary. There will be two manufactured housing parcels created within the development, each of which will be required to construct a private stormwater management facility. The entire development will be serviced with gravity storm sewer mains that drain towards the main stormwater facility at the south end of the plan.







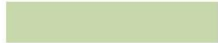


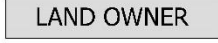





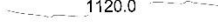





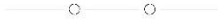




A formal application for community and street names will follow under a separate application.

On behalf of Pacific Investments & Developments Ltd., Urban Systems respectfully requests approval of the outline plan from the Calgary Planning Commission and approval for the land use to be redesignated to single family, multi-family, multi-residential, commercial, industrial and special purpose districts by City Council.

- Urban Systems

Proposed Outline Plan

OUTLINE PLAN LEGEND

	EAST STONEY AREA STRUCTURE PLAN BOUNDARY
	OUTLINE PLAN BOUNDARY
	NAC BOUNDARY
	EXISTING LAND USE
	PROPOSED LAND USE
	ENVIRONMENTAL RESERVE
	MUNICIPAL RESERVE
	MUNICIPAL SCHOOL RESERVE
	PUBLIC UTILITY LOT
	LAND OWNER
	84 STREET ROAD WIDENING (5.0m)
	SEPARATE SIDEWALKS
	REGIONAL PATHWAY (3.0m)
	GREEN CORRIDOR (3.0m)
	PATHWAY/MAINTENANCE ACCESS (3.5m)
	EXISTING CONTOUR
	ROAD DESIGNATION
	STORM SEWER LINE
	SANITARY SEWER LINE
	WATER LINE
	CHAIN LINK FENCE
	WOOD FENCE
	BUS PAD LOCATION
	RIGHT TURNS-IN AND RIGHT TURNS-OUT ONLY
	ACCESS /EGRESS TO SITES
	POND MAINTENANCE ACCESS

Proposed Outline Plan

OUTLINE PLAN STATISTICS

	ha.	ac.	%
Gross Developable Area	96.15	237.59	
Less			
S-UN (Urban Nature District)	1.91	4.72	
Net Developable Area	94.24	232.87	100
R-G (Residential - Low Density Mixed Housing District)	36.52	90.24	39
R-Gm (Residential - Low Density Mixed Housing District)	5.11	12.63	5
R-MH (Residential Manufactured Home District)	10.09	24.93	11
M-G (Multi-Residential - At Grade Housing District)	6.03	14.90	6
I-G (Industrial - General District)	0.68	1.68	1
C-C1 (Commercial Community 1 District)	1.60	3.95	2
S-SPR (Special Purpose - School, Park and Community Reserve District)	9.84	24.31	10
(MSR) 4.85ha / 11.98ac + (MR) 4.99ha / 12.33ac			
S-CRI (PUL) (Special Purpose - City and Regional Infrastructure District)	5.26	13.00	6
ROADS	19.11	47.22	20
<i>Road Widening (84 Street NE)</i>	<i>1.17</i>	<i>2.89</i>	
<i>Road - Original Road Allowance (20.12m)</i>	<i>0.44</i>	<i>1.09</i>	
<i>Road - Collector Street (21.0m)</i>	<i>0.57</i>	<i>1.41</i>	
<i>Road - Modified Collector Street (22.0m)</i>	<i>5.77</i>	<i>14.26</i>	
<i>Road - Modified Residential (18.7m)</i>	<i>0.15</i>	<i>0.37</i>	
<i>Road - Residential (16.0m)</i>	<i>8.50</i>	<i>21.00</i>	
<i>Road - Lanes (7.0m) & Walkways (3.0m)</i>	<i>2.51</i>	<i>6.20</i>	
	94.24	232.91	100

Proposed Outline Plan

DENSITY ANALYSIS					
Density Calculations	Frontage (m)	Hectares/ Acres	Average Lot Width (m)	Anticipated Units	Maximum Units
R-G - Single Family	10047.62	36.52 / 90.24	8.7	1154	1154
R-Gm - Single Family	1551.52	5.11/ 12.63	6.7	231	231
R-MH - Residential Manufactured Home District (9.8 Minimum / 13.7 Maximum)	2068.72	10.09 / 24.93	11.8	175	175
M-G - (35 min uph / 80 max uph)	N/A	6.03 / 14.90	N/A	211	482
TOTAL UNITS				1771	2042
Anticipated Density = 1771 / 94.24 ha (232.87 ac) = 18.79 uph / 7.6 upa					
Maximum Density = 2042/ 94.24 ha (232.87 ac) = 21.7 uph / 8.8 upa					
OUTLINE PLAN STATISTICS					
PEOPLE					
Product Type	Units	Occupancy Rate	People		
R-G - Single Family	1154	3.3	3808		
R-Gm- Single Family	231	3.3	762		
R-MH - Residential Manufactured Home District	175	3.3	577		
M-G - (57.5 uph Average)	346 (Average)	2.2	761		
TOTAL PEOPLE				5908	
JOBS					
Employment Type	Area ha / ac	Intensity Assumptions	Number of Jobs		
HOME BASED	57.75 / 142.70	5908 People / 100 = 59.08 x 3.8 jobs	225		
C-C1	1.60 / 3.95	40 Jobs / ha	64		
I-G (1.0 FAR)	0.54 / 1.33	25 Jobs / ha	14		
SPR (MSR)			50		
TOTAL JOBS				353	
Total People & Jobs = 5908+ 353 = 6261 People and Jobs					
Total People & Jobs / ha = 6261 People & Jobs / 94.24 ha = 66 People and Jobs / hectare					
NAC STATISTICS					
NAC Area	AREA (ha / ac)				
	7.13 / 17.62				
PEOPLE CALCULATION					
Residential Type	Units	Occupancy Rate	Number of People		
M-G - (average 57.5 uph)	254 (average)	2.2	560		
TOTAL NUMBER OF PEOPLE				560	
EMPLOYMENT CALCULATION					
Employment Type	ha / ac Gross Floor Area (m ²)	Total Number of People	Intensity Assumption	Number of Jobs	
MR - Community Centre)	-	-	-	0	
Home Based	3.8 Jobs / 100 people	560	737 People/100 = 7.4 x 3.8 jobs	21	
C-C1	1.60 / 3.95 ac		40 Jobs / ha	64	
TOTAL JOBS				85	
Total People & Jobs = 560 + 85 = 645 People and Jobs					
Total People & Jobs / ha = 645 People / 7.13 ha = 91 People and Jobs / ha					

Proposed Outline Plan



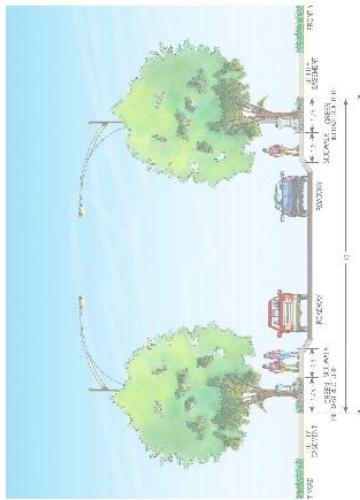
Proposed Outline Plan

LAND USE PLAN

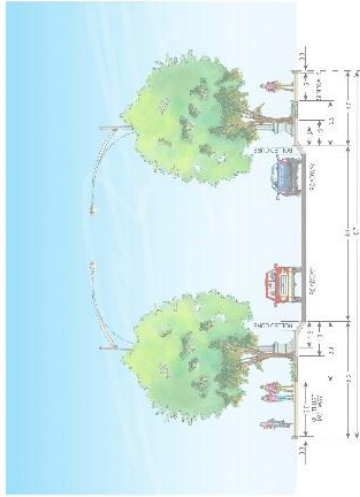
	Hectares	Acres	%
Gross Area	96.15	237.59	
S-FUD to R-G (Residential - Low Density Mixed Housing District)	49.94	123.40	52
S-FUD to R-Gm (Residential - Low Density Mixed Housing District)	7.95	19.64	8
S-FUD to R-MH (Residential Manufactured Home District)	10.46	25.85	11
S-FUD to M-G (Multi-Residential - At Grade Housing District)	6.66	16.46	7
S-FUD to C-C1 (Commercial Community 1 District)	1.97	4.87	2
S-FUD to I-G (Industrial - General District)	0.91	2.25	1
S-TUC to S-CRI (PUL) (Special Purpose - Community Service District)	3.57	8.82	4
S-FUD to S-CRI (PUL) (Special Purpose - Community Service District)	1.82	4.50	2
S-FUD to S-SPR (Special Purpose - School, Park and Community Reserve District)	10.93	27.01	11
S-TUC to S-SPR (Special Purpose - School, Park and Community Reserve District)	0.03	0.07	0
S-FUD to S-UN (Urban Nature District)	1.91	4.72	2

Proposed Outline Plan

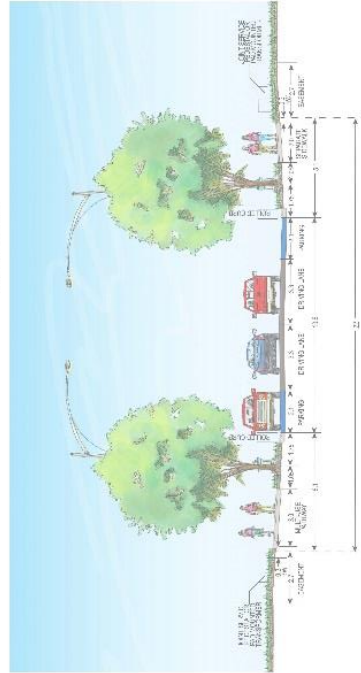
ROAD CROSS SECTIONS



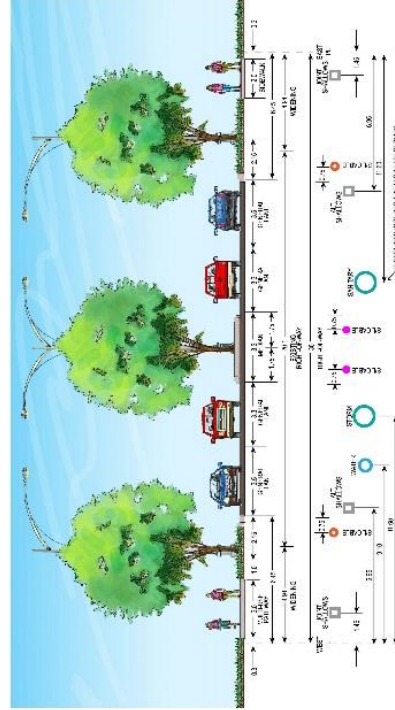
16.0m Residential Street
Scale 1:200



18.7m Modified Residential Street
Scale 1:200



22.0m Modified Collector Street
Scale 1:200



84 STREET NE - 30.0m Modified Local Arterial
Scale 1:200

Subdivision Data Sheet

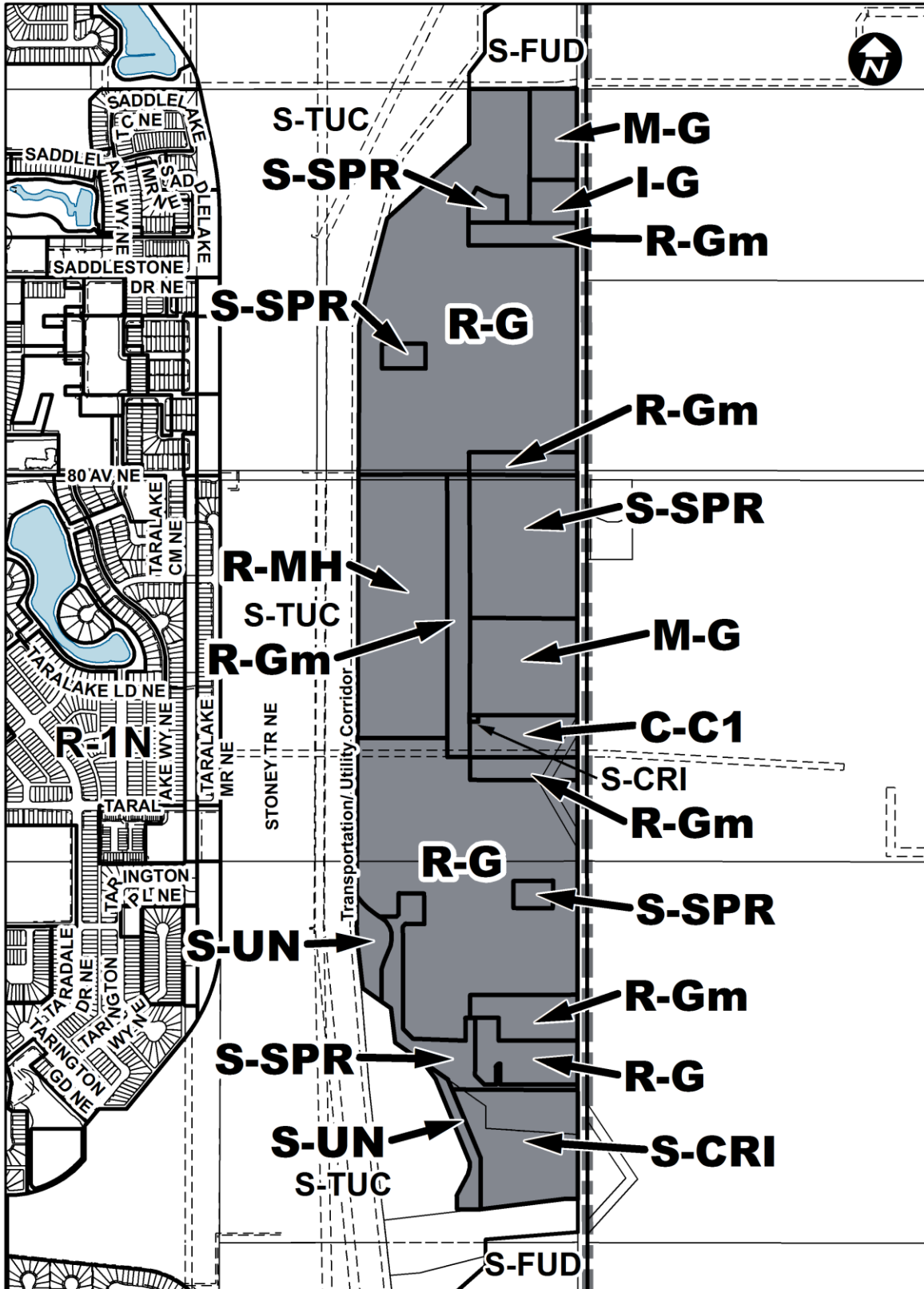
	HECTARES	ACRES
GROSS AREA OF PLAN	96.15	237.59
LESS: ENVIRONMENTAL RESERVE	1.91	4.72
NET DEVELOPABLE AREA	94.24	232.87

LAND USE (Residential)	HECTARES	ACRES	# OF LOTS	# OF UNITS (FOR MULTI RESIDENTIAL)
R-G	36.52	90.24	1154	-
R-Gm	5.11	12.63	231	-
R-MH	10.09	24.93	1	175
M-G	6.03	14.90	3	211
Total Residential	57.75	142.7	1391	-
(Non-Residential)				
C-C1	1.60	13.95	1	-
I-G	0.68	1.68	1	-
Total Non-Residential	2.3	16.58	2	-

	HECTARES	ACRES	% OF NET AREA
ROADS (CREDIT)	19.11	47.22	20%
PUBLIC UTILITY LOT	5.26	13.00	6%

RESERVES	HECTARES	ACRES	% OF NET AREA
MR	4.99	12.33	5%
MSR	4.85	11.98	5%

Proposed Land Use District Map



Letter from Rocky View County



911 - 32 Avenue NE | Calgary, AB | T2E 6X6
Phone: 403-230-1401 | Fax: 403-277-5977
www.rockyview.ca

October 5, 2018

Morgan Huber
City of Calgary
Calgary Municipal Building
12th Floor, 800 MacLeod Trail SE
Calgary, Alberta T2G 2M3

Via email: Morgan.Huber@calgary.ca

Dear Mr. Huber,

RE: Proposed Homestead Outline Plan and Land Use Redesignation LOC2017-0111 and East Stoney ASP Amendments Circulation Response

Thank you for providing the County time to review the Homestead Outline Plan and Land Use Redesignation circulation package, received on September 5, 2018, along with the proposed East Stoney ASP amendments as provided October 1, 2018.

The County identified concerns during the initial preparation and adoption of the East Stoney ASP, primarily with respect to stormwater and traffic impacts on County infrastructure. The County was circulated on the first draft of the Homestead Outline Plan in June of 2017, and comments were provided at that time.

The County understands that the Homestead Outline Plan approval will include conditions to be satisfied prior to tentative plan approval including specific requirements for stormwater and traffic accommodation. These items will be circulated to the County for review and comment and that time.

Based on the information provided with the revised outline plan and in meetings between both Administrations, the County is satisfied that our concerns with the East Stoney ASP will be appropriately addressed through subsequent development stages.

Thank you for the opportunity to comment, and we look forward to continued cooperation with respect to the East Stoney ASP.

Regards,

ROCKY VIEW COUNTY

A handwritten signature in blue ink that reads "Jessica Anderson".

Jessica Anderson
Planning Services

/ja

CC: Neil Younger, Intergovernmental and Corporate Strategy, City of Calgary
Richard Barss, Intergovernmental Affairs, Rocky View County
Sherry Baers, Manager of Planning, Rocky View County

Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15 Street SW, LOC2018-0095

EXECUTIVE SUMMARY

This application was submitted by Inertia on 2018 April 27 on behalf of the landowners, Huijin Ouyang and Bijun Wu. This application proposes to change the designation of a parcel to allow for:

- Multi-residential buildings (e.g. townhouses, fourplexes);
- A maximum building height of 12 metres (an increase from the current maximum of 10 metres);
- A maximum of 5 dwelling units (an increase from the current maximum of 2 dwelling units), based on a density of 111 dwelling units per hectare, and;
- The uses listed in the proposed M-CG designation.

This proposal is compatible with the applicable policies identified in the *Municipal Development Plan*, the *Bankview Area Redevelopment Plan* and meets many of the *Location Criteria for Multi-Residential Infill*. To accommodate this proposal, a minor amendment to the *Bankview Area Redevelopment Plan* is required, changing the subject site from “Conservation” to the “Medium Low Density” typology, as described in Attachment 3.

A development permit for 2502 - 15 Street SW has been submitted by Inertia on 2018 June 20 and is under review. See Attachment 4 for additional information.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to the Bankview Area Redevelopment Plan (Attachment 3);
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares ± (0.12 acres ±) located at 2502 - 15 Street SW (Plan 3908R, Block 2, Lots 17 and 18) from Residential Contextual One / Two Dwelling (R-C2) District to Multi-Residential – Contextual Grade-Oriented (M-CG) District; and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15 Street SW, LOC2018-0095

BACKGROUND

The subject site is located in the community of Bankview, south of 24 Avenue SW and east of 15 Street SW. A four storey apartment building is located to the west and north, on parcels designated as Multi-Residential Contextual Medium Profile (M-C2) District. Parcels to the east and south contain single detached dwellings, designated as Residential Contextual One/Two Dwelling (R-C2). The parcel located three parcels south of subject site at 2516 - 15 Street SW was recently redesignated to from R-C2 to M-C2. It should be noted that there is an existing legal non-conforming apartment building located two parcels south of the subject site at 2512 - 15 Street SW.

The subject site is currently developed with a small, late 1940's single detached dwelling. Topographically, the subject site is highest on its west boundary and slopes downward to the east.

As identified in *Figure 1*, the community of Bankview has experienced a population decline from its peak in 1981

Figure 1: Community Peak Population

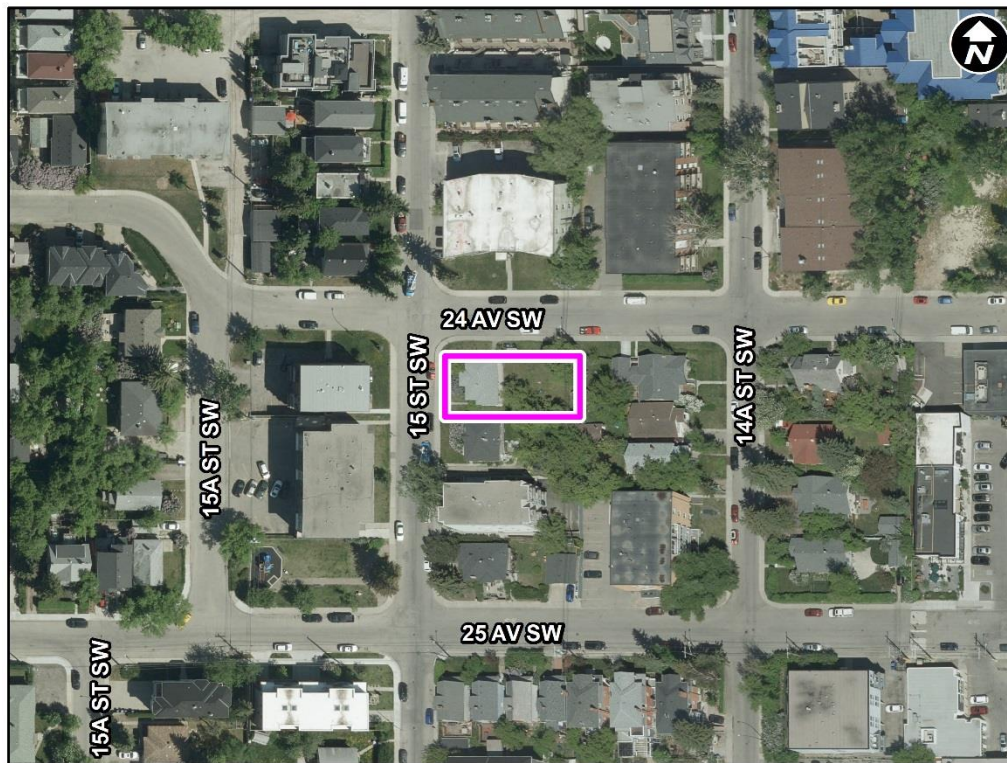
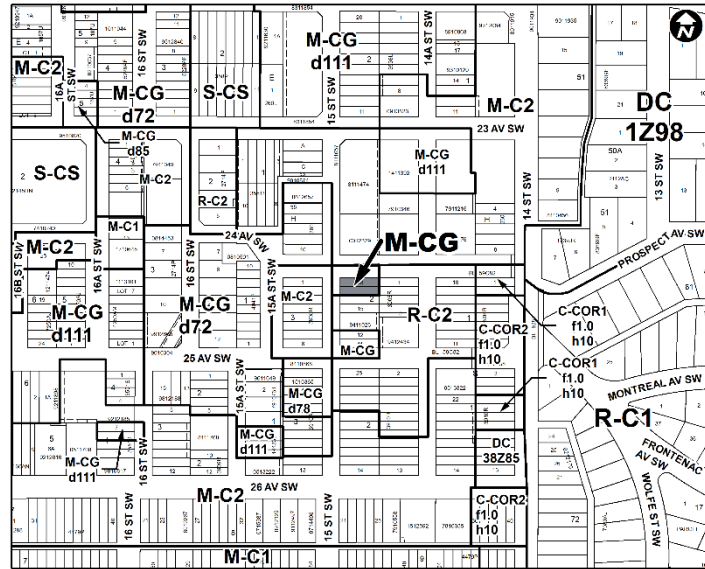
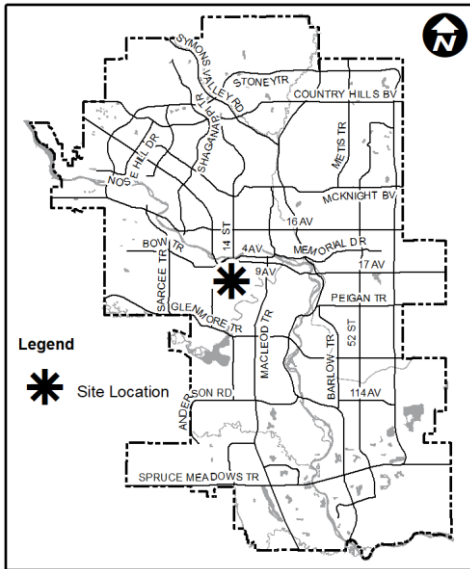
Bankview	
Peak Population Year	1981
Peak Population	5590
2017 Current Population	5090
Difference in Population (Number)	-500
Difference in Population (Percent)	-9%

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Bankview](#) community profile.

Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15
Street SW, LOC2018-0095

Location Maps



Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15 Street SW, LOC2018-0095

INVESTIGATION: ALTERNATIVES AND ANALYSIS

On 2018 April 27, a land use amendment application was submitted that proposed to change the designation of a single residential parcel (the “subject site”) to Multi-Residential Contextual Grade-Oriented (M-CG) District. After review, Administration determined that this application aligned with the *Municipal Development Plan* (MDP) and the *Location Criteria for Multi-Residential Infill*. The existing local area policy, the *Bankview Area Redevelopment Plan* (ARP), identifies this parcel as part of the “conservation” land use area, which allows for a maximum of two dwellings units per parcel. To enable this application, a minor amendment to the land use map contained within the ARP is required to identify the parcel as part of the “Medium Low Density Redevelopment” area.

Planning Considerations

Land Use

The existing Residential Contextual One / Two Dwelling (R-C2) District allows for contextually sensitive redevelopment in the form of duplex dwellings, semi-detached dwellings and single detached dwellings. This application proposed to redesignate the subject site to Multi-Residential Contextual Grade-Oriented (M-CG) District which would allow for:

- Multi-residential buildings (e.g. townhouses, fourplexes);
- A maximum building height of 12 metres (an increase from the current maximum of 10 metres);
- A maximum of 5 dwelling units (an increase from the current maximum of 2 dwelling units), based on a density of 111 dwelling units per hectare, and;
- The uses listed in the proposed M-CG designation.

The proposed district is appropriate for this area as it is located on a corner parcel, is in close proximity to transit and a planned corridor (the 14 Street “Main Street”).

The Residential – Grade-Oriented Infill (R-CG) District was contemplated by the Applicant prior to submission of this application. As described in the Applicant’s submission letter (Attachment 1), R-CG would have only allowed for 3 units and they specifically desired a minimum of 4 units. The Applicant therefore decided to apply for M-CG.

Development and Site Design

The rules of the proposed Multi Residential – Contextual Grade-Oriented (M-CG) District will provide basic guidance for the future site development including appropriate uses, height and building massing, landscaping and parking. Given that this site does not have access to a rear lane, additional items that will be considered as part of the development permit review process include:

- Improving the pedestrian realm by requiring boulevard trees within the public right of way. This provides landscaping along a public street, helps minimize the visual impact of

Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15 Street SW, LOC2018-0095

multiple driveways while ensuring the new driveway flares are not consolidated into larger expanses of concrete;

- Ensuring that the building facades utilize high quality finishing materials and are visually interesting.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant citizens and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised on-line.

The Bankview Community Association was circulated as part of Administration's standard practice. In their response, they expressed a number of concerns with this proposed application, many of which were related to the future potential building design.

They feel that M-CG is not appropriate as it would allow for the 4 units that are shown in the application's conceptual plan, instead of 3 units, which the Community Association feels is more appropriate for the area. They further stated that they could support 4 units if the Applicant would commit to public realm improvements along 24 Avenue SW, including but not limited to landscaping that included benches and trees as condition(s) of development permit approval.

Regarding potential building design, there is concern that all of the front drives along 24 Avenue SW and the "blank wall" facing 15 Street SW will diminish the pedestrian experience within the public realm. As such, the Community Association suggests that the applicant minimize the number of front driveways and ensure building faces add visual interest through varied and high quality materials. They have also expressed a preference for a stacked townhouse building form.

The complete letter is attached to this report (Attachment 2). It should be noted that this letter (dated 2018 May 29) was submitted to Administration prior to development permit DP2018-2891 being submitted on 2018 June 20 and as such, the Community Association's comments do not take in account the design details that have since been proposed.

Administration received 1 objection letter and no support letters from citizens by the CPC report submission date. The stated reason for the objection was that 4 units would be too much for the neighborhood due to a lack of on-street parking available within the area in the evenings. The letter suggested that 2 to 3 units would be more appropriate.

Administration considered the concerns expressed by the in the circulation comments received as part of the review of this proposed land use amendment application.

Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15 Street SW, LOC2018-0095

<u>Community Concern</u>	<u>How Administration Considered Concern</u>
4 Units (M-CG) too dense, prefer 3 units (R-CG) or less	<ul style="list-style-type: none"> Proposed M-CG aligns with <i>Municipal Development Plan</i> and the <i>Location Criteria for Multi-Residential Infill</i> guidelines, as described in the subsequent section of this report.
Too many front driveways along 24 Avenue	<ul style="list-style-type: none"> The active Development Permit includes a Prior to Decision condition requiring the applicant to utilize additional boulevard trees to minimize the visual impact of multiple front driveways
West building elevation will be “blank wall”	<ul style="list-style-type: none"> The active Development Permit plans provides a west building elevation that is visually interesting. See Attachment 4 for further information.
Would support with public realm improvements as condition of development	<ul style="list-style-type: none"> The active Development Permit Prior to Decision condition requires the applicant to enhance the 24 Avenue SW streetscape by utilizing boulevard trees. Rolled curb instead of curb cuts would be utilized to avoid uneven sidewalks
Prefer stacked townhomes building form	<ul style="list-style-type: none"> Administration’s authority to regulate building form is limited by Land Use Bylaw 1P2007 rules Proposed building form (“Multi-Residential”) is a Discretionary Use listed within M-CG District
Prefer interesting building facades	<ul style="list-style-type: none"> The active Development Permit plans provides building elevations that are visually interesting. See Attachment 4 for further information.

Further to the above, Administration encouraged the applicant to contact the Bankview Community Association to discuss the issues expressed in their circulation response letter. The applicant contacted the Community Association on 2018 June 27 to review the active development permit plans. The applicant has informed Administration that the Community Association generally “liked the design and understood the challenges of lane-less parcel.”

No City-led engagement was undertaken as part of this application.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission’s recommendation and the date of the Public Hearing will be advertised.

Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15 Street SW, LOC2018-0095

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The site is located within the ‘City, Town’ area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory – 2009)

The subject site is located within the ‘Residential – Developed – Established’ area as identified on Map 1: Urban Structure in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies regarding respecting and enhancing neighborhood character, general developed residential areas and established areas land use.

Bankview Area Redevelopment Plan (Statutory – 1986)

The subject site is identified as a Conservation land use area, which only allows a maximum of two dwelling units per parcel. If this application is to be approved, a minor amendment to the Land Use Policy (Figure 2) will be required to show this site as part of the “Medium Low Density” area (see Attachment 3.) This ARP amendment is consistent with *Municipal Development Plan* policies, as described in the previous section.

Location Criteria for Multi-Residential Infill (Non-Statutory – 2016)

The subject site meets approximately half of the guideline criteria for consideration of multi-residential infill. The guidelines are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP or local area policy plans, to assist in determining the appropriateness of an application in the local context.

The site satisfies multiple criteria, namely being located on a corner parcel, within 400 metres of a transit stop, adjacent to a Multi-Unit development (across 24 Avenue SW and 15 Street SW and in close proximity to a planned corridor (the 14 Street “Main Street”). Conversely, the site does not satisfy the criteria of being within 600 metres of the primary transit network, fronting onto a higher standard roadway, adjacent to an open space or having direct lane access.

Social, Environmental, Economic (External)

The recommended land use amendment will provide for a moderate increase in residential density, allowing for a more efficient use of existing public infrastructure. In addition, this proposal would encourage an increase in socio-economic diversity within the area by providing a variety of housing types and forms. No environmental issues have been identified at this time.

Planning & Development Report to
Calgary Planning Commission
2018 October 18

ISC: UNRESTRICTED
CPC2018-1205
Page 8 of 8

**Policy Amendment and Land Use Amendment in Bankview (Ward 8) at 2502 - 15
Street SW, LOC2018-0095**

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is consistent with the applicable policies of the *Municipal Development Plan* and the *Bankview Area Redevelopment Plan*, meets many of the *Location Criteria for Multi-Residential Infill* and is consistent with the intent of the Multi-Residential – Contextual Grade-Oriented District of Land Use Bylaw 1P2007.

ATTACHMENT(S)

1. Applicant's submission letter
2. Letter from Community Association
3. Proposed amendment to the Bankview Area Redevelopment Plan
4. Active Development Permit (DP2018-2891) - Site plan and building elevations

Applicant's Submission

Design Intent and Context:

The subject parcel is located in the community of Bankview and consists of 0.05207 ha. We are proposing a development with four-unit rowhouse building. Front doors will face 24 AV SW & 15 ST SW. Parking would be provided at grade in individual, attached garages accessed from 24 AV SW.

The current land use designation is (R-C2) Residential - Contextual one/two dwelling which allows for single detached or semi-detached dwelling and a maximum height of 10m. To support the proposed development, we are applying to change the designation from R-C2 to M-CG Multi Residential - Contextual Grade Oriented District.

The Land Use Bylaw describes the M-CG district as follows:

The Multi-Residential - Contextual Grade Oriented District is intended to apply to the Developed Area has Multi-Residential Development that will typically have higher numbers of Dwelling Units and traffic generation than low density residential dwellings ; has Multi- Residential Development designed to provide some or all units with direct access to grade; provides for Multi-Residential Development in a variety of forms; has Multi-Residential Development of low height and low density; allows for varied building height and front setback areas in a manner that reflects the immediate context; is intended to be in close proximity or adjacent to low density residential development; provides outdoor space for social interaction; and provides landscaping to complement the design of the development and to help screen and buffer elements of the development that may have impacts on residents or nearby parcels.

With a rowhouse form, it would be conventional to apply for the R-CG designation. However, because R-CG allows a density of 75 units per hectare, that designation would only allow for 3 units ($0.05207 \text{ ha} \times 75 = 3.905$ units). We have therefore applied for a M-CG designation solely to meet the density requirement. Should the development authority feel that a relaxation on the density of R-CG would be possible, we would be open to asking for an R-CG designation instead.

Regardless of the specific designation, the site has specific attributes which make it ideal for an increase in intensity:

- The site is only 2 blocks (a 3 minutes walk) from 14 ST SW, which is a designated Main Street.
- The site is only 2 blocks (a 2 minutes walk) from 26 AV SW, which is a collector road.
- The site is only 6 minutes' walk to Bankview Community Association and Bankview Playing Field.
- The site is only 6 minutes' walk to Buckmaster Park.
- The site is less than 5 minutes' drive to Marda Loop commercial uses.
- The site is only 10 minutes' drive to Mount Royal University.

Policies (and Variations):

Municipal Development Plan (MDP):

The proposed project fundamentally meets the goals of the Municipal Development Plan.

This City policy encourages more housing options in established communities, more efficient use of infrastructure, and more compact

Applicant's Submission

built forms in locations with direct easy access to transit, shopping, schools and other community services.

The MDP encourages modest redevelopment of the Established Area. (3.5.3 (a.)) The proposed development modestly intensifies the use of the land to a density more appropriate for a central Calgary community.

The MDP requires that "ground and lower levels of developments should demonstrate a strong relationship to the human scale and contribute positively to the public realm and street." (2.4.2 (b.)). Given that the site is laneless, driveways accessing from 24 Avenue SW are designed to have adequate length and make sure that the sidewalk is protected from any vehicles parked in the driveway.

Bankview Area Redevelopment Plan (ARP):

The ARP locates this parcel in a Conservation area, where the policy is to retain the existing neighbourhood quality and character. Our modest four-unit project is a good transitional form between the existing low-density housing stock to the south and east, and the four-storey apartment blocks across the street to the west and north.

Our proposal is fundamentally a grade-oriented, low density form, and maintains the family-oriented character of this block.

Bylaw (and Relaxations):

The proposed development is at a preliminary stage of design. At this point, relaxations are not anticipated. Should any be proposed in the future, these will be within the intent of City policy.

Engagement:

Given the small increment in density proposed, no pre-application meeting was scheduled prior to this land use redesignation application. The Bankview Community Association will be consulted as this application and the Development Permit application progress.

Letter from the Bankview Community Association

Bankview Community Association

2418 17th Street Southwest, Calgary, Alberta T2T 4M8

Tel. (403) 244-2544



Date: May 29, 2018

To: Tom Schlodder
By email to: tom.schlodder@calgary.ca

File Reference: LOC2018-0095
Subject Address: 2502 15 St SW

Dear Mr. Schlodder,

The BCADC (Bankview Community Association Development Committee) has reviewed the proposal for a land use redesignation on the subject address from R-C2 to MC-G and an increase in density from 1 single detached home on 1 lot to a 4 unit multi-dwelling on one lot (see site plan submitted with redesignation application).

MC-G (Multi-Residential – Contextual Grade-Oriented District) is a multi-residential designation in the developed area that is primarily for townhouses and fourplexes. M-CG zoning would allow 5.8 units on this site (111 units per hectare @ .05207 ha site size). The MC-G zoning would require a modifier to d77 to allow a maximum of 4 units. The site plan is disappointing in its layout and proliferation of front drives. This suggests the applicant is unable to sensitively accommodate 4 units on the site. A better option for this site is R-CG (Residential – Grade-Oriented Infill District) to allow 3 units. R-CG is a residential designation that is primarily for rowhouses but also allows for single detached, side by side and duplex homes that may include a secondary suite.

We request that zoning be changed to R-CG and not MC-G, as proposed by the applicant. The BCADC supports an endeavour to build up to 3 units on site. Although we appreciate the provision of a preliminary site plan, the BCADC believes that there are many improvements that could be made. There is an excellent opportunity to use the east lane for vehicular access to one or more of the properties. We discourage vehicular access and garages solely off 24th Avenue SW. A row of garages essentially creates a wall which separates the units from the public realm at the ground level. 24th Avenue is an important link to community hubs including Starbucks and Our Daily Brett. The BCADC may be supportive of up to 4 units were vehicular access reconsidered and public realm improvements provided. This residential street would greatly benefit from well-designed community shared space, including benches, trees and landscaping.

The BCADC is concerned about the blank wall of the development facing 15 ST SW. Light quality would be poor, and this represents a missed opportunity for a preferred west orientation. According to the concept, the development faces north in it's entirety. Light quality is more limited in a northern exposure.

The BCADC would further suggest a stacked townhouse development which would offer considerable improvement in aesthetics and design excellence for residents. The BCADC encourages building facades which add visual interest and address the public realm through use of varied and high-quality materials. Monolithic and cookie-cutter designs must be avoided. Most importantly, MC-G zoning is not appropriate for this site.

Thank you for inviting and reviewing the Community Association's comment.

Respectfully,
Bankview Community Association
Jennifer Miller

Letter from the Bankview Community Association

Director, Development Committee

Cc:

Cllr. Evan Woolley, evan.Woolley@calgary.ca, eaward8@calgary.ca

Nathan Berko, President, Bankview Community Association, president@bankview.org

Bankview Development Committee Members

Proposed Amendment to the Bankview Area Redevelopment Plan

WHEREAS it is desirable to amend the Bankview Area Redevelopment Plan Bylaw 7P86, as amended;

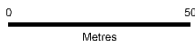
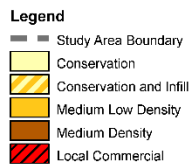
AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26, as amended:

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

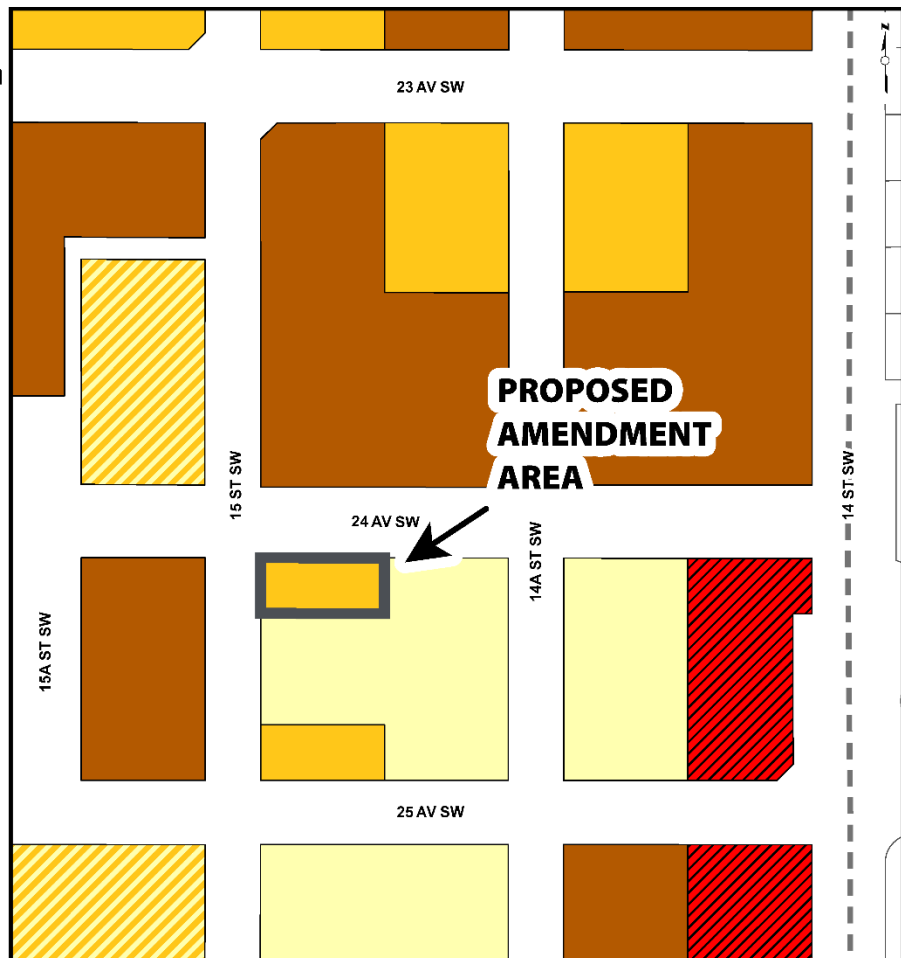
1. The Bankview Area Redevelopment Plan attached to and forming part of Bylaw 7P86, as amended, is hereby further amended as follows:
 - (a) Amend Figure 2 entitled "Land Use Policy", by changing 0.05 hectares \pm (0.12 acres \pm) located at 2502 - 15 Street SW (Plan 3908R, Block 2, Lot 17 and 18) from "Conservation" to "Medium Low Density" as generally illustrated in the sketch below:

Bankview Area Redevelopment Plan

Fig. 2
Land Use Policy



This map is conceptual only. No measurements of distances or areas should be taken from this map.



Proposed Development Summary

A development permit application (DP2018-2891) was submitted by Inertia Corporation on 2018 June 20. The development permit application is for a three-storey, four-unit rowhouse development which includes a total of four on-site parking stall, each attached to their respective unit and accessed from a single front driveway. The following image (Figure 1) from the development permit submission provides a general overview of the proposal and are included for information purposes only.

Administration's review of the development permit will determine the ultimate building design, number of units and site layout details such as parking, landscaping, and site access. No decision will be made on the development permit application until a decision has been rendered by Council on this land use redesignation.

Figure 1: Rendering of the proposed rowhouse from the corner of 24 Avenue and 15 Street SW



**Land Use Amendment in Beltline (Ward 8), Downtown Commercial Core and
Downtown East Village (Ward 7), and Inglewood (Ward 9) at multiple properties,
LOC2018-0089**

SUMMARY

This application has been submitted by Van Horne Outdoor Limited Partnership on behalf of Canadian Pacific Railway (CP) on 2018 April 20 to redesignate five individual parcels of Canadian Pacific Railway (CP) owned lands from Special Purpose – City and Regional Infrastructure (S-CRI) District to Direct Control (Special Purpose – City and Regional Infrastructure) to add the additional discretionary uses of Signs – Class F (Third Party Advertising Sign) and Signs – Class G (Digital Third Party Advertising Sign). The proposed signage is comprised of a large video screen using LED (Light Emitting Diode) technology, using a digital feed, displaying non-animated images, at a cycle of not less than 6 seconds per cycle.

Due to the unique characteristics of this application, the land use amendment requires a Direct Control district, to accommodate the proposed signage. The existing land use designation, Special Purpose – City and Regional Infrastructure (S-CRI) District, is intended to provide infrastructure and utility, facilities and systems for public transportation, and uses operated by Federal, Provincial and Municipal levels of government and does not include digital display signs. The existing class of signs allowed in the S-CRI district were purposefully paired down in Bylaw 1P2007 to reflect the utilitarian nature of the district and as such does not contain these types of signs.

The proposed Direct Control District Bylaw sets forth provisions for:

- Maximum sign area (2.13m x 7.62m);
- Maximum number of signs per face of each overpass structure (one);
- Maximum light levels from sunrise to sunset and from sunset to sunrise;
- Safety studies, monitoring and maintenance; and
- Term limits for approval of initial development permits and subsequent renewals (10 years)

If approved, these digital signs will replace the existing static signs that currently exist and re clad the remaining surface of the railway structure with an unsigned, metal finish. This program would mean a reduction of the existing static signs by approximately 50 percent as well as improving the overall appearance of the overpass structures through a re cladding of the remaining exposed portions of the overpasses to conceal the structures. The proposed development permits will be presented to CPC for review and decision at a later date. The subject locations are:

1. Macleod Trail SE (south face),
2. 1 Street SE (north face),
3. 1 Street SW (south face),
4. 4 Street SW (south face),
5. 8 Street SW (both north and south face) and
6. 9 Avenue SE immediately west of the Blackfoot Trail SE overpass (west face).

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Two development permits for the proposed signs have been submitted in respect of two locations under (1 Street SE – north face of structure) and (Macleod Trail-south face of structure), dated 2018 August 09 and are under review.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 115R - 9 Avenue SW, 221R - 9 Avenue SE, 537 - 9 Avenue SE, 830R - 10 Avenue SW, 1797 - 17 Avenue SE and 915 - 4 Street SW (Plan 8110620, Block C; Plan 8110620, Block A; Plan A, Block RLY, Lot 24; Plan A, Lot RLY; Plan 8110620, Block H, Plan RY9, Block RLY, Lot 24; Plan 8110620, Block E) from Special Purpose – City and Regional Infrastructure (S-CRI) District to DC Direct Control to accommodate digital third party advertising signs on railway overpasses owned by the Canadian Pacific Railway within the Centre City, with guidelines (Attachment 1).
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

Although the CP is federally regulated, the Board of Transport Commissioners for Canada (renamed the Canadian Transport Commission) entered into agreements with The City of Calgary, dating as far back as 1909, to allow the City to “construct and maintain public highways, level and subway, crossings of the right of way and tracks of the Company on certain streets within the limits of the City”.

Recognizing that the activity of hosting electronic third-party advertising signs on select overpasses is outside of what could be termed normal rail operation, the Canadian Pacific Railway Company has concurred to submit to the City’s discretionary Bylaw for the purposes of regulating the location and impacts of this type of this new sign technology on the five railway overpasses.

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Site Context

Five of the six subject railway structures are located within the Centre City proper with the 9 Avenue SE site residing within the community of Inglewood. These existing railway structures form underpasses for the aforementioned roadways and currently host static signs. CP maintains the structures, wing walls along the approaches and lighting of the underpasses. The City of Calgary is responsible for the maintenance and upkeep of the sidewalks and road surfaces for the approaches and beneath the underpasses. Arrangements between The City and CP have allowed The City to complete programs of underpass improvements at The City's expense.

Land use patterns along the north side of the CP corridor are largely developments of office, parking structure, surface parking lots, Palliser Hotel, Calgary Tower, retail and food service under the Commercial-Residential CR-20 land use district and Direct Control Bylaws. Land use patterns along the south side of the rail corridor include parking structures, surface parking, utilities, hotel, residential, office and restaurants under the Centre City Mixed Use CC-X district. The 9 Avenue SE site resides immediately adjacent to the Blackfoot Trail overpass amid a mix of industrial, commercial, recreational and residential development where both Blackfoot Trail and the CP line cross over 9 Avenue SE.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Planning Considerations

Land Use

The S-CRI District does list Sign – Class F (Third Party Advertising Sign) and Sign – Class G (Digital Third Party Advertising Sign) as discretionary uses, however, only to be approved in instances where the signs are used for business carried on by, or on behalf of The City. Evaluating the type of signage to be used primarily for third-party advertising, Administration considered the following intent behind these 2 types of signage;

- Sign – Class F (Third Party Advertising Sign) is by definition a sign that displays copy directing attention to a business, commodity, service or entertainment that is conducted, sold or offered elsewhere than on the site where the sign is located and does not contain a digital display.
- Sign – Class G (Digital Third Party Advertising Sign) is a sign intended to display copy directing attention to a business, commodity, service or entertainment that is conducted, sold or offered elsewhere than on the site where the sign is located. The copy is displayed by means of digital display but does not contain copy that is full motion video or otherwise giving the appearance of animation or movement.

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Development and Site Design

As previously mentioned, development permits for digital display signs at the Macleod Trail and 1 Street SE railway crossing are currently under review. These development permits contain the details of exact location, mounting systems, source of power, LED nomenclature and maintenance and replacement protocol. These development permits will come before Calgary Planning Commission in the near future.

ANALYSIS

As noted above, one of the signs is proposed to be located over City road right-of-way (4 Street SW). Bylaw 1P2007 significantly restricts the use that can be made of roads, and requires that all development be on a parcel, which does not include a road. The DC Bylaw has been drafted to allow the sign use over the 4 Street SW road right-of-way, and to allow development permit application approval where the associated land is not a “parcel”. In addition, while the City’s standard practice is to extend land use mapping to the mid-line of the road adjacent to the parcel being designated, in the case of the 915 4 Street SW parcel, land use mapping to the midline of the road adjacent to the parcel, land use mapping will be extended across 4 Street SW to the boundary of the parcel to the east of 4 Street SW to ensure the DC land use is applied to the entire relevant portion of 4 Street SW.

Given the nature of the predominant office and non-residential uses along 9 Avenue SW on the north side of the CPR line, the appropriateness of the installation of this type of signage focused on its interaction with vehicular traffic, pedestrians and active modes considerations. Development patterns and land use along the south side of the CPR line adjacent to 10 Avenue SW introduced consideration of rules addressing proximity to residential development (emerging along both the north and south sides of 10 Avenue SW).

The proposed DC Direct Control Bylaw requires the Applicant to mitigate any issues that may affect public safety. Independent safety reviews are also completed for The City after one year post-commissioning, and at 3 year intervals thereafter.

Separation from existing and potential residential development including hotel uses is well inside the 125.0 metre standard (50 metres \pm). To offset concerns of glare and rogue lighting affecting residential uses adjacent to the subject sites, the applicant has introduced and employed technologic properties as well as more stringent night time light levels. A more stringent nighttime light level, not to exceed 170 nits, has been agreed upon by Administration and the Applicant and has been codified as such in the proposed Direct Control Bylaw.

Removal of the existing static signs and refinishing those portions of the structures not occupied by the digital signs with a metal cladding is a positive step in upgrading the appearance of the overpass structures.

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Infrastructure

Controlled Field Demonstration

Administration participated in a field demonstration 2018 August 03 whereby a digital sign, representative of a typical installation, was hung in place from the railroad property over the face of the 1 Street SE overpass to allow viewing of the digital display sign over a period of 5.5 hours.

In the period following the field demonstration, based on feedback from Administration, the applicant has revised both the size and location of the proposed screens to a maximum of 2.13 meters high by 7.62 meters wide. Originally proposed to be located directly above the travel lanes and over a portion of the sidewalks, the sign installations will be near the center of each overpass with no portion of the sign projecting over the sidewalk system. The area of the overpass structure not occupied by the screen will be finished in a metal cladding.

Transportation Networks

The sign locations are proposed along underpass structures within the Center City and Inglewood. The roadways form part of the transportation network, and is the primary access for vehicles, pedestrians and cyclists entering and leaving the Downtown core, and between the Downtown Core and Inglewood. Each location accommodates tens of thousands of vehicle, pedestrian, and cyclist cumulative trips daily.

A Transportation Report was submitted in support of the proposed application. The report highlights several observations, and provides recommendations for installation, monitoring, and assessing mitigation measures for the digital display signs.

The Transportation Report will form the basis of each safety study required in conjunction with each development permit application.

Stakeholder Engagement, Research and Communication

Stakeholder engagement was primarily an applicant-led engagement dating back to 2016. In order to promote authentic engagement with stakeholders, the applicant conducted what they termed a two-phase engagement process. In Phase One, the applicant met with City Councilors, local business associations, community associations, and neighborhood associations.

In second phase of the engagement, the applicant states they continued to meet with stakeholders, hosted a live testing of the technology, worked with The City to provide information for their website, and also worked with a third party research firm to conduct a survey with Calgary residents about digital signage (Attachment 4).

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The City-led engagement was of a 'Listen & Learn' spectrum that included an on-line survey, a project website and communications with local associations and Councillor offices. In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

The Engage Spectrum level for this project was 'Listen & Learn' which is defined as "We will listen to stakeholders and learn about their plans, views, issues, concerns, expectations and ideas." Feedback collected through the City-led engagement program will be used by Administration to make recommendations to the applicant through the land use application and development permit processes.

As this application and the location of the proposed signage has the potential to affect those beyond the immediate vicinity of the proposed locations, an online engagement approach with targeted communications was implemented to ensure the City was reaching those that live, work and play in the area. The full details of the applicant engagement report and The City engagement report accompany this report as Attachments 4 (City) and 5 (Applicant).

Strategic Alignment

Municipal Development Plan (MDP)

As a visionary document, the MDP does not go into the fine grain of uses within each land use district, rather, it refers Administration to local area plans for guidance and implementation. In this case, Administration consulted the Centre City Plan, Centre City Urban Design Guidelines and the Downtown Underpass Urban Design Guidelines.

Centre City Plan & Beltline Area Redevelopment Plan

The CPR line provides a physical division between the Downtown and the Beltline Community. The Centre City Plan, recognizing the importance of the CPR rail corridor through the Centre City, established that this area warrants policy and as such has identified the area as *Canadian Pacific Railway Special Area/Corridor*. The goal is to establish guiding policies for this corridor between the Elbow River and Crowchild Trail SW and between 9 Avenue S and 10 Avenue S, inclusive of both sides of the street that recognize the corridor's unique economic benefits, operational characteristics and long-term redevelopment potential.

As of this date, there is yet to be a comprehensive Special Area/Corridor policy in effect. However, review of both the Centre City Plan and the Beltline ARP have commenced and are considering policies for the corridor.

Centre City Plan

As with the MDP, the Centre City Plan does not go into a fine grain level of addressing uses such a signage. The Plan does, however, set out goals and visions for creating attractive neighborhoods and their connection with one another and to the Downtown.

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In a section of the Plan setting visions and goals for connectivity and linkages (underpasses/overpasses), policy cornerstones have been set in an effort to ensure that the CP corridor innovatively bridges the CP tracks between the Downtown and Beltline neighbourhoods. The underpasses contained within this land use application, as well as others outside of it, provide vehicular and pedestrian connections under the CP railway corridor are intended to accommodate multiple movement modes or combinations of modes (e.g. cars, public transit, pedestrians, alternative modes and any combinations of these, etc.).

Policy cornerstones of this section of the Plan include:

- Wherever possible, underpasses/overpasses shall have an active edge treatment that may include retail or community services, recreational or entertainment facilities;
- Brighter colour schemes should be incorporated that create more inviting pedestrian environments;
- Underpasses/overpasses should be well lit and may incorporate electronic surveillance; and
- Underpasses/overpasses should include embedded public art features.

In the case of an improvement or restoration of an existing underpass the following should also be considered:

- Appropriate treatments of retaining walls with public art celebrating the CPR heritage in the form of murals, 3-D sculptural elements or any new electronic media that would animate the pedestrian zone and be complementary to structural elements; and
- Painting the original metal structure white to create a more inviting pedestrian environment by adding colour at night with special lighting features to lighten the underpass for safety.

Examples of these improvements include the 8 Street SW Underpass which has seen an investment of \$8.8 Million dollars where digital signage was integrated into the lighting solution for the underpass and the 4 Street underpass. The 4 Street SW underpass is currently in the final stages of construction and is expected to have an investment of \$7.54 Million dollars where an interactive digital public art installation is a key element of the underpass. Lastly the first underpass project at 1 Street frames the railway and was a project that The City invested \$4.6 Million dollars; this is an example of where third party signage was removed and relocated to execute this comprehensive design.

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Beltline Area Redevelopment Plan (ARP)

The Beltline ARP recognizes four neighbourhoods represented by the major north-south streets (14 Street S.W., 8 Street S.W., 4 Street SW and Macleod Trail), the CPR tracks, the north-south streets also serve as the connections or “bridges” into Downtown as they physically breach the CPR tracks via the railway underpasses. Once again, ARP does not go into the finer details of signage but does respect the relationship between the Beltline and the downtown core in making safe, convenient and attractive connections.

Downtown Underpass Urban Design Guidelines

The Downtown Underpass Design Guideline (the “Guideline”) is a non-statutory document providing comprehensive urban design guidance for the future development of any new underpass and future improvement of existing underpasses within the Centre City area. The Guideline includes urban design principles and design solutions which address urgent and pressing issues related to Calgary’s downtown underpasses.

Every underpass is attached to developed or developable lands. Successful underpass design and improvements rely on a coordinated approach to public improvement working with private developments. The Guideline considers underpass design to include the adjacent developments. According to the Centre City Plan, CP continues to be a central transportation corridor in the long term but with future developments, it complements and defines adjoining uses.

Integrated design solutions with lighting elements is a primary focus of the Centre City Plan as supported by the Underpass Enhancement Program. It is premature at this point to evaluate what, if any, effect the proposed digital signs will have on the above referenced underpasses. The location and orientation of the signs is primarily designed to attract the attention of vehicles with pedestrian exposure also an intended audience. However, through the monitoring of the installations, Administration may find opportunities to compliment the underpass improvements made by The City rather than to complete with them.

Canadian Pacific Railway – City of Calgary Agreements

The Board of Transport Commissioners for Canada (since renamed the Canadian Transport Commission) entered into agreements with the City of Calgary, dating as far back as 1909, to allow the City to construct and maintain public highways, level and subway, crossings of the right of way and tracks of the CPR on certain streets within the limits of the City.

Social, Environmental, Economic (External)

The recommended land use allows for the replacement of outdated and unattended temporary signage and provides an opportunity for social and economic stimulus through a wider variety of advertising. The addition of these 2 new types of signage and future development permits have the potential to reduce the amount of placard and vinyl signage by half over what exists today.

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Risk Assessment

The objectives of sign regulations are numerous: to prevent unreasonable distraction of drivers, to provide maximum visibility at intersections, to protect against fire hazards, to protect passers-by from projecting falling signs and to promote the aesthetics of a community. With no precedent applications or installations to guide Administration, this application has been reviewed with a balanced emphasis on aesthetics and public safety.

In most instances, the combination of banner and placard signs extend the full length of the railway structures over the roadways. Removal of these temporary signs and replacement with the proposed digital media will reduce the amount of advertising by roughly 50 percent. Aesthetically speaking, this reduction of clutter would be a positive improvement. Those portions of the overpass structure not occupied by the digital sign will be clad in a metal finish thereby improving the overall appearance of the structures.

The application of digital signs above or within public right-of-way has not been contemplated by The City or within LUB 1P2007. As such, The City requested additional analysis from the Applicant. The transportation report submitted by the Applicant team contained evaluation of risk, analysis of sight lines, discussion of the in-field demonstration and proposed a framework for development permit (DP) evaluation and monitoring. This framework forms a large part of the proposed DC Direct Control District and is summarised as follows:

1. In conjunction with each development permit, the Applicant is to provide an independent transportation safety study in support of the application. Any proposed mitigation measures to be included as part of the DP package to the satisfaction of the Development Authority.
2. On-going monitoring of the locations is to occur at years 1, 4, 7 and 10 of the associated Development Permit. Monitoring can include such things as an In-Service Road Safety Review, a Road Safety Audit and an evaluation of near misses and collisions in the area.
3. The City reserves the right to direct the Applicant to cease displaying digital copy during recreation of a severe collisions and during post-evaluation of an incident where the digital sign is suspected to be a contributing factor.
4. Safety studies submitted either as part of the on-going monitoring program or as part of a collision investigation shall have any proposed mitigations put in place prior to enabling digital copy.
5. Should The City determine that the signs pose an unacceptable public safety risk that cannot be mitigated, the Development Permit can be cancelled with written notice.
6. The City will request the Applicant/Owner to enter into an Indemnification Agreement at each subject DP location.

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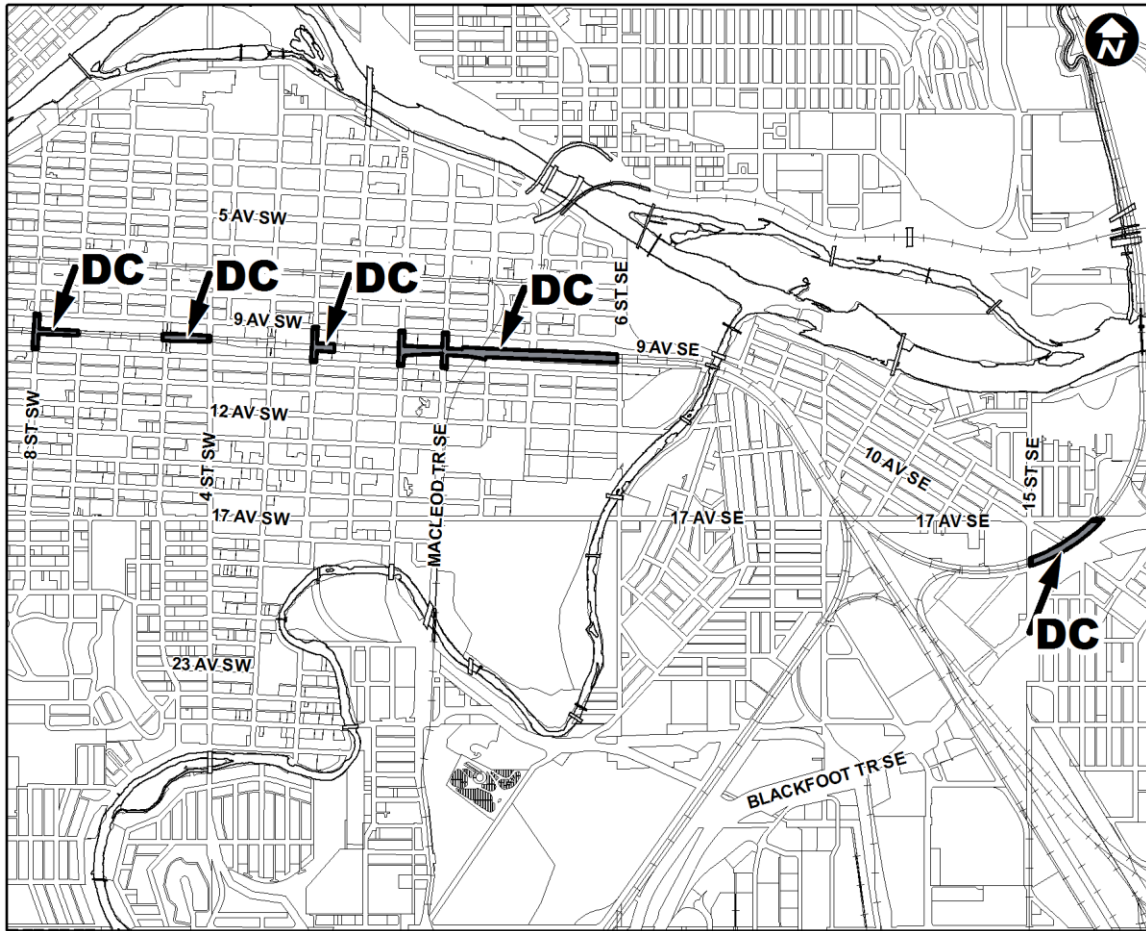
REASON(S) FOR RECOMMENDATION(S):

Administration finds the addition of Sign – Class F and G to the existing land use for the subject parcels will result in an overall reduction in the number and size of existing signage as well as the improved general appearance of the overpass structures themselves.

ATTACHMENT(S)

1. Proposed Land Use District Maps
2. Proposed Direct Control Bylaw
3. Applicant submission
4. City of Calgary Engagement Summary Report
5. Applicant Engagement “What we heard” report
6. Beltline Neighbourhood Association letter
7. Victoria Park Business Revitalization Zone letter
8. Calgary Downtown Association letter

Proposed Land Use Map



Proposed Direct Control Guidelines

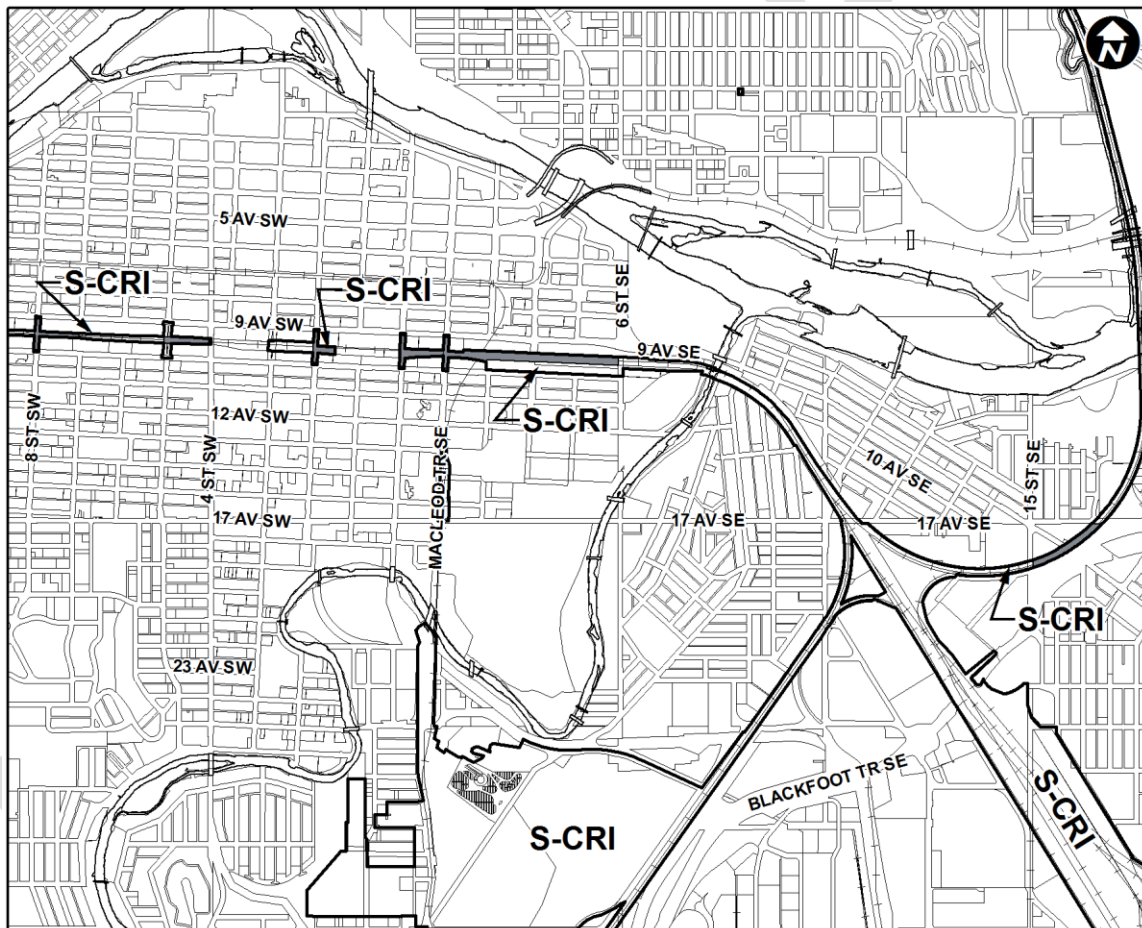
WHEREAS it is desirable to amend the Land Use Bylaw Number 1P2007 to change the land use designation of certain lands within the City of Calgary;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26 as amended;

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

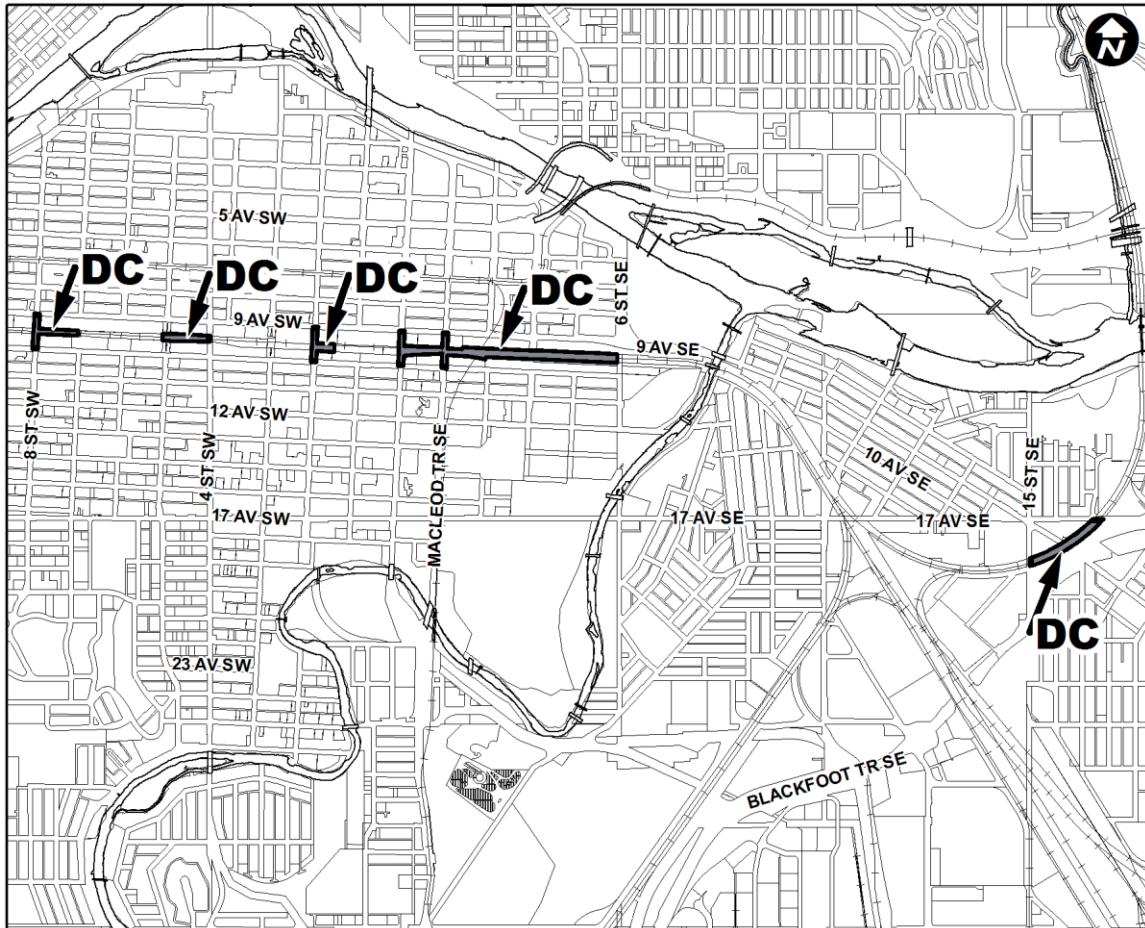
1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".

SCHEDULE A



Proposed Direct Control Guidelines

SCHEDULE B



DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control District is intended to:

- (a) allow for the additional **discretionary uses** of **Sign – Class F** and **Sign – Class G** on railway bridges owned, controlled or operated by Canadian Pacific Railway and its successors and assigns; and
- (b) provide for specific **development** rules and standards applicable to **Digital Message Signs, Third Party Advertising Signs** and **Digital Third Party Advertising Signs** attached to or located on railway bridges owned, controlled or operated by Canadian Pacific Railway and its successors and assigns.

Compliance with Bylaw 1P2007

2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Proposed Direct Control Guidelines

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The **permitted uses** of the Special Purpose – City and Regional Infrastructure (S-CRI) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Discretionary Uses

- 5 The **discretionary uses** of the Special Purpose – City and Regional Infrastructure (S-CRI) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District with the addition of:

- (a) **Sign – Class F**; and
- (b) **Sign – Class G**.

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Special Purpose – City and Regional Infrastructure (S-CRI) District of Bylaw 1P2007 apply in this Direct Control District.

Additional Sign Rules

- 7 (1) Notwithstanding section 5(1), section 26(1) and Part 3, Division 5 of Bylaw 1P2007, Canadian Pacific Railway and its successors and assigns, or its authorized agent, may apply for a **development permit** for a **Digital Message Sign, Third Party Advertising Sign, or Digital Third Party Advertising Sign** on either or both sides of a railway bridge which it owns, controls or operates, whether or not the bridge is located on a **parcel**.
- (2) The **Development Authority** may approve a **sign** referenced in subsection (1) provided:
- (a) no part of the **sign** protrudes below the bridge structure or obscures or materially diminishes the legibility of any signage required by a government or regulatory authority;
 - (b) only one **Sign – Class F** or **Sign – Class G** is located on each face of the bridge;
 - (c) the **sign** does not change the minimum vertical clearance envelope between the bridge and road surface as required by the **Development Authority**;
 - (d) the **sign area** does not exceed a vertical dimension of 2.13 metres by a horizontal dimension of 7.62 metres;
 - (e) the **sign** does not have a height greater than 10.0 metres, when measured from the top of the **sign** to the surface of the road directly beneath the **sign**;

Proposed Direct Control Guidelines

- (f) the light output for the **digital display** when measured from the **sign** face at its maximum brightness is no greater than:
- (i) 7500 Nits from sunrise to sunset; and
 - (ii) 170 Nits from sunset to sunrise;
- (g) a safety study for the **City**, completed by a qualified engineer licensed to practice in Alberta, is submitted in conjunction with each **development permit** application, and the **Development Authority** is satisfied that mitigation measures identified in the study will minimize safety risks to vehicles, cyclists, and pedestrians;
- (h) if the proposed **sign** is located less than 30.0 metres from an intersection, pedestrian crosswalk or railway crossing, a study for the **City**, completed by a qualified engineer licensed to practice in Alberta, is submitted with the **development permit** application indicating that the **sign** does not impede or obstruct the intersection, pedestrian crosswalk or railway crossing at the proposed location;
- (i) the initial **development permit** is issued for a period of ten (10) years, subject to the rules in subsection 7(4) of this Direct Control District, and all subsequent **development permits** are issued for a term not exceeding ten (10) years; and
- (j) there are conditions on the **development permit** requiring:
- (i) that the **sign owner** perform ongoing monitoring of the impact of the **sign**, including impacts on collision activity on any upstream intersections;
 - (ii) that the **sign owner** submit safety studies for the **City**, as required by the **Development Authority**, which studies must include an assessment of whether the **sign** poses a safety risk to vehicles, cyclists or pedestrians and recommended mitigation measures to minimize such safety risks, to the **Development Authority** at regular intervals;
 - (iii) that the **sign owner** comply with all recommendations and mitigations set out in any safety study required by the **Development Authority** to be submitted in connection with the **sign**; and
 - (iv) that, in the event of a collision resulting in a fatality or injury at an intersection **adjacent** to or approaching the **sign** that requires response by the Calgary Police Service collision reconstruction team, the **Development Authority** may require that the **sign owner**.

Proposed Direct Control Guidelines

- A. have the **sign** display an inactive mode for a reasonable period during incident management;
 - B. reset the **sign** to conditions present at the time of the incident to assist with collision reconstructions;
 - C. submit a safety study for the **City**, completed by a qualified engineer licensed to practice in Alberta, that includes an updated risk assessment of the **sign** and proposed mitigation measures;
 - D. have the **sign** display an inactive mode until such time as the study required in subsection (iii) has been completed and the mitigation measures identified therein have been implemented, provided that the **City's** review of such study must be completed within a reasonable time after such study has been submitted by the **sign owner**; and
 - E. perform any other actions reasonably necessary to assist the Calgary Police Service collision reconstruction team in collision reconstruction.
- (2) The rules set out in subsections 7(2)(d) and 7(2)(e) of this Direct Control District may be relaxed by the **Development Authority**, provided that the test for relaxation set out in Bylaw 1P2007 has been met.
- (3) In addition to the conditions set out in Section 38 of Bylaw 1P2007, the **Development Authority** may, as a condition of issuing a **development permit** for a **sign** under subsection 7(2), impose conditions respecting traffic safety.
- (4) Subject to subsection 7(5), in addition to the suspension or cancellation rules set out in Bylaw 1P2007, the **Development Authority** may suspend or cancel a **development permit** issued under subsection 7(2) if:
- (a) the **sign owner** has not complied with a safety study required by the **Development Authority** to be submitted in connection with the **sign**; or
 - (b) in the opinion of the **Development Authority**, the mitigative measures identified in a safety study required by the **Development Authority** to be submitted in connection with the **sign** or otherwise proposed by the **sign owner** will not adequately address the risks identified in the study.
- (5) The **Development Authority** may only suspend or cancel a **development permit** in accordance with subsection 7(4) after providing the **sign owner** with advance written notice and a reasonable period for the **sign owner** to address any non-compliance matters or to propose alternative mitigative measures, provided that:

Proposed Direct Control Guidelines

- (a) upon receipt of written notice of suspension or cancellation of the **development permit**, the **sign owner** must have the **sign** display an inactive mode; and
 - (b) the **sign** must remain in an inactive mode until such time as any non-compliance matters or alternative mitigative measures have been resolved and implemented to the satisfaction of the **Development Authority**.
- (6) In addition to the rule in section 43(3) of Bylaw 1P2007, the **sign owner** must physically remove the **sign** within 30 days of notification by the **Development Authority** of cancellation of the **development permit**.

Non-Application of Rules Contained in Bylaw 1P2007

8 The following sections of Bylaw 1P2007, and the rules contained therein, do not apply to **Digital Message Signs, Third Party Advertising Signs and Digital Third Party Advertising Signs** attached to or located on railway bridges owned, controlled or operated by Canadian Pacific Railway and its successors and assigns:

- (a) Section 104 – Digital Message Sign;
- (b) Section 111 – Prohibited Locations for Third Party Advertising Signs;
- (c) Section 112 – Siting of Third Party Advertising Signs;
- (d) Section 113 – Height and Size of Third Party Advertising Signs;
- (e) Section 114 – General Rules for Third Party Advertising Signs;
- (f) Section 115.2 – Prohibited Locations for Digital Third Party Advertising Signs
- (g) Section 115.3 – Siting of Digital Third Party Advertising Signs;
- (h) Section 115.4 – Height and Size of Digital Third Party Advertising Signs; and
- (i) Section 115.5 – General Rules for Digital Third Party Advertising Signs.

Applicant Submission

Land Use Redesignation Rationale Van Horne Outdoor Signage Program August 30, 2018

General Overview

The proposed digital signs are not the primary use of the Canadian Pacific Railway and as such requires this land use amendment to add the discretionary uses of **Sign – Class F** and **Sign – Class G** to the Special Purpose – Community Institution (S-CRI) zoning.

Van Horne Outdoor (VHO) is proposing to eliminate the current outdated Class F signs that cover the full width of the overpasses in favour of a modern Class G digital display. This improvement effort ultimately will allow for the removal of the outdated static billboards and replace them with smaller modern digital displays (approximately 50% reduction in overall size of the signage). The modernization program will improve the overall bridge aesthetics, including new cladding and the opportunity to place a neighborhood logo, City of Calgary branding, or other gateway marker on the bridge alongside the sign. The signage also provides an opportunity for community messaging through Van Horne's offer to donate 15% of digital display time back to the community.

This application is tied to a signage program with an overall net reduction in third party advertising in the downtown core, resulting in VHO removing 10 outdated Class F signs on CP rail bridges, and replacing them with 7 smaller modern Class G displays. The modernization of the signage is an opportunity to address the bridge aesthetics and better integrate the bridges into the surrounding environment. The bridges targeted in this program are:

1. 1st Street S.E. - facing north
2. 1st Street S.W. - facing south
3. 8th Street S.W. - facing north and south
4. 9th Ave S.W. - facing west
5. 4th St S.W. - facing south
6. Macleod Trail - facing south

Land Use Bylaw 1P2007

The CP Rail bridges are currently zoned as S-CRI which allows for the development of Class F and Class G Digital Third Party Advertising Signs, but only as discretionary uses under certain conditions which are limited in scope and may not be applicable to the sites subject to this proposed land use redesignation. Further, the development standards and other rules that would normally be applicable to Class F and Class G Digital Third Party Advertising Signs in the S-CRI district do not adequately contemplate the placement of Third Party Advertising Signs on existing rail overpasses on sites where boundary conditions may require unique design and safety considerations.

VHO proposes a Direct Control District to accommodate the proposed signage since there is no other land use district in the Land Use Bylaw which provides for the proposed combination of permitted and discretionary uses and specific development standards to accommodate the installation and operation of Class G signage located on an existing CP Rail bridge. The proposed Class G signs in this application will adhere to the standards in Calgary's Land Use Bylaw 1P2007 with respect to the following technical operating parameters:

1. Maintain a copy duration of a minimum of 6 seconds before switching to the next copy;
2. Have a maximum transition time between copy not exceeding 0.25 seconds;
3. Not contain full motion video or transition effects (motion, fading in and out, dissolving,

Applicant Submission

- blinking, intermittent, or flashing light or the illusion of such effects);
4. Not contain sequential copy messages on a single digital display, or sequenced on multiple digital displays;
 5. Contain ambient light sensors;
 6. Not increase the light levels adjacent to the digital displays by more than 3.0 LUX above the ambient light level; and
 7. Operate with maximum luminance levels of 7,500 nits from sunrise to sunset and below the allowable 350 nits required by the bylaw from sunset to sunrise. This adherence is to ensure not only compliance during the daytime, but a respect for concerns around lighting in the evening.

Additionally, our new signs will use advanced optical LED down angle technology to ensure the light will not extend above the sign and impact surrounding areas. Not only does the sign use the most advanced lighting technology with the employment of the optical down angled LED, but it also brings enhanced lighting aspects to the bridge that integrate the existing lighting under the bridge with the overall landscape. The optical down angle LED decreases light pollution, by redirecting light downward and with more power efficiency. The lighting thus creates a positive experience for passersby, mitigating any adverse impacts of bright lights. These lights have been tested and are significantly less bright than car tail lights or traffic lights (average traffic lights are at least 10,000 nits). See attached documentation from Media Resources, Inc. (MRI) that describes the technology.

Furthermore, the proposed Class G signs will not require *any* major redevelopment of the rail land as the signs will be simply attached to the face of the existing steel/concrete bridges and engineered to CP Rail's rigorous engineering standards. The signage is compatible with the bridge structure and in fact enhances the current bridge aesthetics by addressing the rust and chipped paint on the bridge. By modernizing the signage and the bridge, the signage proposal enhances the character of the streetscape by providing additional lighting and improved bridge aesthetics (see images attached). The Class G sign design is compatible with the general architectural lines and forms of nearby buildings by blending into and enhancing the existing bridge structure. The signage does not obstruct the horizon line as it is attached to an existing structure.

Stakeholder Engagement and Consultations

In order to promote authentic engagement with stakeholders, VHO facilitated a two-phase engagement process. In Phase One, VHO met with city councillors, local business associations, community associations, and neighborhood associations. When councillors or other stakeholders requested additional consultation with specific parties, we engaged in order to answer questions and receive feedback on the proposal.

In Phase Two of engagement, VHO continued to meet with stakeholders, hosted a live testing of the technology, worked with the City to provide information for their website, and also worked with a third party research firm to conduct a survey with Calgary residents about digital signage.

Applicant Submission

The live testing of the technology took place on August 2nd between 5:30 pm-10:45 pm at 9 Ave S.E. and 1 St. S.E. The event was promoted on the City of Calgary website 22 days in advance of the engagement effort. In anticipation of the pop-up, VHO notified all stakeholders that were engaged in Phase 1 engagement efforts. This included all city councillors, the Mayor's office, the Inglewood BIA, Victoria Park BRZ, Calgary Downtown Association, Beltline Neighborhood Association, Beltline Community Association, Inglewood Community Association, Calgary Arts Commons, Mountain Equipment Co-Op, and Strategic Group. Additionally, city staff invited relevant internal stakeholders. VHO also invited members of the Calgary Planning Commission.

During the pop-up testing, VHO handed out 295 brochures to passersby and stakeholders in attendance between 5:30 pm and 10:45 pm.

The August 2nd pop-up testing was also used as an opportunity to address safety concerns. VHO invited two traffic engineers on-site to collaborate with City of Calgary engineers and Transportation Department. This includes Jason Dunn from Bunt and Associates and Michael Tantala, an expert traffic engineer, Visiting Lecturer in the School of Architecture at CUNY, and Research Fellow at Princeton, who has extensive experience working in Canada and the United States and is the preeminent expert on digital signage projects in North America. Mr Tantala has over 30 publications on engineering and traffic safety, including research on digital signage. An official assessment from the traffic engineers will be submitted to the City indicating no safety concerns.

The traffic engineers did multiple drive tests during daylight and evening hours as well as on-site technical assessment of the signage. Additionally, we invited a lighting engineer from Media Resources International to test the lighting levels of the signage and speak to City staff about the functionality of the technology, along with the light levels of surrounding signs. At 11 pm, the sign operated at only 173 nits on the brightest colour (white). The City's Land Use Bylaw permits signage to operate at 350 nits after sunset which is more than two-times the levels that were recorded at the demonstration.

In addition to the live testing, VHO worked with Innovative Research Group, a third-party research firm, to conduct a survey to gauge Calgary resident's opinions of digital billboards. Between July 17-24, 300 residents responded to the phone survey. Key findings include:

- Initially, 81% of residents do not think digital billboards in Calgary are a bad idea.
- After learning more about the details of the digital billboards, the number increased from 81% to 88%.

A full copy of the phone survey report was provided to City Staff. The pop-up testing and survey proved to be very important in our engagement process. While the survey allowed us to gather specific data related to the project from residents across Calgary, the pop-up testing allowed us to reach a more targeted population and prove that the signs will be safe and light levels are appropriate for the downtown environment.

Two engagement reports were submitted to City Staff that detail the extent of engagement efforts, including more detail on the live testing.

Applicant Submission

Conclusion

The new signage provides an opportunity for capital investment in the bridges by VHO, including new cladding, branding, and lighting. The extensive stakeholder engagement efforts index the extent to which the proposed signage aims to not only adhere to municipal regulations, but is generally supported by the community.

Precedent has been set in Mississauga, Vaughan, and Montreal--demonstrating the overall potential for such improvements. However, this application takes seriously the unique local context and regulations. The reduction in sign size, adherence to nit levels (and operating at lower levels after sunset), reduction in number of signs, and opportunity for community messaging on the signs, demonstrate the ways that this is an opportunity to bring an overall improvement to the bridge structures and the community.

City of Calgary Engagement Report Summary

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised

Summary of comments received

- Each of the 5 subject sites were posted with the City's standard placard sign
- Adjoining property owners were notified also through standard practices
- Distributed over 5 sites, multiple Community Association and Business Revitalization Zones were notified; they are:
 - o Calgary Downtown Association, Beltline Neighbourhood Association, Inglewood Community Association, Inglewood BIA, and Victoria Park BRZ
- Nearly 100% of comments received both in support or opposition were electronically generated and directed to the File Manager
- Comments in support of the application (Calgary Downtown Association, Victoria Park BRZ and Beltline) referenced the benefit to local businesses, area residents and broader public
- Comments in opposition to the application (Beltline Neighborhood Association and numerous Beltline and Vic Park residents) cite potential hazards to the motoring public and pedestrians, and light pollution as primary concerns
- To assess the conflict with pedestrians and vehicular traffic, City forces (Traffic Engineering and Planning) participated in a field demonstration commissioned by the applicant, whereby Administration was able to assess the relationship of the proposed signage to existing signalized intersections during daylight and nighttime hours
- During the 5 hour demonstration, over 300 information pamphlets were distributed to passer-byers using the pedestrian network in the vicinity of the demonstration
- From shared field observations and technical review, the applicant's traffic engineering generated a Safety Report addressing potential conflicts, measures to mitigate/off-set conflicts and a program for monitoring the installations with periodic reports sent directly to Calgary Traffic Engineering
- Comments from Administration as well as the public at large questioned the chosen size of the signs (2.8m x 8.0m);

City of Calgary Engagement Report Summary

Applicant-led engagement

The Applicant conducted their own communications and engagement program which included:

- Community Association & Business Improvement Association (BIA/ BRZ) meetings (Calgary Downtown Association Beltline Neighbourhood Association, Inglewood Community Association, Inglewood BIA, Victoria Park BRZ)
- Meetings with area stakeholders (Calgary Arts Commons, Mountain Equipment Co-op and Strategic Group)
- Pop-up engagement and signage demonstration at 1 Street SE Underpass on August 2
- Phone survey with
- Information brochure handed out

Please refer to Attachment 3 to see the Applicants summary of their engagement program.

City-led engagement

Engagement overview

The Engage Spectrum level for this project was 'Listen & Learn' which is defined as "We will listen to stakeholders and learn about their plans, views, issues, concerns, expectations and ideas." Feedback collected through the City-led engagement program will be used by Administration to make recommendations to the Applicant through the Development Permit process.

As this application and the location of the proposed signage has the potential to affect those beyond the immediate vicinity of the proposed locations an online engagement approach with targeted communications was implemented to ensure we were reaching those that live, work and play in the area.

Engagement results

- Online survey from July 15 – 31, 2018
 - 155 people participated in the survey with varying degrees of support, concern and general commentary regarding signage.

What we heard

Citizens provided a diversity of comments and the main themes, ideas and concerns identified by citizens were as follows:

- The majority of comments received were generally unsupportive of the application for the following reasons;
 - Perceived that the signage could be distracting and poses safety concerns for motorists, cyclists and pedestrians
 - Perceived as being aesthetically unpleasing and detracts from the look/ feel of the area
 - Concern with the brightness of the signs and the potential of light pollution affecting nearby residents and businesses.
 - Perceived as having no public benefit
- Comments that were supportive of the application felt that this could be an opportunity for revitalization by; improving the look of underpasses.
- Some comments made that they could be supportive if special considerations were made to the light levels and potentially offering community benefit.

City of Calgary Engagement Report Summary

What we did

Feedback collected through our engagement program will be used by The City to inform the detailed review of the Development Permit Application. Below is a response to how the main concerns raised through engagement will be addressed.

Issue identified	Response and/or changes made to application
Citizens shared concerns about motorist, cyclist and pedestrian safety as this would be a distraction.	City Traffic Engineering evaluated the field demonstration and subsequent Transportation Safety Report prepared by the applicant's traffic consultant and conditionally accepted the report based on a regime of monitoring and inspection.
Citizens shared concerns about the brightness of the signs and the potential for light pollution	The applicant has stated that the signs will be at, or lower than, the threshold established in LUB 1P2007 for nighttime standards; the applicant has declared a maximum night time illumination of 170 nits which has been included in the accompanying Direct Control Bylaw will also be added as a condition to any Development Permit approval.
Citizens shared concerns about these signs being aesthetically displeasing and detracts from the look/ feel of the area	This application proposes state-of-the-art technology to replace the existing static placard signage and has the potential include public service announcement content. Further, this proposal reduces the signable area of the overpasses by almost 50% in addition to covering the existing, exposed structures with cladding.

Communications overview

A communications plan was developed to inform the community about the project and the engagement opportunities. The tactics used to inform the public included:

- Project specific website (<http://www.calgary.ca/PDA/pd/Pages/Current-studies-and-ongoing-activities/CPR-Van-Horne-Digital-Application.aspx>) that shares information about the project and feedback opportunities.
- Social media posts to encourage participation in the online survey
- Communications with local Community Associations and Councillor offices to share engagement opportunities

Please note that the application was also subject of a news media article which also directed participation in the survey.

Applicant Engagement What We Heard Report

WHAT WE HEARD
Van Horne Outdoor
Engagement Report
Phase 1: Spring 2016-June 2018

General Overview:

Van Horne Outdoor has been working closely with city staff on a proposal to remove 10 outdated static third party signs, to be replaced by seven modern digital signs on CP bridges in the City of Calgary. Our proposal, seeks to take a place-based approach that carefully and respectfully engages with the unique character of the City of Calgary. As such, we have developed an in-depth engagement process, with two phases of engagement that ensures a two-way dialogue. Building on the City of Calgary’s Engage guidelines, as well as recommendations from staff, we have sought a process that promotes authentic engagement and directly addresses and responds to feedback from all stakeholders.

Van Horne Outdoor began engagement with business associations, community organizations, and the City of Calgary (including former councilors) in the spring of 2016. Our long-term phased approach is one that encouraged dialogue and allowed us to provide stakeholders with necessary information about the project in order to generate informed responses.

This document summarizes the first phase of engagement, initiated by the applicant. The second phase of engagement is a direct response to the City of Calgary Engage framework and will take place between July 11-August 2, 2018. This includes a survey conducted by a third-party research group, social media engagement by the City of Calgary, and a pop-up testing of the project. Findings from the second phase will be reported in early August.

Stakeholder Report

Stakeholder Name	Relevant Context/Notes	What We Heard	Our Response
Inglewood BIA	Proposed sign in ward: 9th Avenue SE	In our June 14, 2018 meeting, the BIA asked questions about how the signage integrates with the proposed 9th Avenue public art installation under Blackfoot Trail and asked if a neighborhood specific logo could be installed instead	We are only installing new signage on the west side of the bridge to avoid any visual conflict with their proposed public art project. We also would look forward to working with the BIA and community to install a neighbourhood logo.

Applicant Engagement What We Heard Report

		of a City of Calgary logo.	
Victoria Park Business Revitalization Zone President	1st Street SE, 4th Street SW, Macleod, and 9th Avenue SE (on border)	Sent two letters of support in August 2016 and May 2018. One concern addressed in their letter was how to integrate their lighting initiative with the lighting on the bridge. They also inquired about how community content would be approved for display on the signage.	We would be happy to explore ways to integrate lighting that fits with Victoria Park's initiative, as long as it aligns with City of Calgary approvals. We also answered questions around approval of content.
Beltline Neighbourhood Association	4 th Street SW and 8 th Street SW	Opposed. General concerns about third party advertising and lack of integration with Underpass Enhancement Guidelines were outlined in a letter that was sent to the City of Calgary in May 2018.	We have outlined our efforts to integrate the program with the Underpass Enhancement Guidelines in our application with the City of Calgary and have sought feedback around CPTED and design from a local expert. Our program compliments the City's enhancement program through similar use of materials and lighting.
Mountain Equipment Co-Op	At the direction of Councillor Woolley	Interested in potential for local advertising opportunities	We answered questions about opportunities for advertising.

Applicant Engagement What We Heard Report

		and ability for the sign program to address current safety issues with the bridges.	
Strategic Group	At the direction of Councillor Woolley	Interested in potential of project to further improve bridges, where needed and community oriented messaging. Also suggested we meet with local CPTED expert, Kelly Sundberg.	Adhered to request to speak to Dr. Sundberg and followed-up to shared back what we heard from Dr. Sundberg.
Dr. Kelly Sundberg	At the direction of Strategic Group	Enthusiastic about project, especially in relation to CPTED, lighting, and improvements to deteriorated bridges. Expressed distaste for CPTED lighting.	Increased lighting via light spires and signage avoids the need for added CPTED lighting. The new lighting also provides an additional deterrent from those trying to access the tracks or seeking shelter under the bridge where it is unsafe.
Inglewood Community Association	Proposed sign in ward: 9th Avenue SE	Waiting on reply	
Calgary Downtown Association	Relevant for gateway to downtown core	Sent letter of support to City Council. Expressed concerns around the current state of underpasses (the aesthetics) and see the project as an opportunity to improve the look	We discussed opportunities to address the concerns around the look of the bridges as well as support the Downtown Association's efforts to support local businesses with advertising.

Applicant Engagement What We Heard Report

		of the bridges. Previous CEO and current CEO were engaged.	
Beltline Community Association	4 th Street SW and 8 th Street SW	Requested meeting; waiting on reply	

Common concerns and questions

Across stakeholder groups and municipalities, similar concerns arise around digital signage. In Calgary, questions around pedestrian experience, lighting impacts on residential buildings, and community benefit emerged in our engagement process.

Pedestrian and cyclist experience: The City of Calgary has made a significant commitment to the Underpass Enhancement program on several underpasses across the City. Our program seeks to improve the overall look of the bridges (including addressing rust and chipped paint) and the pedestrian and cyclist experience by creating a holistic design that incorporates the bridges with added cladding, appropriate lighting, and the most up-to-date technology. This program is not just about signage, but overall design and modernization efforts that contribute to a more welcoming urban environment.

Lighting: Lighting was a general overarching concern from stakeholders. Most stakeholders, in some form, inquired if “the signs are going to be too bright?” Light pollution and “bright” signage is a common concern, across locales. Our response is to develop solutions that mitigate any concern about light pollution for residents, pedestrians, and drivers. Not only will we commit to operating our signs at a lower nit level at night (as conditioned by our agreement via the Land Use Bylaw), but we also employ the latest technology that prevents light pollution. Our investment in the Optical Down Angle technology (outlined in our application), addresses stakeholder concerns around light pollution. We not only commit to this in our application, but believe that demonstrating how the technology operates is key to the engagement process. Therefore, as part of our second phase of engagement, we will host a testing of the project that offers all stakeholders, including residents, to see how the technology addresses any concern around lighting and other project aspects.

Community benefit: Like most municipal issues, residents are concerned about the impact of such proposals (i.e. lighting) as well as community benefits and the value they might generate. The primary community benefits, as outlined in our applications, include: improved bridge aesthetics and a stronger urban realm, the overall reduction of existing vinyl based signage, smaller signs that better match the bridge structure, community messaging (i.e. amber alerts, road closures, etc.), local advertising, possible gateway markers/logos, better lighting, and deterring those from seeking shelter under the bridge from having access (overall safety).

Applicant Engagement What We Heard Report

Conclusion

Our first phase of engagement, lasting from spring 2016-June 2018, allowed us to engage in dialogue with various stakeholders. With exception of one group, we received support and enthusiasm about the project, most notably in relation to the opportunity to improve the bridges at no cost to taxpayers. Stakeholders asked questions around lighting, safety, cost, bridges, and community benefits. In general, stakeholders were most persuaded by the modernized bridge aesthetics and opportunity for community messaging and local advertising. We look forward to the second phase of engagement and reporting our findings.

Attached: Please see attached for letters of support.

Beltline Neighborhood Association Letter



Aug 7, 2018

The City of Calgary
Circulation Control
P.O. Box 2100 Station M
Calgary, AB T2P 2M5

ATTN: Richard Goecke, File Manager

RE: LOC2018-0089 – Land Use Amendment for Class G ‘Digital Third Party Advertising’ @ CP Rail Underpasses

Dear Richard,

As a follow up to our previous letter dated May 17, 2018 we would like to provide some additional feedback with respect to this Land Use Redesignation application. Our previously stated position was of non-support for this application. Having subsequently met with the applicant (on June 15, 2018) and other stakeholders, the BNA would like to provide further feedback and consideration of some of the details that we'd like to see changed before we could support this application.

Include meaningful improvements to the underpasses

The applicant has pitched this project as a "Calgary Underpass Improvement Project" and uses language (such as 'outdated' vs. 'modern') to position this as an improvement to the area. However, this type of signage is vehicle-oriented and not in keeping with the pedestrian-oriented character of our community. The proposed signage risks disrupting and eroding the quality of the public realm in the Beltline.

The BNA could support meaningful changes to the underpasses to mitigate CPTED issues and improve the quality of the urban experience beyond the applicant's proposal to simply add cladding to mask the "dilapidated" condition. More meaningful improvement could take the form of repainting the rusting railway bridges, repairing overhead leaks from the railway deck, and replacing railings and outdated wall-pack fixtures in line with what has already been done on most of the underpasses using BCIF (Beltline Community Investment Fund) funding from the community.

Keep the 4 Street SW underpass billboard-free

The applicant has proposed adding a digital billboard to the 4 Street SW underpass where currently there is no existing third-party advertising of any kind. The community and the City have currently invested \$6.7-million in revitalizing this underpass including \$2.2-million for the "animated light" public art set to be installed in this space. Adding an LED billboard with third-party advertising would seriously undermine the efforts to improve this space and would compromise the impact of this significant public art investment for the City. The BNA is strongly opposed to a digital billboard at this location.

Reduce the per-sign size and the overall net square footage of advertising in the Beltline

The 29'x8' billboards described in the application are completely out of scale with the dense urban environment at these locations. On a previous project located in Vaughan, Ontario, the applicant actually installed digital billboards that were nearly the same size (1ft narrower) across a 6-lane wide roadway that is approximately 25ft wider than the roadways at the proposed locations in the Beltline ([Dufferin St.](#) vs. Macleod Trail).

The mock-up photo included in the application of a digital billboard at the 9 Ave SE underpass is a misrepresentation that makes the 8'x29' billboard size described in the application appear significantly smaller than reality. A 29ft-wide billboard would in fact cover the entire span of the eastbound vehicle lanes as well as the south pedestrian sidewalk.

The BNA strongly opposes any new digital billboards that extend beyond the width of the two-lane roadway (the intended target audience for the advertisements). For the narrow Beltline underpasses, this would mean something closer to a 20ft-wide billboard.

The applicant is also using misleading language by stating that the application "will bring an overall reduction in third party advertising in the downtown core, resulting in the removal of 10 outdated signs on rail bridges, and replacing them with seven smaller displays". The application, as submitted, currently proposes replacing eight 5'x32' billboards in the Beltline with six 8'x29' digital billboards, which would actually result in a

Beltline Neighborhood Association Letter



net increase of 112 square feet of additional advertising space. That's not even factoring in that these new billboards will also be presenting new ads every 10 seconds, which would result in dramatically more advertisements per month.

The BNA could only support an application that actually resulted in a significant overall net decrease in square footage of third party advertising in the Beltline.

Limit the hours of operation to mitigate residential impact

The applicants claim of using "downward LED technology" comes without any technical substantiation in the form of viewing angle vs. brightness specifications. While the applicant has claimed this feature is a way of mitigating the impacts of the digital advertising on residential units, the billboards appeared just as bright when viewed from the +15 overpass on 1 Street SE across the 9th Avenue during their August 2nd demonstration (despite the applicant's claim that the billboard would "almost appear blacked-out" from above). The best mitigation of the residential impacts of these signs would be to have "off hours" when the signs do not operate, possibly in line with the City's noise bylaw (10PM-7AM). The BNA could not support 24-hour operation of billboards using the technology that has been proposed and demonstrated.

Ensure accountability that's operator-agnostic

It's important to point out that CP Rail's last venture with All Vision LLC in developing digital billboards in Canada resulted in the signs being quickly sold off to an operator by the name of Outfront Media in 2017. While CP Rail's and All Vision's outdoor advertising joint venture, Van Horne Outdoor, was announced back in 2015, to date the company has yet to launch a public website or appear to be in the business of operating third-party advertising. If this is any indication of what lies ahead for Calgary, the BNA would want to ensure that any agreements or commitments with the applicant with respect to the operations of the billboards be clearly articulated in the proposed direct control (or otherwise captured by the City in bylaw form) so that they are clearly understandable and enforceable regardless of any transfer of ownership or operations of these digital billboards to another company.

Include meaningful long-term community benefit

The application proposes a list of community benefits that in reality would provide little actual benefit back to the Beltline community directly impacted by these signs. Given the impacts to our community and our residents posed by these signs for advertising that is clearly targeted to out-of-area commuters, we'd like to see some of the revenues reinvested into the Beltline. A tangible benefit could be achieved from directing 5%-10% of the advertising revenues from these signs back towards the Beltline Neighbourhoods Association for community programming, where funding is currently lacking to support the nearly 25,000 residents that live in the Beltline.

Treat the Beltline like any other residential neighbourhood in Calgary

The introduction of LED-backlit digital billboards into a dense residential community such as the Beltline in Calgary poses challenges to our goals of building a better community and public realm for all of Calgary. We would like to emphasize that the introduction of digital billboards into any residential community in Calgary would not be acceptable to most local residents and that any relaxations made within the Beltline would set a new precedent for all other residential neighbourhoods in Calgary. Therefore, any relaxations made in the Beltline should only be considered under the lens of what would be appropriate for any residential neighbourhood in the city.

Thank you for your consideration and for giving the BNA an opportunity to provide a comment.

Sincerely,

A handwritten signature in black ink, appearing to read 'Tyson Bolduc'.

Tyson Bolduc
Director of Planning and Urban Development
Beltline Neighbourhoods Association

A handwritten signature in black ink, appearing to read 'Peter Oliver'.

Peter Oliver
President
Beltline Neighbourhoods Association

Victoria Park Business Revitalization Zone Letter



May 24, 2018

Re: CP rail underpass digital signage

To Whom it May Concern:

Pursuant and in addition to our letter of August 31, 2016 (attached), The Victoria Park Business Improvement Area (formerly BRZ), would like to reiterate our support.

The Victoria Park BIA has made a point of generally opposing most large format 3rd party commercial advertising because there is no public benefit. In this case, the flexible nature of the digital platform can allow for this and it has expressly been communicated, unlike with other advertising agencies, that there will be opportunities for community, not-for-profit and public service content.

A new factor that has emerged in solidifying our support is the mitigation of waste and environmental impact. Not continually reproducing hard copies, trucking and installing/de-installing does have benefits.

We would request further discussion and engagement around what integration may be possible. The Victoria Park BIA is about to launch the longest wirelessly controlled experiential lighting project in western Canada and we will be able to mimic the colours and patterns of any lighting installation in the city (Calgary Tower for example). For special events, the cumulative impact of being able to use one colour palate across multiple locations and platforms can be quite impactful. We also would request further detail of how community/NFP content will be submitted and approved.

To reiterate again, we believe the proposal will be a welcome addition to Centre City underpasses, will benefit our member businesses, local residents, and the wider Calgary community.

Kind regards,

A handwritten signature in black ink that reads "David Low".

David Low
Executive Director

Victoria Park Business Revitalization Zone Letter



August 31, 2016

Re: CP/All Vision Proposal

To Whom it May Concern:

The Victoria Park Business Revitalization Zone (BRZ) is pleased to support CP and All Vision's joint proposal. Digital signage along downtown rail underpasses will build on current revitalization efforts, and contribute positively to the public realm.

We are pleased to note CP and All Vision's offer of complimentary advertising to local festivals and events, which will further benefit residents and employees by connecting them to their community. We are also very pleased about the opportunity for the digital signs to feature parking, roads and traffic information to benefit commuters.

The proposed digital signage will furthermore provide welcome, additional lighting and visual interest to downtown Calgary's underpasses.

In summary, we believe the proposal will be a welcome addition to Centre City underpasses, and will benefit our member businesses, local residents, and the wider Calgary community alike.

Kind regards,

A handwritten signature in black ink that reads "David Low". The signature is written in a cursive, flowing style.

David Low
Executive Director

Calgary Downtown Association Letter



May 14, 2018

The City of Calgary
P.O. Box 2100, Station M
Calgary, AB, T2P 2M5

Attn: Mayor & Members of Council

Re: Digital Signage – CP Rail Underpass Improvement Project in the Downtown

Please accept this letter on behalf of the Calgary Downtown Association (CDA) in support of the proposed introduction of digital signage where existing vinyl banner-type signs are in place, namely on the CP Rail underpasses on 1st Street, 8th Street, 9th Avenue, 4th Street and Macleod Trail.

We understand the new technology will provide more flexibility to the advertising space, allowing real time information on traffic and public service announcements, such as Amber Alerts, parking, etc to be displayed. There would also be more flexibility for advertisers to engage in dynamic creative campaigns, including complimentary display times for city and community groups as well as preferred rates for local businesses. We believe that light from these signs will enhance safety of the areas where they are located, in addition to becoming complimentary to the pedestrian and cycling experience. Moreover, we are pleased to see this project uphold urban design principles in the downtown underpass guidelines as well as land use bylaws governing digital displays.

We support this digital signage project to improve CP Rail underpasses in the downtown and trust our comments will be helpful. Should you require anything further, please call me directly at (403) 215-1565.

Yours truly,

A handwritten signature in blue ink that reads 'Brad Krizan'.

Brad Krizan
Board Chair & Interim Executive Director

cc: Mr. Christopher Bentler, Van Horne Outdoor
Mr. Jeff Fielding, City Manager, City of Calgary
CDA Urban Development Committee

SUITE 520, LANCASTER BUILDING
304-8th Avenue SW
Calgary, Alberta T2P 1C2

403-215-1570
info@downtowncalgary.com

WEB downtowncalgary.com
BLOG getdown.ca



Land Use Amendment in Pine Creek (Ward 13) at 507 – 210 Avenue SW, LOC2018-0151

EXECUTIVE SUMMARY

This application was submitted by Stantec Consulting, on behalf of Athem United / UAC II Corp representing the ownership group (Catherine A. Duffin, Timothy A. Duffin and 2007 United Lands Corp), on 2018 June 26. The application proposes to re-designate 6.13 hectares (15.15 acres) of undeveloped land within the southern community of Pine Creek from Residential – One Dwelling (R-1s) District to Residential – Low Density Mixed Housing (R-G) District to allow for:

- a wider variety of low density residential dwellings, including semi-detached dwellings, rowhouse buildings, duplex dwellings, and cottage housing clusters, in addition to the uses already allowed (e.g. single detached homes, secondary suites and backyard suites); and
- the uses listed in the proposed R-G designation.

The proposal conforms to the *West Macleod Area Structure Plan*, is in keeping with the applicable policies of the *Municipal Development Plan*, and conforms to the intent of the Pine Creek outline plan as approved in 2017.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 6.13 hectares ± (15.15 acres ±) located at 507 – 210 Avenue SW (Portion of Plan 1013290, Block 1, Lot 1) from Residential - One Dwelling (R-1s) District to Residential – Low Density Mixed Housing (R-G) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

Stantec Consulting, on behalf of Athem United / UAC II Corp. representing the ownership group (Catherine A. Duffin, Timothy A. Duffin, John David Duffin and 2007 United Lands Corp.), submitted the subject land use amendment application to The City on 2018 June 26 and has provided a summary of their proposal in the Applicant's Submission (Attachment 1).

Planning & Development Report to
Calgary Planning Commission
2018 October 18

ISC: UNRESTRICTED
CPC2018-1151
Page 2 of 8

Land Use Amendment in Pine Creek (Ward 13) at 507 - 210 Avenue SW, LOC2018-0151

The subject site is located in the neighbourhood of Creekside within the community of Pine Creek and is subject to the policies of the *West Macleod Area Structure Plan*. The *West Macleod Area Structure Plan* provides policy direction in four communities in southern Calgary.

On 2017 January 26, Calgary Planning Commission approved the Pine Creek outline plan, LOC2015-0112 (Attachment 2), followed by land use approval by Council on 2017 March 13 (CPC2017-092).

Tentative plan, SB2017-0180 Pine Creek Phase 01, to the northeast of the subject site, was approved by the Subdivision Authority on 2018 September 7. Subdivision of the subject application area has not yet occurred.

Land Use Amendment in Pine Creek (Ward 13) at 507 - 210 Avenue SW, LOC2018-0151

Site Context

The subject site, 507 – 210 Avenue SW, is situated in the southwest quadrant of the City in the developing community of Pine Creek. The area is bounded by 210 Avenue SW to the north and Sheriff King Street S to the east. To the west and south, the site follows the City of Calgary / Municipal District of Foothills boundary.

The Pine Creek outline plan is approximately 71.33 hectares (176.26 acres) in size and will be subdivided at the tentative plan stage into the appropriate lots and parcels. The portion of the subject site to be redesignated from R-1s District to R-G District, under the subject application, is approximately 6.13 hectares (15.15 acres) and is situated along Creekside Drive SW, the neighbourhood's main north-south road, and will be adjacent to R-1s and R-2 land use parcels to the west and a mix of R-1s, R-G, C-N2, and S-SPR land use parcels to the east.

Historically, the subject lands have been used for agricultural purposes including farming and grazing and have never been developed.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The land use amendment (Attachment 3) allows for a wider range of building types that are compatible with the 'Residential Area' and applicable policies identified in the *West Macleod Area Structure Plan* and in the *Municipal Development Plan*, as discussed in the Strategic Alignment section of this report.

Planning Considerations

Subdivision Design

The proposed land use amendment is not proposing any changes to the approved Pine Creek outline plan, as the parcel depths, road network, and servicing will remain the same. Access to the subject parcels will remain from the lane, as no access has been granted from Creekside Drive SW. All conditions from the Pine Creek outline plan, LOC2015-0112, shall still apply.

Land Use

The subject site is currently designated as Residential – One Dwelling (R-1s) District and is intended to accommodate residential development in the form of single detached dwellings in the developing area. Secondary suites may also be accommodated as a permitted use on the same parcel. The R-1s District allows for a maximum building height of 11 to 12 metres, depending on the parcel size and width, and a maximum of one main residential building (a building containing one or more dwelling units, but not including a backyard suite).

The proposed Residential – Low Density Mixed Housing (R-G) District is intended to apply to low density neighbourhoods within the developing area and accommodates a wide range of low density residential development in the form of cottage housing clusters, duplex dwellings, rowhouse buildings, semi-detached dwellings and single detached dwellings. Secondary suites (one Backyard Suite or Secondary Suite per unit) can also be accommodated in the R-G district

Land Use Amendment in Pine Creek (Ward 13) at 507 - 210 Avenue SW, LOC2018-0151

as well as carriage house lots. Secondary suites do not count against allowable density. The R-G District allows for a maximum building height of 12 metres, a maximum height of a backyard suite on a laned parcel of 10 metres, and a maximum of one main residential building unless the proposal includes cottage housing clusters. The intent of the subject application is to allow for more flexibility of housing choices in the developing greenfield area.

Density

The proposed application would allow for an increase in the anticipated number of lots from 85 lots to an anticipated 135 lots, a total increase of 50 lots. The Pine Creek outline plan (approved in 2017) proposed to accommodate a residential unit range between 1207 and 1551 units equating to an anticipated residential density of 17.78 units per hectare (7.19 units per acre). With an increase of 50 lots, the anticipated residential unit range increases to a minimum of 1257 units, equating to an increase in the overall anticipated residential density of 18.51 units per hectare (7.49 units per acre). This density increase provides a closer alignment to the suggested 20 uph (8 upa) by the *Municipal Development Plan* and contributes towards meeting the minimum residential density of 25 uph (9 upa) required when calculated over the entire *Area Structure Plan* area.

The *West Macleod Area Structure Plan* (Map 7) identifies a specific Residential Area density of 17.3 uph (7 upa). The anticipated residential density of 18.51 uph (7.49 upa) exceeds the minimum density requirement for the Residential Area.

The application meets the intent, goals and policies of the *Municipal Development Plan* and *West Macleod Area Structure Plan*.

Environmental

The proposed application is located within an undeveloped 'greenfield' area. Historically, the lands have been used for agricultural purposes as identified through an Environmental Site Assessment report, in which the potential for negative environmental conditions existing at the site were noted as being low.

No other environmental issues have been identified for the subject lands.

Transportation

All Conditions of Approval from the Pine Creek outline plan, LOC2015-0112, shall apply and remain for this amended application. All vehicle access for the proposed R-G District will remain from the lane.

Pedestrian access to the subject area is available from Creekside Drive SW while vehicle access is available from the rear lanes. The area will be served by Calgary Transit bus service with various stops located along Creekside Drive SW and Creekside Boulevard SW. On-street parking adjacent to the subject area will be unrestricted. A Transportation Impact Assessment was not required as part of this application; however, one was submitted with the Pine Creek outline plan to establish the street classifications and intersection lane configurations for the greater area.

Land Use Amendment in Pine Creek (Ward 13) at 507 - 210 Avenue SW, LOC2018-0151

Utilities and Servicing

Water, sanitary and storm servicing for the plan area was established under the previously approved outline plan (LOC2015-0112) and will not change as a result of this proposed land use amendment.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

There is no Community Association for the subject area and no letters from adjacent landowners or the general public were received.

No public meetings were conducted by the applicant or Administration in direct relation to this application.

Administration considered the relevant planning issues specific to the proposed redesignation and has determined the proposal to be appropriate. The design compatibility of discretionary uses with respect to the *West Macleod Area Structure Plan* and the surrounding neighbourhoods will be reviewed at the development permit stage.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory - 2014)

The subject site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the *SSRP* makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns strategies (subsection 8.14).

Municipal Development Plan (Statutory - 2009)

The subject site is located within the 'Residential – Developing – Future Greenfield' area of the *Municipal Development Plan (MDP)*. The *Municipal Development Plan* provides guidance for the development of new communities through the policies of the *West Macleod Area Structure Plan*. The applicable *MDP* policies encourage residential development that is diverse in nature, including a mixture of housing types.

The proposal is in keeping with relevant *MDP* policies as the R-G District provides for a variety of low density housing types.

Land Use Amendment in Pine Creek (Ward 13) at 507 - 210 Avenue SW, LOC2018-0151

South Macleod Trail Regional Policy Plan (Non-statutory - 2007)

The *South Macleod Trail Regional Policy Plan* provides further policy framework at a regional level and identifies the subject lands as 'Residential' and 'Conservation Study Area'. The proposed application is consistent with the policies of this plan.

West Macleod Area Structure Plan (Statutory - 2014)

The *West Macleod Area Structure Plan* provides more detailed direction with detailed policies and guidelines for development. The subject site is located within the 'Residential Area' on Map 4: Land Use Concept.

Housing Diversity (7.1) policies identify that a minimum of 30 per cent of the housing units within a community should be non-single detached housing units to meet the needs of different income groups and lifestyles. This includes, and is not restricted to, duplex dwellings, semi-detached dwellings, townhouse dwellings and secondary suites.

The proposed application is consistent with the applicable policies of this plan.

Pine Creek Outline Plan (Non-statutory - 2017)

On 2017 January 26, Calgary Planning Commission approved the Pine Creek outline plan, LOC2015-0112, on 2017 January 26, followed by land use approval by Council on 2017 March 13. The proposed redesignation of 6.13 hectares (15.15 acres) within the Pine Creek outline plan supports residential development that meets density requirements and provides a variety of residential development options in the community. The proposed application still conforms with the general intent of approved outline plan.

Our Shared Boundary: An Intermunicipal Development Plan for the Municipal District of Foothills and The City of Calgary (Statutory - 2017)

Within the *Municipal District of Foothills – City of Calgary Intermunicipal Development Plan*, the subject site is located in the interface area as indicated on Map 2: Interface Area. However, since the subject site is part of an approved Area Structure Plan, the application is not subject to the Interface Area policies as per Policy 2.5.4.

Social, Environmental, Economic (External)

The recommended land use district allows for a wider range of housing types than the existing Residential – One Dwelling (R-1s) District and still meets the density targets outlined within the *West Macleod Area Structure Plan* and associated outline plan. The proposal meets the objectives of the *Municipal Development Plan* to ensure a choice of housing forms, tenures and affordability to accommodate the needs of current and future Calgarians.

Land Use Amendment in Pine Creek (Ward 13) at 507 - 210 Avenue SW, LOC2018-0151

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed land use amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal conforms to the *West Macleod Area Structure Plan* and is in keeping with applicable policies of the *Municipal Development Plan*. The proposed Residential – Low Density Mixed Housing (R-G) District is intended to allow for additional low density residential forms to be built, providing flexibility for the developer of this site and better accommodating the housing needs of different age groups, lifestyles and demographics.

ATTACHMENT(S)

1. Applicant's Submission
2. Approved Outline Plan, LOC2015-0112
3. Land Use Amendment

Applicant's Submission

Stantec Consulting Ltd. ('Stantec') is pleased to submit the attached Land Use Amendment (LUA) application for Creekside on behalf of Timothy A Duffin, Catherine A Duffin, John David Duffin, and 2007 United Lands Corp. Please note that Anthem United / UAC II Corp. is representing the entire ownership group for the purposes of this application. The LUA application pertains to the lands located within the neighbourhood of Creekside, in the community of Pine Creek, civically addressed as 507 - 210 Avenue SW and legally described as:

PLAN 1013290
BLOCK 1
LOT 1
EXCEPTING THEREOUT ALL MINES AND MINERALS
AREA: 71.327 HECTARES (176.25 ACRES) MORE OR LESS

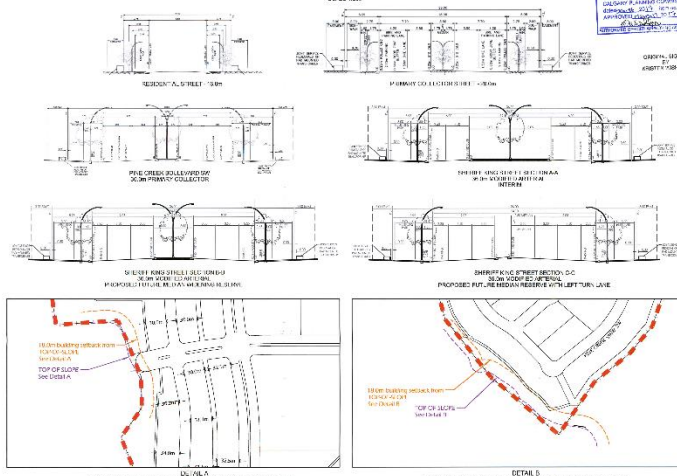
The Subject Lands are located within the approved Pine Creek Outline Plan ('OP') and currently designated as Residential – One Dwelling (R-1s), which allows for single detached homes with the potential for secondary suites. The LUA intent is to redesignate lots along Creekside Drive SW from R-1s to Residential – Low Density Mixed Housing (R-G) to allow for a wider variety of low density residential development.

The housing types included with R-G are similar in nature to R-1s, and as such still fall within the anticipated densities and proposed neighbourhood form approved in the Pine Creek OP. Prior unit calculations for the subject lands under the R-1s designation were anticipated at 85 total units. Total lots within the subject lands under the R-G District are estimated at 135.

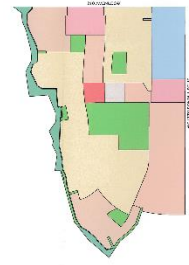
Approved Outline Plan, LOC2015-0112



ROAD CROSS SECTIONS



LAND USE PLAN



CONTEXT PLAN



OUTLINE PLAN LEGEND

- OUTLINE PLAN BOUNDARY
- LAND USE
- R-15
- R-2
- R-3
- R-4
- R-5
- R-6
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OUTLINE PLAN STATISTICS

Category	Area (sqm)	Area (sqft)	Percentage
Overall Statistics			
Overall Site Area	71,200	176,200	100%
Developable Site Area	67,000	170,200	94%
Non-Developable Area	4,200	11,000	6%
Land Use Statistics			
R-15	15,000	38,800	22%
R-2	30,000	77,600	45%
R-3	10,000	25,800	15%
R-4	5,000	12,900	8%
R-5	3,000	7,700	5%
R-6	2,000	5,100	3%
R-7	1,500	3,800	2%
R-8	1,000	2,500	1%
R-9	800	2,000	1%
R-10	700	1,800	1%
R-11	600	1,500	1%
R-12	500	1,200	1%
R-13	400	1,000	1%
R-14	300	700	1%
R-15	200	500	1%
R-16	100	250	0%
R-17	100	250	0%
R-18	100	250	0%
R-19	100	250	0%
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R-80	100	250	0%
R-81	100	250	0%
R-82	100	250	0%
R-83	100	250	0%
R-84	100	250	0%
R-85	100	250	0%
R-86	100	250	0%
R-87	100	250	0%
R-88	100	250	0%
R-89	100	250	0%
R-90	100	250	0%
R-91	100	250	0%
R-92	100	250	0%
R-93	100	250	0%
R-94	100	250	0%
R-95	100	250	0%
R-96	100	250	0%
R-97	100	250	0%
R-98	100	250	0%
R-99	100	250	0%
R-100	100	250	0%



PINE CREEK OUTLINE PLAN

Section 9-22-1-5

Stantec

URBAN SYSTEMS

2337 0902 09
Date: 2016/06/22
OUTLINE PLAN
PLN-OP-01

Scale: 1:2000

Proposed Land Use Amendment

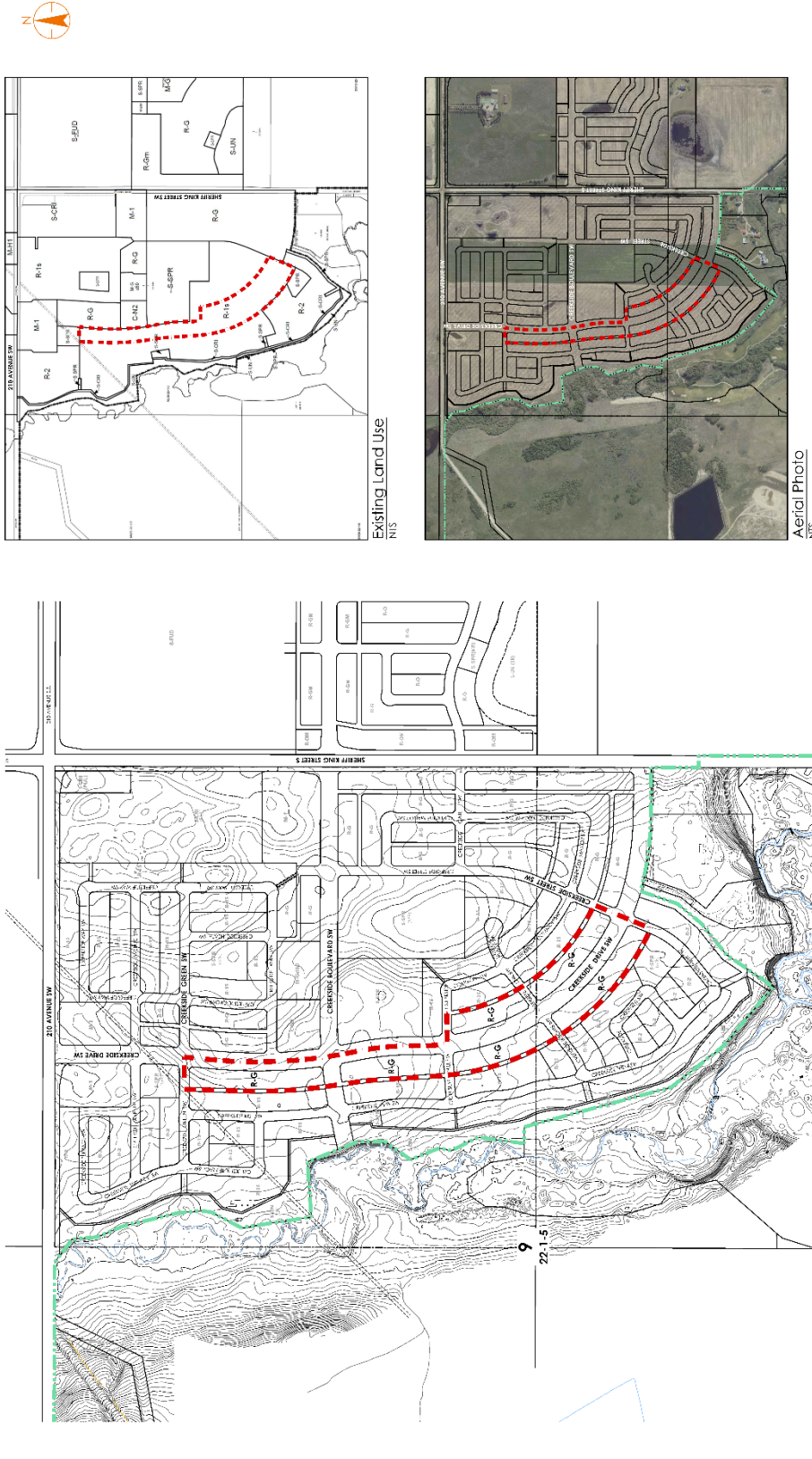


FIGURE 1.0 | CREEKSIDE
Land Use Amendment
 Plan 1013290, Block 1, Lot 1
 PREPARED FOR: TIMOTHY A. DUFFIN, CATHERINE A. DUFFIN,
 JOHN DAVID DUFFIN, and 2007 UNITED LANDS CORP.
 City of Calgary Boundary
 Subject Lands: R-1s to R-G 6.13 ha± (15.14 ac±)
 Frontage within Subject Lands = 1,070m (3,510')
 Anticipated lots under approved R-1s Designation = 85
 Anticipated lots under proposed R-G Designation = 135

Note: Street names as per proposed tentative plans.

Anthem United
Stantec
 116500541
 September 26, 2018

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ISC: UNRESTRICTED
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Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 – 26 Avenue SW, LOC2018-0167

EXECUTIVE SUMMARY

This application was submitted by Arup Datta Architecture Ltd on 2018 July 18 on behalf of the landowner, Kasa Enterprises Inc. The application proposed to change the designation of this property from Residential – Contextual One/Two Dwelling (R-C2) District to Mixed Use – Active Frontage (MU-2f1h10) District to allow for:

- a medical clinic with office and retail units;
- a maximum building height of 10.0 metres;
- a maximum floor area ratio of 1.0; and
- the uses listed in the proposed MU-2 District.

The proposal is in keeping with the applicable policies of the *Municipal Development Plan* (MDP), which identifies the subject site appropriate for added population and commercial and service uses. However, a minor map amendment to the Richmond Area Redevelopment Plan (ARP) is required to accommodate the proposed land use redesignation.

A development permit for the medical clinic, office and retail building has been submitted and is under review.

ADMINISTRATION RECOMMENDATION:

That the Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendment to Richmond Area Redevelopment Plan (Attachment 3); and
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw the proposed redesignation of 0.08 hectares ± (.20 acres ±) located at 2104 - 26 Avenue SW (Plan 8997GC, Block 8, Lots 1 and 2) from Residential – Contextual One / Two Dwelling (R-C2) District to Mixed Use – Active Frontage (MU-2f1h10) District; and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

**Planning & Development Report to
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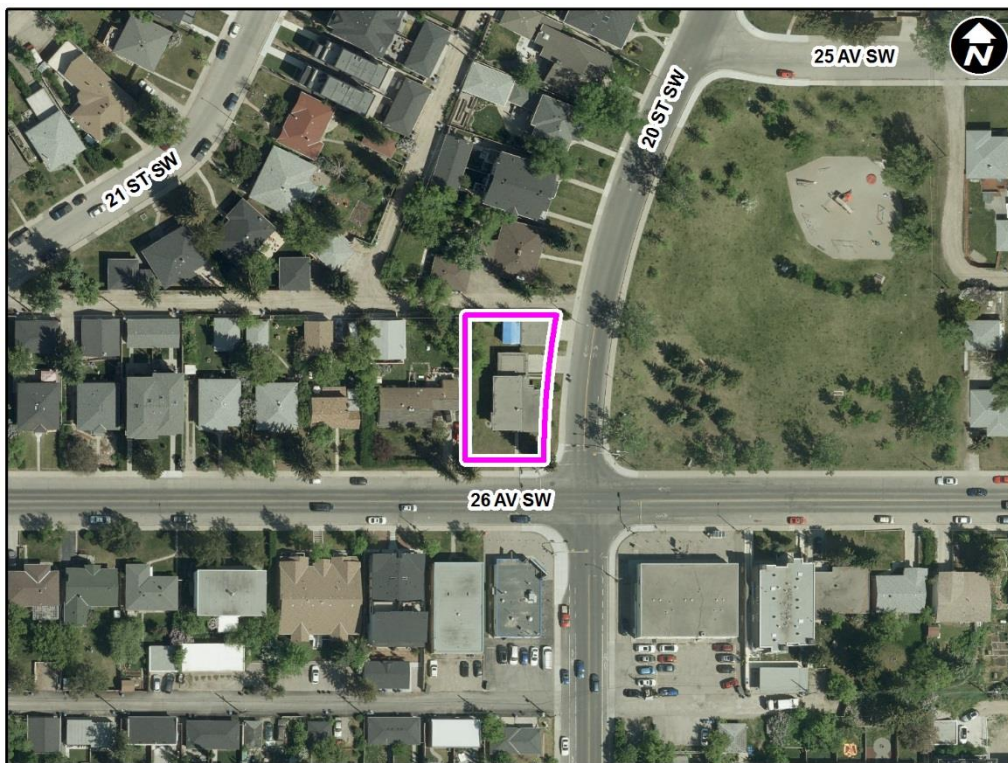
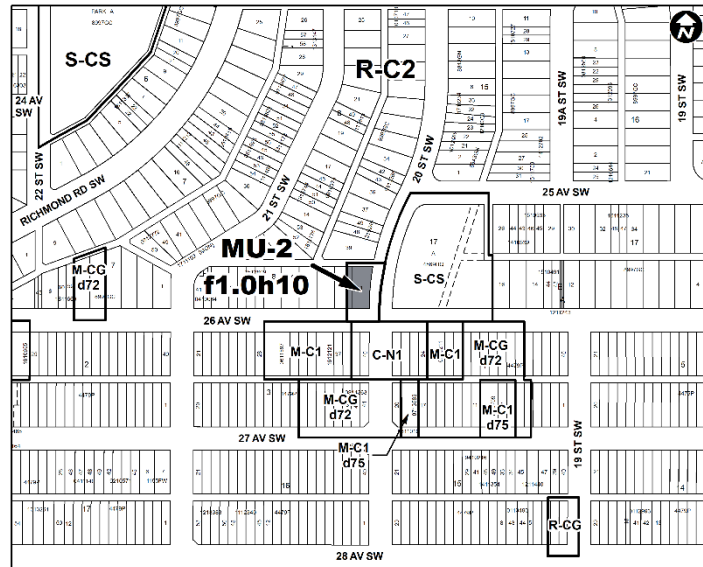
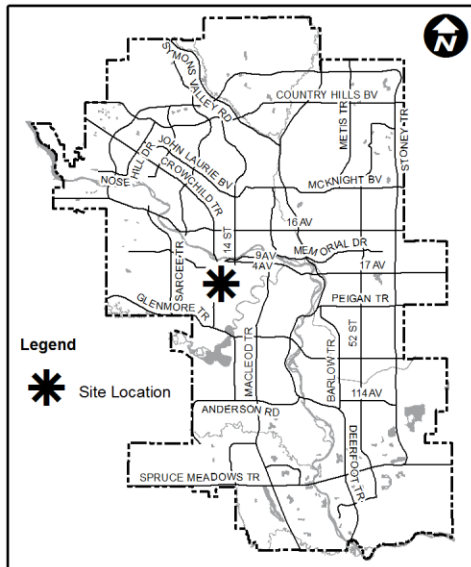
**Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 - 26
Avenue SW, LOC2018-0167**

BACKGROUND

The land use amendment application has been submitted to The City of Calgary by Arup Datta Architecture Ltd. on behalf of the landowner Kasa Enterprises Inc. on 2018 July 18 (see Attachment 1 for the Applicant's Submission). A development permit (DP2018-3436) for a medical, office and retail building has also been submitted by Arup Datta Architecture Ltd. on 2018 July 18 and is under review (see Attachment 4 for additional information).

Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 - 26
Avenue SW, LOC2018-0167

Location Maps



Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 - 26 Avenue SW, LOC2018-0167

Site Context

The subject site is located in the community of Richmond at the northwest corner of 26 Avenue SW and 20 Street SW. Surrounding development is characterized by a mix of single and semi-detached homes and commercial development. To the south of the site, parcels are developed with commercial (dry cleaning, hair salon and retail stores) and multi-residential buildings, and to the east is a City park with a playground.

The site is approximately 0.08 hectares \pm (0.20 acres \pm) in size with approximate dimensions of 23.0 metres by 41.0 metres and slopes upwards from all portions of the site. A rear lane exists along the north portion of the site. The property is currently developed with a single detached dwelling and a rear detached garage with parking pad accessed from 20 Street SW.

As identified in Figure 1 below, the community of Richmond has not risen above its peak population of 5,080 in 1968, and has most recently seen its community decline by 198 residents. The potential of adding a medical, office and retail building at the subject site would not only strengthen the existing commercial business at the intersection of 26 Avenue SW and 20 Street SW, but could also serve as a catalyst to attract more residents and local business to the community.

Figure 1: Community Peak Population

Richmond	
Peak Population Year	1968
Peak Population	5,080
2017 Current Population	4,882
Difference in Population (Number)	-198
Difference in Population (Percent)	-3.9%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [Richmond](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for a range of building form and uses that have the ability to be compatible with the immediate surrounding built form of the existing neighbourhood. While a minor amendment to the ARP is required, the proposal generally meets the objectives of the applicable policies as discussed in the Strategic Alignment section of this report.

Planning Considerations

Land Use

The existing R-C2 District is a residential designation in developed areas that is primarily for single-detached, semi-detached and duplex homes. Single detached homes may include a

Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 - 26 Avenue SW, LOC2018-0167

secondary suite. The R-C2 District allows for a maximum building height of 10.0 metres and a maximum of two dwelling units.

The proposed MU-2 District is intended to be characterized by buildings that have: street-oriented design, facades with multiple uses and frequent entries at grade facing the street and appropriate transition to lower scale residential buildings on adjacent parcels through building location, building massing and landscaping. The MU-2 District contains a range of discretionary commercial uses which include: convenience food store, pet care service and restaurant neighbourhood. The proposed land use will also contain a maximum floor area ratio of 1.0 and maximum height of modifier of 10.0 metres (approx. 3 storeys).

Development and Site Design

The rules of the proposed MU-2 District provide basic guidance for future site development including appropriate uses, height and building massing, landscaping and parking. Given the specific context of this corner site, additional items that are being considered through the development permit process include, but are not limited to:

- ensuring an engaging built interface along both 26 Avenue SW and 20 Street SW frontages;
- achieving transition to lower scale residential buildings on adjacent parcels through building location and massing and landscaping.
- emphasis on the at grade retail entrances; and
- ensuring that the buildings facades on both the avenue and street have sufficient transparent glazing.

Transportation Networks

The subject site is a corner lot, with lane access that fronts on to 26 Avenue SW. The site is approximately 40 metres from a Westbound Route 6 bus stop, which goes to the Westbrook LRT station (Primary Transit route, approx. 3.6 kilometres away) that has routes to the West and Downtown Core. The site is also approximately 65 metres away from an Eastbound Route 6 bus stop, which provides service to the Downtown Core and LRT routes.

The subject site is within Residential Parking zone “O”, which currently does not have any restricted on-street parking, except on 26 Avenue SW for a snow event for the Route 6 bus.

At the time of redevelopment, all bylaw required parking requirements (stall size, amount, etc.) are to be met on site.

Utilities and Servicing

Water and sanitary sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time.

Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 - 26 Avenue SW, LOC2018-0167

Storm is not available to service the site. Individual servicing connections as well as appropriate stormwater management will be considered and reviewed at development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders (i.e. Community Association, Ward Councillor and adjacent landowners) and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

Administration received comments from the Richmond/Knob Hill Community Association on the proposal (See Attachment 2), which are summarized below:

- generally supportive of the subject parcel being redeveloped as a low density mixed-use development with retail/commercial at grade as the parcel is located: on the NW corner of two collector roads, adjacent to retail/commercial developments and currently has an older dwelling (which does not afford the best use of the site given its location);
- a low density mixed-use development in this location would be of benefit to the community, as it would add to the mix of shops at the intersection and help further activate the park; and
- the City should consider redesignating the 2100 block on 26 Avenue SW to a mixed-use land use district which would be more appropriate (i.e. which would allow for a higher density and height).

Administration received one letter of opposition to the application. Reasons stated for the opposition are summarized below:

- caveat on title specifically details what is allowed to be developed on the subject site, which does not include a commercial building; and
- should comply with the City Council adopted Richmond ARP, which designates the site as 'Conservation and Infill'.

Administration considered the relevant planning issues and policies specific to the proposed redesignation, and has determined the proposal to be appropriate. As mentioned, the subject site is identified within the MDP as being appropriate (where warranted) for increased population and a mix of commercial and service uses (i.e. medical clinic). The MU-2 District not only offers a range of discretionary commercial uses, but also includes dwelling units. Further, the district contains guidelines for the appropriate transition to lower scale residential buildings on adjacent parcels through building location, building massing and landscaping. As such, the MU-2 District is deemed to be appropriate as it serves an effective transition between the existing commercial and residential districts, which surround the subject site. In terms of the restrictive covenant, the City is not subject to this item when conducting its review and any enforcement is a civil matter between property owners.

Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 - 26 Avenue SW, LOC2018-0167

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The site is located within the City, Town area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use patterns.

Municipal Development Plan (Statutory – 2009)

The subject parcel is located within the 'Residential – Developed – Inner City' area as identified on Map 1: Urban Structure in the MDP. The applicable MDP policies encourages the redevelopment of inner-city communities by adding population and a mix of commercial and service uses, while at the same time respecting the scale and character of the existing neighbourhood.

The proposal is in keeping with relevant MDP policies as the rules of the MU-2 District provide for a development form that may be sensitive to the existing residential development in terms of its built form and has residential, commercial and service uses as listed uses, which would be of benefit to the existing community.

Richmond Area Redevelopment Plan (Statutory, 1986)

The subject parcel is located within the 'Conservation/Infill' areas as identified on Map 2: Land Use Policy in the *Richmond Area Redevelopment Plan* (ARP), which is intended for:

- low profile infill development that is compatible with the surrounding dwellings.

In order to accommodate the proposed MU-2 District, a minor amendment to Map 2 is required to change the land use category to 'Local Commercial' (Attachment 3), which is intended to:

- provide for goods and services catering to the needs of surrounding neighbourhoods; and
- have uses that include: banks, dry cleaners, small offices and shops and retail food stores.

Social, Environmental, Economic (External)

An Environmental Site Assessment was not required for this application.

The recommended land use allows for a wider range of housing types and commercial uses than are currently permitted under the R-C2 District and as such, the proposed change can provide may better accommodate the housing and service needs of different age groups, lifestyles and demographics.

Policy Amendment and Land Use Amendment in Richmond (Ward 8) at 2104 - 26 Avenue SW, LOC2018-0167

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are not significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the Municipal Development Plan. The proposed MU-2 District was designed to be implemented in proximity to or directly adjacent to low-density residential development. The proposal allows for a building form that has the ability to be compatible with the established building form and offers a range of commercial uses that can serve the needs of the community and surrounding neighbourhoods.

ATTACHMENT(S)

1. Applicant's Submission
2. Community Association Submission
3. Proposed Amendment to the Richmond Area Redevelopment Plan
4. Proposed Development (DP2018-3436)

Applicant's Submission



ARCHITECTURE
INTERIOR DESIGN
MASTER PLANNING
URBAN PLANNING

ARUP DATTA ARCHITECT LTD.

July 18, 2018
Project #1663

PROJECT: 2104 – 26TH AVE SW, CALGARY, ALBERTA

RE: BRIEF DESCRIPTION OF THE PROJECT & DESIGN RATIONALE FOR THE PROPOSED DEVELOPMENT REGARDING ITS APPROVAL FOR REZONING & DP APPLICATION

We are pleased to submit this narrative as an important preamble for the combined Re-Zoning and Development Permit application for this project. Our Client's intent for the project is to create a unique Mixed-Use project to have retail use on the main floor and Medical, Dental & Professional offices at the 2nd floor.

The Client's ultimate objective is to create initially a 2-level facility which will consist of a small amount of CRU spaces at main ground level of 183m² and 446m² at the second level for Medical, Dental, & Professional offices. All the parking is at grade with no underground parking.

BACKGROUND:

The site is bounded by 26th avenue SW and 20th street on the south and east side of the property and residential on the west and a lane on the north. The site sits on the transit corridor of the City of Calgary and there are bus stops directly to the east of the property along 26th avenue.

The applicant has already completed a Pre-App #PE-2017-01397 and based on the Pre-Application Assessment Form comments and advisory comments received from planning have been addressed.

The current zoning is R-C2 residential designation and the intent of our client is to rezone the property to MU-2. Under this land use designation, it accommodates a mix of commercial (Medical Clinic and Office as discretionary uses) and residential uses and responds to local area context by establishing maximum building height for individual parcels.

The total site area of the lot is 0.076 HA (760.80m²) approximately. The frontage of the site along 26 Avenue SW is 19.79 meters & along 20th street is 36.72 meters. The pedestrian access to the site is from the south and east side and proposing the vehicular access to the site from the lane.

1. PROJECT OBJECTIVE:

Our client's objective is to develop this parcel with a good quality Mixed-Use retail and professional offices facility which has a great demand in this neighborhood of Calgary. It

1909 - 10th Avenue, SW, Calgary, Alberta, T3C 0K3 e-mail:adal@telus.net Tel: (403) 244 8818 Fax: (403) 244 8982

Applicant's Submission

2

will enhance this community and will draw a fair amount of pedestrian traffic from this neighborhood.

The total overall parking at grade provided is 14 stalls.

We are proposing 2 main entries to the building from 26th avenue & 20th street SW and one can obtain access to the building from the parking area at the rear of the building.

The Architectural design of the building will be good quality complimenting the urban fabric of this area and the current guidelines.

2. **PLANNING ITEMS:**

The proposed development will have a great presence in the Community. It meets the uses which are within permitted & discretionary uses proposed.

3. **DENSITY:**

The maximum floor area ratio is 1.0 and our total density meets the requirement.

4. **SETBACK AREAS:**

The front, rear and east side setbacks are provided as required per the zoning bylaw. The west setback is proposed as a 1.2m setback as indicated on the Pre-Application submission and not identified as a concern.

The justification for the 1.2m setback is that any development on the site under the original RC-2 zoning could have been built to the 1.2m setback up to a 10m height. The proposed new building is within the 10m height and there are no windows on the west side therefore privacy will not be an issue and the 1.2m setback provides the same access as required by transportation per a RC-2 site therefore we respectfully request a variance to permit the 1.2m setback on the west side of the property.

5. **LANDSCAPING:**

The proposal will provide Landscaping with combination of hard and soft landscaping having great connectivity. There are no existing City street trees therefore Four (4) American Elm street trees will be provided as requested by Parks.

6. **BUILDING HEIGHT:**

The proposal meets the requirements for maximum building height of 10.0m.

PROJECT DESIGN FEATURES & ANALYSIS:

This proposal has evolved following extensive reviews of 3 different options with the City of Calgary Planner and it was suggested that we should proceed with rezoning to MU-2 zoning.

We have developed this proposal considering the zoning and keeping in view of the economic impact and cultural social needs of this area.

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Applicant's Submission

3

- Well sited project in terms of Urban Design, Building Massing along 26th avenue and 20th street SW.
- The design addresses all urban design issues.
- Well connected for pedestrians from around the site and within the site.
- High quality building design & use of materials, Glazing, Brick/Stone/Aluminum & Dryvit exterior cladding materials.
- Good quality landscaping all along the two roads on the east and west sides of the property.
- The parking area is well buffered by means of good landscape design with minimum impact to the neighborhood.
- Development will have a low density compared to the allowable maximum which will not have any negative impact within this area.
- Site lighting will be properly designed to meet all the guidelines.

URBAN DEVELOPMENT:

- All services and guidelines will be dealt with in detail as we move forward with the project.

WASTE RECYCLING:

- Our site plan indicates the location of the Waste Recycling using the Molok system.

TRANSPORTATION:

The access for the parking is from the lane and we are providing more parking stalls than required. Loading is proposed from the lane and parking driveway with designated times that loading requirements can occur.

PARKS/LANDSCAPING:

In general, we shall comply with the overall landscaping guidelines.

Applicant's Submission

4

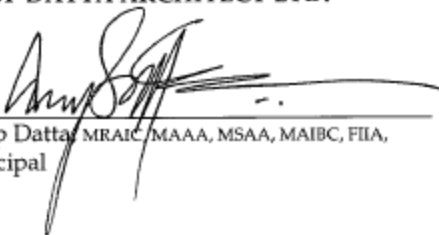
CONCLUSION:

The design and overall size of the project creates an economically viable project for the client. We have optimized the development potential of the property which provides the Client this opportunity to create a good quality project for the area.

Considering the well thought out development and all the positive qualities of the project we are seeking enthusiastic support from the City of Calgary to deal with this application in a favorable manner so that we can move forward with Rezoning and the DP application together.

RESPECTFULLY SUBMITTED:

ARUP DATTA ARCHITECT LTD.



Arup Datta, MRAIC, MAAA, MSAA, MAIBC, FIAA,
Principal

Community Association's Submission

From: Doug Roberts [mailto:development@richmondknobhill.ca]
Sent: Friday, September 14, 2018 9:52 AM
To: Friedman, Jarred B. <Jarred.Friedman@calgary.ca>
Cc: Dennis Cant <[REDACTED]>; Dana Hill <[REDACTED]>; Nancy Miller <[REDACTED]>; Nicholas Milliken <[REDACTED]>; Shamir Charania <[REDACTED]>; Chris Harris <[REDACTED]>; President RKHCA <president@richmondknobhill.ca>
Subject: [EXT] Community Association Comments on LOC2018-0167 -- 2104 26 AV SW

Mr. Friedman

The Development Committee for the Richmond/Knob Hill Community Association (the "Association") has reviewed the captioned application to redesignate the subject parcel from the existing R-C2 land use district to an MU-2 f1.0 h10 land use district (the "Application") and provides the following comments at this time.

The Association is generally supportive of the subject parcel being redeveloped as a low density mixed-use development with retail/commercial at grade, as:

- 1) the subject parcel is located on the NW corner of a signalled intersection of 2 collector roads, 20 ST SW and 26 AV SW (the "Intersection");
- 2) there are existing retail/commercial developments on the SW and SE corners of the Intersection, and a park on the NE corner of the Intersection (the "Park");
- 3) the subject parcel is currently occupied by an older low density dwelling, which the Association does not consider to be the best use for the subject parcel given its location at the Intersection; and
- 4) a low density mixed-use development in this location would be of benefit to Richmond/Knob Hill, as it would add to the mix of shops at the Intersection and potentially help to further activate the Park.

However, the Association sees a number of issues that should be addressed, some of which relate specifically to the Application and others of which would relate to any low density mixed-use development on this block face. These issues include the following:

- 1) Mix of Uses — The Association feels that dwelling units on the upper floor(s) of the proposed development would be more appropriate than offices in this location, as dwelling units would help to increase density and the diversity of available housing options in the area by providing dwelling units that are both:
 - (a) more affordable for first-time homebuyers; and
 - (b) more suitable for seniors and other homebuyers looking to downsize, than the 2- and 3-storey single detached and semi-detached dwelling units that comprise the bulk of the redevelopment that has taken place to date in our community.

Community Association's Submission

2) Inconsistency with Adjacent R-C2 Parcels to the West — The proposed MU-2 land use district does not transition overly well to the adjacent R-C2 parcels to the west in certain respects, such as front setback. In this regard, if the City were to decide that this application warrants approval, it would be helpful for the City to at the same time address what uses/land use districts it would consider appropriate for the remainder of the unredeveloped properties along the east half of this 2100 block face. To what extent would the City be inclined to approve applications to redesignate those other R-C2 parcels to MU-2, or to some other mixed-use or higher density land use district? If they would be so inclined, then it is quite possible that the floor area ratio and building height limits proposed in the Application are lower than they should be.

3) City of Calgary Restrictive Covenant — It has recently been brought to our attention that a mixed-use development in this location may contravene the provisions of a 1950 City of Calgary restrictive covenant that is registered against title to all or substantially all of the parcels in this area north of 26 Avenue SW (the "Covenant"), particularly Paragraph 3 thereof which states that "No Lot forming part of this scheme or any building erected thereon shall be used for any trade or business or otherwise than for private residential purposes". A copy of the Covenant is attached for your convenience.

Thank you.

Doug Roberts
Chair, Development Committee
Richmond/Knob Hill Community Association


development@richmondknobhill.ca

Proposed Amendment to the Richmond Area Redevelopment Plan

WHEREAS it is desirable to amend the Richmond Area Redevelopment Plan Bylaw 17P85, as amended;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26, as amended:

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

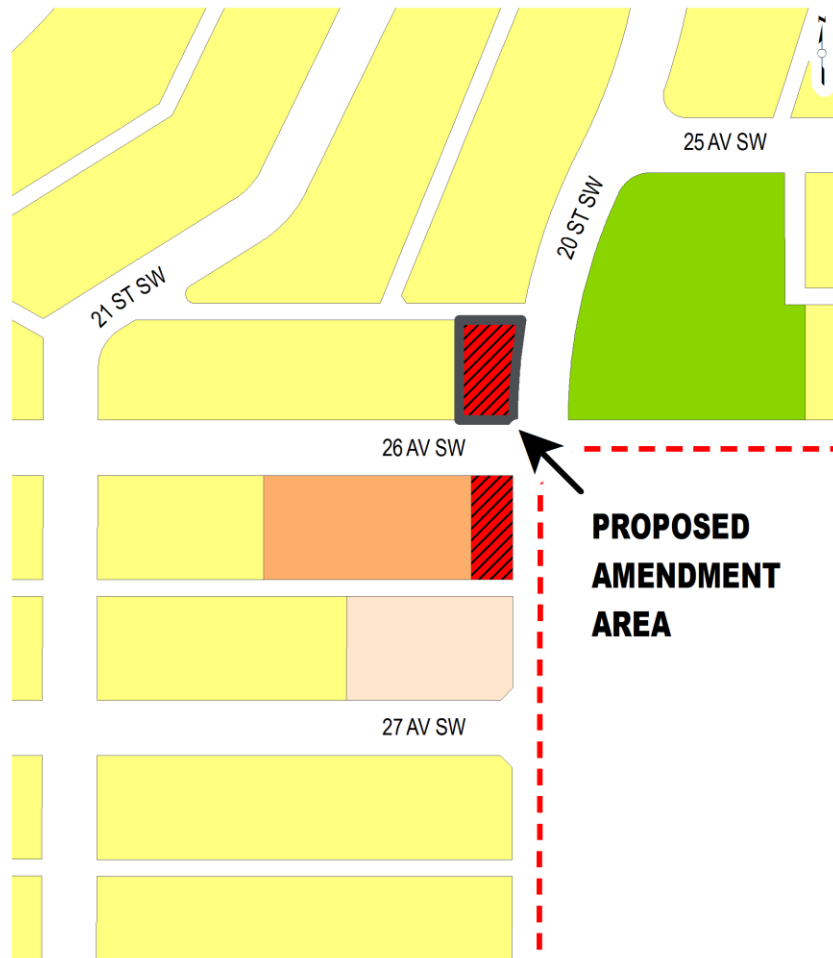
1. The Richmond Area Redevelopment Plan attached to and forming part of Bylaw 17P85, as amended, is hereby further amended as follows:
 - (a) Amend Map 2 entitled "Land Use Policy" by changing 0.08 hectares \pm (0.20 acres \pm) located at 2104 – 26 Avenue SW (Plan 8997GC, Block 8, Lots 1-2) from 'Conservation/Infill' to 'Local Commercial, as generally shown in the sketch below:

Map 2

Land Use Policy

Legend

-  Study Area Boundary
-  Conservation/ Infill
-  Low Density Residential
-  Medium Density Residential
-  Open Space
-  Local Commercial

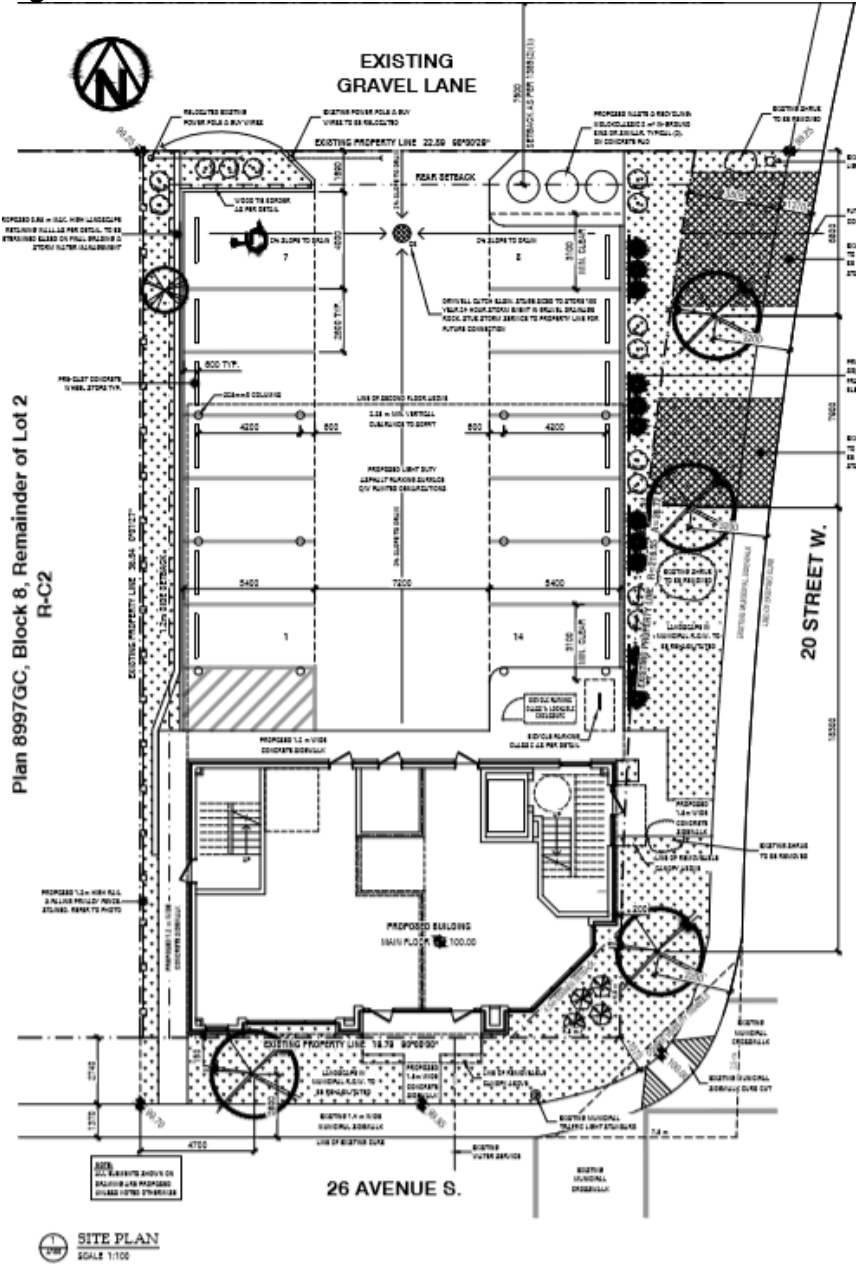


Proposed Development (DP2018-3436) Summary

A development permit application (DP2018-3436) has been submitted by Arup Datta Architecture Ltd. on 2018 June 25. The proposal is for a two-storey medical, office and retail building. The development permit application is currently on hold, pending resubmission of amended plans by the Applicant. The following excerpts (Figures 1-4) are from the initial development permit submission, and are being provided as a general overview of the proposal and for information purposes only.

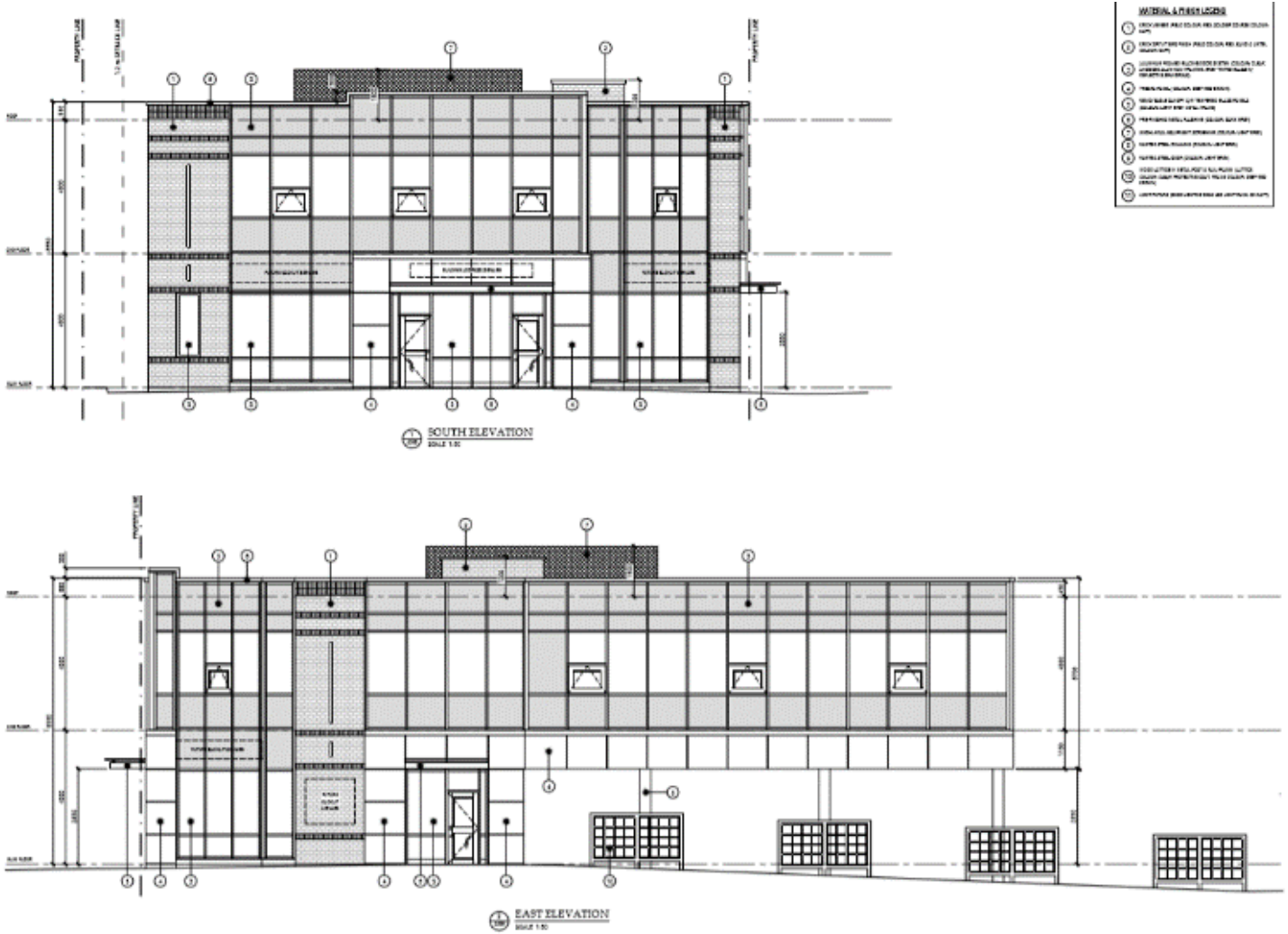
Administration's review of the development permit will determine the ultimate building design, number of units and site layout details such as parking, landscaping and site access. No decision will be made on the development permit application until Council has made a decision on this land use redesignation.

Figure 1: Site Plan



Proposed Development (DP2018-3436) Summary

Figure 2: South (26 Avenue SW) & East (20 Street SW) Elevations



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ISC: UNRESTRICTED
CPC2018-1112
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Land Use Amendment in Valleyfield (Ward 9) at 4575 – 25 Street SE, LOC2018-0177

EXECUTIVE SUMMARY

This land use amendment application was submitted on 2018 August 01 by Certus Developments Inc, as owner and applicant. The application proposes to change the designation of the subject parcel from a DC Direct Control District based on the *Land Use Bylaw 2P80 C-2* General Commercial District to Commercial – Corridor 3 f2.0h16 (C-COR3 f2.0h16) District of *Land Use Bylaw 1P2007* to allow for:

- a wider range of mid-scale commercial and retail uses along major roads to support a primarily industrial area;
- a maximum floor area ratio (FAR) of 2.0;
- a maximum building height of 16.0 metres; and
- the uses listed in the proposed C-COR3 designation.

The proposal is in keeping with applicable policies of the *Municipal Development Plan* and the *Southeast Industrial Area Structure Plan*.

A change of use development permit application for a Cannabis Store was submitted on 2018 April 25. The Development Authority issued a refusal decision for DP2018-1947 on 2018 August 09, as Cannabis Store is not a listed use in the existing direct control district.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.35 ± hectares (0.86 acres±) located at 4575 – 25 Street SE (Plan 9912583, Block 3, Lot 3) from DC Direct Control District to Commercial – Corridor 3 f2.0h16 (C-COR3 f2.0h16) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

This land use amendment application was submitted on 2018 August 01 by Certus Developments Inc., as owner and applicant. As indicated in the Applicant's Submission (Attachment 1), the applicant proposing a land use designation that is consistent with the existing commercial land use district that will allow for a transition from *Land Use Bylaw 2P80* to the current *Land Use Bylaw 1P2007*. A land use redesignation would accommodate a wider range of allowable uses, including the Cannabis Store use. A development permit for a Cannabis Store was submitted on 2018 April 25 and was refused on 2018 August 09.

Land Use Amendment in Valleyfield (Ward 9) at 4575 - 25 Street SE, LOC2018-0177

Site Context

The subject site is located in the industrial area of Valleyfield, southeast of the intersection at Barlow Trail SE and Peigan Trail SE. Lands immediately to the north and south of the site are developed with commercial and restaurant uses. Lands farther south and to the east are developed with light and medium general industrial uses. The residential community of Dover is located north of Peigan Trail SE.

The site is approximately 0.35 hectares \pm (0.86 acres \pm), and is developed with a one storey commercial building and surface parking. The site is accessed from 25 Street SE.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This proposal allows for a range of commercial and retail uses, and meets the objectives of applicable policies as discussed in the Strategic Alignment section of this report.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

This application proposes to redesignate the site from the existing DC Direct Control District (*Bylaw 114Z98*), which is based on the C-2 General Commercial District of *Land Use Bylaw 2P80* to the Commercial Corridor 3 f2.0h16 (C-COR3 f2.0h16) District of *Land Use Bylaw 1P2007*.

The proposed C-COR3 f2.0h16 District, with the density and height modifiers of 2.0 FAR and 16.0 metres, is consistent with existing Direct Control bylaw currently in effect. The intent of the C-COR3 land use district is to allow for a wider range of mid-scale commercial and retail uses along major roads to support a primarily industrial area. The allowable uses include the Cannabis Store use.

The existing building on the subject parcel is less than 16 metres in height and has floor area ratio of 0.1. The density and intensity of the proposed C-COR3 f2.0h16 District is aligned with the relevant policies, supporting a range of uses and flexibility for future redevelopment of the site.

Development and Site Design

The rules of the proposed C-COR3 f2.0h16 District will provide guidance for the future development of the site, including appropriate uses, height and parcel coverage. Vehicular access to the site is provided from 25 Street SE. The site is developed with an existing one storey commercial building and surface parking. A development permit application for comprehensive redevelopment of the site has not been submitted.

Land Use Amendment in Valleyfield (Ward 9) at 4575 - 25 Street SE, LOC2018-0177

Environmental

No environmental issues have been identified at this time.

Transportation

A Transportation Impact Assessment or Parking Study was not required as part of this land use amendment application. Vehicular access to the subject site is from 25 Street SE. The site is well served by Calgary Transit with bus stops located within 100 metres of the development on 25 Street SE.

Utilities and Servicing

Water, sanitary, and storm servicing are available for the subject site and can accommodate potential redevelopment without the need for off-site improvements at this time.

Stakeholder Engagement, Research and Communication

In keeping with Administration's practices, this application was circulated to stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

Administration received a letter in opposition to the application from the public. Reasons stated for opposition are summarized below:

- concerns with the proposed maximum building height, which is inconsistent with the height of existing buildings and may potentially obscure visibility of existing buildings and signage from the main road; and
- concerns with the proposed floor area ratio (FAR) of 2.0, which is inconsistent with the existing intensity of development and potentially impact the parking situation in the area.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP), which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Municipal Development Plan (Statutory, 2009)

The site is located within the "Standard Industrial" typology as identified on Map 1: Urban Structure Map in the *Municipal Development Plan* (MDP). The Standard Industrial typology is intended to accommodate existing planned industrial areas that contain a mix of industrial uses

Land Use Amendment in Valleyfield (Ward 9) at 4575 - 25 Street SE, LOC2018-0177

at varying intensities. These areas continue to offer a broad variety of industrial uses and as the area redevelops, the industrial character should be maintained. Notwithstanding, uses that support the industrial function of this area and cater to the day-to-day needs of area businesses and their employees may be supported.

Southeast Industrial Area Structure Plan (Statutory, 1996)

The subject site is identified within the “*Existing Commercial District*” in the *Southeast Industrial Area Structure Plan (ASP)* on Map 2: Land Use and Transportation Plan. The ASP recognizes the need for commercial development in the Southeast Industrial Area to support the needs of the industrial employees. The ASP supports commercial development of an intensity that does not undermine the overall intent of the industrial policies for this area and where the road system and utility services are capable of accommodating such development.

Social, Environmental, Economic (External)

The proposed land use district would help to implement the policy goals of providing a wide range of commercial and retail uses that cater to the day-to-day needs of area businesses and their employees, while maintaining the industrial character and function of the area.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed land use amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal

REASON(S) FOR RECOMMENDATION(S):

The proposal is consistent with the applicable policies of the *Municipal Development Plan* and the *Southeast Industrial Area Structure Plan*, and the intent of the C-COR3 District.

ATTACHMENT(S)

1. Applicant's Submission

Applicant Submission

**4575 25th STREET
SE PROJECT
RATIONALE JULY
2018**

LAND USE RE-DESIGNATION APPLICATION ✓

**RE: 4575 25th Street
SE, CALGARY,
ALBERTA**

Owner:
Certus Developments Inc.

Corporate Directors:
Ron Ghitter
Jim Mitchell

Legal Description:
Plan 991 2583, Lot 3, Block 3

Information below is provided in support of a Land Use Re-designation application for the above noted address. Certus Developments Inc. is requesting a re-designation for the above noted site and offers this information as a lead in explanation to support the formal land use re-designation application.

The above noted retail property is a single level multi-tenant building of approximately 4,021 sf on 0.86 acres (0.346 ha) located on 25th Street SE adjacent to Barlow Trail. There are currently 4 tenants in the building, all with mid term leases in place. There is also a drive-thru bank machine on the site. The development was constructed over 2000/2001. The tenant listing is provided below.

The property is currently governed by a direct control Bylaw 114298 which received council approval in October 5, 1998, almost 20 years ago. This DC Bylaw is based upon the Permitted and Discretionary Uses of the C-2(16) Local Commercial District, which in itself is based upon the General Rules contained in Section 33 of Bylaw 2P80. As this 2P80 bylaw is somewhat dated, there are inherent limitations to what uses are permitted on the site and hence, there limiting factors as to how Certus is able to lease the centre.

As an example, Bylaw 25P20 18, related to the introduction of various Cannabis businesses, was approved April 5, 2018. This bylaw serves to amend Land Use Bylaw 1P2007 but does not reflect any revisions to Bylaw 2P80. As such, our property is unable to seek a Cannabis Store as an approved Permitted Use.

Applicant Submission

Therefore, we have analyzed the situation and as a result, we are seeking a Land Use re-designation from the current DC Bylaw 1222007 to a new Land Use District being Commercial – Corridor 3 f##h# (C-COR3 f##h#).

The following chart represents the current tenant mix at the property. At this time, we are wanting to proactively position ourselves to be ready for future tenant turnover at the centre and as such felt it was appropriate to update the zoning on the project. We are also giving consideration to constructing a small expansion at the centre by way of a small free-standing building, however we are not in a position to do a more strenuous re-development at the centre due to the leases in place.

Unit Number	Rentable Area SqFt	Business Name	Suggested Defined Use Description
1	1,017	Opal Souvlaki	Restaurant Food Service Only
2	1,352	Subway Sandwich	Restaurant Food Service Only
4	600	Bee's Convenience	Grocery Store
5	1,052	Liquor Palace	Liquor Store
	N/A	Bank Machine	N/A
Total GLA	4,021		

Therefore, following a detailed review of various available land use districts, we have determined that the C-COR3 (f:2.0h:16) is the best option at this time. We have reviewed adjacent land parcel zoning and felt that this was most appropriate Land Use District.

We look forward to moving this application forward.

Thank you.

Certus Developments Inc.
July 30, 2018

Land Use Amendment in Section 23 (Ward 12) at 5758, 5820, 5920 and 6020 – 94 Avenue SE, LOC2018-0163

EXECUTIVE SUMMARY

This application was submitted on 2018 July 13 by Stantec Consulting on behalf of the landowner, Powell Chuckwagon Racing Inc and proposes to change the designation of these properties from the Industrial – General (I-G) District to a DC Direct Control District based on the Industrial – General (I-G) District with the additional use of Asphalt, Aggregate and Concrete Plant. This DC redesignation allows the existing use to remain on site and gives the business the potential to grow on site. The proposal meets the policies for the site in the *Southeast Industrial Area Structure Plan* and the *Municipal Development Plan*. No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT** by bylaw the proposed redesignation of 7.35 hectares ± (18.0 acres ±) located at 5758, 5820, 5920 and 6020 - 94 Avenue SE (Plan 8055AG, Block 1, Lots 13 and 14; Plan 0112417, Block 1, Lots 17 and 18) from Industrial – General (I-G) District **to** DC Direct Control District to accommodate the additional use of Asphalt, Aggregate and Concrete Plant use, with guidelines (Attachment 2); and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

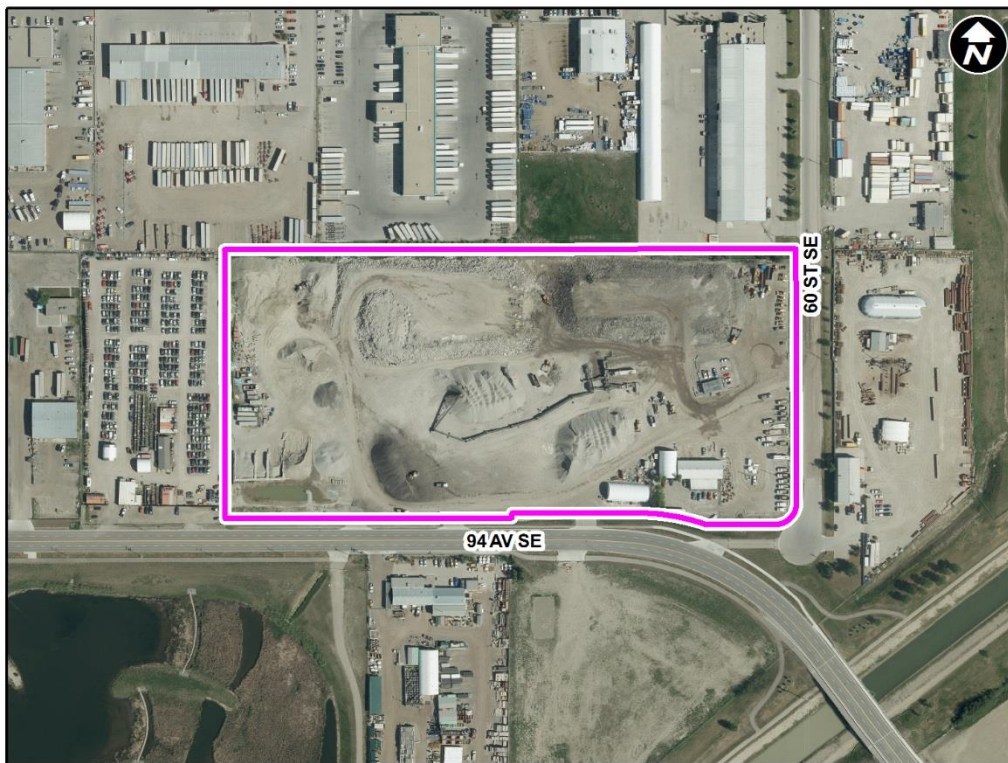
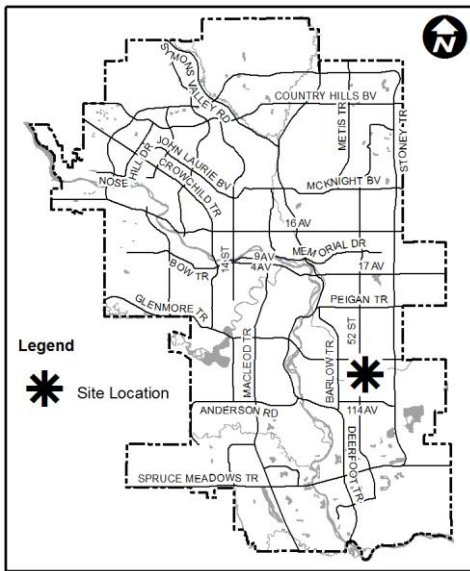
None.

BACKGROUND

The subject site is occupied by Calgary Aggregate Recycling and is in use as an Asphalt, Aggregate and Concrete Plant, a use that has been in place for more than 20 years. Due to the existence of this use on the site prior to its redesignation to the Industrial – General (I-G) District with the adoption of Land Use Bylaw 1P2007, the use exists on the site as a legal non-conforming use, allowing the operators to continue operating the use on the site but not to expand the use on site. As noted in the Applicant's Submission (Attachment 1), the operator of the site, KLS Earthworks & Environmental (owners of Calgary Aggregate Recycling) requested that Stantec Consulting Ltd. submit this application on behalf of the landowner, Powell Chuckwagon Racing Inc. in order to bring the site and its operations fully into compliance with the land use bylaw and to enable potential future expansion of the business on the site.

Land Use Amendment in Section 23 (Ward 12) at 5758, 5820, 5920 and 6020 - 94
Avenue SE, LOC2018-0163

Location Maps



Land Use Amendment in Section 23 (Ward 12) at 5758, 5820, 5920 and 6020 - 94 Avenue SE, LOC2018-0163

Site Context

The subject site is located in the southeast industrial community known as Section 23. It is bordered by 94 Avenue SE to the south, 60 Street SE to the east, auto wrecking/salvage operations to the west and various industrial businesses adjacent to the site to the north. The application area consists of four titled parcels that operate as one site with a primary access point onto the graveled 60 Street SE, and additional available access points along 94 Avenue SE to the south.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This application was submitted to enable the continued use of the site as an Asphalt, Aggregate and Concrete Plant and to enable possible future expansion of the operation. The site is currently designated as Industrial – General (I-G) and contains the Calgary Aggregate Recycling (CAR) business. Since the Asphalt, Aggregate and Concrete Plant use existed on site prior to having been transitioned to the I-G designation, it has been allowed to continue operation as a legal, non-conforming use, but would not be eligible to modify or expand the use.

Administration considered redesignation of the site to the Industrial – Heavy (I-H) District, which lists the Asphalt, Aggregate and Concrete Plant use as discretionary. However, given the policies in the *Southeast Industrial Area Structure Plan (ASP)* that state that the site should be considered for redesignation to a general industrial district upon provision of services, and given that the area is now fully serviced, it was recommended that the site be redesignated to a DC Direct Control District that would enable the continuation of the Asphalt, Aggregate and Concrete Plant use, but that would adopt all other rules of the I-G District (see Attachment 2). This proposal meets the needs of the business operating on the site, as well as ensuring that if this use was discontinued on the site, future development would transition to general industrial uses rather than other heavy industrial uses.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

This application proposes redesignation of the site from the standard Industrial – General (I-G) District to a DC Direct Control District that adopts all uses and rules of the I-G District with the additional discretionary use of Asphalt, Aggregate and Concrete Plant.

Development and Site Design

This proposed land use redesignation would enable future expansion of the Asphalt, Aggregate and Concrete Plant use that currently exists on site, or any of the other listed uses in the I-G District. Any changes to the development on site would be subject to review with the appropriate permit process, and would be expected to comply with the rules and purpose of the I-G District.

Land Use Amendment in Section 23 (Ward 12) at 5758, 5820, 5920 and 6020 - 94 Avenue SE, LOC2018-0163

The applicant has indicated that they have no immediate plans for expansion or development on the site.

Environmental

Due to the site's proximity to the Western Headworks Canal, the applicant has been advised that particular care and attention must be paid to ensuring that overland stormwater runoff to the Canal is controlled in order to protect the canal from any potential contamination. If any changes are proposed to the site in the future, Administration will ensure that the development is in conformance with this requirement.

Transportation

The Calgary Transportation Plan (CTP) and the *Southeast Industrial ASP* identify 94 Avenue SE as an Industrial Arterial Road, which prioritizes the efficient movement of heavy trucks and goods, but accommodates all modes of travel.

The subject area is well served by transit service which is located in close proximity in both the eastbound and westbound directions, near 54 Street SE.

Utilities and Servicing

The original development on the site dates back to a time when servicing was not available in the area, therefore, the site is currently not serviced. Since the time of the original development, water, sanitary, and storm servicing have been installed and are available for connection in 94 Avenue SE. The mains are adequately sized to support future development of the site under the proposed land use. The site can continue to operate without servicing for the current and/or similar type of low intensity industrial uses, and then connect to services in the future when the site redevelops with a more intensive use that requires servicing. Future redevelopment of the site and connecting to the services will trigger their obligation to pay for their share of the utilities and the applicable levies and fees at that time.

Stakeholder Engagement, Research and Communication

In keeping with Administration's practices, this application was circulated to stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online. Given the industrial nature of the area, there is no Community Association to notify.

One letter was received from an adjacent landowner that cited concerns with dust and debris from stockpiles that negatively affects the operation of the adjacent business.

Land Use Amendment in Section 23 (Ward 12) at 5758, 5820, 5920 and 6020 - 94 Avenue SE, LOC2018-0163

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSRP) which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Municipal Development Plan (Statutory, 2009)

The *Municipal Development Plan* (MDP) identifies this site as a Standard Industrial Area and calls for a mix of industrial uses at varying intensities, with the industrial character of the area maintained even as the areas redevelop. Policies for the area reinforce the need to allow a variety of industrial uses in the area, and to provide a range of mobility options.

Southeast Industrial Area Structure Plan (Statutory, 1996)

The *Southeast Industrial Area Structure Plan* (ASP) identifies this site as “Existing I-4 Limited Serviced Industrial District,” based on the largely unserved nature of the area at the time when the ASP was approved. The ASP contains policies for this category upon provision of services: “*As and when the limited-serviced industrial areas are fully serviced, they can be redesignated to I-2 General Light Industrial District upon receipt of applications submitted by the landowners/developers affected.*” Since this area has since been fully serviced, the policies in the I-2 General Light Industrial District should be applied, which call for a range of light industrial and associated uses. As the former I-2 District is most comparable to the I-G District, the proposed DC district with I-G as a base is considered to be in line with the policies in place for this site.

Social, Environmental, Economic (External)

This application, and the opportunity that it would provide to the existing business on site, supports the *Municipal Development Plan's* (MDP) objectives of building a prosperous economy and supporting business and investment in our city.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed land use amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

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CPC2018-1202
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Land Use Amendment in Section 23 (Ward 12) at 5758, 5820, 5920 and 6020 - 94 Avenue SE, LOC2018-0163

Risk Assessment

This Land Use Amendment would provide certainty of use to the applicant, but does not guarantee that any future development proposals will be approved, including expansion of the Asphalt, Aggregate and Concrete Plant use. Any changes to the site will be considered independently at the Development Permit stage. However, there are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

This application enables the continued use of the site as an Asphalt, Aggregate and Concrete Plant and guides future development of the site in line with general industrial uses that are consistent with the Council-approved policies in place for the area, including the *Municipal Development Plan* and the *Southeast Industrial Area Structure Plan*.

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed DC Direct Control District

Applicant Submission



Land Use Redesignation Applicant's Submission Not Including Secondary Suites

PL 1263 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this form to ensure it will fit the space requirements of the report. Supplementary information can be provided separately in your application if required.

On behalf of KLS Earthworks & Environmental ('KLS'), Stantec Consulting Ltd. ('Stantec') is pleased to submit the attached Land Use Redesignation ('LUR') application for the lands located at 6020 94 Avenue SE, legally described as PLAN 8055AG BLOCK 1; LOTS 13, 14, and PLAN 0112417 BLOCK 1; LOTS 17, 18, ('subject lands'). The subject lands are bordered by 94 Avenue SE to the south, 60 Street SE to the east, and auto wrecking operations to the west.

PROPOSED DEVELOPMENT

KLS has recently purchased the subject lands with the intent of consolidating business operations to Calgary from Rocky View County. Previously owned by Calgary Aggregate Recycling ('CAR'), the subject lands have been operating under the non-conforming use of Asphalt, Aggregate and Concrete Plant within the Industrial – General (I-G) district of the Land Use Bylaw (1P2007). KLS is looking forward to working with the City to bring the subject lands into conformance through a Land Use Redesignation ('LUR') from I-G to Direct Control (DC) in order to improve the property, as well as enhance their business within the city of Calgary.

SOUTHEAST INDUSTRIAL ASP

The subject lands are located within the Southeast Industrial Area Structure Plan ('ASP'), and classified as "Existing I-4 Limited Serviced Industrial District". The I-4 district was established in the ASP due to the absence of key municipal services such as water and sanitary storm sewers in these areas, as well as poor access roads and lack of satisfactory storm drainage. Land uses in this district are intended to include limited building area, generate of low traffic volumes, and minor water/sanitary sewer needs. The subject lands currently conform with this description of I-4; having limited building area within the large parcel, while generating low traffic volumes and requiring minimal water and sanitary sewer services.

Since adoption of the ASP in 1996, access to municipal infrastructure has significantly improved in the area through surrounding development over time. Connections in this area to key municipal services such as water, sanitary sewer, and storm are now readily available, in addition to improved access roads along 94 Avenue and 60 Street SE.

Based on current City policy, redesignating the subject lands to Industrial – Heavy (I-H) as a means to bring the Asphalt, Aggregate and Concrete Plant use into conformance would require an amendment to the ASP. This amendment would redesignate the lands as "Existing I-3 Heavy Industrial District" within the ASP to accommodate the existing use. However, since the use of Asphalt, Aggregate and Concrete Plant has been functioning successfully at this location prior to and since adoption of the ASP, a redesignation to DC with a base district of I-G has been chosen as a more appropriate option.

The DC will add the discretionary use of Asphalt, Aggregate and Concrete Plant to the I-G base district, allowing the subject lands to remain in conformance with the intent of the Southeast Industrial ASP while bringing the subject lands into conformance with the Land Use Bylaw. This option was looked upon favourably by City staff during the June 14, 2018 pre-application meeting (refer to pre-application comments PE2018-00744), and would afford KLS the opportunity to consider site improvements in the future.

PRE-APPLICATION COMMENTS

We appreciate the feedback obtained from the pre-application meeting held June 14, 2018, and are proceeding with the DC application as recommended. At this time, KLS intends to leave the current titles as is and will utilize buildings of a temporary nature on the Subject Lands. Should KLS choose to consolidate operations in the future, we appreciate being made aware of the development permit, offsite-levy, and consolidation requirements both during the pre-application meeting and through the Pre-Application Assessment Form – CPAG review dated June 28, 2018.

ISC: Protected

Proposed Direct Control Guidelines

WHEREAS it is desirable to amend the Land Use Bylaw Number 1P2007 to change the land use designation of certain lands within the City of Calgary;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26 as amended;

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

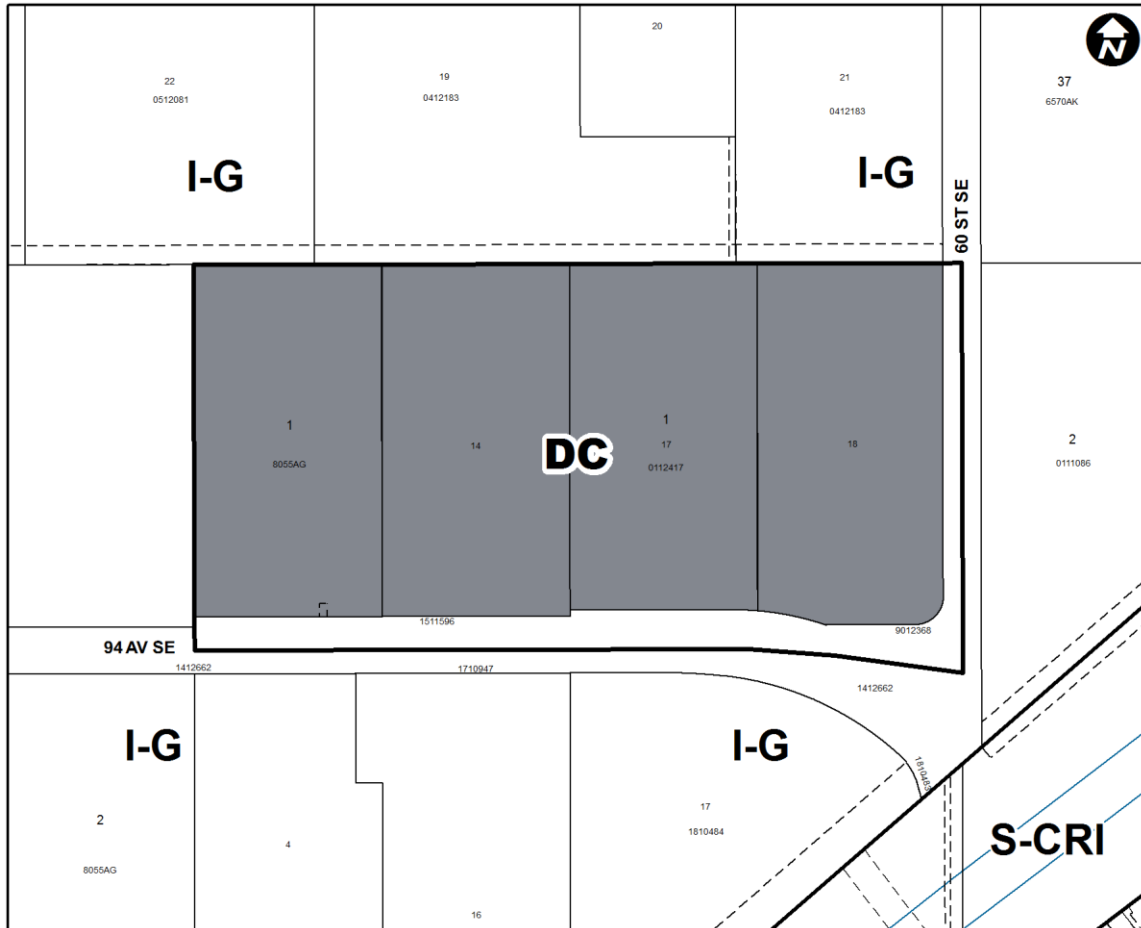
1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".

SCHEDULE A



Proposed Direct Control Guidelines

SCHEDULE B



DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control District is intended to:

- (a) provide for the additional **discretionary use** of **Asphalt, Aggregate and Concrete Plant**.

Compliance with Bylaw 1P2007

2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

4 The **permitted uses** of the Industrial – General (I-G) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Proposed Direct Control Guidelines

Discretionary Uses

5 The ***discretionary uses*** of the Industrial – General (I-G) District of Bylaw 1P2007 are the ***discretionary uses*** in this Direct Control District with the addition of:

- (a) **Asphalt, Aggregate and Concrete Plant.**

Bylaw 1P2007 District Rules

6 Unless otherwise specified, the rules of the Industrial – General (I-G) District of Bylaw 1P2007 apply in this Direct Control District.

TEXT FOR DISCUSSION
ONLY

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ISC: UNRESTRICTED
CPC2018-1179
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Land Use Amendment in Inglewood (Ward 9) at 740 - 19 Street SE, LOC2018-0066

EXECUTIVE SUMMARY

This land use amendment application, known as “The Grid”, was submitted by B&A Planning Group on 2018 March 22, on behalf of 2017052 Alberta Ltd and with authorization from the owner CANHUSK Real Estate ULC. The application proposes to change the land use designation of a vacant parcel located in the community of Inglewood from Commercial – Corridor 3 f2.8h12 (C-COR3 f2.8h12) District to a DC Direct Control District to:

- allow for a maximum building height of 50.0 metres – approximately 16 storey (an increase from the current maximum of 12.0 metres – approximately 4 storey);
- allow for a maximum floor area ratio (FAR) of 5.0 (an increase from the current maximum FAR of 2.8);
- use density bonusing to achieve the maximum FAR and height; and
- allow for the uses listed in the Direct Control District

The proposed DC Direct Control District is based on the Multi-Residential – High Density Medium Rise (M-H2) District. The proposal to increase density in a strategically located vacant site near major transit infrastructure aligns with municipal policies and guidelines including The *Municipal Development Plan* (MDP) and *Transit Oriented Development* (TOD) Guidelines.

No development permit has been submitted at this time.

The subject parcel is located in the 30-35 Noise Exposure Forecast (NEF) of the *Calgary International Airport Vicinity Protection Area* (AVPA) Regulation. The AVPA Regulation restricts certain sensitive uses including residential within the 30-35 NEF. An amendment to the AVPA Regulation is required at the development permit stage to allow for a development that includes any of the prohibited uses listed in the AVPA Regulation. The City is undertaking discussions with The Province and the Calgary International Airport Authority to consider if any exemptions to the AVPA Regulation may be considered to accommodate strategic growth within key established neighbourhoods. The recommendations in this report recognise these discussions.

Land Use Amendment in Inglewood (Ward 9) at 740 - 19 Street SE, LOC2018-0066

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission:

A. recommends that Council hold a Public Hearing and:

1. **ADOPT**, by bylaw, the proposed redesignation of 0.28 hectares ± (0.68 acres ±) located at 740 - 19 Street SE (Plan 17JK, Block X) from Commercial Corridor 3 f2.8h12 (C-COR3 f2.8h12) District to DC Direct Control District to accommodate a mixed-use development, with guidelines (Attachment 2);
2. Give first reading to the proposed Bylaw;
3. **WITHHOLD** second and third reading until the outcome of discussions on potential amendments and/or exemptions to the *Calgary International Airport Vicinity Protection Area* Regulation have been determined by The City of Calgary, the Province of Alberta and Calgary's Airport Authority; and,

B. **AUTHORIZE** Administration, once the above discussions in recommendation 3 have been completed, to make an application to the Minister of Municipal Affairs for an amendment to the *Calgary International Airport Vicinity Protection Area* (AVPA) Regulation, if applicable. The application would be made after receiving a development permit and conducting all necessary public consultation in accordance with the requirements of the AVPA Regulation, to allow for residential development.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

As per the application submitted on 2018 March 22 by B&A Planning Group on behalf of 2017052 Alberta Ltd and with authorization from the owner CANHUSK Real Estate ULC, the proposed land use amendment application, known as The Grid, seeks to redesignate the vacant parcel from Commercial – Corridor 3 f2.8h12 (C-COR3 f2.8h12) District to a DC Direct Control District. The intent is to allow for a mixed-use development with a maximum height of 50.0 metres, when bonusing is considered. The subject parcel is located approximately 300 metres of a future Bus Rapid Transit (BRT) station and is the east gateway of the Inglewood community. Refer to Attachment 1 for the applicant's submission.

Application History

On 2017 April 10, B&A Planning Group, enquired about the opportunity to redesignate the subject parcel to allow for a mixed-use development with ground level retail and residential units above. Administration assessed the information provided and highlighted concerns including access to the site, and the AVPA Regulation. The proposal was also evaluated against the draft *Inglewood Area Redevelopment Plan* dated March 2017. However, at that point and due to a mapping error the subject parcel was shown as a park.

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**ISC: UNRESTRICTED
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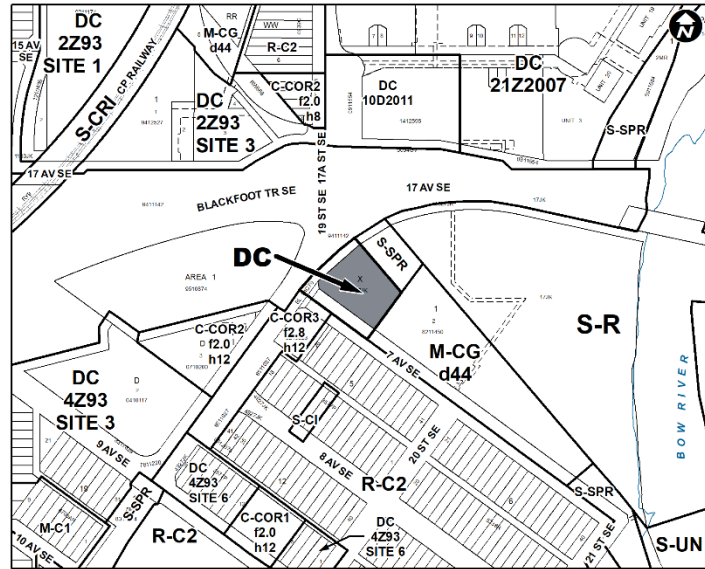
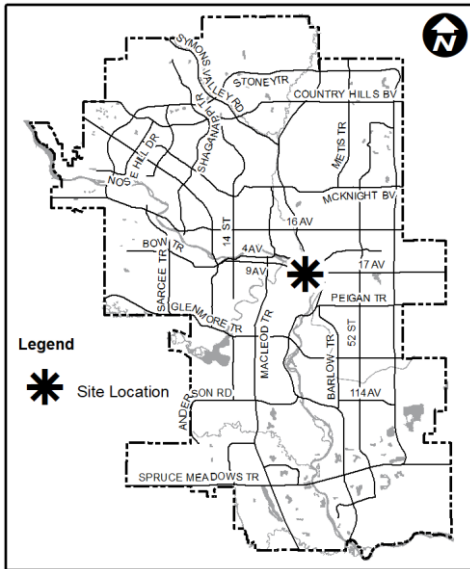
Land Use Amendment in Inglewood (Ward 9) at 740 - 19 Street SE, LOC2018-0066

On 2018 March 8, after a number of applicant-led public engagement, B&A Planning Group, enquired about a 53.0 metres (17 storey) tall mixed-use building to be considered under the Multi-Residential – High Density High Rise (M-H3) District. Administration suggested a lower multi-residential district including the Multi-Residential – High Density Medium Rise (M-H2) District be considered. The appropriateness of a DC Direct Control District was also discussed.

On 2018 March 22, Administration received the land use amendment application. The proposal was to redesignate the subject site to Multi-Residential – High Density Medium Rise (M-H2) District with a maximum height of 50.0 metres. After discussion with the applicant, B&A Planning Group, and prior to circulation, the application was amended to redesignate the parcel to a DC Direct Control District, based in M-H2 District. The application was circulated to internal and external stakeholders for review and comments on 2018 April 26.

Land Use Amendment in Inglewood (Ward 9) at 740 - 19 Street SE, LOC2018-0066

Location Maps



Land Use Amendment in Inglewood (Ward 9) at 740 - 19 Street SE, LOC2018-0066

Site Context

The 0.28 hectares ± (0.68 acres ±) flat vacant parcel is located in the community of Inglewood at the intersection of 19 Street SE and 7 Avenue SE. This intersection is the eastern entranceway to the community. The surrounding area is characterized by a wide variety of land uses, and developments including parks, commercial/retail, low density residential and community service developments. These uses and development are complemented by existing and under-construction transit infrastructure including a Bus Rapid Transit (BRT) Station. The area will function as a transit node once construction of the BRT Station is completed at the end of 2018.

Parcels adjacent to the subject site are predominantly park space and road-right-of-ways. To the north there is a parcel designated as Special Purpose – School, Park and Community Reserve (S-SPR) District, and is occupied by a park. An undesignated road-right-of-way for Blackfoot Trail SE is also north of the site.

To the south, there is a parcel designated as Commercial – Corridor 3 f2.8h12 (C-COR3 f2.8h12) District occupied by a gas station, and parcels designated as Residential - Contextual One / Two Dwellings (R-C2) District predominately occupied by single detached dwellings.

To the west, the parcel is adjacent to an undesignated road-right-of-way for Blackfoot Trail SE. And to the east, there is a parcel designated as Multi-Residential Contextual Grade Oriented (M-CGd44) District occupied by a park.

Furthermore, the park area to the east, comprised of three parcels, includes baseball diamonds, open green spaces and parking areas. The park area is included in the proposed Bend in the Bow project. The Bend in the Bow is an unfunded City project that seeks to preserve, enhance and celebrate the natural, environmental and cultural heritage of the Pearce Estate Park, the green spaces along the Bow River, Inglewood Bird Sanctuary and the Inglewood Wildlands.

The site is near the Primary Transit Network with the Blackfoot / 19 Street SE Bus Rapid Transit (BRT) Station being built at approximately 300 metres west of the site. A bus stop exists in front of the parcel, along 19 Street SE.

The subject parcel is approximately 200 metres north of 9 Avenue SE which is a Main Street Corridor. And within a 400 metres radius from the site, there are a variety of community amenities including the Colonel Walker Elementary School, the Pierce Estate Park & Sam Livingston Fish Hatchery, the Bow River Pathway and the Bow River. The Inglewood Bird Sanctuary, and Inglewood Wildlands are approximately one kilometre east.

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As identified in Figure 1, the population in the community of Inglewood achieved its peak population in 2017.

Figure 1: Population Trends in the Community of Inglewood

Inglewood	
Peak Population Year	2017
Peak Population	4,008.0
2017 current population	4,008.0
Difference in Population (number)	0.0
Difference in Population (percentage)	0%

Source: The City of Calgary 2017 Civic Census

Figure 2 recognises that prior to the 1990s the population in the community of Inglewood declined. The population in the community experienced a significant increase in the first decade of the 2000s, yet population did not exceed the population achieved in 1970. After 2010 the population has increased at a slower pace surpassing the 1970 population. The community achieved its peak population in 2015 and 2017. Between peak populations the population increase was 1.8 percent. In addition, population projection indicates that by 2039, the community of Inglewood will reach a population of 5,140 residents. Allowing intensification in strategically located parcels, can help achieve the community's projected population.

Figure 2: Population growth in the Community of Inglewood

Year	Population	Population increase / decrease (in numbers)	Population increase / decrease (in percentages)
1970	3,557		
1980	2,677	- 880	-25 %
1990	2,556	- 121	-4.5 %
2000	2,868	+ 312	+12.0 %
2010	3,427	+ 559	+ 19.5 %
2015	3,935	+ 508	+ 12.9 %
2017	4,008	+ 73	+1.8 %
2039 Projected population	5,140	+ 1,132	+22 %

Source: The City of Calgary - Geodemographics

Additional demographic and socio-economic information may be obtained online through the [Inglewood community](#) profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The application received on 2018 March 22 to redesignate the subject parcel to Multi-Residential – High Density Medium Rise (M-H2) District was amended shortly after, to redesignate the parcel from Commercial – Corridor 3 f2.8h12 (C-COR3 f2.8h12) to a DC Direct Control District. The DC District enables a maximum building envelope that takes the context into consideration.

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Planning Considerations

As noted in the Strategic Alignment section of this report, the proposed land use amendment application aligns with municipal policies and guidelines. The proposal is also supported by key organizing planning principles where significant redevelopment happens on underutilized or vacant parcels located in strategic areas including transit nodes and corridors. Additionally, the approach supports preservation of areas that have strong historical character.

The proposed DC Direct Control District supports a building envelope that transitions from the proposed maximum height to a lower height adjacent to the low density area. The DC District seeks to activate the interface between the building and the park space and encourages a quality design to enhance the east entranceway to the community of Inglewood.

Land Use

The subject parcel is currently designated as Commercial – Corridor 3 f2.8h12 (C-COR3 f2.8h12) District. This land use district is intended to be located along major roads and industrial areas to accommodate mid-scale retail, and medium to large eating and drinking uses. The district does not allow for residential uses. The current land use district supported the gas station that existed on the parcel from 1962 until 2011.

The DC Direct Control District is supported by section 20 of Bylaw 1P2007 as the DC District:

- Allows for a building massing that addresses the unique context where the parcel is located;
- Contain direction to address the site's constraints and opportunities; and,
- Include density bonusing provision.

The proposed DC District included in Attachment 2, was extensively discussed with the applicant and the Inglewood Community Association as well as with City staff and it incorporates many aspects of these discussions. Below are some highlights of this proposed DC District:

- The maximum height is to be achieved in the northwest corner of the parcel, the furthest point away from the low residential areas along 7 Avenue SE. Restricts the maximum height along 7 Avenue SE and the east property line. Additional height can be realised at different distances measured from the property line.
- Excludes uses listed in the base district that generate high traffic volume to address access constraints.
- Provides for a variety of discretionary uses that are allowed under the AVPA Regulation.
- Provides for density bonusing of up to 1 FAR and six additional metres of building height. Bonusing items include monetary contribution, affordable housing units and universally accessible dwelling units.
- Inclusion of larger trees, at the time of planting.
- Includes a list of exterior materials that are to be used to celebrate the character and history of the community of Inglewood.
- Includes rules related to the location of at-grade parking areas.

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Development and Site Design

At the time of development permit, the Development Authority is to consider the following recommendations:

- The building should be carefully designed to integrate with the surrounding context and avoid monotonous edge.
- The crown of the building should include texture, geometrical forms, or architectural features that reflect the character of the community. In addition, lighting could be employed to accentuate the building's crown.
- All façades of the building must be equally treated with aesthetically pleasant architectural elements and finishing materials.
- Primary colours to be considered as accent colours only.
- Blank walls should be avoided.
- Request a study that describes the architectural styles, and historical elements unique to the community of Inglewood. The development permit application must demonstrate how the findings of the study are included in the building design.
- Encouraged the following ornamental trees in landscaped areas: Ivory Silk Tree Lilac / Japanese Tree Lilac (*Syringa Reticulata* "Ivory Silk"); Ussurian Pear, Harbin Pear, Manchurian Pear (*Pyrus ussuriensis*); Poplar (Balsam Poplar); Pincherry (*Prunus pennsylvanica*); Cherry Prinsepia (*Prinsepia sinensis*); Crabapple (*Malus*); and Swedish Columnar Aspen (*Populus Tremula* "Erecta")

Environmental

Due to the gas station that existed on the site, at development permit stage the applicant shall submit a current Remedial Action Plan (RAP) and/or Risk Management Plan (RMP) addressing previously identified soil and groundwater concerns in previous environmental reports prepared for the parcel.

Transportation

As per the Calgary Transportation Plan (CTP), the regional street network consists of Blackfoot Trail SE / 17 Avenue SE (arterial street), future 17 Avenue SE Transitway bordering the northern edge of the parcel, 19 Street SE (arterial street) bordering the western edge of the parcel, and 7 Avenue SE (collector street) to the south of the site. Currently, 7 Avenue SE is a cul-de-sac and does not connect to 19 Street SE.

A Transportation Impact Assessments (TIA) was provided in support of land use amendment application. The TIA was completed for a potential mixed-use development with approximately 6,000 square feet of retail space and approximately 160 residential units. At development permit stage, the network improvements will include the connection of 7 Avenue SE to 19 Street SE through a right-in-right-out to/from northbound 19 Street that accommodates the vehicular traffic to/from the parcel and the community via 7 Avenue SE. A center median extension along 19 Street to facilitate the right-in-right-out and traffic calming measures along 7 Avenue SE and 20 Street SE will be determined at the development permit stage.

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The future 17 Avenue Transitway corridor - Blackfoot Trail / 9 Avenue SE Bus Rapid Transit (BRT) Station will be located approximately 300 metres west of the parcel. The parcel is currently served by Bus Route 1 with a bus stop located directly adjacent to the western edge of the parcel along 19 Street SE. The 17 Avenue Transitway service is anticipated to commence at the end of 2018.

As part of the Transitway project, a multi-use pathway connects 19 Street SE to the Bus Rapid Transit station. The multi-use pathway running parallel to the Transitway corridor will provide for great active modes connection from the parcel to Calgary's Downtown and/or over the Deerfoot Trail to 17 Avenue SE. A pedestrian connection (sidewalk) along 7 Avenue SE is required to connect the parcel to 19 Street SE.

A parking study may be required at the development permit stage.

Utilities and Servicing

The proposed development is within the 1:100 year "Flood Fringe" flood risk area. As such, the proposed development is subject to Land Use Bylaw 1P2007, Part 3, Division 3, Clause 60 which specifies that the main floor elevation, mechanical equipment, and primary electrical boxes must all be located above the 1:100 year flood level.

Sanitary sewers are available to service the development. As part of the development permit application process, a Sanitary Servicing Study must be submitted to determine whether upgrades are required to the existing public infrastructure.

Water mains are available. Upgrades to the public water system are anticipated and will be evaluated in detail at the development permit stage after the review of a Fire Flow calculation letter.

Storm sewers are not available. Stormwater servicing will require a storm main extension along 7 Avenue SE at the development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, the application was notice posted on-site and circulated to internal and external stakeholders. Notification letters were sent to adjacent landowners. Information was also included in the Planning and Development Map which is an online tool accessible by the public that includes relevant information on planning applications.

In addition to the standard engagement practices for land use amendment applications, the application was presented at a City led information session and was extensively discussed with the Inglewood Community Association. The application was subject to The City's communication strategy that was developed to inform the broader community about active projects in the area and associated engagement opportunities at engage.calgary.ca/TheGrid.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent owners. In addition, Commission's recommendation, the date of Public Hearing will be advertised.

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Engagement and Public Meetings

Applicant Led engagement

B&A Planning Group, the applicant, led their own engagement program which included various meetings with the Inglewood Community Association and an open house prior to submitting the land use amendment application.

The applicant started discussions with the Inglewood Community Association on 2017 July. At that time, a 22 storey building was presented. After additional meetings with the community association, the applicant sent 700 postcard invitations to an applicant-led open house which took place on 2018 February. As per the information received by the applicant, a 20 storey building was presented and 120 community residents attended this session. Attendees provided feedback related to height, massing, shadowing, vehicular access, and traffic. Administration did not attend any of the applicant led events as a formal application had not been received at that time. The Applicant has provided a summary of their engagement which can be found in Attachment 5.

Community led engagement

On 2018, March 01, a community led town hall took place. Administration attended and provided clarification on planning process, the Land Use Bylaw and context. Feedback on the specific building height was not provided as a formal application had not been received yet. The applicant presented an amended proposal for a 17 storey building. Concerns expressed during the event related to height, massing, land use bylaw, proximity to bird sanctuary and potential bird collisions, shadowing, access, traffic and parking.

City led engagement

Administration assessed the application and it was deemed that City-led engagement was required to collect feedback from the community to inform the DC District. This included opportunities for both in-person and online engagement. Outcomes of the engagement were shared with the Applicant and recommendations were made, as part of our review.

On 2018 May 07, The City held an information session where projects happening in the communities of Inglewood and Ramsay were presented. This land use amendment application was included in the event. Administration provided rationale for using a DC Direct Control District and highlighted the site's constraints and opportunities. A survey to inform the DC was available to the residents at the event and online between 2018 May 07 and 2018 May 20.

A total of 62 responses (16 in person and 46 online) were received. Concerns expressed at the event and through the survey were related to building height (especially along 7 Avenue SW), massing, community character being lost, precedent setting, traffic, and parking. The What We Heard Report with verbatim comments can be found [here](#).

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The survey feedback highlighted the importance for transitioning the building massing to the low density residential areas to the south, landscaping and tree size, creating a great interface with the park, architectural features and materials. This feedback informed provisions included in the DC District.

Citizens and Community Association Comments

In addition to the feedback received via the survey, 33 letters were received through the standard circulation and notice posting processes. Fifty six percent (18 responses) were opposed to the proposed land use amendment application. Twenty three percent (7 responses) were neutral but outlined concerns; 17 percent (5 responses) were in support of the proposed development and one percent provided comments not related to the application. Concerns related to height, shadowing, privacy, parking, access from 7 Avenue SE, proximity to the Inglewood Bird Sanctuary, site contamination, and history and character of the area. Administration responded to the letters and met with concerned residents, where possible.

The Inglewood Community Association provided a letter of opposition dated 2018 May 16. A letter with additional information dated 2018 October 02, was received prior to Calgary Planning Commission. The new letter reflects the extensive discussion between Administration and the community association related to the application and the DC District provisions. Although the concern remains with the maximum height of 50.0 metres, the community association recognizes the time taken to discuss the file and that the DC District addresses other community concerns including bonusing items, and public improvement projects. Both letters sent by the Inglewood Community Association are included in Attachment 3.

What we heard / what we did

The table below provides a summary of the main concerns expressed by the community. The table also includes a response on how this feedback was addressed by The City and/or the Applicant to inform the final proposal.

What we heard (Issue or opportunity identified)	What changes were made and/or response to the issue identified
<p>Concern about building height regarding the height of the building being too tall to fit into the scale of the existing neighbourhood. Concern that the height proposed will overshadow neighbouring homes.</p>	<p>The DC District includes provisions that enable a building massing that steps up from 14.0 metres to 50.0 metres. The DC also identifies the specific area within the parcel where the maximum building height is allowed. A shadow study was prepared and shows that shadow casts are generally over Blackfoot Trail SE.</p>
<p>Desire for the design of the building to align with the historic character of the neighbourhood (e.g. materials such as brick, wood or sandstone; Inglewood's character and history associated with railway).</p>	<p>The DC District include provisions to achieve a development that is consistent with the context. The DC District also includes provisions that identify adequate finishing materials for the future development.</p>

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Concern that the proposed development does not “fit” into the community and would not be appropriate to build a large, modern-looking tower in this area. No consideration given for the natural environment nearby such as the Bow River and the Inglewood Bird Sanctuary.	Same as above. The DC District includes a reference that The City of Calgary Bird-Friendly Guidelines need to be followed for the future development.
Concerns regarding traffic, access and parking. Issues with traffic for those who live in, visit, or commute through the area – especially given an expected increase in traffic if the building is built at the maximum height. The nearby intersection (19 Street SE and Blackfoot Trail SE) is already challenging; re-routing traffic would cause further issues.	Access to the parcel is from 7 Avenue SE. An updated / new Transportation Impact Assessment and parking study may be required at development permit stage. Road improvements would be required at development permit stage to gain a safe access to the site.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The recommendation in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)*. The plan directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Municipal Development Plan (Statutory, 2009)

The area where the site is located is identified as Residential Developed Inner City (Map 1: Urban Structure). The Inner City Area comprises residential communities that were primarily subdivided and developed prior to the 1950s. Many of these areas have undergone redevelopment in recent years. Intensification and change will continue to occur within the Inner City Area.

Sites within the Inner City Area may intensify, particularly in transition zones adjacent to areas designated for higher density (i.e., Neighbourhood Main Street), or if the intensification is consistent and compatible with the existing character of the neighbourhood. A range of intensification strategies should be employed to modestly intensify the Inner City Area, from parcel-by-parcel intensification to larger more comprehensive approaches at the block level.

The proposal generally aligns with MDP’s city-wide policy that encourages intensification including housing diversity and choices policies, shaping a more compact urban form, creating great community, and urban design. Section 2.1 links land use decision to transit and encourages the provision of more housing choices in locations close to job market and in areas well served by the Primary Transit Network. Section 2.2 directs future growth in a way that foster a more compact efficient use of land, creates complete communities, allows for greater mobility choices and enhances vitality and character of local neighbourhoods. Section 2.4 refer to the 13 urban design elements that apply to developments across the city to create quality

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design, especially for tall buildings such as the one that could be realized under this land use amendment proposal.

The *Calgary Transportation Plan* (2009) identifies Blackfoot Trail as part of the Primary Transit Network (Map 2: Primary Transit Network), part of the Primary Bike Route (Map 1: Primary Cycling Network) and a Supporting Goods Movement Corridor (Map 5: Primary Goods Movement Network).

Inglewood Area Redevelopment Plan (Statutory, 1993)

The subject parcel is within the Residential Land Uses (Map 6: Generalized Land Use – Future). The prime objective for these areas is to encourage construction of more housing to increase the population in the community of Inglewood. Residential policies in Section 2.3 notes that new residential development should respect the surrounding housing and contribute to an attractive streetscape and The City should work with the Community to facilitate processing of application for residential development. The section also notes that appropriate ways and where feasible, facilitate the creation of residential development opportunities on privately owned land.

The plan also shows the ideal South Downtown Bypass – Preferred Route (Map 18: South Downtown Bypass – Preferred Route) which is a transit route that would connect the Center City with east Calgary. The preferred alignment generally matches the alignment of the 17 Avenue SE Bus Rapid Transit (BRT) project which is currently under construction.

Draft - The East Historical Calgary Area Redevelopment Plan (dated 2018 May)

The May 2018 draft Historic East Calgary Area Redevelopment Plan has no official status until it is adopted by Council. Administration reviewed the draft to understand the context and recognize what is being planned for the area where the parcels is located.

The subject parcel is located in a Transit Oriented Development (TOD) area as is within the 200 metres radius from the Blackfoot Trail / 9 Avenue SE BRT Station. The parcel is considered as Community High Density building block with a maximum FAR of 4. The draft plan also includes provision for heritage density to allow for additional FAR and building height across the plan area. It is expected that the proposed land use amendment will comply with the new policy.

Airport Vicinity Protection Area (2009)

The subject parcel is located in the 30-35 Noise Exposure Forecast (NEF) and as such the following uses are prohibited: residences, schools, medical care facilities and campgrounds. In accordance with Section 3 of the AVPA Regulation, when a development permit for any of the prohibited uses is proposed, an amendment to the AVPA Regulation is required.

An application for land use redesignation is not subject to the AVPA Regulation, however, this application was circulated to the Calgary International Airport Authority for information and comments, and the response letter is included as Attachment 4. The Calgary Airport Authority objects to this proposal as the intended land use (residential) does not comply with the *Calgary International Airport Vicinity Protection Area* (AVPA) Regulation as residences are considered

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prohibited uses within the 30-35 NEF contours. The letter also notes that the proposal does not conflict with any other areas of review.

Administration is recommending first reading of the land use amendment application and is requesting that Council authorize Administration to apply for an amendment to the *Calgary International Airport Vicinity Protection Area (AVPA) Regulation* if required at development permit stage subject to the results of ongoing discussions between The City and Calgary Airport Authority.

As per section 7(1) of the AVPA Regulation, an application to amend the AVPA Regulation must include a resolution of the Council and that the Council supports the proposed amendment. Administration, with authorization from Council, may make the application to the Minister of Municipal Affairs. Such amendment requests may be considered when The Minister is satisfied that reasonable consultation on the proposed amendment has taken place with any affected municipality and landowners, the Airport Operator and the general public. Administration cannot render a decision on a development permit that includes any of the prohibited uses listed in the AVPA Regulation until an exemption to the regulation has been granted.

The City is working with the Calgary Airport Authority and The Province to discuss possible amendments or exemptions to the AVPA Regulation. The outcome of which will determinate details on any application that requires an amendment to the regulation.

Furthermore, the application was also circulated to NAV Canada and a letter of no objection was received.

Transit Oriented Development Guidelines (Non-Statutory, 2005)

This Guideline provides direction for Transit Oriented Development (TOD) typically located within 600 metres of a transit station for Light Rail Transit (LRT) station or Bus Rapid Transit (BRT) stations where an LRT station will eventually develop. TOD encourages higher density development near the station to make transit convenient for more people and encourage ridership.

This form of development utilizes existing infrastructure, optimizes use of the transit network and creates mobility options for transit riders and the local community. Successful TOD provides a mix of land uses and densities that create a convenient, interesting and vibrant community. The proposed land use redesignation will allow for a mixed-use transit oriented development.

Social, Environmental, Economic (External)

The proposed land use amendment allows for the efficient use of land by supporting intensification on a site located within a Transit Oriented Development area, and linking density to transit infrastructure. Intensification at strategic locations promotes the preservation of the community character and history in other areas of the community. Promoting development that enhances the interface with community green space, allows for social interaction.

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Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed land use amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

Even if the proposed DC District provides certainty for a specific maximum building envelope, additional work will be required at development permit stage to assure the building reflects the character and history of the Inglewood community.

The major risk for the developer is to not be able to realize a mixed-use development that includes residential uses if an amendment to the AVPA regulation is not granted. Other development options could be explored to allow for alternative developments (e.g. a mixed-use development that includes office, hotel and retails uses listed in the DC District).

If the negotiation with the Airport Authority to implement amendments or exemptions to the AVPA Regulation takes longer than two years (from the day of first reading of this Bylaw), this land use amendment application will lapse.

REASON(S) FOR RECOMMENDATION(S):

The proposal to increase density in a strategically located vacant site near major transit infrastructure aligns with municipal policies and guidelines including *The Municipal Development Plan*, the *Inglewood Area Redevelopment Plan* and *Transit Oriented Development Guidelines*.

The DC District enables a building mass that transitions to the surrounding low density area and enhances the visual experience at the east entranceway to the community. The DC District also encourages an active interface with the adjacent park.

The recommendation to move forward with first reading only supports the discussions The City, Calgary Airport Authority and The Province are undertaking with respect to possible amendments or exemptions to the AVPA Regulation.

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed DC Direct Control District
3. Inglewood Community Association Letters
4. Calgary International Airport Letter
5. Applicant led engagement summary

Applicant's Submission

B&A Planning Group has been authorized by CANHUSK Real Estate ULC and retained by 2017052 Alberta Ltd. to pursue a land use amendment for 740 – 19 Street SE. The vacant site is located in eastern part of the community of Inglewood near the intersection of 17 Avenue SE / Blackfoot Trail and 19 Street SE. The property's current Land Use designation is an auto-oriented Commercial – Corridor 3 (C-COR3f2.8h12) District, which previously accommodated a Husky gas station (demolished in 2011). The site has been vacant and is undergoing environmental remediation.

Our proposal is for a Direct Control District based on the Multi-Residential – High Density Medium Rise (M-H2) District that acknowledges the site's unique location and interface. The proposed designation is intended to accommodate a development concept consisting of a residential high rise (16 storey) on the northern portion of the site facing 17 Avenue SE / Blackfoot Trail and residential development that steps down towards the existing single detached residential development. The ground floor of the building will activate the 19th Street SE frontage with small scale commercial uses and interface with 7th Avenue SE and the future Bend in the Bow Park with townhouse development.

Our proposed land use amendment is intended to accommodate transit oriented development as the site is located ~210m away from the future Blackfoot Trail / 9 Avenue BRT station; anticipated to be complete in late 2018. It will have convenient access to this station by way of a multi-use pathway across 19 Street, which will also connect to existing pathways along the Bow River and beyond. Due to the site's proximity to a future BRT Station; our application delivers on the Municipal Development Plan's direction to sensitively intensify sites within 400 metres of the Primary Transit Network. Other MDP direction that support our proposal include "Shaping a more compact urban form," "Complete communities," "Neighbourhood infill and redevelopment," "Creating great communities," and "Housing diversity and choice."

As our site is located within the Airport Vicinity Protection Area (AVPA) which restricts residential development; we are hopeful that ongoing negotiations between the City of Calgary and the Airport Authority will enable the intensification of this strategic site.

We look forward to Administration, Calgary Planning Commission and Council support of our application.

Proposed Direct Control Guidelines

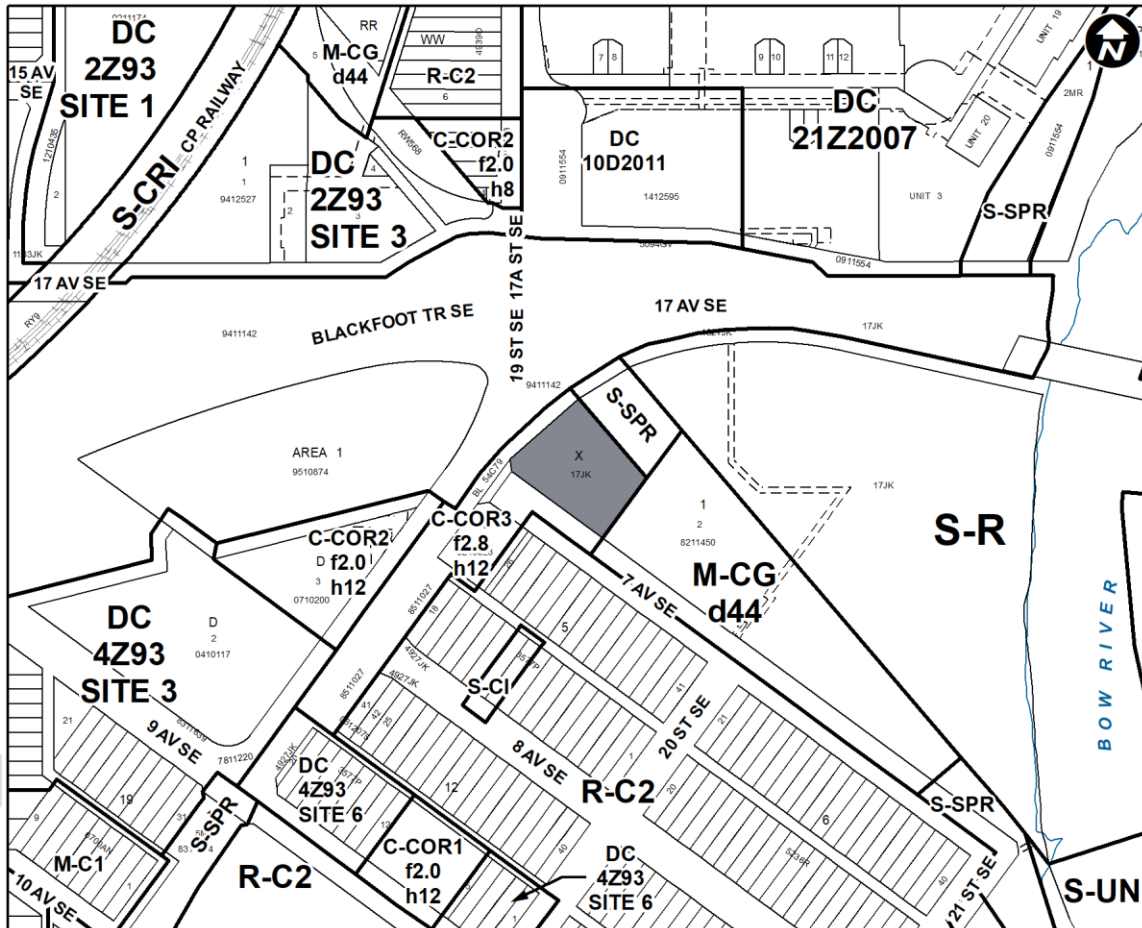
WHEREAS it is desirable to amend the Land Use Bylaw Number 1P2007 to change the land use designation of certain lands within the City of Calgary;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26 as amended;

NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

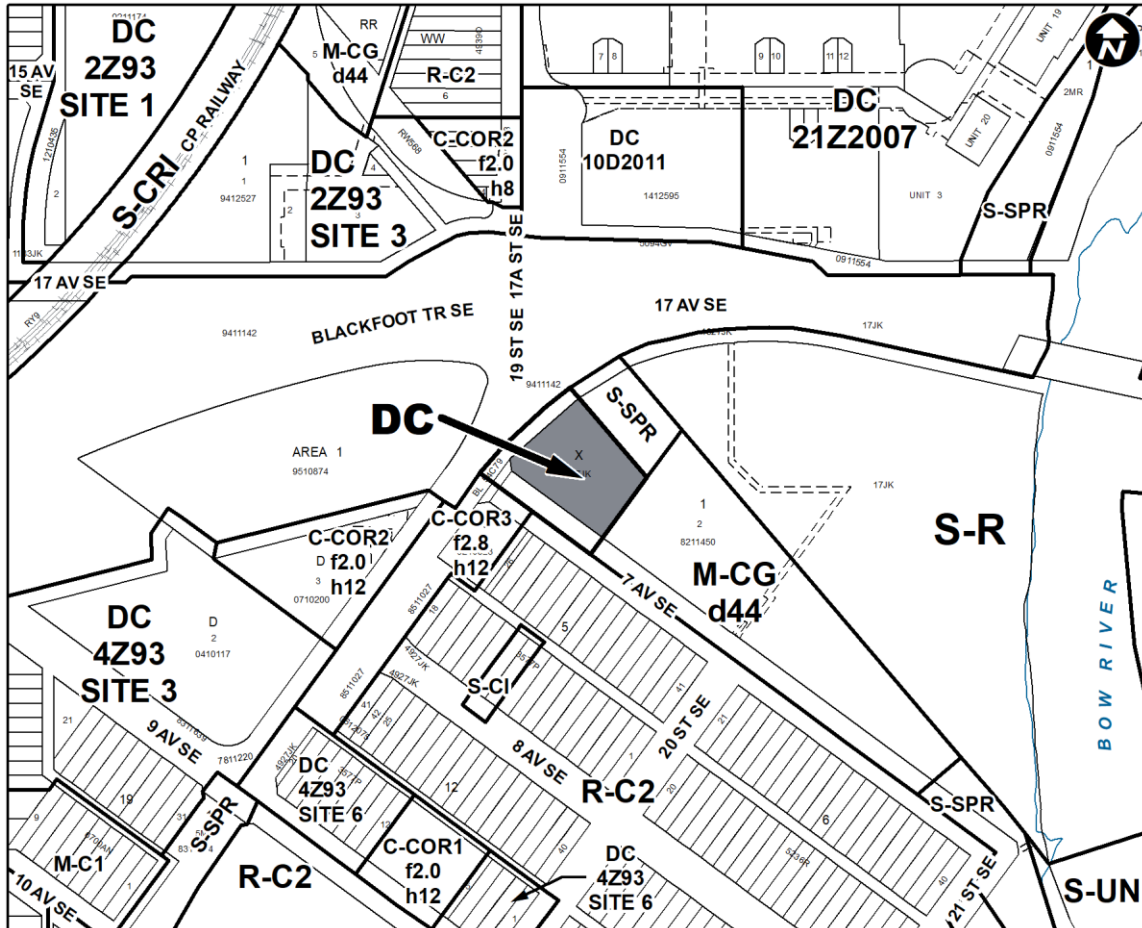
1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".

SCHEDULE A



Proposed Direct Control Guidelines

SCHEDULE B



DIRECT CONTROL DISTRICT

Purpose

1 This **Direct Control District** is intended to:

- (a) increase **density** in close proximity to the Blackfoot Trail / 9 Avenue SE Bus Rapid Transit (BRT) station;
- (b) allow for a transit oriented development that celebrates and enhances the visual experience in the Inglewood community's east gateway;
- (c) enable a **building** massing that transitions and is sensitive to the low **density** residential **development** located south of 7 Avenue SE, and encourages an active interface area with the **Park** space adjacent to the north and east **property lines**; and
- (d) provide for a bonusing system to balance the increased intensity allowed under this Direct Control District.

Proposed Direct Control Guidelines

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

General Definitions

- 4 In this Direct Control District:

- (a) “**affordable housing**” means non-market housing **units** provided within a **development** owned by the **City** and operated by a housing provider approved by the **City** to provide non-market housing.
- (b) “**universally accessible dwellings units**” means **units** that are not **affordable housing units** and that meet the following enhanced accessibility standards:
- (i) ramps, where required, have a slope no greater than 5.0 per cent;
 - (ii) a power door operator is provided at the main **building** entrance;
 - (iii) a barrier-free path of travel of at least 1.0 metres wide is provided from the **unit's** entrances to the living space;
 - (iv) all rooms and spaces provide a clear floor area of at least 1.5 metres diameter circle for the turning of a wheelchair or mobility device;
 - (v) a barrier-free path of travel between the **unit** to all on-site amenities including **balconies**, **decks**, parkade area, and **common amenity space** is provided;
 - (vi) all floor surfaces areas are firm, level and slip resistant with changes between surface materials that are beveled and leveled or are no greater than 13 millimetres;
 - (vii) doors provide a minimum clear width of 1.0 metres and provide clear spaces at the latch side to enable wheelchair users to maneuver through the door;
 - (viii) door and window hardware, cabinet pulls, and closets hardware are located at a maximum height of 1.0 metres; and
 - (ix) all electrical controls including switches and outlets are installed at a height between 0.60 metres and 1.0 metres.

Permitted Uses

- 5 (1) The **permitted uses** of the Multi-Residential – High Density Medium Rise (M-H2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.
- (2) The following **uses** are **permitted uses** in this Direct Control District if they are located on the ground floor of a **building** containing **Dwelling Units**, and face the northeast or southeast boundaries of this Direct Control District:
- (a) **Assisted Living;**

Proposed Direct Control Guidelines

- (b) **Artist's Studio;**
- (c) **Live Work Unit;**
- (d) **Outdoor Café;**
- (e) **Restaurant: Food Service Only – Small; and**
- (f) **Restaurant: Neighbourhood.**

Discretionary Uses

6 The **discretionary uses** of the Multi-Residential – High Density Medium Rise (M- H2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District with the addition of:

- (a) **Instructional Facility;**
- (b) **Hotel; and**
- (c) **Post-secondary Learning Institution.**

and with the exclusion of:

- (a) **Child Care Services;**
- (b) **Information and Service Provider;**
- (c) **Place of Worship – Medium;**
- (d) **Place of Worship – Small;**
- (e) **Service Organization; and**
- (f) **Specialty Food Store.**

Bylaw 1P2007 District Rules

7 Unless otherwise specified, the rules of the Multi-Residential – High Density Medium Rise (M-H2) District of Bylaw 1P2007 apply in this Direct Control District.

Maximum Floor Area Ratio

- 8** (1) Unless otherwise referenced in subsection (2), the maximum **floor area ratio** is 4.0.
- (2) The maximum **floor area ratio** referenced in subsection (1) may be increased in accordance with the bonus provisions set out in section 10 of this Direct Control District, to a maximum of 5.0.

Density

9 There is no minimum or maximum **density** for the **parcel**.

Bonus Floor Area Ratio and Height Earning Items

- 10** (1) Two or more of the following items may be used to earn a **density** bonusing of 1.0 **floor area ratio** and an additional 6.0 metres of **building height**:
- (a) 5.0 per cent of the total number of **units** within the **development** are **affordable housing units**;
 - (b) 5.0 per cent of the total number of **units** within the **development** are **universally accessible dwelling units**;
 - (c) 15.0 per cent of the total number of **units** within the **development** are three bedroom **units**; or,

Proposed Direct Control Guidelines

- (d) monetary contribution towards public enhancement projects in the east area of the community of Inglewood, where:
 - (i) monetary contribution will be calculated at **development permit**, following the formula below:
 - (ii) monetary contribution = contribution rate x total floor area in square metres (total floor area above the maximum **floor area ratio** of 4.0).
 - (iii) contribution rate = 50 per cent of average land value at the time of **development permit** application.
- (2) Public enhancement projects associated with the monetary contribution provided in accordance with subsection 10(1)(d) must be identified at the time of **development permit**.

Building Setbacks

- 11 (1) The minimum **building setback** from a **property line** shared with 7 Avenue SE is 3.0 metres.
- (2) Unless otherwise referenced in subsection (3) the minimum **building setback** from a **property line** shared with 19 Street SE is 3.0 metres.
- (3) The minimum **building setback** from a **property line** shared with 19 Street SE may be reduced to zero, at the discretion of the **Development Authority**, when **units** and individual **uses** located at **grade** with an exterior wall facing the **street** provide:
 - (a) individual, separate, direct access to **grade**;
 - (b) an entrance that is visible from the **street**; and
 - (c) sidewalks that provide direct exterior access to the **unit** or the **use**.
- (4) In all other cases the minimum **building setback** from a **property line** is 1.5 metres.

Landscaping

- 12 at least 50.0 per cent of the required **landscaped area** must be provided at **grade**.

Rules for Landscaped Areas

- 13 (1) Notwithstanding section 551(3), of Bylaw 1P2007, the maximum **hard surfaced landscaped area** is 50.0 per cent of the required **landscaped area**.
- (2) Notwithstanding section 551(5), **landscaped areas** located above **grade** must be a minimum of 40.0 per cent **soft surfaced landscaped area**, designed as a common **amenity space** and be accessible to all occupants of the **development**.
- (3) Unless otherwise referenced in subsection (4), 50.0 per cent of the required **landscaped area** within **setback areas** can be **hard surfaced landscaped area**

Proposed Direct Control Guidelines

- when designed as an **outdoor amenity space**.
- (4) Where a **side setback area** shares a **property line** with another **parcel**, the **hard surfaced landscaped areas** within that **side setback area** may exceed 50.0 per cent of the required **hard surfaced landscaped area** in that **side setback area**.

Additional Planting Requirements

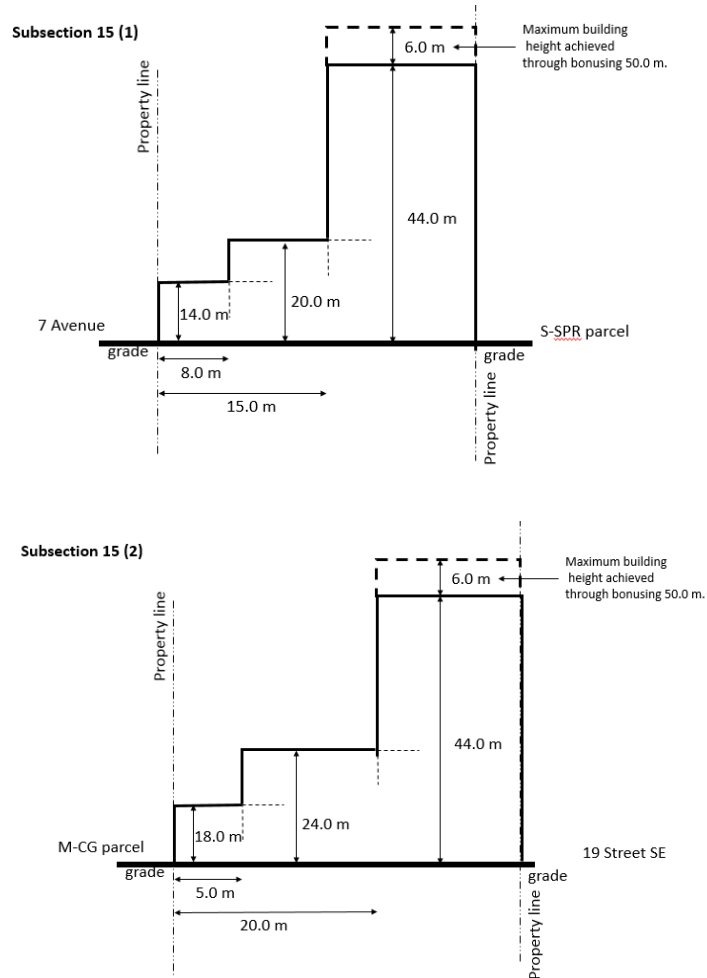
- 14 (1) **Soft surfaced landscaped** areas must meet the following planting requirements:
- (a) notwithstanding Section 552(4), of Bylaw 1P2007, deciduous trees must have a minimum **calliper** of 65 millimetres and at least 50.0 per cent of the provided deciduous trees must have a minimum **calliper** of 85 millimetres at the time of planting; and
 - (b) notwithstanding Section 552(5), of Bylaw 1P2007, coniferous trees must have a minimum height of 3.0 metres and at least 50.0 per cent of the provided coniferous trees must have a minimum height of 4.0 metres at the time of planting.

Building Height

- 15 (1) where the **parcel** shares a **property line** with 7 Avenue SE the maximum **building height** is:
- (a) 14.0 metres measured from **grade** at the shared **property line**;
 - (b) 20.0 metres measured from **grade** within 8.0 metres from the shared **property line**; and
 - (c) 44.0 metres measured from **grade** within of 15.0 metres from the shared **property line**.
- (2) Where the **parcel** shares a **property line** with 2040 - 7 Avenue SE, the maximum **building height** is:
- (a) 18.0 metres measured from **grade** within **property line**;
 - (b) 24.0 metres measured from **grade** within 5.0 metres from the shared **property line**; and
 - (c) 44.0 metres measured from **grade** within 20.0 metres from the shared **property line**.
- (3) In all other cases, the maximum **building height** is 44.0 metres.
- (4) The maximum **building height** in subsections (1)(c), (2)(c), and (3) may be increased in accordance with the bonus provisions set out in section 10 of this Direct Control District to a maximum of 50.0 metres.
- (5) The following diagrams illustrate the rules of subsections (1), and (2):

Proposed Direct Control Guidelines

Illustration 1: Building Height and Cross Section in the DC Direct Control District



Rules for At Grade Uses or Units

16 For the additional **permitted uses** listed in section 5(2) parking requirements can be reduced to a maximum of 70.0 per cent when two or more of the following are met:

- (a) outdoor amenity spaces with seating areas no less than 10.0 square metres are provided;
- (b) **signs** or banners occupy less than 10.0 per cent of the façade area of the **use**; or,
- (c) 40.0 per cent or more of any portion of the **building** that is not glazing is finished with one or more of the following materials:
 - (i) sandstone;
 - (ii) limestone;
 - (iii) traditional brick;
 - (iv) metal; or

Proposed Direct Control Guidelines

(v) wood.

Building Materials, Design Elements and Signage

- 17 (1) Accent materials for the **building** must be one or a combination of:
- (a) sandstone;
 - (b) limestone;
 - (c) traditional brick;
 - (d) metal; or
 - (e) wood.
- (2) Glazing material must meet The City of Calgary Bird Friendly Guidelines.
- (3) The façade of the **building** from **grade** up to 18.0 metres of height must include one or more of the following features:
- (a) façade articulation that reflects the architectural styles of the community;
 - (b) design elements, such as cornice lines, window sills, window bays, canopies, decorative flatwork, decorative brackets, ornamented windows, stone-clad base, and metal cornices; and,
 - (c) texture and building materials including sandstone, limestone, traditional brick, wood or metal.
- (4) All **signs** within this Direct Control District must follow a consistent style and have at least one common feature such as form, material, or colour.

Location of at grade parking areas

- 18 At **grade** parking areas must not be located between any **building** and a **property line** within this Direct Control District.

Projections Into Setback Areas

- 19 (1) Unless otherwise specified below, **buildings** and **balconies** must not be located in any **setback area**.
- (2) Eaves, architectural overhangs, and **balconies** of a **building** may project into any **setback area** to a maximum of 3.0 metres when facing the northeast or southeast boundaries of this Direct Control District.
- (3) Parkade ramps, associated curbs, guard rails, and venting may project into any **setback area**.
- (4) Portions of a **building** below **grade** may extend into any **setback area**, where those portions are used as a parking structure.
- (5) **Signs** may be located in any **setback area**, and where so located, must be in accordance with Part 3, Division 5 of Bylaw 1P2007.

Inglewood Community Association Letters



INGLEWOOD COMMUNITY ASSOCIATION
1740 24TH AVE SE
CALGARY, ALBERTA
T2G 1P9
PHONE: 403-264-3835
EMAIL: info@icacalgary.com

May 16, 2018

Development Circulation Controller
Development and Building Approvals #8201
Box 2100, Station M
Calgary, Alberta
T2P 2M5

Dear Lisette Burga Ghersi:

Re: LOC2018-0066, 740 19 Street SE

The Planning Committee (PC) has reviewed the application regarding Rezoning from C-COR3 to M-H2f5h50d580 at the above location. We have met with the applicant over several months as this project iterated from the original height of 22 storeys, including as you know, the open forum hosted by the ICA that you attended.

It is fair to say that we have found the applicant's approach to the community to be confusing to the point of being obstreperous throughout, since they have steadfastly refused to listen to the community's clear expectations for the form of any development on the site. The only reason that has been given for this willful refusal to negotiate has been project economics: we do not consider that their bottom line is relevant to good planning choices. We fully expect the site to develop and think that the six storey SoBow across the street to be an appropriate height considering its proximity to single family homes that will be impacted on 7th, 8th and 9th Avenues. It is also in the AVPA regulation area and cannot be developed any further under current rules. For some time, we have indicated that issues of overlooking, shading, environmental impacts (proximity to the flood fringe and bird sanctuary), transportation routes, etc. associated with the project make the height and size of the proposal unequivocally unsuitable for the location. Any suggestion that TOD density could be considered in supporting this hitherto unheard-of density belies the commitment made to both Inglewood and Ramsay at the beginning of the ARP consultation that these regulations would not be appropriate in our heritage communities.

We are fully aware of the proposed upzoning include in the latest ARP draft. We did not approve (nor were shown) this recent amendment and do not support it. We will be doing our best to have the area rezoning (assuming of course that AVPA is ever lifted) changed

Inglewood Community Association Letters

to neighborhood mid-rise and would support an application that complied with that guidance. The height proposed is still more than twice what is reasonable on the site. It is not considered acceptable to cast aside two years of negotiation on the ARP to effectively go in and gut one third of the east arm of East Inglewood. Therefore, a motion to not to support the permit was proposed by the PC and was passed unanimously at a well attended May 7 general meeting.

I would also note that distribution of a survey addressing a development permit for the building at the limited notification Open House on Monday April 7 to be optically very inappropriate. We would hope that the Planning Department might at least pretend that the land use change was still open for consideration before going on to the DP stage. I would appreciate receiving a written copy of the formal decision on the permit. If you have any questions, please call me at 403-263-4896.

Yours very truly,

INGLEWOOD COMMUNITY ASSOCIATION
Planning Committee

A handwritten signature in black ink, appearing to be 'L.J. Robertson', with a long horizontal line extending to the right.

L.J. Robertson, Chair

Inglewood Community Association Letters



INGLEWOOD COMMUNITY ASSOCIATION
1740 24TH AVE SE
CALGARY, ALBERTA
T2G 1P9
PHONE: 403-264-3835
EMAIL: info@icacalgary.com

October 2, 2018

Development Circulation Controller
Development and Building Approvals #8201
Box 2100, Station M
Calgary, Alberta
T2P 2M5

Dear Lisette Burga Ghersi:

Re: LOC2018-0066, 740 19 Street SE

This correspondence will serve to augment rather replace our May 16 correspondence to you as we stand by our original comments. The Inglewood Community Association Board appreciates the time that you have taken to reach out to us as the format of the proposed Direct Control bylaw evolved.

We understand that you will incorporate our most recent suggestions of reducing the monetary compensation “bonussing” options to one choice only wherein any funded project would be something that would be the result of community consultation and the overall formula would involve the choice of two or more of the options.

For clarity, we would like to underline that, as much as we appreciate the opportunity for comment, our input after the actual LOC was submitted was very limited. We were under the impression that the process would be a collaborative negotiation between ourselves and the applicant but were given no relaxation at all on the height portion of the equation. We did not show any appetite for bonussing either, yet its introduction has made an astronomical height increase even worse. For the record, we want to emphasize that in the one session with the applicant, our attempts to negotiate were completely rebuffed. The “reduction of height” that took place from their first proposal to actual application (22 storeys to 16) we see as posturing since the original proposal was triple the regulation in place.

We are and have been in an unfortunate position where, while having an existing ARP, the phantom draft ARP clouds the assessment with heretofore unheard-of heights, along with a generic application of TOD density which has also not yet been discussed with or

Inglewood Community Association Letters

agreed to by the community. As such, we are having incomplete/draft policy imposed upon us that was in no way negotiated or revised at all. Again, we reiterate that we fully support development of this site but only in a way that is contextual for the location and is compatible with the neighborhood, i.e. something along the height of six floors, which provides ample density and is already in existence in the SoBow complex.

Yours very truly,

INGLEWOOD COMMUNITY ASSOCIATION
Planning Committee

A handwritten signature in dark ink, appearing to be 'L.J. Robertson', with a long horizontal line extending to the right.

L.J. Robertson, Chair

Calgary International Airport Authority Letter



June 01, 2018

File: YYC 1567

The City of Calgary
Development & Building Approvals #B201
P.O. Box 2100 Station "M"
Calgary, Alberta, T2P 2M5

Attention: Lisette Burga Ghersi
Land Use Amendment LOC2018-0066
Land Use Change at 740 19 Street SE

Dear Lisette Burga Ghersi:

The purpose of this letter is to reply to your request of April 27, 2018, to review the Land Use Amendment LOC2018-0066 with respect to the proposed Land Use Change at 740 19 Street SE to accommodate development of a 16 storey residential tower.

The Calgary Airport Authority **objects** to this proposal as the land use amendment **does not comply** with the Calgary International Airport Vicinity Protection Area (AVPA) Regulation. The proposed amendment area is located within the 30-35 NEF (Noise Exposure Forecast) contours and residences are considered **prohibited** uses within this area (Schedule 3, TABLE). Section 3(3) of the Regulation also states that "...no subdivision approval may be given and no development permit may be issued by a municipality relating to land in the Protection Area if the proposed use of that land is a prohibited use...". Further, the Airport Authority would object to any exemption request that is related to this proposal.

The proposal does not conflict with any other areas of our review.

I trust that the above comments will be of use to you in your review of this proposal. If you have any questions or require further information regarding these comments, please do not hesitate to contact me at 403.735.5343 or stefank@yyc.com.

Yours truly,

Stefan Kutac
Development Coordinator

Cc: Gord Falk, Calgary Airport Authority
Mariel Higuerey, Calgary Airport Authority

LOC2018-0066

Page 1 of 1

Applicant's Engagement Summary

Summary of Engagement Activities

Date	Activity	Purpose / Discussion	Outcome
July 13, 2017	Meeting with the Inglewood Community Association	<p>The project team held an informal meeting with the Inglewood Community Association to provide an introduction to the project.</p> <p>At this time, the project team was proposing a 22-storey tower on the site.</p> <p>Some board members were not supportive of the proposed building height, but were supportive of development on the vacant property.</p>	The project team reduced the height of the tower to 20 storeys prior to attending a meeting with the Inglewood Community Association Planning Committee
November 1, 2017	Meeting with the Inglewood Community Association Planning Committee	<p>The project team met with the Inglewood Community Association Planning Committee to introduce the committee to the project.</p> <p>The team presented revised plans, showing a 20-storey tower and 14 townhouse units.</p> <p>Some members of the ICA remained unsupportive of the concept due to the building height. Attendees also noted traffic and parking concerns, as well as the AVPA regulations.</p>	The project team was concurrently developing a project brochure to introduce the concept to the community, facilitate conversation and collect feedback on the proposed concept from the broader public.
December 6, 2017	Project brochure distributed to nearly 700 adjacent homes, businesses and key community stakeholders	<p>The purpose of the brochure was to introduce the project to neighbours and invite them to contact the project team or schedule a meeting to discuss the project, ask questions and provide input.</p> <p>The project brochure shared the concept of a</p>	The project team understood the desire for a public engagement opportunity and began planning a public open house to be held in February 2018.

Applicant's Engagement Summary

		<p>20-storey tower and 14 townhouse units.</p> <p>The project team received a number of phone calls and emails about the project between December 2017 and February 2018. During this time, the team received a number of requests for a broad public engagement opportunity.</p> <p>A number of project supporters also identified themselves throughout this time.</p>	
<p>February 10, 2018</p>	<p>Public Open House at Cold Garden</p>	<p>The purpose of the open house was to:</p> <ul style="list-style-type: none"> • Share information with the broader public about the proposed development and the project process • Engage in thoughtful conversation • Answer questions • Listen to and collect comments about the project team's vision for The Grid <p>The WWH report contains information about how the event was promoted and a summary of feedback we heard from the community including:</p> <ul style="list-style-type: none"> - Density - Height - Access of 19 Street SE - Look and feel of the building - Parking 	<p>Over 120 people attended the Public Open House and 65 feedback forms were collected.</p> <p>Shortly after the open house, the project team was invited by the Community Association to attend a Town Hall meeting, where the project was further discussed.</p>

Applicant's Engagement Summary

<p>March 1, 2018</p>	<p>Town Hall meeting at Inglewood Community Association</p>	<p>The project team was invited to speak and answer questions at a Town Hall meeting about the proposed concept.</p> <p>A number of community members expressed their remaining concerns about the proposed development.</p>	<p>After the Public Open House and Town Hall meeting, the project team responded to the feedback received by further reducing the height to 17 storeys.</p> <p>Upon receiving comments from The City of Calgary, the project team further reduced the height to 16 storeys to enable the development to fit within the-M-H2 District.</p> <p>Additional modifications to the concept in response to public feedback:</p> <ul style="list-style-type: none"> ○ Narrowed the tower component to from its original approximate 750 m width, resulting in a less imposing mass of a building ○ Located the tower at the northwest corner of the site, oriented towards 19 Street SE furthest away from the existing single detached residential development ○ Remaining development transitions and steps down in height, south along 19 Street SE to 5 storeys ○ 6 Storey development transitions and steps down to 2 storey townhouses across the street from existing single detached homes along 7th Avenue SE ○ Similar density on the site of 5 FAR but with better location of height to provide a more sensitive transition to the existing residential community
<p>March 15, 2018</p>	<p>What We Heard report distributed to email subscriber list</p>	<p>The project team sent an email update to the subscriber list, thanking them for attending the open house and sharing the WWH report.</p>	<p>NA</p>
<p>April 19, 2018</p>	<p>Project update to email</p>	<p>The project team sent an email update to the subscriber list to share</p>	<p>The project team revised the application to a Direct Control district at the request of the City to</p>

Applicant's Engagement Summary

	distribution list	<p>information about how community feedback and recommendations from The City led the project team to reduce the proposed land use from MH-3 to MH-2 (16 storeys).</p> <p>The update also explained the City's recommendation for a Direct Control district and the benefits of this.</p>	better address interface and transition issues.
April 25, 2018	Project update to email distribution list	<p>The project team sent a second update to the community sharing information about the final Land Use Redesignation application submission for a Direct Control district based on MH-2 and shared information about next steps.</p> <p>The update shared the refined concept with renderings and updated shadow studies.</p> <p>It also shared responses to each of the main themes of concerns the project team had heard to date from the public.</p>	NA
September 6, 2018	Meeting with Inglewood Community Association	<p>The project team met with the Inglewood Community Association to discuss the final application.</p> <p>The project team shared updated renderings and shadow studies, demonstrating how the community's feedback shaped the updated concept including a significant reduction in height, updated look and feel of the building, and the modification to a</p>	NA

Applicant's Engagement Summary

		<p>Direct Control district to help mitigate concerns.</p> <p>The Community Association expressed their concerns with the height of the 16-storey tower.</p>	
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News Coverage

January 11, 2018	Metro news	http://www.metronews.ca/news/calgary/2018/01/11/inglewood-pushes-back-against-20-storey-tower.html
January 27, 2018	Global News	https://globalnews.ca/news/3991262/residents-businesses-weigh-in-on-proposed-20-storey-high-rise-in-inglewood/
February 7, 2018 (5PM)	CTV News	https://calgary.ctvnews.ca/video?binId=1.1201914#1311759
February 7, 2018 (6PM)	CTV News	https://calgary.ctvnews.ca/proposed-highrise-draws-ire-of-some-inglewood-residents-1.3794470
February 10, 2018	CTV News	https://calgary.ctvnews.ca/inglewood-residents-investigate-new-development-planned-for-the-community-1.3798431
February 15, 2018	Globe and Mail	https://www.theglobeandmail.com/real-estate/calgary-and-edmonton/plan-for-condo-tower-in-calgarys-inglewood-strikes-anerve/article37961103/
March 2, 2018	CBC News	http://www.cbc.ca/news/canada/calgary/inglewood-the-grid-tower-1.4558933.
March 23, 2018	CREB	http://www.crebnow.com/common-ground/