



AGENDA

CALGARY PLANNING COMMISSION

July 26, 2018, 1:00 PM
IN THE COUNCIL CHAMBER
Members

Director M.Tita, Chair
Director R. Vanderputten, Vice-Chair
Councillor J. Gondek
Councillor E. Woolley
Citizen Representative M. Foht
Citizen Representative C. Friesen
Citizen Representative A. Palmiere
Citizen Representative L. Juan
Citizen Representative J. Scott
Citizen Representative P. Gedye
Mayor N. Nenshi

1. CALL TO ORDER
2. OPENING REMARKS
3. CONFIRMATION OF AGENDA
4. CONFIRMATION OF MINUTES
 - 4.1 Minutes of the Regular Meeting of the Calgary Planning Commission, 2018 July 12
5. CONSENT AGENDA

None
6. POSTPONED REPORTS

(including related/ supplemental reports)

None
7. ITEMS FROM OFFICERS, ADMINISTRATION AND COMMITTEES
 - 7.1 DEVELOPMENT ITEMS
None

7.2 PLANNING ITEMS

- 7.2.1 Policy Amendments – Southeast Industrial Area Structure Plan (Ward 12), M-2018-001, CPC2018-0863
- 7.2.2 Land Use Amendment in Haysboro (Ward 11) at 9515 and 9527 Horton Road SW, LOC2018-0081, CPC2018-0859
- 7.2.3 Land Use Amendment in Inglewood (Ward 9) at 1302 – 11 Avenue SE, LOC2018-0112, CPC2018-0868
- 7.2.4 Enabling Successful Infill Development, CPC2018-0888
- 7.2.5 Enabling Successful Rowhouse Development in the R-CG District, CPC2018-0883
- 7.2.6 Land Use Amendment in North Glenmore Park (Ward 11) at 5315 - 19 Street SW, LOC2018-0057, CPC2018-0902
- 7.2.7 Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 – 33 Avenue SW, LOC2018-0098, CPC2018-0901
- 7.2.8 Land Use Amendment in Glamorgan (Ward 6) at 3 Gissing Drive SW, LOC2018-0106, CPC2018-0919
- 7.2.9 Policy Amendment and Land Use Amendment in Mount Pleasant (Ward 7) at 602 - 20 Avenue NW, LOC2018-0119, CPC2018-0890
- 7.2.10 Land Use Amendment in Westwinds (Ward 5) at 76 Westwinds Crescent NE, LOC2018-0103, CPC2018-0869
- 7.2.11 Land Use Amendment in Sunnyside (Ward 7) at 902 and 904 – 2 Avenue NW, LOC2018-0097, CPC2018-0849
- 7.2.12 Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 438 - 8 Street NE, LOC2017-0127, CPC2018-0841
- 7.2.13 Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 230 - 7A Street NE, LOC2018-0021, CPC2018-0840
- 7.2.14 Land Use Amendment in Thorncliffe (Ward 4) at 6327 Tregillus Street NW, LOC2018-0047, CPC2018-0896

7.3 MISCELLANEOUS ITEMS

- 7.3.1 9 Avenue SE Bridge Replacement - Inglewood (Ward 9) & East Village (Ward 7), 9 Avenue SE over Elbow River, CPC2018-0932

8. ADJOURNMENT



INDEX FOR THE 2018 JULY 26 REGULAR MEETING OF CALGARY PLANNING COMMISSION

NOTE:
PLANS SUBMITTED TO THE COMMISSION, AS PART OF THE DEPARTMENTAL REPORTS,
ARE INCLUDED FOR THE SOLE PURPOSE OF ASSISTING THE COMMISSION IN MAKING
A DECISION AND ARE NOT TO BE USED FOR ANY OTHER PURPOSE.

ISC: Unrestricted



PLANNING ITEMS

ITEM NO.: 7.2.1

Michele Bussiere

COMMUNITY:

Southeast Industrial (Ward 12)

FILE NUMBER:

M-2018-001 (CPC2018-0863)

PROPOSED POLICY AMENDMENT:

Amendments to the Southeast Industrial Area Structure Plan

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.2

Ian Bamford

COMMUNITY:

Haysboro (Ward 11)

FILE NUMBER:

LOC2018-0081 (CPC2018-0859)

PROPOSED REDESIGNATION:

From: Industrial – General (I-G) District

To: Industrial – Commercial (I-C) District

MUNICIPAL ADDRESS:

9515 and 9527 Horton Road SW

APPLICANT:

Riddell Kurczaba Architecture

OWNER:

PBA Land Developments Ltd

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.3

Chris Wolfe

COMMUNITY:

Inglewood (Ward 9)

FILE NUMBER:

LOC2018-0112 (CPC2018-0868)

PROPOSED REDESIGNATION:

From: Residential – Contextual One / Two Dwelling (R-C2) District

To: Mixed Use – General (MU-1f2.0h14) District

MUNICIPAL ADDRESS:

1302 – 11 Avenue SE

APPLICANT:

Amrit Design Drafting Services

OWNER:

Veena Mann
Rambir Mann

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.4

Tammy Henry

COMMUNITY:

City Wide

FILE NUMBER:

CPC2018-0888

PROPOSED BYLAW AMENDMENTS:

Amendments to Land Use Bylaw (1P2007)
(Enabling Successful Infill Development, Phase 1)

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.5

Stephen Pearce

COMMUNITY:

City Wide

FILE NUMBER:

CPC2018-0883

PROPOSED BYLAW AMENDMENTS:

Amendments to Land Use Bylaw (1P2007)
(Enabling Successful Rowhouse Development in the
R-CG District)

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.6

Colleen Renne-Grivell

COMMUNITY:

North Glenmore Park (Ward 11)

FILE NUMBER:

LOC2018-0057 (CPC2018-0902)

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS:

5315 – 19 Street SW

APPLICANT:

CivicWorks Planning + Design

OWNER:

David A Johnston

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.7

Colleen Renne-Grivell

COMMUNITY: South Calgary (Ward 8)

FILE NUMBER: LOC2018-0098 (CPC2018-0901)

PROPOSED AMENDMENT: Amendment to the South Calgary/Altadore Area
Redevelopment Plan

PROPOSED REDESIGNATION: From: Residential – Contextual One / Two Dwelling
(R-C2) District

To: Multi-Residential – Contextual Grade-Oriented
(M-CG) District

MUNICIPAL ADDRESS: 1519 - 33 Avenue SW

APPLICANT: M K M Design

OWNER: Trung Bien

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.8

Dino Civitarese

COMMUNITY: Glamorgan (Ward 6)

FILE NUMBER: LOC2018-0106 (CPC2018-0919)

PROPOSED REDESIGNATION: From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS: 3 Gissing Drive SW

APPLICANT: Civicworks Planning + Design

OWNER: Duc Thi Nguyen

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 7.2.9

Jennifer Cardiff

COMMUNITY:

Mount Pleasant (Ward 7)

FILE NUMBER:

LOC2018-0119 (CPC2018-0890)

PROPOSED AMENDMENT:

Amendment to the North Hill Area Redevelopment Plan

PROPOSED REDESIGNATION:

From: Residential – Contextual One / Two Dwelling
(R-C2) District

To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS:

602 - 20 Avenue NW

APPLICANT:

New Century Design

OWNER:

Sharon J West

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.10

Jennifer Cardiff

COMMUNITY:

Westwinds (Ward 5)

FILE NUMBER:

LOC2018-0103 (CPC2018-0869)

PROPOSED REDESIGNATION:

From: DC Direct Control District

To: Industrial – Commercial (I-C) District

MUNICIPAL ADDRESS:

76 Westwinds Crescent NE

APPLICANT:

Rick Balbi Architect

OWNER:

2047553 Alberta Ltd (Amarpreet Singh Bains)

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.11

Steve Jones

COMMUNITY:

Sunnyside (Ward 7)

FILE NUMBER:

LOC2018-0097 (CPC2018-0849)

PROPOSED REDESIGNATION:

From: Multi-Residential – Contextual Grade-Oriented
(M-CGd72) District

To: DC Direct Control District to accommodate multi-
residential development with density bonus

MUNICIPAL ADDRESS:

902 and 904 – 2 Avenue NW

APPLICANT:

O2 Planning and Design

OWNER:

Robert Froese

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.12

Sara Kassa

COMMUNITY:

Bridgeland/Riverside (Ward 9)

FILE NUMBER:

LOC2017-0127 (CPC2018-0841)

PROPOSED AMENDMENT:

Amendment to the Bridgeland-Riverside Area
Redevelopment Plan

PROPOSED REDESIGNATION:

From: Residential – Contextual One / Two Dwelling
(R-C2) District

To: DC Direct Control District to accommodate a
specific multi-residential development as a
permitted use

MUNICIPAL ADDRESS:

438 – 8 Street NE

APPLICANT:

O2 Planning and Design

OWNER:

Yasmin Kanji
Shafik Kanji

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.13

Sara Kassa

COMMUNITY:

Bridgeland/Riverside (Ward 9)

FILE NUMBER:

LOC2018-0021 (CPC2018-0840)

PROPOSED AMENDMENT:

Amendment to the Bridgeland-Riverside Area
Redevelopment Plan

PROPOSED REDESIGNATION:

From: DC Direct Control District

To: DC Direct Control District to accommodate a
specific multi-residential development as a
permitted use

MUNICIPAL ADDRESS:

230 – 7A Street NE

APPLICANT:

O2 Planning and Design

OWNER:

Zakir Kanji
Adil Mawji

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.14

Giyen Brenkman

COMMUNITY:

Thornccliffe (Ward 4)

FILE NUMBER:

LOC2018-0047 (CPC2018-0896)

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Contextual One / Two Dwelling
(R-C2) District

MUNICIPAL ADDRESS:

6327 Tregillus Street NW

APPLICANT:

Adele Margaret Caton

OWNER:

Chiu Ki Ma
Nuey Ling Wong
Wai Man Wong

ADMINISTRATION RECOMMENDATION:

APPROVAL

MISCELLANEOUS ITEMS

ITEM NO.: 7.3.1

Evan Fer

COMMUNITY:

Inglewood (Ward 9) and East Village (Ward 7)

FILE NUMBER:

CPC2018-0932

PROPOSED:

9 Avenue SE Bridge Replacement Project Report

ADMINISTRATION RECOMMENDATION:

FOR INFORMATION ONLY

MINUTES OF THE CALGARY PLANNING COMMISSION

HELD ON THURSDAY, 2018 JULY 12, AT 1:00 P.M.

COUNCIL CHAMBERS, CITY HALL

PRESENT:

Mr. Matthias Tita (Chairman)	Director of Calgary Growth Strategies
Mr. Ryan Vanderputten	Director of Transportation Planning
Ms. Jyoti Gondek	Councillor, Ward 3
Mr. Colin Friesen	Citizen Representative
Mr. Melvin Foht	Citizen Representative
Ms. Lourdes Juan	Citizen Representative
Mr. James Scott	Citizen Representative
Mr. Paul Gedye	Citizen Representative

ABSENT:

Mr. Evan Woolley	Councillor, Ward 8
Mr. Andrew Palmiere	Citizen Representative

CONFIRMATION OF AGENDA:

AMENDMENT:

Item 6.03 (LOC2018-0039 (CPC2018-0486)) has been withdrawn from the Agenda at the request of the Applicant.

AMENDMENT:

Item 6.04 (LOC2017-0391 (CPC2018-0812)) has been withdrawn from the Agenda at the request of the Applicant.

The 2018 July 12 Calgary Planning Commission Agenda was adopted, as amended.

Moved by: J. Scott

Carried: 7 – 0

CONFIRMATION OF MINUTES:

The minutes of the meeting held on 2018 June 28 were approved.

Moved by: L. Juan

Carried: 7 – 0

ADJOURNMENT:

The meeting was adjourned at 2:35 p.m.

Moved by: C. Friesen

Carried: 7 – 0

Minutes prepared by: **Kimberly Holberton**

The minutes of the Calgary Planning Commission, held 2018 July 12, will be confirmed on 2018 July 26.

UNCONFIRMED

ITEM NO.: 3.01

Michael Davis

COMMUNITY:

Crescent Heights (Ward 7)

FILE NUMBER:

LOC2018-0075 (CPC2018-0844)

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1) District

To: Residential – Contextual One Dwelling (R-C1s) District

MUNICIPAL ADDRESS:

616 Centre A Street NW

APPLICANT:

Stephen Ho Architect

OWNER:

Yuk Ching Ho

ADMINISTRATION RECOMMENDATION:**APPROVAL****PLANNING COMMISSION DECISION:**

2018 July 12

The Calgary Planning Commission recommended that Council:

1. **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares \pm (0.13 acres \pm) located at 616 Centre A Street NW (Plan 4456R, Block 34, Lot 18-20) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District, in accordance with Administration's recommendation; and
2. Give three readings to the proposed Bylaw.

Moved by: J. Scott**Carried: 6 – 1**

Opposed: M. Foht

ITEM NO.: 5.01**COMMUNITY:**

Gareth Webster

Patterson (Ward 6)

FILE NUMBER:

DP2017-3596 (CPC2018-0835)

PROPOSED DEVELOPMENT:

New: Multi-Residential Development (1 building)

MUNICIPAL ADDRESS:

940 Patrick Street SW

APPLICANT:

Statesman Cougar Ridge Ltd

OWNER:

Manco Family Trust

ADMINISTRATION RECOMMENDATION:**APPROVAL****PLANNING COMMISSION DECISION:**

2018 July 12

The Calgary Planning Commission **APPROVED** the proposed New: Multi-Residential Development (1 building) at 940 Patrick Street SW, in accordance with Administration's recommendation.

Moved by: P. Gedye**Carried: 4 – 3**

Opposed: J. Gondek, C. Friesen and L. Juan

Reasons for Opposition from Mr. Friesen:

- I did not support this development permit for a number of reasons. Slope adaptive design was one of the Policies in the associated Concept Plan that the community expected to see. This was abandoned apparently because of slope stability issues but when questioned regarding the details of geotechnical problems no additional information was provided. All things being equal consultants will always advise against building on a slope even when it is feasible.
- The building is essentially one large block with only limited attempts to articulate or break the mass. It does not appear to be sensitive to adjacent existing development as called for in the Concept Plan. City administration acknowledges that there were architectural issues with the building but it was not referred to UDRP. UDRP would seem to be the ideal mechanism to give independent credence to the judgements of City administration when there is controversy unless of course administration is concerned they will not be supported.
- There are a number of buildings built on slopes in the city but when asked why this site is different the answer was we trust our consultants and they advise against building on the slope. In general, when questioned regarding architectural issues the answer from administration was it meets the City's criteria and it was the best we could do, without

providing much additional detail. The nature of these answers is not particularly encouraging or convincing. For this reason and because the design itself did not seem to fulfill some basic goals I could not support this development permit.

UNCONFIRMED

ITEM NO.: 6.01

Ezra Wasser

COMMUNITY:

East Shepard Industrial (Ward 12)

FILE NUMBER:

LOC2018-0109 (CPC2018-0803)

PROPOSED REDESIGNATION:

From: Industrial – General (I-G) District

To: Industrial – Commercial (I-C) District

MUNICIPAL ADDRESS:

4807 - 112 Avenue SE

APPLICANT:

Rick Balbi Architect

OWNER:

Telsec Property Corporation

ADMINISTRATION RECOMMENDATION:

APPROVAL

PLANNING COMMISSION DECISION:

2018 July 12

The Calgary Planning Commission recommended that Council:

1. **ADOPT**, by bylaw, the proposed redesignation of 2.59 hectares \pm (6.40 acres \pm) located at 4807 – 112 Avenue SE (Plan 0713806; Block 12; Lot 9) from Industrial – General (I-G) District to Industrial – Commercial (I-C) District, in accordance with Administration's recommendation; and
2. Give three readings to the proposed Bylaw.

Moved by: J. Scott

Carried: 7 – 0

ITEM NO.: 6.02

David Mulholland

COMMUNITY:

Taradale (Ward 5)

FILE NUMBER:

LOC2016-0334 (CPC2018-0251)

PROPOSED REDESIGNATION:

From: Commercial – Community 1 (C-C1) District

To: Multi-Residential – High Density Low Rise (M-H1) District

MUNICIPAL ADDRESS:

7099 – 80 Avenue NE

APPLICANT:

Manu Chugh Architect

OWNER:

Taralake Point Investments Inc

ADMINISTRATION RECOMMENDATION:**APPROVAL****PLANNING COMMISSION DECISION:**

2018 July 12

The Calgary Planning Commission recommended that Council:

1. **ADOPT**, by bylaw, the proposed redesignation of 0.49 hectares \pm (1.20 acres \pm) located at 7099 – 80 Avenue NE (Plan 1111377, Block 3, Lot 4) from Commercial – Community 1 (C-C1) District **to** Multi-Residential – High Density Low Rise (M-H1) District, in accordance with Administration's recommendation; and
2. Give three readings to the proposed Bylaw.

Moved by: J. Gondek**Carried: 7 – 0**

ITEM NO.: 6.03

Matthew Atkinson

COMMUNITY:

Tuxedo Park (Ward 7)

FILE NUMBER:

LOC2018-0039 (CPC2018-0486)

PROPOSED AMENDMENTS:

Amendments to the North Hill Area Redevelopment Plan

PROPOSED REDESIGNATION:

From: Mixed Use – Active Frontage (MU-2f3.0h20)
District

To: Mixed Use – Active Frontage (MU-2f3.5h25)
District

MUNICIPAL ADDRESS:

2620 Centre Street NE

APPLICANT:

Mediated Solutions

OWNER:

M & Ryan Holding Ltd

ADMINISTRATION RECOMMENDATION:

APPROVAL

**WITHDRAWN BY
APPLICANT**

ITEM NO.: 6.04

Desmond Bliek

COMMUNITY:

Richmond-Knob Hill (Ward 8)

FILE NUMBER:

LOC2017-0391 (CPC2018-0812)

PROPOSED POLICY AMENDMENTS:

Amendments to the Marda Loop Area Redevelopment Plan

PROPOSED CLOSURE:

0.003 hectares \pm (0.008 acres \pm) of road adjacent to 2232 - 33 Avenue SW

PROPOSED REDESIGNATION:

From: Multi-Residential – Contextual Low Profile (M-C1) District and Undesignated Road Right-of-Way

To: DC Direct Control District based on the Mixed Use – Active Frontage (MU-2) District to accommodate opportunities for mixed-use multi-residential development with commercial uses

MUNICIPAL ADDRESS:

2232, 2236, and 2240 - 33 Avenue SW

APPLICANT:

Civicworks Planning + Design

OWNER:

Gateway 33 Ltd

ADMINISTRATION RECOMMENDATION:

APPROVAL

WITHDRAWN BY APPLICANT

ITEM NO.: 6.05

Madeleine Krizan

COMMUNITY:

Killarney/Glengarry (Ward 8)

FILE NUMBER:

LOC2018-0060 (CPC2018-0851)

PROPOSED POLICY AMENDMENT:Amendment to the Killarney/Glengarry Area
Redevelopment Plan**PROPOSED REDESIGNATION:**From: Residential – Contextual One/Two Dwelling
(R-C2) DistrictTo: Residential – Grade-Oriented Infill (R-CG)
District**MUNICIPAL ADDRESS:**

3604 Richmond Road SW

APPLICANT:

Inertia

OWNER:Huijin Ouyang
Bijun Wu**ADMINISTRATION RECOMMENDATION:****APPROVAL****PLANNING COMMISSION DECISION:**

2018 July 12

The Calgary Planning Commission recommended that
Council:

- A. 1. **ADOPT**, by bylaw, the proposed amendments to the Killarney/Glengarry Area Redevelopment Plan, in accordance with Administration's recommendation; and
2. Give three readings to the proposed Bylaw.

Moved by: L. Juan**Carried: 7 – 0**

- B. 1. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares ± (0.15 acres ±) located at 3604 Richmond Road SW (Plan 732GN, Block 3, Lot 1) from Residential – Contextual One/Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District, in accordance with Administration's recommendation; and
2. Give three readings to the proposed Bylaw.

Moved by: L. Juan**Carried: 7 – 0**

ITEM NO.: 7.01

Gareth Webster

COMMUNITY:

Medicine Hill (Ward 1)

PROPOSED:

Medicine Hill Concept Plan (Verbal Report)

ADMINISTRATION RECOMMENDATION:

FOR INFORMATION ONLY

Administration presented the Calgary Planning Commission the Concept Plan for the Residential Village in Medicine Hill.

UNCONFIRMED

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0863
Page 1 of 6

Policy Amendments – Southeast Industrial Area Structure Plan (Ward 12), M-2018-001

EXECUTIVE SUMMARY

This report discusses Administration-initiated amendments to the *Southeast Industrial Area Structure Plan* (ASP) to:

- remove the 400 metre setback from the decommissioned Phosphogypsum Stack Two; and
- add a 450 metre setback to the disposal area of operating hazardous waste management facilities.

The ASP amendment was initiated by Administration as a result of ongoing applications affected by the setbacks and to align with recent amendments to the *Municipal Government Act* (MGA).

ADMINISTRATION RECOMMENDATION(S):

That Calgary Planning Commission recommend that Council hold a Public Hearing: and

1. **ADOPT**, by bylaw, the proposed amendments to the Southeast Industrial Area Structure Plan (Attachment 1); and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

On 2015 October 05, Council approved minor amendments to the *Southeast Industrial Area Structure Plan* to remove the 400 metre setback from the decommissioned Phosphogypsum Stack One (CPC2015-178).

The decommissioning of Phosphogypsum Stack One and removal of the 400 metre setback allowed for development of schools, hospitals, food establishments, or residential uses within the previous 400 metre setback area.

Phosphogypsum Stacks (drying ponds)

In 2017, Viterra Inc completed decommissioning and capping of Phosphogypsum Stack Two. Similar to Stack One, a Risk Management Plan (RMP), has been accepted by Alberta Environment. The City of Calgary Parks department will govern its long term management.

**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0863
Page 2 of 6**

**Policy Amendments - Southeast Industrial Area Structure Plan (Ward 12),
M-2018-001**

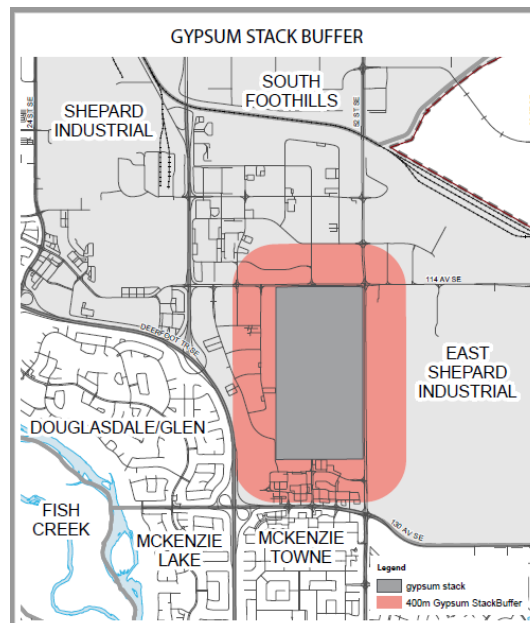
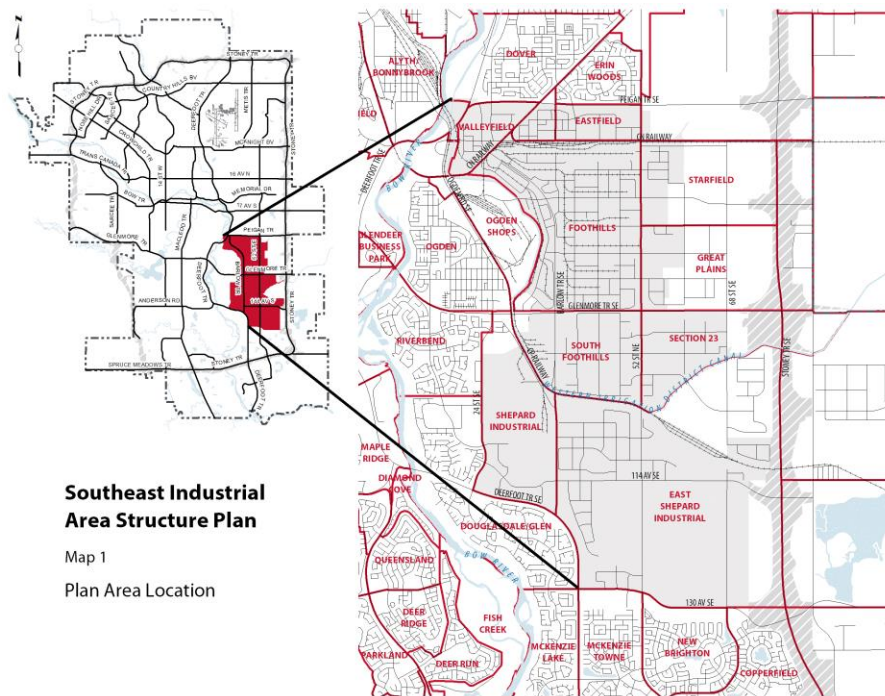
While operational, and throughout decommissioning and capping of the phosphogypsum stacks, a setback prohibited development of schools, hospitals, food establishments and residential uses within 400 metres of the property line of the stacks. The setback is no longer required and the proposed amendments to the ASP will allow the previously restricted uses.

Hazardous Waste Management Facilities

Changes to the MGA in 2017 require a 450 metre setback from the disposal area of operating hazardous waste management facilities. The setback prohibits the development of schools, hospitals, food establishments and residential uses within the setback area. To align with amendments to the MGA, Administration is proposing amendments to the *Southeast Industrial ASP* to identify and delineate the setbacks. Refer to Schedule P of the *Southeast Industrial ASP* for Hazardous Waste Management Facility setback.

Policy Amendments - Southeast Industrial Area Structure Plan (Ward 12), M-2018-001

Location Maps



Policy Amendments - Southeast Industrial Area Structure Plan (Ward 12), M-2018-001

Site Context

The *Southeast Industrial ASP* covers a land area of approximately 3,763 hectares (9,299 acres) in size, comprising primarily industrial uses, with limited commercial uses. The Plan Area is bounded by Peigan Trail SE to the north, Deerfoot Trail SE to the west, Deerfoot Trail SE and 130 Avenue SE to the south and Stoney Trail SE to the east.

The decommissioned phosphogypsum stack site is located at 12525 – 52 Street SE, 12655 and 12655R - 52 Street SE and 12725 – 52 Street SE. The four parcels total approximately 130 hectares (320 acres) in size.

The hazardous waste management facilities are located at 9611 – 44 Street SE, 10905 – 48 Street SE, 7905 – 46 Street SE, 5366 – 55 Street SE and 10501 Barlow Trail SE.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposed amendments to the *Southeast Industrial ASP* would align the policy with MGA requirements and the recent decommissioning of the phosphogypsum stack and hazardous waste management facilities setback.

Infrastructure

Transportation Networks

Transportation networks in the ASP are not affected by the proposed amendments.

Utilities and Servicing

Utilities and servicing are not affected by the proposed amendments.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, the ASP amendment to allow for additional uses within the 400 metre stack setback was circulated to adjacent landowners. A website on www.calgary.ca was created to inform the public of the amendment and included project information and an overview of the planning process.

Hazardous waste setbacks automatically apply around any hazardous waste management facility as regulated by the Subdivision and Development Regulation of the MGA. This setback is mandatory with or without the ASP therefore Administration did not send a letter to adjacent landowners.

Commission's recommendation and the date of the Public Hearing will be advertised.

Policy Amendments - Southeast Industrial Area Structure Plan (Ward 12), M-2018-001

Engagement

No citizen comments or comments from landowners circulated were received. No public meetings were held by Administration. There is no community association in the plan area.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The sites are located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to these sites, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The sites, are located within the 'Standard Industrial Area of the *Municipal Development Plan* (MDP). The applicable MDP policies encourage a broad variety of industrial uses and intensities that support business in Calgary.

The proposed amendments to the ASP are in keeping with relevant MDP policies.

Municipal Government Act – Subdivision and Development Regulation (43/2002):

The Municipal Government Act – Subdivision and Development Regulation specifies that a Development Authority shall not approve an application for school, hospital, food establishment or residential use or building site proposed for a school, hospital, food establishment or residence within 450 metres of the disposal area of an operating or non-operating hazardous waste management facility.

Southeast Industrial Area Structure Plan (Statutory, 1996)

The current *Southeast Industrial ASP* identifies a setback within 400 metres of Phosphogypsum Stack Two. The proposed amendments to remove reference of the setback on stack two and all references to the phosphogypsum stack are supported by Administration.

The current policy of the *Southeast Industrial ASP* does not identify a setback of 450 metres to operating hazardous waste management facilities. The proposed amendment to add the setback to the identified parcels and references to hazardous waste management facilities in the plan area is supported by Administration.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0863
Page 6 of 6

Policy Amendments - Southeast Industrial Area Structure Plan (Ward 12), M-2018-001

Social, Environmental, Economic (External)

Phosphogypsum Stack Two has been decommissioned and Alberta Environment and Parks has accepted the Risk Management Plan, therefore the 400 metre setback can be removed.

Amendments to the *Municipal Government Act*, to include a setback from active hazardous waste management facilities, have been accepted by the Government of Alberta, therefore, the 450 metre setback to hazardous waste management facilities will be added.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposed amendments to the *Southeast Industrial ASP* aligns the policy with the current status of Phosphogypsum Stack Two and recent changes to the *Municipal Government Act*.

ATTACHMENT(S)

1. Proposed amendments to the Southeast Industrial Area Structure Plan

Proposed Amendments to the Southeast Industrial Area Structure Plan

1. The Southeast Industrial Area Structure Plan, attached to and forming part of Bylaw 6P96 as amended, is hereby further amended.
 - (a) In section 3.0 entitled "THE PLAN (Map 2)" delete second bullet point and paragraph.
 - (b) Delete the existing Map 1 entitled "Plan Area Location" and replace with revised Map 1 entitled "Plan Area Location", as attached as Schedule A.
 - (c) Delete the existing Map 2 entitled "Land Use and Transportation Plan" and replace with revised Map 2 entitled "Land Use and Transportation Plan", as attached as Schedule B.
 - (d) In the second paragraph of subsection 4.1.3 entitled "Heavy Industrial" delete "east of the *fertilizer complex* property" and replace with "south of the irrigation canal and east of Barlow Trail SE".
 - (e) Delete the existing Map 3 entitled "Regional Road Network" and replace with revised Map 3 entitled "Regional Road Network", as attached as Schedule C.
 - (f) Delete the Map 4 entitled "Dangerous Goods and Truck Routes" and replace with revised Map 4 entitled "Dangerous Goods and Truck Routes", as attached as Schedule D.
 - (g) Delete the existing Map 5 entitled "Water Supply" and replace with revised Map 5 entitled "Water Supply", as attached as Schedule E.
 - (h) Delete the existing Map 6 entitled "Sanitary Sewer System" and replace with revised Map 6 entitled "Sanitary Sewer System", as attached as Schedule F.
 - (i) Delete the existing Map 7 entitled "Storm Sewer System" and replace with revised Map 7 entitled "Storm Sewer System", as attached as Schedule G.
 - (j) Delete the existing Map 8 entitled "Electrical Service" and replace with revised Map 8 entitled "Electrical Service", as attached as Schedule H.
 - (k) Delete the existing Map 9 entitled "Natural Gas" and replace with revised Map 9 entitled "Natural Gas", as attached as Schedule I.
 - (l) Delete the existing Map 10 entitled "Telephone and Cable Services" and replace with revised Map 10 entitled "Telephone and Cable Services", as attached as Schedule J.
 - (m) In the first paragraph of subsection 4.6 "Development Constraint Areas" delete "and the fertilizer complex" and replace with "and the hazardous waste management facilities."

Proposed Amendments to the Southeast Industrial Area Structure Plan

- (n) In section 4.6 entitled “Development Constraints Areas” delete paragraphs four through six and replace with the following:

“A fertilizer manufacturing facility previously operated within the Plan Area. Constructed in 1965 the facility operated until 1987 when the production of anhydrous ammonia, ammonium nitrate, and ammonium phosphate ceased.

Between 1987 and 2005, the production facilities were decommissioned (including the acid plants and flare stack) and the majority of infrastructure was removed from the property. A remedial program to remove impacted soil from the production area and to decommission the phosphogypsum stacks (drying ponds) was also implemented.

When the Southeast Industrial ASP was approved in 1996, a setback was established preventing residential land use within 1,600 metres of the transfer terminal (flare stack). Subsequent environmental assessments resulted in a reduced 1,100 metre setback. In addition, it was determined that schools, hospitals, food establishments, or residential uses would be restricted within 400 metres of the phosphogypsum stacks (drying ponds).

In 2015, the setback for phosphogypsum Stack 1 was removed subsequent to final decommissioning and acceptance of the ongoing Risk Management Plan (RMP) by Alberta Environment and Parks (AEP). Similarly, in 2018 the setback for phosphogypsum Stack 2 was removed following complete decommissioning and acceptance of the RMP by AEP. A fertilizer and farm products distribution terminal currently operates at 11111 Barlow Trail SE.

Long term RMPs are in place for the former fertilizer manufacturing facility and the associated drying ponds. Future land uses with the potential to compromise the integrity of the engineered cover at phosphogypsum stack sites will not be allowed.

Hazardous Waste and Management Facilities are a possible discretionary use in the Plan Area. Land use planning in proximity to Hazardous Waste and Management Facilities are governed by the Subdivision and Development Regulation (SDR) under the Municipal Government Act (MGA).

AEP publishes a list of facilities in the Alberta and Environmental Protection and Enhancement Act (EPEA) to manage hazardous waste and/or hazardous recyclables.”

- (o) In section 4.6 “Development Constraint Areas” under Recommendation, (a) delete subsection (iii) “the fertilizer complex and phosphogypsum stack” and replace with “the hazardous waste management facilities”.
- (p) In section 4.6 “Development Constraint Areas” under “Recommendation” delete sections (b) through (g).

Proposed Amendments to the Southeast Industrial Area Structure Plan

- (q) Under section 4.8 “Environmental Considerations” insert the following before section 4.8.1:

“Historically, the Southeast Industrial ASP saw considerable industrial development; as a result, there are potentially contaminated sites that may impact future development. With new development proposed for the area, it is important that a detailed environmental site assessment is completed to ensure a site is suitable for its intended use.

The environmental status of a site will be determined beginning at the Outline Plan/Land Use Amendment Stage. Pending the findings of the environmental assessment, remediation and/or risk management may be necessary. Where risk management is proposed, the Risk Management Plan (RMP) must be accepted by Alberta Environment and Parks (AEP) and Alberta Health Services (AHS) prior to the development proceeding. Even where The City has granted land use amendment for a site, the presence of contamination may make it unsuitable for the intended use.”

- (r) Delete subsection 4.8.1 entitled “Potential Soil and Groundwater Contamination” and replace with the following: 4.8.1 “Environmental Site Assessment”:

“The preceding section identified the main known features of the Plan Area. The purpose of these policies is to help ensure that any risks associated with past activities on specific sites are identified and addressed:

- a. In conjunction with an Outline Plan/Land Use Amendment application, a developer shall:
 - i. submit a current Phase I ESA to the satisfaction of the Approving Authority to identify any actual or potential or off site human health impacts soil and groundwater contamination and human health impacts and determine if the site is suitable for the intended use;
 - ii. if the Phase I ESA identifies any actual or potential or off-site contamination, submit a current Phase II ESA to the satisfaction of the Approving Authority to determine if there is a requirement for remediation or risk management on the site; and
 - iii. if the Phase II ESA determines a need for site remediation, or risk management, submit a Remedial Action Plan or Risk Management Plan to address the manner and extent that the site will be remediated or managed to render it suitable for the intended use.
- b. The ESA should refer to the more detailed guidelines contained in Appendix A.
- c. Environmental Background Studies must comply with City standards.”

- (s) In the “Supporting Information” section:

- (i) In the Table of Contents subsection 7.4.3 delete “Phosphogypsum Pond” and replace with “Hazard Waste Management Facilities”;
- (ii) Delete the existing Map 11 entitled “Natural Features” and replace with revised Map 11 “Natural Features”, as attached as Schedule K;

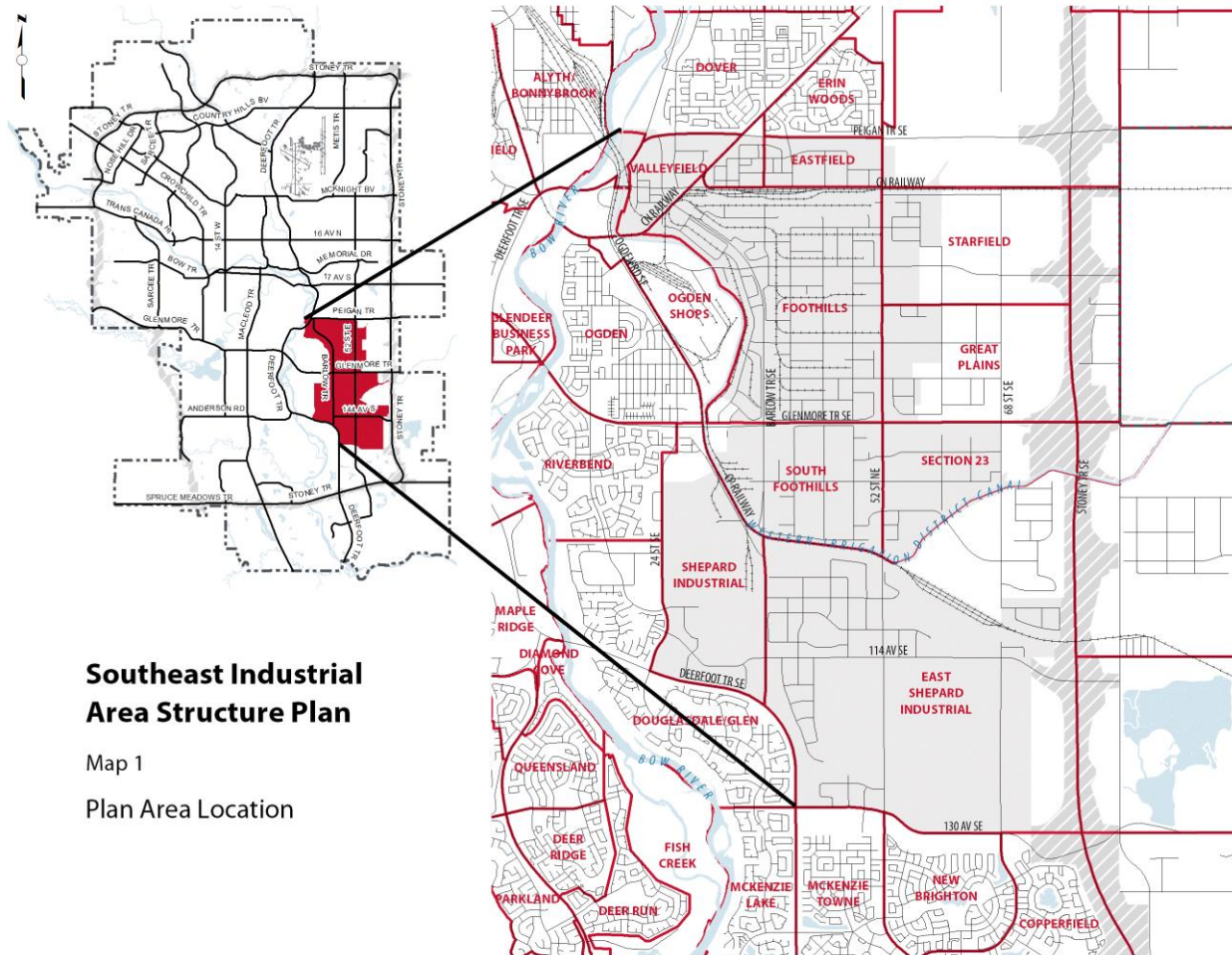
Proposed Amendments to the Southeast Industrial Area Structure Plan

- (iii) Delete the existing Map 12 entitled “Land Ownership (Major Land Owners)” and replace with revised Map 12 entitled “Land Ownership (Major Land Owners)”, as attached as Schedule L;
- (iv) In the first paragraph of section 5.3 entitled “Existing Land Use (Map 13)” delete “(e.g., fertilizer plant and associated phosphogypsum stack)”;
- (v) Delete the existing Map 13 entitled “Existing Land Use” and replace with revised Map 13 entitled “Existing Land Use”, as attached as Schedule M;
- (vi) Delete the existing Map 14 entitled “Policy Context” and replace with revised Map 14 entitled “Policy Context”, as attached as Schedule N;
- (vii) Delete the existing Map 15 entitled “Tri-Party Agreement” and replace with revised Map 15 entitled “Tri-Party Agreement”, as attached as Schedule O;
- (viii) In the first paragraph of section 7.0 entitled “Development Constraint Areas (Map 16)” delete “the Western Co-operative Fertilizers Limited’s fertilizer complex” and replace with “Hazardous Waste Management Facilities”;
- (ix) Delete the existing Map 16 entitled “Development Constraints” and replace with revised Map 16 entitled “Development Constraints”, as attached as Schedule P;
- (x) Under section 7.4.1 delete “Landfill and Sewage Lagoon Sites” and replace with “Landfill, Sewage Lagoon and Hazardous Waste Management Facility Sites”;
- (xi) Delete subsection 7.4.3 entitled “Phosphogypsum Pond” in its entirety; and
- (xii) Delete Section 8.0 entitled “Environmental Considerations” in its entirety.

2. This Bylaw comes into force on the date it is passed.

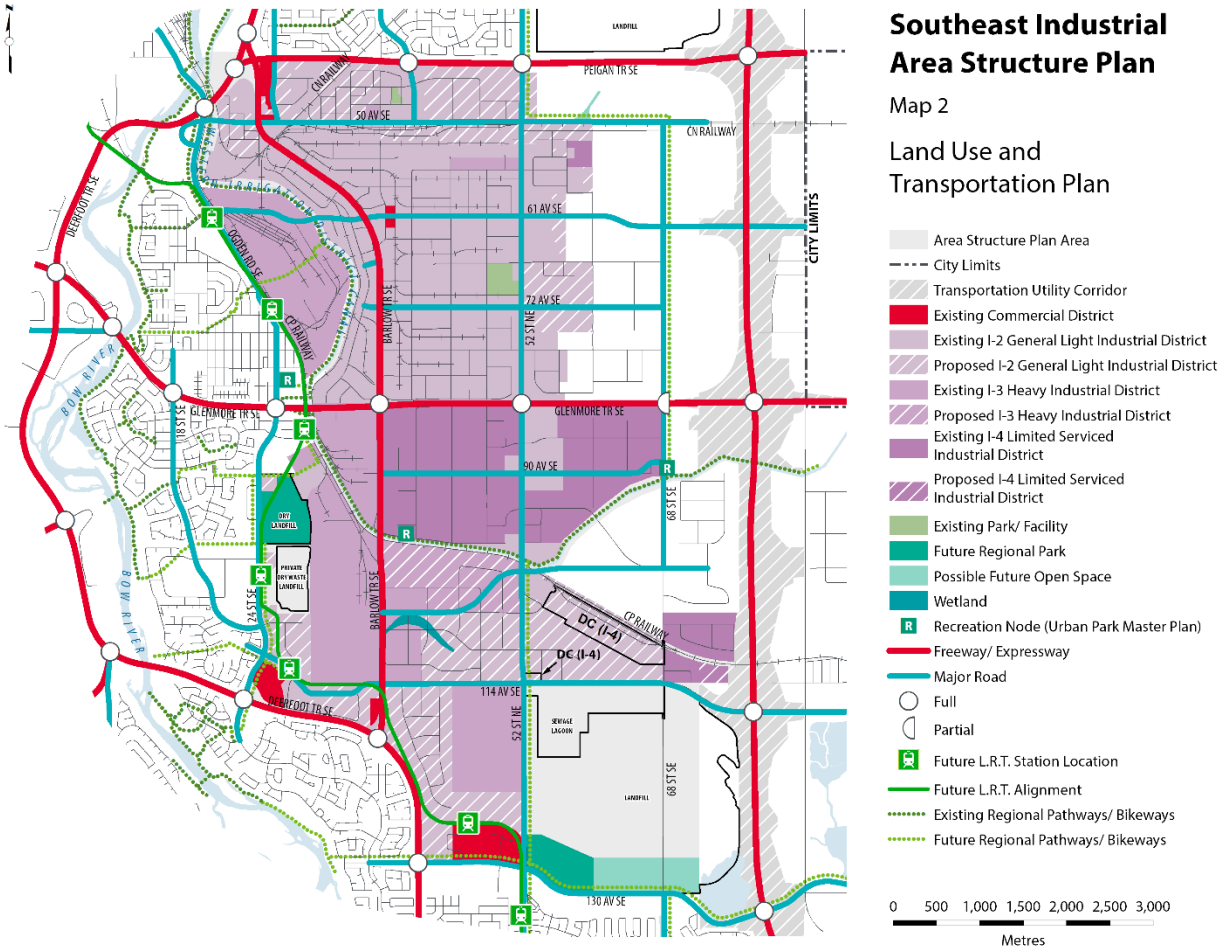
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule A



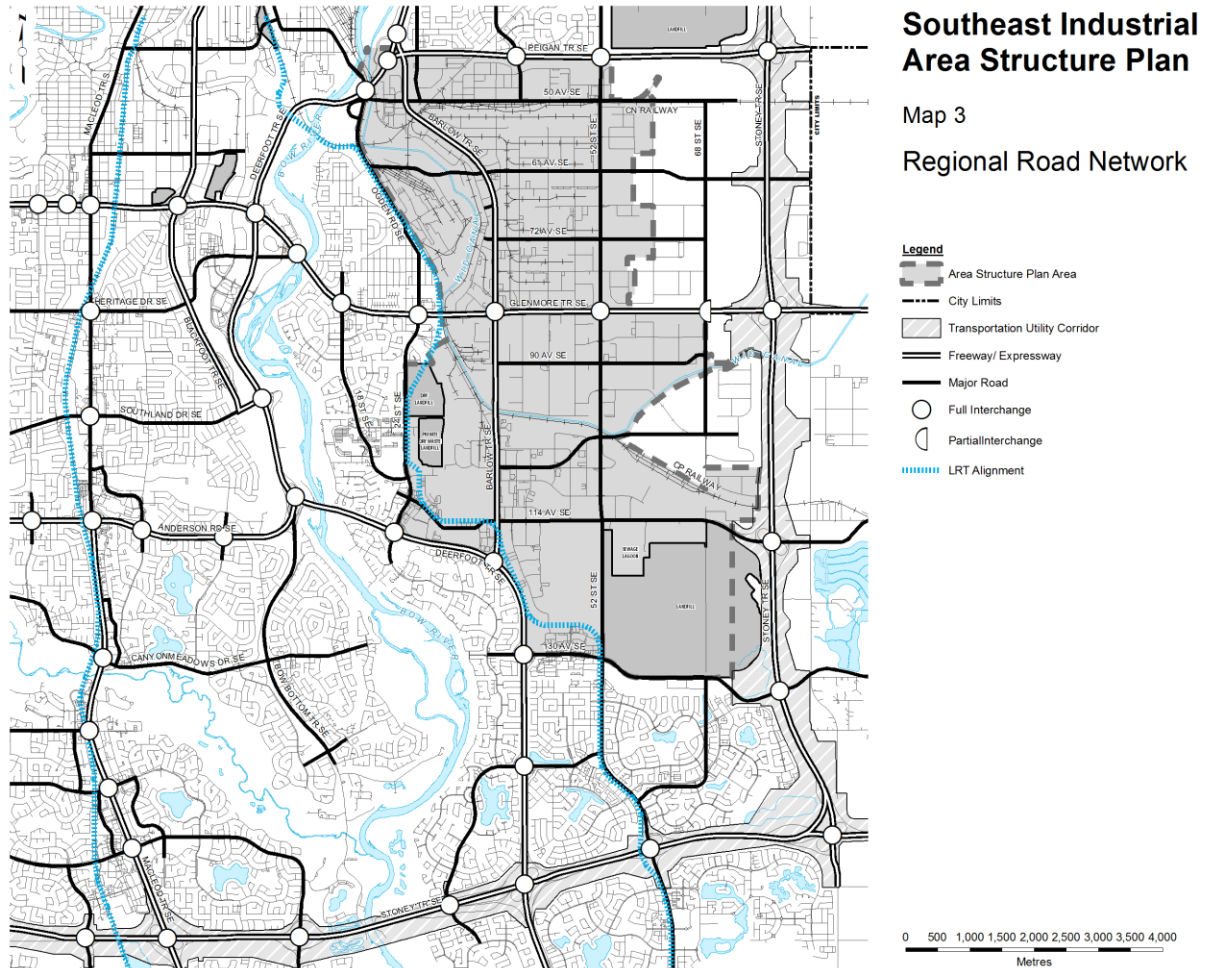
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule B



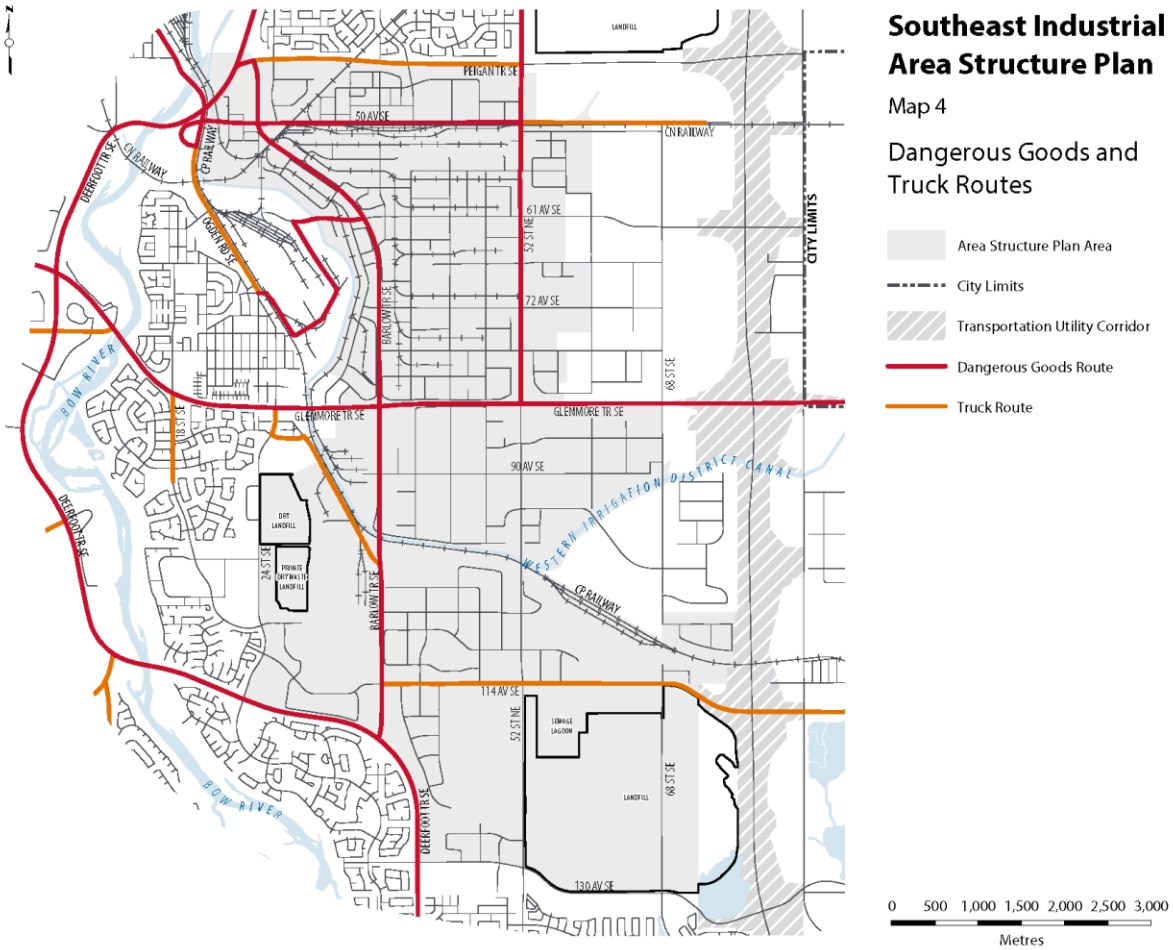
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule C



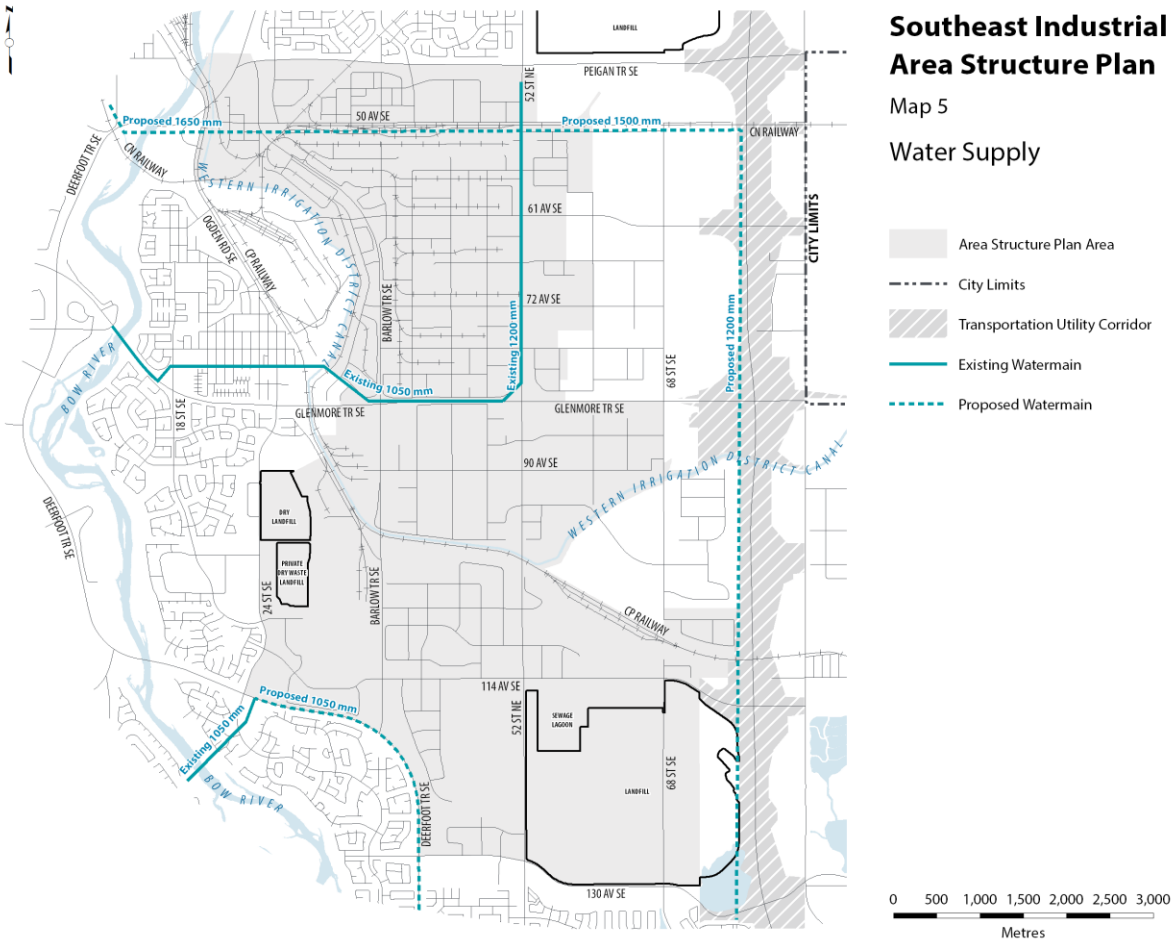
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule D



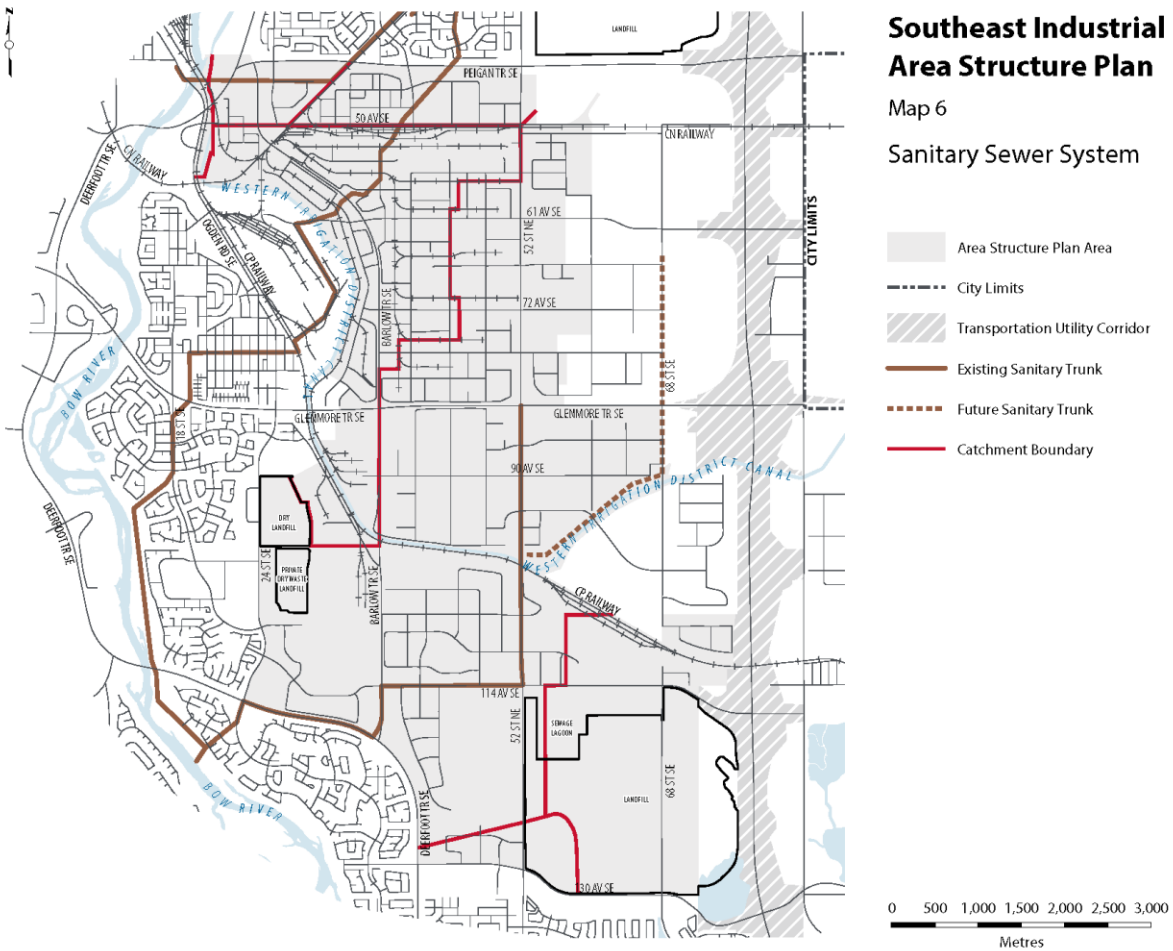
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule E



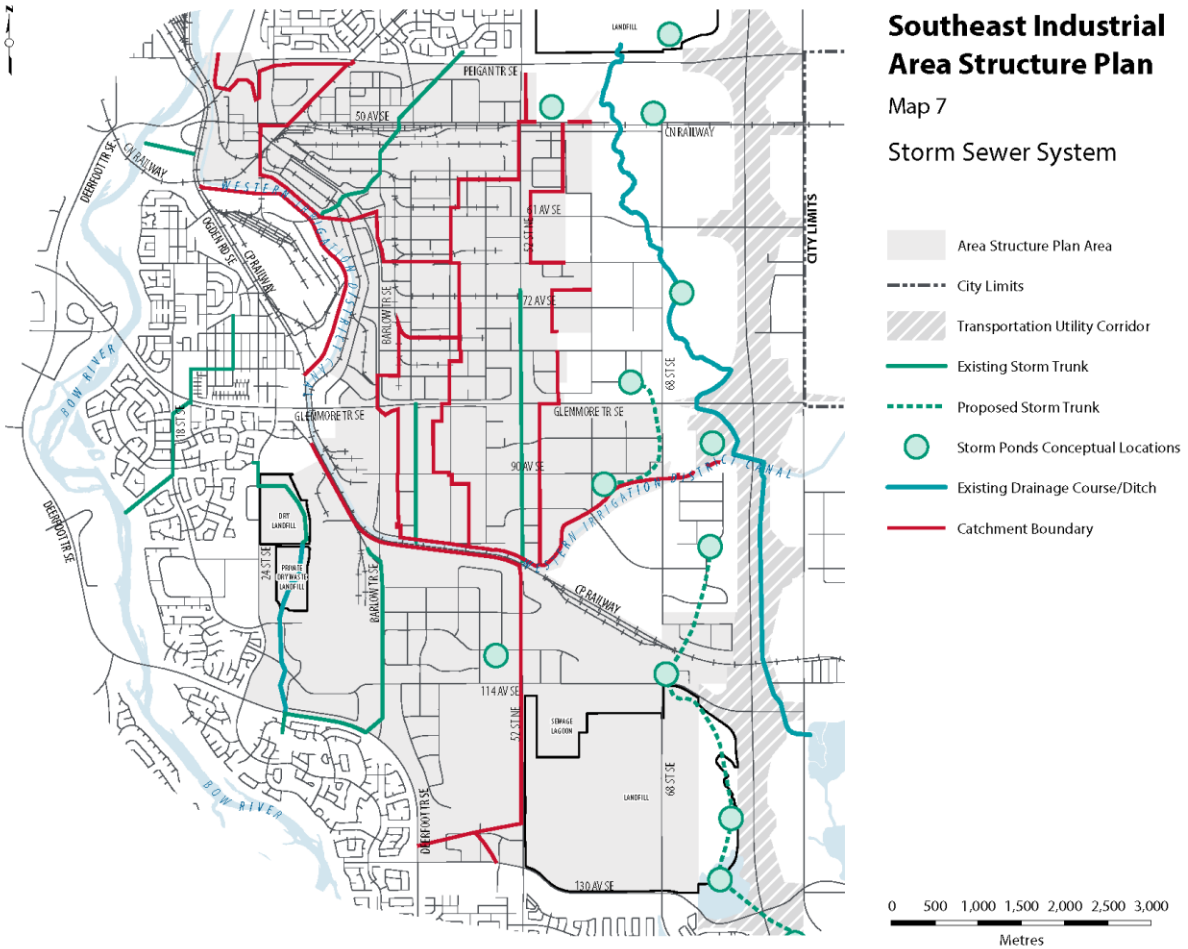
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule F



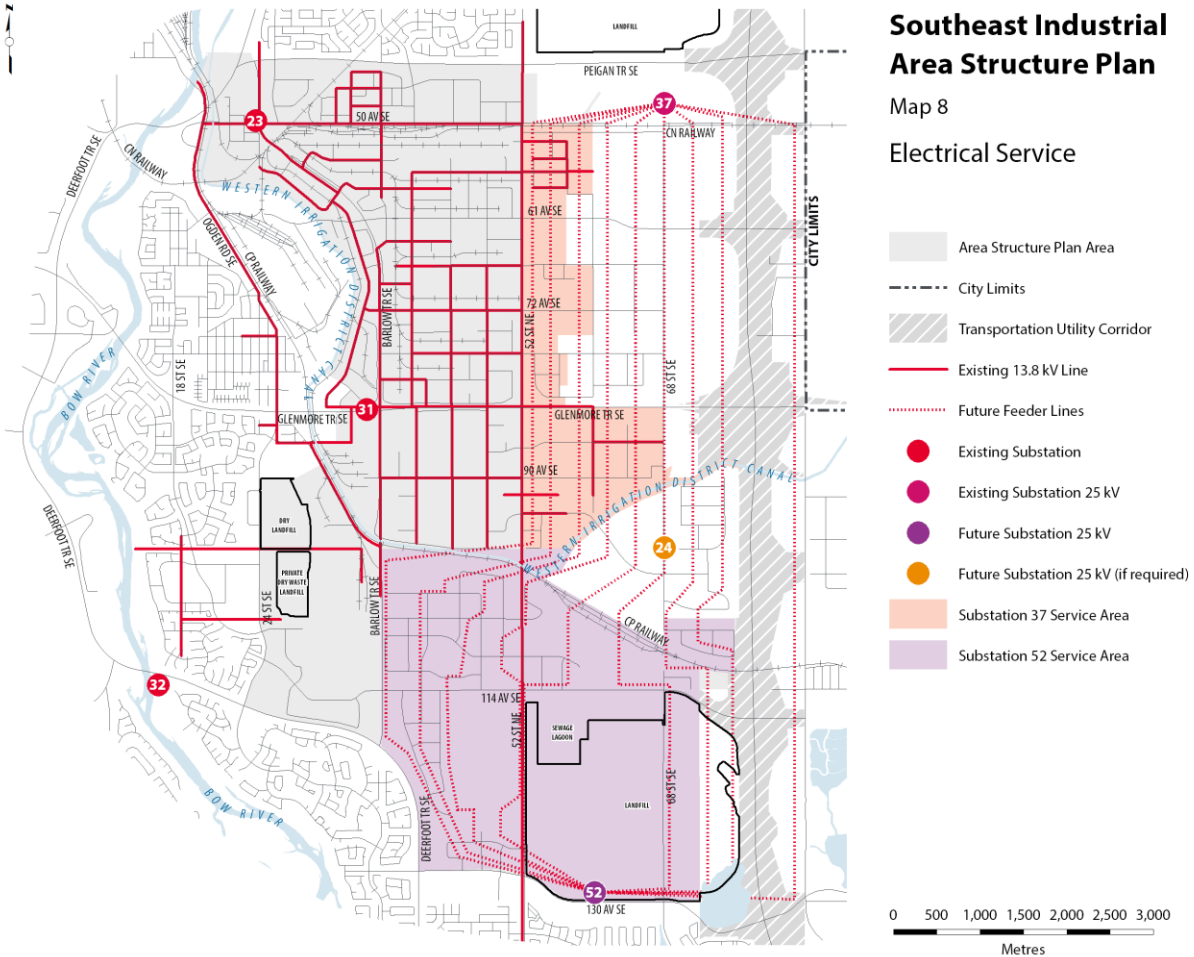
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule G



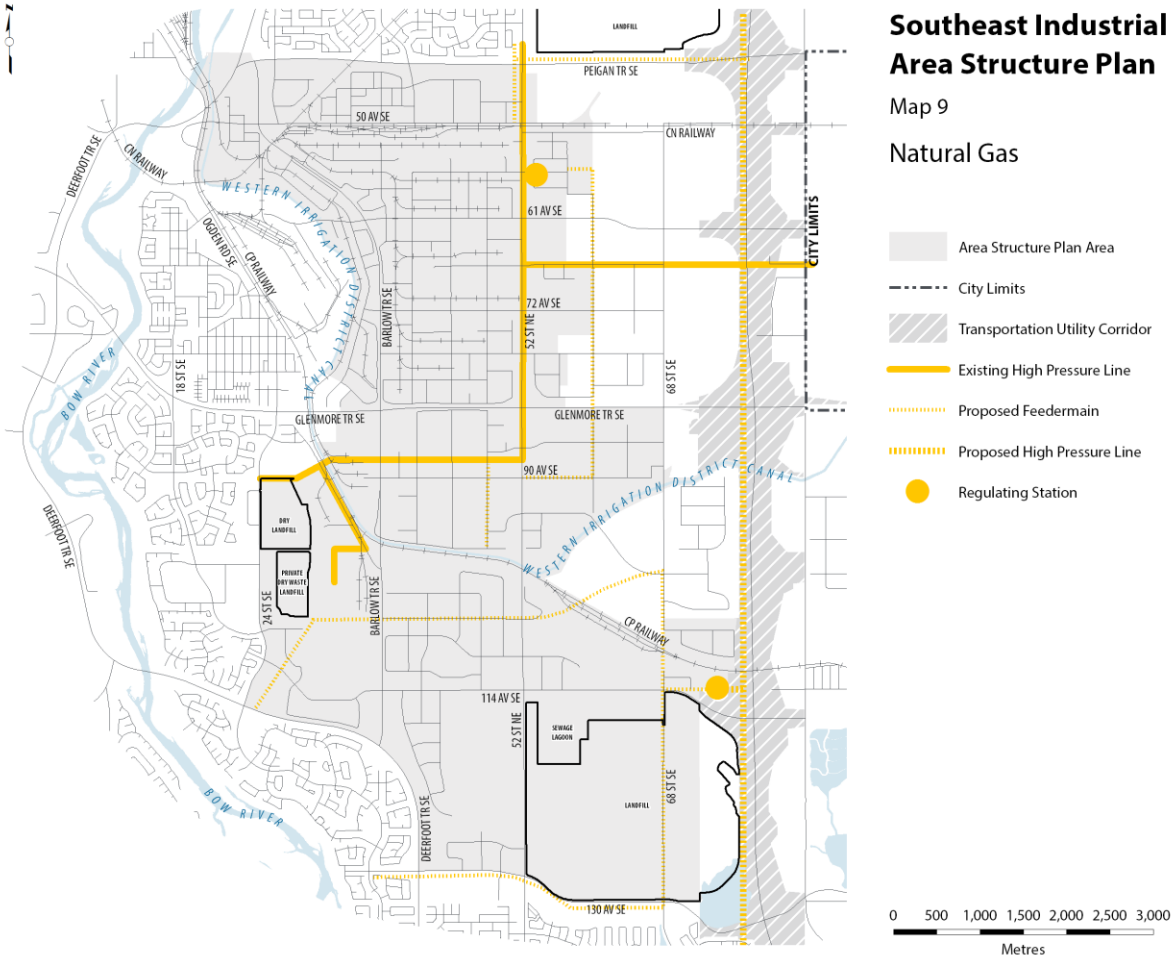
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule H



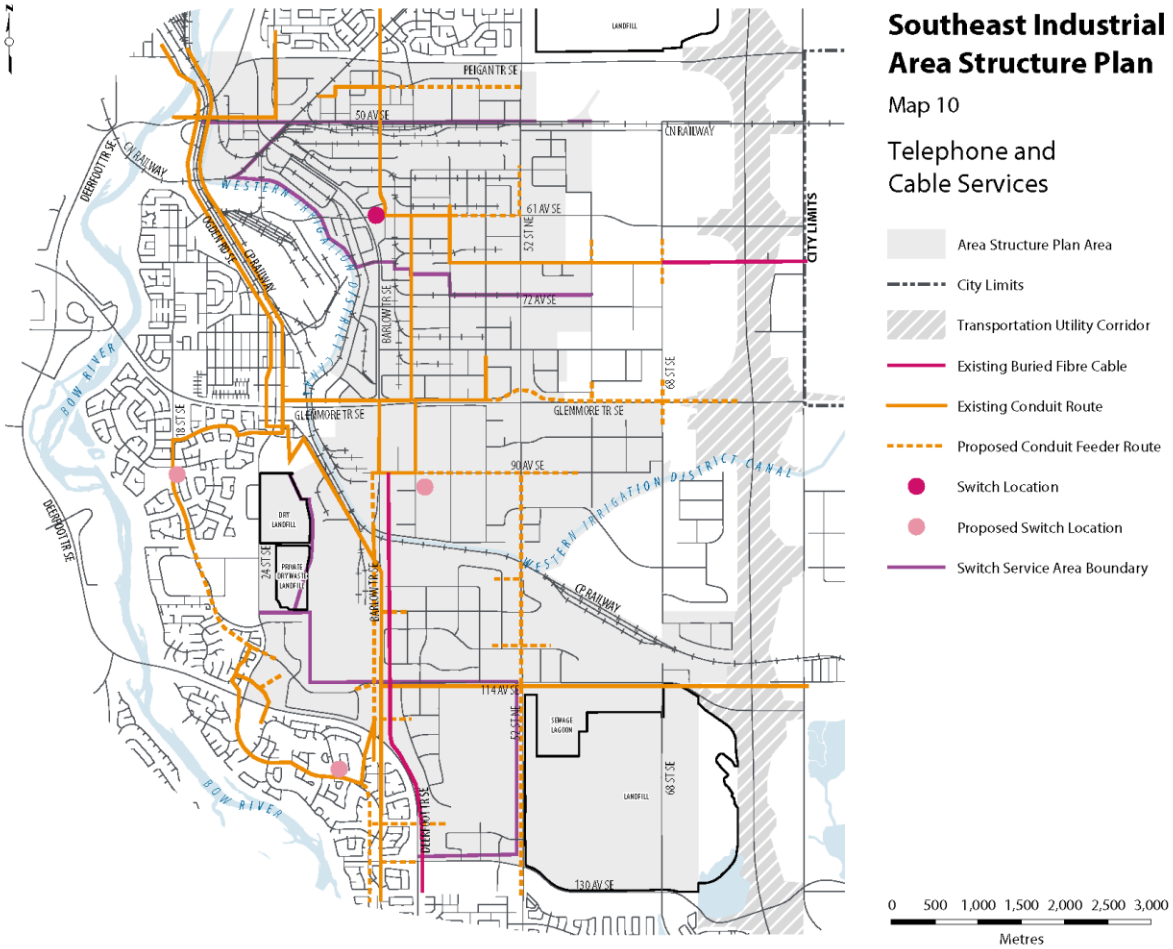
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule I



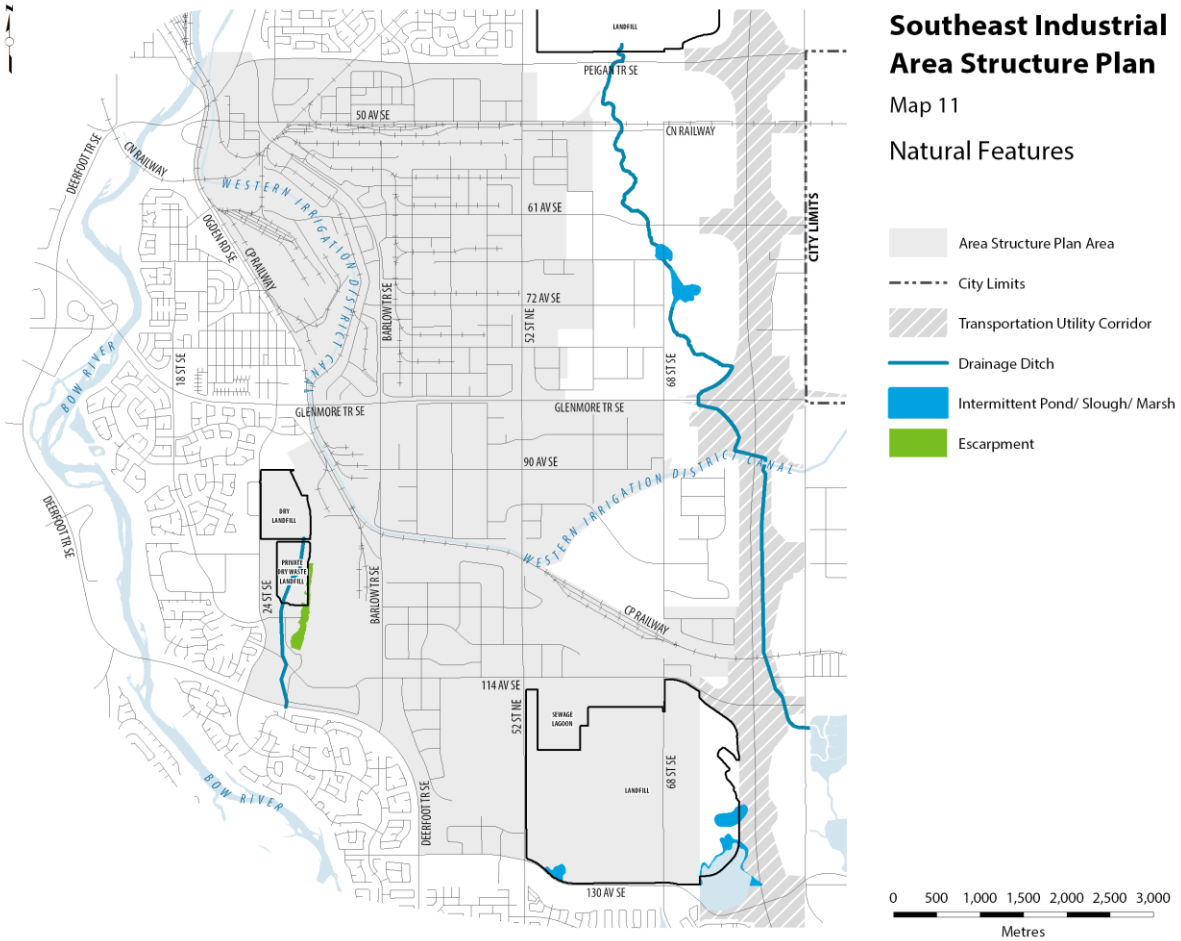
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule J



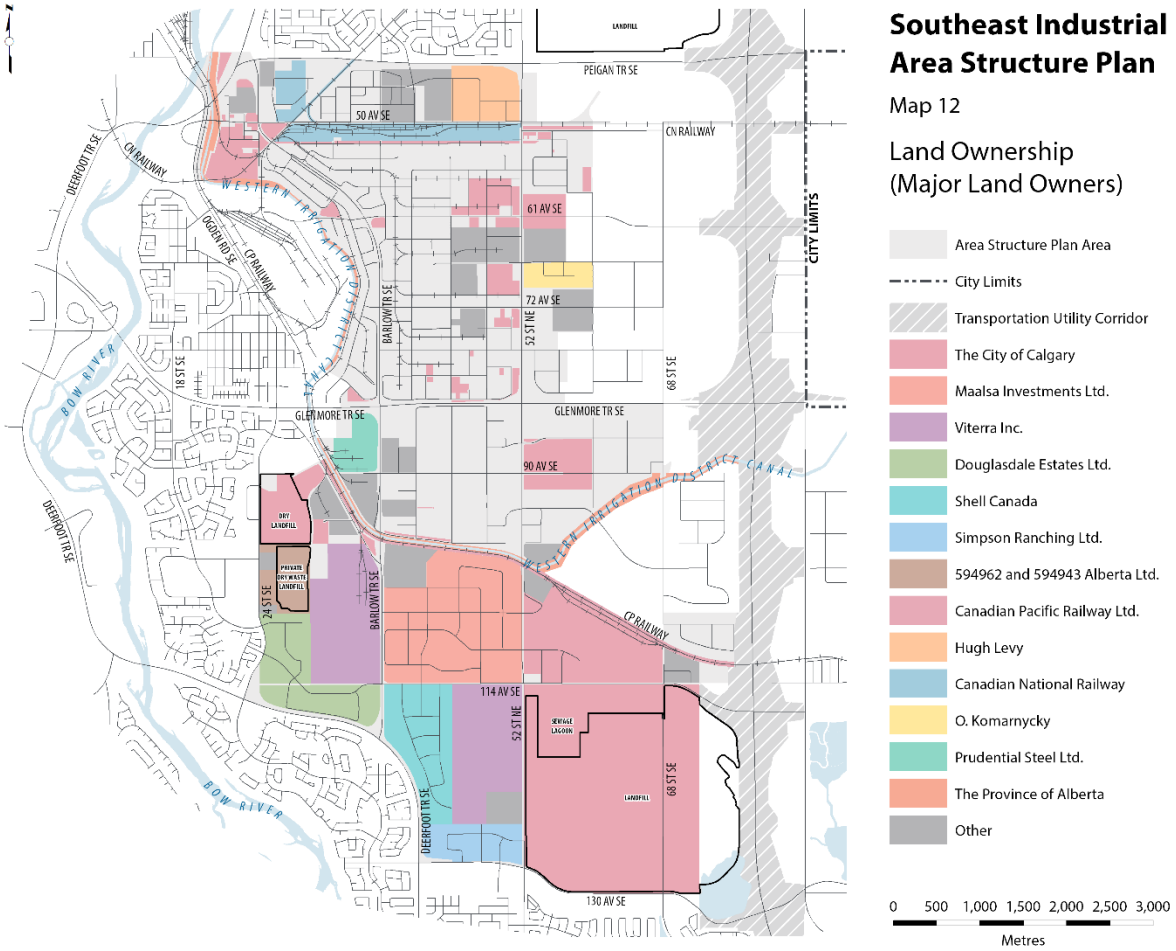
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule K



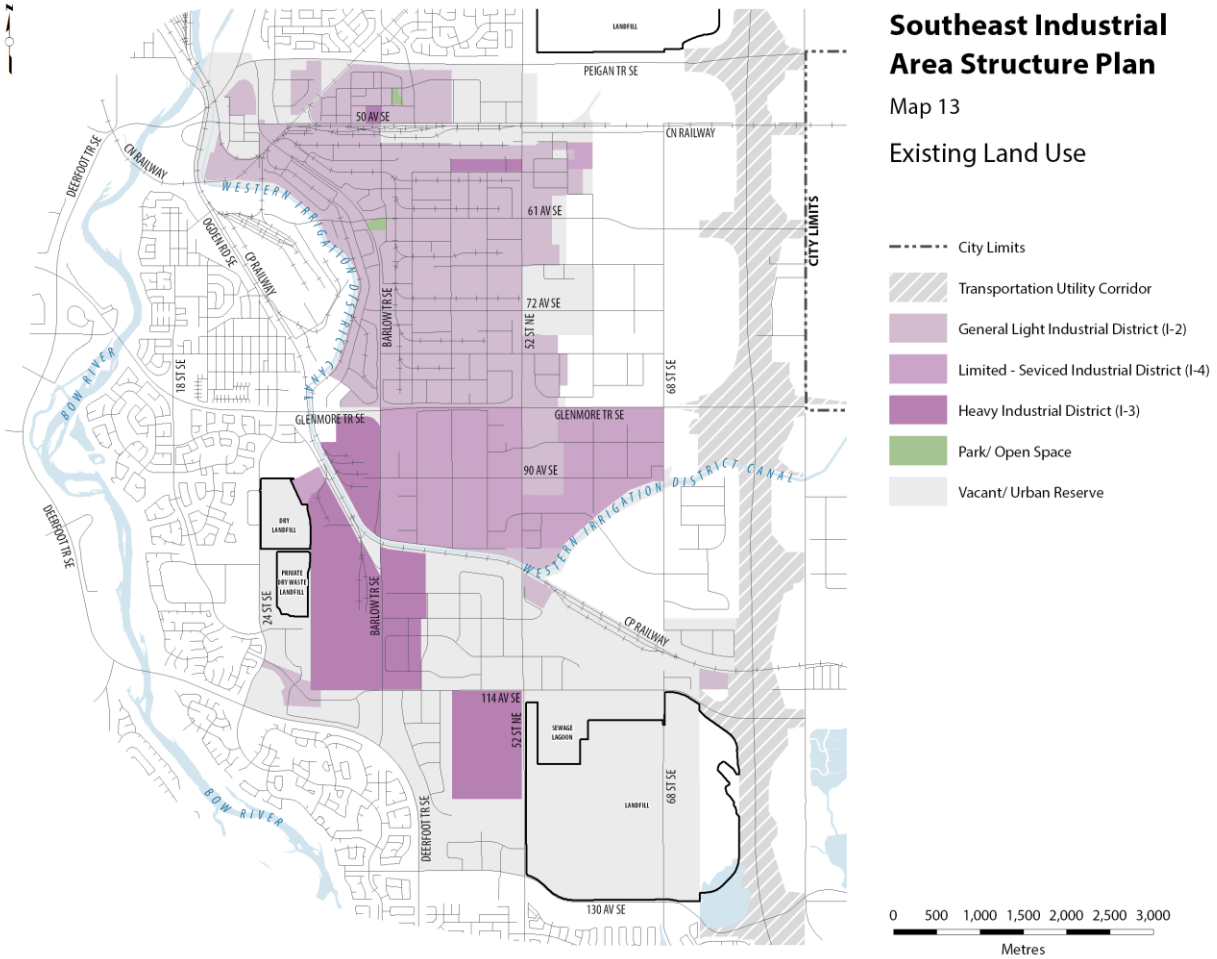
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule L



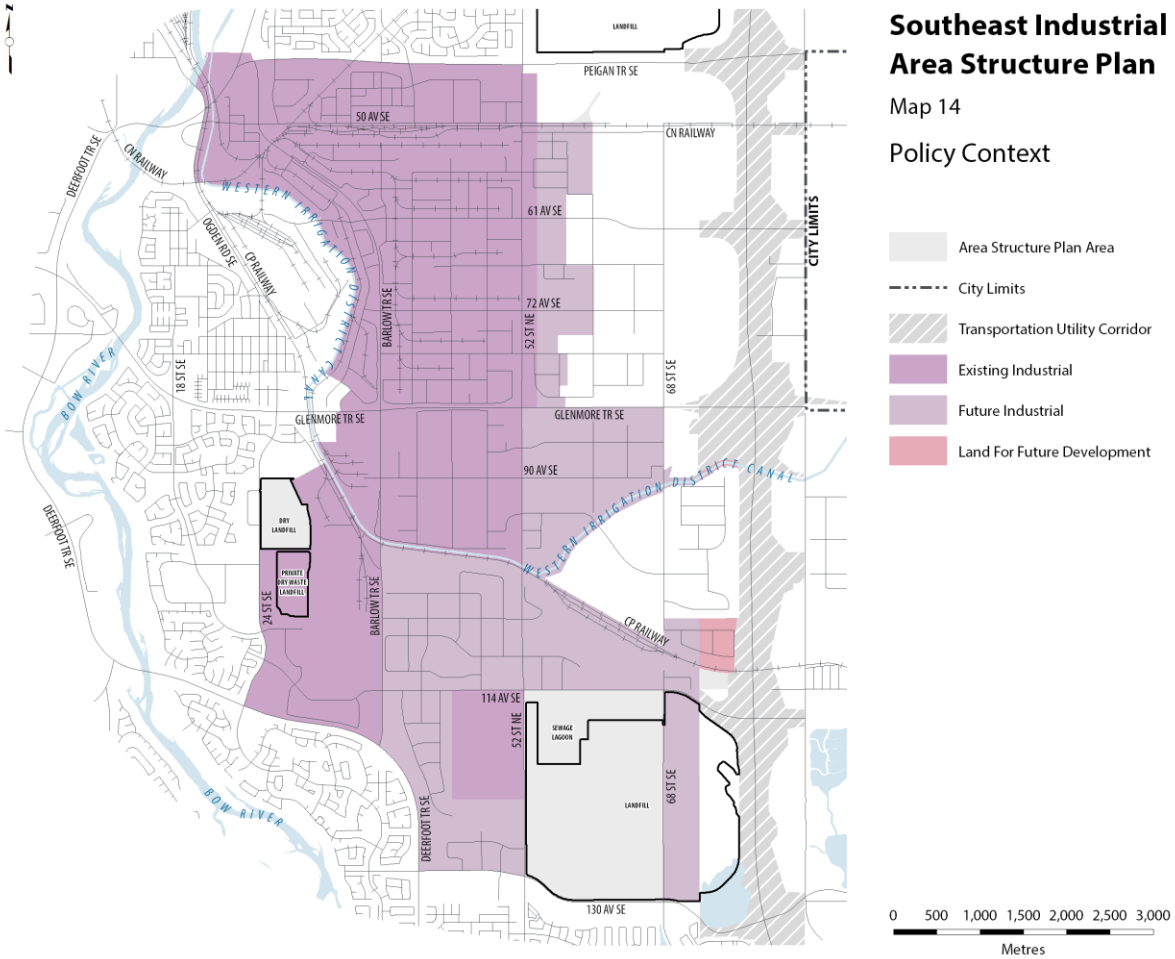
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule M



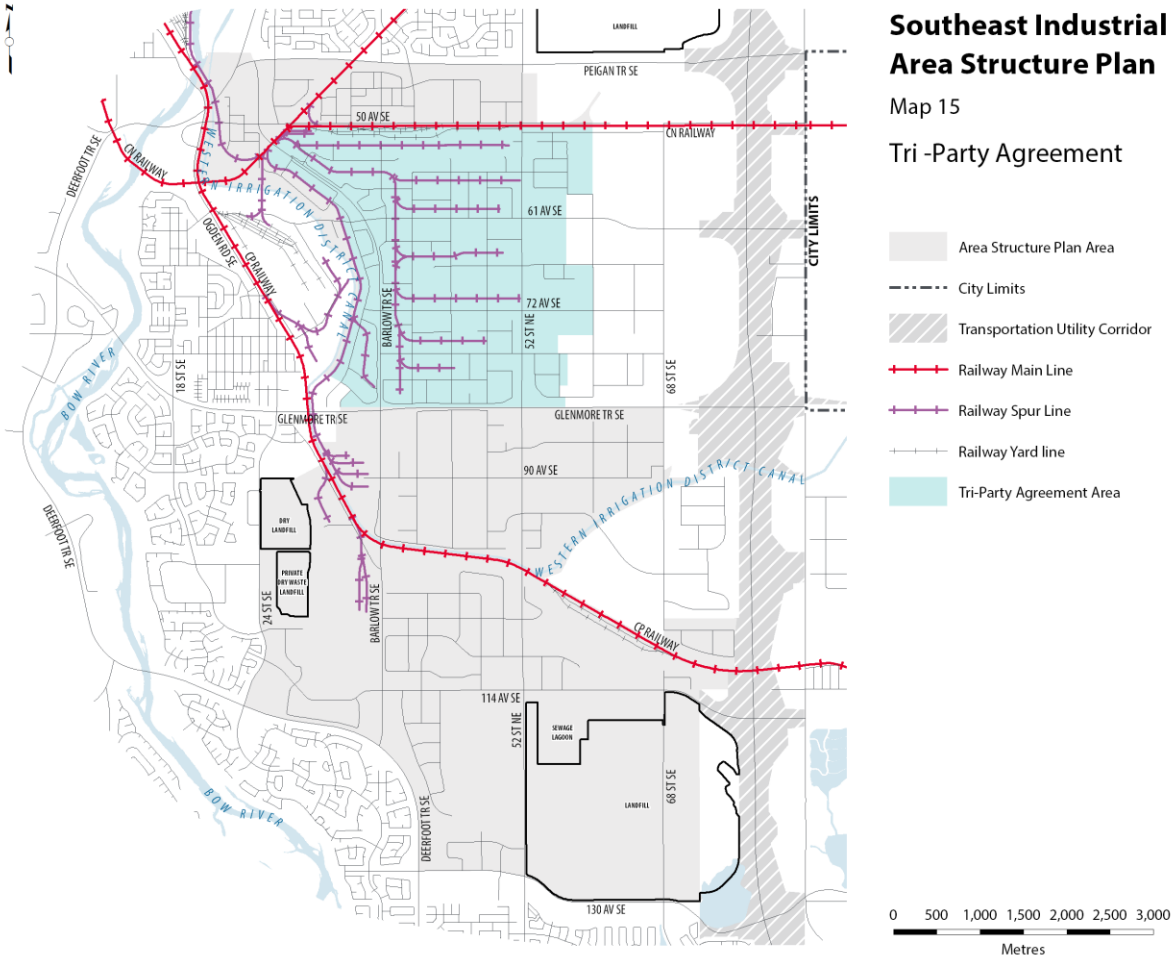
Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule N

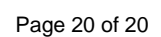


Proposed Amendments to the Southeast Industrial Area Structure Plan

Schedule O



Schedule P



Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0859
Page 1 of 6

**Land Use Amendment in Haysboro (Ward 11) at 9515 and 9527 Horton Road SW,
LOC2018-0081**

EXECUTIVE SUMMARY

This application was submitted on 2018 April 13 by Riddell Kurczaba Architecture on behalf of the landowner PBA Land Developments Ltd. This application proposes to change the designation of these parcels from Industrial – General (I-G) District to Industrial – Commercial (I-C) District to allow for:

- industrial developments with support for commercial uses (e.g. warehouse with commercial storefronts, restaurants, retail stores, industrial buildings with offices and retail stores);
- a maximum building height of 12 metres (a decrease from the current maximum of 16 metres; and
- the uses listed in the proposed I-C designation.

The proposal is in keeping with the applicable policies of the *Municipal Development Plan*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 1.23 hectares ± (3.03 acres ±) located at 9515 and 9527 Horton Road SW (Plan 5403JK, Block 12, Lots 8 and 9) from Industrial – General (I-G) District **to** Industrial – Commercial (I-C) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

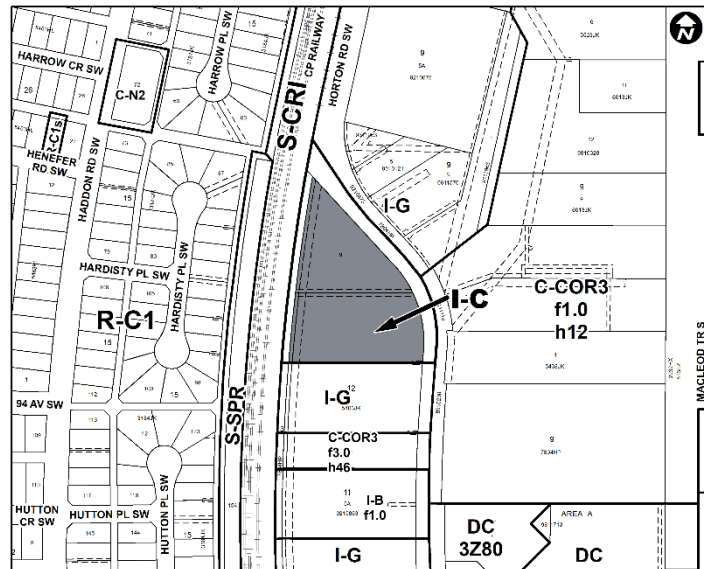
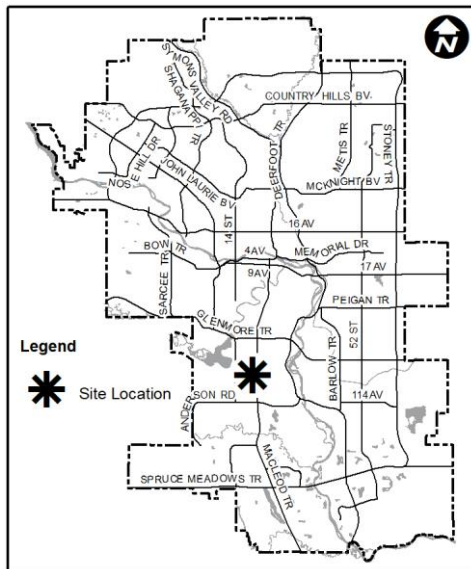
None.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0859
Page 2 of 6

Land Use Amendment in Haysboro (Ward 11) at 9515 and 9527 Horton Road SW,
LOC2018-0081

Location Maps



**Land Use Amendment in Haysboro (Ward 11) at 9515 and 9527 Horton Road SW,
LOC2018-0081**

Site Context

The subject properties are located within the community of Haysboro south of Heritage Drive SW and west of Macleod Trail SW.

Industrial – General (I-G) properties exist to the north and south of the subject sites. To the east the site is designated Commercial – Corridor 3 (C-COR 3) District. To the west, across the LRT and Canadian Pacific tracks, the area is designated Residential – Contextual One Dwelling (R-C1) District.

The subject properties have a total area of approximately 1.23 hectares \pm (3.03 acres \pm) in size. The properties are used as a temporary surface parking lot with approval until 2021.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal is to allow for a broad range of industrial and supportive commercial uses. The proposal meets the objectives of applicable policies noted in the Strategic Alignment section of this report.

Land Use

This application is for the redesignation of the sites from Industrial – General (I-G) District to Industrial – Commercial (I-C) District. The I-C District is intended for light industrial and small-scale commercial uses that are compatible with and compliment light industrial uses. The sites comply with this description. As such, the I-C designation is appropriate for this site.

Implementation

According to the applicant's submission, the intent of the application is to allow for a 2 to 3 storey office building with light industrial and supportive commercial uses. The applicant is seeking the redesignation to provide flexibility for future redevelopment. The I-C district would allow industrial uses and a range of supportive commercial uses.

An alternative to the I-C would be the Industrial – Business (I-B) District. The I-B district is intended for high quality, manufacturing, research and office developments and a limited range of small uses that provide services to offices and industrial uses within the immediate area. It should be noted that Horton Road has a mixture of industrial / commercial type uses that may not meet the intent of the I-B district, as such, the I-C district provides greater flexibility for the site.

The subject sites are located between two LRT stations. As such, the sites fall outside the requirements of transit orientated development.

There is no concurrent development permit application for this site.

Land Use Amendment in Haysboro (Ward 11) at 9515 and 9527 Horton Road SW, LOC2018-0081

Infrastructure

Transportation Networks

A Transportation Impact Assessment (TIA) and parking study may be required at the development permit stage. Horton Road SW is classified as a Collector Street. Access to the property is via Horton Road SW, at the development permit stage access location will be reviewed. The Southland LRT station is located approximately 850 metres walking distance to the south.

Utility and Servicing

Water, and sanitary sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time.

At the development permit stage, a sanitary servicing study will be required which may result in upgrades to the sanitary sewer system.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site for three weeks. Notification letters were sent to adjacent land owners and the application has been advertised to the public online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed to adjacent land owners. In addition, Commission's recommendation, the date of the public hearing and options for providing feedback will be advertised to the public.

The comments of the Haysboro Community Association are attached to this report. Briefly, the Community Association noted the following:

- Two to three storey office building proposed by applicant is not considered good urban design or in keeping with the *Municipal Development Plan*;
- Land use redesignation is an opportunity to create a public realm that can be used to connect the two sides of the community and achieve the goals of the MDP; and
- Need for quality development to make the community liveable, attractive, memorable and functional.

Administration considered the feedback provided by the Community Association that are relevant to the proposed redesignation and has determined that the proposal is appropriate. Urban design, public realm and the qualitative aspects of future development on this site will be reviewed at the development permit stage.

Engagement

No public meetings were held for this application.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0859
Page 5 of 6

Land Use Amendment in Haysboro (Ward 11) at 9515 and 9527 Horton Road SW, LOC2018-0081

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the “City, Town” area identified as Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use is consistent with the SSRP policies including the Land Patterns policies (subsection 8.14).

Municipal Development Plan (Statutory, 2009)

The subject site is located within the “Residential Developed – Established” area according to Urban Structure Map (Map 1 of the *Municipal Development Plan (MDP)*).

Policies are designed to encourage the modest redevelopment of existing residential areas. Development along Horton Road SW, however, is light industrial. As such, the I-C district for the subject sites would not be out of place in terms of development. Equally, it is the intent of the MDP to retain industrial lands where ever possible.

There is no local area plan for Haysboro.

Social, Environmental, Economic (External)

The proposal has the potential to continue to allow for and further support light industrial uses along Horton Road SW.

No environmental issues have been identified at this time.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investments and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0859
Page 6 of 6

**Land Use Amendment in Haysboro (Ward 11) at 9515 and 9527 Horton Road SW,
LOC2018-0081**

REASON(S) FOR RECOMMENDATION(S):

The proposed I-C District is compatible with the established character of the area. The proposal conforms to the applicable policies identified in the *Municipal Development Plan*.

ATTACHMENT(S)

1. Applicant's submission
2. Community Association letter

Applicant's Submission

0081

www.riddell.ca | www.rkvisual.ca | info@riddell.ca

John Riddell, Architect, AAA, AIBC, SAA, MRAIC
Ron Kurczaba, P.Eng.,
*senior corporation

April 13, 2018

The City of Calgary
Development and Building Approvals
800 Macleod Trail SE
Calgary, AB T2P 2M5

To Whom It May Concern:

Land Use Redesignation: 9515 and 9527 Horton Road SW

The purpose of this Land Use Redesignation application is to change the Land Use at 9515 and 9527 Horton Road SW (the 'Site') from I-G Industrial – General district to I-C Industrial – Commercial district. We are requesting this Land Use redesignation to allow planned future development on the site with a higher and better use than the I-G land use will allow. Presently the Site is vacant and primarily used for Surface Parking.

The intent for the Site is to develop a 2-3 storey office building and new light industrial and support commercial uses with associated parking. The I-C land use has an allowable FAR of 1 and a height of 12m which will result in a 2-3 storey building with up to 136,000 sq.ft. on the 3.14 acre site.

The Site is located between the communities of Fairview and Acadia to the east and Haysboro to the west, in the interface between the "Residential-Established" and Macleod Trail "Urban Main Street" policy areas of the Municipal Development Plan. The Site is also located on the west edge of the Macleod Trail SE 'Main Streets' study area which runs from Glenmore Trail to Southland Drive SE. Development opportunities for the Site under the proposed I-C district – as opposed to the existing I-G district – have the potential to increase the employment base in the community, provide additional local services, and provide a more appropriate transition between the residential character to the west and predominantly commercial character of the Macleod Trail Urban Main Street zone.

The MDP supports redevelopment in Standard Industrial areas given their proximity to existing communities and the Primary Transit network. (MDP 3.7.1) The Site is located between the neighbourhood of Haysboro and the retail section of Macleod Trail which forms part of the Main Streets study area; large multi-family residential developments can be found at Macleod Trail and Heritage Drive and Southland Drive. Transit stations are also located at Heritage Drive and Southland Drive; the Site is approximately 800m from each station, which places the Site outside the Southland and Heritage station planning areas/ TOD policy guideline areas.

Several sites along Horton Road within 100m to 300m of the Site have been redesignated to C-COR3. The C-COR3 site to the north of the subject Site has an FAR of 1 and a Maximum Building Height of 16m; the C-COR3 site to the east of the Site has an FAR of 1 and a Maximum Building Height of 12m; the C-COR3 site due south of the Site has an FAR of 3 and a Maximum Building Height of 46m. The Land Use districts on the west side of Macleod Trail are transitioning from Industrial General towards more Commercial uses as evidenced by the C-COR3 uses along Horton Road.

Horton Road currently supports several retail and residential developments between Heritage Drive and Southland Drive. Green space runs between the LRT and CPR tracks on the west side of Horton Road and a sidewalk exists along the eastern side of the road. Residents and employees are the predominant users of Horton Road as there are very few industrial uses on the west side of Macleod Trail

riddell kurczaba architecture engineering interior design ltd.

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p. 403.266.2100 f. 403.266.2170

Applicant's Submission

The IC land use will allow for a greater ability to provide employment on the site in the form of office and commercial / quasi-commercial uses. Proximity to multi-family residential and transit may encourage increased pedestrian movement along Horton Road to and from the Site.

We encourage your support for this Land Use Redesignation; the location of the site is suitable for redevelopment and is supported by the MDP and Main Streets policies.

If you have any questions, please feel free to call me at 403-685-8606.

Yours truly,
Riddell Kurczaba
Architecture Engineering Interior Design Ltd.



Erin Shilliday, AAA
Architect

Community Association Submission



Date: May 2, 2018

File Manager: Ian Bamford

Re: Application LOC2018-0081

Land Use Amendment: I-G to I-C for 9515 Horton Rd SW and 9527 Horton Road SW

Dear Corporate Planning Applications Group,

The Haysboro Planning Committee, a division of the Haysboro Community Association has received and reviewed *Application Notice LOC2018-0081* (the "Application"). The comments included in the Application have indicated the owner is requesting the Land Use re-designation "to allow planned future development on the site with a higher and better use than the I-G land use will allow". Although we agree that the I-C designation does allow for higher and better use development than the current Land Use, we believe the site could further benefit from a Land Use that better aligns with the goals of the current *Municipal Development Plan ("MDP")* adopted by Council in 2009, and also align with Haysboro's own professionally prepared community improvement plans. Specifically, we believe a C-COR2, C-COR3 or MU-2 would be a better alignment to the goals of the *MDP*.

Haysboro, a developed and established residential Calgary community does not have an active *Area Structure Plan*. As such, for planning and development matters we rely heavily on the current *MDP*. Additionally, in 2017 the Federation of Calgary Communities in partnership with the Faculty of Environmental Design at the University of Calgary completed a thorough analysis of Haysboro and have produced four improvement plans for our community. These were completed by 12 master's level planning professionals who consulted Haysboro businesses, community members, developers, the City of Calgary, a local planning and design firm, and the EVDS Faculty over a 1-year period. So although these improvement plans are not official area plans from the City of Calgary, we recognize that the work completed reflects all relevant stakeholders in Haysboro, good planning principles and practices, and current City of Calgary related policies and plans including: the *MDP*, the *Calgary Transportation Plan*, the *Complete Streets Policy and Guide*, *Low Density Residential Housing Guidelines for Established Communities*, *Contextual Dwelling Guidebook*, *Large Retail/Commercial Urban Design Guidelines*, *Main Streets* and the *Southwest Transit Plan*.

MDP Goals: Compact City, Great Communities, Good Urban Design, and Managing Growth and Change

The requestor has indicated that the "intent for the site is to develop 2-3 storey office building and new light industrial and support commercial uses with associated parking". This kind of development is reflective of other developments built from 1969-1978 along the Horton Road corridor, with the only exception being the under developed I-G commercial strip across from the property that was constructed in 1995. As shown in *Appendix I*, there has been a shift to C-COR in this corridor. We further note, that there are three very significant DC districts along this corridor and a MH-3 district. In relation to the *MDP* goal of "Managing growth and change", an I-C Land-Use re-designation in this corridor does not consider stimulating growth and change across the city for the next 50-60 years. Furthermore, the



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Website: www.haysboro.org

Community Association Submission



MDP goal of "Good urban design" should bring into question how this site collaborates and coordinates with the neighbouring area. Instead of trying to revive an outdated industrial build-out for this established and developed area, the site should look to create a public place that integrates with the evolving landscape in this corridor and surrounding area, which is multi-use. This is evidenced by the built-out London Towers development, the Southland Crossing LOC2015-0194, the recently approved Elbow/Southland re-designation CPC2018-095 and the future DC 89Z2006 district development on Heritage and Macleod.

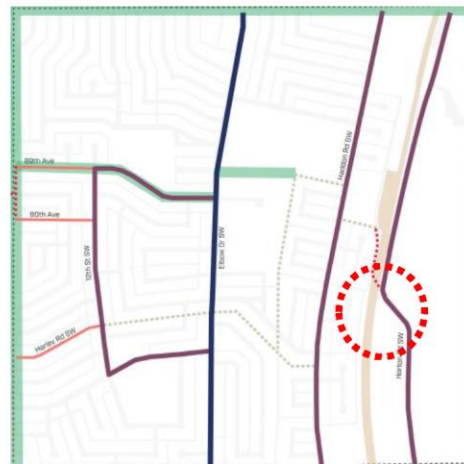
We should further ask, does approving a 3+ acre I-C district in one of the last-remaining undeveloped sites in an established neighbourhood encourage the *MDP* goals of creating a "Compact city" and "Great Communities"? From our community's perspective, we fail to see how another 2-3 storey office building helps us achieve a more complete community as we think about the long-term needs of our community.

MDP Goals: Good urban design, Connecting the city, and Greening the city

The Application acknowledges the close proximity to the Heritage and Southland LRT Stations, being located in the middle of both ~800m away. Even though this does not fall directly into TOD guidelines, it is important we consider it's location from the perspective of the *MDP* goal of "Connecting the city" and look at what implications a land-use may have on this. This is something we explored in our community improvement plans as shown below:



Subject Site



The red-dash indicates an ideal central area for pedestrian crossing

As it stands, there is disconnect between residents on the west and east sides of the tracks, as both are forced to the absolute edges of the community on Heritage and Southland in order to reach the other side. This site being located near the middle of these major roadways offers an opportunity to bridge this disconnect by providing connectivity as suggested by the *MDP* goal of "Connecting the city".



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Community Association Submission



In the short-term we believe this could be achieved by an at-grade crossing to the west of the site. In the long-term there is an opportunity for a pedestrian bridge (shown below).



Related to connectivity and this location is the opportunity to meet the MDP goal of "Greening the city". To the east of the site is an undeveloped strip of greenspace between the Horton and Macleod commercial developments (see next page). With this, there is an opportunity to create a linear park for the commercial district who have no green space (see Appendix II). To the west of the tracks exists a linear dog park / walking path, if a crossing were created next to the site, the two green spaces could be connected (see next page).



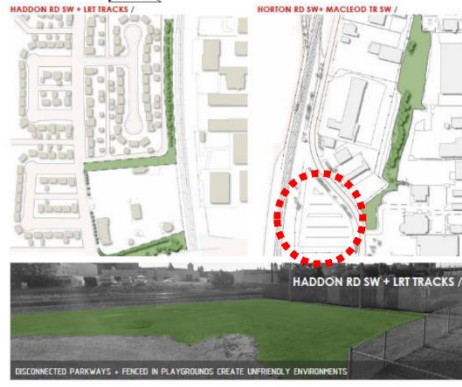
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Community Association Submission



Dark Green space shows existing undeveloped green space.



Existing green space on both sides of the tracks



Opportunity to link the community and green spaces



Connecting the city



Greening the city



Good urban design

Related MDP Goals



New Green Infrastructure + Plantings

New Pathways + Pedestrian Promenade

New Activation + Furniture



CREATE NEW RECREATIONAL OPPORTUNITIES

ADD LOW MAINTENANCE LANDSCAPING

REMOVE FENCING + EXISTING PATHWAY THROUGH UNDERPASS



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Community Association Submission



Related to "Good urban design", "Connecting the city" and "Greening the city", we must consider what is being proposed here; the requestor has indicated they are planning to develop a 2-3 storey office building here. So we must ask ourselves, is this good urban design? Is this conducive to fixing the broken pedestrian link between the tracks? How does it help connect and green the city? From the perspective of the community, we believe the proposed re-designation does not lend favour to any of this. We do not want to see a 50-60 year small-scale office development on a rare 3+ acre site in our established community. We envision stronger place making strategies through a mixed-use destination that can then extend to solve connectivity and greenspace goals.

Closing

Established communities such as Haysboro have few real opportunities to make impactful changes to the overall community. To us, the site in question for this Application is a rare opportunity to provide more than more employment opportunities to the residents of Haysboro given its context to the greater community and surrounding area, and its size at over 3 acres. We hope that the site owner and Planning Commission consider some of the ideas we have included when they think about this Application and site as it relates to great place-making. We see this site as an opportunity to create a public realm that can be used to connect the two sides of our community and achieve the goals of the MDP. In order to achieve this, a higher and better use than I-C can offer should be proposed.

In section 2.4 of the MDP the primary objective of Urban Design is given as follows:

"Make Calgary a liveable, attractive, memorable and functional city by recognizing its unique setting and dynamic urban character and creating a legacy of quality public and private developments for future generations."

This is something we of the community of Haysboro take to heart, we want quality private developments that make our community more liveable, attractive, memorable and functional. To simply treat this rare opportunity as a simple office development would be a disservice to our community and a failure to this primary objective of the Municipal Development Plan.

Thank you on behalf of the Haysboro Community Association,

Jonathan Hamel, MBA
Director-at-large, Planning Committee
Haysboro Community Association



Kourtney Branagan
President
Haysboro Community Association



Sonja Sahlen
Director Planning & Development
Haysboro Community Association



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Community Association Submission



Appendix I – Horton Road Land Use Districts



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Community Association Submission



Appendix II – Horton/Macleod Linear Park Proposal

MACLEOD LINEAR PARK.

EXISTING

A steep sloped unused space located between parcels bear the Macleod Trail commercial district. Due to its steep slope, this areas remains unused but the community. Near its south is a construction yard for storage utilized by a commercial business on Macleod.

PROPOSAL

The Macleod commercial district is an area that the community does not feel is within it. It also lacks any green space. Radix Consultants propose converting this space into a beautified green corridor with pathways and seating options. This linear park is aimed to become a destination that the community can visit. It can also be a place for respite for employees that work in the Macleod commercial district during lunch hours.

OBJECTIVES

OBJECTIVE 1 - Develop a green corridor with multi-use pathways that allows pedestrian and cyclists to safely enjoy all commercial amenities without having to go to Macleod Trail.

OBJECTIVE 2 - Create plenty of landscaping to create an impression that visitors are not within a big box commercial district.

OBJECTIVE 3 - Create seating areas for employees in

the region and community members to respite during lunch hours and breaks.

OBJECTIVE 4 - Create some paved areas with seating and ample lighting.

OBJECTIVE 5 - Plant plenty of trees surrounding the park to give the impression that one is not within a big box commercial district.



Current state of unused green space



Construction yard



Location of Macleod Linear Park



BEFORE



AFTER

1 INTERNAL PATHWAY SYSTEM



Multi-use pathways connecting commercial parcels district through park.

GUIDELINES

1. Pathways should connect to sidewalks at intersections and other pedestrian crossings.
2. Routes should be planned for scenic interest and/or ease of linkage between commercial parcels.
3. Where appropriate, routes may have sufficient lighting for safety and ease of visibility.
4. Trees shall be planted on either side of pathways where appropriate to provide a vegetative canopy.



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Community Association Submission



Appendix II (continued...)

1 LANDSCAPED OPEN SPACES



Calgary, Canada
Open areas to allow for programmable activities.

GUIDELINES

1. Open landscaped area without any obstruction other than plantings.
2. Temporary spaces may be created in the spring and summer months when people want to pursue certain activities.

2 AMPHITHEATRE STYLE LAWN SEATING



Les Jardins de l'Imaginaire, France
Terraced seating to capitalize on natural topography of park.

GUIDELINES

1. Landscaped seating options would embrace the topography of the land.
2. Seats shall be low maintenance with some paved paths and stairs for pedestrian access.
3. Trees shall be scattered through the seating spaces to allow for shade.

3 CREATIVELY PAVED AREAS OF RESPITE



Location Unknown
Creative seating areas with ample lighting for use in evenings.

GUIDELINES

1. Aim to create a unique and dynamic public space that does not follow traditional park design.
2. Use special pavement treatments for an aesthetically pleasing experience.
3. Areas with enough space may have flexible designs that can be redesigned throughout the year (e.g. food trucks, skating rink, ice skating rink).
4. Movable street furniture such as chairs, picnic tables, amphitheatre or stage may be placed to adapt to different programming needs.
5. Lighting must adhere to lighting guidelines and be plentiful for nighttime use.

4 SURROUNDED BY TREES



Location Unknown
Heavily surrounded by trees to block out sight of block like commercial buildings.

GUIDELINES

1. Dense trees shall surround the walls of all commercial parcels to ensure that sight of buildings from park is covered.
2. Trees shall be surrounded with landscaping such as grass to offer a natural vibe.



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Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0868
Page 1 of 7

Land Use Amendment in Inglewood (Ward 9) at 1302 – 11 Avenue SE, LOC2018-0112

EXECUTIVE SUMMARY

This land use amendment application was submitted on 2018 May 14 by Amrit Design Drafting Services, representing Veena and Rambir Mann. The application proposes to redesignate the subject parcel from Residential – Contextual One / Two Dwelling (R-C2) District to Mixed Use – General (MU-1f2.0h14) District to allow for:

- mixed-use development on Inglewood's 12 Street SE;
- a maximum floor area ratio (FAR) of 2.0;
- a maximum building height of 14.0 metres; and
- a broad range of uses, combined with flexibility regarding the location and sizes of uses to support a range of neighbourhood shops and services.

The proposal is in keeping with applicable policies of the *Municipal Development Plan* and the *Inglewood Area Redevelopment Plan*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw the proposed redesignation of 0.07 hectares \pm (0.18 acres \pm) located at 1302 – 11 Avenue SE (Plan A3, Block 5, Lots 35 and 36) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Mixed Use – General (MU-1f2.0h14) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

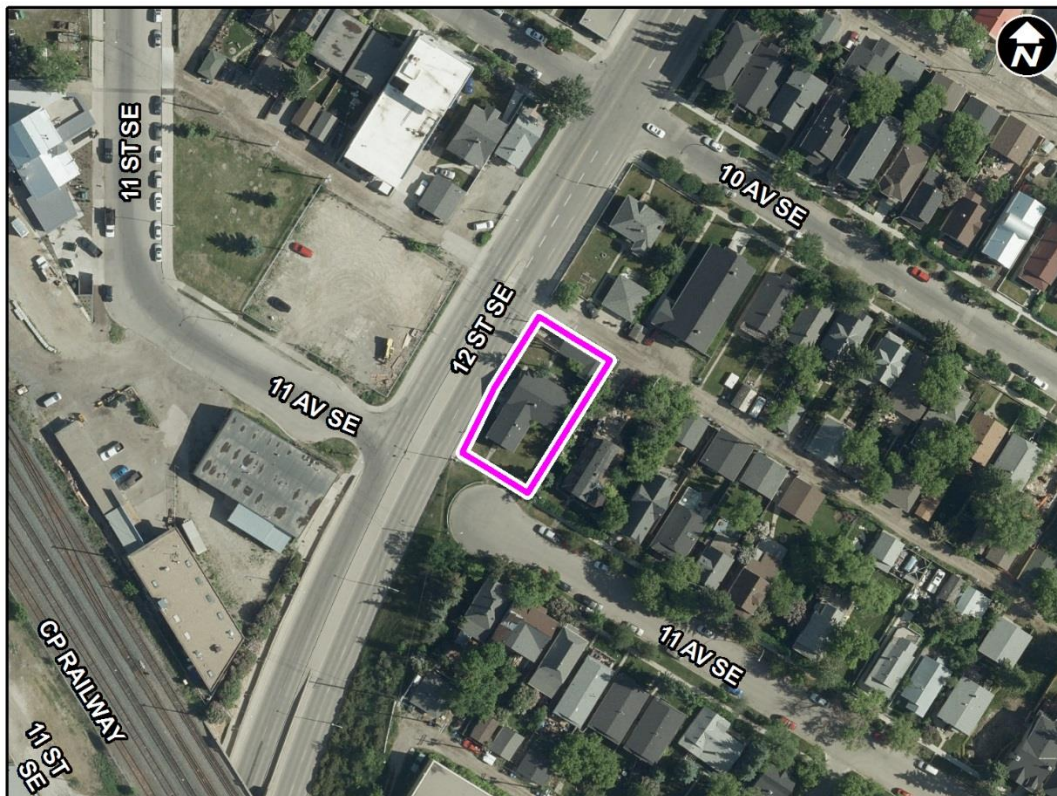
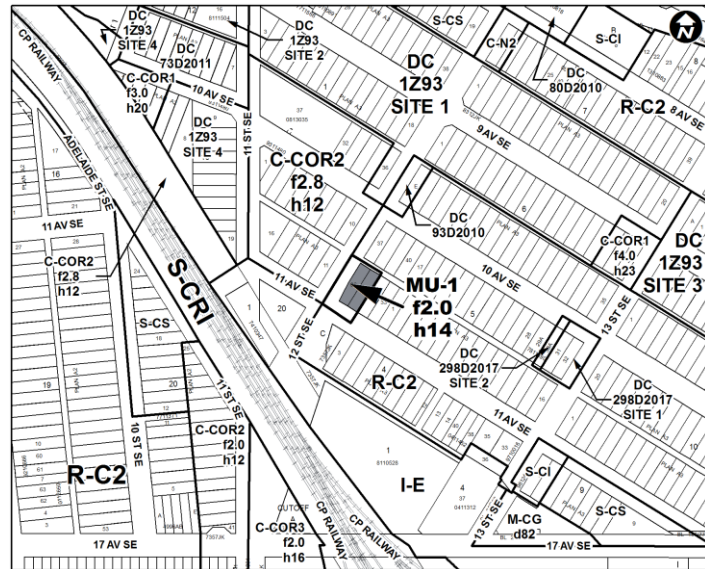
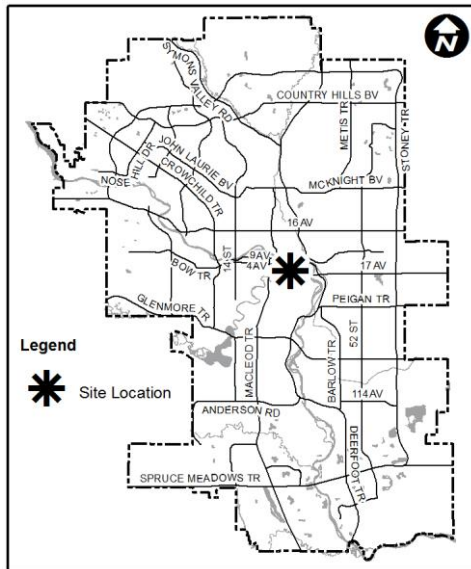
The applicant is contemplating the future use of Cannabis Store and is seeking the redesignation to provide flexibility for future redevelopment. At this time, the applicant is not considering demolition and redevelopment of the site, but rather the allowable uses within the land use district.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0868
Page 2 of 7

Land Use Amendment in Inglewood (Ward 9) at 1302 - 11 Avenue SE,
LOC2018-0112

Location Maps



Land Use Amendment in Inglewood (Ward 9) at 1302 - 11 Avenue SE, LOC2018-0112

Site Context

The subject site is located along the east side of 12 Street SE, just south of the historic main street and just north of the future Inglewood/Ramsay Green Line LRT Station. Vehicular access to the site is off of 11 Avenue SE and the lane to the north. The lands to the east of the site consist of existing low density residential development. Presently, the site comprises a single storey building that is being used for rental accommodations. On-site parking is accessed via the rear lane.

As identified in *Figure 1*, Inglewood's peak population was in 2017, reaching 4,008 residents.

Figure 1: Community Peak Population

Inglewood	
Peak Population Year	2017
Peak Population	4,008
2017 Current Population	4,008
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online through the Inglewood community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal is to allow for mixed-use development on Inglewood's 12 Street SE corridor. The proposal meets the objectives of applicable policies as discussed in the Strategic Alignment section of this report.

Land Use

This application is to redesignate the site from the existing Residential – Contextual One / Two Dwelling (R-C2) District designation to Mixed Use – General (MU-1f2.0h14) District. The proposed MU-1f2.0h14 District is intended to:

- mixed-use development on Inglewood's 12 Street SE;
- a maximum floor area ratio (FAR) of 2.0;
- a maximum building height of 14.0 metres; and
- a broad range of uses, combined with flexibility regarding the location and sizes of uses to support a range of neighbourhood shops and services.

Implementation

The applicant's submission indicates they are interested in using the existing building for cannabis retail use. On 2018 April 05, Council approved a new definition for cannabis retail in Land Use Bylaw 1P2007. This new use is called Cannabis Store and is proposed within specific

**Land Use Amendment in Inglewood (Ward 9) at 1302 - 11 Avenue SE,
LOC2018-0112**

land use districts, including the MU-1 District (to be confirmed at the 2018 July 23 public hearing of Council). A discretionary development permit is also required for change of use, subject to Council's decision on this land use redesignation.

Administration received the associated change of use development permit for Cannabis Store on 2018 April 26 (DP2018-1960). The overall development impact, use area, required parking and any other site planning consideration will be evaluated during the development permit review.

There is another site, 100 metres to the northeast of the subject site at 1309 – 9 Avenue SE, that has also applied for a land use amendment and development permit to facilitate a Cannabis Store (LOC2018-0054, DP2018-1739). The land use amendment for that site is scheduled to be decided at the 2018 July 23 public hearing of Council. As that site is within 300 metres of the subject site, only one of the two may be approved for a Cannabis Store. Development permit applications for Cannabis Stores are processed in the order in which they are received. Decisions on development permits for Cannabis Stores are scheduled to be made starting approximately 2018 July 30 (allowing for operations to commence 2018 October 17, the date set for Federal legalization of recreational cannabis). What this means is that the other site may already be approved for a Cannabis Store by the time this application is presented to Calgary Planning Commission. The applicant has been informed and wishes to proceed because the MU-1 District allows for a greater range of potential uses.

Infrastructure

Transportation Networks

A transportation impact assessment (TIA) and parking study were not required as part of this land use amendment. At the development permit stage, access is anticipated to be from the lane and/or 11 Avenue SE to protect the pedestrian realm along 12 Street SE. As 12 Street SE is a four-lane divided arterial, no direct access should be allowed onto an arterial. The parcel is also located within 150 metres of the future Inglewood / Ramsay LRT station along the Green Line alignment.

Utilities and Servicing

Water, sanitary, and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application has been advertised online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Land Use Amendment in Inglewood (Ward 9) at 1302 - 11 Avenue SE, LOC2018-0112

The Inglewood Community Association indicated general support for the proposed land use redesignation in a letter received on 2018 June 12 (Attachment 2).

Administration received six letters of opposition representing eight individuals. The main concerns raised in the public submissions were the following:

- **Traffic:** There were concerns expressed about site access and impacts to traffic flows on adjacent streets. Parking on adjacent streets was also identified as an issue.
- **Height:** There were concerns about the height allowed by the proposed district given the low-density context of the site. The applicant reduced the height proposed from 20 metres to 14 metres to address this concern.
- **Commercial Use:** There were concerns about having a retail store at the end of a residential block.
- **Area Character:** There were concerns about the loss of the historic character of the community through higher-density, commercial redevelopment.

Engagement

No public meetings were held by the applicant or Administration.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The site is located within the 'Developed – Inner City' area as identified on Map 1: Urban Structure Map in the *Municipal Development Plan (MDP)*. Policies for this area encourage redevelopment to maintain and expand local commercial development that provides retail and service uses in close proximity to residents, especially in the highest density locations. The subject site is located within a 150 metre radius of the future Inglewood / Ramsay LRT Station along the Green Line alignment. The proposed land use amendment application is to allow for the site to develop with higher intensity mixed-use development that is transit-oriented and is in keeping with MDP policies.

Inglewood Area Redevelopment Plan (Statutory, 1993)

The existing *Inglewood Area Redevelopment Plan (ARP)* identifies the site as located within the 'Residential Area' on the Generalized Land Use – Future Map (Map 6). The boundary lines between policy areas on the map are vague and the site is at the intersection between commercial, residential and industrial areas. The overall objective of the residential policies is

**Land Use Amendment in Inglewood (Ward 9) at 1302 - 11 Avenue SE,
LOC2018-0112**

to support population increases so that local services can be maintained. While a redesignation to a mixed-use district is not explicitly endorsed by the ARP, there is no policy that prohibits this land use amendment and Administration believes that this fits the spirit and intent of the ARP. Furthermore, with the future Inglewood / Ramsay Green Line LRT station approximately 150 metres to the south of the site, this land use amendment is considered to align with transit-oriented development objectives.

It should be noted that there are several other City projects underway in Inglewood, including a new ARP for the community of Inglewood and Ramsay, and the 9 Avenue SE Streetscape Master Plan. The new ARP will account for the new Green Line LRT station. Draft policies and land uses are being developed and reviewed. Even though the proposal was assessed under the current Inglewood ARP, it is anticipated the proposed land use district will comply with the new ARP policies.

Social, Environmental, Economic (External)

The recommended land use district will help to implement policy goals of providing more compact, complete communities with a diversity of housing and a variety of shops and services that meet daily needs. The district will help promote high quality walking environments near a future LRT station, which will serve as a focal point for community activity. The district also allows for a broad range of uses, supporting a variety of neighbourhood shops and services and increasing the market viability of the commercial space.

No environmental issues have been identified at this time.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0868
Page 7 of 7

**Land Use Amendment in Inglewood (Ward 9) at 1302 - 11 Avenue SE,
LOC2018-0112**

REASON(S) FOR RECOMMENDATION(S):

The proposal is consistent with the applicable policies of the *Municipal Development Plan* (MDP) and the *Inglewood Area Redevelopment Plan* (ARP). The site is located within close proximity to the future Inglewood / Ramsay Green Line LRT station and the 9 Avenue SE Neighbourhood Main Street.

ATTACHMENT(S)

1. Applicant's Submission
2. Community Association Letter

Applicant's Submission



Land Use Redesignation Applicant's Submission

Not Including Secondary Suites

PL 1263 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this form to ensure it will fit the space requirements of the report. Supplementary information can be provided separately in your application if required.

The owner of this residential building 1302-11 Avenue, S.E. Calgary would like to Rezoning to mixed use & the property to be used as CANNABIS RETAIL BUSINESS on both the houses of this property. The existing floor plans and elevations are attached to this application. It is requested to approve this application as soon as possible so that the owner can be able to use this building for the mixed use purpose in future.
APPLICANT.

Thanks,

S. Jaswal 25/04/2018

SUKHDEV JASWAL
AMRIT DESIGN DRAFTING
SERVICES
417-4774- WESTWINDS DR
N.E. CALGARY

Inglewood Community Association Letter



INGLEWOOD COMMUNITY ASSOCIATION
1740 24TH AVE SE
CALGARY, ALBERTA
T2G 1P9
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EMAIL: info@icacalgary.com

June 12, 2018

Development Circulation Controller
Development and Building Approvals #8201
Box 2100, Station M
Calgary, Alberta
T2P 2M5

Dear Chris Wolfe:

Re: LOC2018-0112, 1302 11 Avenue SE

The Planning Committee (PC) has reviewed the application regarding Rezoning from R-C2 to MU-ih20 at the above location. The existing building is in need of an upgrade and a transition bridging from commercial zoning on the west side of 12 Street is supportable. However, it needs to be done in such a way as to not reduce the residential quality of life as collateral damage. As such, the height proposed is excessive – a more context appropriate height of perhaps 14 m. is recommended with density adjusted to correspond. It is also noted that the site access is quite compromised with no access from 12 Street and minimal parking which likely would make development of a larger building difficult.

A motion to support the permit with those specific amendments was proposed by the PC and was passed at the June 11 general meeting.

I would appreciate receiving a written copy of the formal decision on the permit. If you have any questions, please call me at 403-263-4896.

Yours very truly,

INGLEWOOD COMMUNITY ASSOCIATION
Planning Committee



L.J. Robertson, Chair

Enabling Successful Infill Development

EXECUTIVE SUMMARY

Over the past ten years, Administration has undertaken a number of monitoring programs for residential infill development producing various amendments to the rules of the Land Use Bylaw (the Bylaw). On 2017 April 24, Council directed Administration to create a scoping report regarding 11 recurring items and on 2017 December 18, Council approved the direction of the scoping report, for a two-phased approach, and added a twelfth item to the list.

This report proposes Land Use Bylaw amendments to resolve the items identified for Phase 1, as well as proposes a recommendation for next steps as a result of the review of the remaining items of Phase 2. Phase 2 was originally scheduled to return to Council in 2018, Q4, however, based on analysis and engagement over the past seven months, Administration is prepared to make a recommendation on Phase 2 through this report.

Phase 1 addresses those items that can be resolved through technical Bylaw amendments - front porches and subterranean structures. The proposed Bylaw amendments are in Attachment 1. They accomplish the following outcomes:

1. Front Porches: One concern was the lack of front porches for existing and new infill homes. Administration is proposing encouraging front porches by allowing them to project 1.8 metres into a required front setback and exempting them from parcel coverage.
2. Subterranean Development: The second concern was the impact on soft vegetation, construction activities and foundation impacts (on the adjacent lot), and drainage. While the Land Use Bylaw is not the appropriate place to address drainage concerns or construction activities, Administration is proposing restrictions and allowances for subterranean developments that would help maintain soft vegetation in the front of the property.

The results of the review of the Phase 2 items showed that the remaining items cannot be addressed through individual Bylaw amendments for a number of reasons (see Attachment 2). Rather, a more comprehensive approach is required regarding the rules and districts related to infill development. This will ensure the rules are better aligned with the policies of the Developed Areas Guidebook, ensuring they can better support the policies to achieve desired outcomes.

Administration is proposing to report back in 2019 Q4, with a suggested outline of changes to the infill districts of the Land Use Bylaw. This work will outline how to align policy, guidelines, and Bylaw regulations to better support the evolution of Calgary's neighbourhoods.

Enabling Successful Infill Development

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to Land Use Bylaw 1P2007, (Attachment 1);
2. Give three readings to the proposed bylaw; and
3. Direct Administration to return with an implementation plan outlining options for changes to the Land Use Bylaw 1P2007 through the Standing Policy Committee on Planning and Urban Development no later than Q4 2019.

PREVIOUS COUNCIL DIRECTION / POLICY

On 2017 December 18, Council adopted the recommendations contained in PUD2017-1125 and added another item, building setbacks, which will be included for Phase 2:

“That Council:

1. Direct Administration to report back to the Calgary Planning Commission no later than Q3 2018, with Land Use Bylaw amendments to address issues identified in Phase 1; and
2. Direct Administration to report back to the SPC on Planning and Urban Development no later than Q4 2018, with results from Phase 2.”

On 2017 April 24 (report PUD2017-0313: Monitoring Report on Contextual Single and Semi-detached Dwellings) Council directed Administration to develop a scoping report on whether to move forward with Land Use Bylaw 1P2007 (LUB) amendments for 11 recurring and emerging issues related to infill development and report back by 2017 December. The 11 items raised by Council were:

- Eave and Peak Height Differentiation;
- Massing;
- Front porches;
- Subterranean structures extending beyond above-grade footprints;
- Hardscape coverage;
- Green landscaping;
- Tree retention in the Demolition Permit, Development Permit, and Building Permit stages;
- Drainage;
- Non-conforming/non-standard lots;
- Materials; and
- Vehicle loading and storage.

Enabling Successful Infill Development

BACKGROUND

This Phase 1 report addresses Council's direction to review two recurring topics that could be resolved through technical Bylaw amendments. One concern was the lack of front porches for existing and new infill homes. The objective is to develop technical Bylaw amendments to encourage more porches. The second was the concern of subterranean developments that were larger than the main floor plate of the home. The concern was the potential of this development on sustaining soft vegetation on a lot, construction activities and foundation impacts (on the adjacent lot) and drainage. The objective was to determine if Bylaw amendments for restricting/allowing subterranean development could address these concerns.

Currently, porches are defined as "...an unenclosed, covered structure forming an entry to a building" in the Bylaw. Since these spaces are covered and add to the mass of a building, they currently count towards the amount of the parcel that is allowed to be covered by buildings, referred to as *parcel coverage*. When offered the choice between indoor floor area, that can be used year-round, and exterior space that is often seen as seasonal, the market has favoured indoor floor area.

In Developed Areas, the minimum distance a house can be to the front property line depends on how close the neighbouring homes are from their front property lines. This is referred to as the *contextual front setback*. This ensures an infill home will maintain an established street pattern and limit the visual impact of the new home on neighbouring homes. Each land use district also has a required minimum setback from the nearest part of a house to the front property line, called the *minimum front setback*. This means that if the *contextual front setback* is less than the *minimum front setback*, the development must still be setback to the minimum of the district. Typically, subterranean developments extend to the boundaries of the development above, mimicking the main floor plate. Of the applications that were reviewed, three per cent had subterranean developments larger than the floor area of the main floor, which includes areas that are underneath a porch or rear patio. A total of one per cent of reviewed applications had larger subterranean developments, which includes subterranean developments that extend to the rear property line or have two storeys that are underground. Concerns around drainage and the ability for a tree canopy or opportunities for soft landscaping have been voiced by both Council and citizens. There are currently no limits to the depth that a building can go underground and the development may extend to any property line.

Regarding Phase 2, over the last 10 years, Administration has responded numerous times to issues associated with these topics. Despite these attempts to address concerns, these topics continue to reoccur as a source of contention for one or more stakeholders. These topics are symptomatic of broader concerns of: 1) how the rules fit into the larger vision for communities and the city, and 2) what the intent of the rules is in relation to this vision.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Phase 1

In order to understand the concerns related to the issues and to identify potential solutions, a working group was created. The working group consisted of representatives from Building Industry and Land Development Association, Calgary Region (BILD) and the Federation of

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Calgary Communities (the Federation), individual builders, and a variety of community members.

Over 2000 planning applications from the past three years were reviewed, with the focus on understanding the variety of porch designs for new and existing homes, and the extent to which subterranean developments were going beyond the main floor plate. While most homes have a covered entryway that is technically defined as a porch, Administration rarely sees useable front porches that act as covered outdoor amenity space, large enough to accommodate furniture.

Front Porches

Given that usable front porches aren't often seen with new homes (or retrofitted to existing homes), Administration began this review by looking at the best way to create more opportunities for front porches, while respecting that if not done correctly, these could add mass and impose into the semi-private open space typically considered to be a front yard. There were three basic principles that the working group agreed upon when deciding on the options to solve this issue. The front porch should:

1. Provide an area that is a functional outdoor amenity space.

The working group wanted to have an area that provided enough space for outdoor furniture such as a chair and table, with room to walk around. Administration is proposing to allow a porch to project a maximum of 1.8 metres into the required *contextual front setback* but the *minimum front setback* still needs to be met. Should the house be set back deeper from the *contextual front setback*, the porch may be deeper, providing more space as needed. This proposal meets this principle of a functional outdoor amenity space by providing enough area for chairs, a table, and space for movement.

Alternative options were reviewed, including creating a minimum and maximum size area, but Administration advises that this unnecessarily restricts unique sizes and shapes that may provide visual interest and limit a homeowner's ability to create a porch that suits their needs. See Attachment 3 for the different porch designs considered.

The Bylaw already allows porches to be added to existing buildings, when they meet the rules of the Bylaw, to be exempt from a Development Permit. Exempting porches from parcel coverage will provide the greatest incentive, making it possible for homeowners to have a functional outdoor amenity space without sacrificing indoor floor area.

2. Create a transitional space between the public realm and the private home.

The goal of developing a transitional space was to have an area that was not fully accessible to the public, but open and visible to the public. By limiting the height of the porch platform to 1.2 metres for the portion that projects into a *contextual front setback*, the porch becomes ground-oriented, making it a semi-private space. For any portion of the porch that does not project into this front setback, there is no requirement to meet the 1.2 metre height.

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3. Not produce unintended consequences that would exacerbate other infill concerns such as massing and privacy.

The height restriction for the porch platform will reduce impacts to neighbouring homes by ensuring the massing of the porch is not imposing. This is further enhanced with an additional rule that there may not be an enclosed area, such as a bedroom or loft, above the roof of the porch.

The table below provides a summary of the general concepts of the proposed Bylaw changes for front porches:

Current Bylaw Approach	Proposed Change	Outcome
Porches may not project into a front yard setback unless a relaxation is requested. A relaxation opens the development to the appeal process.	Porches would be able to project into a front yard by 1.8 metres, up to the minimum allowed by the district, without having to request a relaxation.	This would encourage more homeowners to add a porch to their home as there is less of a restriction if you go into the front yard and the porch does not trigger a potential appeal.
Porches are counted towards parcel coverage. This creates a situation for a homeowner to choose indoor living space or outdoor living space (porch) in order to meet parcel coverage requirements, without needing a relaxation.	Porches would not be included into parcel coverage.	This change allows a homeowner to have both indoor and outdoor living spaces as the porch is no longer part of the parcel coverage restriction.
No existing rule	Height of the porch platform is limited to 1.2 metres if it projects into a front setback area.	This allows the porch to project into the front but height is restricted to reduce the massing of the porch to the neighbours.
No existing rule	Portion of the porch that projects into a front setback area, must be open.	The requirement for the sides of the porch to be open will decrease massing and avoid a “billboard effect” for neighbours.

Subterranean Development (Basement and Foundations)

Given that Administration does not have many examples of basement developments projecting beyond the floor plate of the main floor of the home, Administration is taking a proactive approach to this issue to introduce parameters early to ensure successful infill development. Administration began this review by identifying potential concerns with subterranean development, when exceeding the footprint of the main floor above. The concerns are:

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1. Future opportunities for soft vegetation (planting trees and sod) onsite.

The working group unanimously agreed that maintaining opportunities for planting trees or having a lawn onsite, particularly at the front of the home, was important. The proposed amendments require basement and foundation elements to meet the *minimum front setback* for the district. This change will protect an area in the front yard (*front setback area*) from excavation and protect the roots of a tree in a boulevard (see Attachment 4). There were additional discussions regarding restrictions into the rear yard; however, the working group acknowledged that there could be ways to provide vegetation in the rear yard, while still allowing subterranean development beneath. This could include lowering the ceiling of the subterranean development, in order for top soil replacement. Based on this, Administration recommends not providing restrictions in the rear yard at this time.

2. Construction disturbances, including foundation impacts to neighbouring homes.

Construction concerns were separated into those that can be addressed through the Bylaw and those that are governed by other City regulations and Provincial Codes. The Bylaw and Codes can provide limits on the building envelope in order to minimize disturbances. One disturbance identified was around the proximity to the neighbouring side property line, and impacts that construction can have on neighbouring property. The working group was of the opinion that construction disturbances are to be expected, and foundation impacts are addressed through other processes, such as complaints to 311 and the review/inspections of building permits. The group would rather have the Bylaw be flexible in situations where basement and foundation developments would benefit from being located in side setbacks. These situations include where property owners would like underground connections to an accessory building that is often located closer to a side property line, or where a homeowner needs more space underground but does not want to develop into the rear yard as it would impact vegetation. As a result of this discussion, an initial proposal to have basements and foundation developments meet the side setbacks was eliminated.

3. Drainage.

The third concern related to subterranean development is drainage. The focus of this discussion was on two elements: lot grading and storm water runoff. Both of these items are directly addressed through the City's Lot Grading and Drainage Bylaws. Basement and foundation elements could impact both lot grading and drainage. However, alterations to a property can be done that address grading and drainage, while being able to support large subterranean developments. Some examples are low impact development practices such as permeable pavements, absorbent landscaping, green roofs and swales but the effectiveness of each will depend on the characteristics (soil composition, water table, etc.) of the lot being developed.

There is ongoing work with the Lot Grading and Drainage Bylaws. Once this work has been completed, Administration will ensure continued alignment with this initiative and the Land Use Bylaw can be amended to support the outcomes of this review, if appropriate. The table below provides a summary of the general concepts of the proposed Bylaw changes for subterranean development:

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Current Bylaw Approach	Proposed Change	Outcome
Subterranean development has no restrictions. This form of development may extend to any property line.	Subterranean development will be allowed to extend into the side and rear property lines but may not develop within the required front setback area.	This change will help with the protection of trees on public land and provide the opportunity for the choice of soft landscaping in the front yard of a home.

Conclusion

The proposed amendments provide a short term, immediate solution to encourage front porches in the Developed Areas. By allowing the porch to project further into the front yard; no longer including the porch in parcel coverage; and continuing to exempt porches from requiring a development permit under certain circumstances, provides incentives to add them onto new and existing homes. The proposed amendments also take into consideration the potential impacts to neighbours by limiting the height and requiring the portions of the porch that project into the front to be open, to reducing building massing.

The proposed amendment to require subterranean development to meet the front setback, is applicable citywide as there is a clear benefit to having space in the front yard to ensure existing and new public tree root systems are protected. The change also provides the opportunity to plant sod, trees and other vegetation in the front yard, which is visible to the public. The proposed amendments allow flexibility for a nuanced approach where circumstances of a parcel could be accommodated, whereas a broad reaching regulation may be overly restrictive.

Administration is requesting that Calgary Planning Commission recommend that Council hold a Public Hearing and adopt by bylaw, the proposed amendments and give three readings to the proposed bylaw. The amending bylaw is required to be advertised in order to inform the public of the potential change.

Phase 2

Discussions with stakeholders over the past seven months have indicated that the underlying issue with infill development isn't about the specific number related to a rule, but rather about the bigger issue of how infill development compliments the evolving character of a developed community.

Based on this, Administration is proposing that the remaining items be examined through a comprehensive approach, rather than through individual Bylaw amendments. Attachment 2 outlines several explanations behind this approach. Amendments to the Bylaw are valid approaches to solving technical discrepancies, however, based on discussions and analysis, Administration acknowledges that the remaining items are more than technical discrepancies and that a more comprehensive approach to infill development is needed. This approach includes first discussing what is important to stakeholders about their communities balanced with the vision and outcomes of the *Municipal Development Plan*, and then discussing how those priorities are implemented by rules. Key themes in these discussions will be:

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1. Alignment with policy.
2. The interaction of infill development with the public realm.
3. The interaction of infill development with neighbours.
4. The elements of development that matter most.
5. Present and future housing needs.

Over the next year, Administration will have multiple conversations about communities rather than about the individual rules. One of the goals of this approach will be to align policy, guidelines, and Bylaw regulations to better support the evolution of Calgary's neighbourhoods.

Stakeholder Engagement, Research and Communication

As noted earlier in this report, a working group of external partners was engaged for these amendments. The individual members and the organizations and communities they represented are in Attachment 5. The group met on average twice a month to discuss these concerns, as well as to discuss amendments to the Residential – Contextual Grade-Oriented Infill (R-CG) District. This approach was beneficial in getting all viewpoints in one venue to develop solutions. Letters from working group members are in Attachment 6.

Both BILD and the Federation provided updates to their members regarding the progress of this work and the City created a webpage for the broader public.

Strategic Alignment

The proposed Bylaw amendments address Council's direction by providing solutions to encouraging more front porches and concerns regarding basement and foundation developments that extend beyond the floor plate of the main floor. They help provide options and clarity for a number of communities at various stages of their life cycle and help address common, technical concerns related to infill development. This work generally supports the policies of section 3.5 of the Municipal Development Plan which acknowledges that redevelopment should support the revitalization of local communities and create great communities by maintaining quality living environments and enhancing community character and distinctiveness.

Administration is prioritizing work programs to address some of the critical gaps and systemic issues continuously heard at Council, in communities and from industry. Administration has combined efforts to ensure consistent discussions, feedback, and information sharing between initiatives, including the Established Areas Strategy, Main Streets, the Municipal Development Plan Monitoring, the Developed Areas Guidebook, amendments to the R-CG district, and ongoing community planning work. The documents mentioned above are the tools that Administration uses to encourage outcomes that align with the vision of making Calgary a great place to make a living, a great place to make a life.

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Other Considerations

On 2018 March 19, Council adopted amendments to the Bylaw which identified and implemented efficiencies for homeowners and small businesses. These amendments were minor in nature but eliminated some redundancies and clarified some processes for applicants. Administration has monitored this work to ensure the changes have not resulted in unanticipated issues. Administration has not seen any issues with these changes over the past 3 months but will continue to monitor throughout the rest of 2018.

Social, Environmental, Economic (External)

Social

The increase in flexibility for homeowners to build front porches that add functional outdoor amenity space could positively impact the way community members and neighbours interact and build relationships with one another.

Environmental

Limiting the ability for front porches and subterranean development in the front setback will encourage the retention of existing public and private trees.

Economic (External)

Eliminating hurdles for homeowners will minimize development pressures often associated with cost increases and will allow homeowners to make decisions based on needs rather than rules.

Financial Capacity

Current and Future Operating Budget:

No impacts are anticipated.

Current and Future Capital Budget:

No impacts are anticipated.

Risk Assessment

No risk.

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REASON(S) FOR RECOMMENDATION(S):

Administration's recommendations provide incentives to existing homeowners to add a front porch to their home, and to home designers to incorporate front porches in the design of new infill homes. This is done without the drawback of it being included into parcel coverage and taking into consideration the impact of the porch to adjacent homes. The recommended changes to subterranean development supports the protection of public trees, provides an opportunity for soft landscaping in the front yard, while providing flexibility of basement development in the rear yard.

Administration's recommendation regarding the next phase of work will allow for a conversation with stakeholders regarding the bigger picture vision rather than a discussion about individual rules. The outcome of this work will facilitate the alignment of policy, regulation and guidelines, to ensure successful infill development.

ATTACHMENT(S)

1. Proposed Amendments to Land Use Bylaw 1P2007
2. Phase 2 Background
3. Porch Sketches
4. Subterranean Development Sketches
5. Working Group Members
6. Letters from Working Group Members

Proposed Amendments to Land Use Bylaw 1P2007

1. The City of Calgary Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, as amended, is hereby further amended as follows:
 - (a) Delete subsection 334(3) and replace with:

"334(1) Portions of a **building** below the surface of the ground may extend without any limits into a **setback area**, with the exception of the required **front setback area**."
 - (b) Delete subsection 335(3)(a) and replace with:

"335(3)(a) **decks**, eaves, **porches** as described in sections 336 and 339.1, ramps, and stairs when located in any **setback area**; and"
 - (c) Delete subsection 336(1) and replace with:

"336(1) Unless otherwise referenced in subsection (6), **bay windows** and eaves may project a maximum of 0.6 metres into the **front setback area**."
 - (d) Add a new subsection 336(5) as follows:

"336(5) In a **Developed Area**, a **porch** may project a maximum of 1.8 metres into a **front setback area** where:

 - (a) it forms an entry to the main floor of a **Dwelling Unit** of a **main residential building**;
 - (b) the setback of the **porch** from the **front property line** is not less than the minimum setback in the district;
 - (c) the maximum height of the **porch** platform is 1.2 metres measured from **grade**, excluding stairs and a **landing** area not exceeding 2.5 square metres; and
 - (d) the portion of the **porch** that projects into a **front setback area** is unenclosed, other than by a railing, balustrade or **privacy walls** located on **porches** between attached **units**."
 - (e) Add a new subsection 336(6) as follows:

"336(6) Eaves may project an additional 0.6 metres from a **porch** into the **front setback area**, as described in subsection 5."
 - (f) Add a new subsection 339.1 as follows:

"339.1 In a **Developed Area**, a **porch** is exempt from **parcel coverage** where:

 - (a) the **porch** is located between the façade of the **main residential building** and:
 - (i) the **front property line**; or

Proposed Amendments to Land Use Bylaw 1P2007

- (ii) the **side property line** on the **street** side of a **corner parcel**;
 - (b) the **porch** is unenclosed on a minimum of two sides, other than by a railing balustrade or **privacy walls** located on **porches** between attached **units** when the **porch** is at or exceeds the **contextual front setback**;
 - (c) there is no enclosed floor area or **balcony** located directly above the roof of the **porch**.”
- (g) Delete subsection 365(c)(1)(i) and replace with:
- “**365(c)(1)** the addition or exterior alteration may:
- (i) reduce the existing **building setback** from a **front property line** a maximum of 1.5 metres, or 1.8 metres for a **porch**, provided the **building** will comply with the minimum setback from a **front property line** specified in the district; and”
- (h) Delete subsection 537 in its entirety.
2. This Bylaw comes in to force two weeks after the date of approval.

Phase 2 Background

Infill development in existing communities is different than new development in the new communities, in the sense that existing context plays a significant role in defining the character of a community. Knowing the goals of the Calgary Municipal Development Plan, including the accommodation of a significant portion of the future population within developed communities, Administration is cognizant to not discourage redevelopment with overly prescriptive rules that make development more difficult or less attractive, while still ensuring that the elements that make these communities so desirable are not lost with redevelopment. Balancing community desires with market demands are often the competing forces behind desires for rule changes. The topics that require attention, identified by Council, are:

1. Eave and peak height differentiation;
2. Massing;
3. Front porches;
4. Subterranean structures extending beyond above-grade footprints;
5. Hardscape coverage;
6. Green landscaping;
7. Tree retention in the Demolition Permit, Development Permit, and Building Permit stages;
8. Drainage;
9. Non-conforming/non-standard lots;
10. Materials;
11. Vehicle loading and storage; and
12. Setbacks.

Why can these topics not be solved with Bylaw amendments?

Discussions with stakeholders over the past seven months have indicated that the underlying issue with infill development isn't about the specific number related to a rule, but rather about the bigger issue of how infill development compliments the evolving character of a developed community. The reasons for which are as follows:

1. 10 years of amendments have not resolved issues

Since Land Use Bylaw 1P2007 was adopted by Council on 2008 June 1, Administration has reported back to Council several times with individual amendments to the rules for infill development. These include but are not limited to the rules regarding height measurements, parcel width, façade articulation, porches and balconies, permitting process changes, landscaping, and the inclusion of a new infill district. Despite these changes and previous attempts to address concerns, these topics continue to reoccur as a source of contention for one or more stakeholders.

2. The fundamental structure of the Bylaw does not always support policy

The Developed Areas Guidebook (the Guidebook) sets out the planning approach and community framework for Developed Areas. The current structure of the Bylaw does not reflect the building blocks of the Guidebook, making implementation and achievement of our outcomes difficult. Making more individual amendments to the existing Bylaw perpetuates a piecemeal approach.

Phase 2 Background

3. Topics are interrelated

All of the topics listed in this report are interrelated to one another in a way that makes individual amendments difficult as amendments for one topic may impact other topics in an unanticipated way. For example, height, massing and setbacks often have implications on one another when determining the desired presence of an individual development along a block or how an individual development could impact neighbouring properties. Depending on what the desired presence is and considering the interrelationships of these topics may result in different amendments comparative to taking the approach of reviewing these topics individually. To determine these amendments, Administration would have to engage stakeholders on what the development presence should be and how the interaction of development should occur, rather than engaging on the specific number for how tall a building should be or what percentage of a parcel it should cover.

Hardscaping, green landscaping, and tree retention also work together to determine what is appropriate design and coverage for front yards, side yards and backyards. These topics need to be balanced with individual homeowner preferences and the ability for a homeowner to customize their outdoor amenity space. These topics could also impact setbacks and massing since requirements for landscaping could impact how big/wide a development can be. These rules could impact the development of front porches and the ability to accommodate parking. Further rules to restrict subterranean development could affect landscaping and tree retention, but further restrictions on height and massing above grade may mean that development below grade should be considered differently.

4. Topics may not be appropriately addressed by Bylaw changes

Community character is an example of something that is too subjective to be regulated through a Bylaw rule. Topics like materials, sensitive design features, sound urban design principles, and colour choices are not something that rules can fix without being unduly restrictive. Bylaw rules do not take into context the individual character of a street or community. This information is better suited for an urban design guideline or contained within a Local Area Plan.

Porch Sketches

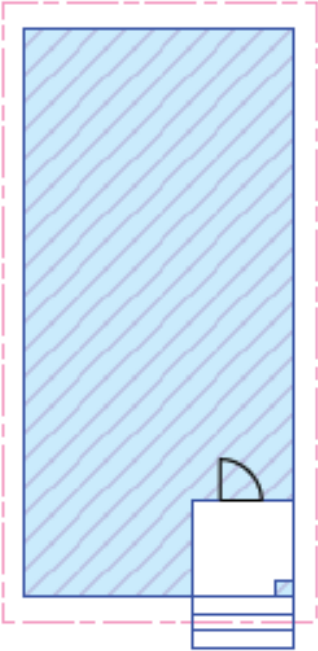


Figure 1 Recessed Porch

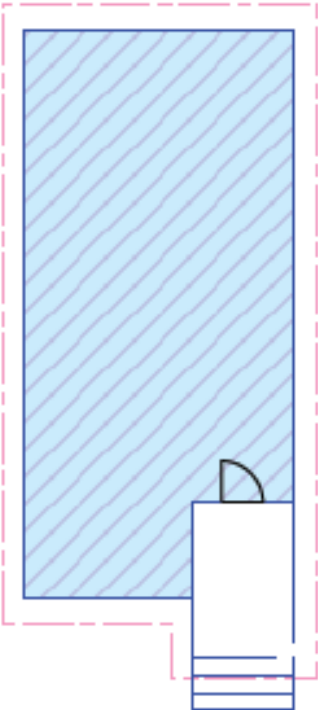


Figure 2 Partially recessed, partially projecting

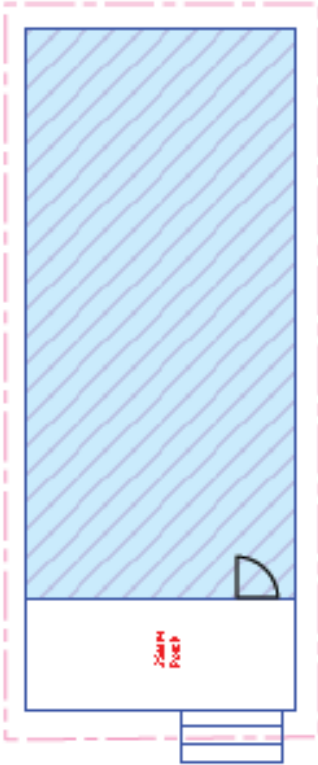


Figure 3 Projecting

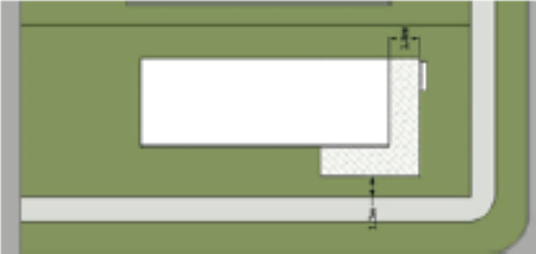
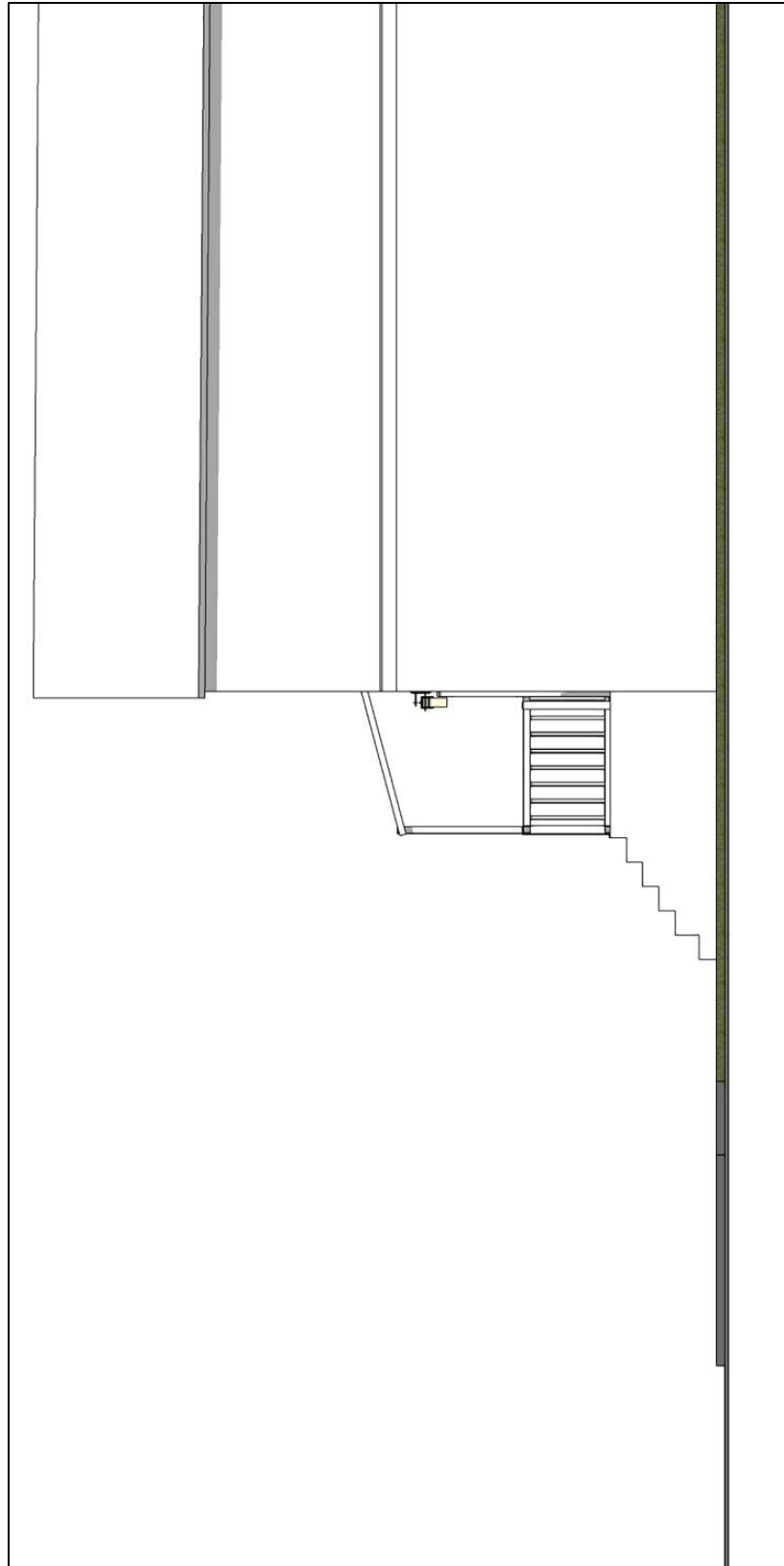


Figure 4
Wraparound
porch on
corner lot

Porches that would benefit from the new regulations.

Porch Sketches



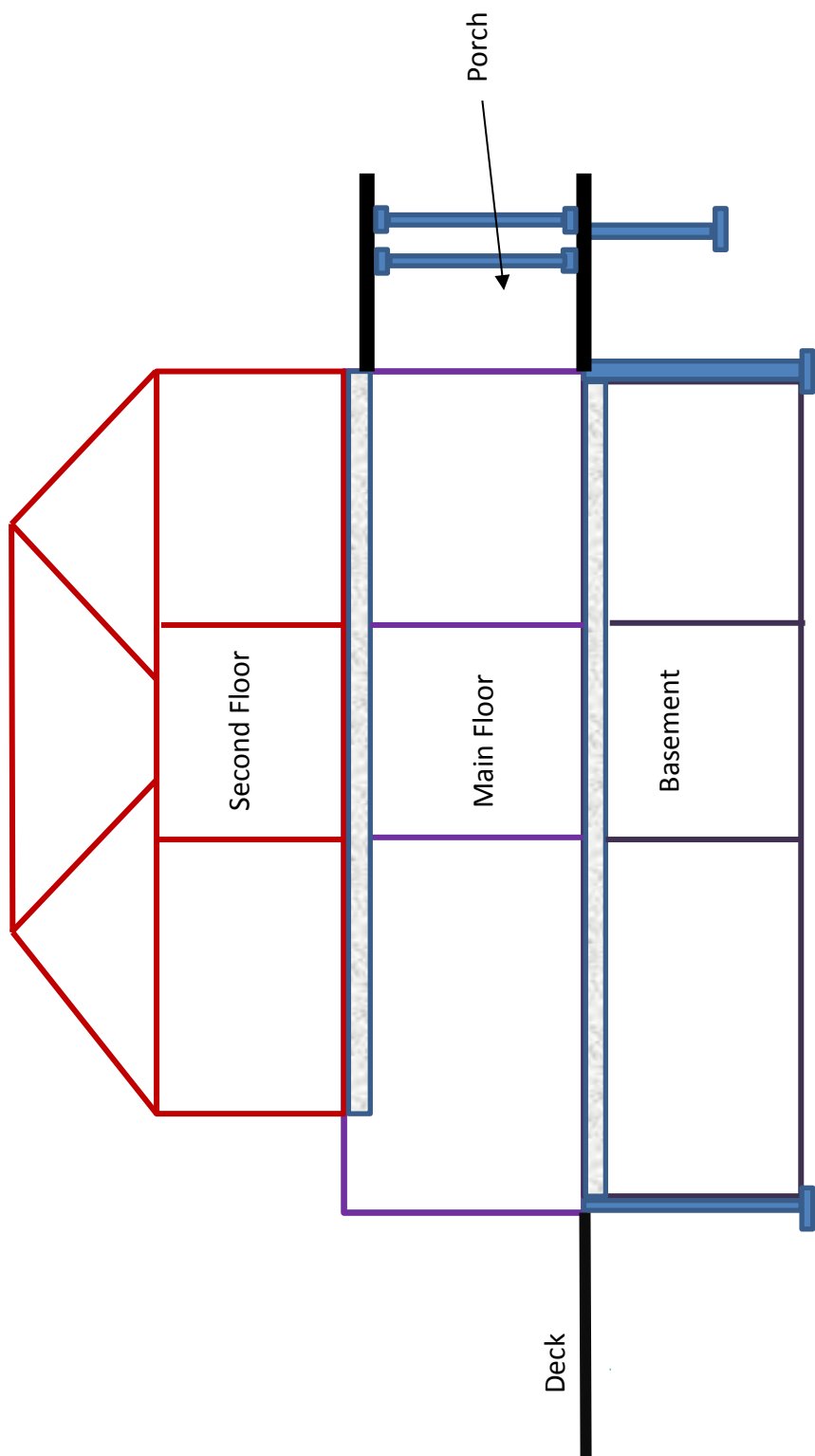
Height of a porch that would be able to project into the front yard (1.2 metres high).

Porch Sketches



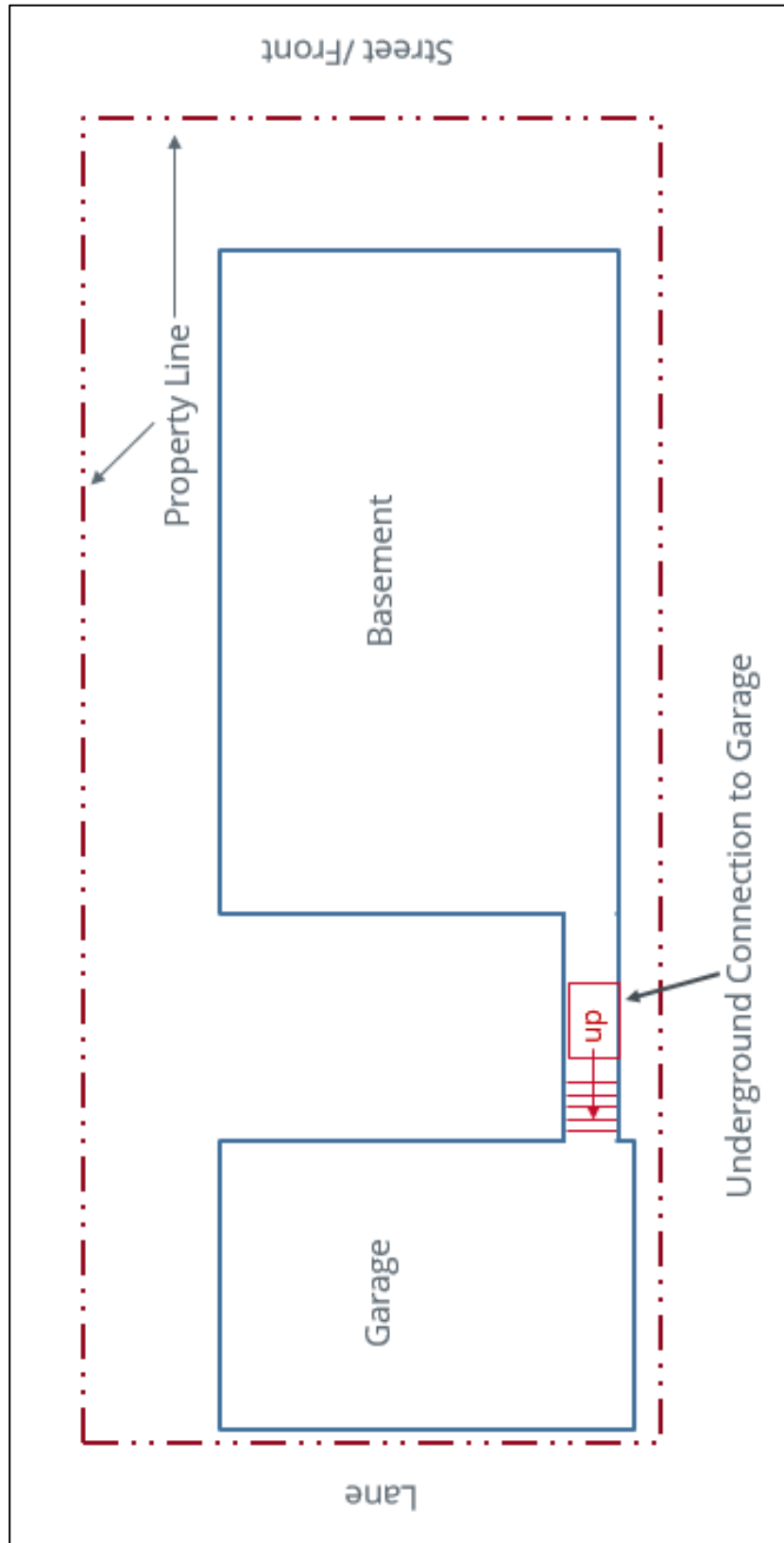
Home with a 1.2 high porch platform and projects into the front yard – neighbour's view.

Subterranean Development Sketches



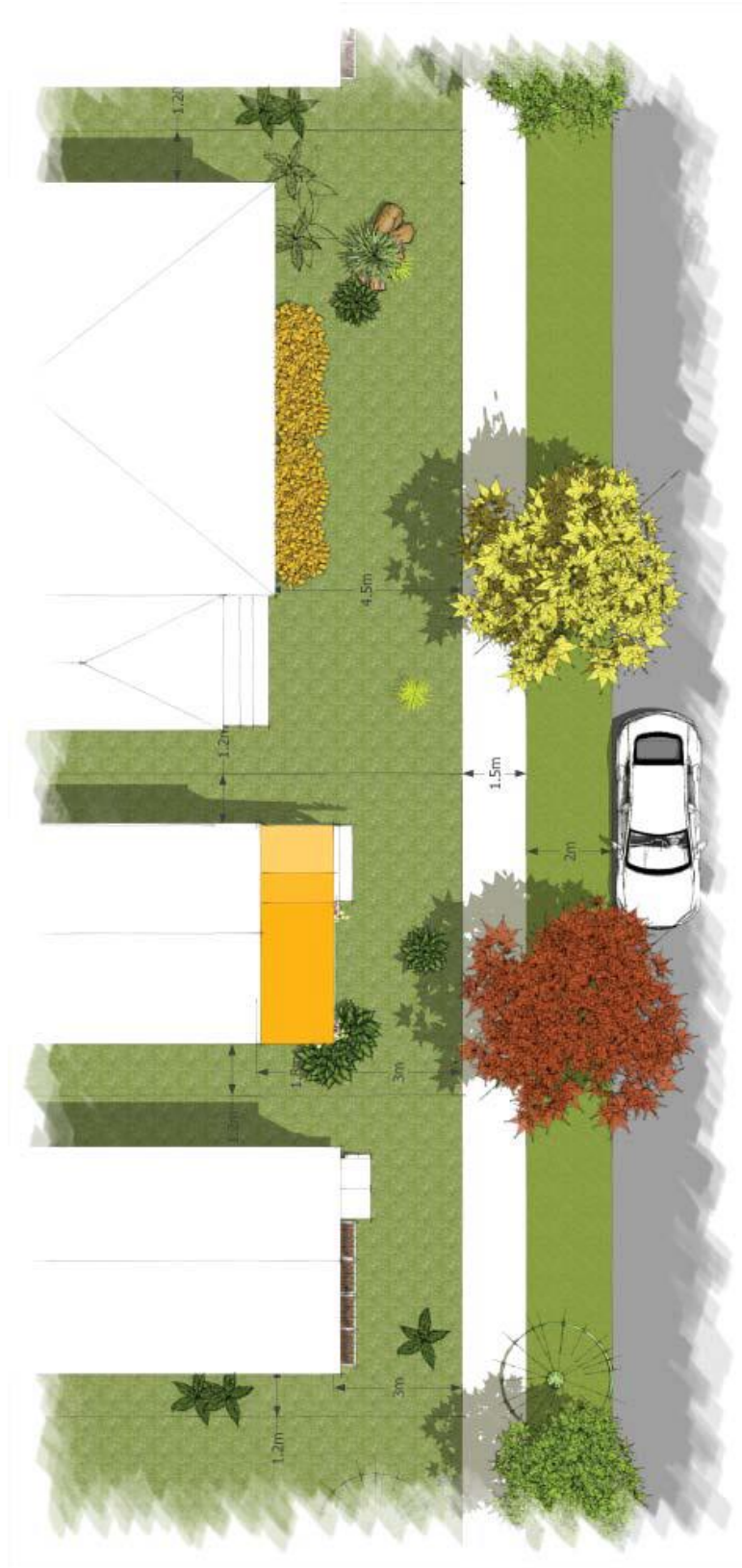
Cross section sketch of a home. In most cases, a basement is within the main floor walls. If the basement is larger than the main floor, in most cases it will go underneath a deck in the rear yard. In some cases, it will go underneath a porch or entry in the front.

Subterranean Development Sketches



Example of a basement development that encroaches into a side yard setback.

Subterranean Development Sketches



Sketch showing the proposed area (front setback) where development may not occur. This is to provide an area to plant trees and sod. This would also protect the root system of a public tree in a boulevard.

Working Group Members

Steve Barnecut	Architect – Industry Representative
Joe Belland	Community Member – Bridgeland/Riverside
Paul Bergmann	Builder – Industry Representative
Mike Borkristl	Builder – Industry Representative
Judy Hoad	Community Member - Parkdale
Bev Jarvis	Director of Policy, Projects & Government Relations - BILD - Calgary Region
Ali McMillan	Community Member – Bridgeland/Riverside
Jennifer Miller	Planner – Federation of Calgary Communities
Ben Morin	Planner – Federation of Calgary Communities
James Reid – Stepped down after 2 meetings	Community Member - Rosedale
Carrie Yap	Planner – Federation of Calgary Communities

Letters from Working Group Members



To: Calgary Planning Commission
From: Paul Bergmann, Winchester Builders
Date: July 19, 2018
Re: Infill Review Working Group

Over the last 6 months I have spent approximately 20 hours in 10 meetings with Community Representatives, City Planners and fellow Building Industry members in discussions led by the City. The intent was to address various Bylaw concerns and issues which seem to be recurring and are seemingly causing friction for some stakeholders.

Specifically, the issues fell into two Categories:

1. Changes to Front Porches and Subterranean Development:
2. Changes to the R-CG District.

In both of these instances, we held fulsome, open discussions about the issues which were important to each of the Stakeholders. I was heartened to see that these discussions were held in a spirit of cooperation, understanding and learning. All of the meetings and discussions were positive, with all parties willing to listen and understand the other Stakeholders.

More specifically, I believe all parties were heard. If someone had a strong opinion on a particular issue, they were allowed to fully develop that idea to allow the other stakeholders to understand their concerns – and the other parties listened. There was then a respectful discussion about how that strong opinion impacted other stakeholders and what some reasonable compromises might look like.

I believe the proposed changes to the Front Porches and Subterranean development, as well as the proposed changes to the R-CG District strike a good balance between the various stakeholder's interests. As a Builder, I can say that while we did not get everything we might have wished for, the proposed changes are a good compromise. They allow for the increase in density the

Letters from Working Group Members

Municipal Development Plan is looking to achieve while being respectful of the existing Homeowners in the Developed Areas. I wholeheartedly support the proposed changes.

I would also like to compliment the City Employees who ran this Stakeholder process. They were professional and Open minded. As good Facilitators, they listened more than they spoke. They did not impose their will on the Community or Building Industry Members, but instead sought to listen and reach reasonable compromises that addressed the issues which had been raised. Kudos to Stephen Pearce and Tammy Henry for running this highly effective and high functioning process.

It was an honour to participate in this process; I appreciate the opportunity,

Pal Bergmann
Winchester Builders

Letters from Working Group Members

inertia

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Friday, July 13, 2018

Calgary Planning Commission
Calgary Municipal Building
P.O. Box 2100, Stn. M
Calgary, Alberta, Canada T2P 2M5

Re: Infill Review Working Group

Since February 1, 2018 I have been a member of the Infill Review Working Group that has been coordinated by Tammy Henry and Stephen Pearce. It is a group composed of industry, community and City representatives. I have been designing infill developments in Calgary since 1999, and brought to the group my experience of designing within the context the Land Use Bylaw (LUB).

The discussions fell into two categories. We discussed encouraging front porches in infill development, and we discussed subterranean development beyond the extent of above grade building.

Front Porches and Subterranean Development

There was general agreement among all parties in the Group that front porches are a desirable feature of infill development, and that the LUB had to be more accommodating of front porches. The current rules discourage porches on most infill parcels by requiring that houses accept a reduction in livable area (and salable area) equivalent to the size of a porch. The proposed rules are well crafted and will result in more porches in Calgary's established areas.

There was less agreement regarding subterranean development. The Group found it difficult to define the problem that restricting such development would solve. There is also a chance that the implementation of these restrictions may have an unpredictable impact on typical underground construction. Further, if the rationale of restricting development under the front setback is to provide growing material for street trees, this would not be fair to homeowners and developers if the City did not actually plant and maintain a street tree. Currently this cannot be assured as the City will not take the initiative to plant a street tree if it is not between a sidewalk and a curb.

Though one of the reasons that subterranean development should be restricted may be the maintenance of proper drainage and stormwater management, many of us in the Working Group felt that this is an engineering concern and that the LUB was not the place to regulate drainage or stormwater management.

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Letters from Working Group Members

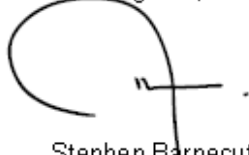
Changes to the R-CG District

The R-CG district is critical to diversifying housing options in Calgary's established areas and to Calgary's sustainable growth. It allows a modest increase in density in appropriate infill locations, and has proven to be popular among builders and home purchases.

Members of the Working Group were all supportive of the district and all wanted to ensure that changes improved development outcomes without discouraging the development of R-CG projects. The proposed changes to the R-CG district satisfy these expectations.

I would like to express my thanks to members of the Working Group for the productive conversations we've had over the past year and a half. I look forward to continuing the conversation with them and other stakeholders as we all seek to make Calgary a better place to live and work.

Best regards,

A handwritten signature in black ink, appearing to read 'Stephen Barnecut', with a large loop at the start and a horizontal stroke at the end.

Stephen Barnecut
Director
Inertia Corporation

Enabling Successful Rowhouse Development in the R-CG District

EXECUTIVE SUMMARY

In the monitoring report presented 2017 November 30, Administration identified three persistent areas of concern expressed by Council, Calgary Planning Commission (the Commission) and community stakeholders regarding the Residential – Grade-Oriented Infill (R-CG) District. These issues were: secondary suites in rowhouses; corner rowhouse interface with neighbouring houses; and rowhouse height.

The proposed amendments to the R-CG District address the technical concerns identified in the R-CG Monitoring Report by:

- Applying greater discretion with regard to the suitability of secondary suites in rowhouse buildings, with policy tools to approve or limit this form where appropriate;
- Removing parking exemptions for backyard suites and allowing them for secondary suites in locations that are more supportive of active modes of transportation, such as walking and public transit;
- Amending the height rules in the R-CG District to apply a consistent method of measurement and to better transition height from existing lower scale development to new, taller infill development; and
- Encouraging more street-oriented rowhouse developments on both corner and mid-block locations and increasing side yard setbacks that provide a buffer between new development on corner parcels and the backyard of the neighbouring property.

In addition, housekeeping amendments are proposed that support more efficient processes for secondary suites in semi-detached dwellings and consistent height regulations for rear garages.

ADMINISTRATION RECOMMENDATION:

That the Calgary Planning Commission recommends that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to Land Use Bylaw (1P2007) (Attachment 1);
2. Give three readings to the proposed bylaw; and
3. **ADOPT**, by resolution, the proposed amendments to the *Policy to Guide Discretion for Secondary Suites and Backyard Suites* (Attachment 3).

PREVIOUS COUNCIL DIRECTION / POLICY

The R-CG Monitoring Report for 2017 was presented to the Commission on 2017 November 30. At the 2017 December 18 meeting, Council approved the Commission's recommendation to direct Administration to review, for the R-CG district, secondary suites, parking for secondary suites, rowhouse maximum building height, and corner rowhouse issues, and report with recommendations to Council, through Calgary Planning Commission, by 2018 Q3.

Enabling Successful Rowhouse Development in the R-CG District

BACKGROUND

When Council adopted the Residential – Contextual Grade-Oriented Infill (R-CG) District in October 2014, Administration was directed to report back to Calgary Planning Commission annually on the number of applications in the R-CG District and the status or outcome of those applications. In the latest R-CG Monitoring Report, Administration identified three persistent areas of concern expressed by Council, the Commission and community stakeholders regarding the R-CG District. These included:

- secondary suites in rowhouses: the rules for secondary suites were perceived as overly permissive since they allow secondary suites in all rowhouse units and provide a parking exemption for small secondary suites. It was felt that these rules do not appropriately account for the additional impacts that secondary suites might have, such as changes to neighbourhood character and additional cars parking on the street;
- corner rowhouse interface with neighbouring houses resulting in buildings that fill the full length of the lot, in some cases overshadowing neighbouring back yards and affecting the privacy of existing residents; and
- a maximum rowhouse height that is higher than what is allowed for single and semi-detached dwellings, resulting in taller buildings that overshadow existing, lower scale buildings.
-

Since the R-CG Monitoring Report was completed, a new issue has been identified regarding inconsistencies in the way that secondary suite applications in semi-detached dwellings are processed. Currently an applicant must apply for a different land use district based on whether the units in the semi-detached dwelling are subdivided or not. This distinction is unnecessary and creates a more complicated and confusing approvals process.

In addition, since the monitoring report was released another non-policy issue has been noted at public hearings regarding the increased number of Waste and Recycling carts stored in the alley due to the increased number of units in rowhouse developments. There are existing regulations and processes in place that adequately regulate the location and storage of the carts therefore the issue is managed through Bylaw enforcement options.

The Waste and Recycling Bylaw requires blue, black and green carts to be placed out no earlier than 7:00 pm the day before collections and to be removed by 7:00 pm from the street or alley on collection day. The development permit plans for rowhouses indicate on-site storage locations for these carts for owners to use between collection days. When carts are left out in the alley, individuals can report the issue to The City and these situations will be addressed by Bylaw enforcement.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The work program to consider recommendations for the R-CG District has been undertaken in collaboration with the *Enabling Successful Infill – Phase 1* project (Report CPC2018-0888), that has similar direction to return to Council through the Commission by 2018 Q3 with recommendations for Land Use Bylaw amendments. Both projects involve similar stakeholders and address specific technical concerns with low density residential infill forms. The

Enabling Successful Rowhouse Development in the R-CG District

amendments that are proposed in the Phase 1 Infill report regarding porches and subterranean development will also apply in the R-CG District.

Secondary Suites in Rowhouses

The R-CG District allows both secondary suites as a permitted use and backyard suites as a discretionary use with individual units in rowhouses, semi-detached dwellings or single detached dwellings. This provides flexibility and choice for home owners and helps increase the diversity and affordability of housing in low-density neighbourhoods.

Secondary suites divide up the space in a house between two households to respond to different household needs, stages of life and economic means. Because suites cannot be sold separately from the primary dwelling they can provide rental housing that contributes to a mix of tenures in a neighbourhood.

Council and residents have expressed concern that allowing suites as a permitted use in rowhouses adds density that is not appropriately accounted for in the rules of the R-CG District. The primary impact of additional units, as identified by stakeholders, is the potential for off-site parking that spills over onto the shared public street.

The perception of additional impacts due to suites has become a barrier to the approval of redesignation applications to the R-CG District that might otherwise be supported. Given a lack of current alternatives in the Land Use Bylaw, Council has used a direct control district that removes secondary suite uses from the R-CG District to manage these applications.

Discussions with stakeholders regarding secondary suites and backyard suites focused on some basic principles regarding suites in rowhouse developments:

- Providing the option for a small rental unit without the added cost of a parking stall was generally thought to be of value by providing housing options generally not otherwise available in low density residential neighbourhoods.
- The option for a suite without on-site parking should only be available in locations where living without a car is reasonably convenient. The measure of this convenience is indicated based on the availability of other mobility options, such as public transit, and access to nearby shops and services.

Parking for Secondary Suites

The infill housing forms in the R-CG District result in more efficient use of land than in other low density residential districts. This means that each house is contained on a smaller parcel, leaving less space for additional parking stalls. The existing parking exemption in the R-CG District for suites that are 45 square metres or less makes it feasible to develop suites on smaller parcels.

Enabling Successful Rowhouse Development in the R-CG District

The parking exemption for small secondary suites is provided because smaller rental units accommodate fewer people. It is not expected that every occupant of a small secondary suite will choose not to own a car; however, they are less likely to own a car than occupant(s) in a larger unit, particularly if this choice is enabled by access to other ways of getting around and meeting one's daily needs.

Currently the parking exemption for small suites applies in all locations and applies to both backyard suites and secondary suites.

A backyard suite, unlike a secondary suite, opens up new floor space in the backyard that wasn't otherwise available as a living space. For this reason, backyard suites, whether small or otherwise, create additional living space that can accommodate additional people and therefore may result in a greater parking demand.

Administration recommends that the parking exemption be applicable to secondary suites only and not include backyard suites. In addition, Administration recommends amending the parking exemption to apply exclusively to secondary suites that:

- are 45 square metres or smaller;
- are located within 600 metres of an LRT platform or 150 metres of a frequent bus route; and
- provide additional storage space for mobility alternatives to a private car, such as bicycles or strollers.

This approach provides certainty for all parties and enables secondary suites without a parking stall where it is appropriate. The working group explored the option of using a policy to relax the parking requirement for secondary suites instead of a rule, however, the rule was preferred since it provides more certainty for communities and applicants while still enabling Administration to relax the parking requirement through a Development Permit, if necessary.

Options for Secondary Suites in Rowhouses

Council's direction to provide recommendations to address secondary suites in the R-CG District has provided the opportunity to explore a number of different options for secondary suites in rowhouses. These options are:

Option 1: Create a modified R-CG District that removes the option for secondary suite uses in the District. This option would make a stock R-CG District available with an "ex" modifier (R-CGex) that excludes the Secondary Suite and Backyard Suite uses from the District. This option would achieve the same outcome as the direct control districts adopted by Council and would be similar to the approach that was taken with the R-C1 and R-C1s districts prior to the secondary suites reform recently approved by Council.

Option 2: Maintain Secondary Suite as a permitted use in the R-CG District while limiting the parking exemption to parcels with more mobility options. This option, with the proposed changes to the parking exemption rule proposed above, would allow Secondary Suites on any parcel that meets the parking exemption criteria.

Enabling Successful Rowhouse Development in the R-CG District

Option 3 (recommended): Change Secondary Suite to a discretionary use when it is proposed in a rowhouse building or in developments with more than one main residential building and adopt a policy to guide where secondary suites should be approved. This is similar to the approach that Council has recently approved for the secondary suites reform.

Discussion

The certainty of a modified R-CG District is an effective way to limit the options on a parcel where a landowner is applying for a specific development; however, the all-or-nothing approach results in problems when The City redesignates large areas simultaneously, such as the redesignations completed for Main Streets. These City led redesignations set in place rules that enable the build out of a variety of developments over time to fulfill the policy direction for the area. In these situations, flexibility in the land use district to apply discretion helps to support many different developments that are aligned with the policy, instead of just facilitating one development, as in the case of a site specific application.

While flexibility to allow a variety of developments that align with policy is a good thing, being overly permissive can also allow developments that do not align well with policy. Listing Secondary Suite as a permitted use means that the development authority must approve the use, whether or not the secondary suites will fit well in the proposed location. The additional requirements for the parking exemption would help to better locate secondary suites in a neighbourhood, but on their own they do not manage other concerns regarding whether the suites fit with the local context.

Listing Secondary Suite as a discretionary use in rowhouses makes it so that Administration can make decisions on individual development applications based on policy, the local context and the specifics of the application. It also means that community stakeholders are included in the decision making process at the development permit stage of the application, instead of just at the land use amendment stage. This approach provides flexibility in the district while also making it so that the individual context of a development is considered in the decision.

Administration's evaluation is that the all-or-nothing approach of a modified district is overly rigid, while the current approach, even with additional parking conditions, is overly permissive.

Administration recommends Option 3, that Secondary Suite be changed to a discretionary use in the R-CG District when it is proposed in a rowhouse building or in developments with more than one main residential building, such as a development with two semi-detached buildings on the same parcel. To support this change Administration proposes amending the *Policy to Guide Discretion for Secondary Suites and Backyard Suites*, adopted earlier this year, to add a new section that would guide Administration's decisions on secondary suites in the R-CG District.

This new policy section (Attachment 3 – Policies for Secondary Suites in the Residential – Grade Oriented Infill (R-CG) District) describes three conditions that, when present, mean that a secondary suite in a rowhouse is more appropriate. These conditions include:

1. the availability of shops and services nearby;
2. the availability of mobility options; and
3. site characteristics that affect the ability of the site to integrate secondary suites within the neighbourhood context.

Enabling Successful Rowhouse Development in the R-CG District

This policy will guide where Administration should or should not support secondary suites in a development. It provides some certainty for stakeholders while also enabling some flexibility to respond to site or development specific factors.

Rowhouse Building Height

As observed at public hearings of Council, adjoining landowners and communities are concerned about the impacts an 11 metre rowhouse could have on the context of the existing houses on the street and character of the neighbourhood.

Maximum Building Height

The maximum building height of single and semi-detached infill dwellings in the low density residential districts is 10 metres. The method for measuring this height (illustrated in Figures 1 and 2 of Attachment 4) adds one metre to allow for the ground to be graded away from the edge of the building and then allows an additional 10 metres above this. This method of measurement effectively results in an 11 metre maximum building height (one metre for grading plus 10 metres). In addition, the line that defines the maximum building height on development plans resulting from this method is flat and does not follow the elevation of the ground, which means that the height of new buildings is not required to follow the slope on the site.

The maximum height of a rowhouse building in the R-CG District is 11 metres measured directly from the ground (illustrated in Figure 3 of Attachment 4). This means that the height of the building follows the existing elevation of the ground and does not add one metre for grading. The 11 metre maximum building height allows for a two to three storey rowhouse that follows the slope of the site.

Because of the different methods to measure building height, the maximum building height in the R-CG District (at 11 metres) is equivalent to maximum building height for single and semi-detached houses in other low density districts (at 1 metre plus 10 metres).

The recommended amendments would make it so the height of all housing in the R-CG district is measured from grade to a maximum of 11 metres. This means that there would be one method for measuring height in the R-CG District that would apply equally to single detached, semi-detached and row housing and that the maximum height of new buildings would follow the slope on a site.

Transition of Building Height Between Parcels

Currently, the maximum height of rowhouse buildings is stepped down at the side on sites that are next door to low density districts other than R-CG (such as R-C1 or R-C2). These stepped heights are based on the height of the neighbouring buildings and step down at right angles (the steps are square). Two issues have been observed with this method of transitioning height: first, it does not apply in all low density districts; and it discourages sloped roofs by using square steps to define the maximum height.

Enabling Successful Rowhouse Development in the R-CG District

The recommended amendment proposes an angled height plane that applies next to all low density districts and the M-CG townhouse district (illustrated in Figures 4 and 5 of Attachment 4). This change reduces the building mass at the side of the building and therefore better manages the transition of height from neighbouring parcels; it will better enable sloped roofs; and it applies to all low density districts.

Rowhouses on Corner Parcels

Corner parcels offer street frontage on two sides. Single detached dwellings typically only face the street on the narrow side (front) of the parcel. This often results in a mostly blank wall with a high fence along the long side of the parcel next to the side street. Corner rowhouse developments encourage house fronts to be located along the long side of a corner parcel, placing front entrances on the side street. Providing house fronts that face sidewalks on both streets improves the experience for people walking by and encourages people to drive more respectfully because the street is recognizable as a residential street.

Facing rowhouses along the length of the side street results in a building that extends the full length of the neighbouring side property line with the back of the rowhouses facing the side of the neighbouring property. This reorientation of the building from front to side also means that more windows and doors will face the side property line than is typical. The key concerns identified with this relationship is that the new rowhouse building may limit the amount of sunlight available in the neighbouring rear yard and that the windows and doors facing onto the neighbouring yard may make it feel less private.

Current building setback rules in the District sometimes require that corner rowhouse buildings be set at a significant distance back from the street. These setbacks result in site layouts where rowhouse buildings are pushed back from the street and closer to neighbouring properties. This has the combined effect of increasing the potential for negative impacts for the neighbour, as outlined above, and reducing the potentially positive impact of the rowhouse on the street. Reducing the front yard setbacks for rowhouses on corner lots would pull the buildings closer to the street and further away from the neighbouring property.

Engagement identified that community stakeholders value front setbacks that are similar with neighbouring parcels (the contextual front setback), however it was felt that allowances for a shallower front yard make sense if the new house were allowed to move forward gradually, the further away it gets from the side of the neighbouring property. This discussion resulted in the recommendation for an angled front setback line for corner parcels that gets closer to the street the further away it gets from the neighbouring side property line (illustrated in Figures 1 to 3 of Attachment 6).

Yards and Fences

One issue identified with locating side-facing rowhouses further forward of neighbouring houses was the potential for an awkward transition from the neighbouring front yard to the back yard of the row house facing the side street. Allowing a patio or deck in this space can complement the neighbouring front yard, so long as the space is unenclosed or enclosed in a low fence that is in keeping with what is allowed in other front yards along the block face. Where a back yard amenity space is enclosed within a six foot high fence next to the neighbour's front yard this

Enabling Successful Rowhouse Development in the R-CG District

transition is overly abrupt. To manage this transition between yards, a new rule is recommended that limits the height of the fence in front of the neighbouring house to four feet tall. A shorter fence would delineate the yard space for the rowhouse, while still maintaining a character more typical of front yards.

The recommended amendments to address corner rowhouse concerns would maintain existing development potential, reduce the impact of a new corner rowhouse on the neighbouring backyard, respect the contextual front setback and manage the transition from the neighbouring front yard to the back of the rowhouse. The proposed amendments are illustrated in Figures 1 through 6 in Attachment 5.

Front Yard Setbacks

In the discussion focusing on solutions for corner rowhouse developments the topic of front yard setbacks came up more generally. The current rules of the R-CG District apply the contextual front setback rules to developments next door to the R-C1 and R-C2 Districts. This means that the front setback in these cases is matched up to the neighbouring front setbacks. Meanwhile, the front setback for R-CG parcels next to other R-CG parcels is the 3.0 metre minimum in the district.

The difference in the application of these rules based on which district is next door has resulted in confusion regarding the front setback rules and concern about the contrast between scenarios. Where an R-CG parcel is next to R-C1 or R-C2 there are concerns that the front setback is overly deep; meanwhile, not considering the contextual setback at all where new development is next to R-CG can result in houses that stick out a long way forward of neighbouring houses instead of transitioning forward more gradually.

In order to balance principles of street-oriented building design with consideration of the existing front yard pattern on the block, the proposed amendments apply the contextual front setback rules for all developments in the R-CG District, but sets a cap of 4.5 metres on the minimum front setback. This means that the minimum front setback will range between 3.0 metres, where the neighbouring building is close to the street, to 4.5 meters where the neighbouring building is set back six metres or more (illustrated in Figures 4 and 5 of Attachment 6). A house may be set further back on a parcel than 4.5 metres, but it will not be required to locate further back based on the rules in the district.

Housekeeping Amendments

The following amendments are proposed to remedy situations in which a rule in the Land Use Bylaw is not fulfilling the intended purpose or where it results in an unnecessarily complicated application process.

Secondary Suites in Semi-detached Dwellings in the R-CG District

There are currently two potential paths for an applicant to follow if they want to add a secondary suite, or legalize an existing secondary suite, in a semi-detached dwelling located in a district where this is not permitted, such as the R-C2 District.

Enabling Successful Rowhouse Development in the R-CG District

The first path, to redesignate to the R-CG District, is open to an applicant when each of the units in a semi-detached dwelling are located on their own individual parcel (meaning that they are subdivided along the party wall). This path results in a consistent process flow from land use amendment through the development permit to the building permit.

Currently, a second, more complicated path must be followed where both units in a semi-detached dwelling are on the same parcel (meaning that the units are not subdivided). In this scenario the applicant must apply to redesignate to the M-CG District, apply for a development permit for a multi-residential development and then apply for a building permit for a secondary suite in a semi-detached dwelling. This path results in unnecessary confusion about the intent of the application, since it keeps changing names, which results in issues at all three stages of the application process, lengthening processing times and causing frustration for all parties involved.

The proposed amendment would fix this unintended process complication by amending the definition of a secondary suite to allow a secondary suite to be approved within a dwelling unit located in a semi-detached dwelling, whether subdivided or not, and amend the secondary suite density rule to continue to allow a maximum of one secondary suite in an individual dwelling unit.

Secondary and Backyard Suites in Multi-Residential Districts

Because there aren't rules for suites in the multi-residential districts, secondary suites and backyard suites in multi-residential districts follow the rules of the R-CG District. This is intended to simplify these applications and reduce repetition of rules in the Land Use Bylaw. One unintended consequence of the way these rules are worded is that suites are not allowed with a semi-detached dwelling in multi-residential districts, even though they are allowed in the R-CG District. The proposed amendments change the wording in the definitions of backyard suite and secondary suite to fix this problem. These amendments will simplify the application process for suites in multi-residential districts.

Height of Rear Attached Garages in Low Density Residential Districts

Administration recommends an amendment to the height rule for rear attached garages. Section 338(3) of the Land Use Bylaw allows a rear attached garage to be built in the rear setback area of a house where it maintains similar dimensions to what is allowed for a rear detached garage. The current wording of the rule makes it so the garage height is measured in relation to the grade at the front or rear property line, which can result in a taller structure than what would be allowed for a rear detached garage on the same parcel. The proposed amendment would make it so the height of a rear attached garage is measured in the same way as a rear detached garage.

Other Issues

Other issues identified in the R-CG Monitoring Report, such as how to apply the Multi-Residential Locational Criteria, how to manage privacy on neighbouring parcels and how to sensitively integrate larger buildings into established neighbourhoods, are common to all the infill districts. Since these are broader issues affecting all infill districts, Council has supported

Enabling Successful Rowhouse Development in the R-CG District

an approach to consider these topics in coordination with other issues that Council has identified regarding infill development. This ongoing work focuses on ways to better integrate infill development into established communities and better align the districts with evolving local area policy, the Developed Areas Guidebook and the Municipal Development Plan (MDP). A discussion of how to approach topics relating to infill districts in alignment with ongoing policy work will be presented to Council in 2018 Q4.

Conclusion

The proposed amendments to the R-CG District address technical concerns identified in the R-CG Monitoring Report by:

- Applying greater discretion with regard to the suitability of secondary suites in rowhouse buildings, with policy to guide Administration's decision;
- Allowing parking exemptions in locations that are more supportive of active modes of transportation, such as walking and public transit;
- Amending the height rules in the R-CG District to apply a consistent method of measurement and to better transition height from existing lower scale development to new, taller infill development; and
- Encouraging more street-oriented rowhouse developments on both corner and mid-block locations and increasing the area in the side yard to provide a buffer between new development on corner parcels and the back yard of the neighbouring property.

Housekeeping amendments serve to support more efficient processes for secondary suites in semi-detached dwellings and consistent height regulations for rear garages.

Stakeholder Engagement, Research and Communication

In collaboration with the Federation of Calgary Communities (the Federation) and Building Industry and Land Development Association– Calgary Region (BILD), Administration established a working group consisting of administrative representatives from the Federation and BILD, community representatives from communities within the Developed Areas (recommended by the Federation) and industry representatives (recommended by BILD). The membership of this group is listed in Attachment 7.

This working group has been meeting bi-weekly to develop recommendations to address the items identified by Council's direction for both the Enabling Successful Infill – Phase 1 and for the R-CG Monitoring. Updates to the public, on the discussions of this group, were provided through distributions from the Federation and BILD to their members. Recommendations have been shared with the broader public through a webpage on the City of Calgary website.

Strategic Alignment

To support greater housing choice and reinforce the character, quality and stability of existing residential neighbourhoods, the Municipal Development Plan (MDP) encourages growth and change to add a diverse mix of ground-oriented housing in existing low-density residential neighbourhoods. Ground-oriented housing is characterized by houses with entries that open at grade and face the street. This type of housing maintains a building pattern that, in its form,

Enabling Successful Rowhouse Development in the R-CG District

mass and site design, is compatible with existing homes in low-density residential neighbourhoods.

The rules of the R-CG District are intended to support the sensitive integration of a wide variety of low-density multi-housing developments, allow for innovative site configurations and facilitate evolving redevelopment of a variety of ground-oriented housing over time. The amendments recommended in this report help support the effective implementation of this intent and better align the R-CG District with MDP policy objectives.

In addition to the technical amendments proposed in this report, Administration is in the process of aligning a variety of streams of work so that, together, they establish a clearer vision for the evolution of established area neighbourhoods and provide tools that better enable outcomes aligned with that intent. These include the Municipal Development Plan Monitoring, updates to the Developed Areas Guidebook, the Established Areas Growth Strategy, Land Use Bylaw improvements and district based Local Area Plan work. This work program will provide the opportunity to integrate, streamline and align secondary policies, such as the Location Criteria for Multi-Residential Infill, within city wide policies, such as the Developed Areas Guidebook.

Social, Environmental, Economic (External)

The proposed amendments are intended to better enable new development that adds a broader variety of housing into existing low density neighbourhoods. Better managed height and setback transitions between existing and new development should also help to reduce some of the stresses associated with infill redevelopment. This should better support the social benefits of providing a greater variety of housing choices that fit a range of household needs and means while reducing conflicts between neighbours.

Because the recommended amendments address a number of the concerns identified with new rowhouse development, less time and negotiation should be needed for applicants to gain support for development proposals. This should help support investment in new rowhouse developments. In addition, locating more housing in areas that already have established infrastructure, amenities, and commercial areas will better support ongoing viability of these neighbourhood features and make better use of existing resources.

Financial Capacity

Current and Future Operating Budget:

No impacts are anticipated.

Current and Future Capital Budget:

No impacts are anticipated.

Enabling Successful Rowhouse Development in the R-CG District

Risk Assessment

The scope of the recommended amendments have focused on items that improve the immediate implementation of the R-CG District. Because these amendments focus specifically on adjustments to existing rules and uses in the R-CG District the risks associated with the proposed amendments are limited.

REASON(S) FOR RECOMMENDATION(S):

Administration's recommendations support more sensitive integration of new rowhouse development in infill locations while providing more development options on corner parcels and enhancing the street orientation of rowhouse developments.

ATTACHMENT(S)

1. Proposed Amendments to the Land Use Bylaw 1P2007
2. Summary of Proposed Amendments
3. Policies for Secondary Suites in the Residential – Grade Oriented Infill (R-CG) District
4. Rowhouse Building Height
5. Proposed Amendments on Corner Parcels
6. Front Yard Setbacks
7. Working Group Members
8. Letters from Working Group Members

Proposed Amendments to Land Use Bylaw 1P2007

1. The City of Calgary Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, as amended, is hereby further amended:
 - (a) Amend subsection 153.1(a)(vii) to add “or a **multi-residential district**” after “R-CG District”.
 - (b) Amend subsections 295(a)(i) through (iii) to delete “that” at the beginning of the subsection.
 - (c) Amend subsection 295(a)(v) to delete “located on the same **parcel** as” and replace it with “contained in”.
 - (d) Amend subsection 295(a)(v) to delete “located on the same **parcel** as” and replace it with “contained in”.
 - (e) Amend subsection 295(a)(vi) to add “or a **multi-residential district**” after “R-CG District” and delete “located on the same **parcel** or **bare land unit** as a **Dwelling Unit**”.
 - (f) Amend subsection 295(a)(vii) to delete “located on the same **parcel** as a **Dwelling Unit**” and replace it with “contained”.
 - (g) Amend subsection 338(3)(a) to delete “**building height**” and replace it with “height, measured from the finished floor of the **private garage**”.
 - (h) Amend subsection 338(3)(b) to add “ for each **Dwelling Unit** located on the **parcel**” at the end of the subsection before the semi-colon.
 - (i) Delete subsections 346(4)(a) and (b) in their entirety and replace them with the following:
 - “(a) must not exceed the lesser of:
 - (i) the **building coverage** of the **main residential buildings**; or
 - (ii) 75.0 square metres for each **Dwelling Unit** located on the **parcel**;
 and”
 - (j) Amend subsection 354(1) to delete “**Secondary Suite** or”.
 - (k) Add a new subsection 354(1.1) as follows:

“(1.1) There must not be more than one **Secondary Suite** contained within a **Dwelling Unit**.”
 - (l) Delete subsection 526(1)(g) in its entirety.
 - (m) Add a new subsection 526(3) as follows:

“(3) A **Secondary Suite** is a **permitted use** in the Residential – Grade-Oriented Infill District where:

 - (a) it is contained within a **Contextual Semi-detached Dwelling, Contextual Single Detached Dwelling, Semi-detached Dwelling, or Single Detached Dwelling**; and

Proposed Amendments to Land Use Bylaw 1P2007

- (b) one ***main residential building*** is contained on a ***parcel***.”
- (n) Add a new subsection 527(1.1) as follows:
 - “(1.1) A **Secondary Suite** is a ***discretionary use*** in the Residential – Grade-Oriented Infill District where one or more of the following occurs:
 - (a) it is contained within a **Rowhouse Building**; or
 - (b) there is more than one ***main residential building*** on a ***parcel***.”
- (o) Delete subsection 534(1) in its entirety.
- (p) Amend subsection 534(2) to delete “for” and replace it with “containing” and to add “**Contextual Semi-Detached Dwelling, Cottage Housing Cluster,** “ before “**Rowhouse Building**” and add “**, Semi-Detached Dwelling or Single Detached Dwelling**” after “**Rowhouse Building**”.
- (q) Amend subsection 535(1) to add “s” to the end of the word “subsection” and add “and (3)” after “(2)”.
- (r) Delete subsection 535(2) in its entirety and replace it with the following:
 - “(2) For a **Rowhouse Building** located on a ***corner parcel*** there is no maximum ***building depth*** where the ***building setback*** from the ***side property line*** shared with another ***parcel*** is a minimum of 3.0 metres for any portion of the **Rowhouse Building** located between the ***rear property line*** and 50.0 per cent ***parcel depth*** or the ***building depth*** of the ***main residential building*** on the adjoining ***parcel***, whichever is closer to the ***rear property line***.
 - (3) Where two or more ***main residential buildings*** are located on a ***corner parcel***, there is no maximum ***building depth*** for a **Duplex Dwelling, Rowhouse Building, Semi-detached Dwelling or Single Detached Dwelling** where:
 - (a) one ***main residential building*** is wholly located between the ***front property line*** and 60.0 per cent ***parcel depth***; and
 - (b) the ***building setback*** is a minimum of 3.0 metres from the ***side property line*** shared with another ***parcel*** for any portion of a ***main residential building*** located between the ***rear property line*** and 50.0 per cent ***parcel depth*** or the ***building depth*** of ***main residential building*** on the adjoining ***parcel***, whichever is closer to the ***rear property***.”
- (s) Delete subsection 537(3) in its entirety.
- (t) Renumber subsection 537(2) to make it subsection (3).

Proposed Amendments to Land Use Bylaw 1P2007

- (u) Delete subsection 537(1) in its entirety and replace it with the following:
 - “(1) Unless otherwise referenced in subsections (2) or (3), the minimum **building setback** from a **front property line** is the greater of:
 - (a) the **contextual front setback** less 1.5 metres to a maximum 4.5 metres; or
 - (b) 3.0 metres.
 - (2) On a **corner parcel**, the minimum **building setback** from a **front property line** may be reduced to:
 - (a) the **contextual front setback** at the **side property line** shared with another **parcel** to a maximum of 6.0 metres; and
 - (b) decreases in equal proportion with the increase in the distance from the shared **side property line**, to a minimum of 3.0 metres.”
- (v) Add a new section 537.1 as follows:

“Fences

537.1 The height of a **fence** above **grade** at any point along a **fence** line must not exceed 1.2 metres for any portion of a **fence** extending between the foremost front façade of the immediately adjacent **main residential building** and the **front property line**.”
- (w) Amend subsection 539(1) to delete “(8)” and replace it with “(11)”.
- (x) Delete subsection 539(7) in its entirety and replace it with the following:

“(7) For a **corner parcel**, the minimum **building setback** from a **side property line** shared with a **street** is 0.6 metres.”
- (y) Delete subsection 539(8) in its entirety.
- (z) Add a new subsection 539(10) as follows:

“(10) Unless otherwise referenced in subsection (11), on a **laned parcel** the minimum **building setback** from a **side property line** for a **private garage** attached to a **main residential building** is 0.6 metres.”
- (aa) Add a new subsection 539(11) as follows:

“(11) On a **laned parcel**, the minimum **building setback** for a **private garage** attached to a **main residential building** that does not share a **side** or **rear property line** with a **street** may be reduced to zero metres where the wall of the portion of the **building** that contains the **private garage** is constructed of maintenance-free materials and there is no overhang of eaves onto an **adjacent parcel**.”

Proposed Amendments to Land Use Bylaw 1P2007

- (bb) Amend subsection 540(1) to delete “or (3)” and replace it with “, (3) or (4)”.
- (cc) Amend subsection 540(2) to delete the “.” and replace it with “where the **building setback** from the **side property line** shared with another **parcel** is a minimum of 3.0 metres for any portion of the **Rowhouse Building** located between the **rear property line** and 50.0 per cent **parcel depth** or the **building depth** of the **main residential building** on the adjoining **parcel**, whichever is closer to the **rear property line**.”
- (dd) Renumber subsection 540(3) to make it subsection (4).
- (ee) Add a new subsection 540(3) as follows:
 - “(3) Where two or more **main residential buildings** are located on a **corner parcel**, the minimum **building setback** from a **rear property line** is 1.5 metres for a **Duplex Dwelling, Rowhouse Building, Semi-detached Dwelling or Single Detached Dwelling** where:
 - (a) one **main residential building** is wholly located between the **front property line** and 60.0 per cent **parcel depth**; and
 - (b) the **building setback** is a minimum of 3.0 metres from the **side property line** shared with another **parcel** for any portion of a **main residential building** located between the **rear property line** and 50.0 per cent **parcel depth** or the **building depth** of the **main residential building** on the adjoining **parcel**, whichever is closer to the **rear property**.”
- (ff) Renumber subsection 541(3) to make it subsection (3.1).
- (gg) Delete subsections 541(1) and (2) and replace them with the following:
 - “(1) Unless otherwise referenced in subsections (2) and (3), for a **Contextual Semi-detached Dwelling, Duplex Dwelling, Rowhouse Building, Semi-detached Dwelling or Single Detached Dwelling** the maximum **building height** is 11.0 metres measured from **grade**.
 - (2) Where a **building setback** is required from a **property line** shared with another **parcel** designated with a **low density residential district** or the M-CG District, the maximum **building height**:
 - (a) is the greater of the highest geodetic elevation of a **main residential building** on the adjoining **parcel** or 7.0 metres, measured from **grade**, at the shared **property line**; and
 - (b) increases at a 45 degree angle to a maximum of 11.0 metres measured from **grade**.
 - (3) The maximum area of a horizontal cross section through a **building** at 9.5 metres above average **grade** must not be greater than 75.0 per cent of the maximum area of a horizontal cross section through the **building** between average **grade** and 8.6 metres.”

Proposed Amendments to Land Use Bylaw 1P2007

- (hh) Amend section 542 to delete “(1)”.
 - (ii) Amend subsection 542(b) to add “and” after the semi-colon.
 - (jj) Amend subsection 542(c) to delete “; and” and replace it with “.”.
 - (kk) Delete subsection 542(d) in its entirety.
 - (ll) Amend subsection 544(1) to delete “**Contextual Semi-detached**,” and replace it with “**Contextual Semi-detached Dwelling, Duplex Dwelling**.”
 - (mm) Delete section 545 in its entirety.
 - (nn) Delete subsection 546(2) in its entirety and replace it with the following:
 - “(2) The minimum number of **motor vehicle parking stalls** for a **Secondary Suite** is reduced to 0.0 where:
 - (a) the floor area of a **Secondary Suite** is 45.0 square metres or less;
 - (b) the **parcel** is located within 600.0 metres of an existing or approved capital funded **LRT platform** or within 150.0 metres of **frequent bus service**; and
 - (c) space is provided in a **building** for the occupant of the **Secondary Suite** for storage of mobility alternatives such as bicycles or strollers that:
 - (i) is accessed directly from the exterior; and
 - (ii) has an area of 2.5 square metres or more for every **Secondary Suite** that is not provided with a **motor vehicle parking stall**.
 - (3) **Parcel coverage** excludes the **building coverage** area required by subsection (2)(c).”
2. This Bylaw comes into force two weeks after the date of approval.

Summary of Proposed Amendments

Topic	Existing Rule	Proposed Amendment
<i>Secondary Suites in Rowhouses</i>	Secondary Suites are a permitted use in Single Detached Dwellings, Semi-detached Dwellings and Rowhouse Buildings.	<p>Secondary Suites would be a permitted use in Single Detached Dwellings and Semi-detached Dwellings.</p> <p>Secondary Suites would be a discretionary use in Rowhouse Buildings and where there is more than one main residential building on parcel.</p>
<i>Parking for Secondary Suites</i>	An exemption from the parking requirement is provided for Secondary Suites and Backyard Suites that are less than 45 m ²	<p>The parking exemption would only be available for Secondary Suites and not Backyard Suites.</p> <p>Two conditions would be added for a Secondary Suite to qualify for the parking exemption:</p> <ul style="list-style-type: none"> • It must be close to an LRT platform or frequent bus route; and • Additional storage space must be provided for mobility alternatives like bikes.
<i>Rowhouse Height</i>	<p>The maximum building height of a Rowhouse Building is 11 metres measured from grade.</p> <p>The maximum building height of Single Detached Dwellings and Semi-detached Dwellings is 10 metres plus 1 metre for grading measured according to the elevation at the front or rear property line, whichever is higher.</p>	The maximum building height for all three forms of housing would be 11 metres measured from grade. This method of measuring building height has an equivalent maximum height to R-C2 and is slope adaptive.
<i>Rowhouse Height (continued)</i>	<p>The maximum building height for Rowhouse Buildings is stepped down at the side based on the height of the buildings on the neighbouring properties. These height steps are squared off.</p> <p>This rule applies only next door to some low density residential districts, such as R-C1 or R-C2, and does not apply next door to the R-CG district.</p>	<p>The maximum building height for all housing forms in R-CG would be reduced at an angle as a building gets closer to the neighbouring property based on the height of the buildings on the neighbouring properties.</p> <p>This rule would apply next door to all low density residential districts, R-CG included, as well as the M-CG district.</p>

Summary of Proposed Amendments

Topic	Existing Rule	Proposed Amendment
<i>Corner Rowhouses</i>	Rowhouse Buildings on corner parcels are allowed to be built to the full depth of the parcel to within 1.5 metres of the rear property line with only a standard 1.2 metre wide side yard setback.	Rowhouse Buildings would be allowed to be built to the full depth of the parcel to within 1.5 metres of the rear property line, but would need to provide a 3.0 metre wide side yard next to the neighbouring back yard. This allowance would be extended to apply to other combinations of buildings, such as a development with two Semi-detached Dwellings instead of just a four unit rowhouse.
<i>Setbacks from the Street</i>	A front setback that lines up with the average front yard setback on the neighbouring properties applies to all parcels next to R-C1 or R-C2. The shallowest setback allowed is 3.0 metres and there is no maximum on how far back a building might have to be setback. Next door to R-CG or a multi-residential district the minimum front setback is always 3.0 metres.	The minimum front setback would range from 3.0 metres to 4.5 metres deep, based on how far the neighbouring houses are set back from the front property line, and would apply in all cases, no matter what district is next door.
	The same front setback requirements apply to both corner and mid-block parcels.	A different front setback option would be available on corner parcels that allows the new building to be located closer to the front property line the further away it is from the property next door.
<i>Yards and Fences</i>	The minimum side yard setback on the street side of a corner parcel is 1.2 metres.	The minimum side yard setback on the street side of a corner parcel would be 0.6 metres.
	Any fence that is behind the furthest forward part of the new house is allowed to be up to 2.0 metres tall.	A fence that is built forward of the front of the neighbouring house would only be allowed to be 1.2 metres tall.
	Private yards (amenity space) required for new houses cannot be located in the front yard.	Private yards (amenity space) required for new houses would be allowed in the front yard.

Summary of Proposed Amendments

Topic	Existing Rule	Proposed Amendment
<i>Parcel Coverage</i>	<p>Single detached, semi-detached and cottage housing have a maximum building coverage ranging from 45 to 55 percent.</p> <p>Rowhouses have a maximum building coverage ranging from 45 to 60 per cent.</p>	<p>All forms of housing will have the same maximum building coverage ranging from 45 to 60 per cent.</p>
<i>Secondary Suites in Semi-detached Dwellings</i>	<p>The Secondary Suite definition requires that a Secondary Suite be on the same parcel or bare land unit as a Dwelling Unit in a Single Detached Dwelling, Semi-detached Dwelling or Rowhouse Building. This means that if a Semi-detached Dwelling Rowhouse Building is not subdivided it is not permitted to contain a Secondary Suite.</p>	<p>The Secondary Suite definition would be changed to allow a Secondary Suite to be “contained in” a Single Detached Dwelling, Semi-detached Dwelling or Rowhouse Building. This means that subdivision would not be necessary.</p> <p>The density rules for suites would also be changed to use the same language. This will not change the number of suites allowed per house, it will remain at one Backyard Suite or Secondary Suite per Dwelling Unit.</p>
<i>Suites in Multi-Residential Districts</i>	<p>The rules for multi-residential districts require that Secondary Suites and Backyard Suites in multi-residential districts meet the rules of the R-CG District. The definitions for suites do not include reference to multi-residential districts, therefore suites are not allowed with Semi-detached Dwellings in multi-residential districts, although they are allowed in R-CG.</p>	<p>Reference to multi-residential districts would be added to the definitions of Secondary Suite and Backyard Suite so that the rules of the R-CG District can be applied to suites in multi-residential districts, as intended.</p>
<i>Height of Attached Rear Garages</i>	<p>The maximum height for attached rear garages is not clear about how where the height of the building is measured from. The “building height” term used in the rule results in inconsistency between how the height of an attached rear garage is measured and how the height of a detached rear garage is measured, potentially allowing a taller attached garage.</p>	<p>The method to measure the height of an attached rear garage would be the same as the method for measuring the height of a detached rear garage.</p>

Summary of Proposed Amendments

Topic	Existing Rule	Proposed Amendment
<i>Floor Area of Garages and Sheds</i>	The total building coverage of all detached garages and sheds (Accessory Residential Buildings) is not allowed to exceed 75 m ² for Single Detached Dwellings and 150 m ² for both units in a Semi-detached Dwelling. This rule does not consider Rowhouses that will need larger garages and is more complicated than needed.	The total building coverage for all garages and sheds would be limited to 75 m ² per dwelling unit. This results in the same allowance and makes it so that the rule applies in the same way for all low density housing forms.

**Policies for Secondary Suites
in the Residential – Grade Oriented Infill (R-CG) District**

- (a) Amend the *Policy to Guide Discretion for Secondary Suites and Backyard Suites* to add the following policy at the end:

“D. Policies for Secondary Suites in the Residential - Grade-Oriented Infill (R-CG) District

1. An application for a Secondary Suite should be approved where one or more parking stalls is provided for the Secondary Suite in addition to the parking stall(s) that is required for the main Dwelling Unit.
2. Where a parking stall is not provided, a Secondary Suite may be supported in a Rowhouse Building or within a development with more than one **main residential building** where the following supporting characteristics are present:
 - a. shops, services and institutions are accessible locally;
key indicator
 - i. the suite is located close (within about 600 metres) of a Main Street or Activity Centre identified in a planning policy.
 - b. a variety of mobility options are available;
key indicators
 - i. the suite is located:
 - close to a frequent bus service (within about 150 metres);
 - close to a capital funded LRT platform (within about 600 metres);
 - close to a street containing a bicycle lane or cycle track (within about 400 metres);
 - c. the intensity added with the secondary suite is in keeping with the immediate context;
key indicators
 - i. the suite is located:
 - adjacent to a parcel designated with a land use district that allows multi-residential or commercial uses;
 - adjacent to school sites, parks or parcels designated Special Purpose District;
 - adjacent to local collector and collector roads.
3. The following polices guide the implementation of policy D.2:
 - a. Where two or more of the supporting characteristics are present, Secondary Suites may be approved in all of the Dwelling Units in a development.
 - b. Where one of the supporting characteristics is present, Secondary Suites may be approved in up to half of the Dwelling Units in a development.
 - c. Where none of the supporting characteristics are present, Secondary Suites are not supported in a development.
 - d. The key indicators itemize features that contribute to achieving the support function of the supporting characteristic described. Not all key indicators are required to be present in order to achieve the supporting function of the supporting characteristic.
 - e. The presence of other local features that serve a similar supporting function to the key indicators may be considered when evaluating whether a supporting characteristic is met.
 - f. Distances are provided as general guide and are not intended as precise measurements.”

Building Height

Figure 1: Method for measuring the height of a single or semi-detached dwelling on a flat site in the low density residential districts

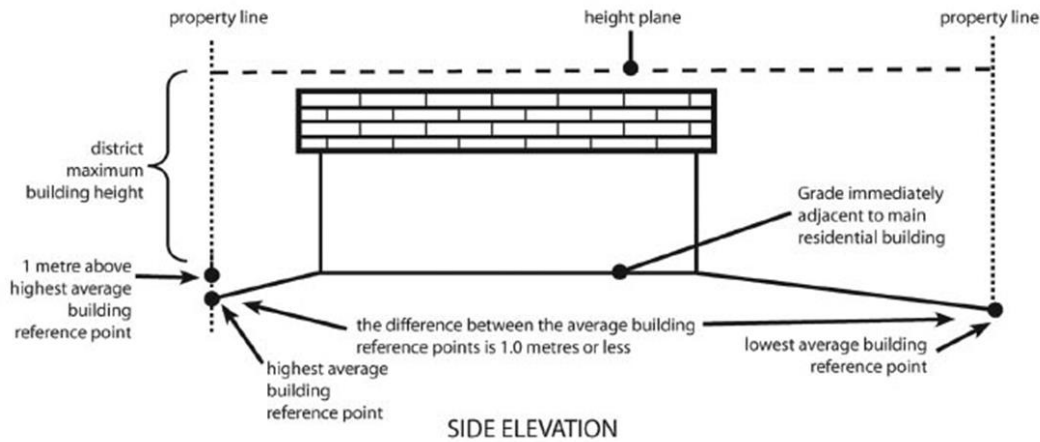
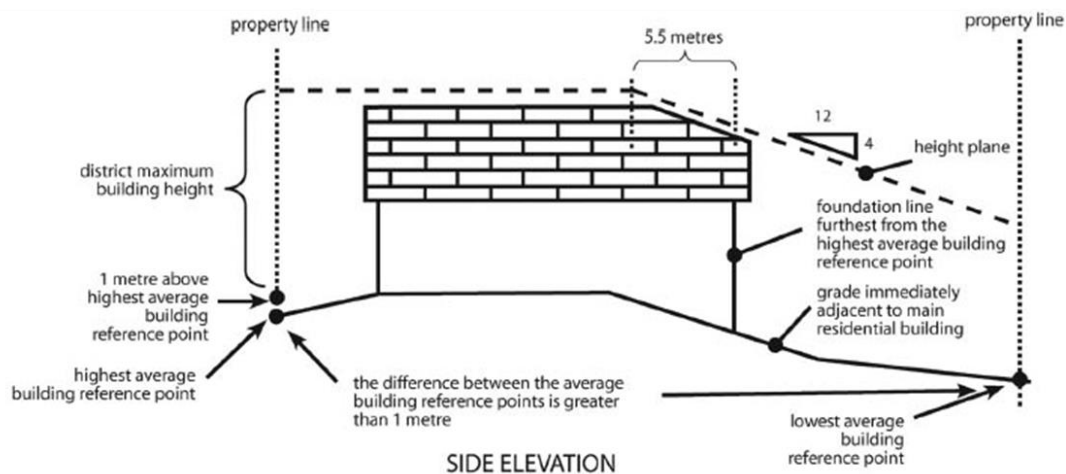


Figure 2: Method for measuring the height of a single or semi-detached dwelling on a sloping site in the low density residential districts



Building Height

Figure 3: Method for measuring building height from grade for rowhouses in the R-CG District

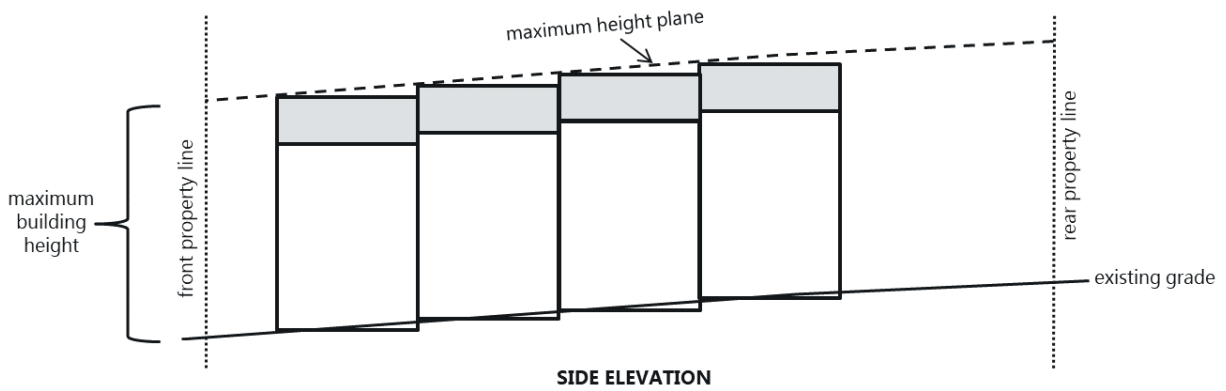
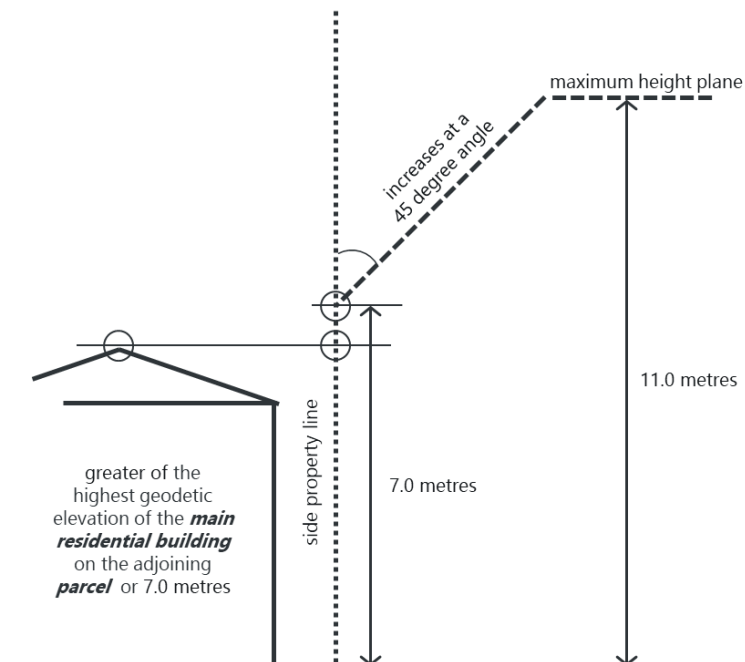
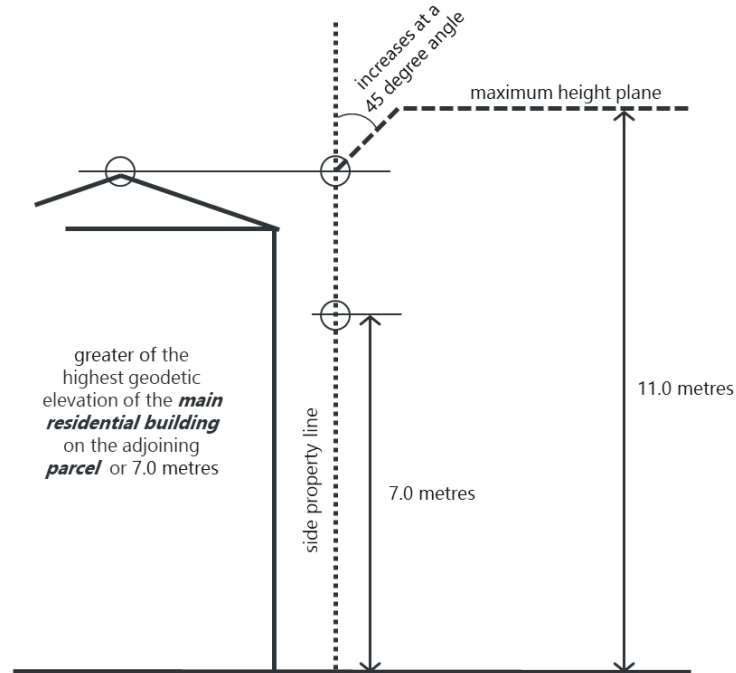


Figure 4: Proposed height chamfer next to a building under 7 metres



Building Height

Figure 5: Proposed height chamfer next to a building over 7 metres



Proposed Amendments on Corner Parcels

Figure 1: Buildable area applying proposed R-CG rules

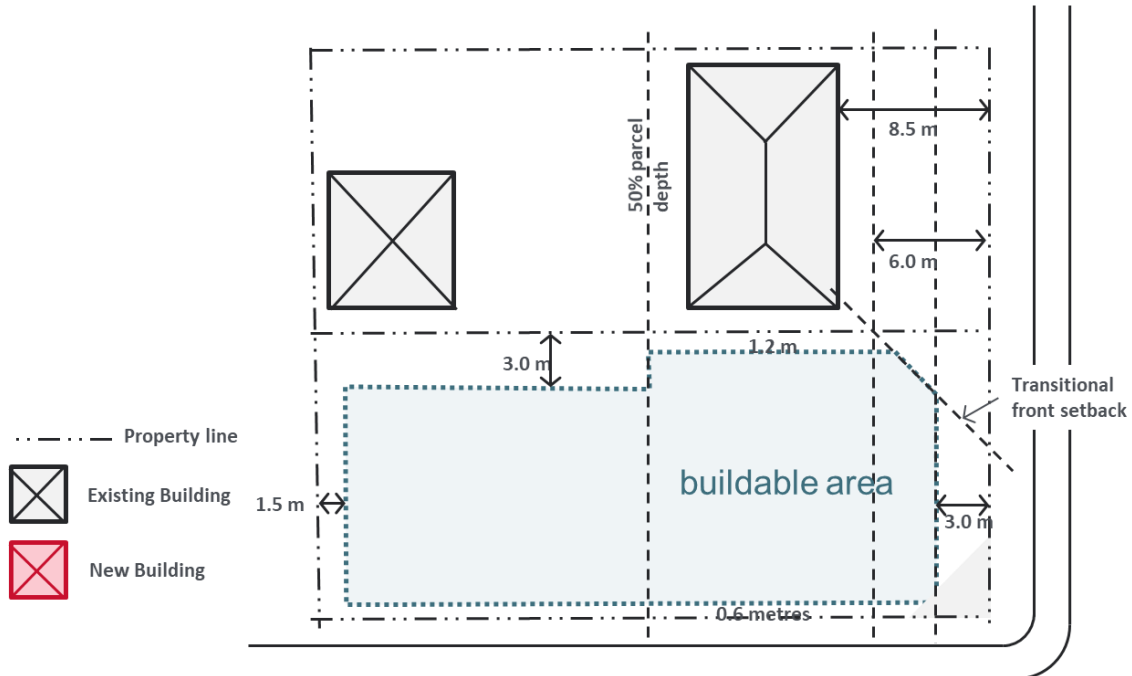
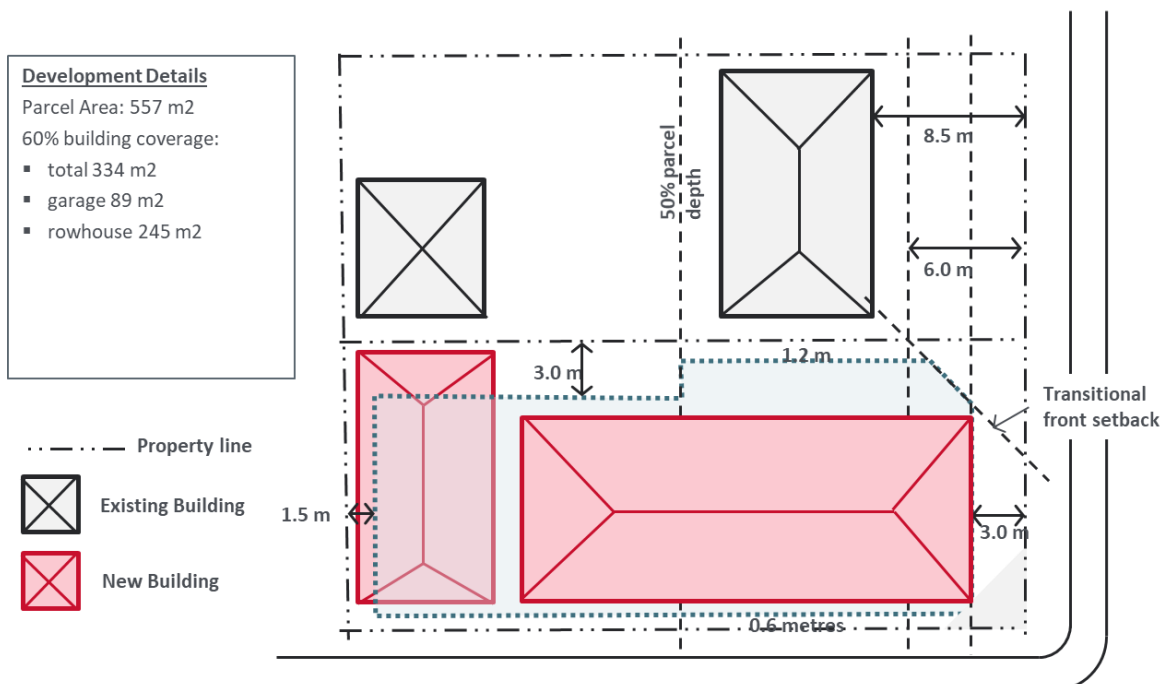


Figure 2: Rowhouse Building applying proposed R-CG rules



Proposed Amendments on Corner Parcels

Figure 3: Two Semi-detached Dwelling development applying proposed R-CG rules

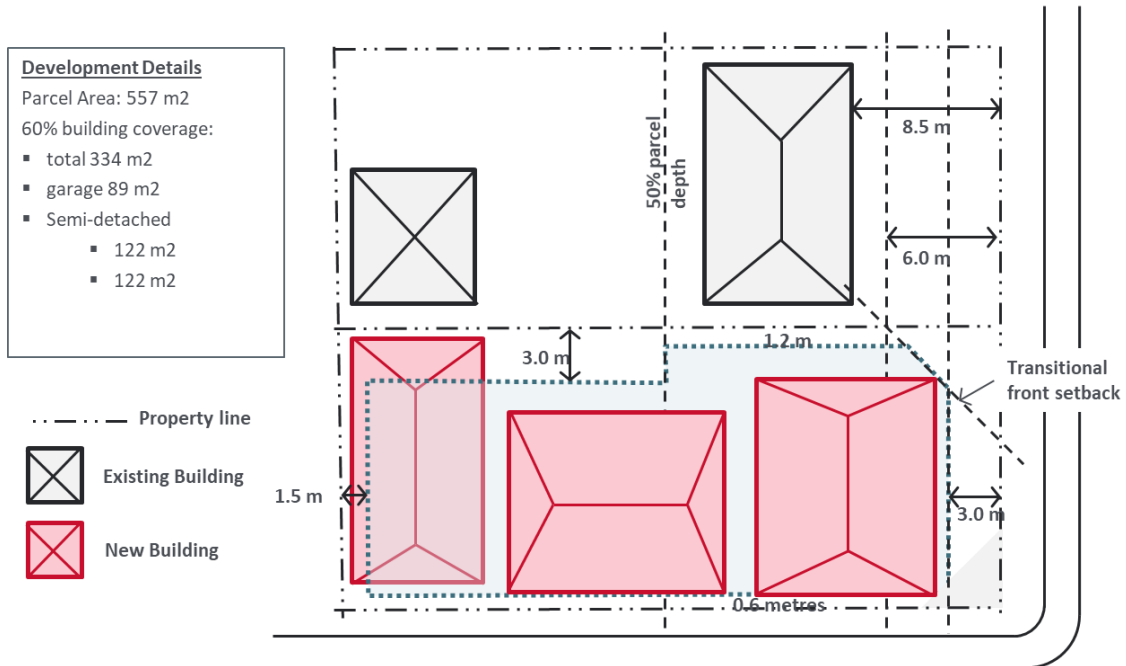
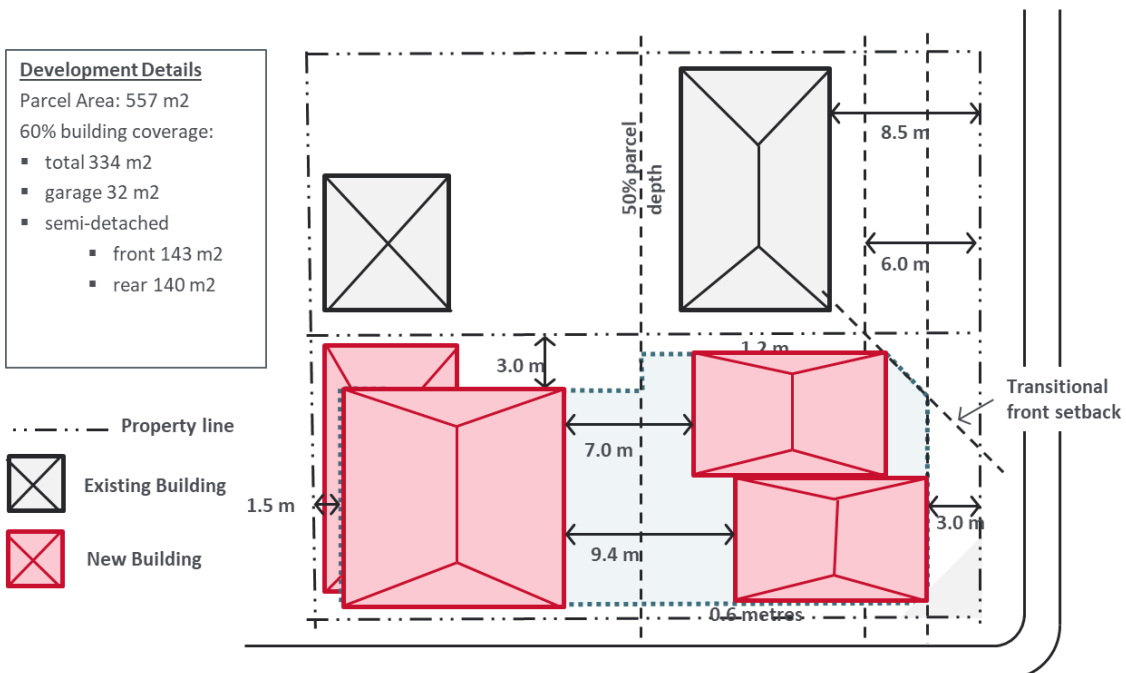


Figure 4: Courtyard style, two semi-detached development applying proposed R-CG rules



Proposed Amendments on Corner Parcels

Figure 5: Buildable area applying proposed R-CG rules adjacent to a new Semi-detached Dwelling

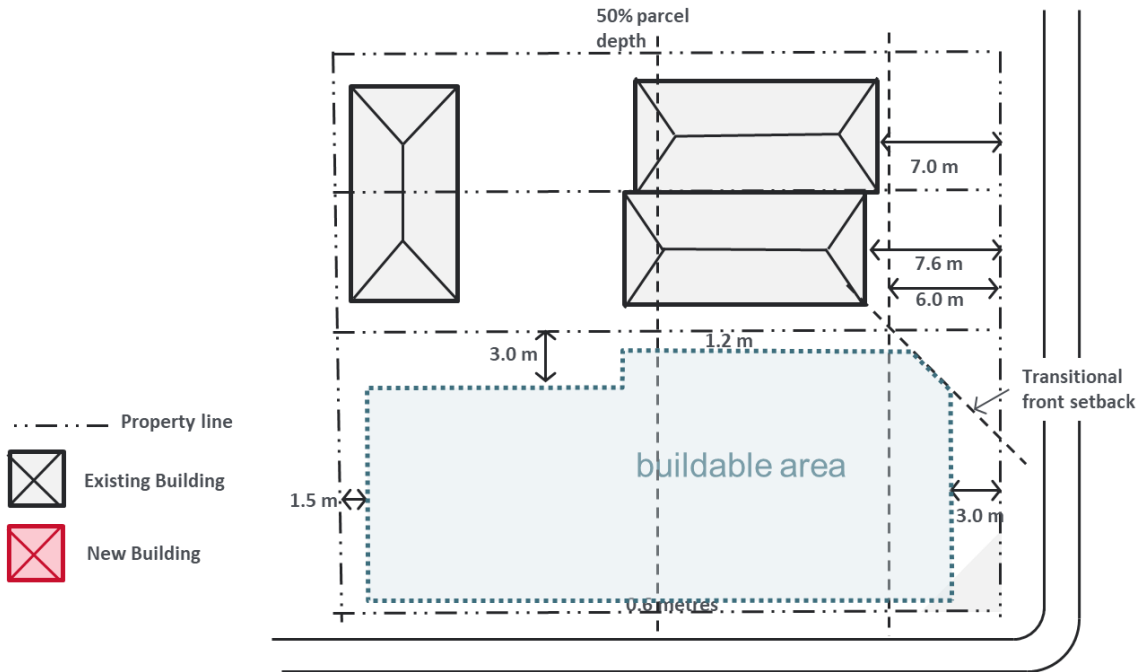
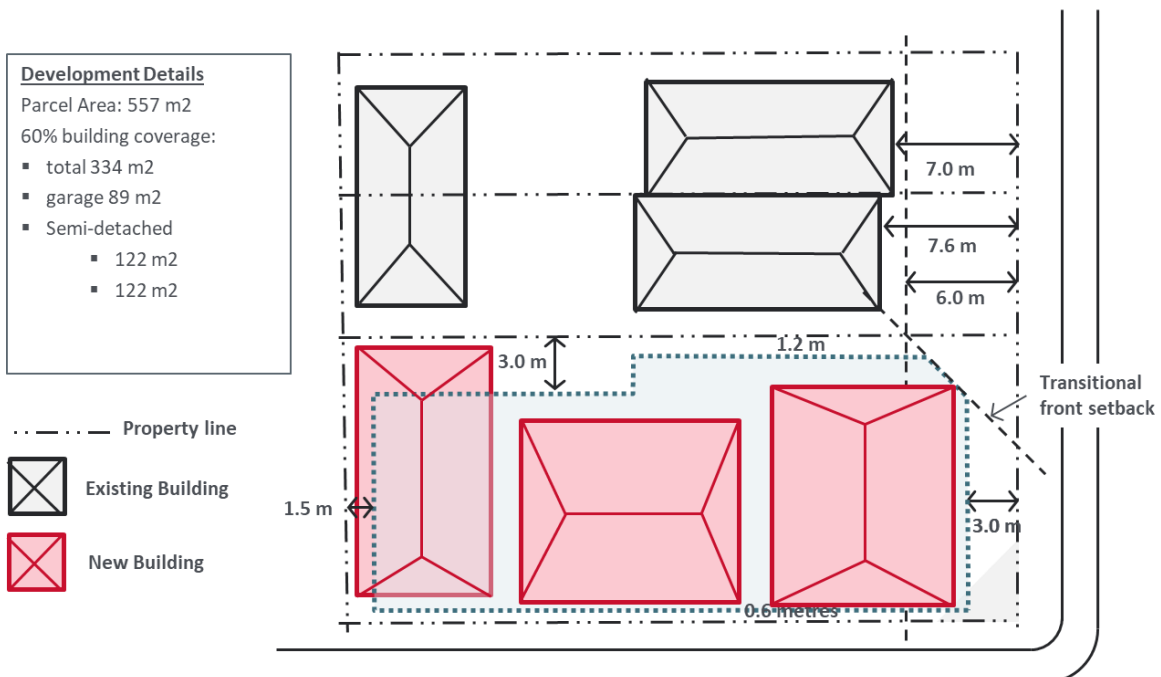


Figure 6: Two Semi-detached Dwelling development applying proposed R-CG rules adjacent to a new Semi-detached Dwelling



Front Yard Setbacks

Figure 1: Proposed transitional front setback on a corner parcel
(neighbouring setback of 6 metres or more)

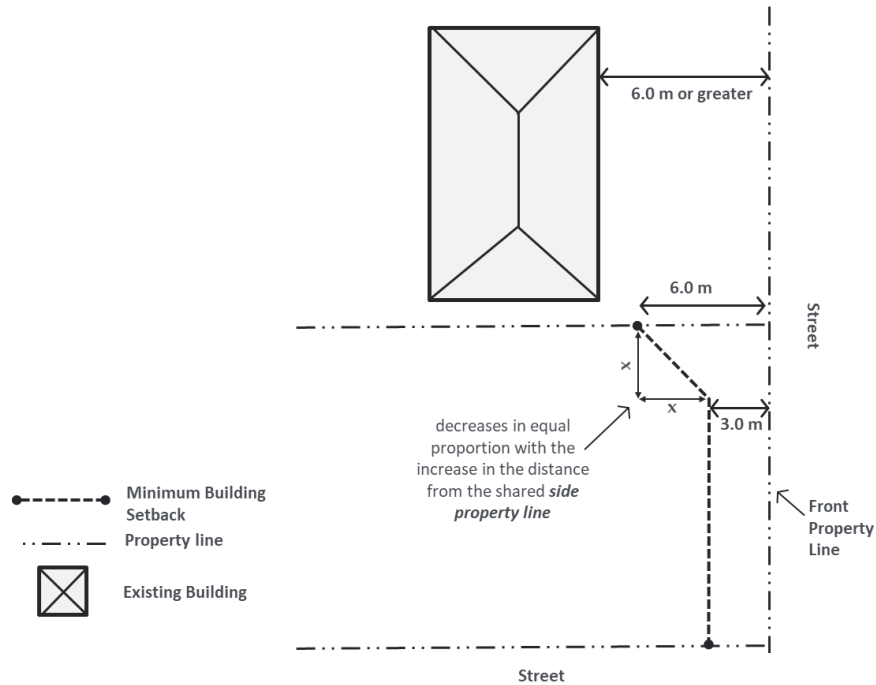
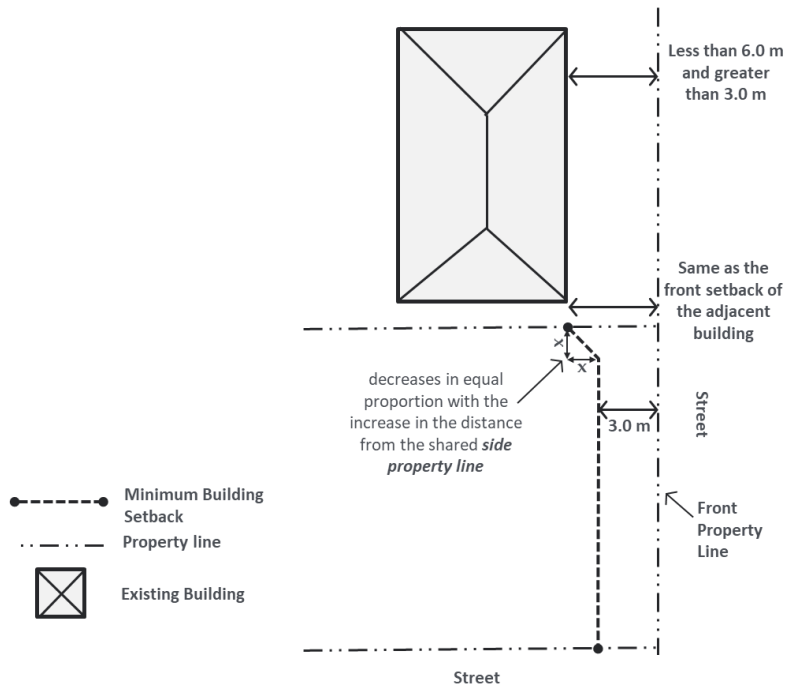


Figure 2: Proposed transitional front setback on a corner parcel
(neighbouring setback of 3 to 6 metres)



Front Yard Setbacks

Figure 3: Proposed transitional front setback on a corner parcel
(neighbouring setback of 3 metres or less)

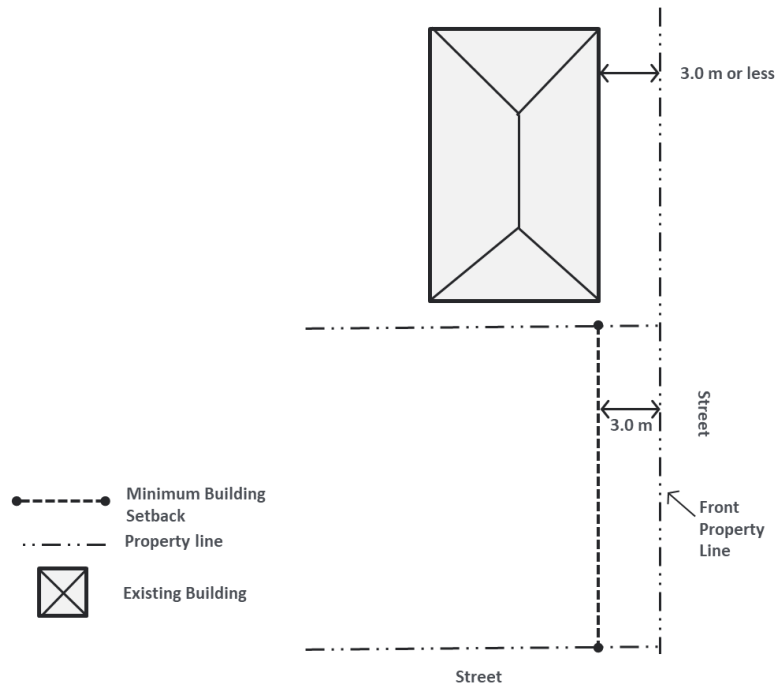
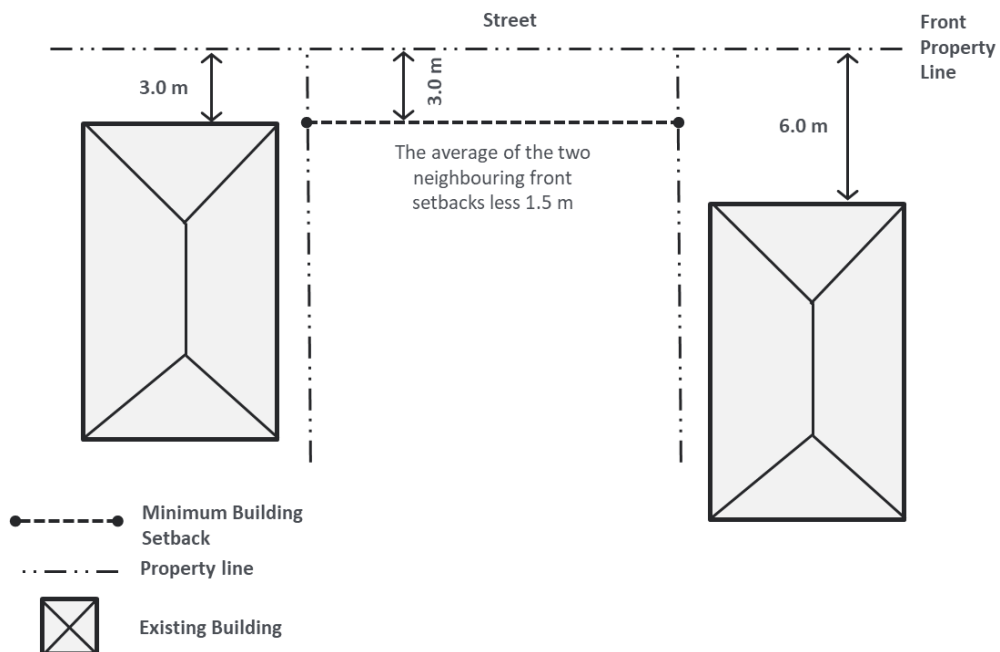
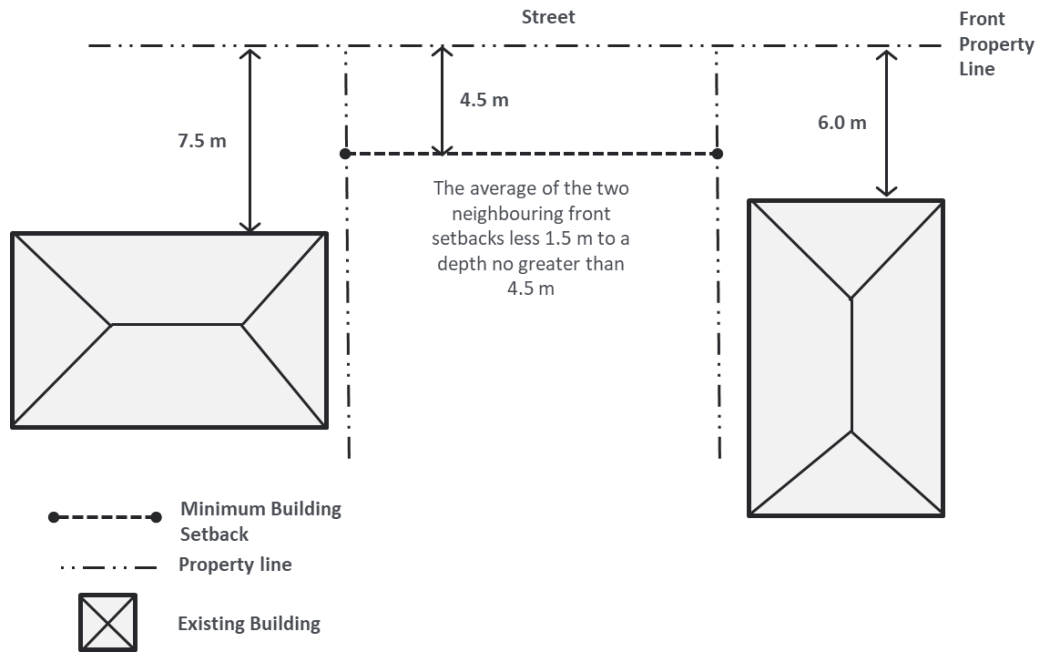


Figure 4: Proposed front setback mid-block with shallower neighbouring front yards



Front Yard Setbacks

Figure 5: Proposed front setback mid-block with deeper neighbouring front yards



Working Group Members

Steve Barnecut	Architect – Industry Representative
Joe Belland	Community Member – Bridgeland/Riverside
Paul Bergmann	Builder – Industry Representative
Mike Borkristl	Builder – Industry Representative
Judy Hoad	Community Member - Parkdale
Bev Jarvis	Director of Policy, Projects & Government Relations - BILD - Calgary Region
Ali McMillan	Community Member – Bridgeland/Riverside
Jennifer Miller	Planner – Federation of Calgary Communities
Ben Morin	Planner – Federation of Calgary Communities
James Reid – Stepped down after 2 meetings	Community Member - Rosedale
Carrie Yap	Planner – Federation of Calgary Communities

Letters from Working Group Members



To: Calgary Planning Commission
From: Paul Bergmann, Winchester Builders
Date: July 19, 2018
Re: Infill Review Working Group

Over the last 6 months I have spent approximately 20 hours in 10 meetings with Community Representatives, City Planners and fellow Building Industry members in discussions led by the City. The intent was to address various Bylaw concerns and issues which seem to be recurring and are seemingly causing friction for some stakeholders.

Specifically, the issues fell into two Categories:

1. Changes to Front Porches and Subterranean Development:
2. Changes to the R-CG District.

In both of these instances, we held fulsome, open discussions about the issues which were important to each of the Stakeholders. I was heartened to see that these discussions were held in a spirit of cooperation, understanding and learning. All of the meetings and discussions were positive, with all parties willing to listen and understand the other Stakeholders.

More specifically, I believe all parties were heard. If someone had a strong opinion on a particular issue, they were allowed to fully develop that idea to allow the other stakeholders to understand their concerns – and the other parties listened. There was then a respectful discussion about how that strong opinion impacted other stakeholders and what some reasonable compromises might look like.

I believe the proposed changes to the Front Porches and Subterranean development, as well as the proposed changes to the R-CG District strike a good balance between the various stakeholder's interests. As a Builder, I can say that while we did not get everything we might have wished for, the proposed changes are a good compromise. They allow for the increase in density the

Letters from Working Group Members

Municipal Development Plan is looking to achieve while being respectful of the existing Homeowners in the Developed Areas. I wholeheartedly support the proposed changes.

I would also like to compliment the City Employees who ran this Stakeholder process. They were professional and Open minded. As good Facilitators, they listened more than they spoke. They did not impose their will on the Community or Building Industry Members, but instead sought to listen and reach reasonable compromises that addressed the issues which had been raised. Kudos to Stephen Pearce and Tammy Henry for running this highly effective and high functioning process.

It was an honour to participate in this process; I appreciate the opportunity,

Pal Bergmann
Winchester Builders

Letters from Working Group Members

inertia

Inertia Corporation
1140-B 44th Avenue SE
Calgary, AB T2G 4W6

Friday, July 13, 2018

Calgary Planning Commission
Calgary Municipal Building
P.O. Box 2100, Stn. M
Calgary, Alberta, Canada T2P 2M5

Re: Infill Review Working Group

Since February 1, 2018 I have been a member of the Infill Review Working Group that has been coordinated by Tammy Henry and Stephen Pearce. It is a group composed of industry, community and City representatives. I have been designing infill developments in Calgary since 1999, and brought to the group my experience of designing within the context the Land Use Bylaw (LUB).

The discussions fell into two categories. We discussed encouraging front porches in infill development, and we discussed subterranean development beyond the extent of above grade building.

Front Porches and Subterranean Development

There was general agreement among all parties in the Group that front porches are a desirable feature of infill development, and that the LUB had to be more accommodating of front porches. The current rules discourage porches on most infill parcels by requiring that houses accept a reduction in livable area (and salable area) equivalent to the size of a porch. The proposed rules are well crafted and will result in more porches in Calgary's established areas.

There was less agreement regarding subterranean development. The Group found it difficult to define the problem that restricting such development would solve. There is also a chance that the implementation of these restrictions may have an unpredictable impact on typical underground construction. Further, if the rationale of restricting development under the front setback is to provide growing material for street trees, this would not be fair to homeowners and developers if the City did not actually plant and maintain a street tree. Currently this cannot be assured as the City will not take the initiative to plant a street tree if it is not between a sidewalk and a curb.

Though one of the reasons that subterranean development should be restricted may be the maintenance of proper drainage and stormwater management, many of us in the Working Group felt that this is an engineering concern and that the LUB was not the place to regulate drainage or stormwater management.

Inertia Corporation • (403) 464-7721 • inertia@architecture.ca
1140-B 44th Avenue SE, Calgary, AB T2G 4W6

Letters from Working Group Members

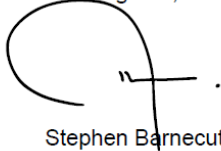
Changes to the R-CG District

The R-CG district is critical to diversifying housing options in Calgary's established areas and to Calgary's sustainable growth. It allows a modest increase in density in appropriate infill locations, and has proven to be popular among builders and home purchases.

Members of the Working Group were all supportive of the district and all wanted to ensure that changes improved development outcomes without discouraging the development of R-CG projects. The proposed changes to the R-CG district satisfy these expectations.

I would like to express my thanks to members of the Working Group for the productive conversations we've had over the past year and a half. I look forward to continuing the conversation with them and other stakeholders as we all seek to make Calgary a better place to live and work.

Best regards,

A handwritten signature in black ink, appearing to read 'Stephen Barnecut', with a large circular flourish on the left side.

Stephen Barnecut
Director
Inertia Corporation

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0902
Page 1 of 7

Land Use Amendment in North Glenmore Park (Ward 11) at 5315 - 19 Street SW, LOC2018-0057

EXECUTIVE SUMMARY

This land use redesignation application was submitted by CivicWorks Planning + Design on 2018 March 12 on behalf of the landowner David A Johnston. This application proposes to redesignate a single residential parcel from Residential – Contextual One Dwelling (R-C1) District to Residential – Grade-Oriented Infill (R-CG) District to allow for:

- rowhouses in addition to building types already allowed in the current district (e.g. single-detached dwellings);
- a maximum building height of 11 metres (an increase from the current maximum of 10 metres);
- a maximum of four dwelling units (an increase from the current maximum of one dwelling units); and
- the uses listed in the R-CG designation.

This redesignation is intended to accommodate a four-unit rowhouse development. An associated development permit application, DP2018-2259, is under review (Attachment 3). The proposal conforms to applicable policies of the *Municipal Development Plan* and meets four of the eight *Location Criteria for Multi-Residential Infill*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing, and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares ± (0.14 acres ±) located at 5315 – 19 Street SW (Plan 3401HR, Block 30, Lot 12) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Grade-Oriented Infill (R-CG) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

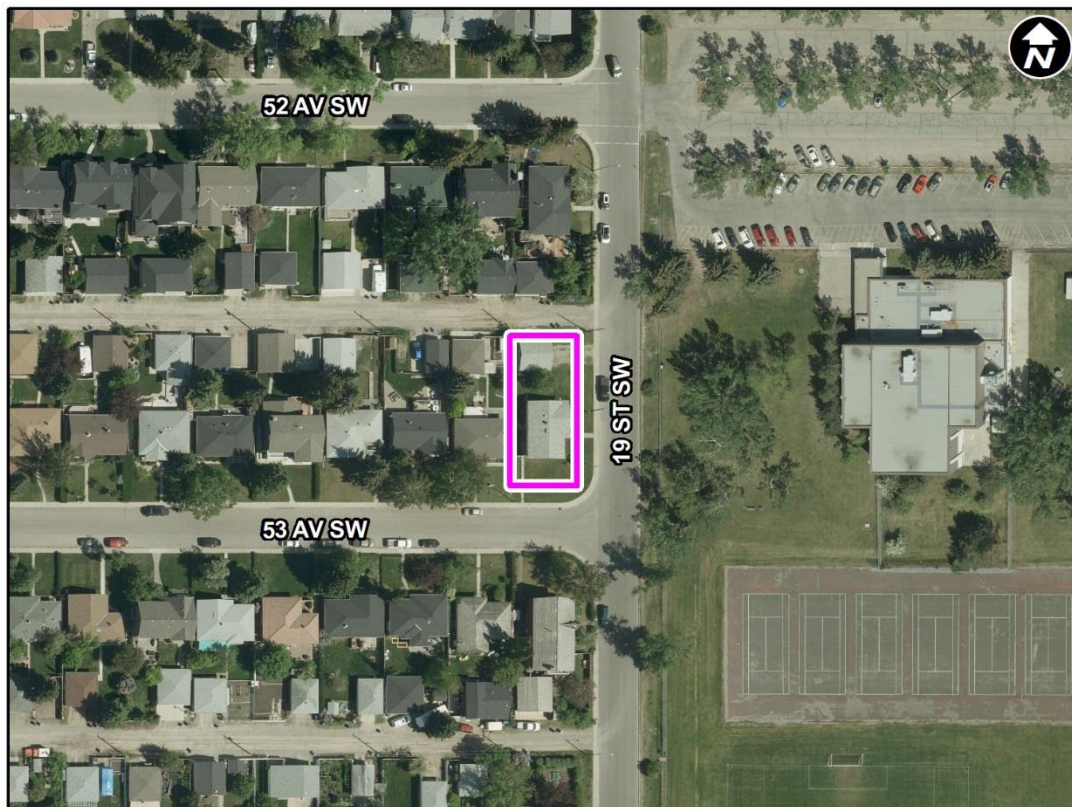
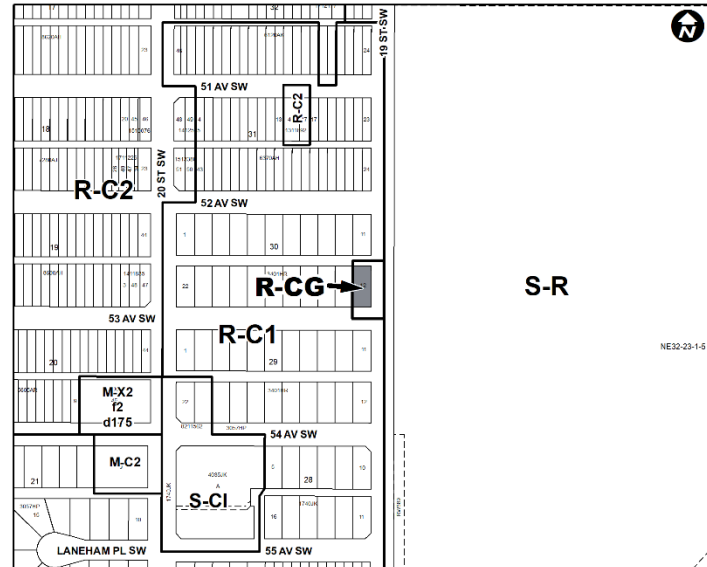
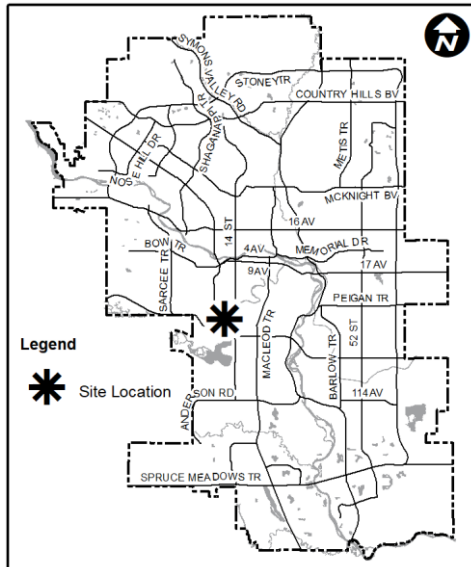
**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0902
Page 2 of 7**

**Land Use Amendment in North Glenmore Park (Ward 11) at 5315 - 19 Street SW,
LOC2018-0057**

BACKGROUND

Location Maps



Land Use Amendment in North Glenmore Park (Ward 11) at 5315 - 19 Street SW, LOC2018-0057

Site Context

The subject site is located in the community of North Glenmore Park, on the northwest corner of 53 Avenue and 19 Street SW. Surrounding development is characterized by low-density residential development in the form of single detached housing, with the predominate land use of Residential – Contextual One Dwelling (R-C1) District in the near vicinity. Glenmore Athletic Park is located directly across 19 Street SW.

The site is approximately 0.06 hectares in size with approximate dimensions of 16.0 metres wide by 36.0 metres deep. A rear lane exists to the north of the site. The site is currently developed with a one storey, single detached dwelling, with access to a rear yard garage via 19 Street SW.

As identified in *Figure 1*, the community of North Glenmore Park reached its peak population in 1970 with 3,776 residents. The current population for the community is 2,396, a decline of 1,380 residents from peak population.

Figure 1: Community Peak Population

North Glenmore Park	
Peak Population Year	1970
Peak Population	3,776
2017 Current Population	2,396
Difference in Population (Number)	1,380
Difference in Population (Percent)	-37%

Source: The City of Calgary 2017 Civic Census

Additional demographic and socio-economic information can be obtained online through the [North Glenmore Park](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal represents a modest density increase of an established area parcel of land and allows for development that will be compatible with the low-density residential character of the existing neighbourhood.

Application Review

The applicant submitted a land use redesignation and associated development permit application on 2018 March 12 and 2018 May 10 respectively. The development permit (DP2018-2259) is for a permitted use Rowhouse Building. The permit has been reviewed by Administration, and Administration is ready to recommend approval of the development permit, pending Council's approval of the land use.

**Land Use Amendment in North Glenmore Park (Ward 11) at 5315 - 19 Street SW,
LOC2018-0057**

Land Use

The subject site is currently designated Residential – Contextual One Dwelling (R-C1) District, which is intended for development in the form of single detached dwellings in developed areas of the city. The district allows for a maximum of one dwelling unit on site and a building height of 10 metres.

The proposed Residential – Grade-Oriented Infill (R-CG) District is a low density residential designation that is primarily for two to three storey (11 metres maximum) rowhouse developments where the façade of each dwelling unit must directly face a public street. At the maximum permitted density of 75 units per hectare, this site could accommodate up to four dwelling units.

The R-CG District also allows for a range of other low-density housing forms such as single detached, semi-detached and duplex dwellings. Secondary suites (one Backyard Suite or Secondary Suite per unit) are also allowable in R-CG developments. Secondary suites do not count against allowable density and do not require motor vehicle parking stalls, when proposed in the R-CG District, provided they are below 45 square metres in size.

Infrastructure

Transportation Networks

Pedestrian and vehicular access to the site is available from 19 Street SW. Pedestrian and vehicular access to the site is available from 19 Street SW. However, at the time of redevelopment, vehicular access will be required from the lane. The area is served by Calgary Transit bus service, with stops located approximately 230 metres walking distance from the subject site providing service to the downtown core. On-street parking by the site is restricted by permit for residents only. A Transportation Impact Assessment was not required as part of this application.

Utilities and Servicing

Water, sanitary and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. Individual servicing connections, as well as appropriate storm-water management will be considered and reviewed at development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

**Land Use Amendment in North Glenmore Park (Ward 11) at 5315 - 19 Street SW,
LOC2018-0057**

The North Glenmore Park Community Association was circulated on this application. After considering both the land use and development permit applications, the Association responded with a letter in opposition of the application (Attachment 2). The main concern highlighted was that this application does not meet the criteria they have established as a Community Association regarding appropriate locations for R-CG development, specifically that this site is not on a collector road and that it is not zoned R-C2.

Administration received fourteen letters of opposition to the application. Reasons stated for opposition are summarized below:

- concern over increased traffic congestion in the area;
- concern that this redesignation is from an R-C1 parcel to R-CG as opposed to an R-C2 parcel to R-CG, citing that the jump from one to four units is significant and will have a more immediate impact on the neighbourhood;
- parking concerns, especially along 19 Street SW at peak times when the Glenmore Athletic Park is being heavily used;
- concern that this application does not fit in with the character and context of the community;
- concern that the R-C1 area will be eroded away with redesignations, losing a balance of single dwelling units as well as duplex dwellings and multi-residential units;
- concern that this application is not compatible with the intent of the R-C1 district which is the designation for the balance of homes in the area; and
- concern that the location of the application is not appropriate for increases in density, as it is not located on a collector road, adjacent to commercial or other multi-residential development.

Administration has reviewed the relevant planning concerns specific to the proposed redesignation. The applicant will be required to provide on-site parking at the time of Development Permit, which will help alleviate parking concerns. The City's policy direction is to support higher density redevelopment in the inner-city, as this will allow for a mix of densities and types of housing within individual blocks.

Engagement

The applicant, CivicWorks Planning + Design, in collaboration with RNDSQL, engaged the community through on-site signage, a project website and a postcard drop to surrounding neighbours and adjacent property owners. The postcards outlined the proposed land use redesignation and referenced the corresponding project website. The signage placed on-site outlined the proposed land use change and encouraged interested community members to submit feedback.

The applicant also hosted an information session for members of the community, providing plans and renderings of the associated DP application, and an opportunity for further discussion and questions of clarification to be asked.

**Land Use Amendment in North Glenmore Park (Ward 11) at 5315 - 19 Street SW,
LOC2018-0057**

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory – 2009)

The subject site is located within the ‘Residential, Developed: Established’ area as identified on Map 1: Urban Structure of the *Municipal Development Plan* (MDP). The applicable MDP policies encourage redevelopment of established area communities that is similar in scale and built form to existing development, including a mix of housing such as townhouse and rowhousing. The MDP also calls for modest intensification of the established area, an area serviced by existing infrastructure, public amenities and transit. The proposal is in keeping with relevant MDP policies, as the rules of the R-CG District provide for redevelopment form that will be sensitive to existing residential development in terms of height, built form and density.

Location Criteria for Multi-Residential Infill (Non-statutory – 2014)

The *Location Criteria for Multi-Residential Infill* is a tool for review of redesignation applications in the developed areas. The criteria are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP or local area policy plans, to assist in determining the appropriateness of an application in the local context.

The proposed land use aligns with four of the eight criteria. The site is on a corner parcel with direct lane access. It is within 230 metres walking distance from a transit stop and is located directly across from the Glenmore Athletic Park. Criteria not met include being within 600 metres of a BRT stop, being on a collector or higher standard road, being along a corridor or activity centre and located beside a non-residential or multi-unit development.

Moderate intensification in this location has minimal impact on adjacent properties and is therefore considered appropriate.

Social, Environmental, Economic (External)

The recommended land use allows for a wider range of housing types than the existing R-C1 District. Therefore, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics.

An environmental site assessment was not required for this application.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0902
Page 7 of 7

**Land Use Amendment in North Glenmore Park (Ward 11) at 5315 - 19 Street SW,
LOC2018-0057**

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and, therefore, there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the *Municipal Development Plan*. The proposed R-CG District was designed to be implemented in proximity or adjacent to low-density residential development. The proposal allows for a range of building types that have the ability to be compatible with the established building form of the existing neighbourhood and can better accommodate the housing needs of different age groups, lifestyles and demographics.

ATTACHMENT(S)

1. Applicant Submission
2. Community Association Letter
3. Development Permit Drawings (DP2018-2259)

Applicant's Submission



460 - 5119 Elbow Drive SW
Calgary, Alberta T2V 1H2

P 403 201 5305
F 403 201 5344

MARCH 08, 2018

City of Calgary
Planning and Building
800 MacLeod Trail SW
Calgary AB T2P 2M5

RE: Land Use Redesignation from R-C1 to R-CG: 5315 - 19 ST SW | Lot 12, Block 30, Plan 3401HR

The subject parcel is located in the community of North Glenmore Park and consists of 0.0565ha of privately owned land. RNDSOR Inc. has retained CivicWorks Planning + Design Inc. to undertake a land use redesignation process to facilitate the construction a 2-storey four-unit Rowhouse Building with front doors facing 53 Avenue SW and 19 Street, a four-bay garage structure and no provisions for secondary suites on site. The proposed use is well-suited to the site, given its surrounding context, lot characteristics and location.

The site's current R-C1 (Residential – Contextual One Dwelling) District allows for primarily single-detached dwellings and limits this parcel to one household regardless of configuration. In support of the proposed development, this application seeks to amend the existing R-C1 (Residential – Contextual One Dwelling) District to a R-CG (Residential – Grade-Oriented Infill) District.

Like R-C1, the R-CG District is a Low Density Residential District intended to facilitate grade-oriented development. The intent of the R-CG District is to; accommodate grade-oriented development in the form of Rowhouse Buildings, Duplex Dwellings, Semi-detached Dwellings and Cottage Housing Clusters; allow Secondary Suites and Backyard Suites with new and existing residential development; provide flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing over time; and accommodate site and building designs that are adaptable to the functional requirements of evolving household needs.

The subject lands do not fall within the boundaries of any Local Area Plans and are governed by higher level, city-wide policy like the Municipal Development Plan (MDP) and Developed Areas Guidebook (DAG), which support greater housing choice and reinforce more complete and resilient residential neighbourhoods. The MDP identifies ground-oriented housing as a key component of complete communities and encourages growth and change in low density residential neighbourhoods through the addition of a diverse mix of ground-oriented housing options.

PLANNING RATIONALE

The subject site features numerous characteristics that make it especially appropriate for the proposed R-CG land use change, which will directly facilitate the development of new and innovative inner-city housing options for Calgarians:

Corner Lot: The subject site occupies a corner lot, allowing the proposed development to contribute to the neighbourhood streetscape by addressing both 53 Avenue and 19 Street SW with grade-oriented unit entrances.

Direct Lane Access: The subject site enjoys direct lane access, facilitating a development that orients vehicle access to the rear lane, creating an uninterrupted, pedestrian-friendly streetscape interface along 53 Avenue and 19 Street SW.

Collector Road: The subject site is located along a Collector standard road, ensuring both ease of access and traffic capacity for future residents.

Proximity To Transit: The subject site is ~225m from two transit stops (Route 7 & 107), and is also within ~800m of a primary transit stop (Route 7, 18, 20, 63, 72, 107, 182, 306) along the Crowchild Trail Primary Transit Network corridor. Calgary's Primary Transit Network provides communities with daily reliable public transit service, with a frequency of every 10 minutes or less, 15 hours a day, 7 days a week.

North Glenmore Park Community Association



NORTH GLENMORE PARK COMMUNITY ASSOCIATION

2231 Longridge Dr. SW Calgary, AB T3E 5N5 403-246-4243

www.ngpca.ca

June 19, 2018

Circulation Control
Planning & Development
PO Box 2100 Station M
IMC8201

Attention: Colleen Renne-Grivell, File Manager:

By email to: Colleen.Renne-Grivell@calgary.ca

Dear Ms. Renne-Grivell;

Re: 5315 – 19th Street SW (LOC2018-0057; Amendment from RC-1 to R-CG)

Further to the North Glenmore Park Community Association's (NGPCA) and Planning & Area Redevelopment Committee's (PARC) response of April 6, 2018, we affirm our stated opposition regarding the proposed re-designation application at 5315 -19th Street SW (LOC2018-0057).

Council recently approved similar R-CG re-designations within our community at the following two locations:

- 2103 – 53rd Avenue SW (LOC2018-0022)
- 5102 – 20th Street SW (LOC2017-0380)

The only other example of R-CG development in the community occurred several years ago at 5404, 5406, 5408 and 5410 - 21st Street SW. Our community experience with this application has been mixed, with concerns raised by proximate residents about spill-over parking from the site and the number and management of garbage collection bins on the laneway.

There was a well-attended public engagement session at our community hall on Monday June 11th which involved sponsorship from the NGPCA, the City of Calgary, the applicant (RNSQR) and their planning consultant (CivicWorks).

It is our view that while the two recent applications satisfied a majority of City Council's criteria for locating multi-residential development (including R-CG) into low density communities, primarily as they were located on bus-route corridors and within and zoned as an R-C2 district, similar factors are not evident in the current application.

North Glenmore Park Community Association

The City's "Location Criteria for Multi-Residential Infill" ("LCMRI"; PUD2015-0364; PUD2016-0405 Att 1) offer up some criteria that may be considered as a "guideline" in considering an R-CG rezoning:

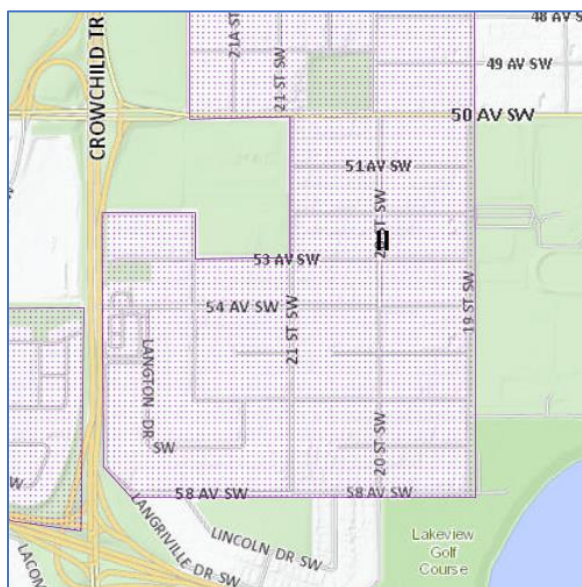
These criteria are not meant to be applied in an absolute sense to determine whether or not a site should be recommended for approval. In general, the more criteria an application can meet, the more appropriate the site may be considered for multi-residential infill development. In some cases, there may be applications that are appropriate but meet only a few criteria, or may meet multiple criteria but are determined not to be appropriate. These will need to be considered based on the scale and type of development proposed in relation to the local context.

The attached LCMRI schedule outlining the 8 suggested criteria for such developments confirms that the proposed site satisfies 4 of the 8 criteria, but equally fails to satisfy half the criteria:

- It is not on a Collector or Higher Standard roadway on either frontage (19th Street or 53rd Avenue). 19th Street and 53rd Avenue are 9.6 m wide from gutter to gutter compared to the minimum required 12.3 m of a designated Collector Road. This poses overcrowding and traffic safety concerns.
- It is not within 600m of a BRT stop.
- It is not along a corridor or an activity center.
- It is not beside or anywhere near a non-residential or multi-unit development.

Additionally, local context is important in land use amendment applications such as this. Important to this application are the following further considerations:

- Parking. The west side of adjacent 19th Street is identified as a restricted Residential Parking Permit Zone "II". The proximity of our community to the Glenmore Athletic Park and to Central Memorial High School supported the need for this restricted parking area. The many public sporting activities in Glenmore Athletic Park put the neighbourhood parking at a premium, particularly on weekends.



North Glenmore Park Community Association

- Community context. This would be the first successful application in the community in placing a four or five plex row house immediately adjacent to an R-C1 district. The three previous approvals have been within the R-C2 land use district. Our community is concerned that while R-CG is considered to be "low density residential", it should respectfully transition to the existing housing stock. This application fails to do so.

The proposed does not meet the objectives in the MDP for Infill Redevelopment 2.2.5 – Bylaw 19P2017. *The City promotes infilling that is sensitive, compatible and complementary to the existing physical patterns and character of neighbourhoods.* This application is located in a predominantly R-C1 neighbourhood and does not meet a substantial amount of City of Calgary location criteria. Quadrupling density for the lot by rezoning R-C1 to R-CG would be an unprecedented, abrupt change.

For all the above reasons, our community does not support the current application.

Sincerely,

Jennifer McClure
Chair, Planning & Area Redevelopment Committee
North Glenmore Park Community Association
Copies:
Ward 11 Councillor Jeromy Farkas
Ward 08 Councillor Evan Wooley
CivicWorks Planning (David White)

North Glenmore Park Community Association

LOC2018-0057 (5315 - 19TH STREET SW)

PUD2015-0364
ATTACHMENT 1

Location Criteria for Multi-Residential Infill (as adopted 2014 March)

In order to assist in the evaluation of land use amendment applications and associated local area plan amendments, the following criteria shall be applied and reported on in Administration reports to Calgary Planning Commission. These criteria are not meant to be applied in an absolute sense to determine whether or not a site should be recommended for approval. In general, the more criteria an application can meet, the more appropriate the site is considered for multi-residential infill development (all other things being considered equal). The following table represents a proposed checklist for preferred conditions to support land use amendments in low density residential areas. It is to be used in the review and evaluation of land use amendment applications for the following districts or direct control districts based on the following districts:

Multi-residential – Contextual Grade-Oriented (M-CG) District

Multi-residential – Contextual Low Profile (M-C1) District

Multi-residential – Contextual Medium Profile (M-C2) District

Subject Site	Comments
On a corner parcel.	Corner developments have fewer direct interfaces with low density development. Corner sites avoid mid-block development that could signal speculation that the entire block is appropriate for redevelopment.
Within 400m of a transit stop.	Allows for greater transit use, providing more mobility options for residents of multi-dwelling developments. Can reduce motor vehicle usage, thereby minimizing vehicle traffic impact on community.
Within 600m of an existing or planned Primary Transit stop or station.	Allows for greater transit use, providing more mobility options for residents of multi-dwelling developments. Can reduce motor vehicle usage, thereby minimizing vehicle traffic impact on community.
On a collector or higher standard roadway on at least one frontage.	Minimizes traffic on local streets.
Adjacent to existing or planned non-residential development or multi-dwelling development.	Creates an appropriate transition between low density and other more intensive land uses or larger scale buildings.
Adjacent to or across from existing or planned open space or park or community amenity.	Creates an appropriate transition between low density and other land uses.
Along or in close proximity to an existing or planned corridor or activity centre.	Creates an appropriate transition between low density and other land uses.
Direct lane access.	Improves pedestrian environment for local residents by limiting the creation of multiple or high frequency use driveways across local sidewalks.

PUD2015-0364 Att 1.docx
ISC: UNRESTRICTED

CRITERIA SATISFIED

CRITERIA NOT SATISFIED

Page 1 of 1

Development Permit Drawings (DP2018-2259)

The associated development permit is currently under review. The proposal consists of a four unit rowhouse development. The development permit plans are included for information purposes only.

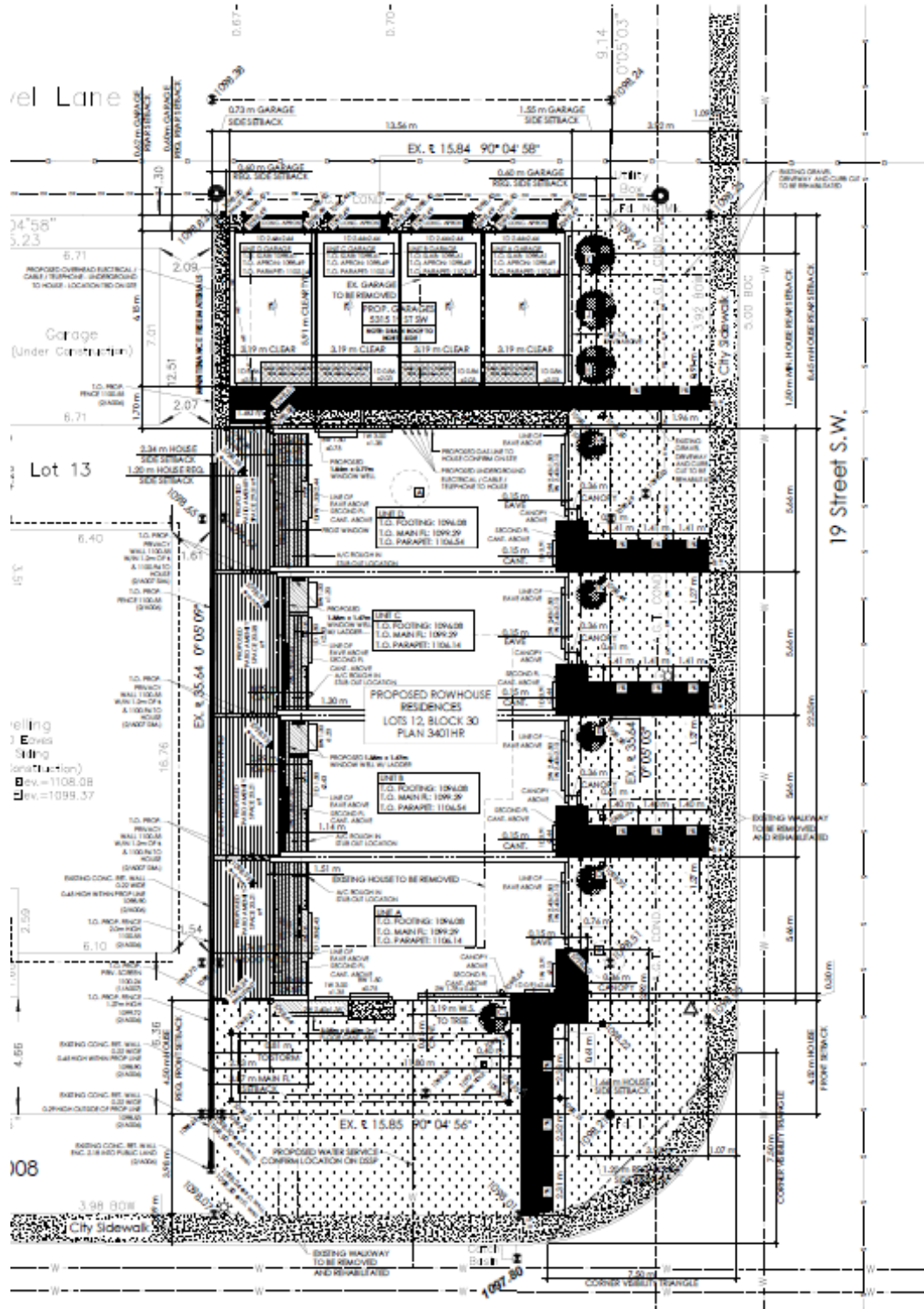
Administration's review of the development permit will determine the building design, number of units and site layout details such as parking, landscaping and site access. The development permit is on hold; no decision will be made on the development permit until Council has made a decision on this land use redesignation.

Figure 1: Development Permit Rendering (View from 19 Street SW)



Development Permit Drawings (DP2018-2259)

Figure 2: Site and Landscaping Plan



Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0901
Page 1 of 6

Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 – 33 Avenue SW, LOC2018-0098

EXECUTIVE SUMMARY

This land use redesignation application was submitted by M K M Design on 2018, May 2 on behalf of the landowner Trung Bien. This application proposes to redesignate a single residential parcel from Residential – Contextual One / Two Dwelling (R-C2) District to Multi-Residential – Contextual Grade-Oriented (M-CG) District to allow for:

- multi-residential buildings (e.g. townhouses, fourplexes);
- a maximum building height of 12 metres (an increase from the current maximum of 10 metres);
- a maximum of six dwelling units (an increase from the current maximum of two dwelling units); and
- the uses listed in the M-CG designation.

This redesignation is intended to accommodate a four-unit residential development. An associated development permit application, DP2018-2113, is under review (Attachment 4). A minor map amendment to the South Calgary/Altadore Area Redevelopment Plan (ARP) is required to accommodate the proposed land use redesignation and development permit.

The proposal conforms to the ARP as amended, aligns with applicable policies of the *Municipal Development Plan* and meets four of the eight *Location Criteria for Multi-Residential Infill*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing, and

1. **ADOPT**, by bylaw, the proposed amendment to the South Calgary/Altadore Area Redevelopment Plan (Attachment 3); and
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares ± (0.14 acres ±) located at 1519 - 33 Avenue SW (Plan 4479P, Block 64, Lots 31 and 32) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Multi-Residential – Contextual Grade-Oriented (M-CG) District; and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

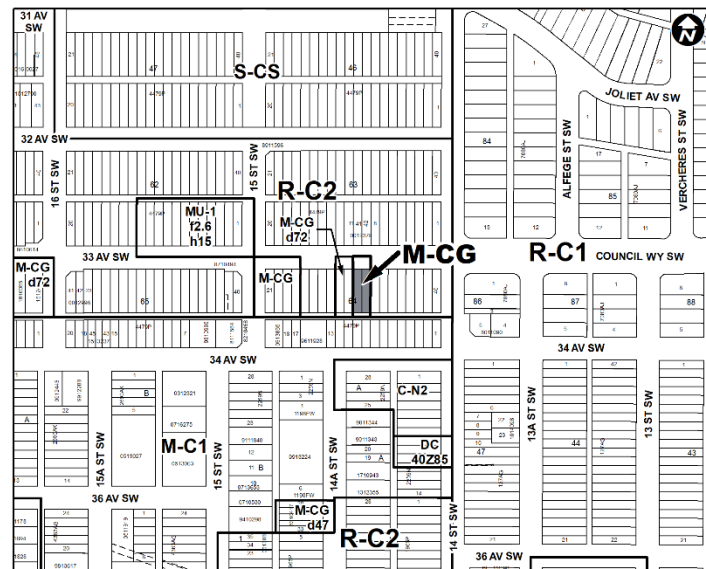
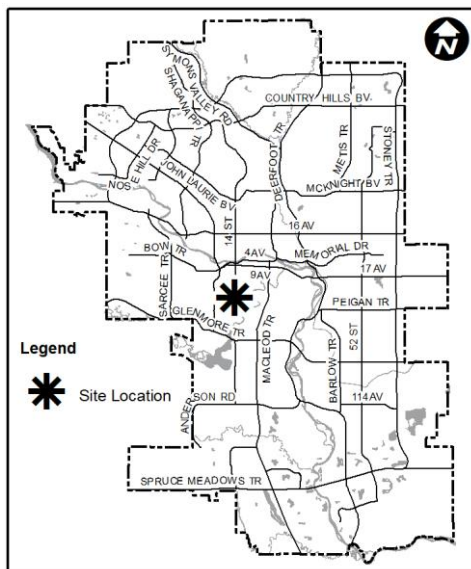
Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0901
Page 2 of 8

Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 - 33 Avenue SW, LOC2018-0098

BACKGROUND

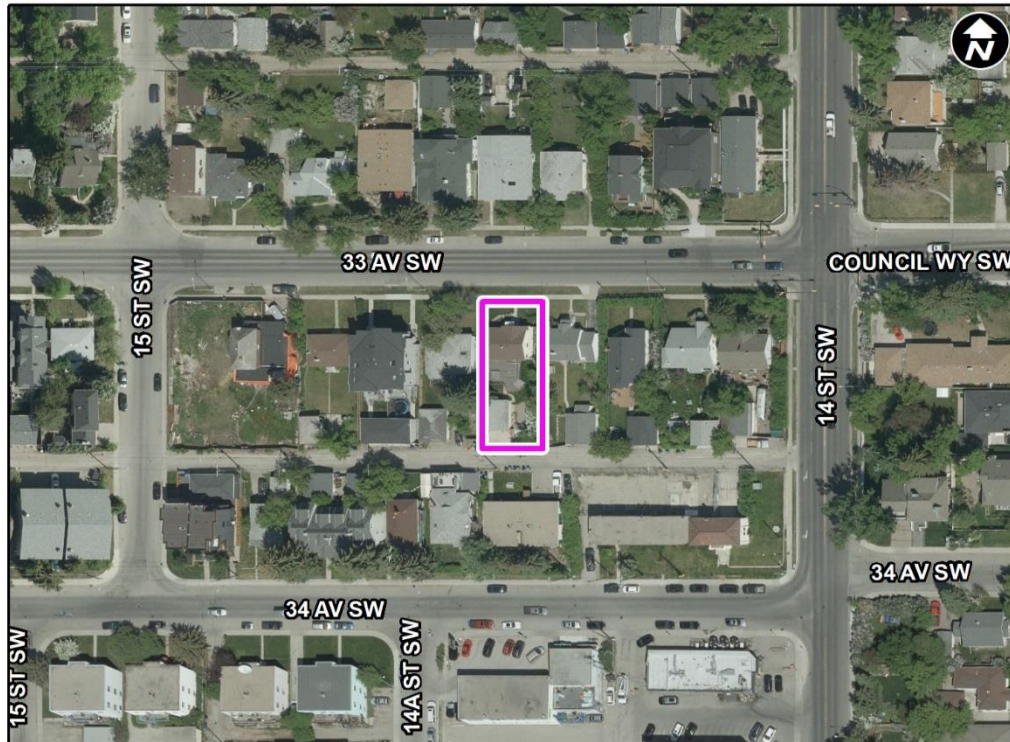
Location Maps



Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0901
Page 3 of 8

**Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 -
33 Avenue SW, LOC2018-0098**



Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 - 33 Avenue SW, LOC2018-0098

Site Context

The subject site is located in the community of South Calgary, mid-block on the south side of 33 Avenue SW, between 14 and 15 Streets SW. Surrounding development is characterized by single detached and semi-detached housing, with the primary designation being Residential – Contextual One / Two Dwelling (R-C2) District. However, there have been a number of recent land use redesignations in the near vicinity to allow for higher density development, including redesignations to the Multi-Residential – Contextual Grade-Oriented (M-CG) District and the Mixed Use - General (MU-1) District. In addition, this site is along the 33 Avenue SW Main Street, which will be proposing land use redesignations to allow for additional density. Therefore, this area will see significant change in the near future in terms of the built form being developed.

The site is approximately 0.06 hectares in size with approximate dimensions of 15 metres wide by 38 metres deep. A rear lane exists to the south of the site. The site is currently developed with a one storey, single detached dwelling, with access to a rear yard garage via the rear lane. As identified in *Figure 1*, the community of South Calgary reached its peak population in 2016. Much of the growth over the past 30 years is associated with unit construction – from 1985 to 2017, the total number of occupied dwelling units has increased by 35 percent.

Figure 1: Community Peak Population

South Calgary	
Peak Population Year	2016
Peak Population	4,118
2017 Current Population	4,038
Difference in Population (Number)	80
Difference in Population (Percent)	-1%

Source: The City of Calgary 2017 Civic Census

Additional demographic and socio-economic information can be obtained online through the [South Calgary](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for a modest increase in density and allows for development that is compatible with the massing, layout and use of other buildings in the area. Subject to a minor amendment to the ARP, the proposal generally meets the objectives of applicable policies as outlined in the Strategic Alignment section of this report.

Application Review

The applicant submitted a land use redesignation and associated development permit application on 2018 May 2. The development permit (DP2018-2113) is for a discretionary use

**Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 -
33 Avenue SW, LOC2018-0098**

Multi-Residential Development. The permit has been reviewed by Administration, and Administration is ready to recommend approval of the development permit, pending Council's approval of the land use.

As mentioned, the subject site is along the 33 Avenue SW Main Street. The applicant did have initial discussions with the Main Streets team as to their proposal, but as the timing of the presentation to Calgary Planning Commission for the 33 Avenue Main Street will not be until later this year or early in 2019, the applicant opted to proceed with their application at present.

Land Use

The subject site is currently designated Residential – Contextual One / Two Dwelling (R-C2) District, which is intended for development in the form of single detached, semi-detached and duplex dwellings in developed areas of the city. The district allows for a maximum of two dwelling units and a building height of 10 metres.

The proposed Multi-Residential – Contextual Grade-Oriented (M-CG) District is intended for low height and low density grade-oriented multi-residential development. The maximum building height allowed within this district is 12.0 metres. The maximum density allowed is 111 units per hectare; this would accommodate 6 units on this site.

Infrastructure

Transportation Networks

Pedestrian access to the site is available from 33 Avenue SW and vehicular access is provided from the rear lane. The area is served by Calgary Transit bus service, with stops located approximately 46 metres walking distance from the subject site providing service to the downtown core. On-street parking to the site is non-restricted. A Transportation Impact Assessment was not required as part of this application.

Utilities and Servicing

Water and sanitary sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. However, storm mains are not available. Individual servicing connections, as well as appropriate storm-water management will be considered and reviewed at development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application has been advertised on the Planning and Development online map.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be

**Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 -
33 Avenue SW, LOC2018-0098**

posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

The Marda Loop Community Association was circulated on this application. After considering both the land use and development permit applications, the Association responded with a letter in support of the application (Attachment 2), citing the existing M-CG parcels in close proximity and the fact that this proposal does not exceed the anticipated scale of redesignation with the 33 Avenue SW Main Streets initiative.

Administration received one letter of opposition to the application. Reasons stated for opposition are summarized below:

- Concern over increased pressure on existing infrastructure and concern over who would bear the cost if future upgrades are required.

Administration has reviewed these concerns and has confirmed that there are no improvements to infrastructure required at this time.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The site is located within the City, Town area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory – 2009)

The site is located within the Residential Developed: Inner City area as identified on Map 1: Urban Structure of the *Municipal Development Plan* (MDP). Although the MDP makes no specific reference to the subject site, the land use proposal is consistent with MDP policies regarding Developed Residential Areas, neighbourhood infill and redevelopment and housing diversity and choice.

South Calgary/Altadore Area Redevelopment Plan (Statutory – 1986)

The subject site is located within the Residential Conservation Area of the *South Calgary/Altadore Area Redevelopment Plan* (ARP), as identified on Map 2. The intent of the Residential Conservation Area is to maintain the low density, single and duplex development, while allowing for some low profile infill development that is compatible with the existing structures. To accommodate the proposed M-CG District, a minor amendment to Map 2 is required to change the land use category of the subject site to Residential Low Density (Attachment 3).

**Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 -
33 Avenue SW, LOC2018-0098**

Location Criteria for Multi-Residential Infill (Non-Statutory – 2014)

The *Location Criteria for Multi-Residential Infill* is a tool for review of redesignation applications in the developed areas. The criteria are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP or local area policy plans, to assist in determining the appropriateness of an application in the local context.

The proposed land use aligns with four of the eight criteria. The site has direct lane access, is within 46 metres walking distance from a transit stop and is located along a corridor and collector road (33 Avenue SW). Criteria not met include being within 600 metres of a BRT stop, being located beside a non-residential or multi-unit development and being across from a park.

Social, Environmental, Economic (External)

The proposed M-CG District allows for a wider range of housing types than the existing R-C2 District. Therefore, the proposed change would diversify housing stock in the community, allowing it to meet the needs of different age groups, lifestyles and demographics.

An environmental site assessment was not required for this application.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and, therefore, there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0901
Page 8 of 8

**Policy Amendment and Land Use Amendment in South Calgary (Ward 8) at 1519 -
33 Avenue SW, LOC2018-0098**

REASON(S) FOR RECOMMENDATION(S):

The proposal conforms to the *South Calgary/Altadore Area Redevelopment Plan* as amended and is in keeping with applicable policies of the *Municipal Development Plan*. The proposed Multi-Residential – Contextual Grade-Oriented (M-CG) District allows for a broader range of building types that can better accommodate the housing needs of different age groups, lifestyles and demographics. In addition, this proposal aligns with the anticipated vision of the 33 Avenue SW Main Street and recent land use redesignations that have occurred in the area.

ATTACHMENT(S)

1. Applicant Submission
2. Community Association Letter
3. Proposed Amendment to the South Calgary/Altadore Area Redevelopment Plan
4. Development Permit Drawings (DP2018-2113)

Applicant's Submission



u98

To Whom it may Concern,

Thank you for your consideration in the matter of land use re-designation to M-CG.

MKM Design, together with Lighthouse Custom Homes, is respectfully applying for a land use re-designation on the parcel of land located at 1519 33rd Ave SW. We are asking the City of Calgary and the Community Association of Marda Loop to grant us a re-zoning designation to M-CG, allowing us to benefit the community by building a new structure that will be home to four families.

Our proposed development will consist of a fourplex structure with a detached garage, directly accessed via the alley. Our building will have all four units with at grade entrances, facing on to 33rd Ave. This allows us to fall under the permitted use portion of the bylaw, and not require any further relaxations from the City or the Community Association. With an approval for land use re-designation, we will be able to comply with all the land use by-laws for M-CG, as outlined in our supporting documentation.

The M-CG zoning is specifically designed by the City with the best interests of the community in mind. These regulations and projects serve the purpose to respect and enhance quality of life in well established, beautiful inner city communities. As the developer, we share this mission in our attempt to provide attainable, cohesive, and diverse inner city living opportunities for Calgarians. We believe this project to be beneficial for the community because it is compatible with the character of the existing neighborhood, it is within a very short walking distance to several transit stops on 33 Avenue SW and the garage has direct lane access. It allows for greater transit use, providing more mobility options for residents of multi-dwelling developments and can reduce motor vehicle usage and on street parking by each home having their own parking stall in the garage.

Thank you for your attention to this matter.

Sincerely,

A handwritten signature in black ink, appearing to read 'Andrea Deydey', with a stylized flourish at the end.

Andrea Deydey
President
Lighthouse Custom Homes
andrea@lighthousecustomhomes.ca
403-483-6191

Community Association Letter



3130 16 Street SW
Calgary, AB, T2T 4G7

June 1, 2018

Circulation Control
Planning & Development #8201
PO Box 2100 Station M
Calgary, AB, T2P 2M5
Email: CPAG.Circ@calgary.ca

Attn: Colleen Renne-Grivell, colleen.renne-grivell@calgary.ca

SENT BY EMAIL

Dear Ms. Renne-Grivell;

RE: Community Association Feedback for LOC2018-0098

Thank you for providing us the opportunity to offer feedback on this application that reflects the vision of the Marda Loop Communities Association (MLCA). The following comments have been written with consideration towards what is best for our communities and our city.

This application appears to meet the intent of The City's Location Criteria for Multi-Residential Infill and in our opinion does not exceed the anticipated scale of redesignation that will occur along this corridor as part of the 33rd Avenue SW Main Streets Initiative. Additionally, there are currently two other M-CG designated parcels on the south 1500 block of 33rd Avenue SW, one of which is directly adjacent to this parcel. The MLCA is supportive of the proposed location of this M-CG land use district, however, we would prefer to see the density modifier of 72 units per hectare added to the designation to provide more certainty that a 4-plex development is in fact what we are supporting at this location. The associated Development Permit DP2018-2113 does depict a four unit development.

The MLCA continues to encourage developers to engage with us and neighbours of proposed developments. A prospective buyer of this property contacted the MLCA in February 2018 regarding this application, however, there has been no further engagement with the MLCA by the applicant. To-date the MLCA has not been contacted by any members of the community regarding this application.

If you have any questions regarding these comments, please contact Lauren Makar at your convenience.

Regards,



Lauren Makar
Director - Planning & Development
Marda Loop Communities Association
development@mardaloop.com

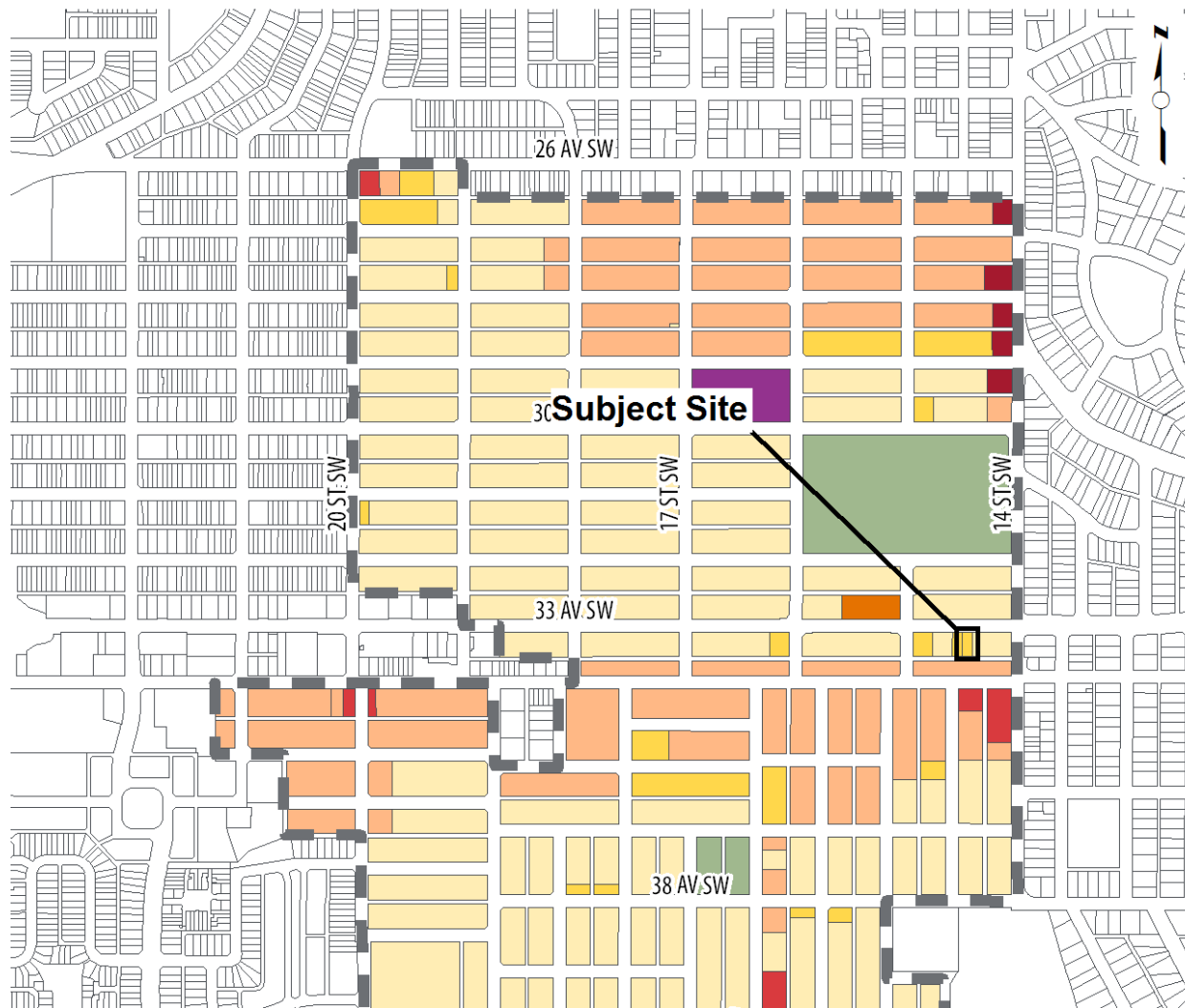


Doug Fraser
President
Marda Loop Communities Association
president@mardaloop.com

cc: Evan Woolley, Ward 8 Councillor, The City of Calgary evan.woolley@calgary.ca

Proposed Amendment to the South Calgary/Altadore Area Redevelopment Plan

1. Amend the South Calgary/Altadore Area Redevelopment Plan, being Bylaw 13P86, as amended, is hereby further amended as follows:
 - (a) Amend Map 2 entitled "Land Use Policy" by changing 0.06 hectares \pm (0.14 acres \pm) located at 1519 - 33 Avenue SW (Plan 4479P, Block 64, Lots 31 and 32) of Map 2 entitled "Land Use Policy" from "Residential Conservation" to "Residential Low Density", as generally shown in the sketch below.

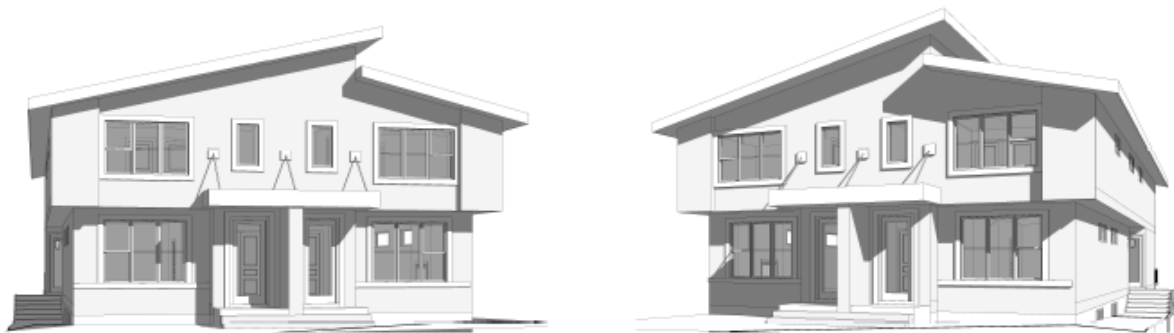


Development Permit Drawings (DP2018-2113)

The associated development permit is currently under review. The proposal consists of a four-unit multi-residential development. The development permit plans are included for information purposes only.

Administration's review of the development permit will determine the building design, number of units and site layout details such as parking, landscaping and site access. The development permit is on hold; no decision will be made on the development permit until Council has made a decision on this land use redesignation.

Figure 1: Development Permit Rendering (Front and Rear Facade)



Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0919
Page 1 of 7

Land Use Amendment in Glamorgan (Ward 6) at 3 Gissing Drive SW, LOC2018-0106

EXECUTIVE SUMMARY

This land use redesignation application was submitted by Civicworks Planning + Design on 2018 May 09 on behalf of the landowner Duc Thi Nguyen. The application proposes to change the designation of this property from Residential – Contextual One Dwelling (R-C1) District to Residential – Grade-Oriented Infill (R-CG) District to allow for:

- rowhouses in addition to building types already allowed (e.g. single detached homes, semi-detached, and duplex homes and suites);
- a maximum building height of 11 metres (an increase from the current maximum of 10 metres);
- a maximum of 4 dwelling units (an increase from the current maximum of one dwelling units); and
- the uses listed in the proposed R-CG designation.

This redesignation is intended to accommodate a four-unit residential development. An associated development permit application, DP2018-3272, is under review.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares ± (0.13 acres ±) located at 3 Gissing Drive SW (Plan 786JK, Block 8, Lot 2) from Residential – Contextual One Dwelling (R-C1) District to Residential – Grade-Oriented Infill (R-CG) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

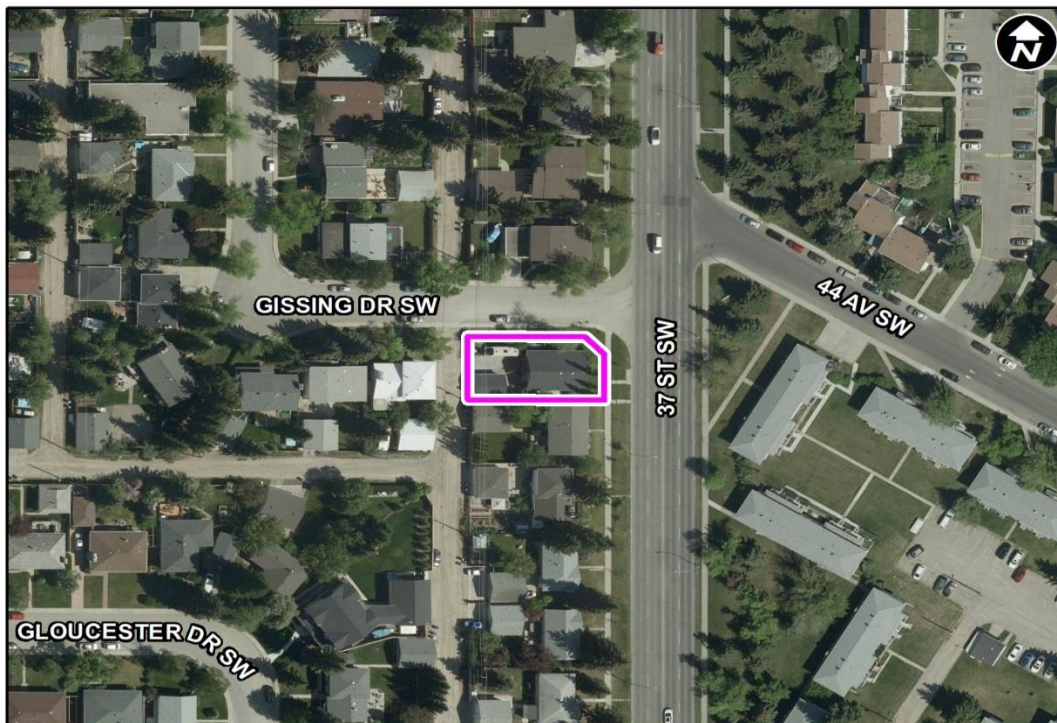
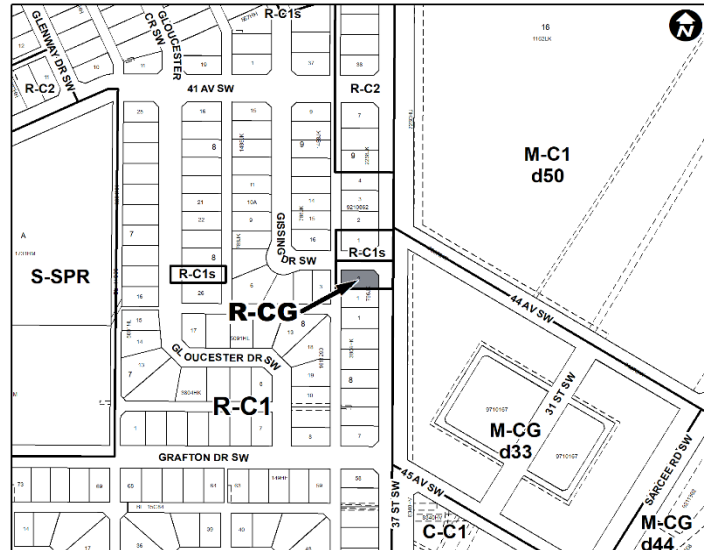
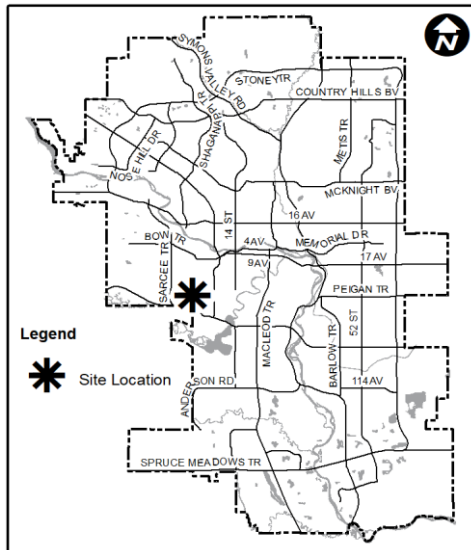
**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0919
Page 2 of 7**

Land Use Amendment in Glamorgan (Ward 6) at 3 Gissing Drive SW, LOC2018-0106

BACKGROUND

Location Maps



Land Use Amendment in Glamorgan (Ward 6) at 3 Gissing Drive SW, LOC2018-0106

Site Context

The site is located at the corner of Gissing Drive SW and 37 Street SW (an arterial road) in the community of Glamorgan. Single detached dwellings dominate the streetscape to the north, south and west as those surrounding lands are all designated R-C1 Residential– Contextual One Dwelling District. Across 37 Street SW is a large cluster of townhouse development on lands designated as M-CG Multi-Residential – Contextual Grade-Oriented District. Across Gissing Drive SW one parcel has recently been redesignated to Residential– Contextual One Dwelling (R-C1s) District.

The site is approximately 0.05 hectares in size and has a rear lane.

As identified in *Figure 1*, the community of Glamorgan's population reached its peak in 1982, dropped in the late 1980s and has remained relatively stable at its current level for some time.

Figure 1: Community Peak Population

Glamorgan	
Peak Population Year	1982
Peak Population	7,258
2017 Current Population	6,385
Difference in Population (Number)	-873
Difference in Population (Percent)	-12%

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Glamorgan](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for a modest density increase and for building types that are compatible with the established building form of the existing neighbourhood. The proposal generally meets the objectives of applicable policies as discussed in the Strategic Alignment section of this report.

Land Use

The existing Residential – Contextual One Dwelling (R-C1) District is a residential designation in developed areas that is for single detached homes and related accessory uses (home occupations, garages, etc.). The district allows for a maximum building height of 10 metres.

Land Use Amendment in Glamorgan (Ward 6) at 3 Gissing Drive SW, LOC2018-0106

The proposed Residential – Grade-Oriented Infill (R-CG) District is a low density residential designation that is primarily for two to three storey (11 metres maximum height) rowhouse developments where one façade of each dwelling unit must directly face a public street. The maximum density of 75 units per hectare would allow for up to four dwelling units on the subject site.

The R-CG District also allows for a range of other low-density housing forms such as single detached, semi-detached and duplex dwellings. Secondary suites (one Backyard Suite or Secondary Suite per unit) are also allowable in R-CG developments. Secondary suites do not count against allowable density and do not require motor vehicle parking stalls in the R-CG district provided they are below 45 square metres in size.

Infrastructure

Transportation Networks

Pedestrian and vehicular access to the site is available from Gissing Drive SW, 37 Street SW and the rear lane. The area is served by Calgary Transit bus service with a stop located approximately 190 metres walking distance on 37 Street SW providing service to the Westbrook/Heritage BRT and the 13 Mount Royal bus route. 37 Street SW is identified as part of the Primary Transit network.

Utilities and Servicing

Water and sanitary sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. Storm connections are not currently available. Individual servicing connections as well as appropriate stormwater management will be considered and reviewed at the development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

The application was circulated to the Glamorgan Community Association. Their written response is included as Attachment 2. The community association's objections centre on the proposal not being in alignment with the Glamorgan/Glenbrook Design Brief, which they believe seeks to maintain the current R-1 status of the site.

Land Use Amendment in Glamorgan (Ward 6) at 3 Gissing Drive SW, LOC2018-0106

All other public engagement was applicant-led. The applicant did their own notice posting of the site and answered directly many of the phone calls/letters of concern sent to them by residents. The applicant therefore chose to engage on a one-on-one basis instead of a formal public event, as it was deemed that an open public event was not likely to lower the degree or change the reasons for opposition to the development.

Twelve letters of objection were received by Administration from individual residents, as well as a single petition signed by a group of 7 residents. The objections centred on the potential impacts of more units on the availability of on-street parking, increased traffic and the belief that a departure from R-1 zoning will destroy the nature of the community.

As stated elsewhere in the report, Administration believes the proposed designation aligns with the local Design Brief and with broader City policy. Traffic and parking issues will be considered at the development permit stage.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The subject parcel is located within the Residential - Developed - Inner City area of the *Municipal Development Plan* (MDP). The applicable MDP policies encourage redevelopment of inner-city communities that is similar in scale and built form to existing development, including a mix of housing such as townhouses and rowhousing. The MDP also calls for a modest intensification of the inner city, an area serviced by existing infrastructure, public amenities and transit. The proposal is in keeping with relevant MDP policies as the rules of the R-CG District provide for development form that may be sensitive to existing residential development in terms of height, built form and density.

Glamorgan/Glenbrook Design Brief (Non-Statutory, 1971)

The subject parcel is located within the Low-Density Residential area on the Land Use map of the Glamorgan/Glenbrook Design Brief. As the plan gives no further guidance as to what this entails, and the RC-G designation is a low-density designation within the Land Use Bylaw, Administration believes that the proposal is in alignment with the Design Brief.

Land Use Amendment in Glamorgan (Ward 6) at 3 Gissing Drive SW, LOC2018-0106

Location Criteria for Multi-Residential Infill (Non-statutory, 2014)

While the proposed R-CG District is not a multi-residential land use, the *Location Criteria for Multi-Residential Infill* was amended to consider all R-CG redesignation proposals under these guidelines as the R-CG allows for a building form comparable to other “multi-residential” developments.

The guidelines are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP or local area policy plans, to assist in determining the appropriateness of an application in the local context.

The proposed land use aligns with six of the criteria, as follows:

- The site is a corner parcel.
- The site is located within 200 metres of both a transit stop
- The Primary Transit Network located on 37 Street SW.
- The site has lane access.
- Multi-residential development exists across the street to the east.
- 37 Street SW is an arterial street.

In all, the proposal aligns strongly with the criteria, and considering the minimal intensification proposed, is therefore considered appropriate.

Social, Environmental, Economic (External)

The recommended land use allows for wider range of housing types than the existing RC-2 District and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics.

An environmental site assessment was not required for this application

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to current or future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0919
Page 7 of 7

Land Use Amendment in Glamorgan (Ward 6) at 3 Gissing Drive SW, LOC2018-0106

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal conforms to the *Glamorgan/Glenbrook Design Brief* and is in keeping with applicable policies of the *Municipal Development Plan*. The proposed R-CG District is intended for parcels located near or directly adjacent to low density residential development. The proposal allows for a range of building types that have the ability to be compatible with the established building form of the existing neighbourhood and can better accommodate the housing needs of different age groups, lifestyles and demographics.

ATTACHMENT(S)

1. Applicant's submission
2. Community Association comments

Applicant's Submission

0106

CIVICWORKS
PLANNING + DESIGN

460 - 5119 Elbow Drive SW
Calgary, Alberta T2V 1H2

P 403 201 5305
F 403 201 5344

MAY 8, 2018

City of Calgary
Planning and Building
800 MacLeod Trail SW
Calgary AB T2P 2M5

RE: Land Use Redesignation from R-C1 to R-CG: 3 - Gissing Drive SW | Lot 2, Block 8, Plan 786JK

APPLICATION SUBMISSION PACKAGE CONTENTS:

1. Completed Application Form [x1]
2. Current copy of Certificate(s) of Title (searched within 30 days) [x1]
3. Copies of any Restrictive Covenants, Utility Rights-of-Way, Easements, or City Caveats registered on the Title(s) [x1]
4. Letter of Authorization [x1]
5. Fee : Cheque in the amount of \$5,050 [x1]
6. Site Photos - Sheet P1 [x1]
7. Completed Site Contamination Statement [x1]
8. Completed Abandoned Well Declaration Form [x1]
9. Presence or Absence of Abandoned Wells Map [x1]
10. Applicant Statement [x1]
11. Site Plan - Sheet C1 [x15]

Applicant's Submission



460 - 5119 Elbow Drive SW
Calgary, Alberta T2V 1H2

P 403 201 5305
F 403 201 5344

MAY 8, 2018

City of Calgary
Planning and Building
800 MacLeod Trail SW
Calgary AB T2P 2M5

RE: Land Use Redesignation from R-C1 to R-CG: 3 - Gissing Drive SW | Lot 2, Block 8, Plan 786JK

The subject parcel is located in the community of Glamorgan and consists of 0.054ha of privately owned land. Eagle Crest Construction has retained CivicWorks Planning + Design Inc. to undertake a land use redesignation process to facilitate the construction a four-unit Rowhouse Building with front doors facing Gissing Drive and 37 Street SW. The proposed use is well-suited to the site, given its surrounding context, lot characteristics and location.

The site's current R-C1 (Residential – Contextual One Dwelling) District allows for single detached dwellings and limits this parcel to one household regardless of configuration. In support of the proposed development, this application seeks to amend the existing R-C2 (Residential – Contextual One / Two Dwelling) District to a R-CG (Residential – Grade-Oriented Infill) District.

Like R-C1, the R-CG District is a Low Density Residential District, and is intended to facilitate grade-oriented development. The R-CG District accommodates a variety of grade-oriented development in forms like Rowhouse Buildings, Duplex Dwellings, Semi-detached Dwellings and Cottage Housing Clusters. The District provides flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing over time, and accommodate site and building designs that are adaptable to the functional requirements of evolving household needs.

The subject lands do not fall within the boundaries of any Local Area Plans and are governed by higher level, city-wide policy like the Municipal Development Plan (MDP) and Developed Areas Guidebook (DAG), which support greater housing choice and reinforce more complete and resilient residential neighbourhoods. The MDP identifies ground-oriented housing as a key component of complete communities and encourages growth and change in low density residential neighbourhoods through the addition of a diverse mix of ground-oriented housing options.

PLANNING RATIONALE

The subject site features numerous characteristics that make it especially appropriate for the proposed R-CG land use change, which will directly facilitate the development of new and innovative housing options for Calgarians:

Corner Lot: The subject site occupies a corner lot, allowing the proposed development to contribute to the neighbourhood streetscape by addressing both Gissing Drive and 37 Street SW with grade-oriented unit entrances.

Direct Lane Access: The subject site enjoys direct lane access, facilitating a development that orients vehicle access to the rear lane, creating an uninterrupted, pedestrian-friendly streetscape interface along Gissing Drive and 37 Street SW.

Major Road: The subject site is located along 37 Street SW – a Major Road – ensuring both ease of access and traffic capacity for future residents.

Proximity To Transit: The subject site is ~100m / ~150m from two primary transit stops (Route 72, 73 & 13), and is adjacent to the 37 Street Primary Transit Network corridor. Calgary's Primary Transit Network provides communities with daily reliable public transit service, with a frequency of every 10 minutes or less, 15 hours a day, 7 days a week.

Proximity To Multi-Residential Development: The subject site is proximate to a number of existing large scale multi-residential developments sited directly across 37 Street SW.

Applicant's Submission



CITY-WIDE POLICY ALIGNMENT

This proposed land use redesignation and associated development vision is consistent with the city-wide goals and policies of the Municipal Development Plan, which encourage: the development of more innovative and affordable housing options in established communities; more efficient use of infrastructure; and more compact built forms in locations with direct and easy access to transit, shopping, schools and other community services.


COMMUNITY ENGAGEMENT

Eagle Crest Construction is committed to being a good neighbour and working with surrounding community members and stakeholders throughout the application process. As part of that commitment, Eagle Crest Construction and the project team have undertaken a community engagement process in support of this application to ensure a clear and transparent process for all stakeholders. Stakeholders like the Community Association and Ward Councillor's office are actively invited to participate in our process, which focuses on informative and fact-based engagement and communications. Key elements of our engagement process include:

On-site Signage | To be installed on-site at time of submission

To supplement the usual City of Calgary notice signage that is associated with Land Use Redesignation and Development Permit applications, Eagle Crest Construction and the project team deploy on-site signage that notifies neighbours and surrounding community members of a proposed land use change.

The signage outlines the land use change and development vision for the subject site and directs interested parties to get in touch with the project team via a dedicated email inbox. All inquiries, questions and comments are received, compiled, and responded to by the project team in a timely manner.



Proposed Land Use Change

Hello neighbour!
We are proposing a land use change at this address:
3 - Gissing Drive SW | R-C1 to R-CG

The proposed land use redesignation will contribute to the continued vibrancy and vitality of Calgary's established neighbourhoods and facilitate a development vision that will add a four-unit Rowhouse Building with front doors facing Gissing Drive SW and 37 Street SW. The proposed use is well-suited to the site, given its surrounding context, lot characteristics and location.

The site's current R-C1 (Residential - Contextual One Dwelling) District allows for single detached dwellings. Like R-C1, the R-CG (Residential - Grade-Oriented Infill) District is a low density residential district that facilitates street-oriented development, with flexible parcel dimensions and building setbacks. The R-CG District allows for a diversity of housing choices and accommodates site and building designs that are adaptable to the evolving household needs of Calgarians.

If you have any questions, comments or concerns, get in touch:
Email engage@civicworks.ca or call 587.747.0317

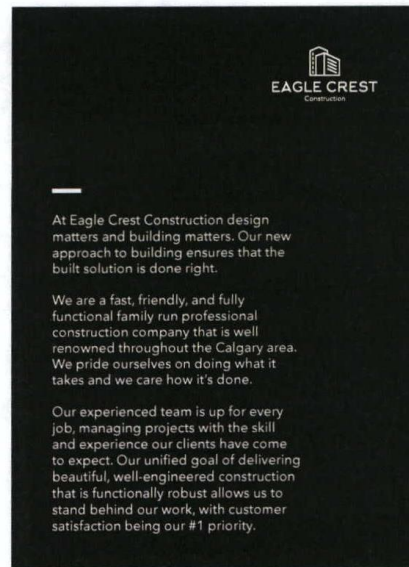
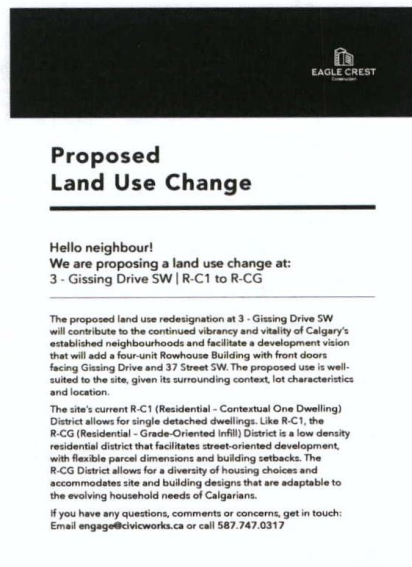
Applicant's Submission



Surrounding Area Postcard Drop | ~100 surrounding area neighbours

Paired with on-site signage, hand delivered postcards ensure that surrounding area neighbours and adjacent property owners are aware of the proposed land use change and associated development vision.

The postcards outline the proposed land use change and ultimate development vision for the subject site and direct interested parties to get in touch with the project team via a dedicated phone line and email inbox. All inquiries, questions, and comments are received, compiled, and responded to by the project team in a timely manner.



CONCLUSION

The proposed land use redesignation is in keeping with the city-wide goals and policies of the Municipal Development Plan and will facilitate a development vision that will introduce new and innovative housing options for Calgarians looking to live in established communities that enjoy excellent access to transit, existing infrastructure and community amenities. For the reasons outlined above, we respectfully request that Administration, Calgary Planning Commission and Council support this application.

Should you have any questions, comments, or concerns, please contact me at 403-889-4434 or boris@civicworks.ca.

Sincerely,

Boris Karn, Planner
B.HSc., M.Plan.

CIVICWORKS
PLANNING + DESIGN

Community Association Response

----- Forwarded Message -----

From: "ostromb" [REDACTED]
To: "dino civitarese" <dino.civitarese@calgary.ca>, "cpag circ" <cpag.circ@calgary.ca>
Cc: "sara" [REDACTED], "Kathy Morgan" <eaward6@calgary.ca>, "Ward6 - Frano Cavar" <ward6@calgary.ca>, "CAWard6 - Suzy Trottier" <suzy.trottier@calgary.ca>
Sent: Thursday, June 7, 2018 9:58:25 AM
Subject: Community Response LOC2018-0106 at 3 Gissing Drive S.W. - e-mail Part 1

Good Morning Everyone : Please note that there will be photos send as well but will be sent in separate e-mails due to the size. Please let me know that you have received all of them.

I am writing on behalf of the Glamorgan Community Association regarding LOC2018-0106 at 3 Gissing Dr. S.W. This zoning request is unusual as it is request for a major deviation not only from R-1 Housing which comprises the adjacent streets but the Proposed Land Use is one that is unknown within the boundaries of the Glamorgan Community. The "application" only speaks to a possible future outcome at some point. It is extremely vague. There are no plans attached to indicate that the applicant is truly serious about the intentions for this site.

This is a clear example of "spot zoning" that was disallowed by previous City Councils as this type of "spot zoning" changes not only the character of the lot in question but it becomes the anomaly along the entire street. The Glamorgan Community Association does not support this zoning request as it has far reaching implications that will affect the adjacent neighbours, all residents along Gissing Drive and the Glamorgan Community as a whole. This entire street is purposely zoned R-1 and was designed for this type of zoning.

The Glamorgan/Glenbrook Design brief is our policy document and specifically lays out areas within our community for wide variety of various zoning types. Glamorgan is currently used by the University of Calgary Urban Studies Course as an example of thoughtful and sustainable zoning. This type of application goes against the principals used in the design of our community. This type of application also undermines the contract in place with R-1 homeowners as laid out in the Municipal Government Act. R-1 homeowners purchase properties and pay a premium with the expectation that the R-1 zoning remains in place with one (1) dwelling on site.

We have received concerns from a number of the directly affected residents and we trust that their objections have been sent to the File Manager and will form part of the formal file.

Please note that there are photos and they will be sent by separate e-mails due to the size.

Our concerns are as follows:

- 1) This is an interesting application, as to the best of our knowledge, the landowners are not the current occupants.
- 2) We are aware of a number of objections from adjacent and directly affected property owners.

Community Association Response

3) There are aging infrastructure issues. Given that this area of our Community was built in the 1950's, there is potential for significant upgrades to this site as well as all of the affected residents along Gissing Drive, which will come at a substantial cost.

4) Included are a number of photos of the existing house, garage and parking pad, along with east/west views of Gissing Drive, the alley and north/south views of 37 Street.

5) Gissing Drive is currently zoned for 2 hour parking. Parking is very limited and this street currently has parking issues that were well documented by the media in September, 2013. This is a very short portion of Gissing Drive that moves into a corner as you travel westbound. Eastbound travel ends with either a right or a left turn on to 37 St, S.W. This is a bit of a hidden intersection with very little room for additional parking. There is no parking allowed along 37 Street from Richmond Road to Glenmore Trail. This creates an issue with the alley as many people simply park during the day.

6) Access to the lot from 37 St is not allowed as there is a major infrastructure corridor that runs under the boulevard and 37 St is deemed a major corridor. The set-backs for all of the homes located along 37 St is more than the norm due to the topography of the lots and the elevation drop of approximately 3 meters from the top of the lots to the street. This allows for mitigation of drainage issues as well as safety concerns.

7) Fire and safety concerns. Due to the severe nature of recently reported fires within the Calgary area, the potential size and massing of the development and disruption of the existing setback from the adjacent home to the south is of deep concern.

8) There has been no work done to date by the City of Calgary with respect to any type of re-development along 37 St nor engagement with residents within our Community. With the pending permanent closure of 45 St. off of Glenmore Trail, 37 St. will be our only north bound access point from Glenmore Trail. The impacts of the Ring Road and new overpass at 37 St and Glenmore Trail will also have an impact.

The City of Calgary has a fiduciary duty to review these applications and make the appropriate recommendations based on the collective input, that take into account the issues surrounding these sites and the impacts on the existing neighbourhood. We do not support the removal of the R-C1 designation. R-C1 provides stability of streetscape as well as community. There are many parcels located within our Community currently zoned for a number of housing types as well as commercial. Given the lack of information in the circulation package and the potential impacts on the street, alley and directly affected neighbours, we do not support this application.

Respectfully submitted,
Beryl Ostrom
President, Glamorgan Community Association

Policy Amendment and Land Use Amendment in Mount Pleasant (Ward 7) at 602 - 20 Avenue NW, LOC2018-0119

EXECUTIVE SUMMARY

This land use redesignation application was submitted by New Century Design on 2018 May 25 on behalf of the landowner Sharon J West. This application proposes to redesignate a single residential parcel from Residential – Contextual One / Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District to allow for:

- Rowhouse, suites, semi-detached and duplex homes, in addition to the building types already allowed (e.g. single detached, semi-detached and duplex homes and suites).
- A maximum building height of 11 metres.
- A maximum of four dwelling units (an increase from the current maximum of two dwelling units).
- The uses listed in the proposed R-CG district.

A minor amendment to the *North Hill Area Redevelopment Plan* is required to accommodate the proposed land use redesignation. This proposal conforms to the Area Redevelopment Plan as amended and is in keeping with applicable policies of the *Municipal Development Plan*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendment to the North Hill Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 0.06 ± hectares (0.15 ± acres) located at 602 - 20 Avenue NW (Plan 2934O; Block 22; Lots 1 and 2) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Residential – Grade-Oriented Infill (R-CG) District; and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

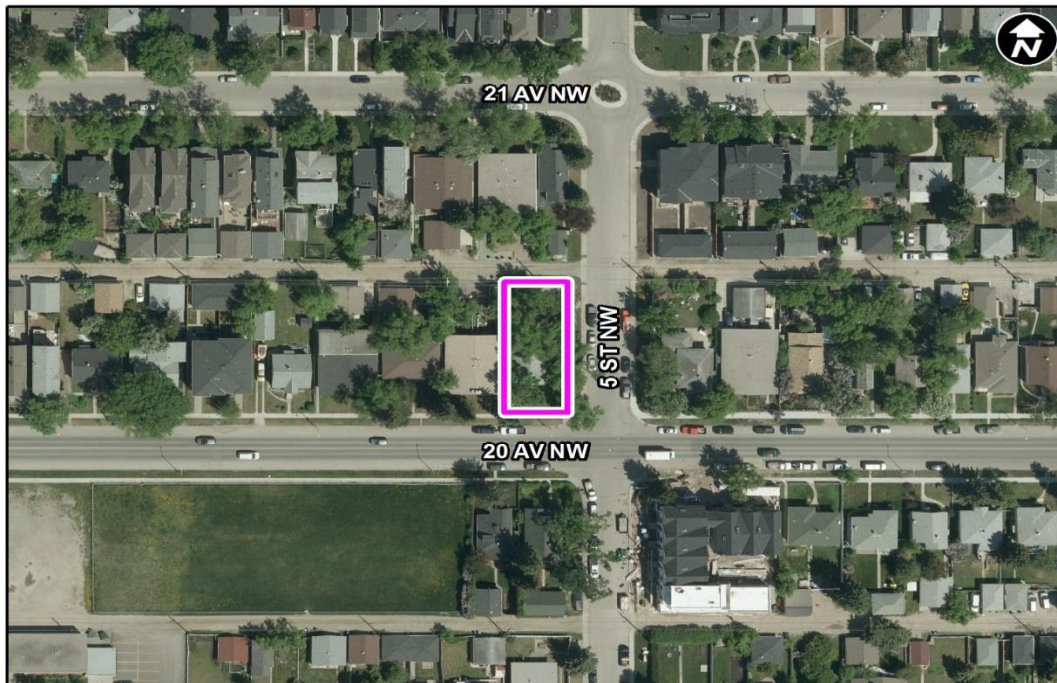
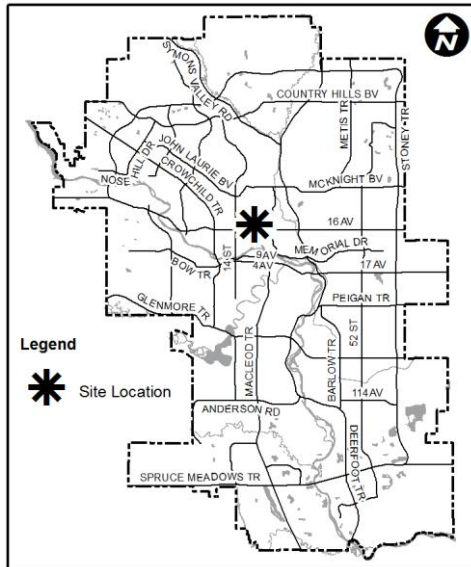
None.

BACKGROUND

The community of Mount Pleasant is subject to the policies of the *North Hill Area Redevelopment Plan* which provides direction in relation to future redevelopment of Mount Pleasant, Tuxedo and Capitol Hill.

**Policy Amendment and Land Use Amendment in Mount Pleasant (Ward 7) at 602 -
20 Avenue NW, LOC2018-0119**

Location Maps



Policy Amendment and Land Use Amendment in Mount Pleasant (Ward 7) at 602 - 20 Avenue NW, LOC2018-0119

Site Context

The subject property is located within the inner-city community of Mount Pleasant within a predominately low density district, one block west of the 4 Street NW Main Street. Low density residential development in the form of single and semi-detached dwellings surround the site. An infill development (R-CG) exists across from the subject site, on the opposite corner, and is developed with an eight-unit rowhouse, developed on two parcels, that is also surrounded by low density (R-C2) development. The former St. Joseph's School Site exists on the opposite side of 20 Avenue NW, and is currently vacant.

Mt. Pleasant Park, Community Sportsplex and Community Association are all located 500 metres north of the property. The property is well served by public transit, with bus stops within 250 metres or a 3 minute walk of the site.

The site is approximately 0.06 hectares in size, with approximate dimensions of 15 metres by 36 metres. A gravel lane exists at the rear of the site. The property is currently developed with a one-storey single detached dwelling and a single-car garage accessed from 5 Street NW. Redevelopment of the site will require direct vehicular access via the lane only.

As identified in *Figure 1*, the community of Mount Pleasant has seen population growth over the last several years reaching its population peak in 2017.

Figure 1: Community Peak Population

Mount Pleasant	
Peak Population Year	2017
Peak Population	5,811
2017 Current Population	5,811
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Mount Pleasant](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This proposal allows for a range of building types that have the ability to be compatible with the established building form of the existing neighbourhood. Though a minor amendment to the *North Hill Area Redevelopment Plan* is required, the proposal generally meets the objectives of applicable policies as discussed in the Strategic Alignment of this report.

**Policy Amendment and Land Use Amendment in Mount Pleasant (Ward 7) at 602 -
20 Avenue NW, LOC2018-0119**

Land Use

The subject property is currently designated Residential – Contextual One / Two Dwelling (R-C2) District, which is intended for development in the form of single detached, semi-detached and duplex dwellings in developed areas of the city. Single detached homes may include a secondary suite. The district allows for a maximum of two dwelling units and a building height of 10 metres.

The proposed Residential – Grade-Oriented Infill (R-CG) District is primarily intended for two to three storey (11 metres maximum) rowhouse developments where one façade of each dwelling unit must directly face a public street. The maximum density of 75 units per hectare would allow for up to four (4) dwelling units on the subject site.

The R-CG District also allows for a range of other low-density housing forms such as single detached, semi-detached and duplex dwellings. Secondary suites (one Backyard Suite or Secondary Suite per unit) are also allowable in R-CG developments. Secondary suites do not count against allowable density and do not require motor vehicle parking stalls in the R-CG District provided they are below 45 square metres in size.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

The Mount Pleasant Community Association was circulated this application and responded indicating general support, as the proposal provides opportunity for high density development on a busier street, that is often more difficult to redevelop.

One letter of opposition was received indicating the following concerns:

- Decreased green space (private lawn).
- Increased street parking.
- Increased traffic.
- Duplex development is an adequate density increase for this site.

Administration considered the relevant planning issues specific to the proposed redesignation and has determined the proposal to be appropriate. The design compatibility of discretionary uses with respect to the surrounding neighbourhood will be reviewed at the development permit stage.

Policy Amendment and Land Use Amendment in Mount Pleasant (Ward 7) at 602 - 20 Avenue NW, LOC2018-0119

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The subject parcel is located within the 'Residential - Developed - Inner City' area of the *Municipal Development Plan* (MDP). The applicable Municipal Development Plan policies encourage redevelopment of inner-city communities that is similar in scale and built form to existing development, including a mix of housing such as townhouses and rowhousing. The Municipal Development Plan also calls for a modest intensification of the inner city, an area serviced by existing infrastructure, public amenities and transit.

The proposal is in keeping with relevant Municipal Development Plan policies, as the rules of the R-CG District provide for development form that may be sensitive to existing residential development in terms of height, built form and density.

North Hill Area Redevelopment Plan (Statutory, 2000)

The subject parcel is located within the 'Low Density Residential Area' on Map 2 of the *North Hill Area Redevelopment Plan*. The Low Density Residential Area is intended to accommodate primarily single and semi-detached housing. To accommodate this proposal, a minor amendment is required to Map 2 of the Area Redevelopment Plan to identify the subject parcel as Low Density Residential or Low Density Multi Dwelling (Attachment 2).

The proposed amendments to the ARP are deemed appropriate given the intent and contextual nature of the proposed R-CG District.

Location Criteria for Multi-Residential Infill (Non-statutory, 2014)

While the proposed R-CG District is not a multi-residential land use, the *Location Criteria for Multi-Residential Infill* was amended to consider all R-CG redesignation proposals under these guidelines, as the R-CG allows for a building form comparable to other "multi-residential" developments.

The guidelines are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the Municipal Development Plan or local area policy plans, to assist in determining the appropriateness of an application in the local context.

The subject parcel meets the five of the eight location criteria as follows:

- located on a corner;
- within 400 metres of a transit stop;
- on a collector roadway;

Policy Amendment and Land Use Amendment in Mount Pleasant (Ward 7) at 602 - 20 Avenue NW, LOC2018-0119

- in close proximity to an existing activity centre (4 Street Main Street); and
- with direct lane access.

The subject parcel does not meet the following three location criteria as follows:

- within 600 metres of an existing or planned Primary Transit stop;
- adjacent to or across from existing or planned open space or park or community amenity; and
- adjacent to existing or planned non-residential development or multi-unit development.

The proposed moderate site intensification of this proposal is anticipated to have a minimal impact on adjacent properties, and is therefore considered appropriate.

Social, Environmental, Economic (External)

The recommended land use allows for wider range of housing types than the existing R-C2 District and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics.

An Environmental Site Assessment was not required for this application.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal conforms to the *North Hill Area Redevelopment Plan* as amended and is in keeping with applicable policies of the *Municipal Development Plan*. The proposed R-CG District is intended for parcels located near or directly adjacent to low density residential development. The proposal allows for a range of building types that have the ability to be compatible with the established building form of the existing neighbourhood and can better accommodate the housing needs of different age groups, lifestyles and demographics.

**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0890
Page 7 of 7**

**Policy Amendment and Land Use Amendment in Mount Pleasant (Ward 7) at 602 -
20 Avenue NW, LOC2018-0119**

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed Amendment to the North Hill Area Redevelopment Plan
3. Mount Pleasant Community Association Letter

Applicant's Submission

This is a proposal for Land-Use Redesignation from an existing R-C2 corner lot in Mount Pleasant to an R-CG designation to support future development of rowhousing. Located at 602 20th Av NW at the intersection of 20th Av and 5th St NW, the existing bungalow and storage garage will be removed and replaced with a proposed ground-oriented rowhouse development following a DP application in the future.

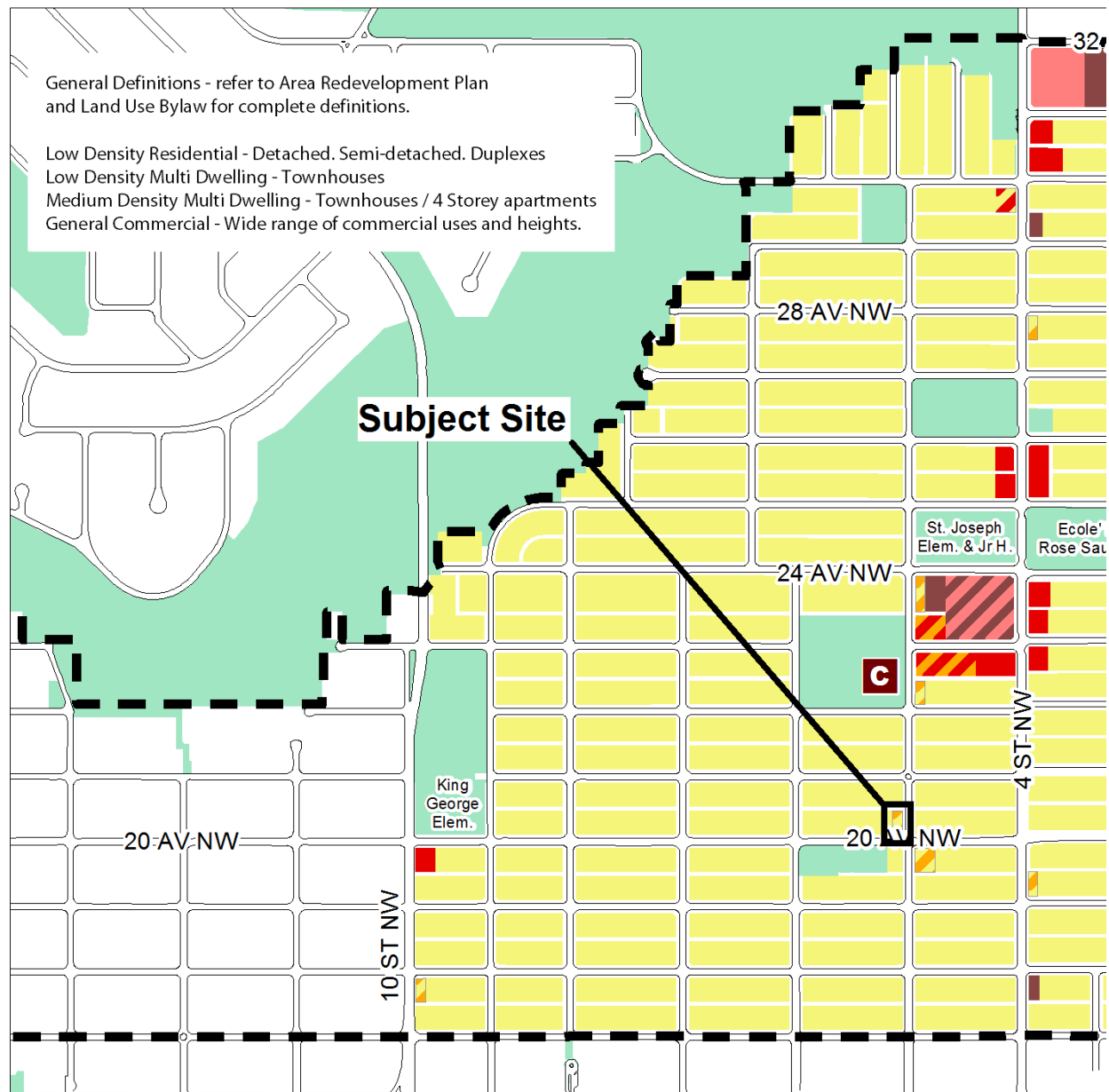
Mount Pleasant is a great location for increased density in the form of row housing, and several R-CG projects have been completed and are underway currently. The existing neighbourhood consists of a mix between single and semi-detached homes in R-C2 districts, with higher density and mixed uses spread throughout.

602 20th Av NW is: One block West of the #2 and #404 bus routes on 4th Street NW; Five blocks West of the #3, #300, and #301 bus routes on Centre Street North; Four blocks North of the #19 bus route on 16 Av NW; and, within three blocks of several #404 bus stops. This project is directly across 20 Av from a green space, within walking distance of Mount Pleasant Park and the community association, and a short walk from several businesses along 4th St NW and 16th Av NW.

A primary goal of building rowhousing projects is allowing more people to purchase in desirable neighbourhoods. With a smaller overall footprint per unit than a typical single or semi-detached dwelling, future proposed row house units will offer a more affordable listing price that will allow a greater diversity of people access to a great location.

Proposed Amendment to the North Hill Area Redevelopment Plan

1. The North Hill Area Redevelopment Plan attached to and forming part of Bylaw 7P99, as amended, is hereby further amended as follows:
 - (a) Amend Map 2 entitled “North Hill Area Redevelopment Plan”, by changing 0.06 hectares \pm (0.15 acres \pm) located at 602-20 Avenue NW (Plan 29340; Block 22; Lot 1,2) from “Low Density Residential” to “Low Density Residential or Low Density Multi Dwelling” as generally shown in the sketch below:



Mount Pleasant Community Association Letter

June 19, 2018

The Mount Pleasant Community Association is generally supportive of zoning such as this as brings higher density opportunities on busier streets which are often more difficult to redevelop. We prefer to see a development application at the same time as the Land Use Amendment so we can see the intended structure and use.

Chris Best

Mount Pleasant Community Association Board Director

Planning, Transportation and Land Use (PTLU) Committee Chair

**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0869
Page 1 of 6**

**Land Use Amendment in Westwinds (Ward 5) at 76 Westwinds Crescent NE,
LOC2018-0103**

EXECUTIVE SUMMARY

This application was submitted by Rick Balbi Architect on 2018 May 07 on behalf of the landowner 2047553 Alberta Ltd (Amarpreet Singh Bains). The application proposes to change the designation of the parcel from a Direct Control District Bylaw 103Z94 to an Industrial – Commercial (I-C) district to allow for:

- Industrial development with support commercial uses (e.g. warehouses with commercial storefronts, restaurants, retail stores, industrial buildings with offices and retail stores)
- The uses listed in the proposed I-C district, including medical clinic.

The applicant is interested in accommodating uses that are not provided for in the current DC District on the subject site, specifically a medical use.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.65 hectares \pm (1.60 acres \pm) located at 76 Westwinds Crescent NE (Plan 0410759, Block 5, Lot 10) from DC Direct Control District to Industrial – Commercial (I-C) district; and
2. Give three reading to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

Direct Control Bylaw 103Z94 initially covered the majority of the Westwinds industrial area, anticipating the development of a predominately light industrial community with limited ancillary commercial development and access restrictions to the adjacent collector roadway. As the area has developed, however, numerous land use redesignations have occurred in the northern end of the community, creating a diverse community of commercial, industrial, business and special purpose development within proximity to the McKnight Westwinds L.R.T Station and the residential community of Castleridge. The subject site currently retains the DC designation and is the last site in the Westwinds Industrial area to be developed.

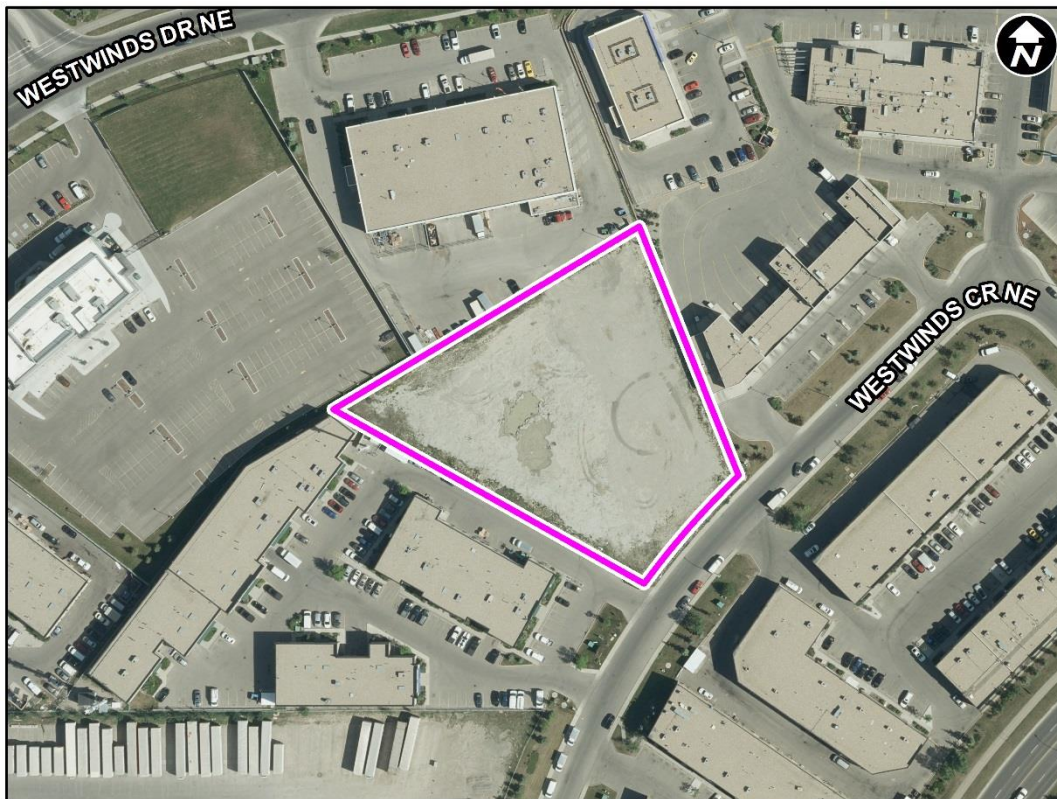
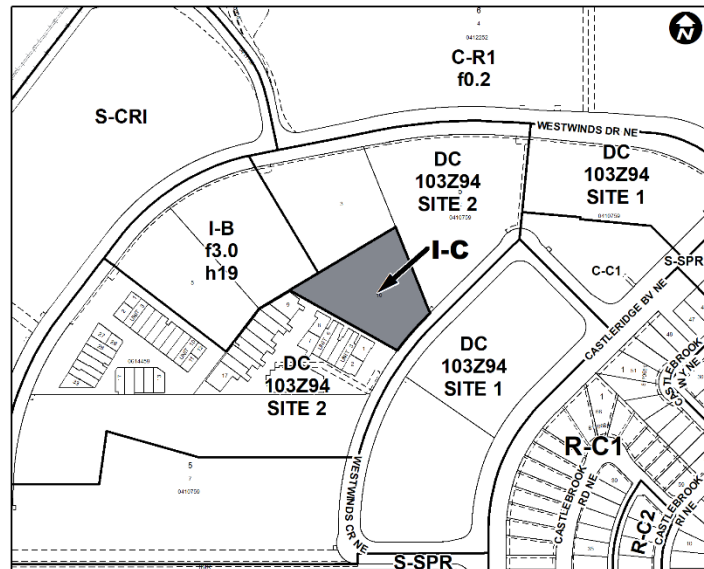
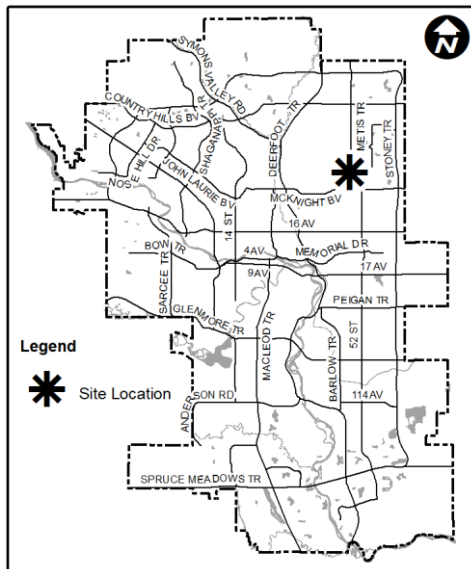
A development permit application (DP2018-1098) proposing two industrial buildings for warehousing and office uses was approved in mid-June 2018.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0869
Page 2 of 6

Land Use Amendment in Westwinds (Ward 5) at 76 Westwinds Crescent NE,
LOC2018-0103

Location Maps



**Land Use Amendment in Westwinds (Ward 5) at 76 Westwinds Crescent NE,
LOC2018-0103**

Site Context

The subject site is located in the northeast community of Westwinds, south of 64 Avenues NE and east of Castleridge Boulevard NE. While the south end of the community is predominately industrial, the area north of Prairie Winds park has developed as a mix of business industrial, general light industrial and community commercial land uses. The subject site is located within the northern portion of the community.

The site is approximately 0.65 hectares \pm (1.60 acres \pm) in size, and is currently undeveloped and relatively flat with no landscaping on site. Immediately adjacent to the site are light industrial developments that fall within the same DC District as the subject property, with the exception of Calgary Police Association building located behind the subject site and which is designated as an Industrial – Business (I-B) district. A Commercial – Community 1 (C-C1) District site developed as a strip mall exists 190 metres from the site and a regional commercial (C-R1) District site exists 750 metres from the site.

The area is well served with public transit (bus and L.R.T) and is adjacent to the low-density residential area of Castleridge, which exist directly east of the Westwinds community.

As a non-residential area, there is no population, demographic or socio-economic data for the Westwinds community.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposed redesignation would support the industrial focus on the Westwinds community, while providing opportunity for small scale compatible commercial development in proximity to L.R.T services and a residential community.

Land Use

The existing DC District is based on the (I-2) Industrial 2 District of the previous 2P80 Land Use Bylaw, and allows for a wide range of light industrial uses, as well as a variety of commercial uses. There are limits, however, to the amount of commercial uses permitted (10 percent of floor area) and access restrictions to Castleridge Boulevard NE.

The proposed Industrial – Commercial (I-C) District allows for light industrial uses and small scale commercial uses that are compatible and complementary to the light industrial focus. Retail and consumer service uses are limited in their allowable use area. As the I-C district provides a greater variety of land uses than the existing DC District, the applicant has chosen to pursue a land use redesignation to I-C for this property.

During the review of the application, alternative land use options were explored by Administration and the applicant to enable the most suitable interface between the subject property and the larger, existing Westwinds community. Given the diversity of land uses already established in the area, which include industrial, business and commercial, and the proximity to both a L.R.T Station and the residential community of Castleridge, the I-C District is believed to be appropriate.

Land Use Amendment in Westwinds (Ward 5) at 76 Westwinds Crescent NE, LOC2018-0103

Infrastructure

Transportation Networks:

The subject site is located within a 1.1 kilometre or 13 minute walk to the McKnight Westwinds C-Train Station, and 290 metre or 4 minute walk to bus stops on Castleridge Boulevard. The Industrial Arterial roadways of 64 Avenue NE and Metis Trail NE are in close proximity. Access to the site is via Westwinds Crescent NE and there is no rear lane.

Utilities and Servicing:

Water, sanitary and storm sewer mains are available to service the site and can accommodate the proposed land use without the need for off-site improvements at this time.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advised.

There is no community association established for the Westwinds community.

Administration received letters of objection from two citizens, one of whom represented the neighbouring development's condo board. The following concerns were cited:

- Bottle Depot is a permitted use within the I-C district, which is an inappropriate use in proximity to businesses that serve children.
- Land use districts should be the same for properties with the same street access.
- On-street parking on Westwinds Crescent makes the area congested. High traffic operations are not appropriate in this area.
- Concerns that the development will not provide enough on-site parking.
- Industrial zoning for this parcel is most appropriate.

Administration considered all relevant planning issues specific to the proposed redesignation and addressed the issues as follows:

- Currently, both industrial and commercial land uses have access via Westwinds Crescent.
- Based on the approved development permit, 61 parking stalls will be provided on-site, while only 23 stalls are required for the current warehousing and office land uses. Sufficient on-site parking will be available to support the small-scale commercial

Land Use Amendment in Westwinds (Ward 5) at 76 Westwinds Crescent NE, LOC2018-0103

development allowable within the I-C district. Any parking relaxations would be evaluated at the development permit stage.

- The Westwinds area has developed with a diversity of land uses, including industrial, business and commercial, suggesting that small-scale compatible commercial uses are supported and appropriate in this area.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory – 2009)

The Municipal Development Plan (MDP) identifies the subject site as *Industrial – Employee Intensive*. The intent of this area is to provide for manufacturing, warehousing and mixed industrial/office developments that have high labour concentrations and require access to Primary Transit Network. Industrial-Employee Intensive areas should contain predominantly industrial uses. Other uses that support the industrial function may be allowed.

According to the MDP, industrial areas must offer flexibility to support a variety of uses while remaining predominantly industrial and resist the encroachment of non-industrial uses.

The proposed land use redesignation will continue to require predominantly industrial uses on the subject site, while providing the opportunity for small-scale compatible commercial uses for the benefit of employees of the area and residents of the adjacent residential community.

Local Area Plan

There is no local area plan for the Westwinds Community.

Social, Environmental, Economic (External)

This proposal will contribute and support the predominately industrial area of Westwinds, while providing local amenities for employees of the area and residents of the adjacent residential community.

An environmental site assessment was not required for his application.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0869
Page 6 of 6

**Land Use Amendment in Westwinds (Ward 5) at 76 Westwinds Crescent NE,
LOC2018-0103**

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed land use amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposed land use redesignation is aligned with applicable policies identified in the *Municipal Development Plan* by maintaining a predominately industrial land use on the subject site. Opportunities for small-scale compatible commercial uses will be allowed within close proximity to L.R.T services and for the benefit of both the employees of the area and residents of the adjacent residential community. The proposal has the ability to complement the existing mix of light industrial, commercial, business and community infrastructure land uses already established within the Westwinds community.

ATTACHMENT(S)

1. Applicant's Submission

Applicant's Submission

This application is for a proposed Land Use Redesignation in the Community of Westwinds, from Direct Control (bylaw 103Z94) to Industrial - Commercial (I-C) to provide increased flexibility of use while retaining the general intent of the area.

The subject property is located at 76 Westwinds Crescent NE and has a total area of approximately 1.6 acres. It is located centrally within the general area bounded by Metis Trail NE to the west, 64 Avenue NE to the north, Castleridge Boulevard NE to the east and Westwinds Drive NE to the south. The site is accessed via Westwinds Crescent NE and does not abut Castleridge Boulevard or Westwinds Drive.

The currently vacant site is located in a larger area covered by bylaw 103Z94, which is based on the I-2 district of bylaw 2P80 and additionally addresses interface and access onto major streets, particularly Castleridge Boulevard and Westwinds Drive. Over time, several properties within this area have been redesignated to reflect purely commercial districts, resulting in a broad mix of both industrial and commercial uses on the immediately surrounding parcels.

While there is currently no local-level planning policy in place, the parcels are identified within the Municipal Development Plan as within the Industrial – Employee Intensive area, which recommends a minimum employee threshold of 100 jobs per gross developable hectare with predominantly industrial uses.

The intent of this application is to accommodate additional uses, specifically medical, within one of two proposed buildings. The proposed buildings will be industrial in nature, with the majority of the bays reflecting light industrial office/warehouse. A development application is currently underway for the development of this site; however, it is not being requested concurrently with this land use application. Based on the intended development of this site, the recommended employee threshold is achievable with purely light industrial uses, and this would be expected to increase with the increased flexibility provided by the I – C district.

Given that the proposed district supports the goals of the Municipal Development Plan and is in alignment with surrounding development, we would respectfully request your support of this application.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0849
Page 1 of 6

Land Use Amendment in Sunnyside (Ward 7) at 902 and 904 – 2 Avenue NW, LOC2018-0097

EXECUTIVE SUMMARY

This application was submitted by O2 Planning and Design on 2018 May 02 on behalf of the landowner Robert Froese. The application proposes to change the designation of the properties from Multi-Residential – Contextual Grade-Oriented (M-CGd72) District to a DC Direct Control District to allow for:

- multi-residential buildings (e.g. townhouses, apartment buildings);
- a maximum building height of 16 metres (an increase from the current maximum of 12 metres);
- a maximum building floor area of approximately 1,400 square metres based on a building floor to parcel area ratio (FAR) of 2.5; and
- the uses listed in the DC(M-C2) designation with the additional uses of both a secondary suite and backyard suite on the same parcel.

The proposal allows for a land use with a density and height that are compatible with surrounding development and in alignment with the applicable policies of the *Municipal Development Plan* and the *Hillhurst/Sunnyside Area Redevelopment Plan*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.13 acres \pm) located at 902 and 904 – 2 Avenue NW (Plan 2448O, Block 11, Lots 21 and 22) from Multi-Residential – Contextual Grade-Oriented (M-CGd72) District **to** DC Direct Control District to accommodate multi-residential development with density bonus, with guidelines; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

Land Use Amendment in Sunnyside (Ward 7) at 902 and 904 - 2 Avenue NW, LOC2018-0097

Site Context

The subject parcels are located along Second Avenue NW, at the northwest corner of Second Avenue and Eighth Street NW in the community of Sunnyside. The sites are approximately 350 metres from the LRT platform and within walking distance of downtown. The subject sites are currently occupied by single detached dwellings with detached garages facing the lane. Surrounding the subject site are a mix of single and semi-detached dwellings as well as multi-residential developments to the east and south.

As identified in *Figure 1*, the community of Sunnyside reached its population peak in 2017.

Figure 1: Community Peak Population

Sunnyside	
Peak Population Year	2017
Peak Population	4,206
2017 Current Population	4,206
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online on [Sunnyside](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for a range of building types that have the ability to be compatible with the established building form of the existing neighbourhood. The proposal meets the objectives of applicable policies as discussed in the Strategic Alignment of this report.

Land Use

The current land use district for the site is Multi-Residential – Contextual Grade-Oriented (M-CGd72) District. This would allow for a multi-residential building on the site with a building height of 12 metres or approximately three storeys. The density of 72 units per hectare within the district would allow for approximately 4 units on both parcels.

The proposed land use district is a DC Direct Control District based on the Multi-Residential – Contextual Medium Profile (M-C2) District of Land Use Bylaw 1P2007 (Attachment 2). Section 20 of the Land Use Bylaw indicates that DC Direct Control Districts must only be used for developments that, due to their unique characteristics, innovative ideas or unusual site constraints, require specific regulation unavailable in other land use districts. A Direct Control District has been used for this application to allow for specific density bonus provisions in the Hillhurst/Sunnyside Area Redevelopment Plan. In addition to allowing for medium density multi-residential development, the key components of Direct Control District include:

Land Use Amendment in Sunnyside (Ward 7) at 902 and 904 - 2 Avenue NW, LOC2018-0097

- it allows for a maximum height of 16.0 metres and a maximum density of 2.5 FAR;
- it allows for both a secondary suite and a backyard suite on each parcel, while also exempting them from the parking requirement; and
- it allows for the implementation of the density bonus provisions in the *Hillhurst/Sunnyside Area Redevelopment Plan*. The Area Redevelopment Plan allows for an increase in density to a maximum floor area ratio of 2.5 through the density bonus provision.

Implementation

This application is not tied to plans. No development permit has been submitted at this time. The applicant has indicated the intent to pursue development permit applications for secondary suites and backyard suites on each parcel. The overall size of the suites, required parking and any other site planning consideration will be evaluated at the development permit stage subject to Council's decision on this land use redesignation application.

Infrastructure

Transportation Networks

The subject site is located adjacent to transit stops for several bus routes on Second Avenue NW as well as 350 metres from the Sunnyside LRT Station. Vehicular access is available from the existing rear lane. A Traffic Impact Assessment was not required as part of this application.

Utilities and Servicing

Water, sanitary and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. Individual servicing connections as well as appropriate stormwater management will be considered and reviewed at development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised on-line.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

The Hillhurst Sunnyside Planning Committee (HSPC) was circulated as part of this application. A letter was submitted by the HSPC, which indicated general support for the land use amendment (Attachment 3). However, they did raise a number of points regarding the potential new development which will be addressed through a future development permit for the site. These included the built form, heritage, parking and traffic.

**Land Use Amendment in Sunnyside (Ward 7) at 902 and 904 - 2 Avenue NW,
LOC2018-0097**

There were four letters received from the surrounding residents. Two of these was in support of the proposal and two were in opposition to the proposed land use. Those opposed were concerned with loss of trees, too many people on a small lot, loss of sunlight as well as traffic and parking issues.

Engagement

While the applicant did not hold a formal public open house, they informed the neighbours of the application, presented their proposal to a Jane's Walk in the community, presented at a Community Association meeting and provided updates on a project webpage.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP). While the SSRP makes no specific reference to the site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The subject site is located within the Developed Residential –Inner City Area as identified on Map 1 of the *Municipal Development Plan*. Both City-Wide policies and Inner City Area policies apply. In general, these policies encourage redevelopment in inner city communities that is similar in scale and built-form to existing development, including a mix of housing. In addition, *Municipal Development Plan* policies encourage higher residential densities in areas that are more extensively served by existing infrastructure, public facilities, and transit.

Hillhurst/Sunnyside Area Redevelopment Plan (Statutory, 1988)

The subject site falls within the Transit Oriented Development Area of the Area Redevelopment Plan, approved by Council in 2009 February. The subject site is situated in the area identified as Medium-Density on the Land Use Policy Area Map of the Area Redevelopment Plan. This area has the ability to accommodate small-scale urban infill development and provide new housing choices that fit within the existing community character and strengthen the pedestrian-friendly nature of the neighbourhood. The proposed land use amendment would include a maximum density of 2.5 FAR and a building height of 16 metres in alignment with the Area Redevelopment Plan.

In 2012 November, Council approved an amendment to the *Hillhurst/Sunnyside Area Redevelopment Plan* to include density bonus provisions, which allow for a density increase up to the maximum floor area ratio specified in the Area Redevelopment Plan. The density increase is subject to a contribution to the community amenity fund. This fund has been established as a means of gaining public amenities in exchange for a level of density that surpasses the allowable base density under the provisions of the land use district.

**Land Use Amendment in Sunnyside (Ward 7) at 902 and 904 - 2 Avenue NW,
LOC2018-0097**

Social, Environmental, Economic (External)

The recommended land use allows for wider range of housing types than the existing land use district and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics.

An Environmental Site Assessment was not required for this application.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the *Municipal Development Plan* and the *Hillhurst/Sunnyside Area Redevelopment Plan*. The proposal represents a modest increase in density for this inner city parcel of land and allows for a development that can be compatible with the character of the existing neighbourhood. In addition, the subject parcel is located within walking distance of several transit stops, and has direct lane access.

ATTACHMENT(S)

1. Applicant's Submission
2. Proposed Direct Control District Guidelines
3. Hillhurst/Sunnyside Community Association Letter

Applicant's Submission

This application proposes to redesignate the parcels located at 902 and 904 2nd avenue northwest from Multi-Residential - Contextual Ground Oriented (M-CG d72) to a Direct Control based on Multi-Residential – Contextual Medium Profile (M-C2). The proposed redesignation will bring the land use of the subject parcels in conformance with the policies for this area of Sunnyside as outlined in the Hillhurst-Sunnyside Area Redevelopment Plan (ARP).

The subject parcels sit within the area designated as “medium-density” in the ARP. The suggested maximum density for this area is 2.5 FAR and the suggested maximum height is 16 metres. The MC-2 designation aligns with the policy objectives of the ARP. The DC is required to implement the bonus density provisions of the Hillhurst-Sunnyside ARP.

The owner of the site, Sunnyside Sustainable Living, has no immediate intentions to develop to the full potential of M-C2, which is typically delivered in a built form of 4-5 storey multi-residential buildings. Sunnyside Sustainable Living's near-term development plan is to develop a basement suite in the principal residence on each parcel and a laneway home on each parcel. The land use redesignation will allow for these development intentions to proceed while bringing the land use designation in alignment with the ARP objectives. This direction was provided by the City Planning department and the area Councillor's office so that a future land use redesignation is not required.

The proposed addition of a basement unit and laneway suite will contribute to the ongoing evolution of Sunnyside and will include the following key attributes:

- **Community Character:** The retention of the two existing 1900 era bungalows maintains the existing character of Sunnyside.
- **Transit Oriented Development:** Situated within 200 metres of the Sunnyside LRT station, the development contributes to the evolution of Hillhurst/Sunnyside as a complete transit oriented community.
- **Moderate Density:** The addition of two units on the site aligns with the the City of Calgary goal of encouraging redevelopment to increase density and create a pedestrian-friendly environment.
- **Affordability:** The addition of a basement suite and laneway home provides housing opportunities for families to live in Sunnyside at a lower price point than, more affordable price point.
- **Lane activation.** The addition of laneway homes on the two parcels will activate the lane by creating more living activity and makes good use of existing infrastructure.

Proposed Direct Control Guidelines

Purpose

- 1 This Direct Control District is intended to:
- (a) provide for medium **density** mid-rise multi-residential **development** in compliance with the policies of the applicable local area redevelopment plan; and
 - (b) implement the **density** bonus provisions of the Hillhurst/Sunnyside Area Redevelopment Plan.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The **permitted uses** of the Multi-Residential – Contextual Medium Profile (M-C2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Discretionary Uses

- 5 The **discretionary uses** of the Multi-Residential – Contextual Medium Profile (M-C2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District.

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Multi-Residential – Contextual Medium Profile (M-C2) District of Bylaw 1P2007 apply in this Direct Control District.

Floor Area Ratio

- 7 (1) Unless otherwise referenced in subsection (2), the maximum **floor area ratio** is 0.72.
- (2) The maximum **floor area ratio** may be increased to 2.5 in accordance with the **density** bonus provisions contained in section 8 of this Direct Control District.

Density Bonus

- 8 (1) For the purposes of this section: “Cash Contribution Rate” means: \$17.85 per square metre for the year 2018. The Cash Contribution Rate will be adjusted annually on January 1 by the **Development Authority**, based on the Statistics Canada Consumer Price Index for Calgary.
- (2) A **density** bonus may be earned by a contribution to the Hillhurst/Sunnyside Community Amenity Fund, such that:
- Cash Contribution Amount = Cash Contribution Rate x Total floor area in square metres above the **floor area ratio** of 0.72.

Proposed Direct Control Guidelines

- (3) A **density** bonus may be earned by the provision of an off-site improvement in accordance with Section 3.1.5.4 of the Hillhurst/Sunnyside Area Redevelopment Plan, where the allowable bonus floor area in square metres is equal to the cost of construction of the off-site improvement divided by the Cash Contribution Rate, such that:

Allowable bonus floor area = Total construction cost of the off-site improvement / Cash Contribution Rate.

Total construction cost will not include any construction costs necessary to fulfill the infrastructure requirements of a **development permit** for a **development** equal to or less than a **floor area ratio** of 0.72. Details of the construction cost will be determined through the **development permit** process.

Accessory Suite – Density

- 9 (1) There must not be more than one **Secondary Suite** located on a **parcel**.
- (2) There must not be more than one **Backyard Suite** located on a **parcel**.
- (3) A **Secondary Suite** and a **Backyard Suite** may be located on the same **parcel**.
- (4) A **Secondary Suite** or a **Backyard Suite** must not be separated from the main residential **use** on a **parcel** by the registration of a condominium or subdivision plan.

Backyard Suite and Secondary Suite Motor Vehicle Parking Stalls

- 10 There is no **motor vehicle parking stalls** requirement for **Backyard Suite** or **Secondary Suite**.

Hillhurst/Sunnyside Community Association Letter

June 8, 2018

Steve Jones, M.Pl., MCIP, RPP
Senior Planner
Community Planning (North Team)
The City of Calgary

Emailed to: steve.jones2@calgary.ca

RE: LOC2018-0097 | 902 & 904 2nd Avenue NW | Land Use Amendment from M-CGd72 to Direct Control M-C2 District

Dear Mr. Steve Jones,

The Hillhurst Sunnyside Planning Committee (HSPC) is pleased to offer comments on the above application. We understand that the applicant/developer, Sunnyside Sustainable Living intends to rezone the site from its current M-CGd72 *Multi-Residential Contextual Grade-Oriented* designation which allows for two residential units per parcel to MC-2 *Multi-Residential – Contextual Medium Profile* District to allow for three residential units per parcel.

The application seeks a maximum height of 16m (or 4-5 storeys) and 2.5 FAR so that a future rezoning application is not required. The HSCA sees no reason that City should not request a concurrent application since the developer's intentions can change or the property sold. If the developer already knows how they want to develop the parcels, then a concurrent application seems appropriate.

While we are generally in support of the Land Use Redesignation, we have some concerns. We also feel it is important to highlight special considerations to decision-makers at City Council and provide further local context to the application. We have additional comments to provide to the applicants ahead of the DP submission.

Hillhurst Sunnyside Area Redevelopment Plan (ARP)

We understand that the applicant seeks to achieve the TOD vision through the provision of middle housing. The *Medium Density Area* of the Transit Oriented Development Area plan concept calls for modest increases to density, including small scale infill residential development, such as through laneway housing and secondary suites.

The application proposes both a modest density increase by creating separate dwellings on the main floor, the basement and the addition of laneway suites in the future (lane activation is envisioned in the community-led and ongoing Bow to Bluff or B2B initiative). Laneway suites should also be designed in a sensitive manner with input from affected neighbours to reduce privacy concerns. Any future development on the alley should consider burying the overhead power lines and improvements to visibility for safer mobility at the end of the lane.

Hillhurst/Sunnyside Community Association Letter

Affordable housing is paramount to the ARP. Most of the new infill housing stock or condo buildings built to date include 1-2 bedrooms catered to the higher income demographic. The ARP provides vision for diversity in housing, including the specific recommendation for affordable family-oriented housing. The applicant has expressed interest in partnering with local non-profit housing agencies as to provide affordable housing for young families, help support the local elementary school and promote build on the diversity of Sunnyside.

Heritage

Both buildings are in the process of restoration, which has seen very positive reactions from the community. We have yet to see increased heritage retention; the ARP states that *the conservation and enhancement of heritage resources...will be encouraged as will the sensitivity of renovation and new development to the existing character of heritage buildings*. At this time, only two residences in Sunnyside have been designated as protected Municipal Historic Resources. **In exchange for additional density and additional economic benefits to the owner, we strongly recommend that these homes be designated and protected from future development.**

Mobility

2 Avenue NW is the main street of Sunnyside. 2 Avenue also sees a significant amount of speeding and cut-through traffic to bypass congestion on Memorial Drive NW. We submitted several Traffic Study Requests in the immediate area to City Transportation Planning in recent years; however we have not been approved for any traffic calming improvements in Hillhurst Sunnyside to assist with pedestrian, bike and automobile safety. We would further like to work with the City with traffic management as per the Mobility Assessment & Plan considerations under Section 4.3.8 of the ARP.

The applicant had indicated that their discussion with the area councillor included a sidewalk extension at the northwest corner of 8 Street and 2 Avenue NW. We request coordination with another condo redevelopment project in the area, DP2017-3511 as that developer has committed to funding a crosswalk with flashing lights at the east side of 2 Avenue and 8 Street connecting to Sunnyside Elementary School.

We support the reduction of parking as the site is within the Transit Oriented Development area, as a *complete community* with full gamut of services and city bus and car-sharing services. The current intended demographic could be less inclined to own a vehicle, but this may change in the future. Per the ARP (p.92), *"Dwellings in new multi-family developments are not to receive parking passes regardless of their off-street parking provisions"*.

Bow to Bluff

The objectives of the 2012 Bow to Bluff Urban Design Framework describe the need for individual development in the area to form a cohesive part of the 9A Street/LRT corridor. Bow to Bluff promotes developments that strengthen pedestrian connections and public realm recommendations. As B2B is no longer funded by Council's Enmax Parks Legacy Fund (originally funded for approximately \$5.7 million), we request that City Council uphold its promise to the community and provide funding for Bow to Bluff.

Community Engagement

Public engagement from the applicant has been very positive:

- The developer/applicant has kept the HSPC and the neighbours informed about this application several months before the application was submitted

Hillhurst/Sunnyside Community Association Letter

- The applicant presented his vision to approximately 40 participants on the HSCA-hosted Jane's Walk on alternative and affordable housing on May 5
- The applicant's team presented their application to the HSCA planning committee on May 8
- The applicant continues to provide project updates on the Sunnyside Sustainable Living social media page (facebook.com/sunnysidegrocery)
- The applicant informed us that there will be additional engagement once the Development Permit is submitted

Please contact the undersigned should you have any questions.

Sincerely,

Hillhurst Sunnyside Planning Committee
Hillhurst Sunnyside Community Association

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0841
Page 1 of 12

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 438 - 8 Street NE, LOC2017-0127

EXECUTIVE SUMMARY

This land use redesignation application was submitted by O2 Planning and Design on 2017 April 27 on behalf of the landowners Yasmin Kanji and Shafik Kanji. This application proposes to change the designation of this property from Residential – Contextual One / Two Dwelling (R-C2) District to a ‘tied to plans’ DC Direct Control District to allow for a specific multi-residential development on this property. The intent of the land use approach is twofold, one of which is to enable moderate intensification of two additional units, as well as enable redevelopment of this site without presupposing the planning outcomes or Council decision on the new policy planning work Bridgeland-Riverside community is currently undergoing.

To evaluate the feasibility of a ‘tied to plans’ land use approach, a development permit level of analysis is required. As such, this redesignation application is accompanied by a development permit application (DP2017-1699) submitted on 2017 April 27. The development permit proposes a three-storey multi-residential building with four residential units. A simplified version of the development permit drawings are ‘tied’ to the proposed DC Direct Control District as Schedule C. The Development Authority is prepared to approve the development permit application, subject to Council’s decision on this land use and policy amendment application.

An amendment to the *Bridgeland-Riverside Area Redevelopment Plan* (ARP) is required to accommodate the proposed land use redesignation discussed further. The proposal conforms to the ARP as amended, and is in keeping with applicable policies of the *Municipal Development Plan*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to the Bridgeland-Riverside Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares ± (0.11 acres ±) located at 438 – 8 Street NE (Plan 4647V, Block 115, Lot 11) from Residential – Contextual One / Two Dwelling (R-C2) District **to** DC Direct Control District to accommodate a specific multi-residential development as a permitted use with guidelines (Attachment 3); and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0841
Page 2 of 12Page 1 of 12**

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
438 - 8 Street NE, LOC2017-0127**

BACKGROUND

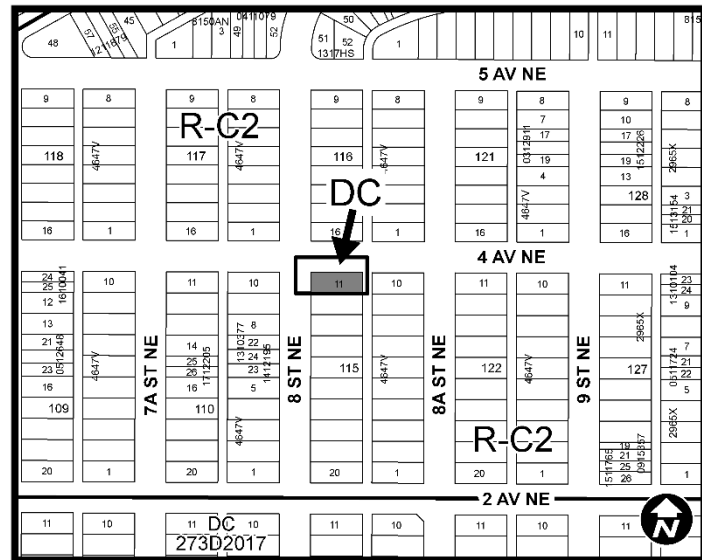
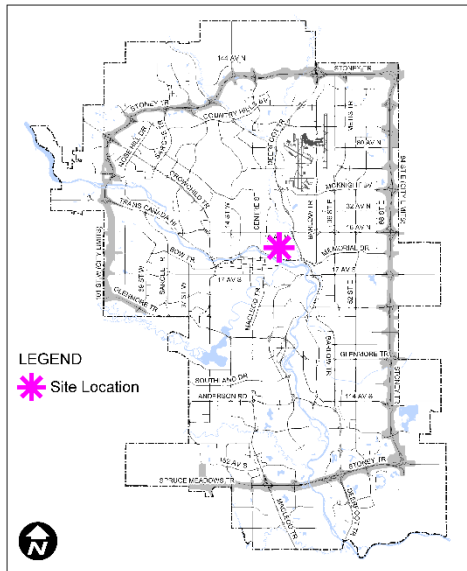
The community of Bridgeland-Riverside is subject to the policies of the *Bridgeland-Riverside Area Redevelopment Plan* (ARP) originally approved on 1980 September 3. A new ARP is in the process of being created by The City in consultation with the Bridgeland-Riverside Community Advisory Group and the greater community. It is anticipated that the ARP work will be presented to Council by Winter 2018/ Spring 2019.

In the interim, The City cautions applicants in advancing with applications within the community that propose redevelopment and are not aligned with the existing policy. If there are issues from the applicant's perspective with delaying their decision dates to align with the new ARP, The City will continue to process and make recommendations to the Calgary Planning Commission. Recommendations for land use amendments and decisions for development permits will be based upon the analysis of the compatibility of the proposal with the surrounding land use and context.

**Planning & Development Report to
Calgary Planning Commission
2018 July 26
Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
438 - 8 Street NE, LOC2017-0127**

**ISC: UNRESTRICTED
CPC2018-0841
Page 3 of 12**

Location Maps



Site Context

The subject site is located in the community of Bridgeland-Riverside south of 4 Avenue NE and east of 8 Street NE. Surrounding development is characterized primarily by a mix of single and semi-detached homes. The predominant land use in this area is Residential - Contextual One / Two Dwelling (R-C2) District.

The site is approximately 0.05 hectares in size with approximate dimensions of 13.58 by 33.51 metres. A rear lane exists to the east of the site. The property is currently developed with a fenced in, abandoned single detached dwelling, that is in a state of disrepair. The land is primarily flat with the largest grade difference being 0.67 metres from the south-east corner, sloping downwards to the north-east corner, along the lane.

As identified in *Figure 1*, the community of Bridgeland-Riverside has seen population growth over the last several years reaching its population peak in 2017.

Figure 1: Community Peak Population

Bridgeland-Riverside	
Peak Population Year	2017
Peak Population	6,332
2017 Current Population	6,332
Difference in Population (Number)	± 0
Difference in Population (Percent)	0%

Additional demographic and socio-economic information may be obtained online through the [Bridgeland- Riverside](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for a specific multi-residential development that is compatible with the established built form of the existing neighbourhood. Though a minor amendment to the *Bridgeland-Riverside Area Redevelopment Plan* (ARP) is required, the proposal generally aligns with the objectives of applicable policies as discussed in the Strategic Alignment of this report.

Land Use

The existing Residential – Contextual One / Two Dwelling (R-C2) District is a designation in developed areas of the city that is primarily for single detached, semi-detached and duplex homes. Single detached homes may include a secondary suite. The R-C2 District allows for a maximum building height of 10 metres and a maximum of two dwelling units.

The proposed DC Direct Control District (Attachment 3) is based on the existing R-C2 District with the additional permitted use of Permitted Development. The proposed permitted use (i.e. Permitted Development) refers to Schedule C of the proposed DC Direct Control District which includes simplified development drawings that are consistent with the development permit application (DP2017-1699), for a three-storey multi-residential building with four residential units

that has been reviewed concurrently with this land use redesignation application. The four residential units are two additional units to what would have been allowable under the base R-C2 District.

The purpose of the simplified development drawings is to identify the key elements of the proposed development to provide certainty for the building's number of units, height, form and location on site. Developments which conform to Schedule C will be a permitted use. The development permit is consistent with the simplified drawings contained in Schedule C of the proposed DC District.

The proposed DC District also ensures that if this specific development is not built, the rules of the existing R-C2 District would continue to apply. No other forms of multi-residential development would be allowed on this parcel without a new land use redesignation. What this does is provide certainty to the community that if the concurrent development is not built, the base district rules then apply.

This land use approach was developed following a review of an initial development permit application that proposed a slightly less compatible building in height and form to the surrounding built form. In working with the applicant, the height, form and other design changes were made to make the development more compatible with adjacent properties. This concurrent and 'tied to plans' approach reflects the substantial commitment to a creative design that is a contextual fit, and ensures a high level of certainty for all stakeholders.

Urban Design

The concurrent development permit application (DP2017-1699) was reviewed by The City's Urban Design Review Panel (UDRP) even though the scope of the application does not trigger a review by UDRP. Administration's City Wide Urban Design team also provided comments on the application.

The UDRP comments (Attachment 6) were strongly supportive of the proposed development, citing how the innovative proposal added a new residential typology and greater diversity in Bridgeland-Riverside while offering well-designed street frontages that enhance the adjacent avenue and street. The review also encouraged further study of the exteriors of both the third floor level and garage to further blend in the massing with the surrounding context. This informed the final revisions to the development, as found in Schedule C of the proposed DC Direct Control District (Attachment 3) and the comprehensive overview of the project (Attachment 8). The applicant's response to UDRP's comments is also included under Attachment 7.

Infrastructure

Transportation Networks

The subject site is within a four minute walk (approximately 350 metres) from transit stops for Routes 90 along 1 Avenue NE, and a nine minute walk (approximately 600 metres) from transit stops for Route 17 and Routes 19, 745 and 872 along 8 Avenue NE. Transit stops for Route 5

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
438 - 8 Street NE, LOC2017-0127**

along Edmonton Trail is a nine minute walk (approximately 700 metres). The Bridgeland - Memorial C-Train Station is also within a twelve minute walk (approximately 1000 metres).

Vehicular access is available from the existing rear lane to the east. This provides access to the proposed attached garage providing the four unit development with four residential stalls. No visitor stalls are proposed on site. A traffic impact assessment was not required as part of this application or the associated development permit application.

Utilities and Servicing

Water and sanitary sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. Storm sewer is not immediately available for connection, but appropriate stormwater management solutions are being evaluated and reviewed through the concurrent development permit process as part of DP2017-1699.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

The Bridgeland-Riverside Community Association was circulated as part of this land use application, in its initial application stage. A letter was submitted (Attachment 4) indicating that they do not support the redesignation for the following summarised reasons:

- this redesignation should wait for the new Area Redevelopment Plan (ARP) that will determine suitable areas for increased density through a community wide engagement process, rather than let this application set a precedent without a larger vision in place;
- the proposed development is contextually inappropriate due to the lack of similarly higher density built forms in close proximity to the proposed; and
- there are ample other locations more appropriate for higher density within the community, including the new Main Street area undergoing redesignation.

When the Community Association provided their letter at the initial circulation stage, the application was not pursuing a DC Direct Control District based on the Residential – Contextual One / Two Dwelling (R-C2) District with the additional Permitted Use of Permitted Development. This approach was formulated during the review process to respond to community concerns while also supporting a high-quality design development. Therefore, the following comments pertain to the associating development permit, prior to the DC District and so are no longer applicable:

- there is no reassurance if the land use is adopted that the concurrent development permit would be the development realised on the site; and

**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0841
Page 7 of 12 Page 1 of 12**

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
438 - 8 Street NE, LOC2017-0127**

- there is a risk that this is an up-zoning exercise that could lead to the parcel being sold off and developed with another multi-residential development.

Finally, the Community Association also included the following in their letter:

- a sketch emphasizing the areas where density is welcomed within the community;
- a review of the proposal against the current ARP policies; and
- a review of the proposal against the *Location Criteria for Multi-Residential Infill* guideline.

Administration received 22 letters from adjacent landowners in response to the circulation and notice posting of the site. Three of these were in support for the increased densification, while the other 19 were in opposition. The opposition letters expressed concerns with the following:

Land Use-related comments:

- there are sufficient alternative locations for high density living in Bridgeland such as the Main Street project area, Edmonton Trail and in the Riverside area;
- until the areas currently identified for higher density within Bridgeland are completely built out, the existing R-C2 parcels should not be redesignated;
- the LOC is not tied to plans providing no guarantee that the proposed project is what will get built;
- the historical integrity of the neighbourhood should be protected;
- approving such a redesignation will set a dangerous precedent;
- adding three more units to the land use is not an effective densification strategy- too low of a number;
- spot zoning lead to disjointed development decisions; and
- spot zoning lead to loss of neighbourhood charm.

Development Permit-related comments:

- the proposed fourplex will attract more renters rather than home owners;
- the proposed multi-residential typology does not fit with the surrounding single family typology;
- the development proposes limited landscaping;
- concern with the proposed development's height exceeding what would otherwise be allowable under the R-C2, causing adverse impact on adjacent houses;
- the proposed does not acknowledge the historical and architectural significance of adjacent homes;
- approving a lot coverage of more than 45 percent will negatively change the future of the mixed context community;
- the modern development does not fit with the neighbourhood's older character feel;
- the lane is not able to accommodate the four separate car accesses and required number of waste and recycling bins;
- the proposed negatively impacts the adjacent parcel to the south through the proposed large building footprint, massing, solid wall treatment to the south,

shadowing of their backyard, and privacy issues caused by direct sight lines into the backyard; and

- the proposed negatively impacts the adjacent parcel to the east through the proposed massing, resulting in privacy issues caused by direct sight lines into the backyard.

Parking-related comments:

- a visitor stall is not provided, leading to an adverse impact on on-street parking availability if the development is approved;
- if the development gets approved, off-street parking requirements should be adhered to; and
- current lack of on street parking (partly due to the proximity of several churches) will be further aggravated by the addition of four units, possibly with more than one car ownership per unit.

Policy-related comments:

- the proposed should wait for the new ARP work which will provide an overall vision for the area through a transparent process, rather than this proposal's spot zoning;
- allowing this redesignation ahead of the ARP work is pre-mature; and
- the community should be the one to determine the best areas for developments within Bridgeland, through the new ARP process.

Miscellaneous comments:

- if the R-C2 land use designation is not preserved, this will have negative impacts on property values;
- the re-zoning would increase the subject property's value at the expense of the surrounding R-C2 parcels;
- the associated construction noise, generated waste and impacts on surrounding sidewalk maintenance will negatively impact the surrounding neighbours;
- adding four families to a single parcel over the maximum of two under the current land use, would create more noise and adversely affect the peace and quiet;
- the state of the subject abandoned property as an eye sore, should not be used as a threat to allow this new proposed development to go forward; and
- the Main Street project already pulled back redesignating parcels north of 2 Avenue NE
-

Administration considered the relevant planning issues specific to the proposed redesignation and has determined the final proposal, as presented, to be appropriate.

Engagement

On 2018 March 5, the applicant met with the Bridgeland-Riverside Community Association to present the details of this concurrent land use amendment and development permit application. The meeting was attended by the Community Association's Planning Committee members

along with a few community residents. Members of Administration attended to observe the presentation and provide any process-related clarifications.

Following the meeting, on 2018 March 26, the applicant held a public information session at the Bridgeland-Riverside Community Association. The applicant used a postcard maildrop to notify neighbouring residents of the sessions, while a local community interest group further advertised the session on social media. Approximately 25 residents attended the session.

Members of Administration were present to provide clarification on the proposal and process-related questions. The key themes that emerged from the information session largely echoed those documented in the letters from adjacent land owners. A detailed applicant-provided overview is available in Attachment 5.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The subject site falls within the 'Developed – Residential – Inner City' area as identified on Map 1 of the *Municipal Development Plan* (MDP). The applicable MDP policies encourage redevelopment of inner city communities that is similar in scale and built form to existing development, including supporting revitalization by adding population. The MDP also calls for a modest intensification of the inner city; an area serviced by existing infrastructure, public amenities and transit.

The proposal is in keeping with relevant MDP policies by providing for development form that is sensitive to existing low density residential development in terms of height, built form and density. The development also maximises front door access to both the street and avenue, encouraging pedestrian activity.

Bridgeland-Riverside Area Redevelopment Plan (Statutory, 1980)

The subject parcel is located within the 'Conservation' area on Figure 3- Generalized Land Use of the *Bridgeland-Riverside Area Redevelopment Plan* (ARP). The Conservation area is intended to accommodate primarily single and duplex housing with the appropriate land use designation identified as R-2 which is equivalent to the Residential – Contextual One / Two Dwelling (R-C2) District. While the proposed DC District is based on the R-C2 District, the addition of Permitted Development as a permitted use triggers the requirement for a minor amendment to the *Bridgeland-Riverside ARP* because it is allowing for a multi residential development not otherwise listed under the Conservation area. The text of the proposed amendment to Residential Implementation policy 2 is included in Attachment 2.

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
438 - 8 Street NE, LOC2017-0127**

The proposed ARP amendment is appropriate given that the Permitted Development use contained within the proposed DC District establishes a building form that is contextual with the surrounding residential development in the area.

As discussed in the Background section of this report, a new ARP is in the process of being created by The City in consultation with the Bridgeland-Riverside Community Advisory Group and the greater community. It is anticipated that the ARP will be presented to Council by Winter 2018/ Spring 2019. The new ARP will take into consideration any ARP amendments that are adopted by Council up to that point.

This proposed land use district is believed to be an effective approach that will enable redevelopment of this site without presupposing the planning outcomes or Council decision on the comprehensive land use and policy planning work being carried out as part of the new ARP for the area. This proposal reflects a balanced solution between providing respect for the existing character of the area, while also meeting MDP intensification policies which encourage sensitive redevelopment of inner-city communities. Sensitive redevelopment is manifested in the final development's design that responds to the community, Administration's and UDRP's concerns, while supporting high-quality design. Given the unique and specialised nature of this proposal, Administration therefore does not anticipate that this application will set precedent or pre-determine the future land use pattern within this existing low density residential area and/or the outcome of the new ARP. The prescriptive nature of the proposed district, will result in future development of the site to occur in a manner that is compatible to both the existing area context and/or any future development and building typologies that may be allowable in the area subject to Council's approval of the new ARP.

It is for these above reasons that Administration is not in support of a more conventional up-zoning of this parcel to a standard multi-residential land use district to allow for intensification.

Location Criteria for Multi-Residential Infill (Non-statutory, 2014)

The subject site aligns with four of the eight criteria for ideal placement of multi-residential infill.

The guidelines are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP or local area policy plans, to assist in determining the appropriateness of an application in the local context.

The subject parcel does not meet half of the location criteria because it is located along local residential streets, is not adjacent to an amenity or another multi-residential development, and does not lie within 600 metres of a primary transit route.

However, moderate intensification in this corner parcel with direct lane access has a contextually sensitive impact on adjacent properties, and is therefore considered appropriate. It is also located within 350 metres from the nearest transit stop and in close proximity to 1 Avenue NE, which is classified as a Neighbourhood Main Street.

Social, Environmental, Economic (External)

The recommended land use allows for wider range of housing types than the existing R-C2 District and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics. It would also support The City's Age-Friendly Policy, particularly in accommodating individuals interested in aging-in place with elderly care readily available amongst families living within the same neighbourhood.

An environmental site assessment was not required for this application.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

The intent of this land use approach is to allow for a specific development outcome on this property as contained in Schedule C of the proposed DC Direct Control District. If significant building design changes occur between Council's potential approval of this land use amendment and a future building permit application, it would render the concurrent development permit to be non-compliant with this DC Direct Control District.

Historically, land use amendment applications that were 'tied to plans' provided little to no flexibility with respect to development outcomes. However, with this land use proposal, if the contemplated development is not realised in the future, the proposed DC Direct Control District is designed to also allow for all of the other uses allowed under the existing R-C2 District.

Additionally, while the proposed minor amendment to the *Bridgeland-Riverside Area Redevelopment Plan* (ARP) is reviewed as appropriate and in general alignment to applicable policies as discussed in the Strategic Alignment of this report, there is a small risk of misalignment with the direction of the new ARP work currently undergoing.

This proposed land use district is believed to be an effective approach that will enable redevelopment of this site without pre-determining the future land use pattern within this existing low density residential area and/or the outcome of the new ARP.

**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0841
Page 12 of 12Page 1 of 12**

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
438 - 8 Street NE, LOC2017-0127**

REASONS FOR RECOMMENDATIONS:

Administration recommends approval of this application as it has the potential to enable development in accordance with *Municipal Development Plan* policies that call for modest intensification of the inner city in a redevelopment scale that is similar to the surrounding built environment.

Development following from this application has the potential to allow more Calgarians the freedom to choose to live, work, and meet their day-to-day needs in a location well served by existing infrastructure, services, and employment.

ATTACHMENTS

1. Applicant's Submission
2. Proposed Amendment to the Bridgeland-Riverside Area Redevelopment Plan
3. Proposed DC Direct Control Guidelines
4. Bridgeland-Riverside Community Association Letter
5. Applicant Engagement Overview
6. Urban Design Review Panel Comments
7. Applicant's Response to Urban Design Review Panel Comments
8. Comprehensive Project Overview

Applicant Submission

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Applicant's Submission

O2 Planning + Design is proposing this land use amendment to redesignate the parcel located at 438 8 Street NE in the community of Bridgeland from R-C2 to a Direct Control District tied to plans for a specific project as proposed in a concurrent development permit. This redesignation will allow for the development of a 4-unit family focused townhouse project on the parcel.

The request for a change of land use for this parcel is based on a proposed new building as submitted in a concurrent development permit. The proposed building has been designed as a specific response to the site with careful consideration of the neighbourhood context and character and has been designed to provide the following benefits over a development designed to meet the rules of the existing R-C2 land use district:

- The corner parcel provides an opportunity to orient the building to two streets with two doors facing each street. This represents an improvement over an R-C2 development that would have a side yard facing the avenue, resulting in no street activation.
- The provision of a front patios facing both 8 Street and 4 Avenue provide further activation of the streetscapes by providing outdoor street-level gathering spaces where residents of the units can interact with other neighbourhood residents. The typical relationship between homes on 8 street and the street is the presence of a retaining wall and large front lawn that disconnects the home is from the streetscapes.
- The development has been designed in response to the unique conditions of the site. Specific attention has been made to respect the scale and character of the neighbourhood.
- The design of the development provides a variety of unit sizes which offer choice to potential buyers of various incomes and socio-economic demographics along with inter-generational living opportunities. A typical R-C2 parcel in Bridgeland is redeveloped into one or two luxury homes attainable by a single demographic.
- The parcel's location on the north end of the block results in minimal shadowing impact on adjacent properties.
- The development contributes to meeting the objectives of the Municipal Development Plan goal of intensifying inner-city neighbourhoods. The proposed land use permits a single unit more than the current land use permits.
- Located within walking distance of the shops and services of the 1st Avenue main street, the development will offer the opportunity for its residents to live a more walkable less auto-reliant lifestyle.

Proposed Amendment to the Bridgeland-Riverside Area Redevelopment Plan

1. The Bridgeland-Riverside Area Redevelopment Plan attached to and forming part of Bylaw 11P80, as amended, is hereby further amended as follows:
 - (a) In Part 2, under Section 3 entitled "Residential", subheading "Implementation", Policy 2, at the end of the sentence add "with the exception of land located at 438 – 8 Street NE which is suitable for multi-residential development."

Proposed DC Direct Control Guidelines

Purpose

- 1 This Direct Control District is intended to provide for a multi- residential development as a **permitted use** where it conforms to the plans in Schedule “C”.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Defined Uses

- 4 In this Direct Control District:

(a) “**Permitted Development**” means a **use**:

- (i) that consists of one **main residential building** containing four **units**;
- (ii) that has **motor vehicle parking stalls**; and
- (iii) that conforms with the plans attached to this Direct Control District as Schedule “C”.

Permitted Uses

- 5 (1) The **permitted uses** of the Residential – Contextual One/Two Dwelling (R-C2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

- (2) The following **uses** are the **permitted uses** in this Direct Control District provided the **development** conforms with the plans attached as Schedule “C” of this Direct Control District:

(a) **Permitted Development.**

Discretionary Uses

- 6 The **discretionary uses** of the Residential – Contextual One/Two Dwelling (R-C2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District.

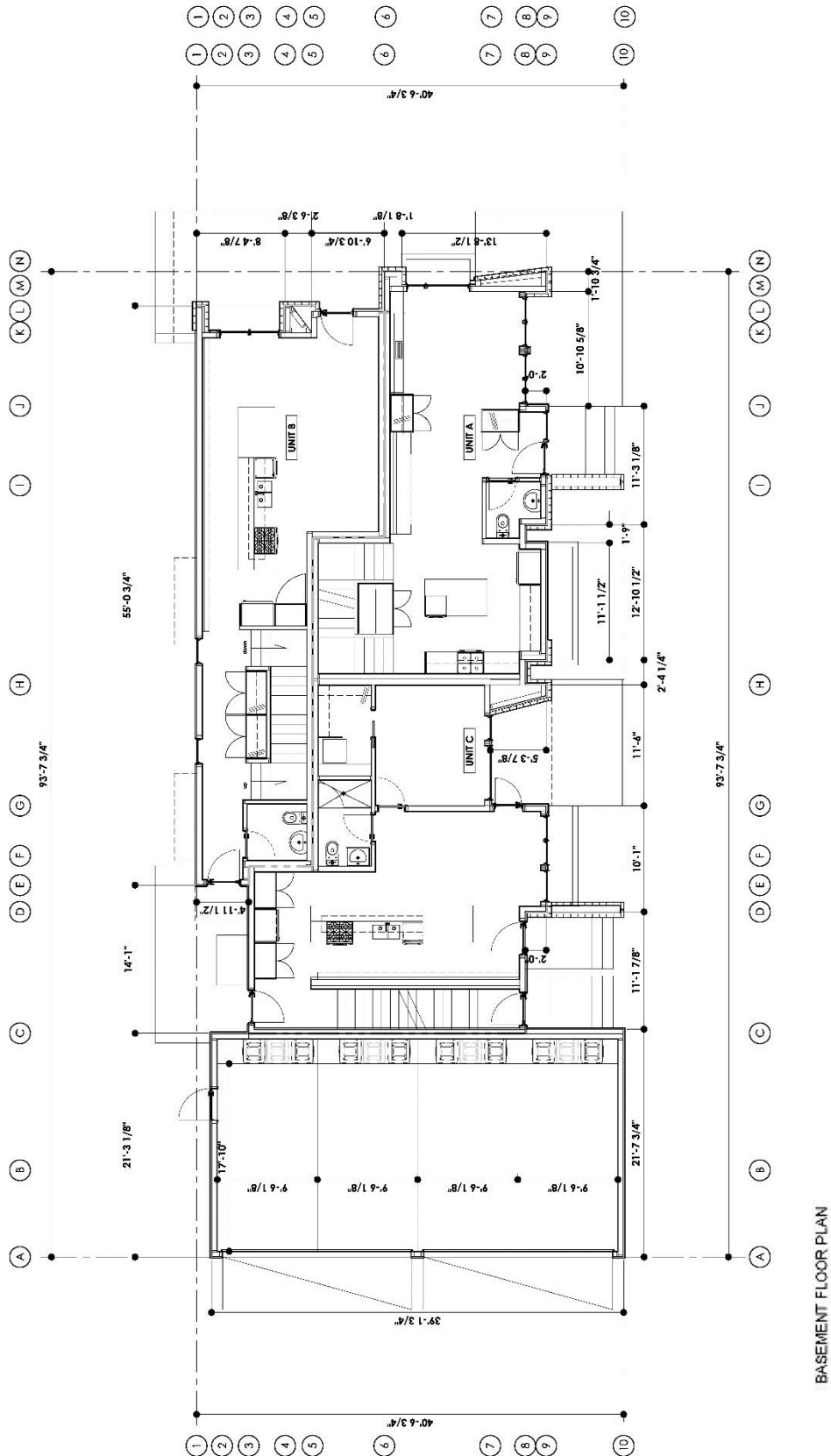
Bylaw 1P2007 District Rules

- 7 Unless otherwise specified, the rules of the Residential – Contextual One/Two Dwelling (R-C2) District of Bylaw 1P2007 apply in this Direct Control District.

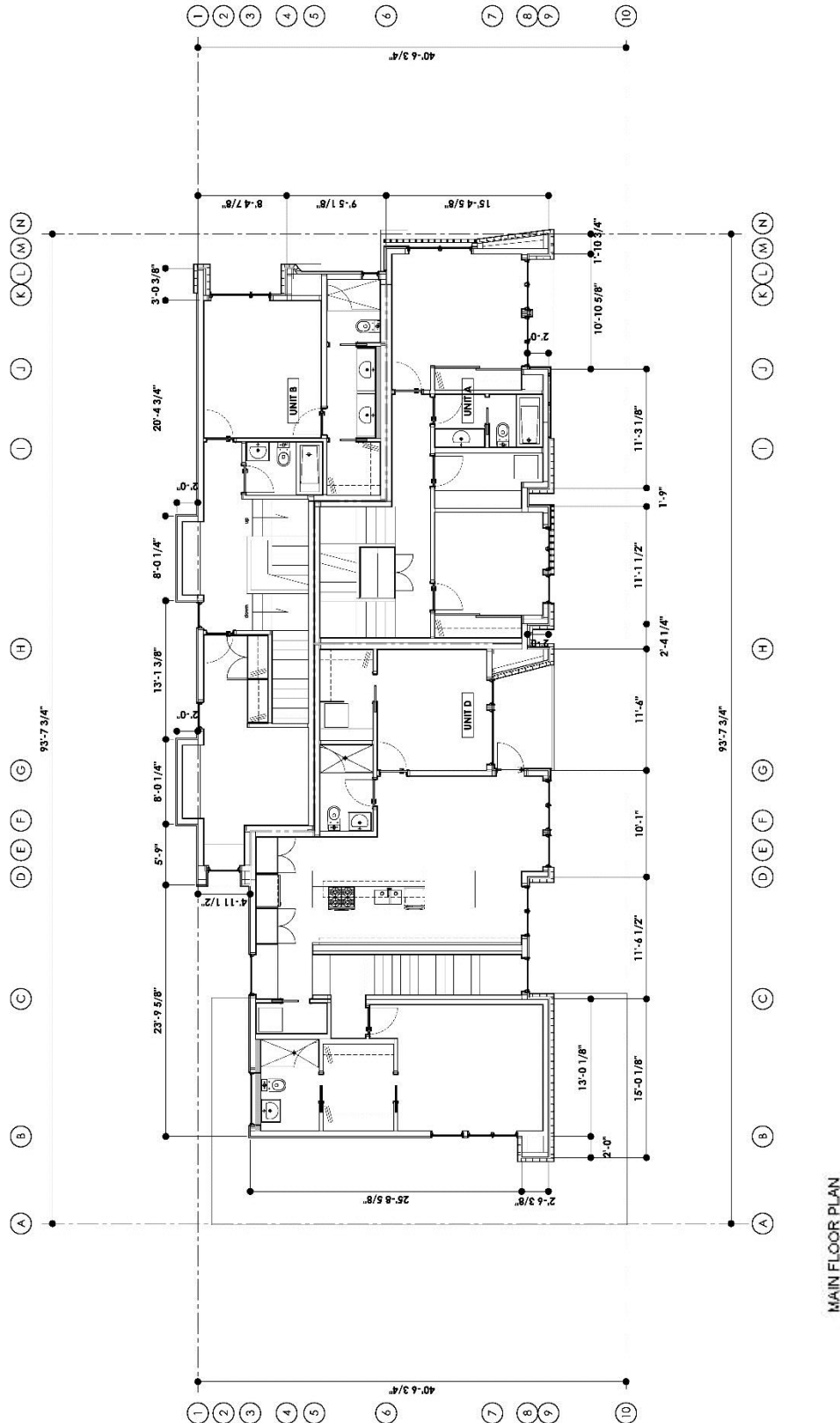
Development Plans for Permitted Development

- 8 Comprehensive plans must be submitted to the **Development Authority** as part of a **development permit** application for **Permitted Development**. In considering such an application, the **Development Authority** must ensure the **development** plans conform with the plans attached to this Direct Control District as Schedule “C”.

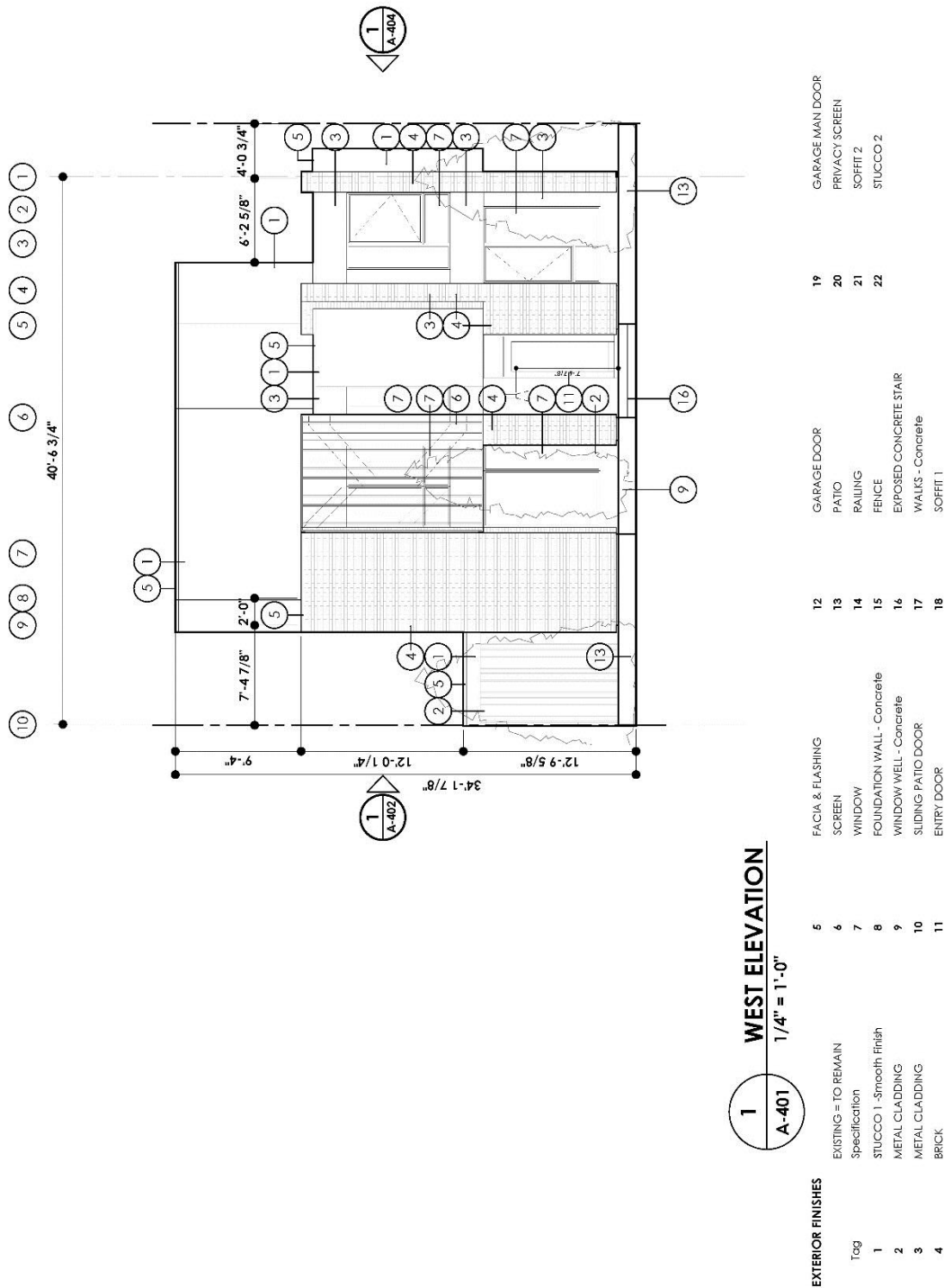
Proposed DC Direct Control Guidelines



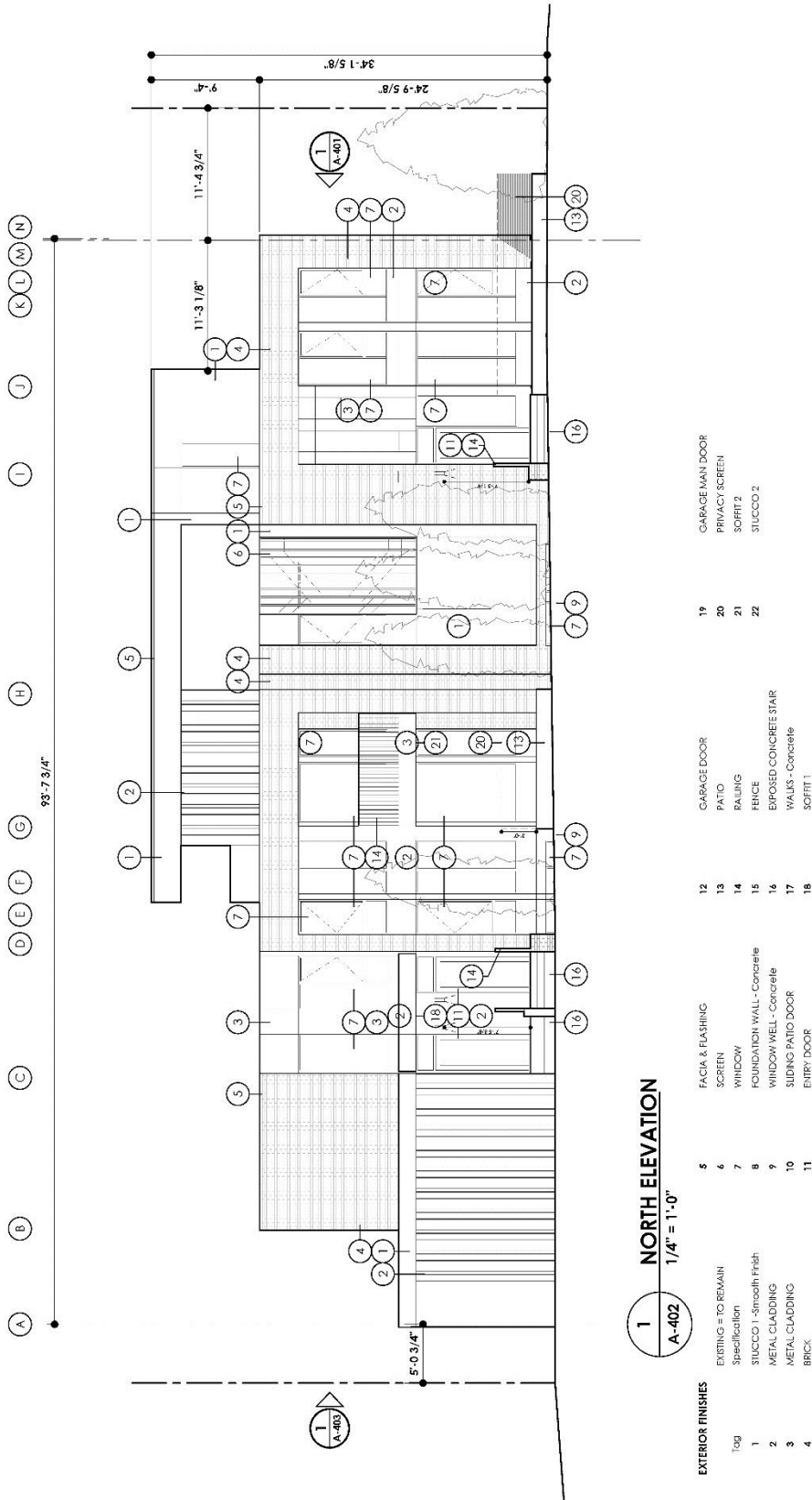
Proposed DC Direct Control Guidelines



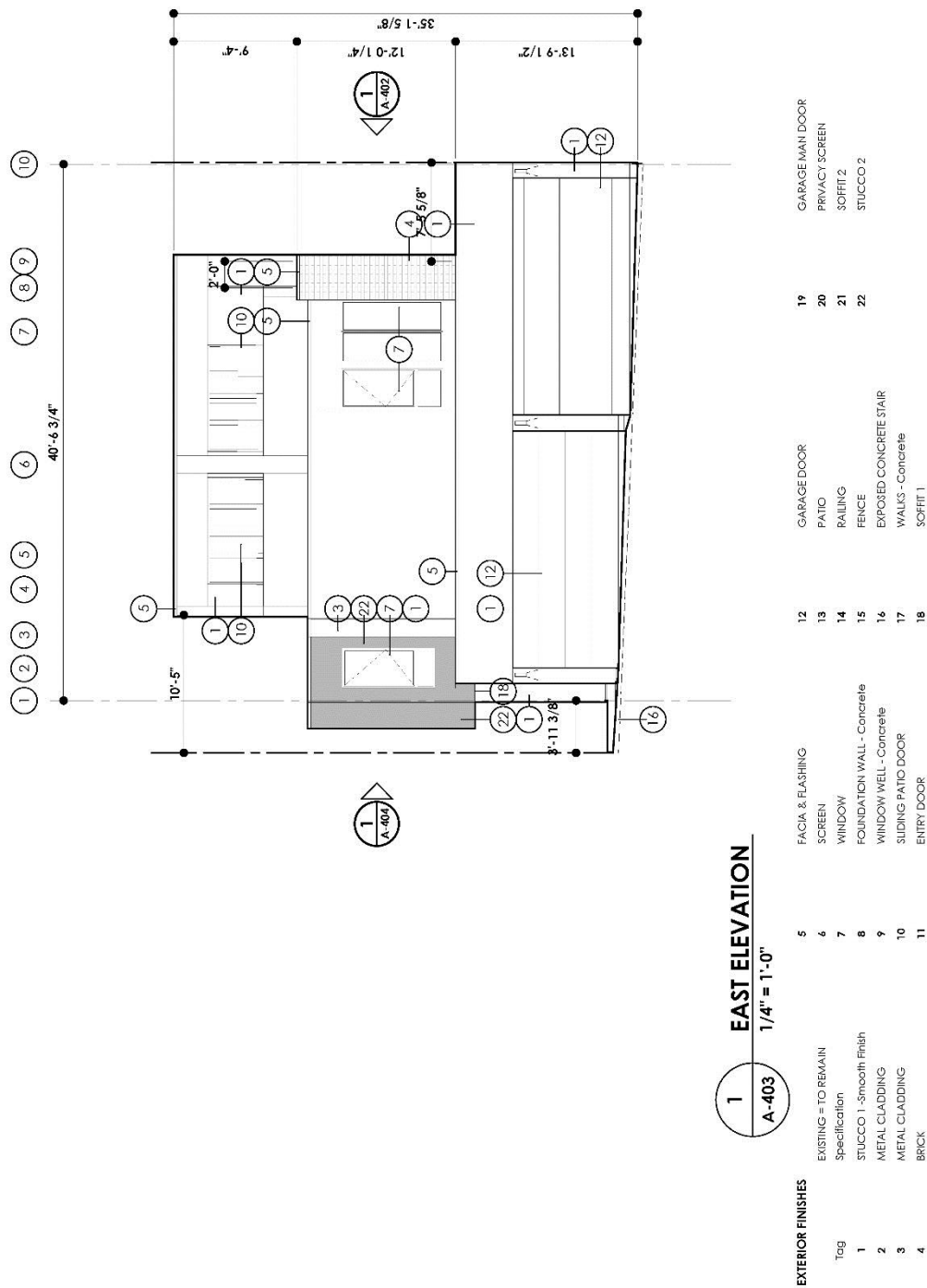
Proposed DC Direct Control Guidelines



Proposed DC Direct Control Guidelines



Proposed DC Direct Control Guidelines



Proposed DC Direct Control Guidelines



Bridgeland-Riverside Community Association Letter



Planning Committee
917 Centre Avenue NE Calgary AB T2E0C6
brcacalgary.org

30 May 2017

Circulation Control
Planning, Development & Assessment #8201
The City of Calgary
PO Box 2100 Station M
Calgary AB T2P2M5

Attn: CPAG.Circ@calgary.ca
cc: Sara Kassa, File Manager (sara.kassa@calgary.ca)
Ali McMillan, BRCA Planning Director (planning@brcacalgary.org)

To Whom It May Concern:

RE: LOC2017-0127 (438 – 8 Street NE)

Thank you for the opportunity to comment with respect to the application for a Land Use Amendment affecting land at 438 – 8 Street NE (LOC2017-0127).

This Land Use Application was discussed at a meeting of our Planning Committee convened 18 May 2017. Notice of that meeting was given to neighbours adjacent to the subject parcel. Despite the relatively short notice that preceded the meeting, approximately 15 neighbours attended, as did many regular Planning Committee members. The applicant was also invited to the meeting, and did attend.

The comments below regarding the LOC application reflect the feedback of both neighbours and the BRCA Planning Committee members present at the 18 May 2017 meeting.

The application is seeking a new land use designation for the site of M-CG instead of the existing R-C2. We are strongly opposed to such redesignation by means of this application for several reasons:

- a. The Bridgeland-Riverside community will soon, after a long period of advocacy, benefit from a rewriting or an updating of our Area Redevelopment Plan (ARP). This endeavour is in its very preliminary stages. Although most stakeholders understand that there will very likely be areas of increased density contemplated by this planning exercise, it is exceeding problematic to preordain the outcome of a broad community-wide engagement process that is today in its most preliminary phase. The zoning proposed, in the particular location proposed, is met with significant opposition both by individuals who are members of the BRCA Planning Committee

Bridgeland-Riverside Community Association Letter

and also the local area residents that attended the meeting. Residents who have communicated on this file with BRCA feel very strongly that a significant upzoning of a parcel without the pending framework of the broader planned approach is detrimental to the community as a whole, and we do not wish to see precedent set in what is our current "conservation zone" without a larger vision in place. We do not speak here of "precedent" in any file-specific sense (we realize that the outcome of one application is not "precedent" *per se* for the next one), but the contextual reality is that this application seeks a significant upzoning very much "in the sensitive middle" of the area that is now slated for fresh long-term planning. A decision of this magnitude in that location could not help but impact the larger planning process, pre-engagement.

- b. Outside of the coming ARP which will address any changes of zoning within the community, the zoning type proposed, in the particular location proposed, is met with significant opposition both by individuals on the BRCA Planning Committee and also by local area residents, due to the lack of context of the M-CG zoning within the surrounding area. The parcel in question is situated in the heart of the RC-2 region of the community, a significant distance from the rather extensive regions of the community already zoned for M-CG and higher density build forms. It is inappropriate to place a non-contextual zoning type in this location, where there are ample redevelopment locations within Bridgeland/Riverside for this zoning type. There are extensively remediated historical homes and new infills adjacent and in close proximity to this site that respect the current zoning configuration.
- c. The City is also currently in the process of implementing City-initiated re-zoning for a significant corridor of properties one block south of this application site between first and second avenues via the Main Streets Project, which allow for up-zoned built forms including M-CG. With the pending re-development area created by Main Streets and the prevalence of M-CG zoning permissible within it, the City has created a more suitable area for this exact zoning after extensive research and engagement of their own. The parcel in question is considerably outside this region. Issues here are not just the long-term future for what is currently called the "conservation zone" but also the correspondence between contemplated built forms in the Main Streets areas and the area containing this application. The BRCA has been largely accepting and participatory regarding these increased density objectives within the areal limits of the Mainstreets Project (which affects approximately 14 full blocks of our neighborhood). But extending similar zoning (and the ensuing built forms) to the very centre of the remaining, yet-to-be-studied-or-considered R-C2 "conservation zone" is not justifiable. It may never be justifiable, but certainly it is not justifiable today without broader consideration / community-wide planning being completed first.
- d. Further, the zoning that has been applied for is not tied to development permit plans. While a concurrent DP for the land is under City review (DP2017-1699), this LOC application is not actually tied to those plans in a formal sense. If the higher zoning was hypothetically approved, there is no assurance that the proposed design will lead to the proposed result. With an approved M-CG zoning decision in hand, the developer/applicant in theory could change architectural direction and develop a project that exploits the full extent of the MC-G capacities: *i.e.* increased density, unit count, building height, and streetscape impact. Or the

Bridgeland-Riverside Community Association Letter

developer/applicant could elect to sell the site with its newly approved zoning to another developer who has made no appealing development claims to the community. Or any one of many imaginable external factors could intervene to disrupt even a well-intentioned project, leaving the site with M-CG zoning unconstrained by current development permit plans.

It is rare in the experience of our Committee that unanimity presents itself on any topic. In this case, however, all persons attending the meeting on 18 May 2017 agreed in clear terms. We are squarely opposed to this LOC application as is the Board of Directors of the BRCA. It is unnecessary in relation to density objectives in our neighbourhood at this time, it completely undermines the dwindling R-C2 "conservation zone" and will stifle creative consideration of that area in the forthcoming ARP process, and it has been brought forward in a manner that is pre-emptive of all such considerations despite BRCA and various neighbours having expressed these concerns directly and clearly to the applicant pre-application.

The granting of this application could invariably lead to other such applications that would effectively extend the density and built forms theorized by the Main Streets Project, but without regard for that Project's geographical limits. If the desire for such an extension had been adequately confirmed by prior engagement of our community, then BRCA would have been the first to urge Mainstreets to extend yet further north. It was not, and we have not.

It seems impossible to comment upon this application without emphasizing its reality on a map. Below is a simple map of our neighbourhood.



The lands highlighted in yellow are the Bow Valley lands, which have long been the subject of their own (post-General Hospital) planning and which are designed for large

Bridgeland-Riverside Community Association Letter

multi-family developments.

The areas highlighted in green comprise the so-called "East Riverside Master Plan" lands. Many institutional owners are stakeholders in this area and huge changes are forthcoming, including Alberta Health Services' announced \$130M redevelopment of its cornerstone site. The East Riverside Master Plan has outlined significant opportunity for multi-family/ mixed use/ townhouse developments within this area plan.

The area highlighted in blue generally defines the Main Streets lands, which are midrise and transitional multi-family development locations. And the black line is roughly where the escarpment begins with the escarpment (steep hill) being a strong topographic feature of our neighbourhood, which has a natural and pronounced impact upon building feasibility, built forms, and traffic patterns and connections.

In the remaining area that has been left un-highlighted—all of which falls within what the current ARP calls the "Conservation Zone"—the site of this application is identified in red. In our view this map makes it obvious that on the eve of deciding how to plan the evolution of the neighbourhood, that this proposal is inappropriate. To explain in metaphor...imagine planning a garden for the next 10 or 20 years, but somebody now proposes to plant a tall evergreen in the very middle as a pre-planning step? Maybe an evergreen is right for that spot, or maybe not, but one thing is for sure: if it is planted, then the option to plan the garden will be significantly impaired. The forthcoming task of the ARP project will already be daunting in terms of stitching together and rationalizing these several disparate planning areas and initiatives, but it is bad planning to allow an application such as this to define the future when our community has both embraced and is currently planning additional areas for significant redevelopment as a collaborative approach to achieving MDP goals.

Our community has been enthusiastic about our upcoming Area Redevelopment Plan undertaking as an opportunity to say, "Yes in my backyard" with a planned approach. We find that this application potentially strikes at the heart of that enthusiasm. If this application were to succeed, then we would inevitably be left feeling that the forthcoming ARP process has been hollowed out before it has even begun, and also that the long-term planning guidance that our neighbourhood hopes to secure with its new ARP will instead perhaps simply devolve into more of the "site by site" (aka "*ad hoc*") planning that has exhausted so much energy for all stakeholders, including the City, these past recent years an resulted in a continually adversarial planning process.

The following points made in the existing the Bridgeland-Riverside Area Redevelopment Plan—our current statutory document, arrived at via a process of broadly planning the neighbourhood over many years and with much public input states—are apposite:

- "The Calgary Municipal Plan states that residential densities in the inner city should be increased. The primary reasons are that such changes would better utilize the existing infrastructure and that the strategic locations of such neighbourhoods should be recognized. **However the "appropriate locations" portion of the goal is equally important as other locations could effectively destroy the desirable aspects of inner city living.**" (Page 8, emphasis added)

Bridgeland-Riverside Community Association Letter

- **"To conserve and stabilize the family oriented areas of the community and to ensure their long-term viability."** A primary focus of the Neighbourhood Improvement Program is to ensure that older residential neighbourhoods are improved and stabilized. The Inner City Plan, while calling for increased densities in such areas, also recognizes the need for stabilizing portions of neighbourhoods by **conservation** so that opportunities for family living remain." (Page 9, point 8, emphasis added)
- "That preservation and enhancement of appropriate portions of the community for families with children be strived for, that areas **suitable for higher density development be identified and that such developments minimize impacts on the remainder of the community.**" (Policies, page 10, **emphasis added**). (Parenthetically: obviously a point here is that much of our community has already been identified to accommodate increased densities—especially the majority of our community areas south of First Ave NE and the entire 14 blocks of additional Main Streets rezoning upcoming. The question arising—not for decision today but properly for broad engagement via an ARP process, is what the "remainder of the community" might mean for Bridgeland-Riverside, since these are the areas where the "impacts" are to be "minimized".)

As a final substantive topic, we also understand that the City has typically been using "Location Criteria for Multi-Residential Infill" in its evaluation of applications such as this one. In italics below are our comments on each of the pertinent criteria as relates to the present application:

Location Criteria for Multi-Residential Infill

1. On a corner parcel – references here the MDP Section 2.3.2 b.

a. Respect the existing character of low-density residential areas, while still allowing for innovative and creative designs that foster distinctiveness.

The key point here is "respecting the existing character" of the neighbourhood, especially due to the high-value heritage properties that back onto the site in question.

b. Ensure an appropriate transition of development intensity, uses and built form between low-density residential areas and more intensive multi-residential or commercial areas

This site in question is NOT a transition area but instead lies directly in the heart of the small portion of the remaining R-C2 area ("conservation zone"), one block from the end of the Main Streets zone.

c. Ensure infill development complements the established character of the area and does not create dramatic contrasts in the physical development pattern.

Again due to the high value of the heritage homes to the immediate east and also the nature of the surrounding community context in general, we feel this criterion is not met.

2./3. Within 400 m of a transit station/ 600m of a primary transit stop.

Bridgeland-Riverside Community Association Letter

All of Bridgeland is walkable to shops and transit within relatively short distances. However does that mean all of our community should be multi-residential? We have encouraged density closer to transit, e.g. East Riverside Master Plan as a TOD site situated adjacent to the C-Train station, or Mainstreets upzoning proximate to the 1st Ave bus route. This is why we feel these other areas of our neighbourhood need to be planned in a broad sense while looking at the community as a whole.

4. On a collector or high standard roadway at least on one frontage.

4th Ave NE is not a collector road or high standard roadway but a residential road only. Access onto 4th Avenue NE from Edmonton Trail is constrained by traffic control (no left turns) during peak periods.

5. Adjacent to existing or planned non-residential development or multi-unit development.

Again, this site is in the heart of the "conservation zone" surrounded by R-C2 homes and is not a transition area. There is no non-residential development or multi-unit development in any of the surrounding blocks.

6. Adjacent to or across from an existing or planned open space, park or community amenity.

This criterion is not applicable to this site – no park, open space or community amenity is nearby.

7. Along or in close proximity to an existing or planned corridor or activity center.

This criterion is also not applicable to this site – there is no corridor or activity center anywhere nearby.

8. Direct lane access –

The majority of our community has direct lane access due to the historic nature of the community and the established grid pattern of the streets. For this reason most lanes are very narrow, gravel lanes with wooden power poles that are not well suited for multi-residential access or infrastructure requirements (i.e. garbage handling and removal).

When corner parcels of land such as the site in this case have been publicly considered on their planning merits as candidates for rezoning, both in light of the development trends in Calgary and in our greater community (through rewriting of our ARP), we may one day reach a different conclusion. Or not. But regardless at this time we cannot support this application.

Sincerely,

BRIDGELAND-RIVERSIDE COMMUNITY ASSOCIATION

Per: BRCA Board of Directors
Planning Committee

Applicant Engagement Overview

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E info@o2design.com



438 8 St & 230 7A Street Engagement Summary

Below is a summary of the community engagement activities that O2 Planning + Design and Davignon Martin Architecture completed during the application process.

March 5, 2018 Meeting with BRCA Planning Committee

O2 Planning + Design, Davignon Martin Architecture and the landowner/developer met with the BRCA Planning Committee to present the details of the land use application and the proposed design of the two projects.

The BRCA committee expressed general support of the buildings design but were concerned with the application moving ahead of the ARP revision and that the density, height and massing does not fit with the neighbourhood context. It was agreed with the BRCA committee that a public open house should be held to allow Bridgeland residents have the opportunity be informed about the project.

March 26, 2018 Public Information Session

Residents from the community of Bridgeland-Riverside were invited to a community information session hosted at the Bridgeland-Riverside Community Association Hall. The purpose of this community information session was to provide information and to solicit community feedback on the proposed developments.

The community information session was attended by approximately 25 residents. The format of this information session was an informal discussion with the project's planners, architects, and land owners. Residents had the opportunity to provide feedback using comment forms and sticky notes provided by the applicant. There were 50 comments received and have been categorized into the following themes:

- **Traffic, Parking & City Servicing. 7 Comments of Concern**
Residents expressed concern that the development would lead to increase in traffic, more on-street parking and over burden City services.

Response: The proposed development will include one additional unit than what is permitted under the existing land use will create minimal additional impact to traffic, parking and servicing.
- **Bridgeland-Riverside ARP Alignment: 12 Comments of Concern**
Residents expressed concern that proposed land use amendment application was being made in advance of the ARP update.

Response: Given the delays in progressing the ARP update, the land owner wanted to move advance with the land use and development permit application to meet their development timelines. Waiting for the update to the ARP would cause significant delays in the schedule.

Applicant Engagement Overview

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- **Massing and Height: 9 comments of concern, 1 comment of support**

Residents expressed concern that the development was out of scale in relation to the neighbourhood. The comment of support expressed that the proposed massing and height fits with the context related to the scale of new-build homes in Bridgeland.

Response: Although the scale of the proposed homes is larger than the older homes in the area, the massing and height of the project is very similar to the allowed height and massing of new build single-family and semi-detached homes being built in Bridgeland.

- **Building Design: 2 Comments of Concern, 3 Comments of support**

While some residents were not supportive of the architectural design, three specific comments were received praising the design of the homes.

Response: Architectural design is subjective, but great attention has been made to ensure that the design is responsive to the community context and enhances the public realm of the neighbourhood.

- **Adjacent Privacy, Lighting & Green Space: 3 Comments of Concern**

Residents expressed concern of the loss of privacy, lighting and green space.

Response: The impact of privacy, lighting and green space would be similar with the development of homes allowed under the existing zoning.

Urban Design Review Panel Comments

Urban Design Review Panel Comments

Date:	June 13, 2018	
Time:	1:00 pm	
Panel Members:	Present:	Absent:
	Chad Russill (co-chair)	Janice Liebe (chair)
	Gary Mundy	Glen Pardoe
	Jack Vanstone	Robert LeBlond
	Amelia Hollingshurst	Terry Klassen
	Ben Bailey	Yogeshwar Navagrah
		Bruce Nelligan
		Eric Toker
Advisor:	David Down, Chief Urban Designer	
Application number:	LOC2018-0021/DP2018-0390, LOC2017-0127/DP2017-1699	
Municipal address:	230 7A ST NE, 438 8 ST NE	
Community:	Bridgeland	
Project description:	Multi-Residential Development (1 building, 4 units.) Multi-Residential Development (1 building, 4 units.)	
Review:	first	
File Manager:	Sara Kassa	
City Wide Urban Design:	Angela Kiu	
Applicant:	02 Planning and Design	
Architect:	Davignon Martineau Architects	
Owner:		
Ranking:	Support	

Summary

The Panel recognizes that this development does not require UDRP review and feedback, however commends the applicant for voluntarily seeking input on this innovative proposal for a new residential typology within the neighbourhood of Bridgeland.

Overall, the Panel is excited by the applicant's desire to address a corner residential lot differently than an infill lot and activate both the avenue and street facades through the residential unit configurations and the exterior design. The Panel also commends the intent of the design to provide more variety within this community with regards to scale and cost, ultimately supporting a diverse community fabric and demographic.

The Panel encourages further study of the contrasting expression of the third level and garage exterior treatment. Although it is understood that the current proposal attempts to accentuate the two-storey massing of the building, the graphic illustrations seem to highlight both the third level and garage, as oppose to diminishing their presence.

Although this application does not align exactly with the current ASP for this community, the Panel strongly **supports** this application and the change in zoning to Direct Control for both sites.

Applicant Response

(date)

Urban Design Review Panel Comments

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	N/A
	UDRP Commentary		
	Not Applicable.		
	Applicant Response		
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	N/A
	UDRP Commentary		
	Not Applicable.		
	Applicant Response		
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	Support
	UDRP Commentary		
	The existing public sidewalk is maintained along the street and avenue. Access to residential units are connected to the public sidewalk through hard scaping. A rear parking garage is provided for residents with concrete pads connecting the garage to the rear lane. Private patios and landscaping define the edge condition between the public and private realm.		
	Applicant response		
4	Entry definition / legibility	Entry points are clear and legible	Support
	UDRP Commentary		
	Entry to each of the private residential units is clear and legible. The proposed development activates both street and avenue through meaningful points of entry.		
	Applicant Response		
5	Residential multi-level units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Support
	UDRP Commentary		
	The design proposes two and three storey units at the street level, facing both the street and avenue at this corner site condition. Private outdoor patios, at grade, provide opportunity for furniture and extend towards the sidewalk. They offer physical and visual access to the public sidewalk. Second and third level balconies also contribute to animating the façades.		
	Applicant Response		
6	At grade parking	At grade parking is concealed behind building frontages along public streets.	Support with comment
	UDRP Commentary		
	A private, enclosed garage, is provided for four vehicles (one per residential unit). Street parallel parking is by permit only and a C-train station is located within 10 minutes walking distance. The panel feels this is more than adequate. Designated 'Car to Go' parking locations near-by may assist the applicant in negotiating Community support, as it could minimize Community concerns around vehicle volumes. Some exploration of pushing the garage away from the property line is encouraged as it could allow for some landscape to buffer this portion of the building.		

Urban Design Review Panel Comments

Applicant Response			
7	Parking entrances	Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.	Support
UDRP Commentary			
There are no ramps in this application. The parking entrance is in keeping with the existing context and all other adjacent residential properties; via the rear lane. The panel supports reducing the garage parking capacity from four stalls to three stalls. This will improve the exterior form/massing along the avenue façade by reducing the current bump-out of the garage. Further reducing the private parking capacity will support the use of public transit and increase pedestrian activity.			
Applicant Response			
8	Other		
Applicant Response			
Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i>			
Topic		Best Practice	Ranking
9	LRT station connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	N/A
UDRP Commentary			
Beyond the property lines of this site, a LRT Station is located within a 10 minute walk.			
Applicant Response			
10	Regional pathway connections	Supports walkability via intentional urban design connections to pathway systems.	N/A
UDRP Commentary			
Applicant Response			
11	Cycle path connections	Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.	N/A
UDRP Commentary			
Applicant Response			
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	N/A
UDRP Commentary			
Applicant Response			
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.	Support
UDRP Commentary			

Urban Design Review Panel Comments

	The design provides an exterior walkway along the side setback through the site, connecting back entryways to the garage, laneway and front street. Suggest the addition of gates to discourage public access and enhance CPTED principles.		
	Applicant Response		
14	Open space networks and park systems	Connects and extends existing systems and patterns.	N/A
	UDRP Commentary		
	Applicant Response		
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.	
	UDRP Commentary		
	Applicant Response		
16	Vehicular interface		
	UDRP Commentary		
	Applicant Response		
17	Other		
	Applicant Response		
	Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i>		
	Topic	Best Practice	Ranking
18	Massing relationship to context	Relationship to adjacent properties is sympathetic	Support
	UDRP Commentary		
	Although the proposed massing deviates from the existing residential context by proposing a third story, the design is sensitive to the adjacent two storey structures by stepping back the proposed third storey and articulating the building massing on the shared side-yard elevation. The street and avenue facades project slightly into the required setback, however the panel feels that the projection on the street façade is minimal and as a corner lot, this projection does not diminish the overall street perspective or negatively impact the adjacent property. Visibility triangles for vehicles is not affected. The panel does feel that retracting the garage façade along the avenue would have a positive impact on the avenue perspective and elevation, allowing sufficient space for a landscaping feature to soften the blank wall façade. Overall, the massing addresses both the street and avenue, which is a positive urban design gesture.		
	Applicant Response		
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites	Support
	UDRP Commentary		
	Although the proposed massing deviates from the existing residential context, that being a main residence with a detached garage and a backyard space, the proposed massing is located on north corner sites, and therefore does not introduce shadows onto adjacent backyards.		
	Applicant Response		

Urban Design Review Panel Comments

20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
UDRP Commentary			
See comment #18. This application is innovative in that it is addressing both streets through form and layout. As a corner lot it fronts two streets and the design addresses both streets differently, relating to the attitude of 8 th Street through a duplex massing, and initiating an attitude for the avenue through a townhouse massing. This solution is in fact less than what could be built within the current zoning, further strengthening the rationale behind support of this project.			
Applicant Response			
21	Massing distribution on site		Support
UDRP Commentary			
The panel commends the applicant's intent to provide affordable density and improve the quality of space within the residential units. The massing occupies the entire site, while achieving natural light within each unit and maintaining visual privacy for existing adjacent properties.			
Applicant Response			
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Support
UDRP Commentary			
See comment #5			
Applicant Response			
23	Other		
Applicant Response			
Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i>			
	Topic	Best Practice	Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Support
UDRP Commentary			
The design provides lighting and glazing along both the street and avenue, which is an improvement within the neighbourhood. This improvement lends much needed 'eyes on the street' for the avenue, where laneway access can provide hiding areas. All unit entry doors are visible and provide full glazing. Also see comment # 13.			
Applicant Response			
25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	N/A
UDRP Commentary			
Applicant Response			
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.	N/A

Urban Design Review Panel Comments

	UDRP Commentary		
	Although this was not discussed, the applicant may wish to identify the snow shoveling procedures to the Community Association to address any sidewalk maintenance concerns.		
	Applicant Response		
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.	Support
	UDRP Commentary		
	All residential unit entries have canopies to protect against weather conditions and improve the safety of landings.		
	Applicant Response		
28	Night time design		N/A
	UDRP Commentary		
	Applicant Response		
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.	Support
	UDRP Commentary		
	One of the proposed units is single storey and accommodations could be made to ramp the main entry walkway. This could support the design intent of supporting a diverse community both generationally and economically.		
	Applicant Response		
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.	N/A
	UDRP Commentary		
	Applicant Response		
31	Other		
	Applicant Response		
Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i>			
Topic		Commentary	Ranking
32	(specify)		TBD

Applicant Response to Urban Design Review Panel Comments

Urban Design Review Panel Comments

Date:	June 13, 2018	
Time:	1:00 pm	
Panel Members:	Present: Chad Russill (co-chair) Gary Mundy Jack Vanstone Amelia Hollingshurst Ben Bailey	Absent: Janice Liebe (chair) Glen Pardoe Robert LeBlond Terry Klassen Yogeshwar Navagrah Bruce Nelligan Eric Toker
Advisor:	David Down, Chief Urban Designer	
Application number:	LOC2018-0021/DP2018-0390, LOC2017-0127/DP2017-1699	
Municipal address:	230 7A ST NE, 438 8 ST NE	
Community:	Bridgeland	
Project description:	Multi-Residential Development (1 building, 4 units.) Multi-Residential Development (1 building, 4 units.)	
Review:	first	
File Manager:	Sara Kassa	
City Wide Urban Design:	Angela Kiu	
Applicant:	02 Planning and Design	
Architect:	Davignon Martineau Architects	
Owner:		
Ranking:	Support	

Summary

The Panel recognizes that this development does not require UDRP review and feedback, however commends the applicant for voluntarily seeking input on this innovative proposal for a new residential typology within the neighbourhood of Bridgeland.

Overall, the Panel is excited by the applicant's desire to address a corner residential lot differently than an infill lot and activate both the avenue and street facades through the residential unit configurations and the exterior design. The Panel also commends the intent of the design to provide more variety within this community with regards to scale and cost, ultimately supporting a diverse community fabric and demographic.

The Panel encourages further study of the contrasting expression of the third level and garage exterior treatment. Although it is understood that the current proposal attempts to accentuate the two-storey massing of the building, the graphic illustrations seem to highlight both the third level and garage, as oppose to diminishing their presence.

Although this application does not align exactly with the current ASP for this community, the Panel strongly **supports** this application and the change in zoning to Direct Control for both sites.

Applicant Response

(18.06.21)

We appreciate the panels comments interest in the project, their time commitment and feedback. We have carefully combed through the comments and are providing responses for each sections of the document. We can confirm that we are taking the commentary to heart and have revised the

Applicant Response to Urban Design Review Panel Comments

third level and garage exterior treatment. These elevations have been adjusted to add character that is more compatible with the current proposed two level lower massing.

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	N/A
	UDRP Commentary		
	Not Applicable.		
	Applicant Response		
	N/A		
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	N/A
	UDRP Commentary		
	Not Applicable.		
	Applicant Response		
	N/A		
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	Support
	UDRP Commentary		
	The existing public sidewalk is maintained along the street and avenue. Access to residential units are connected to the public sidewalk through hard scaping. A rear parking garage is provided for residents with concrete pads connecting the garage to the rear lane. Private patios and landscaping define the edge condition between the public and private realm.		
	Applicant response		
	In agreement.		
4	Entry definition / legibility	Entry points are clear and legible	Support
	UDRP Commentary		
	Entry to each of the private residential units is clear and legible. The proposed development activates both street and avenue through meaningful points of entry.		
	Applicant Response		
	In complete agreement.		
5	Residential multi-level units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Support
	UDRP Commentary		
	The design proposes two and three storey units at the street level, facing both the street and avenue at this corner site condition. Private outdoor patios, at grade, provide opportunity for furniture and extend towards the sidewalk. They offer physical and visual access to the public sidewalk. Second and third level balconies also contribute to animating the façades.		
	Applicant Response		
	In agreement.		
6	At grade parking	At grade parking is concealed behind building frontages along public streets.	Support with comment
	UDRP Commentary		

Applicant Response to Urban Design Review Panel Comments

<p>A private, enclosed garage, is provided for four vehicles (one per residential unit). Street parallel parking is by permit only and a C-train station is located within 10 minutes walking distance. The panel feels this is more than adequate. Designated 'Car to Go' parking locations near-by may assist the applicant in negotiating Community support, as it could minimize Community concerns around vehicle volumes. Some exploration of pushing the garage away from the property line is encouraged as it could allow for some landscape to buffer this portion of the building.</p>		
Applicant Response		
<p>In agreement. A designated Car to Go parking location near-by would be a worthy amenity that would further support our project and the community.</p>		
<p>Pushing the garage away from the North façade has been explored. The garage could shift 17" to the South but we feel this will reduce the practicality of the rear walk and doesn't provide a significant enough increase in landscaping where the landscaping would survive.</p>		
7	<p>Parking entrances</p>	<p>Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.</p>
Support		
UDRP Commentary		
<p>There are no ramps in this application. The parking entrance is in keeping with the existing context and all other adjacent residential properties; via the rear lane. The panel supports reducing the garage parking capacity from four stalls to three stalls. This will improve the exterior form/massing along the avenue façade by reducing the current bump-out of the garage. Further reducing the private parking capacity will support the use of public transit and increase pedestrian activity.</p>		
Applicant Response		
<p>Although we agree with the UDRP comments, we are electing to not pursue this item in consideration of the parking concerns brought forth by the Community Association and resident. We feel that in due time, the planning department and CA's will begin considering more parking reductions to proposed developments.</p>		
8	<p>Other</p>	
Applicant Response		
N/A		
<p>Urban Connectivity Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</p>		
	Topic	Best Practice
	Ranking	
9	<p>LRT station connections</p>	<p>Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.</p>
N/A		
UDRP Commentary		
<p>Beyond the property lines of this site, a LRT Station is located within a 10 minute walk.</p>		
Applicant Response		
In agreement.		
10	<p>Regional pathway connections</p>	<p>Supports walkability via intentional urban design connections to pathway systems.</p>
N/A		
UDRP Commentary		
Applicant Response		
N/A		
11	<p>Cycle path connections</p>	<p>Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.</p>
N/A		
UDRP Commentary		
Applicant Response		

Applicant Response to Urban Design Review Panel Comments

	N/A		
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	N/A
UDRP Commentary			
Applicant Response			
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.	Support
UDRP Commentary			
The design provides an exterior walkway along the side setback through the site, connecting back entryways to the garage, laneway and front street. Suggest the addition of gates to discourage public access and enhance CPTED principles.			
Applicant Response			
In agreement. A gate has been added to the rear of the property to discourage public access.			
14	Open space networks and park systems	Connects and extends existing systems and patterns.	N/A
UDRP Commentary			
Applicant Response			
N/A			
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.	
UDRP Commentary			
Applicant Response			
N/A			
16	Vehicular interface		
UDRP Commentary			
Applicant Response			
N/A			
17	Other		
Applicant Response			
N/A			
Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i>			
Topic		Best Practice	Ranking
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UDRP Commentary			
Although the proposed massing deviates from the existing residential context by proposing a third story, the design is sensitive to the adjacent two storey structures by stepping back the proposed third storey and articulating the building massing on the shared side-yard elevation. The street and avenue facades project slightly into the required setback, however the panel feels that the projection on the street façade is minimal and as a corner lot, this projection does not diminish the overall street perspective or negatively impact the adjacent property. Visibility triangles for vehicles			

Applicant Response to Urban Design Review Panel Comments

	is not affected. The panel does feel that retracting the garage façade along the avenue would have a positive impact on the avenue perspective and elevation, allowing sufficient space for a landscaping feature to soften the blank wall façade. Overall, the massing addresses both the street and avenue, which is a positive urban design gesture.		
	Applicant Response		
	We are in agreement with the UDRP comments. As previously mentioned we have elected to keep the 4 garage stalls to help alleviate parking concerns from the Community Association.		
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites	Support
	UDRP Commentary		
	Although the proposed massing deviates from the existing residential context, that being a main residence with a detached garage and a backyard space, the proposed massing is located on north corner sites, and therefore does not introduce shadows onto adjacent backyards.		
	Applicant Response		
	In agreement.		
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
	UDRP Commentary		
	See comment #18. This application is innovative in that it is addressing both streets through form and layout. As a corner lot it fronts two streets and the design addresses both streets differently, relating to the attitude of 8 th Street through a duplex massing, and initiating an attitude for the avenue through a townhouse massing. This solution is in fact less than what could be built within the current zoning, further strengthening the rationale behind support of this project.		
	Applicant Response		
	In agreement.		
21	Massing distribution on site		Support
	UDRP Commentary		
	The panel commends the applicant's intent to provide affordable density and improve the quality of space within the residential units. The massing occupies the entire site, while achieving natural light within each unit and maintaining visual privacy for existing adjacent properties.		
	Applicant Response		
	In agreement.		
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Support
	UDRP Commentary		
	See comment #5		
	Applicant Response		
	In agreement.		
23	Other		
	Applicant Response		
	N/A		
Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i>			
	Topic	Best Practice	Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Support
	UDRP Commentary		

Applicant Response to Urban Design Review Panel Comments

	The design provides lighting and glazing along both the street and avenue, which is an improvement within the neighbourhood. This improvement lends much needed 'eyes on the street' for the avenue, where laneway access can provide hiding areas. All unit entry doors are visible and provide full glazing. Also see comment # 13.	
	Applicant Response	
	In agreement.	
25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.
	UDRP Commentary	
	Applicant Response	
	N/A	
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.
	UDRP Commentary	
	Although this was not discussed, the applicant may wish to identify the snow shoveling procedures to the Community Association to address any sidewalk maintenance concerns.	
	Applicant Response	
	A condo plan and use rules will be developed for the project, to which buyers, multigenerational dwellers or tenants would have to follow.	
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.
	UDRP Commentary	
	All residential unit entries have canopies to protect against weather conditions and improve the safety of landings.	
	Applicant Response	
	In agreement.	
28	Night time design	N/A
	UDRP Commentary	
	Applicant Response	
	N/A	
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.
	UDRP Commentary	
	One of the proposed units is single storey and accommodations could be made to ramp the main entry walkway. This could support the design intent of supporting a diverse community both generationally and economically.	
	Applicant Response	
	In agreement.	
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.
	UDRP Commentary	
	Applicant Response	
	N/A	
31	Other	
	Applicant Response	
	N/A	

Applicant Response to Urban Design Review Panel Comments

Service / Utility Design *Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.*

Topic		Commentary	Ranking
32	(specify)		TBD

Comprehensive Project Overview

TABLE OF CONTENTS

- 1 PROJECT BACKGROUND AN TIME LINE
- 2 SITE CONTEXT
- 3 LAND USE APPLICATION
- 4 ENGAGEMENT SUMMARY
- 5 THE SEARCH FOR THE MISSING MIDDLE
- 6 TYPICAL BUILDER TYPOLOGIES
- 7 BASEMENT AND GARAGE SUITE SOLUTIONS TO DENSIFICATION
- 8 A NEW SOLUTION TO DENSITY
- 9 BUILDING MASSING
- 10 LANDSCAPE PLAN
- 11 DESIGN PRINCIPLES
- 12 URBAN DESIGN - ACTIVATING THE STREET AND THE AVENUE
- 13 MATERIALS
- 14 RENDERS
- 15 FLOOR PLANS
- 16 STREETScape

Comprehensive Project Overview

Project Background + Timeline

Background

The property owners of 438 8 Street & 230 7A Street NE have submitted applications to rezone their parcels to allow for the development of family-oriented townhouses (4 units on each parcels).

438 8 Street is currently designated Residential - Contextual One/Two Dwelling District (R-C2) and 230 7A Street is designated with the recently approved main streets Direct Control District (DC). The proposed land use district of both parcels is a Direct Control District tied to Plans. A Direct Control district tied to plans means that the Development Permits (DP) for the projects will need to be aligned with

the plans accompanying the DC. If the DP does not align with the DC drawings, the land use reverts to the existing land use.

Along with the land use redesignation application, a concurrent development permit has been submitted for each site.

Development Principles

- Moderate intensification and density;
- Site specific design;
- Family friendly housing choice;
- Variety of unit sizes;
- Walkability;
- Street vitality;
- Community interaction.

Land Use Timeline



Comprehensive Project Overview

Site Context



The subject site located at 438 8 Street NE is approximately 455 square metres in size, and is surrounded by R-C2 (Residential - Contextual One/Two Dwellings).

The subject site located at 230 7A Street NE is approximately 455 square metres in size. East of the parcel is St. Michael's Eritrean Orthodox Church, and to the north, west, and south of the site are R-C2 (Residential - Contextual One/Two Dwellings).

Both subject sites are in close proximity to local shops, retail, professional services, community schools, parks and open spaces. These sites are well serviced by Calgary Transit, with route 90 running along the 1st Avenue corridor. Additionally, these sites are within 10-15 minutes walking distance to the Bridgeland-Memorial LRT Station and Bow River pathway system.

Comprehensive Project Overview

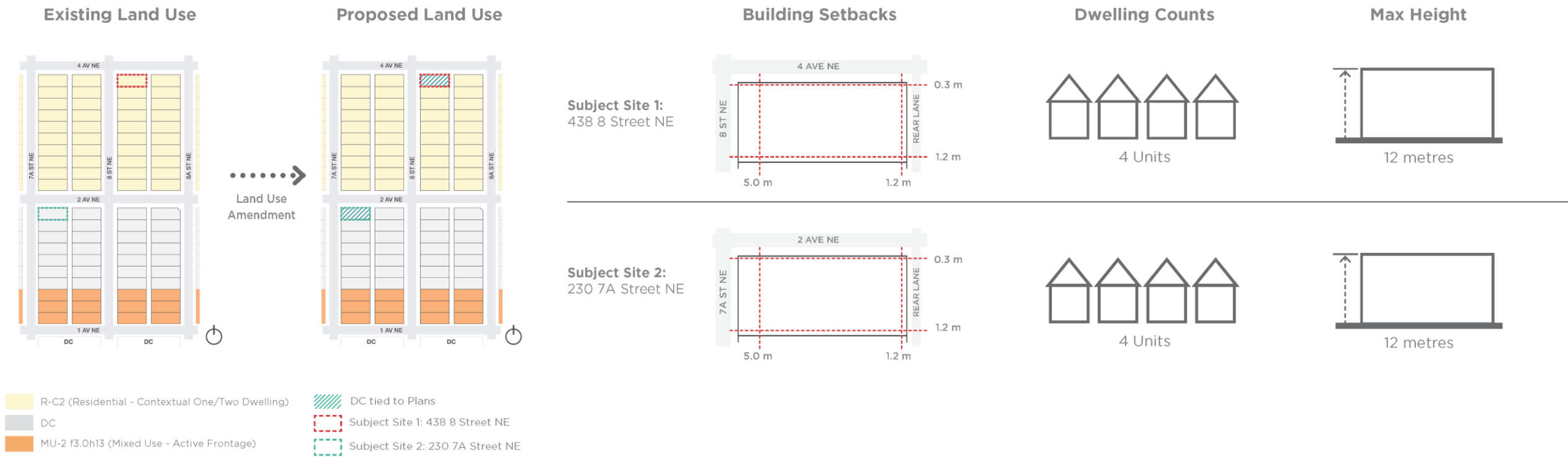
Land Use Application

The land use amendment application proposes to change the existing land use of 438 8 Street from R-C2 (Residential - Contextual One/Two Dwelling District) and the existing land use of 230 7A Street from DC based on R-CG (Residential - Grade-Oriented Infill District) to both a Direct Control District (DC) tied to plans.

The diagrams on this panel explains the concepts regarding the land use amendment.

Direct Control (DC) Tied to Plans:

- DC will be based on a site specific design proposal;
- Development Permit must conform with the plans that were submitted along with the land use application;
- If the property owner wishes to build a different project or sell the site, the existing stock land use will apply;
- A DC tied to plans provides certainty to the community on the project’s design.



Comprehensive Project Overview

Engagement Summary

On March 26, 2018, residents from the community of Bridgeland-Riverside were invited to a community information session hosted at the Bridgeland-Riverside Community Association Hall. The purpose of this community information session was to provide information and to solicit community feedback on the proposed developments located on 438 8 Street and 230 7A Street NE.

The community information session was attended by approximately 25 residents. The format of this information session was an informal discussion with the project’s planners, architects, and land owners. Residents had the opportunity to provide feedback using comment forms and sticky notes provided by the applicant. There were a total of **50 comments received** and have been categorized into the following themes;

- 1. **Traffic, Parking & City Servicing**
- 2. **Bridgeland-Riverside ARP Alignment & Zoning**
- 3. **Building Design**
- 4. **Massing & Height**
- 5. **Adjacent Privacy, Lighting & Green Space**
- 6. **Planning Context & Community Heritage**

The following is a breakdown of the number of received comments that fall under the six main themes;

- 1. Traffic, Parking & City Servicing: **7 Comments**
- 2. Bridgeland-Riverside ARP Alignment & Zoning: **12 Comments**
- 3. Building Design: **5 Comments**
- 4. Massing & Height: **10 Comments**
- 5. Adjacent Privacy, Lighting & Green Space: **3 Comments**
- 6. Planning Context & Community Heritage: **9 Comments**

In addition to these comments, there were **4 comments that are categorized as miscellaneous**. These comments did not fit under any of the five main themes.

Of the 50 comments received, 4 comments were of support, and 46 comments were of concern or opposition. The following is a breakdown of support comments vs. concern or opposition comments categorized under the six main themes.



Comment Comparison Table

	Traffic, Parking & City Servicing	Bridgeland-Riverside ARP Alignment & Zoning	Building Design	Height & Massing	Adjacent Privacy, Lighting & Green Space	Planning Context & Community Heritage	Misc
No. Comments of Support			3	1			
No. Comments of Concern or Opposition	7	12	2	9	3	9	4

Comprehensive Project Overview

THE SEARCH FOR THE ‘MISSING MIDDLE’

“...what every city is missing is something in the middle:

the kind of not-single-family, not-condo-tower-apartment style of housing that provides enough room for a couple of parents and kids, has some kind of ground-level space outside and doesn’t cost a fortune.”
[The Globe and Mail, A search for the ‘missing middle’ of housing, May 23rd 2018]

“Multi-generational living arrangements... are the fastest growing type of household in the country.”
[CTV News, Why more Canadians are embracing multi-generational households, April 5th 2018]

“Ms. Maser says ... peak millennials are purchasing two-storey detached homes in the suburbs as a way to increase affordability over more central areas.”
[The Globe and Mail, Sweet Spot, June 5th 2018]

“Nearly one in five Calgary households are overspending on their housing. They’re paying more than 30 per cent of their income every month towards shelter, which means they’re making decisions between eating or having a roof over their head.”
[Calgary Herald, Calgary lays out eight-year plan for \$320M affordable housing investment, April 30th 2018]

“Twenty-four per cent of those surveyed said they didn’t qualify for a mortgage at all; in Alberta, this rose to 32 per cent, and 50 per cent of 25- to 30-year-olds in Alberta – more than any other province – said they didn’t believe they’d be able to afford to buy a home in the next five years.”
[Globe and Mail, Millennials’ demand for affordability driving housing agenda, November 23rd 2017]

“Rental properties in Montreal, Quebec City and Halifax are performing well, for a variety of reasons. Quebecers have long looked on rentals favourably, and they’re eager to move into units centrally located in search of the live-work-play lifestyle. ... renting offers a significant cost advantage over owning a condo in that market.”
[PWC Canada, Emerging Trends in Real Estate 2018]

“But what every city is missing is something in the middle: the kind of not-single-family, not-condo-tower-apartment style of housing that provides enough room for a couple of parents and kids, has some kind of ground-level space outside and doesn’t cost a fortune.”
[Globe and Mail, A search for the ‘missing middle’ of housing, May 24th 2018]

“For a lot of people, a one-bedroom is basically fine for them and then they have a kid. All of a sudden, they need something bigger. It’s really hard and there is not much out there.”
[CBC News, Affordable housing solutions for ‘missing middle’ explored in Museum of Vancouver workshop, February 1st 2018]

“Back-to-backs, brownstones, bungalow courts, clustered housing, plexes, maisonettes, row-houses, stacked towns and six-packs. Throughout the world, these low-density multi-housing forms provide affordable homes for millions of households. However, in British Columbia, most of our housing is either single-family homes or apartments. As a result, architects and planners are increasingly referring to these other “gentle density” housing solutions as “the missing middle”.”
[Vancouver Sun, ‘Missing middle’ competition offers residential housing solutions, May 12th 2018]

“A looming affordability crisis is poised to hit seniors across the country as the baby boom generation makes its long-predicted shift into its golden years, squeezing the supply of retirement home places and pushing up rents, according to a new report from the rating agency DBRS Ltd.”
[Financial Post, A looming housing affordability crisis is poised to hit seniors across Canada, May 13th 2018]

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NOTES

PROJECT
Bridgeland 4

DATE

SCALE
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

OLD RM-4 SOLUTION

- 1 Lack of privacy
- 2 Circulation issues
- 3 Lack of amenity space
- 4 Blank side elevation

Livable volume is approximately 40,150 ft3
\$650,000 - \$720,000



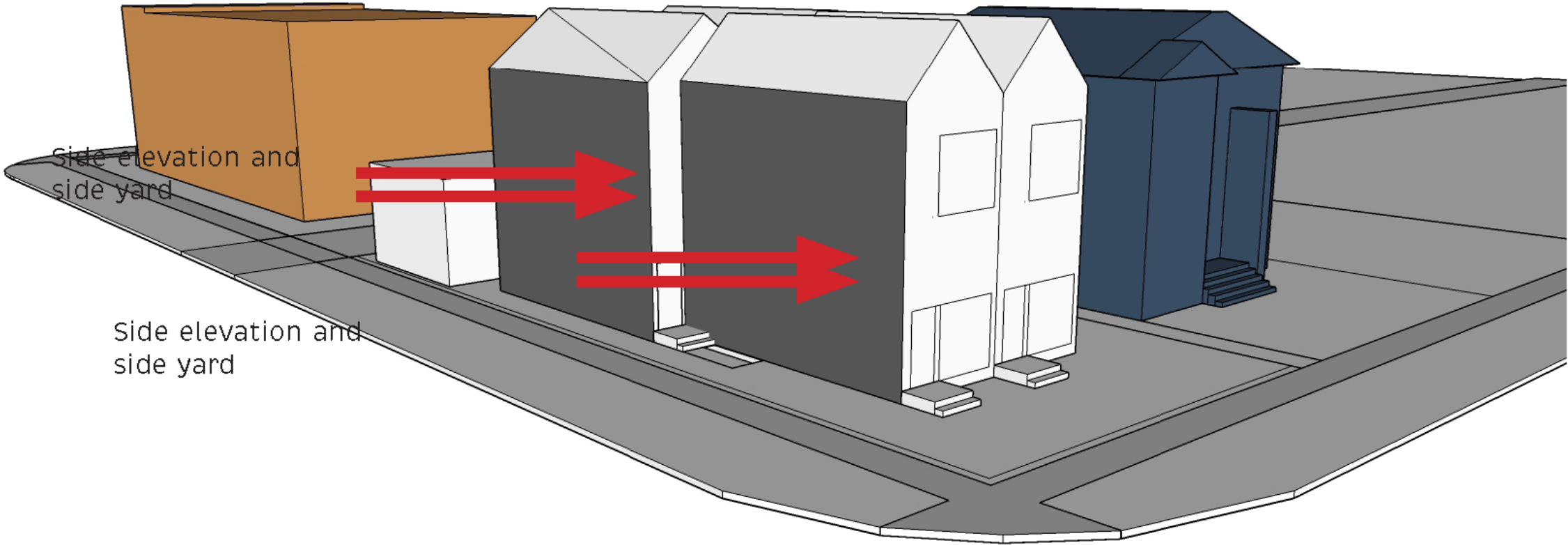
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PROJECT LOCATION

230 7A Street NE
438 8 Street NE

4-SHEET TITLE

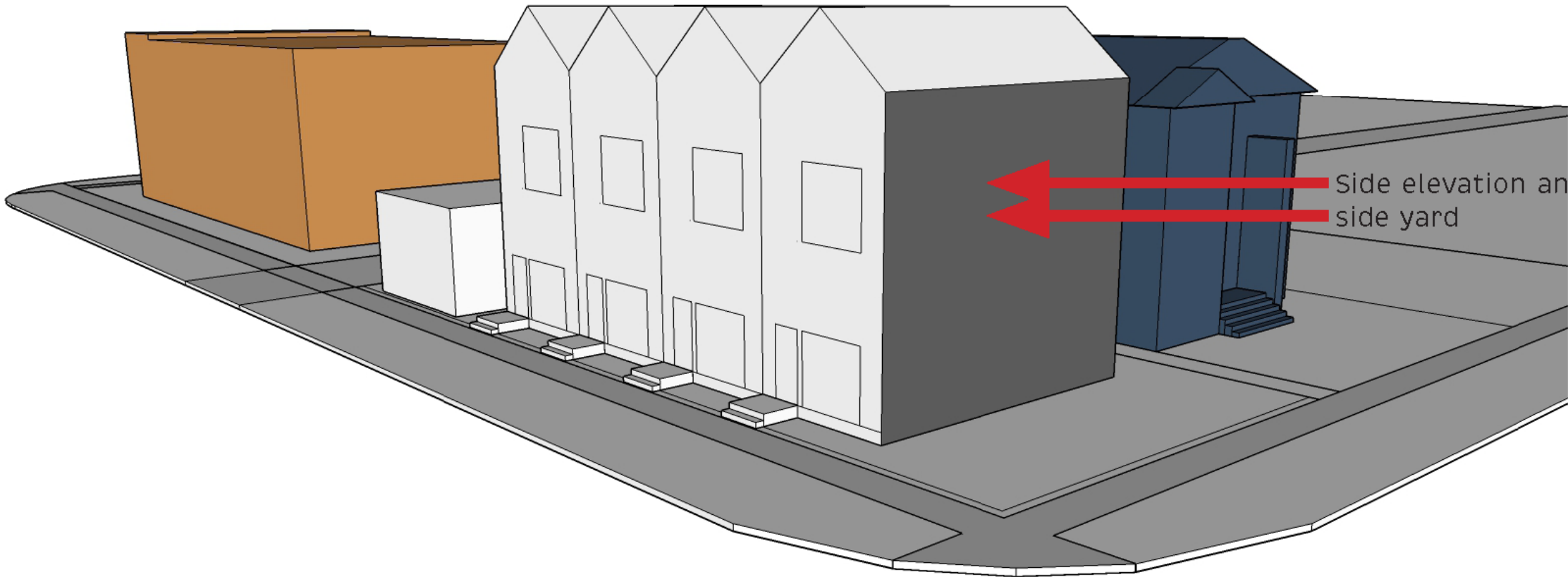
Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

NEW MC-G SOLUTION

- 1 Lack of privacy
- 2 Circulation issues
- 3 Lack of amenity space
- 4 Lack of unit variation
- 5 Overlooking
- 6 Blank side elevation

Livable volume is approximately 101,322 ft³
\$650,000 - \$720,000



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230 7A Street NE
438 8 Street NE

4-SHEET TITLE

Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

DUPLEX WITH GARAGE SUITES

- 1 Lack of unit variation
- 2 Unaffordable for most people
- 3 Blank side elevation

Livable volume is approximately 57,004 ft
\$950,000 - \$1,400,000



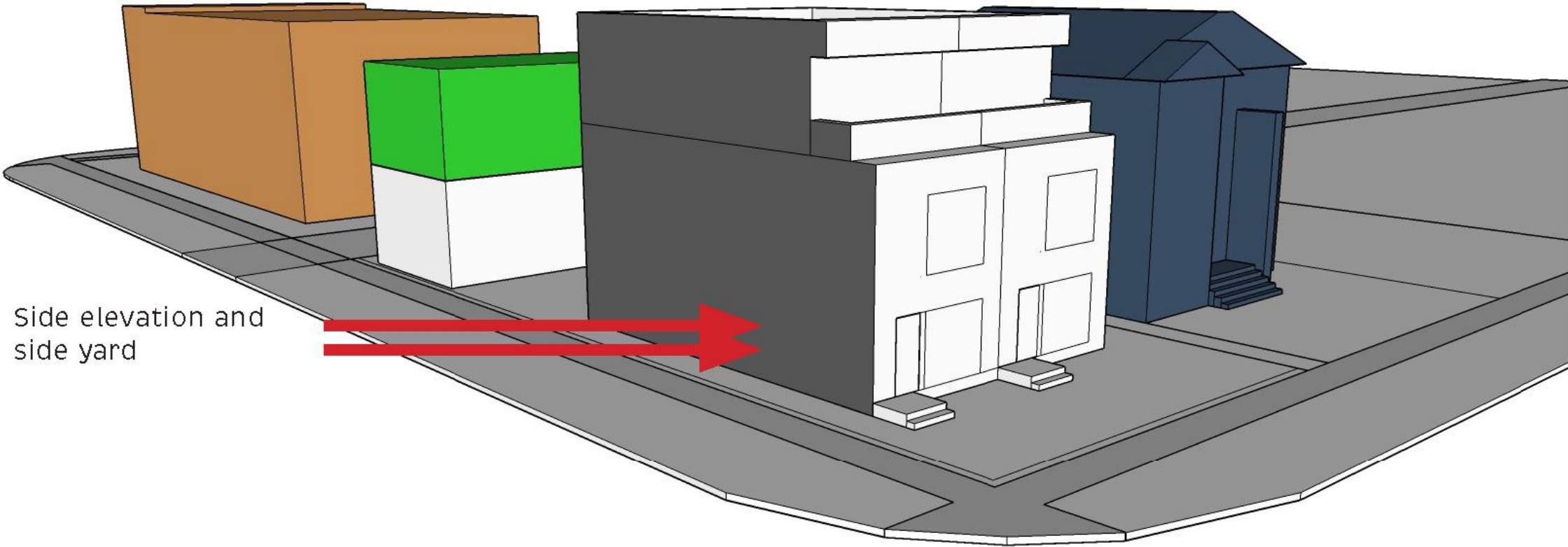
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DESIGNER: NAME: DATE:



Side elevation and
side yard

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230 7A Street NE
438 8 Street NE

4-SHEET TITLE

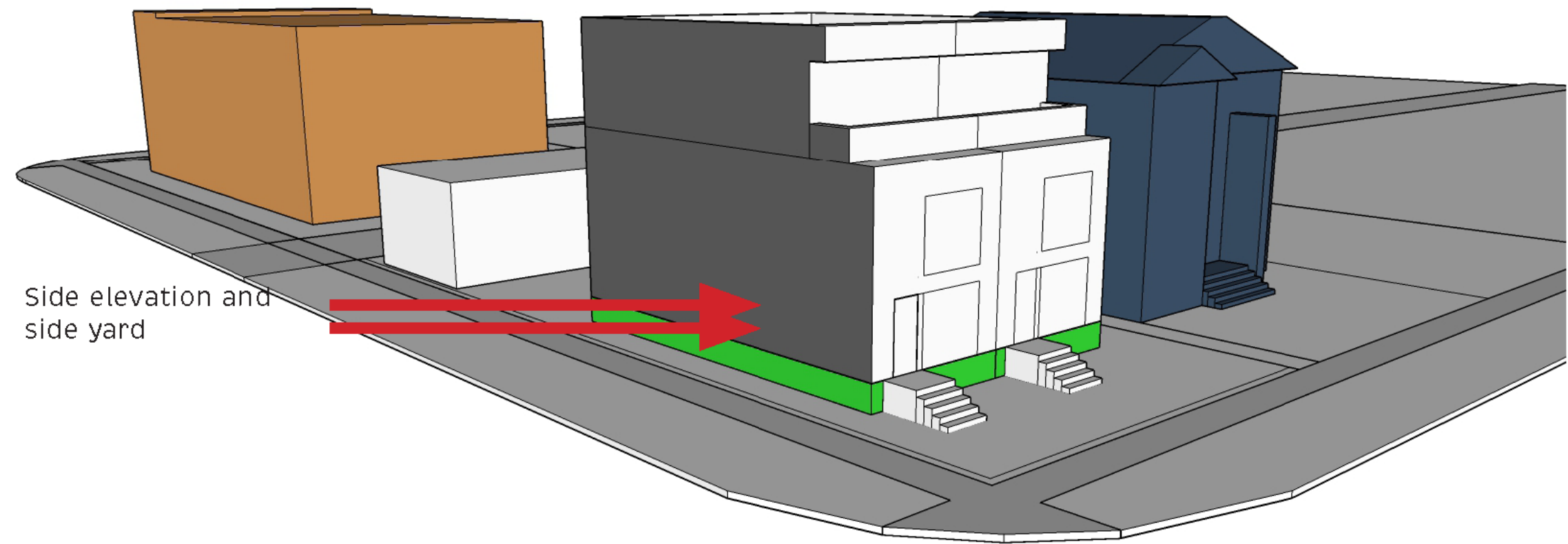
Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

DUPLEX WITH BASEMENT SUITES

- 1 Basement suites are “secondary” solutions
- 2 Unaffordable for most people

Livable volume is approximately 49,252 ft3
\$900,000 - 1,250,000



Side elevation and
side yard

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438 8 Street NE

4-SHEET TITLE

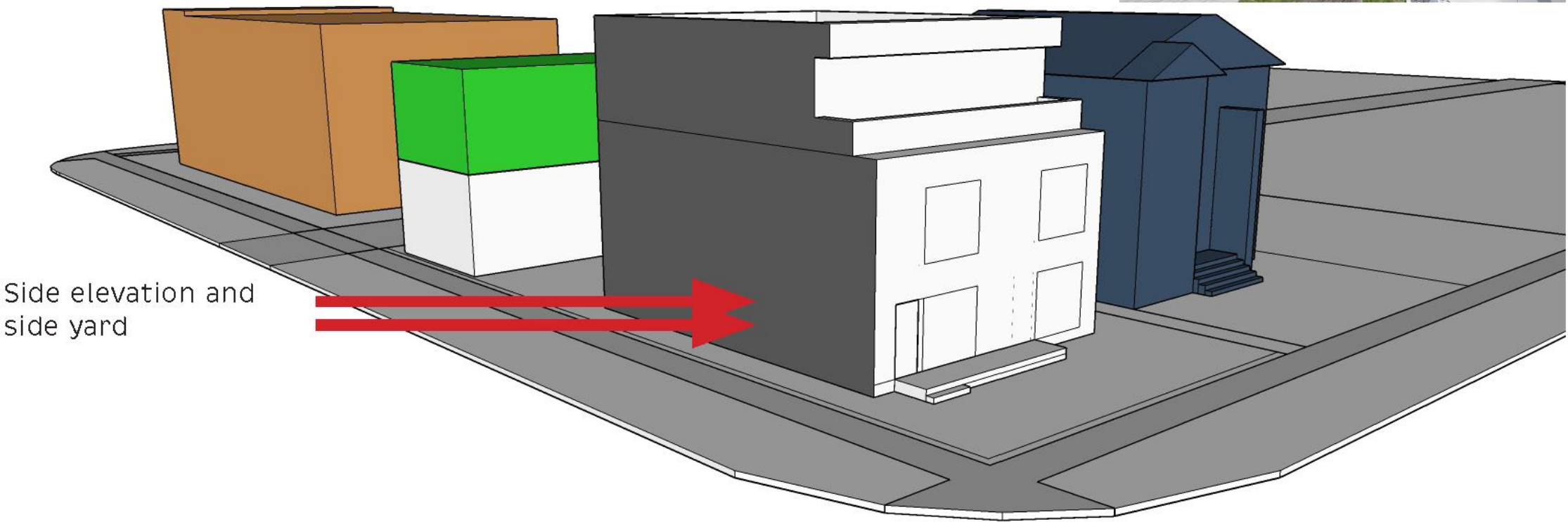
Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

LARGE SINGLE FAMILY

- 1 Unaffordable for most people
- 2 Highest price point

Livable volume is approximately 57,004 ft3
\$1,200,000 - \$1,850,000 and up



Side elevation and
side yard

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PROJECT LOCATION
230 7A Street NE
438 8 Street NE

PROJECT TITLE

Comprehensive Project Overview

BASEMENT SUITES

- 1 Basement suites are not great solutions
- 2 Generally not well lit
- 3 Not great for families
- 4 Generally not well sound proofed
- 5 Ownership by tenant is not an option
- 6 Limitations - not barrier free

Students, short term rentals, low income individuals



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GARAGE SUITES

- 1 Small
- 2 Generally a 1 bedroom / studio layout not conducive to families
- 3 Ownership by tenant is not an option
- 4 Limitations - not barrier free

Students, short term rentals, low income individuals



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PROJECT
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DATE

SCALE
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PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

NEW SOLUTION TO DENSITY

NEW STACKED TOWNHOUSE

- 1 Variety of unit types
- 2 Low, medium, upper price points
- 3 Engages the street and the avenue
- 4 Designed user friendly and engaged amenity space to streetscape

Livable volume is approximately 61,235 ft3
\$375,000 - \$800,000



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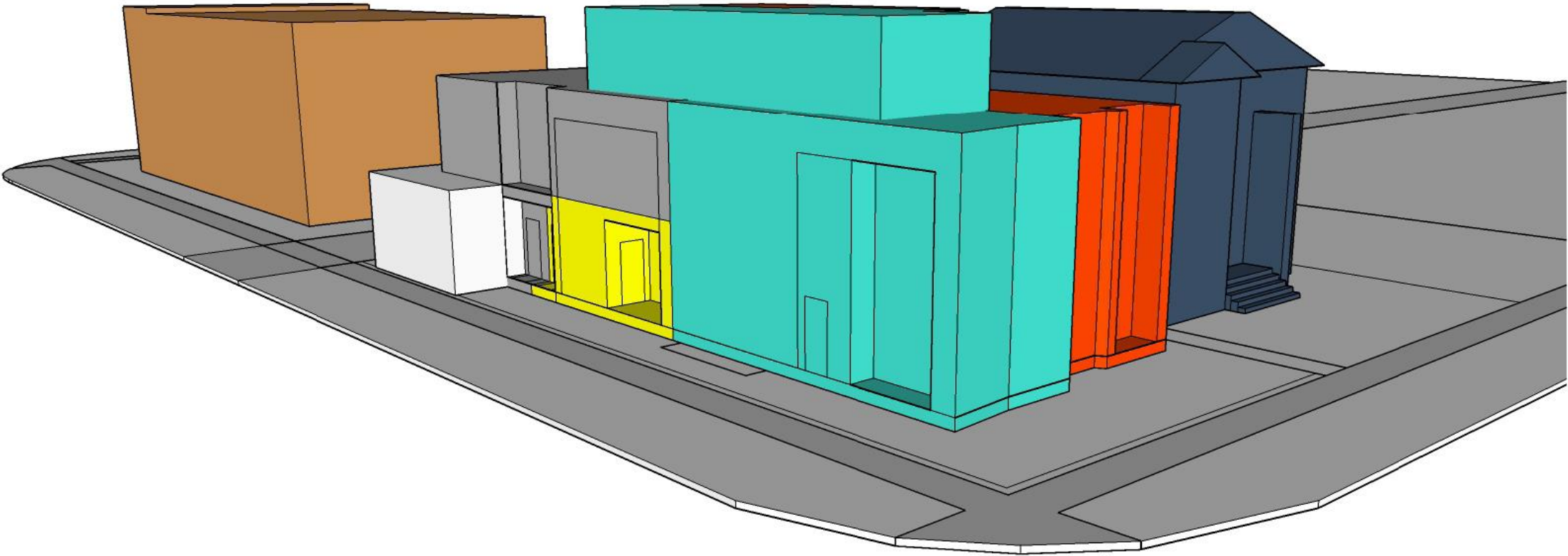
230 7A Street NE
438 8 Street NE

4-SHEET TITLE

Comprehensive Project Overview

NEW SOLUTION TO DENSITY

NEW STACKED TOWNHOUSE



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230 7A Street NE
438 8 Street NE

4-SHEET TITLE

Comprehensive Project Overview

NEW SOLUTION TO DESNISTY

NEW STACKED TOWNHOUSE



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NOTES

PROJECT
Bridgeland 4

DATE

SCALE
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

PROJECT TITLE

Comprehensive Project Overview

NEW SOLUTION TO DENSITY

OLD RM-4 SOLUTION

Livable volume is approximately 40,150 ft3
\$650,000 - \$720,000

NEW MC-G SOLUTION

Livable volume is approximately 101,322 ft3
\$650,000 - \$720,000

DUPLEX WITH GARAGE SUITES

Livable volume is approximately 57,004 ft
\$950,000 - \$1,250,000

DUPLEX WITH BASEMENT SUITES

Livable volume is approximately 49,252 ft3
\$950,000 - 1,250,000

LARGE SINGLE FAMILY

Livable volume is approximately 57,004 ft3
\$1,250,000 - \$1,850,000 and up

NEW PROPOSED STACKED SUITED TOWNHOUSE

Livable volume is approximately 61,235 ft3
\$375,000 - \$800,000

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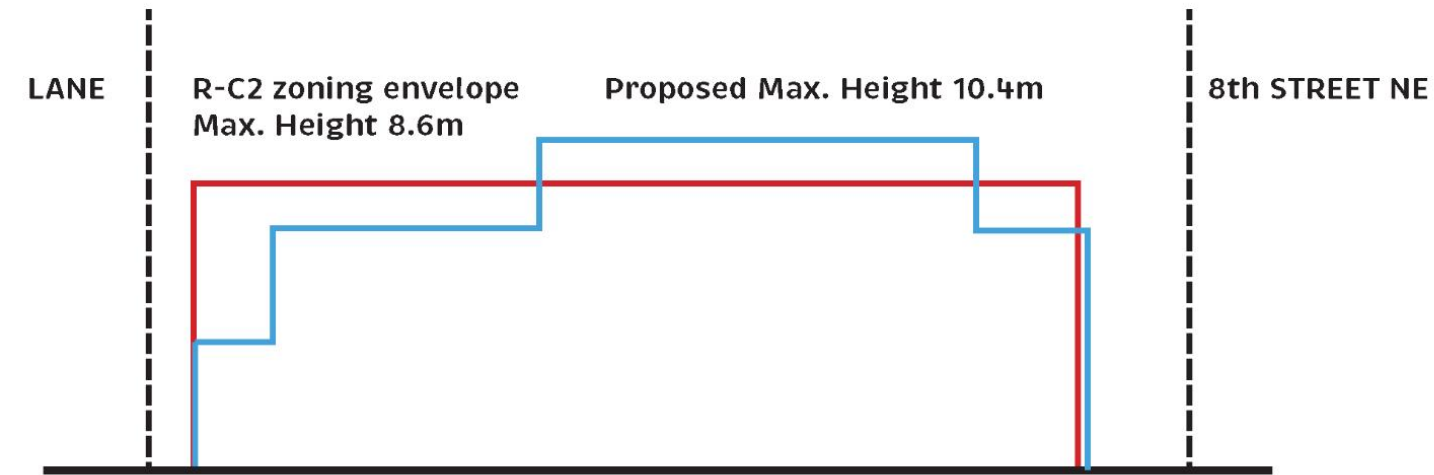
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NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

MASSING 438 8TH STREET



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230 7A Street NE
438 8 Street NE
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438 8 Street NE

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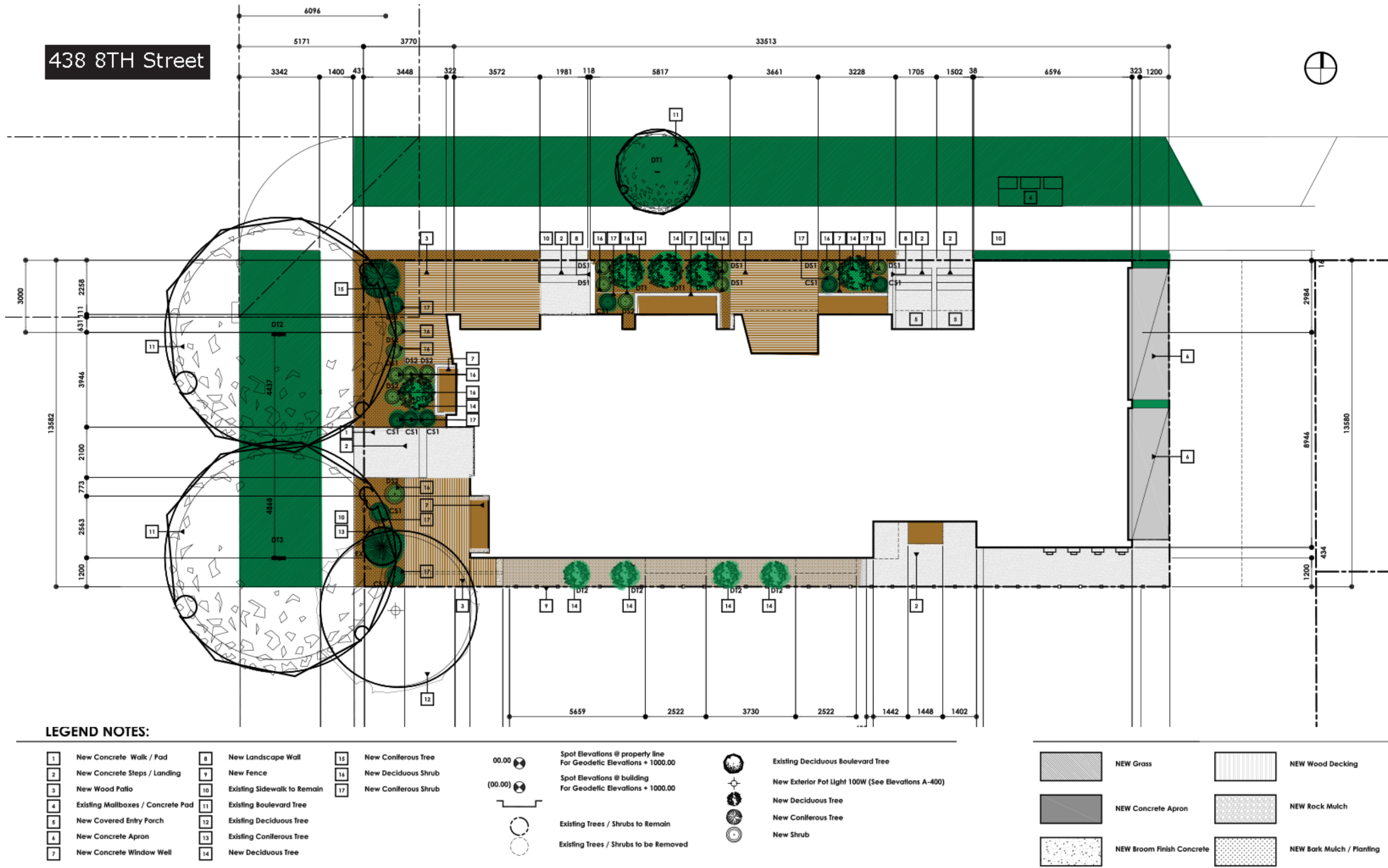
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PROJECT LOCATION
230 7A Street NE
438 8 Street NE

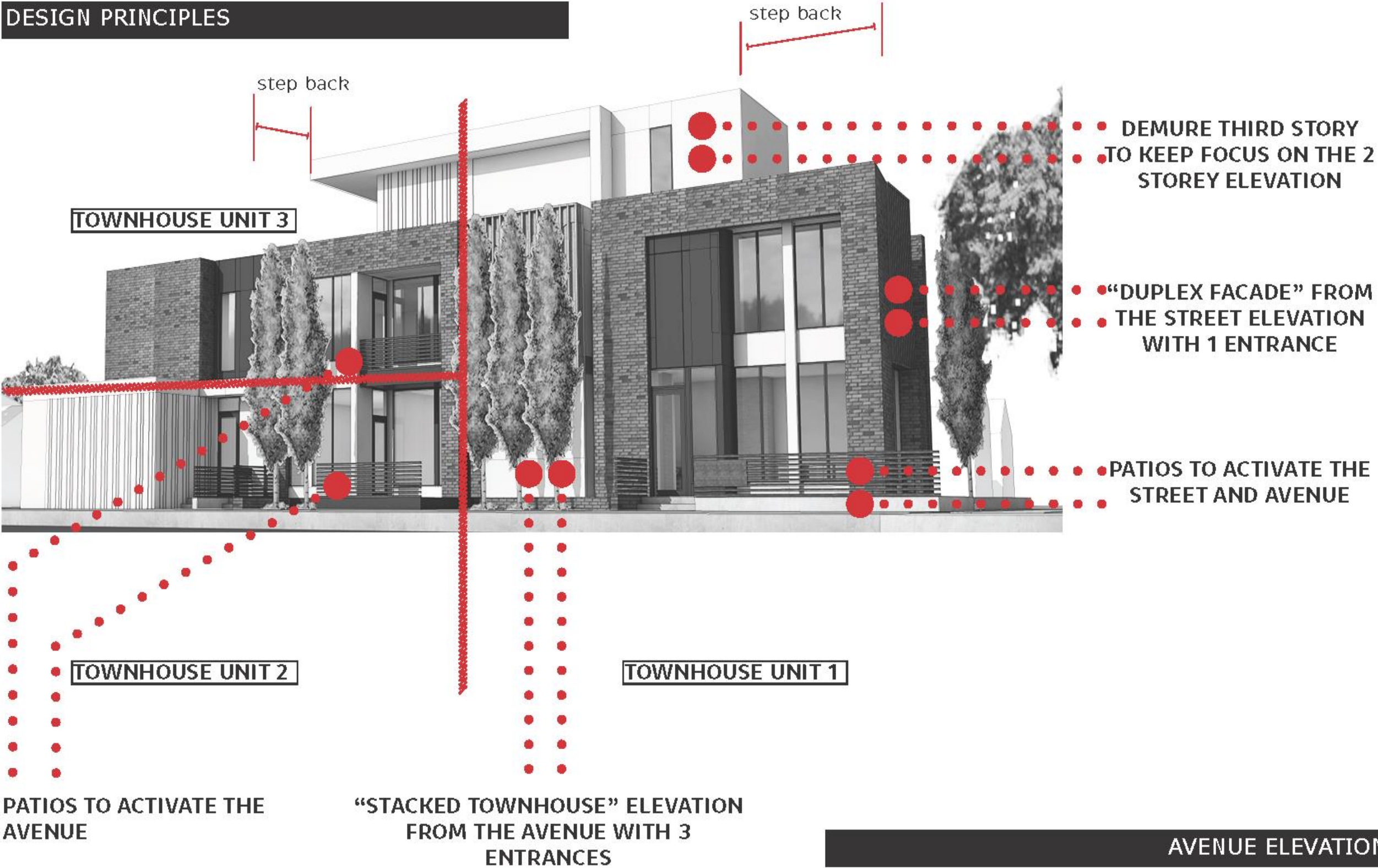
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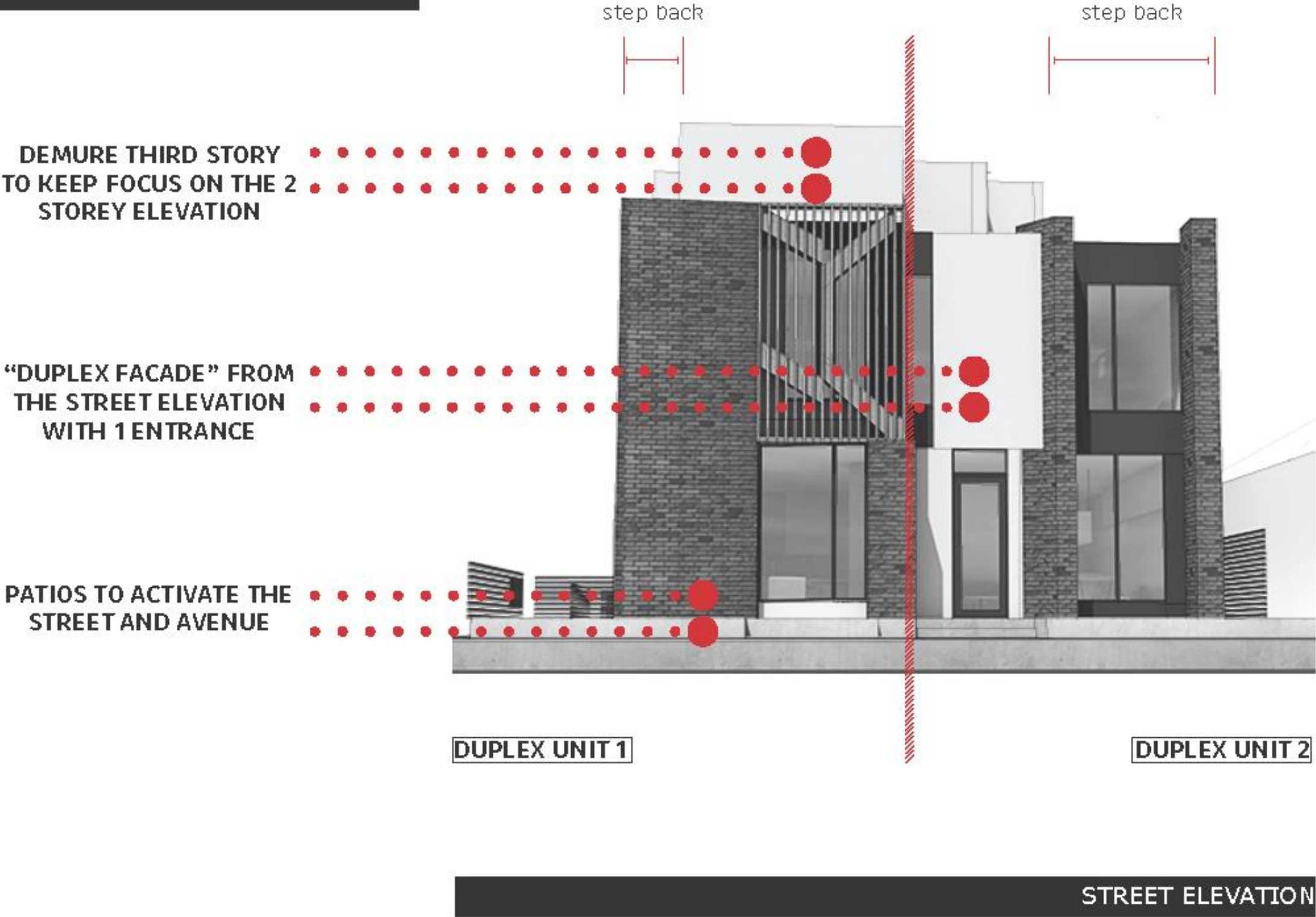
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DESIGN PRINCIPLES



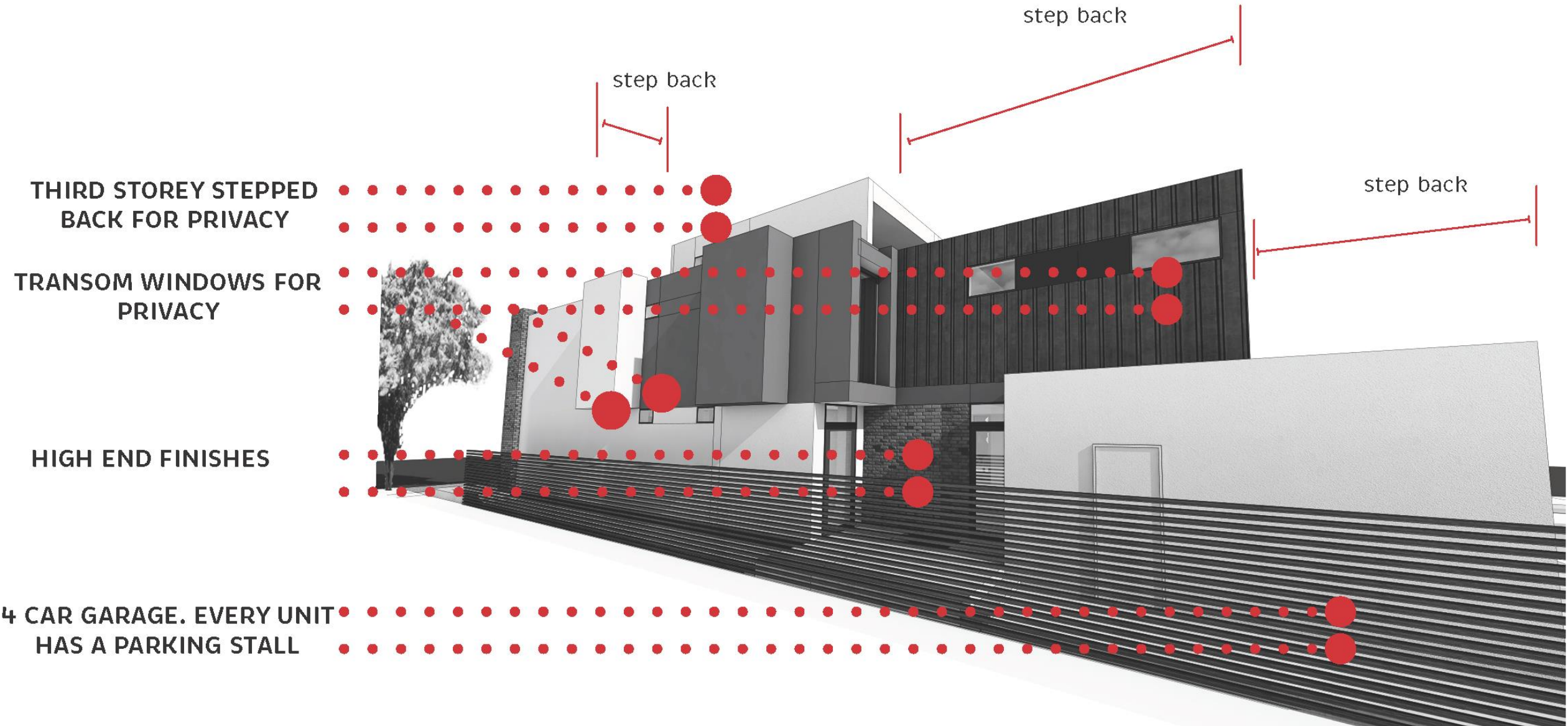
Comprehensive Project Overview

DESIGN PRINCIPLES



Comprehensive Project Overview

DESIGN PRINCIPLES



SIDE YARD ELEVATION

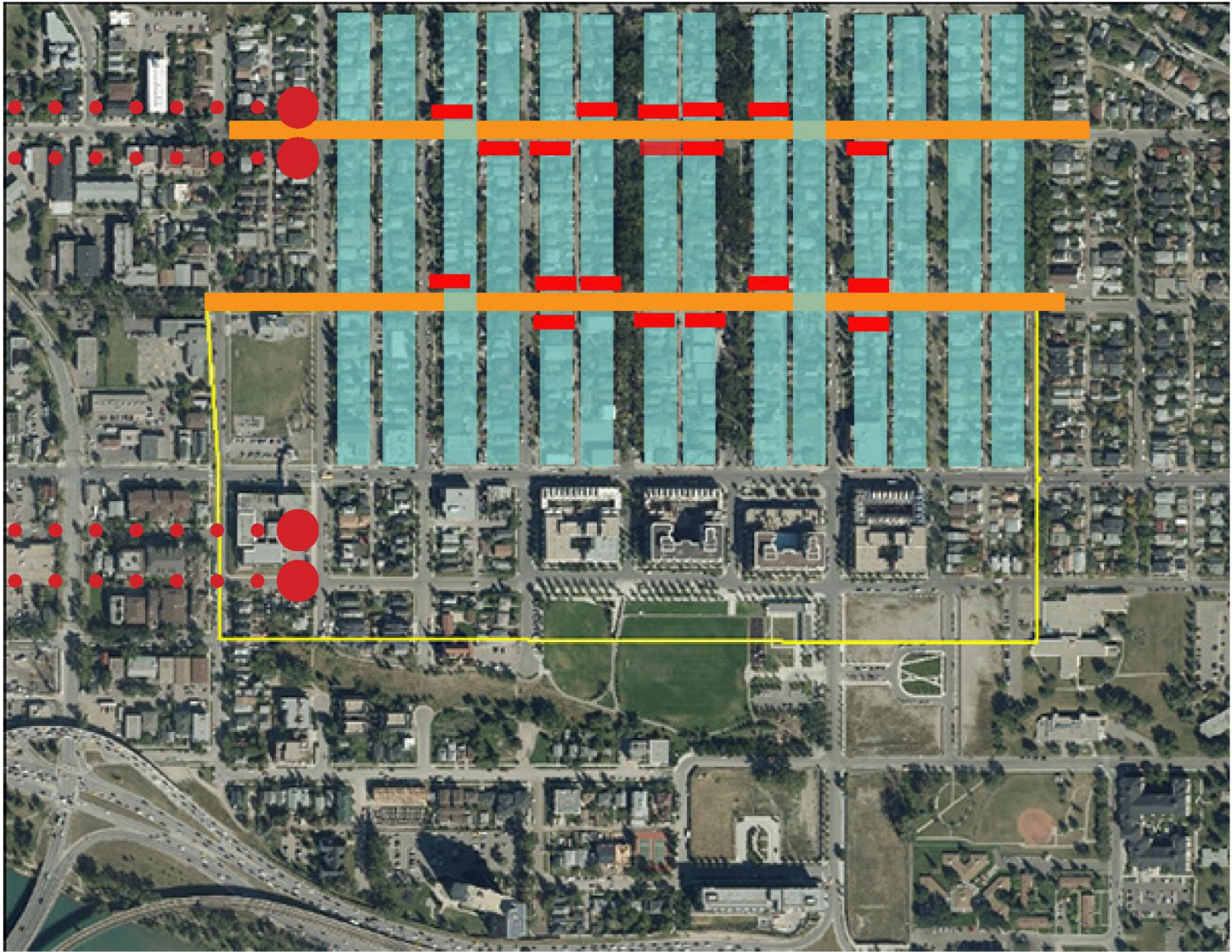
Comprehensive Project Overview

Urban Design - Activating the Street AND avenue

BRIDGELAND

GOOD URBAN DESIGN
ADDRESSES THE AVENUE
AND THE STREET

“MAIN STREETS” INITIATIVE
AREA



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PROJECT LOCATION
230 7A STREET NE
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BRIDGELAND, ALA 36004

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NOTES

PROJECT
Bridgeland 4

DATE

SCALE
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

MATERIALS

Charcoal Metal Panel

Natural Concrete

Charcoal brick

Glass: large vertical windows

Copper Screen

Warm Grey Stucco

Off-white Stucco and Metal Cladding



438 8TH STREET

230 7A STREET



Charcoal Metal Panel

Natural Concrete

Light Brick

Glass: large vertical windows

Warm Grey Stucco

Off-white Stucco

Off-white Metal Cladding

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438 8 Street NE

PROJECT TITLE

Comprehensive Project Overview



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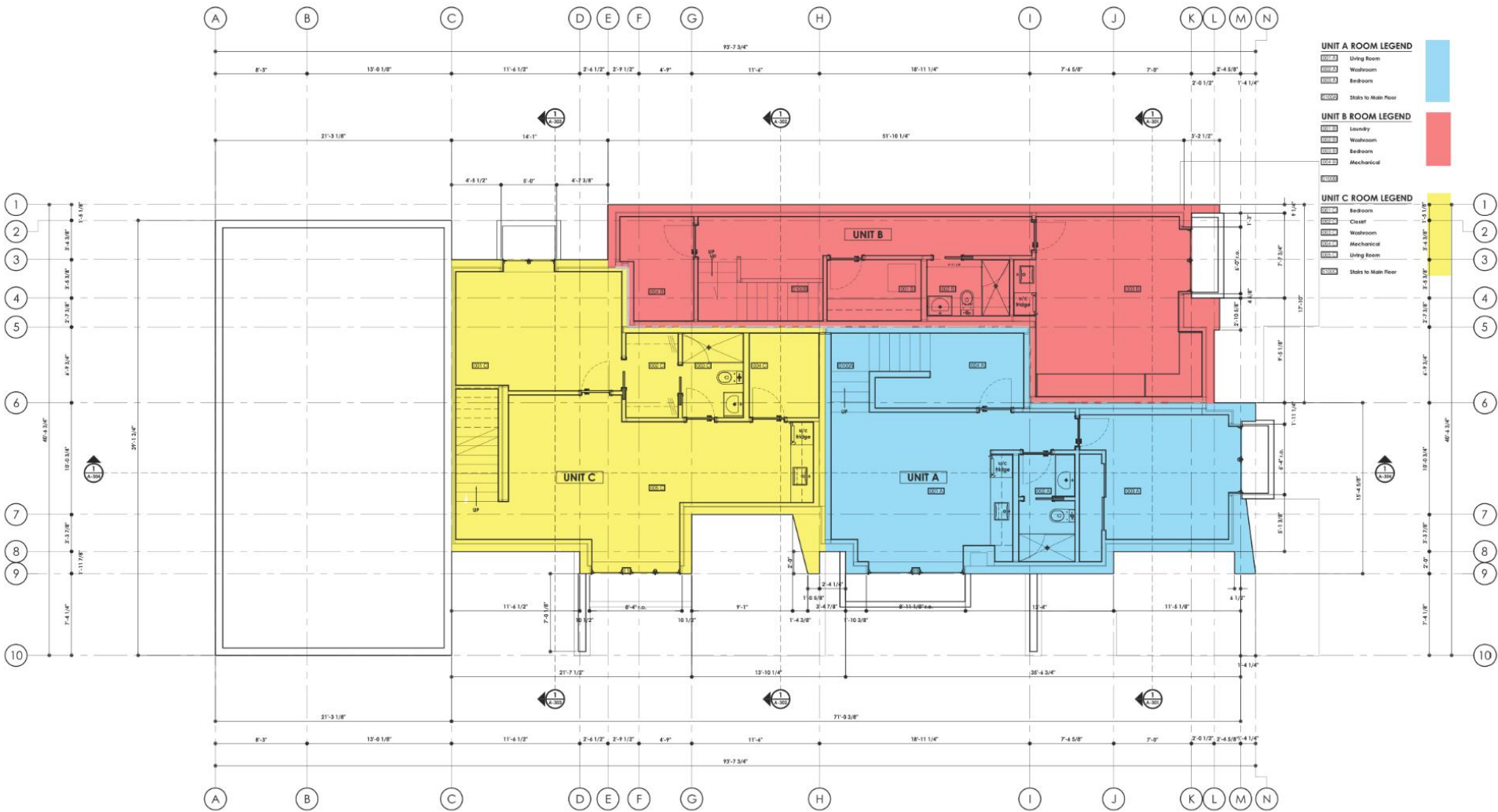
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Comprehensive Project Overview

BASEMENT



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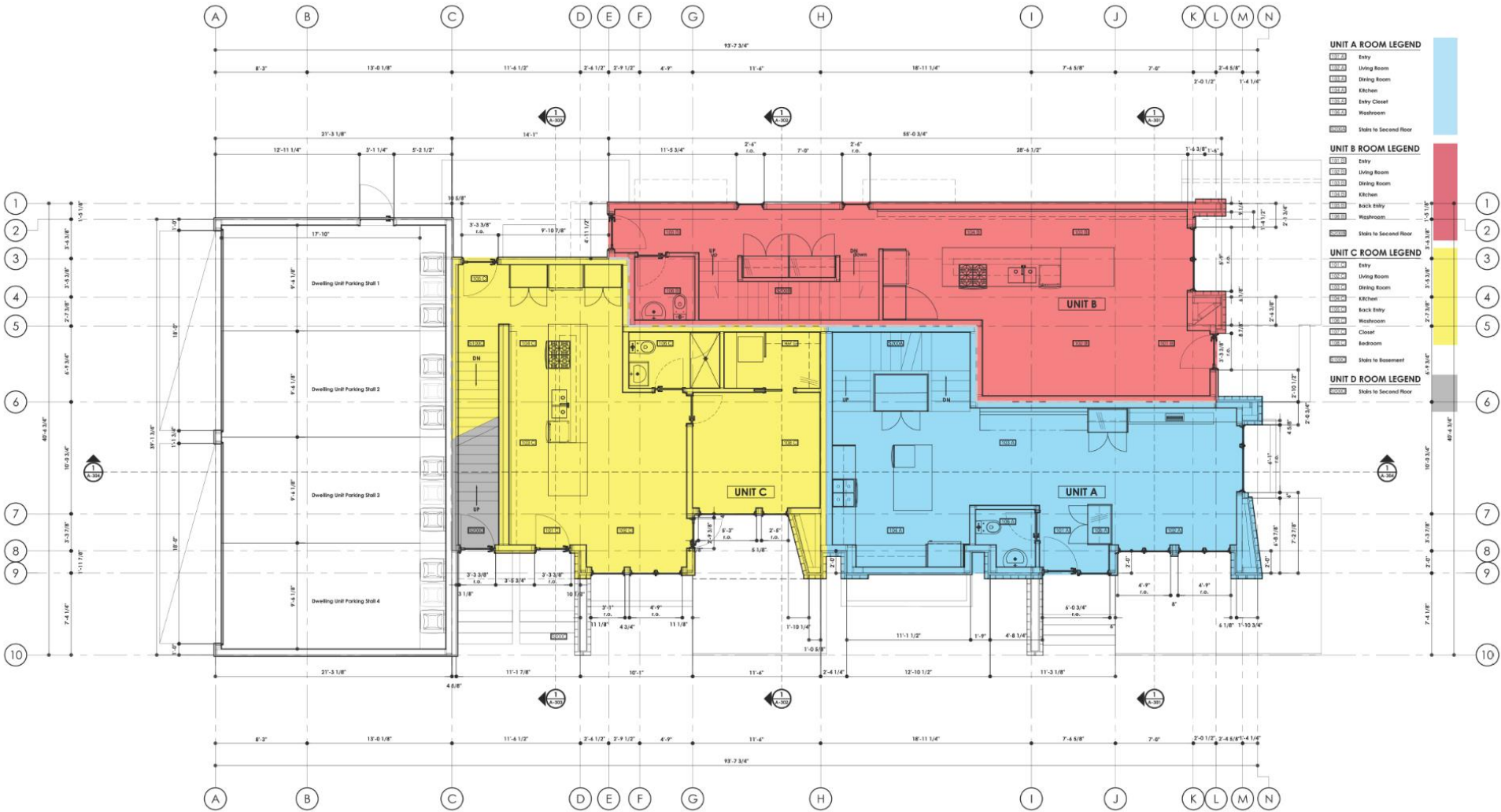
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MAIN FLOOR



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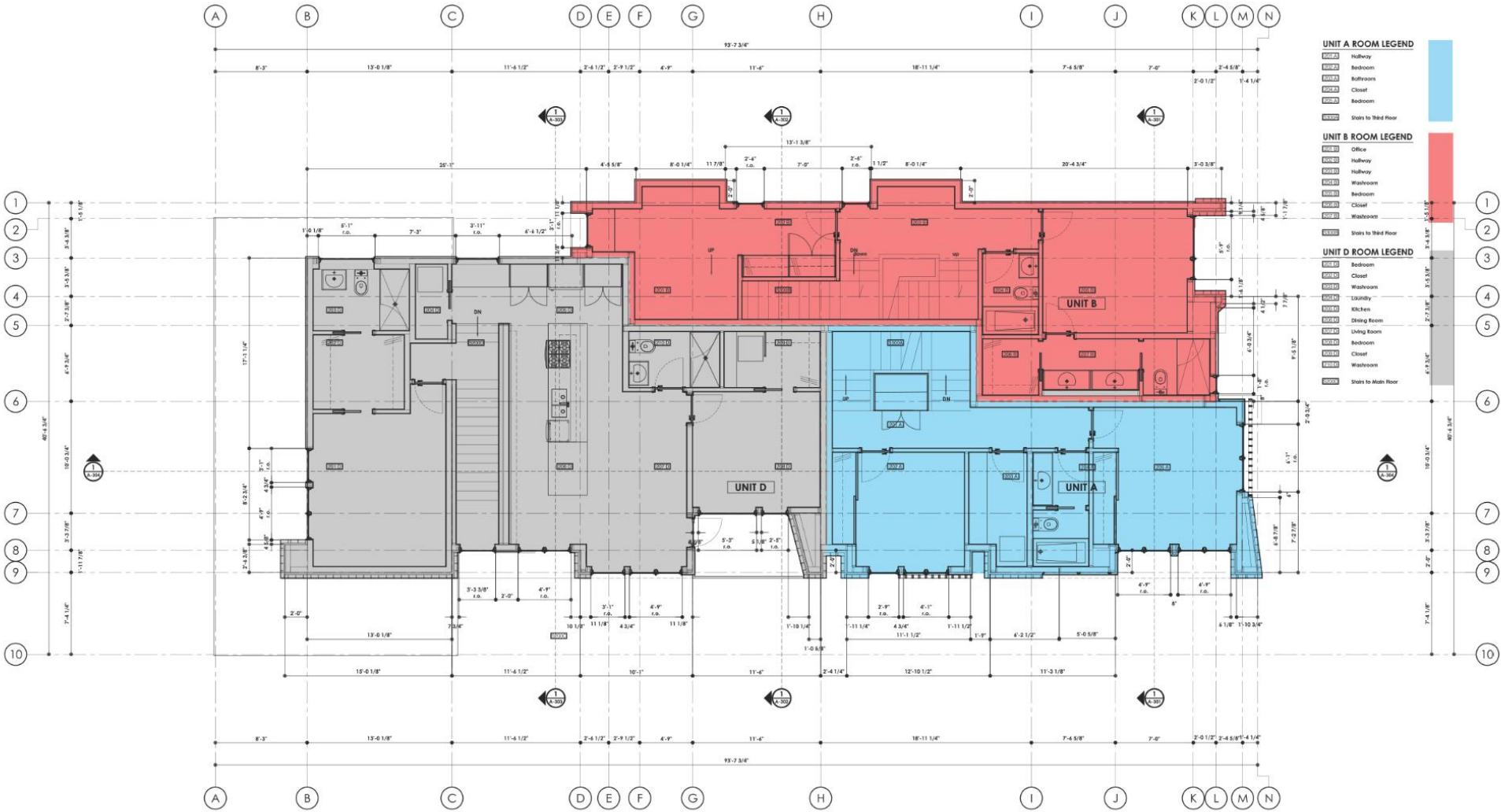
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SECOND FLOOR



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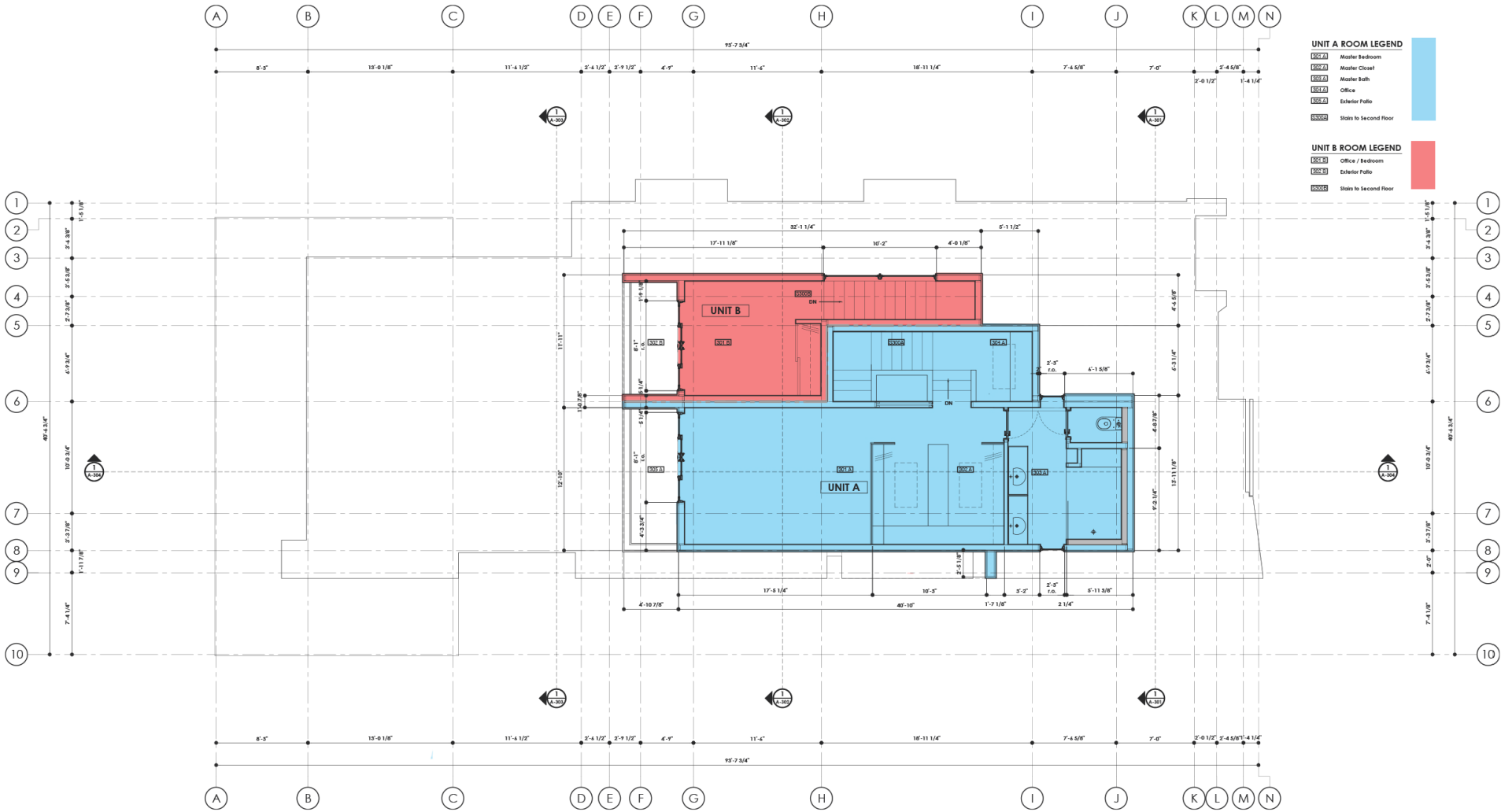
PROJECT LOCATION

230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

THIRD FLOOR



2016 /// 18

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ISSUE No. DATE
REVISION

PROFESSIONAL SEAL(S)
CONSULTANTS

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PROJECT
Bridgeland 4

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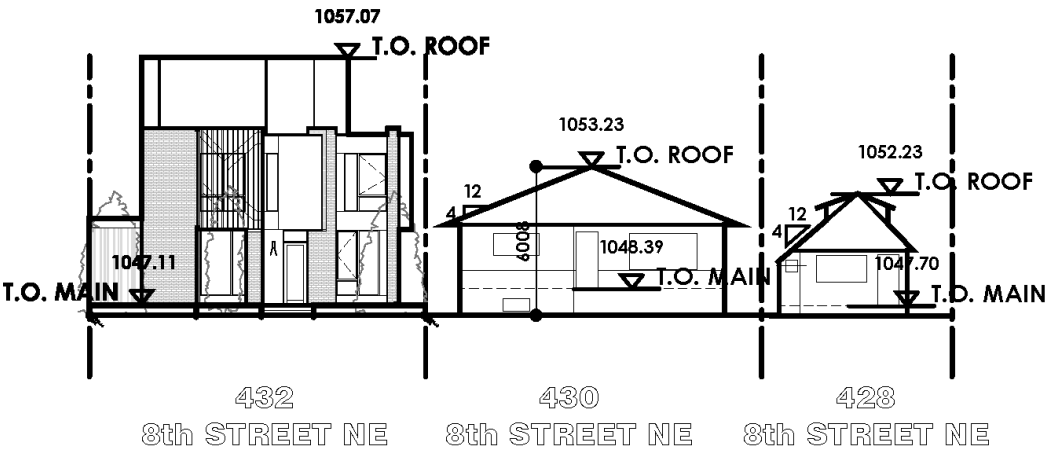
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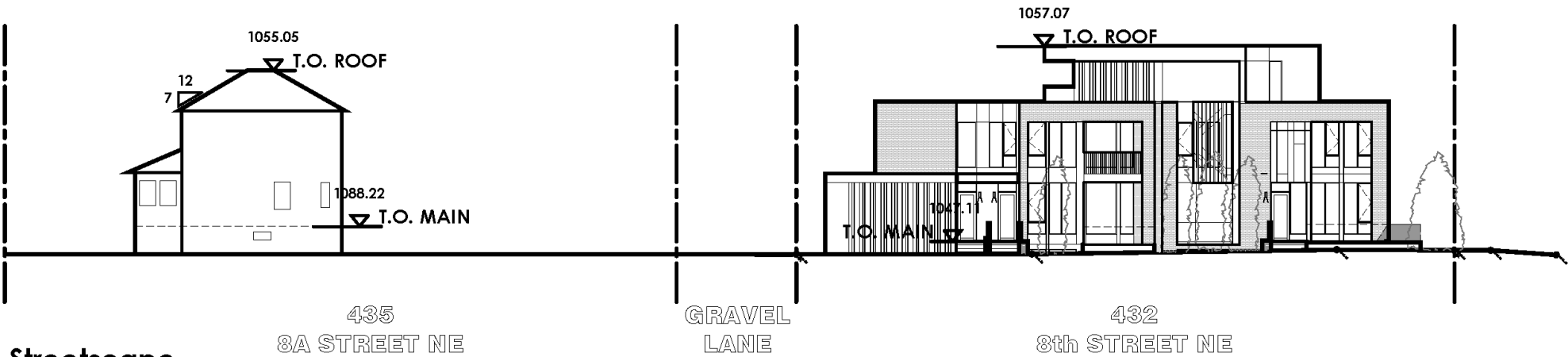
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Comprehensive Project Overview

438 8th Street Streetscapes



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Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0840
Page 1 of 12

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 230 - 7A Street NE, LOC2018-0021

EXECUTIVE SUMMARY

This land use redesignation application was submitted by O2 Planning and Design on 2018 January 29 on behalf of the landowners Zakir Kanji and Adil Mawji. This application proposes to change the designation of this property from a DC Direct Control District (273D2017) to a 'tied to plans' DC Direct Control District to allow for a specific multi-residential development on this property. The intent of the land use approach is twofold, one of which is to enable moderate intensification of one additional unit, as well as enable redevelopment of this site without presupposing the planning outcomes or Council decision on the new policy planning work the community of Bridgeland-Riverside is currently undergoing.

To evaluate the feasibility of a 'tied to plans' land use approach, a development permit level of analysis is required. As such, this redesignation application is accompanied by a development permit application (DP2018-0390) submitted on 2018 January 29. The development permit proposes a three-storey multi-residential building with four residential units. A simplified version of the development permit drawings are 'tied' to the proposed DC Direct Control District as Schedule C. The Development Authority is prepared to approve the development permit application, subject to Council's decision on this land use and policy amendment application.

An amendment to the *Bridgeland-Riverside Area Redevelopment Plan* (ARP) is required to accommodate the proposed land use redesignation discussed further. The proposal conforms to the ARP as amended, and is in keeping with applicable policies of the *Municipal Development Plan*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendment to the Bridgeland-Riverside Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares \pm (0.11 acres \pm) located at 230 – 7A Street NE (Plan 4647V, Block 111, Lot 11) from DC Direct Control District to DC Direct Control District to accommodate a specific multi-residential development as a permitted use with guidelines (Attachment 3); and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

Planning & Development Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0840
Page 2 of 12

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

BACKGROUND

The community of Bridgeland-Riverside is subject to the policies of the *Bridgeland-Riverside Area Redevelopment Plan* (ARP) originally approved on 1980 September 3.

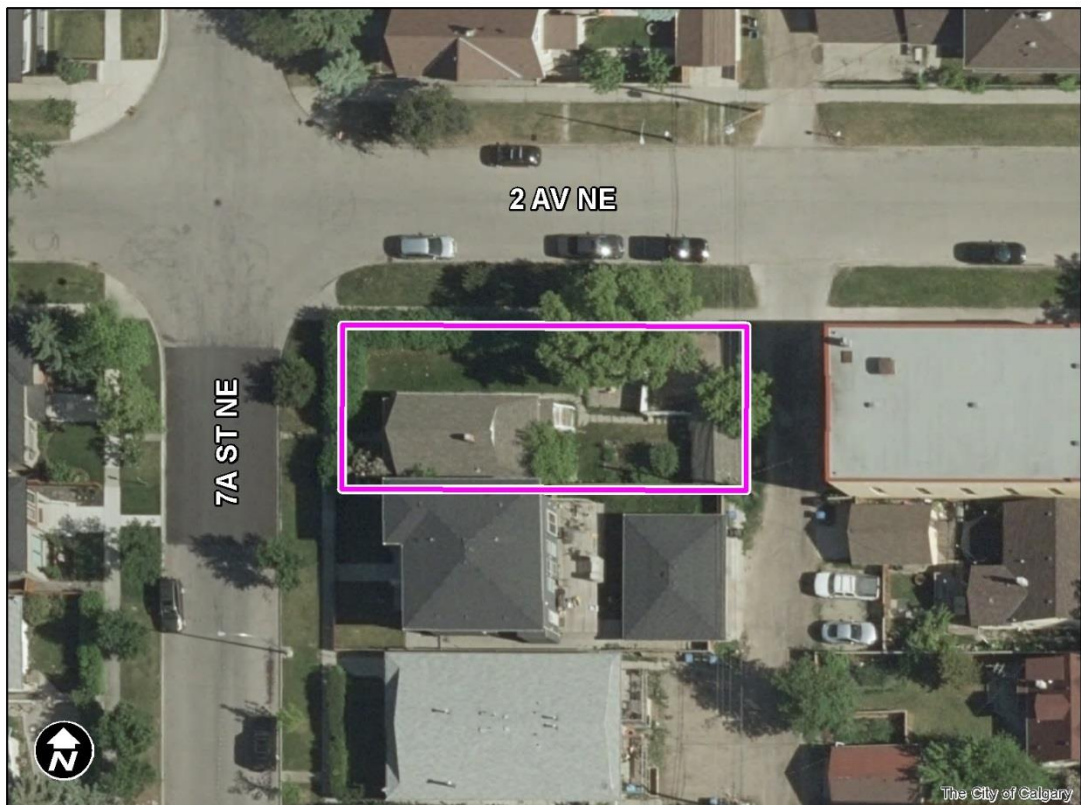
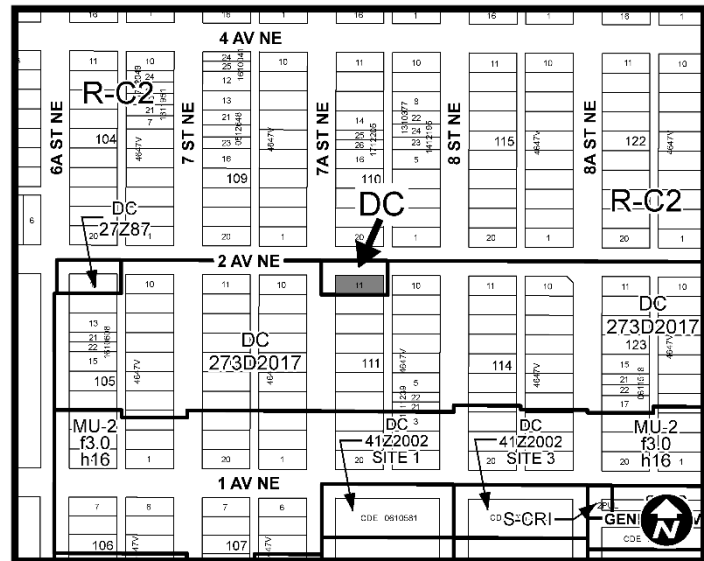
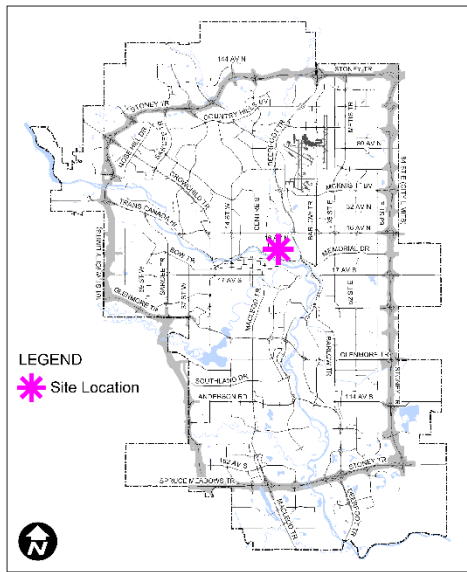
On 2017 August 01 (Bylaws 46P2017, 272D2017, 273D2017 AND 274D2017) Council approved major amendments to portion of the *Bridgeland-Riverside ARP* that coincide with the main street areas of 4 Street/Edmonton Trail NE and 1 Avenue NE. Through these amendments, the main street's adjacent transition areas were redesignated to DC Direct Control District intended to allow for a mix of ground-oriented residential redevelopment while providing an incentive for the retention of character homes. The subject parcel was identified under this transition area and was therefore redesignated under this DC Direct Control District (Attachment 8). The subject parcel however has not been identified to have a character home. For parcels falling under this DC Direct Control that do not contain a character home the rules of the Residential – Grade-Oriented Infill (R-CG) District apply.

In addition, a new ARP is in the process of being created by The City in consultation with the Bridgeland-Riverside Community Advisory Group and the greater community. It is anticipated that the ARP work will be presented to Council by Winter 2018/ Spring 2019.

In the interim, The City cautions applicants in advancing with applications within the community that propose redevelopment and are not aligned with the existing policy. If there are issues from the applicant's perspective with delaying their decision dates to align with the new ARP, The City will continue to process and make recommendations to the Calgary Planning Commission. Recommendations for land use amendments and decisions for development permits will be based upon the analysis of the compatibility of the proposal with the surrounding land use and context.

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

Location Maps



Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 230 - 7A Street NE, LOC2018-0021

Site Context

The subject site is located in the community of Bridgeland-Riverside south of 2 Avenue NE and east of 7A Street NE. Surrounding development is characterized primarily by a mix of single and semi-detached homes with a church located directly to the east. The predominant land use in this area is the Residential - Contextual One/Two Dwelling (R-C2) District north of 2 Avenue, and the DC Direct Control District intended to allow for a mix of ground-oriented residential redevelopment while providing an incentive for the retention of character homes. Further south of the DC District area is a Mixed Use - Active Frontage (MU-2f3.0h16) District intended to accommodate a mix of commercial and residential development along the main street area of 1 Avenue NE.

The site is approximately 0.05 hectares in size with approximate dimensions of 13.56 by 33.53 metres. A rear lane exists to the east of the site. The property is currently developed with a single detached dwelling and detached garage located off the lane. The land is primarily flat with the largest grade difference being 0.64 metres from the south-east corner, sloping downwards to the north-east corner along the lane.

As identified in *Figure 1*, the community of Bridgeland-Riverside has seen population growth over the last several years reaching its population peak in 2017.

Figure 1: Community Peak Population

Bridgeland-Riverside	
Peak Population Year	2017
Peak Population	6,332
2017 Current Population	6,332
Difference in Population (Number)	± 0
Difference in Population (Percent)	0%

Additional demographic and socio-economic information may be obtained online through the [Bridgeland- Riverside](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for a specific multi-residential development that is compatible with the established built form of the existing neighbourhood. Though a minor amendment to the *Bridgeland-Riverside Area Redevelopment Plan* (ARP) is required, the proposal generally aligns with the objectives of applicable policies as discussed in the Strategic Alignment of this report.

Land Use

The existing DC Direct Control District is based on the Residential – Grade-Oriented Infill (R-CG) District which is a residential designation that is primarily for two to three storey (11 metres maximum) rowhouse developments where one façade of each dwelling unit must

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

directly face a public street. The DC also includes rules to provide an incentive for the retention of character homes.

The R-CG District also allows for a range of other low-density housing forms such as single detached, semi-detached and duplex dwellings. Secondary suites (one Backyard Suite or Secondary Suite per unit) are also allowable in R-CG developments. Secondary suites do not count against allowable density and do not require motor vehicle parking stalls in the R-CG District provided they are below 45 square metres in size.

The proposed DC Direct Control District (Attachment 3) is based on the existing R-CG District with the additional permitted use of Permitted Development. The proposed permitted use (i.e. Permitted Development) refers to Schedule C of the proposed DC district which includes simplified development drawings that are consistent with the development permit application (DP2018-0390), for a three-storey multi-residential building with four residential units that has been reviewed concurrently with this land use redesignation application. The four residential units are one additional unit to what would have been allowable under the base R-CG District.

The purpose of the simplified development drawings is to identify the key elements of the proposed development to provide certainty for the building's number of units, height, form and location on site. Developments which conform to Schedule C will be a permitted use. The development permit is consistent with the simplified drawings contained in Schedule C of the proposed DC District.

The proposed DC District also ensures that if this specific development is not built, the rules of the existing Residential – Grade-Oriented Infill (R-CG) District would continue to apply. No other forms of multi-residential development would be allowed on this parcel without a new land use redesignation. What this does is provide certainty to the community that if the concurrent development is not built, the base district rules then apply.

This land use redesignation and concurrent development permit application were applied for following the design review process that an earlier submitted concurrent land use and development permit application located on 438 – 8 Street NE underwent. Once a building design that was compatible in height and form to the surrounding built form was developed for the 8 Street NE development, a similar design was then adopted for the subject parcel which is one block away and contextually similar in regards to its surrounding built form. This concurrent and 'tied to plans' approach reflects the substantial commitment to a creative design that is a contextual fit, and ensures a high level of certainty for all stakeholders.

Urban Design

The concurrent development permit application (DP2018-0390) was reviewed by The City's Urban Design Review Panel (UDRP) even though the scope of the application does not trigger a review by UDRP. Administration's City Wide Urban Design team also provided comments on the application.

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

The UDRP comments (Attachment 6) were strongly supportive of the proposed development, citing how the innovative proposal added a new residential typology and greater diversity to Bridgeland-Riverside while offering well-designed street frontages that enhance the adjacent avenue and street. The review also encouraged further study of the exteriors of both the third floor level and garage to further blend in the massing with the surrounding context. This informed the final revisions to the development, as found in Schedule C of the proposed DC Direct Control District (Attachment 3) and the comprehensive overview of the project (Attachment 9). The applicant's response to UDRP's comments is also included under Attachment 7.

Infrastructure

Transportation Networks

The subject site is within a two minute walk (approximately 200 metres) from transit stops for Routes 90 along 1 Avenue NE, and a six minute walk (approximately 500 metres) from transit stop for Route 5 along Edmonton Trail. The Bridgeland - Memorial C-Train Station is also within an eleven minute walk (approximately 950 metres).

Vehicular access is available from the existing rear lane to the east. This provides access to the proposed attached garage providing the four unit development with four residential stalls. No visitor stalls are proposed on site. A traffic impact assessment was not required as part of this application or the associated development permit application.

Utilities and Servicing

Water, sanitary and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. Individual servicing connections as well as appropriate stormwater management have been considered and reviewed through the concurrent development permit process as part of DP2018-0390 and found to be satisfactory.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, the date of the Public Hearing will be advertised.

The Bridgeland-Riverside Community Association was circulated as part of this application. A letter was submitted (Attachment 4) indicating that they do not support the redesignation for the following summarised reasons:

- the subject site and its surrounding area just benefited from a Main Streets initiative that included an intensive public engagement resulting in the current city initiated land use

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

redesignation, which is appropriately scaled. It is inappropriate for an applicant to be circumventing the City's recent planning process under Main Streets;

- while it is appreciated that the zoning application is tied to the development, the proposed has excessive lot coverage and the Community Association fears that the applicant can still sell the site with this up-zoning;
- the subject site should have a low scale development that transitions best to the low rise R-C2 District area north of 2 Avenue; and
- the Community Association also included that they would support a contextually sensitive R-CG development but not an extension beyond that.

Administration received six letters from adjacent landowners in response to the circulation and notice posting of the site. One of these letters was in support, while the other five were in opposition.

The opposition letters expressed concerns with the following:

Land Use-related comments:

- the proposed land use is not reflective of the Main Street initiative that redesignated the first block north of 1 Ave to allow three units on a site;
- there are sufficient alternative locations for high density living in Bridgeland, that can accommodate the multi-generational living proposal the applicant is advocating through the subject development;
- the proposed density increase is not supported; and
- concern that the increased density to four units will trigger servicing upgrades that will burden the existing community.

Development Permit-related comments:

- the current neighbours live there because they were drawn to the single family dwellings and lifestyle it entails;
- the proposed does not contextually fit with the neighbourhood's smaller homes of historic character;
- proposed massing, privacy, parking, and waste management are also issues;
- the proposed will adversely impact access to sunrise view for surrounding development;
- the proposed appears to be pushing the limits on multiple levels to maximize the developer's profits;
- the proposed negatively impacts the parcel immediately north through shadowing and the loss of privacy caused by direct sight lines into their home from the units oriented along the avenue;
- missed opportunity because the developer is not making use of the character home retention incentive District; and

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

- the lane is not able to accommodate the four separate car accesses and required number of waste and recycling bins. This will also impact the usability of the lane for adjacent parcels.

-

Parking-related comments:

- online registration for visitor parking leads to a lack of ability to determine who is parking illegally, thereby being unable to report illegal parking to Calgary Parking Authority;
- approving four units will lead to an adverse impact on on-street parking availability;
- parking is already an issue at this location due to the adverse impact from the adjacent church; and
- a visitor stall is not provided, leading to an adverse impact on on-street parking availability if the development is approved.

Policy-related comments:

- the current ARP does not support this development; and
- the proposed should wait for the new ARP work which will provide an overall vision for the area that reflects what the community wants rather than allowing for this spot zoning outside of that process.

The one letter of support expressed the following:

- the proposed three storey and four unit development is appropriate and sensitive to the surrounding context;
- shadowing is not an issue to the subject parcel's adjacent dwelling since it is located south of the subject site; and
- the proposed will bring in more residents to the community which is good in supporting neighbourhood businesses and providing more eyes on the street while supporting the MDP sustainability goals.

Administration considered the relevant planning issues specific to the proposed redesignation and has determined the final proposal, as presented, to be appropriate.

Engagement

On 2018 March 05, the applicant met with the Bridgeland-Riverside Community Association to present the details of this concurrent land use amendment and development permit application. The meeting was attended by the Community Association's Planning Committee members along with a few community residents. Members of Administration attended to observe the presentation and provide any process-related clarifications.

Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at 230 - 7A Street NE, LOC2018-0021

Following the meeting, on 2018 March 26, the applicant held a public information session at the Bridgeland-Riverside Community Association. The applicant used a postcard maildrop to notify neighbouring residents of the sessions, while a local community interest group further advertised the session on social media. Approximately 25 residents attended the session.

Members of Administration were present to provide clarification on the proposal and process-related questions. The key themes that emerged from the information session largely echoed those documented in the letters from adjacent land owners. A detailed applicant-provided overview is available in Attachment 5.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The subject site falls within the 'Developed – Residential – Inner City' area as identified on Map 1 of the *Municipal Development Plan (MDP)*. The applicable MDP policies encourage redevelopment of inner city communities that is similar in scale and built form to existing development. The policies also support revitalization by adding population particularly in transition zones adjacent to higher density areas such as the adjacent 1 Ave classified under Map 1 as a Neighbourhood Main Street. The MDP also calls for a modest intensification of the inner city; an area serviced by existing infrastructure, public amenities and transit.

The proposal is in keeping with relevant MDP policies as the proposed four unit multi-residential development provides for development form that is sensitive to existing low density residential development in terms of height, built form and density. The development also maximises front door access to the street encouraging pedestrian activity.

Bridgeland-Riverside Area Redevelopment Plan (Statutory, 1980)

The subject parcel is located within the 'Neighbourhood Limited' area on Figure 3- Generalized Land Use of the *Bridgeland-Riverside Area Redevelopment Plan (ARP)*. The Neighbourhood Limited area is intended to accommodate grade-oriented development including single, semi-detached, duplex, cottage cluster and rowhouse housing. This range of development form is provided by the Residential – Grade-Oriented Infill (R-CG) District. While the proposed DC District is based on the R-CG District, the addition of Permitted Development as a permitted use triggers the requirement for a minor amendment to the *Bridgeland-Riverside ARP* because it is allowing for a multi residential development not otherwise listed under the Neighbourhood Limited area. The text of the proposed amendment to Residential policy 5 is included in Attachment 2.

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

The proposed ARP amendment is appropriate given that the Permitted Development use contained within the proposed DC District establishes a building form that is contextual with the surrounding residential development in the area.

As discussed in the Background section of this report, a new ARP is in the process of being created by The City in consultation with the Bridgeland-Riverside Community Advisory Group and the greater community. It is anticipated that the ARP will be presented to Council by Winter 2018/Spring 2019. The new ARP will take into consideration any ARP amendments that are adopted by Council up to that point.

This proposed land use district is believed to be an effective approach that will enable redevelopment of this site without presupposing the planning outcomes or Council decision on the comprehensive land use and policy planning work being carried out as part of the new ARP for the area. This proposal reflects a balanced solution between providing respect for the existing character of the area, while also meeting MDP intensification policies which encourage sensitive redevelopment of inner-city communities. Sensitive redevelopment is manifested in the final development's design that responds to the community, Administration's and UDRP's concerns, while supporting high-quality design. Given the unique and specialised nature of this proposal, Administration therefore does not anticipate that this application will set precedent or pre-determine the future land use pattern within this existing low density residential area and/or the outcome of the new ARP. The prescriptive nature of the proposed district, will result in future development of the site to occur in a manner that is compatible to both the existing area context and/or any future development and building typologies that may be allowable in the area subject to Council's approval of the new ARP.

It is for these above reasons that Administration is not in support of a more conventional up-zoning of this parcel to a standard multi-residential land use district to allow for intensification.

Location Criteria for Multi-Residential Infill (Non-statutory, 2014)

The subject site aligns with six of the eight criteria for ideal placement of multi-residential infill.

The guidelines are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP or local area policy plans, to assist in determining the appropriateness of an application in the local context.

Though the subject parcel is not directly adjacent to an amenity or located on a higher standard roadway, location criteria such as being located on a corner adjacent to a non-residential development is met. Furthermore, it is located within 200 metres from the nearest transit stop and in very close proximity to 1 Avenue NE, which is which is classified as a Neighbourhood Main Street.

Social, Environmental, Economic (External)

The recommended land use allows for wider range of housing types than the existing land use would, and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics. It would also support The City's Age-Friendly

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

Policy, particularly in accommodating individuals interested in aging-in place with elderly care readily available amongst families living within the same neighbourhood.

An environmental site assessment was not required for this application.

Financial Capacity

Current and Future Operating Budget:

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget:

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

The intent of this land use approach is to allow for a specific development outcome on this property as contained in Schedule C of the proposed DC Direct Control District. If significant building design changes occur between Council's potential approval of this land use amendment and a future building permit application, it would render the concurrent development permit to be non-compliant with this DC Direct Control District.

Historically, land use amendment applications that were 'tied to plans' provided little to no flexibility with respect to development outcomes. However, with this land use proposal, if the contemplated development is not realised in the future, the proposed DC Direct Control District is designed to also allow for all of the other uses allowed under the existing base R-CG District.

Additionally, while the proposed minor amendment to the *Bridgeland-Riverside Area Redevelopment Plan* (ARP) is reviewed as appropriate and in general alignment to applicable policies as discussed in the Strategic Alignment of this report, there is a small risk of misalignment with the direction of the new ARP work currently undergoing.

This proposed land use district is believed to be an effective approach that will enable redevelopment of the site without pre-determining the future land use pattern within the existing low density residential area or the outcome of the new ARP.

**Planning & Development Report to
Calgary Planning Commission
2018 July 26**

**ISC: UNRESTRICTED
CPC2018-0840
Page 12 of 12**

**Policy Amendment and Land Use Amendment in Bridgeland-Riverside (Ward 9) at
230 - 7A Street NE, LOC2018-0021**

REASONS FOR RECOMMENDATIONS:

Administration recommends approval of this application as it has the potential to enable development in accordance with *Municipal Development* Plan policies that call for modest intensification of the inner city in a redevelopment scale that is similar to the surrounding built environment.

Development following from this application has the potential to allow more Calgarians the freedom to choose to live, work, and meet their day-to-day needs in a location well served by existing infrastructure, services, and employment.

ATTACHMENTS

1. Applicant's Submission
2. Proposed Amendment to the Bridgeland-Riverside Area Redevelopment Plan
3. Proposed DC Direct Control Guidelines
4. Bridgeland-Riverside Community Association Letter
5. Applicant's Engagement Overview
6. Urban Design Review Panel Comments
7. Applicant's Response to Urban Design Review Panel Comments
8. Main Streets DC Direct Control District
9. Comprehensive Project Overview

Applicant's Submission

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Calgary | Alberta | Canada

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F 403 228 1320
E info@o2design.com



Applicant's Submission

O2 Planning + Design is proposing this land use amendment to redesignate the parcel located at 230 7A Street NE in the community of Bridgeland from Direct Control (273D2017) to Direct Control District tied to plans for a specific project as proposed in a concurrent development permit. This redesignation will allow for the development of a 4-unit family focused townhouse project on the parcel.

The request for a change of land use for this parcel is based on a proposed new building as submitted in a concurrent development permit. The proposed building has been designed as a specific response to the site with careful consideration of the neighbourhood context and character and has been designed to provide the following benefits over a development designed to meet the rules of the existing R-C2 land use district:

- The corner parcel provides an opportunity to orient the building to two streets with two doors facing each street. This represents an improvement over an R-C2 development that would have a side yard facing the avenue, resulting in no street activation.
- The provision of a front patios facing both 7a Street and 2 Avenue provide further activation of the streetscapes by providing outdoor street-level gathering spaces where residents of the units can interact with other neighbourhood residents. The typical relationship between homes on 7a street and the street is the presence of a retaining wall and large front lawn that disconnects the home is from the streetscapes.
- The development has been designed in response to the unique conditions of the site. Specific attention has been made to respect the scale and character of the neighbourhood.
- The design of the development provides a variety of unit sizes which offer choice to potential buyers of various incomes and socio-economic demographics along with inter-generational living opportunities. A typical R-C2 parcel in Bridgeland is redeveloped into one or two luxury homes attainable by a single demographic.
- The parcel's location on the north end of the block results in minimal shadowing impact on adjacent properties.
- The development contributes to meeting the objectives of the Municipal Development Plan goal of intensifying inner-city neighbourhoods. The proposed land use permits a single unit more than the current land use permits.
- Located within walking distance of the shops and services of the 1st Avenue main street, the development will offer the opportunity for its residents to live a more walkable, less auto-reliant lifestyle.

Proposed Amendment to the Bridgeland-Riverside Area Redevelopment Plan

1. The Bridgeland-Riverside Area Redevelopment Plan attached to and forming part of Bylaw 11P80, as amended, is hereby further amended as follows:
 - (a) In Part 2, under Section 3 entitled "Residential", Policy 5, following bullet point "b" add "c) include multi-residential development for the land located at 230 - 7A Street NE."

Proposed Direct Control Guidelines

Purpose

- 1 This Direct Control District is intended to provide for a multi- residential development as a **permitted use** where it conforms to the plans in Schedule “C”.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Defined Uses

- 4 In this Direct Control District:

- (a) **“Permitted Development”** means a **use**:
- (i) that consists of one **main residential building** containing four **units**;
 - (ii) that has **motor vehicle parking stalls**; and
 - (iii) that conforms with the plans attached to this Direct Control District as Schedule “C”.

Permitted Uses

- 5 (1) The **permitted uses** of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.
- (2) The following **uses** are the **permitted uses** in this Direct Control District provided the **development** conforms with the plans attached as Schedule “C” of this Direct Control District:
- (a) **Permitted Development.**

Discretionary Uses

- 6 The **discretionary uses** of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District.

Bylaw 1P2007 District Rules

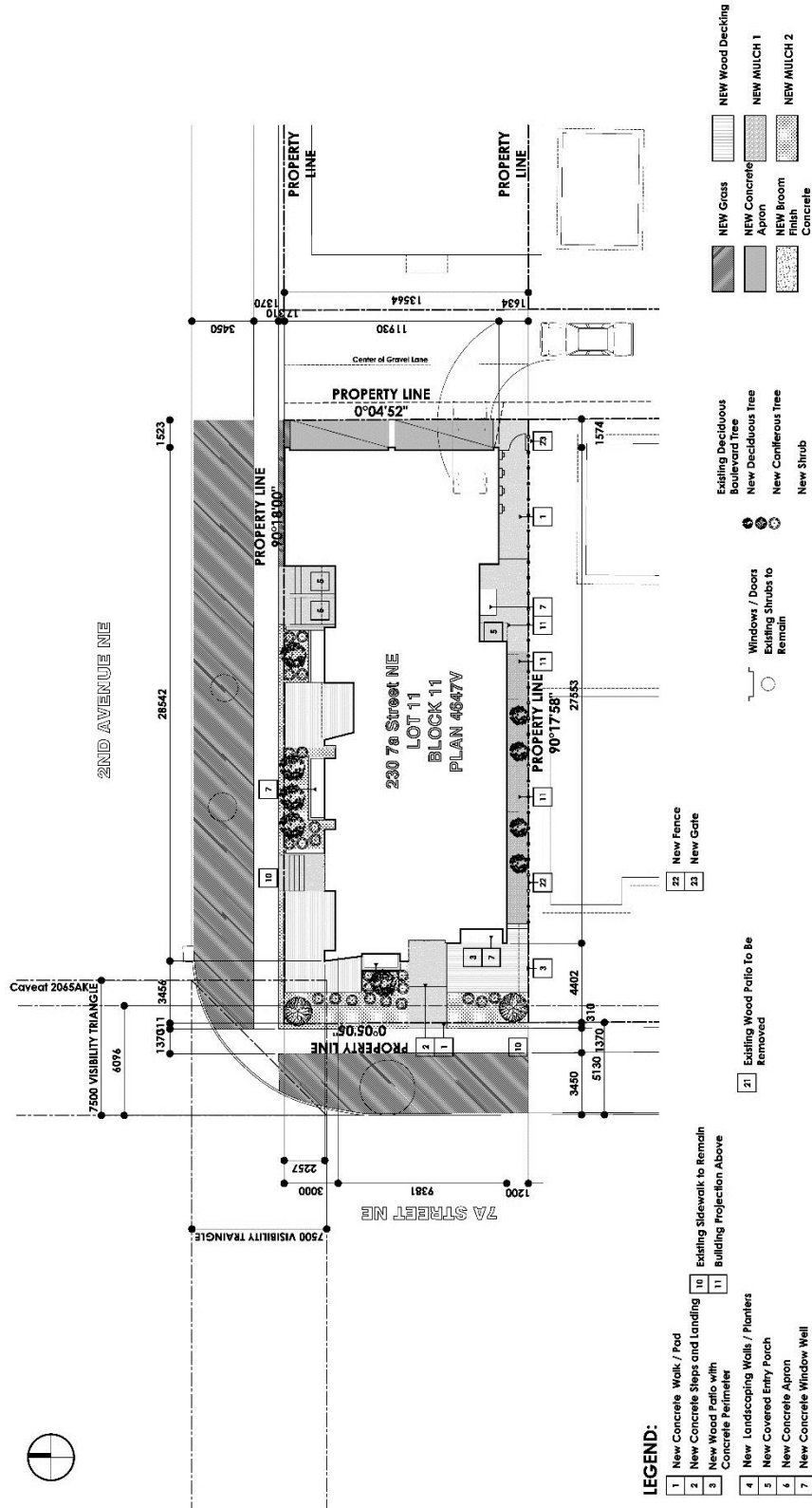
- 7 Unless otherwise specified, the rules of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 apply in this Direct Control District.

Development Plans for Permitted Development

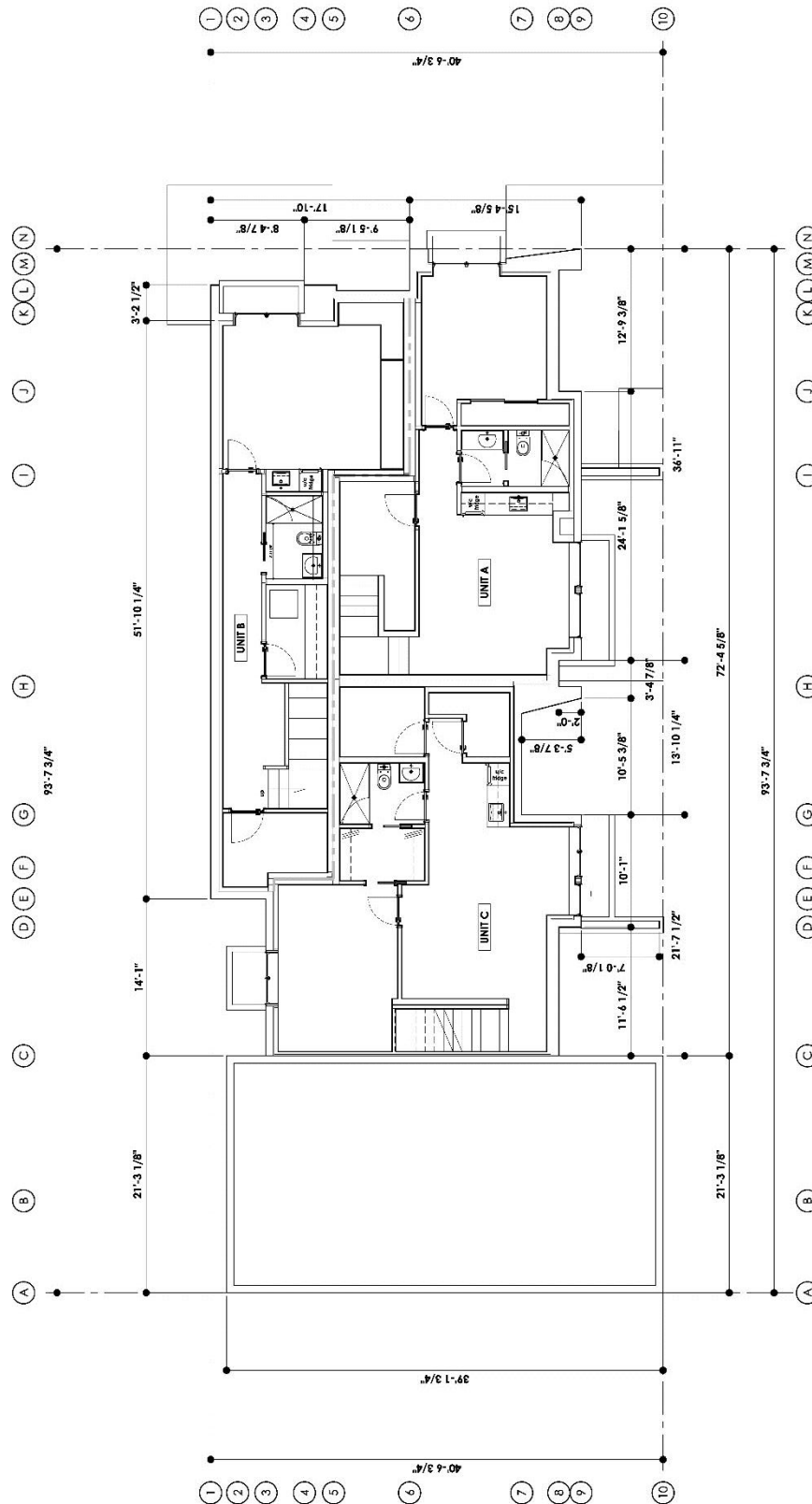
- 8 Comprehensive plans must be submitted to the **Development Authority** as part of a **development permit** application for **Permitted Development**. In considering such an application, the **Development Authority** must ensure the **development** plans conform with the plans attached to this Direct Control District as Schedule “C”.

Proposed Direct Control Guidelines

Schedule C

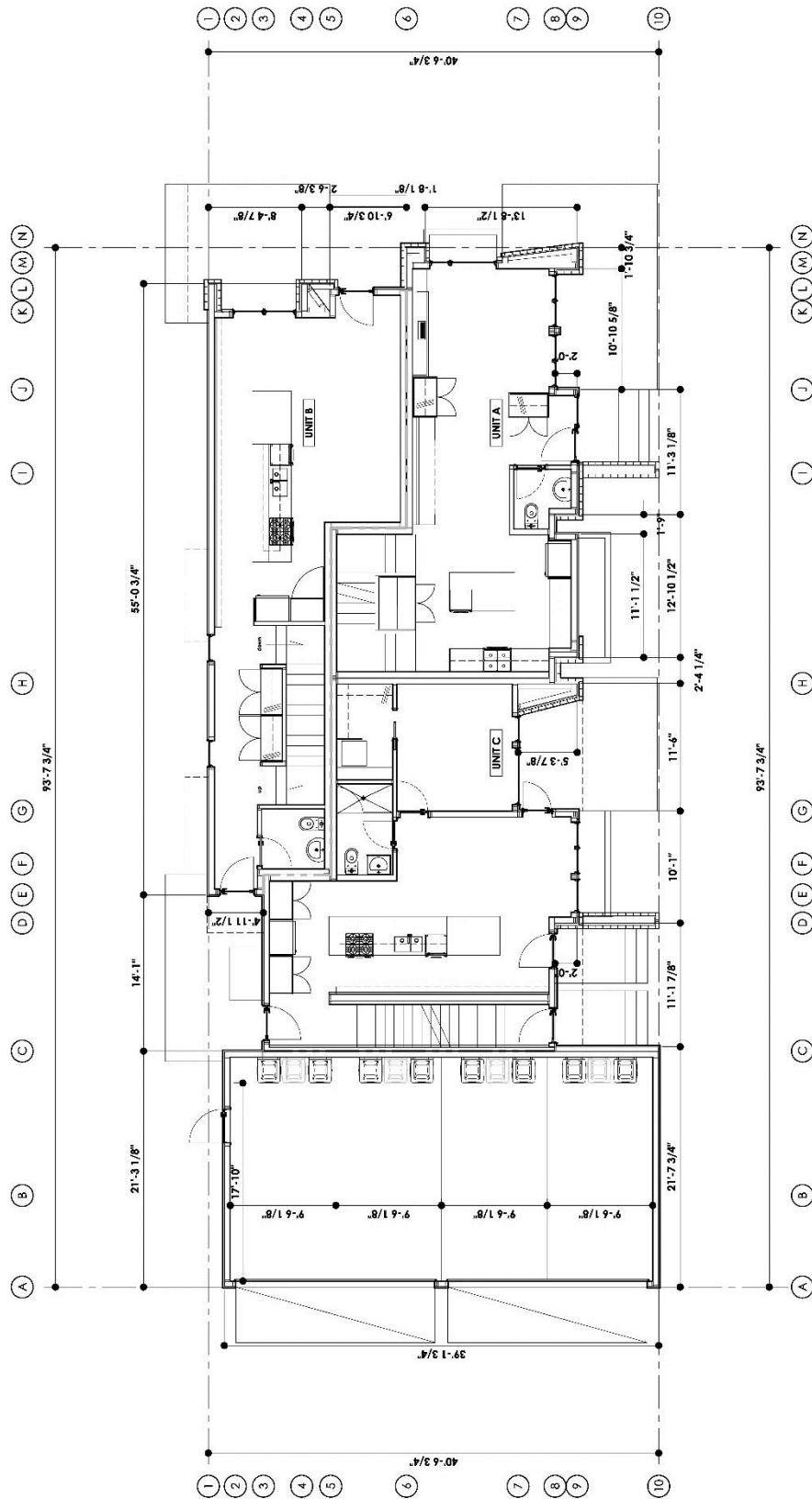


Proposed Direct Control Guidelines



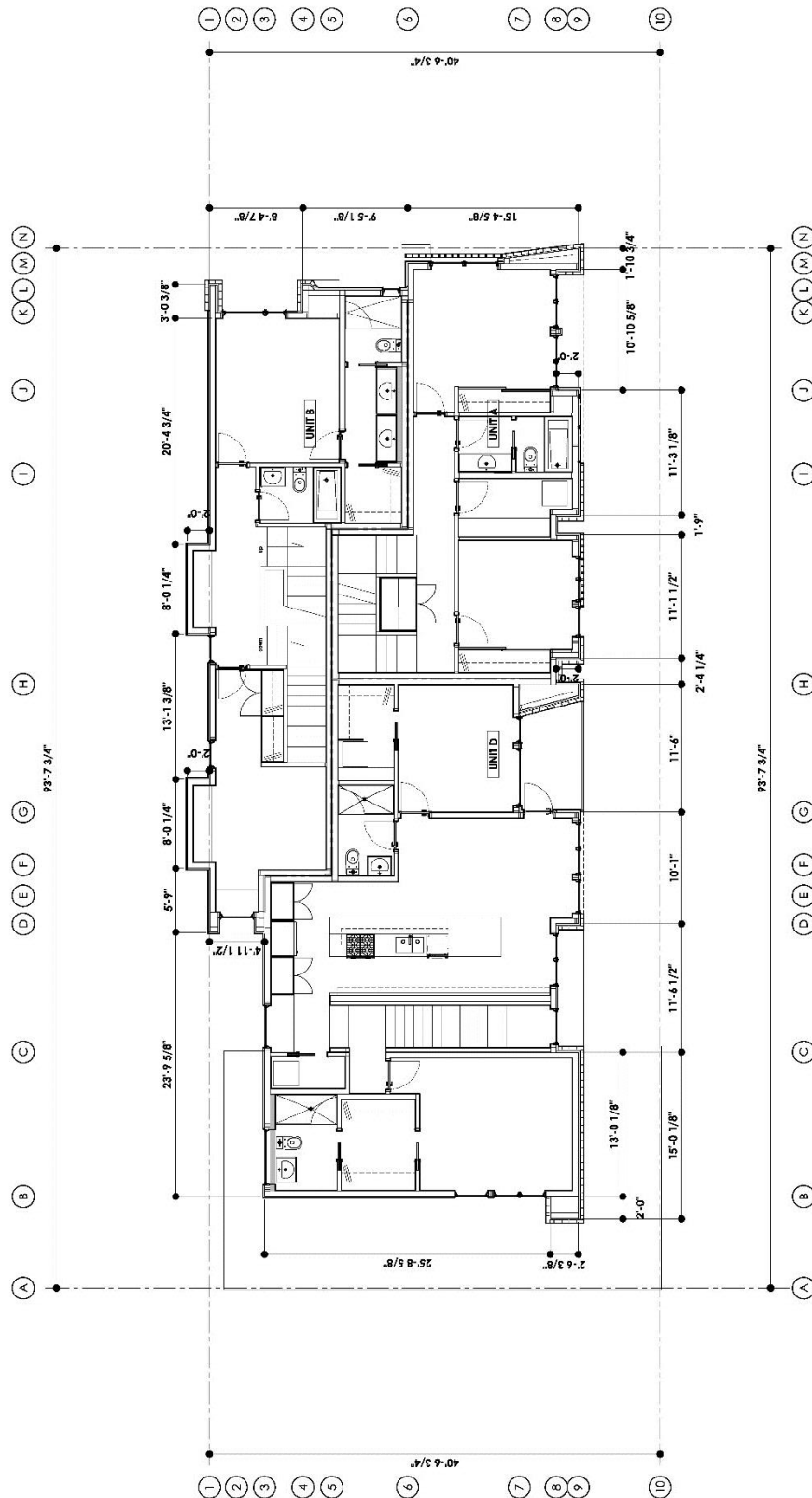
BASEMENT FLOOR PLAN

Proposed Direct Control Guidelines



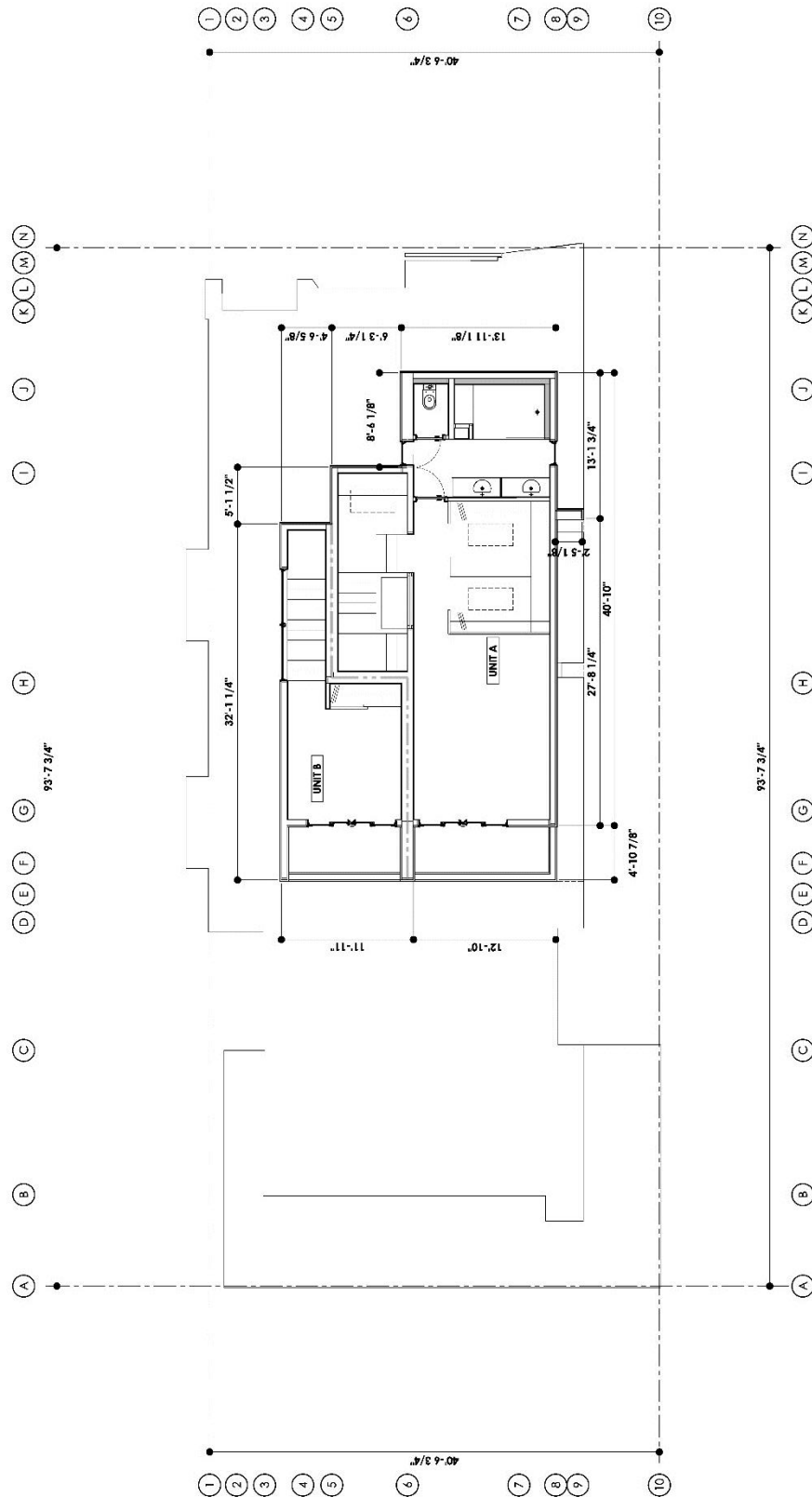
MAIN FLOOR PLAN

Proposed Direct Control Guidelines

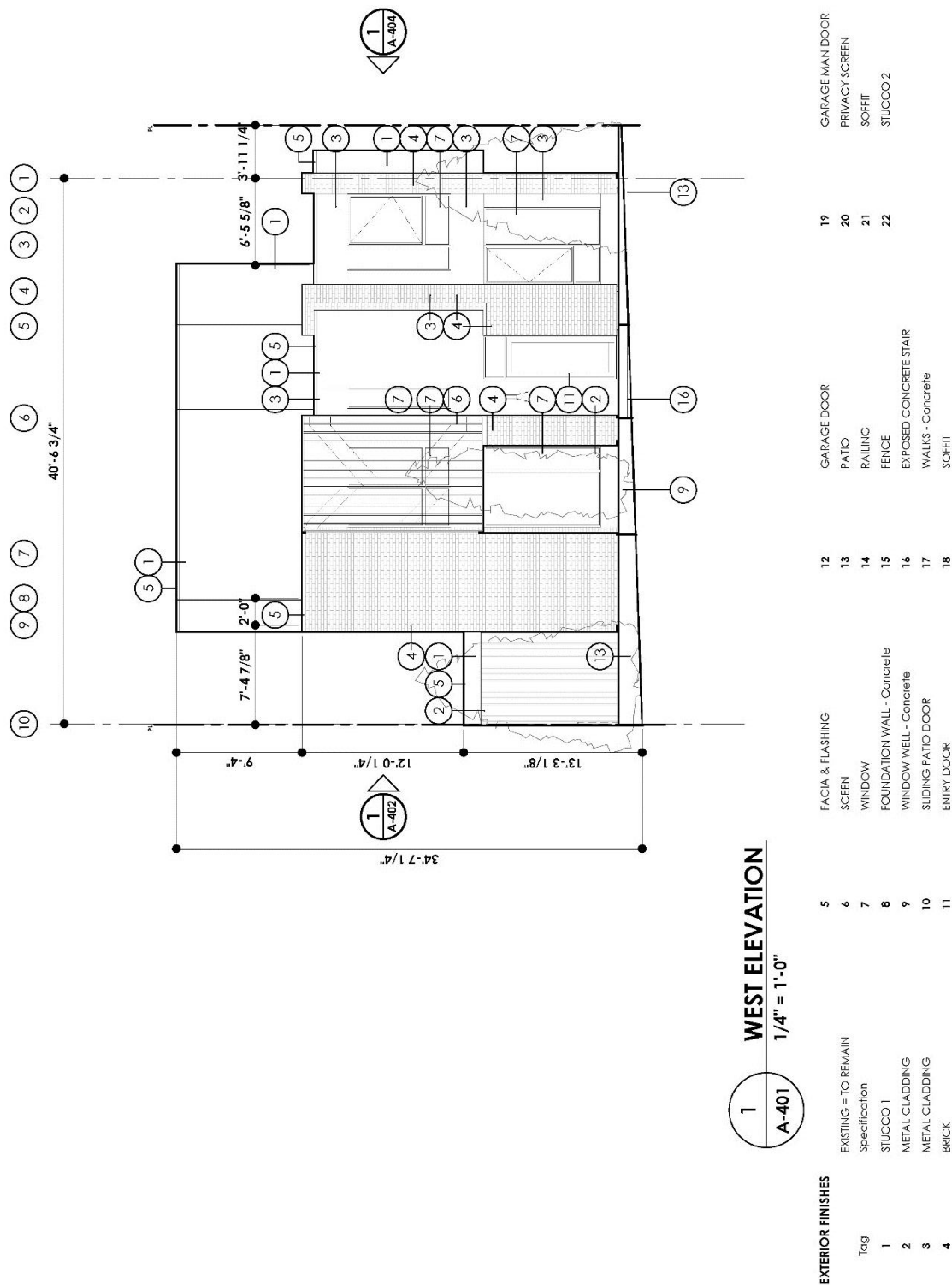


SECOND FLOOR PLAN

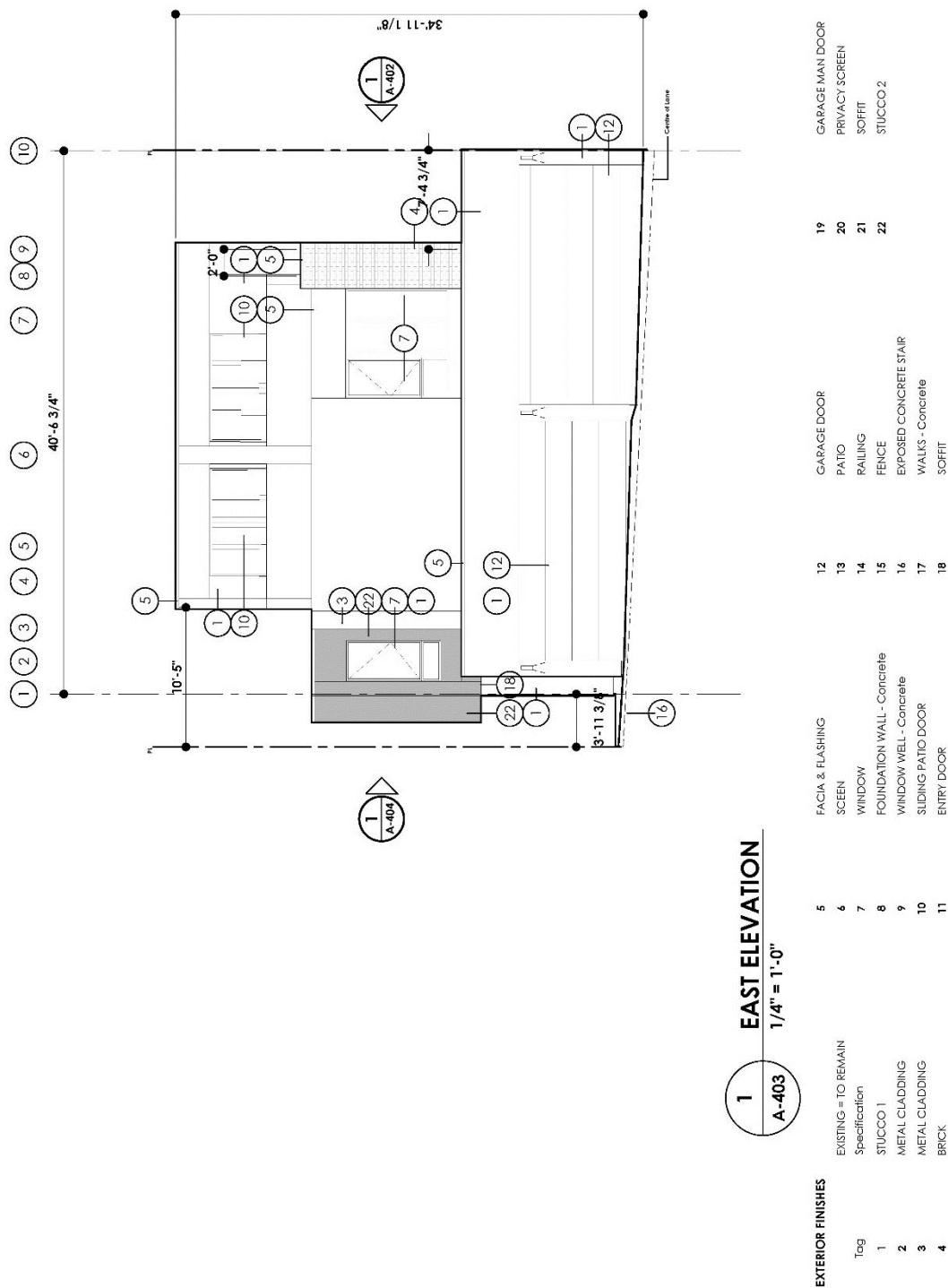
Proposed Direct Control Guidelines



Proposed Direct Control Guidelines



Proposed Direct Control Guidelines



Proposed Direct Control Guidelines



Bridgeland-Riverside Community Association Letter



Planning Committee
917 Centre Avenue NE Calgary AB T2E0C6
brcacalgary.org

19 March 2018

Circulation Control
Planning, Development & Assessment #8201
The City of Calgary
PO Box 2100 Station M
Calgary AB T2P2M5

Attn: CPAG.Circ@calgary.ca
cc: Sara Kassa, File Manager (sara.kassa@calgary.ca)
Ali McMillan, BRCA Planning Director (planning@brcacalgary.org)

To Whom It May Concern:

RE: LOC2018-0021 (230 7A NE)

Thank you for the opportunity to comment with respect to the application for a Land Use Amendment affecting land at 230 7A Street NE (LOC2018-0021).

This Land Use Application was discussed at a meeting of our Planning Committee convened 5 March 2018. Notice of that meeting was given to neighbours adjacent to the subject parcel. Approximately 5 neighbours attended, as did many regular Planning Committee members. The applicant was also invited to the meeting, and did attend.

The comments below regarding the LOC application reflect the feedback of both neighbours and the BRCA Planning Committee members present at the meeting.

The application is seeking a new land use designation for the site of DC (based on M-CG zoning) instead of the existing R-CG allowed. We are opposed to such redesignation by means of this application for several reasons:

- a. The Bridgeland-Riverside community benefited from an updating of a portion of our Area Redevelopment Plan (ARP) through the Main Streets Initiative. The area in question was studied as part of a broader plan—with Council guidance, intensive policy consultation, and public engagement—and decisions were reached to define appropriately scaled zoning. We feel that the existing Direct Control zoning as applied to this very location by the City only just within the last calendar year remains entirely appropriate at this time, and that for the Applicant to be requesting a land use change to further increase density beyond that which has just recently been re-zoned is both

Bridgeland-Riverside Community Association Letter

inappropriate generally and also wasteful in resource terms, considering the investments made into the extremely costly City of Calgary Main Streets policy undertaking.

- b. We can appreciate that the zoning application is tied to development permit plans through this DC. There is the argument that the Applicant can build only "exactly this." However, in addition the previous argument which cuts at the heart of this argument, there are also complications with the approach. In our view there is excessive lot coverage contemplated by the tied application, there are concerns about the ability of the Applicant (like any other DC applicant) to sell the up-zoned site, and, as a result, there is the potential for future DP revisions to accommodate unforeseen changes to the building or other future circumstances.
- c. The site of the application is on the farthest North edge of the recently City-initiated up-zoned region. As such, this specific lot is located geographically at a district edge where presumably scale in the area should be at its lowest, to best allow for transition in consideration of the R-C2 properties immediately to the north.

The density, lot coverage, height and setbacks being sought on this parcel exceed parameters only very recently defined and implemented via City-initiated rezoning. It seems a major stretch for any applicant to be looking already to find ways to circumvent the City's own planning rationale. In view of that rationale, we would certainly support a contextually sensitive R-CG development on this site but do not support extension beyond that.

Sincerely,

BRIDGELAND-RIVERSIDE COMMUNITY ASSOCIATION

Per: BRCA Board of Directors
Planning Committee

Applicant Provided Engagement Overview

O2 Planning + Design Inc.
510 255 17th Ave SW T2S 2T8
Calgary | Alberta | Canada

T 403 228 1336
F 403 228 1320
E info@o2design.com



438 8 St & 230 7A Street Engagement Summary

Below is a summary of the community engagement activities that O2 Planning + Design and Davignon Martin Architecture completed during the application process.

March 5, 2018 Meeting with BRCA Planning Committee

O2 Planning + Design, Davignon Martin Architecture and the landowner/developer met with the BRCA Planning Committee to present the details of the land use application and the proposed design of the two projects.

The BRCA committee expressed general support of the buildings design but were concerned with the application moving ahead of the ARP revision and that the density, height and massing does not fit with the neighbourhood context. It was agreed with the BRCA committee that a public open house should be held to allow Bridgeland residents have the opportunity be informed about the project.

March 26, 2018 Public Information Session

Residents from the community of Bridgeland-Riverside were invited to a community information session hosted at the Bridgeland-Riverside Community Association Hall. The purpose of this community information session was to provide information and to solicit community feedback on the proposed developments.

The community information session was attended by approximately 25 residents. The format of this information session was an informal discussion with the project's planners, architects, and land owners. Residents had the opportunity to provide feedback using comment forms and sticky notes provided by the applicant. There were 50 comments received and have been categorized into the following themes:

- **Traffic, Parking & City Servicing. 7 Comments of Concern**
Residents expressed concern that the development would lead to increase in traffic, more on-street parking and over burden City services.

Response: The proposed development will include one additional unit than what is permitted under the existing land use will create minimal additional impact to traffic, parking and servicing.
- **Bridgeland-Riverside ARP Alignment: 12 Comments of Concern**
Residents expressed concern that proposed land use amendment application was being made in advance of the ARP update.

Response: Given the delays in progressing the ARP update, the land owner wanted to move advance with the land use and development permit application to meet their development timelines. Waiting for the update to the ARP would cause significant delays in the schedule.

Applicant Provided Engagement Overview

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- **Massing and Height: 9 comments of concern, 1 comment of support**

Residents expressed concern that the development was out of scale in relation to the neighbourhood. The comment of support expressed that the proposed massing and height fits with the context related to the scale of new-build homes in Bridgeland.

Response: Although the scale of the proposed homes is larger than the older homes in the area, the massing and height of the project is very similar to the allowed height and massing of new build single-family and semi-detached homes being built in Bridgeland.

- **Building Design: 2 Comments of Concern, 3 Comments of support**

While some residents were not supportive of the architectural design, three specific comments were received praising the design of the homes.

Response: Architectural design is subjective, but great attention has been made to ensure that the design is responsive to the community context and enhances the public realm of the neighbourhood.

- **Adjacent Privacy, Lighting & Green Space: 3 Comments of Concern**

Residents expressed concern of the loss of privacy, lighting and green space.

Response: The impact of privacy, lighting and green space would be similar with the development of homes allowed under the existing zoning.

Urban Design Review Panel comments

Urban Design Review Panel Comments

Date: June 13, 2018
Time: 1:00 pm
Panel Members:
Present: Chad Russill (co-chair)
Gary Mundy
Jack Vanstone
Amelia Hollingshurst
Ben Bailey
Absent: Janice Liebe (chair)
Glen Pardoe
Robert LeBlond
Terry Klassen
Yogeshwar Navagrah
Bruce Nelligan
Eric Toker

Advisor: David Down, Chief Urban Designer
Application number: **LOC2018-0021/DP2018-0390,**
LOC2017-0127/DP2017-1699
Municipal address: 230 7A ST NE, 438 8 ST NE
Community: Bridgeland
Project description: Multi-Residential Development (1 building, 4 units.)
Multi-Residential Development (1 building, 4 units.)
Review: first
File Manager: Sara Kassa
City Wide Urban Design: Angela Kiu
Applicant: 02 Planning and Design
Architect: Davignon Martineau Architects
Owner:
Ranking: **Support**

Summary

The Panel recognizes that this development does not require UDRP review and feedback, however commends the applicant for voluntarily seeking input on this innovative proposal for a new residential typology within the neighbourhood of Bridgeland.

Overall, the Panel is excited by the applicant's desire to address a corner residential lot differently than an infill lot and activate both the avenue and street facades through the residential unit configurations and the exterior design. The Panel also commends the intent of the design to provide more variety within this community with regards to scale and cost, ultimately supporting a diverse community fabric and demographic.

The Panel encourages further study of the contrasting expression of the third level and garage exterior treatment. Although it is understood that the current proposal attempts to accentuate the two-storey massing of the building, the graphic illustrations seem to highlight both the third level and garage, as oppose to diminishing their presence.

Although this application does not align exactly with the current ASP for this community, the Panel strongly **supports** this application and the change in zoning to Direct Control for both sites.

Applicant Response

(date)

Urban Design Review Panel comments

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	N/A
	UDRP Commentary		
	Not Applicable.		
	Applicant Response		
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	N/A
	UDRP Commentary		
	Not Applicable.		
	Applicant Response		
3	Pedestrian-first design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	Support
	UDRP Commentary		
	The existing public sidewalk is maintained along the street and avenue. Access to residential units are connected to the public sidewalk through hard scaping. A rear parking garage is provided for residents with concrete pads connecting the garage to the rear lane. Private patios and landscaping define the edge condition between the public and private realm.		
	Applicant response		
4	Entry definition / legibility	Entry points are clear and legible	Support
	UDRP Commentary		
	Entry to each of the private residential units is clear and legible. The proposed development activates both street and avenue through meaningful points of entry.		
	Applicant Response		
5	Residential multi-level units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Support
	UDRP Commentary		
	The design proposes two and three storey units at the street level, facing both the street and avenue at this corner site condition. Private outdoor patios, at grade, provide opportunity for furniture and extend towards the sidewalk. They offer physical and visual access to the public sidewalk. Second and third level balconies also contribute to animating the façades.		
	Applicant Response		
6	At grade parking	At grade parking is concealed behind building frontages along public streets.	Support with comment
	UDRP Commentary		
	A private, enclosed garage, is provided for four vehicles (one per residential unit). Street parallel parking is by permit only and a C-train station is located within 10 minutes walking distance. The panel feels this is more than adequate. Designated 'Car to Go' parking locations near-by may assist the applicant in negotiating Community support, as it could minimize Community concerns around vehicle volumes. Some exploration of pushing the garage away from the property line is encouraged as it could allow for some landscape to buffer this portion of the building.		

Urban Design Review Panel comments

Applicant Response			
7	Parking entrances	Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.	Support
UDRP Commentary			
There are no ramps in this application. The parking entrance is in keeping with the existing context and all other adjacent residential properties; via the rear lane. The panel supports reducing the garage parking capacity from four stalls to three stalls. This will improve the exterior form/massing along the avenue façade by reducing the current bump-out of the garage. Further reducing the private parking capacity will support the use of public transit and increase pedestrian activity.			
Applicant Response			
8	Other		
Applicant Response			
Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i>			
Topic		Best Practice	Ranking
9	LRT station connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	N/A
UDRP Commentary			
Beyond the property lines of this site, a LRT Station is located within a 10 minute walk.			
Applicant Response			
10	Regional pathway connections	Supports walkability via intentional urban design connections to pathway systems.	N/A
UDRP Commentary			
Applicant Response			
11	Cycle path connections	Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.	N/A
UDRP Commentary			
Applicant Response			
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	N/A
UDRP Commentary			
Applicant Response			
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.	Support
UDRP Commentary			

Urban Design Review Panel comments

	The design provides an exterior walkway along the side setback through the site, connecting back entryways to the garage, laneway and front street. Suggest the addition of gates to discourage public access and enhance CPTED principles.		
	Applicant Response		
14	Open space networks and park systems	Connects and extends existing systems and patterns.	N/A
	UDRP Commentary		
	Applicant Response		
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.	
	UDRP Commentary		
	Applicant Response		
16	Vehicular interface		
	UDRP Commentary		
	Applicant Response		
17	Other		
	Applicant Response		
	Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i>		
	Topic	Best Practice	Ranking
18	Massing relationship to context	Relationship to adjacent properties is sympathetic	Support
	UDRP Commentary		
	Although the proposed massing deviates from the existing residential context by proposing a third story, the design is sensitive to the adjacent two storey structures by stepping back the proposed third storey and articulating the building massing on the shared side-yard elevation. The street and avenue facades project slightly into the required setback, however the panel feels that the projection on the street façade is minimal and as a corner lot, this projection does not diminish the overall street perspective or negatively impact the adjacent property. Visibility triangles for vehicles is not affected. The panel does feel that retracting the garage façade along the avenue would have a positive impact on the avenue perspective and elevation, allowing sufficient space for a landscaping feature to soften the blank wall façade. Overall, the massing addresses both the street and avenue, which is a positive urban design gesture.		
	Applicant Response		
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites	Support
	UDRP Commentary		
	Although the proposed massing deviates from the existing residential context, that being a main residence with a detached garage and a backyard space, the proposed massing is located on north corner sites, and therefore does not introduce shadows onto adjacent backyards.		
	Applicant Response		

Urban Design Review Panel comments

20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
UDRP Commentary			
See comment #18. This application is innovative in that it is addressing both streets through form and layout. As a corner lot it fronts two streets and the design addresses both streets differently, relating to the attitude of 8 th Street through a duplex massing, and initiating an attitude for the avenue through a townhouse massing. This solution is in fact less than what could be built within the current zoning, further strengthening the rationale behind support of this project.			
Applicant Response			
21	Massing distribution on site		Support
UDRP Commentary			
The panel commends the applicant's intent to provide affordable density and improve the quality of space within the residential units. The massing occupies the entire site, while achieving natural light within each unit and maintaining visual privacy for existing adjacent properties.			
Applicant Response			
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Support
UDRP Commentary			
See comment #5			
Applicant Response			
23	Other		
Applicant Response			
Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i>			
	Topic	Best Practice	Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Support
UDRP Commentary			
The design provides lighting and glazing along both the street and avenue, which is an improvement within the neighbourhood. This improvement lends much needed 'eyes on the street' for the avenue, where laneway access can provide hiding areas. All unit entry doors are visible and provide full glazing. Also see comment # 13.			
Applicant Response			
25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	N/A
UDRP Commentary			
Applicant Response			
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.	N/A

Urban Design Review Panel comments

UDRP Commentary		
Although this was not discussed, the applicant may wish to identify the snow shoveling procedures to the Community Association to address any sidewalk maintenance concerns.		
Applicant Response		
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.
		Support
UDRP Commentary		
All residential unit entries have canopies to protect against weather conditions and improve the safety of landings.		
Applicant Response		
28	Night time design	N/A
UDRP Commentary		
Applicant Response		
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.
		Support
UDRP Commentary		
One of the proposed units is single storey and accommodations could be made to ramp the main entry walkway. This could support the design intent of supporting a diverse community both generationally and economically.		
Applicant Response		
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.
		N/A
UDRP Commentary		
Applicant Response		
31	Other	
Applicant Response		
Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i>		
Topic		Commentary
Ranking		
32	(specify)	TBD

Applicant Response to Urban Design Review Panel Comments

Urban Design Review Panel Comments

Date:	June 13, 2018	
Time:	1:00 pm	
Panel Members:	Present: Chad Russill (co-chair) Gary Mundy Jack Vanstone Amelia Hollingshurst Ben Bailey	Absent: Janice Liebe (chair) Glen Pardoe Robert LeBlond Terry Klassen Yogeshwar Navagrah Bruce Nelligan Eric Toker
Advisor:	David Down, Chief Urban Designer	
Application number:	LOC2018-0021/DP2018-0390, LOC2017-0127/DP2017-1699	
Municipal address:	230 7A ST NE, 438 8 ST NE	
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File Manager:	Sara Kassa	
City Wide Urban Design:	Angela Kiu	
Applicant:	02 Planning and Design	
Architect:	Davignon Martineau Architects	
Owner:		
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Summary

The Panel recognizes that this development does not require UDRP review and feedback, however commends the applicant for voluntarily seeking input on this innovative proposal for a new residential typology within the neighbourhood of Bridgeland.

Overall, the Panel is excited by the applicant's desire to address a corner residential lot differently than an infill lot and activate both the avenue and street facades through the residential unit configurations and the exterior design. The Panel also commends the intent of the design to provide more variety within this community with regards to scale and cost, ultimately supporting a diverse community fabric and demographic.

The Panel encourages further study of the contrasting expression of the third level and garage exterior treatment. Although it is understood that the current proposal attempts to accentuate the two-storey massing of the building, the graphic illustrations seem to highlight both the third level and garage, as oppose to diminishing their presence.

Although this application does not align exactly with the current ASP for this community, the Panel strongly **supports** this application and the change in zoning to Direct Control for both sites.

Applicant Response

(18.06.21)

We appreciate the panels comments interest in the project, their time commitment and feedback. We have carefully combed through the comments and are providing responses for each sections of the document. We can confirm that we are taking the commentary to heart and have revised the

Applicant Response to Urban Design Review Panel Comments

third level and garage exterior treatment. These elevations have been adjusted to add character that is more compatible with the current proposed two level lower massing.

Urban Vitality			
	Topic	Best Practice	Ranking
1	Retail street diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.	N/A
	UDRP Commentary		
	Not Applicable.		
	Applicant Response		
	N/A		
2	Retail street transparency, porosity	Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.	N/A
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	Not Applicable.		
	Applicant Response		
	N/A		
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	The existing public sidewalk is maintained along the street and avenue. Access to residential units are connected to the public sidewalk through hard scaping. A rear parking garage is provided for residents with concrete pads connecting the garage to the rear lane. Private patios and landscaping define the edge condition between the public and private realm.		
	Applicant response		
	In agreement.		
4	Entry definition / legibility	Entry points are clear and legible	Support
	UDRP Commentary		
	Entry to each of the private residential units is clear and legible. The proposed development activates both street and avenue through meaningful points of entry.		
	Applicant Response		
	In complete agreement.		
5	Residential multi-level units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Support
	UDRP Commentary		
	The design proposes two and three storey units at the street level, facing both the street and avenue at this corner site condition. Private outdoor patios, at grade, provide opportunity for furniture and extend towards the sidewalk. They offer physical and visual access to the public sidewalk. Second and third level balconies also contribute to animating the façades.		
	Applicant Response		
	In agreement.		
6	At grade parking	At grade parking is concealed behind building frontages along public streets.	Support with comment
	UDRP Commentary		

Applicant Response to Urban Design Review Panel Comments

<p>A private, enclosed garage, is provided for four vehicles (one per residential unit). Street parallel parking is by permit only and a C-train station is located within 10 minutes walking distance. The panel feels this is more than adequate. Designated 'Car to Go' parking locations near-by may assist the applicant in negotiating Community support, as it could minimize Community concerns around vehicle volumes. Some exploration of pushing the garage away from the property line is encouraged as it could allow for some landscape to buffer this portion of the building.</p>																																									
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<p>In agreement. A designated Car to Go parking location near-by would be a worthy amenity that would further support our project and the community.</p>																																									
<p>Pushing the garage away from the North façade has been explored. The garage could shift 17" to the South but we feel this will reduce the practicality of the rear walk and doesn't provide a significant enough increase in landscaping where the landscaping would survive.</p>																																									
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<p>Although we agree with the UDRP comments, we are electing to not pursue this item in consideration of the parking concerns brought forth by the Community Association and resident. We feel that in due time, the planning department and CA's will begin considering more parking reductions to proposed developments.</p>																																									
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Applicant Response																																									
N/A																																									
11	<p>Cycle path connections</p>	<p>Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.</p>																																							
UDRP Commentary																																									
Applicant Response																																									

Applicant Response to Urban Design Review Panel Comments

	N/A		
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	N/A
UDRP Commentary			
Applicant Response			
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.	Support
UDRP Commentary			
The design provides an exterior walkway along the side setback through the site, connecting back entryways to the garage, laneway and front street. Suggest the addition of gates to discourage public access and enhance CPTED principles.			
Applicant Response			
In agreement. A gate has been added to the rear of the property to discourage public access.			
14	Open space networks and park systems	Connects and extends existing systems and patterns.	N/A
UDRP Commentary			
Applicant Response			
N/A			
15	Views and vistas	Designed to enhance views to natural areas and urban landmarks.	
UDRP Commentary			
Applicant Response			
N/A			
16	Vehicular interface		
UDRP Commentary			
Applicant Response			
N/A			
17	Other		
Applicant Response			
N/A			
Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i>			
Topic		Best Practice	Ranking
18	Massing relationship to context	Relationship to adjacent properties is sympathetic	Support
UDRP Commentary			
Although the proposed massing deviates from the existing residential context by proposing a third story, the design is sensitive to the adjacent two storey structures by stepping back the proposed third storey and articulating the building massing on the shared side-yard elevation. The street and avenue facades project slightly into the required setback, however the panel feels that the projection on the street façade is minimal and as a corner lot, this projection does not diminish the overall street perspective or negatively impact the adjacent property. Visibility triangles for vehicles			

Applicant Response to Urban Design Review Panel Comments

	is not affected. The panel does feel that retracting the garage façade along the avenue would have a positive impact on the avenue perspective and elevation, allowing sufficient space for a landscaping feature to soften the blank wall façade. Overall, the massing addresses both the street and avenue, which is a positive urban design gesture.		
	Applicant Response		
	We are in agreement with the UDRP comments. As previously mentioned we have elected to keep the 4 garage stalls to help alleviate parking concerns from the Community Association.		
19	Massing impacts on sun shade	Sun shade impacts minimized on public realm and adjacent sites	Support
	UDRP Commentary		
	Although the proposed massing deviates from the existing residential context, that being a main residence with a detached garage and a backyard space, the proposed massing is located on north corner sites, and therefore does not introduce shadows onto adjacent backyards.		
	Applicant Response		
	In agreement.		
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support
	UDRP Commentary		
	See comment #18. This application is innovative in that it is addressing both streets through form and layout. As a corner lot it fronts two streets and the design addresses both streets differently, relating to the attitude of 8 th Street through a duplex massing, and initiating an attitude for the avenue through a townhouse massing. This solution is in fact less than what could be built within the current zoning, further strengthening the rationale behind support of this project.		
	Applicant Response		
	In agreement.		
21	Massing distribution on site		Support
	UDRP Commentary		
	The panel commends the applicant's intent to provide affordable density and improve the quality of space within the residential units. The massing occupies the entire site, while achieving natural light within each unit and maintaining visual privacy for existing adjacent properties.		
	Applicant Response		
	In agreement.		
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Support
	UDRP Commentary		
	See comment #5		
	Applicant Response		
	In agreement.		
23	Other		
	Applicant Response		
	N/A		
Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i>			
	Topic	Best Practice	Ranking
24	Safety and security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Support
	UDRP Commentary		

Applicant Response to Urban Design Review Panel Comments

	The design provides lighting and glazing along both the street and avenue, which is an improvement within the neighbourhood. This improvement lends much needed 'eyes on the street' for the avenue, where laneway access can provide hiding areas. All unit entry doors are visible and provide full glazing. Also see comment # 13.	
	Applicant Response	
	In agreement.	
25	Pedestrian level comfort - wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.
	UDRP Commentary	
	Applicant Response	
	N/A	
26	Pedestrian level comfort - snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.
	UDRP Commentary	
	Although this was not discussed, the applicant may wish to identify the snow shoveling procedures to the Community Association to address any sidewalk maintenance concerns.	
	Applicant Response	
	A condo plan and use rules will be developed for the project, to which buyers, multigenerational dwellers or tenants would have to follow.	
27	Weather protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.
	UDRP Commentary	
	All residential unit entries have canopies to protect against weather conditions and improve the safety of landings.	
	Applicant Response	
	In agreement.	
28	Night time design	N/A
	UDRP Commentary	
	Applicant Response	
	N/A	
29	Barrier free design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.
	UDRP Commentary	
	One of the proposed units is single storey and accommodations could be made to ramp the main entry walkway. This could support the design intent of supporting a diverse community both generationally and economically.	
	Applicant Response	
	In agreement.	
30	Winter city	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.
	UDRP Commentary	
	Applicant Response	
	N/A	
31	Other	
	Applicant Response	
	N/A	

Applicant Response to Urban Design Review Panel Comments

Service / Utility Design *Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.*

Topic		Commentary	Ranking
32	(specify)		TBD

Main Streets DC Direct Control District

BYLAW NUMBER 273D2017

**BEING A BYLAW OF THE CITY OF CALGARY
TO AMEND THE LAND USE BYLAW 1P2007
(LAND USE AMENDMENT LOC2016-0200)

WHEREAS it is desirable to amend the Land Use Bylaw Number 1P2007 to change the land use designation of certain lands within the City of Calgary;

AND WHEREAS Council has held a public hearing as required by Section 692 of the Municipal Government Act, R.S.A. 2000, c.M-26 as amended;

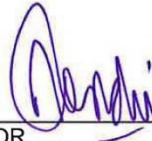
NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".
2. This Bylaw comes into force on the date it is passed.

READ A FIRST TIME THIS 1ST DAY OF AUGUST, 2017.

READ A SECOND TIME THIS 1ST DAY OF AUGUST, 2017.

READ A THIRD TIME THIS 1ST DAY OF AUGUST, 2017.



MAYOR
SIGNED THIS 1ST DAY OF AUGUST, 2017.

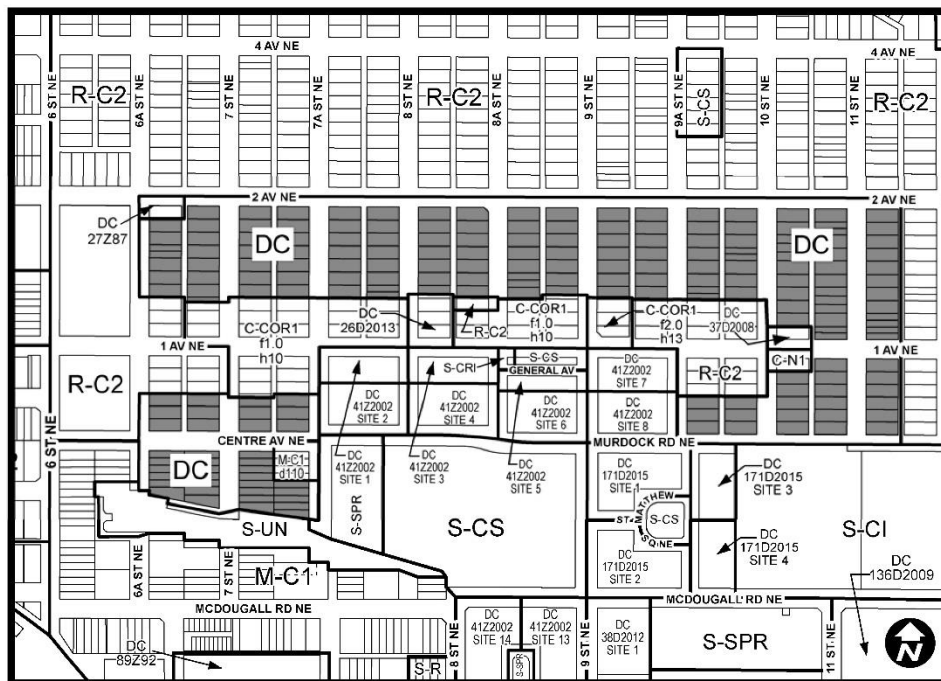


ACTING CITY CLERK
SIGNED THIS 1ST DAY OF AUGUST, 2017.

Main Streets DC Direct Control District

AMENDMENT LOC2016-0200
BYLAW NUMBER 273D2017

SCHEDULE B



DC DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control District is intended to:

- (a) facilitate the conservation of **Character Homes** and allow for a diversity of grade-oriented **development**;
- (b) provide incentive for the **retention** of **Character Homes** through increased options for multi-residential and grade-oriented residential **development** where **Character Homes** are being retained;

Main Streets DC Direct Control District

AMENDMENT LOC2016-0200 BYLAW NUMBER 273D2017

- (c) support the exercise of discretion by the **Development Authority** in relation to **building setbacks**, **building depth**, **building height**, at-grade orientation of multi-residential **units**, motor vehicle parking, landscaping, and **parcel coverage** in order to accommodate the successful **retention** and restoration of **Character Homes** combined with new multi-residential and grade-oriented residential **development**; and
- (d) accommodate grade-oriented **development** in the form of **Rowhouse Buildings**, **Duplex Dwellings**, **Semi-detached Dwellings**, **Single detached Dwellings**, and **Cottage Housing Clusters** on sites without **Character Homes**.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

General Definitions

- 4 In this Direct Control District:

- (a) "**Character Home**" means a **Duplex dwelling**, **Semi-detached Dwelling**, or **Single detached Dwelling** existing on the date of passage of this Direct Control District and which is:
 - (i) listed on the **City's** Inventory of Evaluated Historic Resources;
 - (ii) designated as a Municipal Historic Resource pursuant to the **Historical Resources Act** by a bylaw approved by **Council** or included in the list of addresses in Schedule C; and
 - (iii) determined to meet the criteria for identification of a **Character Home** as set out in Schedule D, following a review by the **Development Authority** at the time of application for a **Development Permit**.
- (b) "**retained**" or "**retention**" means maintaining, conserving, or restoring a **Character Home**, with an emphasis on the **street** facing façade elements of the **Character Home**, and may include:
 - (i) conversion into multiple **Dwelling Units**;
 - (ii) raising of the floor closest to **grade** to allow for more usable **basement** space;
 - (iii) additions and extensions if they are subordinate to or compatible

Main Streets DC Direct Control District

AMENDMENT LOC2016-0200 BYLAW NUMBER 273D2017

with the original form, scale, massing and roof profile of the **Character Home**;

- (iv) replacement of original materials with in-kind substitutes;
- (v) relocation of the **Character Home** on the **parcel(s)** included in the **development**; and
- (vi) listing on the **City's** Inventory of Evaluated Historic Resources or designation as a Municipal Historic Resource pursuant to the **Historical Resources Act** by a bylaw approved by **Council**.

Use Definitions

5 In this Direct Control District:

- (a) “**Character Home Retention Development**” means a **use**:
 - (i) that consists of one or more **buildings**, each containing one or more **units**; and
 - (ii) where all **Character Homes** on the **parcels** included in the **development** are **retained** within the **development**.
- (b) “**Live Work Unit**” means a **use**:
 - (i) where a business is operated from a **Dwelling Unit**, by the resident of the **Dwelling Unit**, but does not include a **Home Occupation – Class 1** or **Home Occupation – Class 2**;
 - (ii) that may incorporate only the following **uses** in a **Dwelling Unit** to create a **Live Work Unit** when located in this Direct Control District:
 - (A) **Artist’s Studio**;
 - (B) **Counselling Service**;
 - (C) **Office**; and
 - (D) **Retail and Consumer Service**, provided any products sold are also made on the premises or directly related to the service provided;
 - (iii) that must be contained within a **Character Home Retention Development**.

Permitted Uses

- 6 (1) The **permitted uses** of the Residential – Contextual One / Two Dwelling (R-C2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.
- (2) A **Rowhouse Building** is a **permitted use** where, at the date of passage of this Direct Control District, a **parcel** does not contain a **Character Home** and where a **Rowhouse Building** complies with the rules of the Residential – Grade-Oriented Infill (R-CG) District and section 347.3 of

Main Streets DC Direct Control District

AMENDMENT LOC2016-0200 BYLAW NUMBER 273D2017

Bylaw 1P2007.

Discretionary Uses

- 7 (1) The *discretionary uses* of the Residential – Contextual One / Two Dwelling (R-C2) District of Bylaw 1P2007 are the *discretionary uses* in this Direct Control District.
- (2) The following are additional *discretionary uses* where, at the date of passage of this Direct Control District, a *parcel* does not contain a *Character Home*, or where a *development* includes the *retention* of a *Character Home*:
- (a) Cottage Housing Cluster; and
 - (b) Rowhouse Building.
- (3) The following are additional *discretionary uses* where a *parcel*, or multiple *parcels* subject to the same *development permit*, contain a *Character Home*:
- (a) Character Home Retention Development; and
 - (b) Live Work Unit.

Bylaw 1P2007 District Rules

- 8 Unless otherwise specified, the rules of the Residential – Contextual One / Two Dwelling (R-C2) District of Bylaw 1P2007 apply in this Direct Control District.

Rules for Parcels Not Containing Character Homes

- 9 Where, at the date of passage of this Direct Control District, a *parcel* does not contain a *Character Home*, the rules of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 apply in this Direct Control District.

Rules for Character Home Retention Development

- 10 (1) Except as otherwise referenced in subsections (2), (3), (4), (5) and (6), where a *Character Home Retention Development* is proposed the rules for a *Multi Residential Development* in the Multi-Residential – Contextual Grade-Oriented (M-CG) District of Bylaw 1P2007 apply in this Direct Control District.
- (2) The maximum *building height* on a *parcel* that shares a *property line* with a *lane* or another *parcel* that has no *buildings* or that has a *building* with a height greater than 6.0 metres above *grade* at that shared *property line*, and where the other *parcel* is designated with a *low density residential district* or M-CG District:
- (a) is 8.0 metres measured from *grade* at the shared *property line*; and
 - (b) increases proportionately to a maximum of 11.0 metres measured from *grade* at a distance of 4.0 metres from the shared *property line*.

Main Streets DC Direct Control District

AMENDMENT LOC2016-0200 BYLAW NUMBER 273D2017

- (3) The maximum **building height** on a **parcel** that shares a **property line** with a **parcel** that has a **building** with a height that does not exceed 6.0 metres above **grade** at that shared **property line**, and where the other **parcel** is designated with a **low density residential district** or M-CG District:
- (a) is 6.0 metres measured from **grade** at the shared **property line**; and
 - (b) increases proportionately to a maximum of 11.0 metres measured from **grade** at a distance of 6.0 metres from the shared **property line**.
- (4) For a **Character Home Retention Development** located on a **parcel** that has a **parcel width** of less than 15.0 metres:
- (a) unless otherwise referenced in subsection (b), the maximum **building height** is 11.0 metres.
 - (b) the maximum **building height** within 4.2 metres of a side **property line** that is shared with another **parcel** that is designated with a low density residential district other than R-CG is the greater of:
 - (i) 8.6 metres;
 - (ii) the contextual **height** plus 1.5 metres, to a maximum of 10.0 metres; or
 - (iii) the **building height** of the contextually adjacent **building** at the shared side **property line**, to a maximum of 11.0 metres.
- (5) The maximum continuous horizontal cross section through any portion of a **building** at 8.0 metres above **average grade** must not be greater than 18.0 metres.
- (6) Any portion of a **building** located at 8.0 metres above **average grade** must be separated from other portions of a **building** located at 8.0 metres above **average grade** by at least 2.4 metres.
- (7) The maximum **parcel width** is 46.0 metres for a **parcel** containing a **Multi-Residential Development**.

Rules for Live Work Units

11 A Live Work Unit:

- (a) must not exceed 50.0 per cent of the **gross floor area** of the **Dwelling Unit**;

Main Streets DC Direct Control District

AMENDMENT LOC2016-0200 BYLAW NUMBER 273D2017

- (b) may have two persons, other than a resident of the **Live Work Unit**, working at the residence where the *use* is located; and
- (c) requires a minimum number of *motor vehicle parking stalls* and *bicycle parking stalls – class 1 or class 2* in accordance with the rules for a **Live Work Unit** in the M-CG District in Land Use Bylaw 1P2007.

Parking Stall Requirements

- 12 Where a *development* includes the *retention* of a **Character Home** and is located on a *parcel* with a *parcel width* of less than 10.0 metres, no more than 2.0 *motor vehicle parking stalls* total is required.

Main Streets DC Direct Control District

**AMENDMENT LOC2016-0200
BYLAW NUMBER 273D2017**

210 10 Street NE
212 7A Street NE
214 9 Street NE
215 7A Street NE
216 8A Street NE
216 9 Street NE
218 7A Street NE
218 9 Street NE
219 7A Street NE
220 7 Street NE
221 9A Street NE
223 7A Street NE
227 8 Street NE
228 8 Street NE
230 8A Street NE
232 8 Street NE
234 7 Street NE
234 8 Street NE
240 11 Street NE
59 7 Street NE
68 7 Street NE
70 6A Street NE
70 7 Street NE

Main Streets DC Direct Control District

AMENDMENT LOC2016-0200
BYLAW NUMBER 273D2017

SCHEDULE D

Schedule D of this Direct Control district outlines the criteria for identification of a **Character Home**, followed by two examples of the application of these criteria, provided for reference purposes. The **Development Authority** will, at the time of application for a **Development Permit**, determine whether the criteria were met and the property was a **Character Home**. Photographic evidence of the property may be used to support this evaluation.

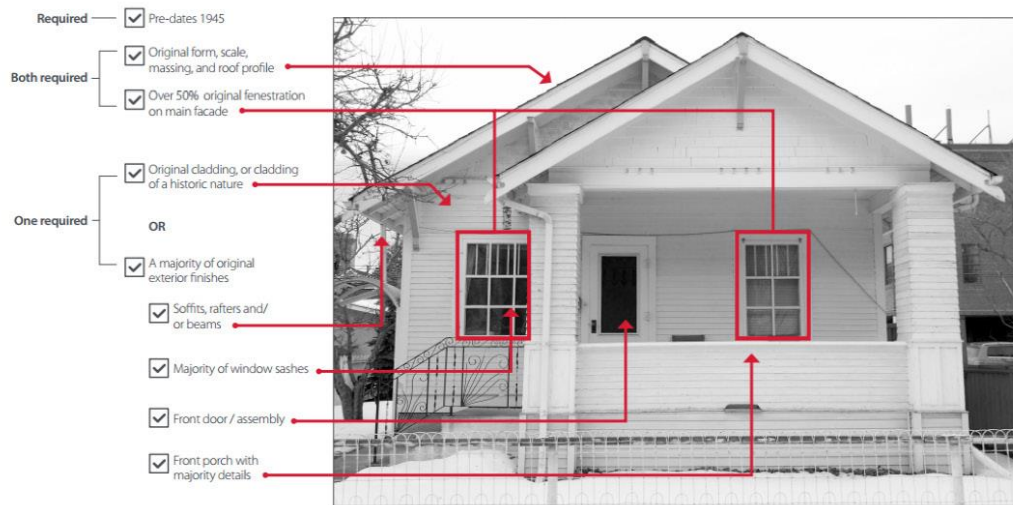
- (a) A **Character Home**, in the opinion of the **Development Authority**, will:
 - (i) have been constructed prior to 1945;
 - (ii) maintain both:
 - (A) the original form, scale, massing and roof profile of the **building** and may include additions and extensions if they are subordinate to or compatible with the original form, scale, massing and roof profile of the **building**; and
 - (B) at least 50 per cent of the original window pattern and openings on the portions of the front and side façades visible from a **street** excluding porch glazing if applicable; and
 - (iii) have one of the following on the portions of the front and side façades visible from a **street**:
 - (A) original cladding or cladding of a historical nature which has gained heritage value in its own right; or
 - (B) a majority of original exterior finishes, comprising two or more of the following items:
 - (i) at least 50 per cent of window sashes;
 - (ii) soffits; exposed rafters, or beams;
 - (iii) front door or doorway assembly; and/or
 - (iv) if present, a front porch with majority of original finishes including piers, columns, balustrades, entablature mouldings, and may include enclosure of the porch with glazing where the majority of other original finishes remains in place and the glazing is of a compatible historical appearance; and

Main Streets DC Direct Control District

AMENDMENT LOC2016-0200 BYLAW NUMBER 273D2017

(iv) not include:

- (A) elements of a permanent nature which have been added to the principal **street** facing façades of the **building** that would not have been present historically and which negatively impact the heritage character and integrity of the home.



Comprehensive Project Overview

TABLE OF CONTENTS

- 1 PROJECT BACKGROUND AN TIME LINE
- 2 SITE CONTEXT
- 3 LAND USE APPLICATION
- 4 ENGAGEMENT SUMMARY
- 5 THE SEARCH FOR THE MISSING MIDDLE
- 6 TYPICAL BUILDER TYPOLOGIES
- 7 BASEMENT AND GARAGE SUITE SOLUTIONS TO DENSIFICATION
- 8 A NEW SOLUTION TO DENSITY
- 9 BUILDING MASSING
- 10 LANDSCAPE PLAN
- 11 DESIGN PRINCIPLES
- 12 URBAN DESIGN - ACTIVATING THE STREET AND THE AVENUE
- 13 MATERIALS
- 14 RENDERS
- 15 FLOOR PLANS
- 16 STREETSCAPE

Comprehensive Project Overview

Project Background + Timeline

Background

The property owners of 438 8 Street & 230 7A Street NE have submitted applications to rezone their parcels to allow for the development of family-oriented townhouses (4 units on each parcels).

438 8 Street is currently designated Residential - Contextual One/Two Dwelling District (R-C2) and 230 7A Street is designated with the recently approved main streets Direct Control District (DC). The proposed land use district of both parcels is a Direct Control District tied to Plans. A Direct Control district tied to plans means that the Development Permits (DP) for the projects will need to be aligned with

the plans accompanying the DC. If the DP does not align with the DC drawings, the land use reverts to the existing land use.

Along with the land use redesignation application, a concurrent development permit has been submitted for each site.

Development Principles

- Moderate intensification and density;
- Site specific design;
- Family friendly housing choice;
- Variety of unit sizes;
- Walkability;
- Street vitality;
- Community interaction.

Land Use Timeline



Comprehensive Project Overview

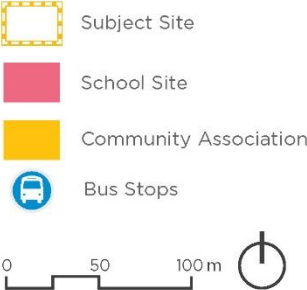
Site Context



The subject site located at 438 8 Street NE is approximately 455 square metres in size, and is surrounded by R-C2 (Residential - Contextual One/Two Dwellings).

The subject site located at 230 7A Street NE is approximately 455 square metres in size. East of the parcel is St. Michael's Eritrean Orthodox Church, and to the north, west, and south of the site are R-C2 (Residential - Contextual One/Two Dwellings).

Both subject sites are in close proximity to local shops, retail, professional services, community schools, parks and open spaces. These sites are well serviced by Calgary Transit, with route 90 running along the 1st Avenue corridor. Additionally, these sites are within 10-15 minutes walking distance to the Bridgeland-Memorial LRT Station and Bow River pathway system.



Comprehensive Project Overview

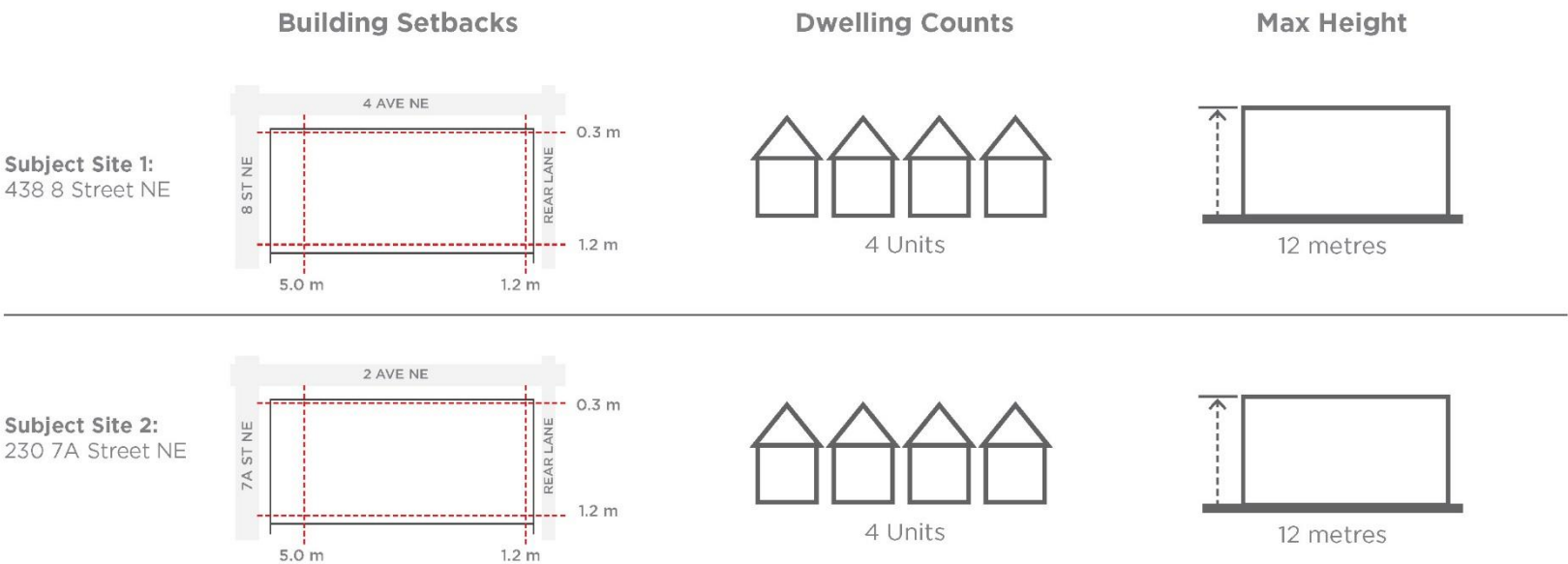
Land Use Application

The land use amendment application proposes to change the existing land use of 438 8 Street from R-C2 (Residential - Contextual One/Two Dwelling District) and the existing land use of 230 7A Street from DC based on R-CG (Residential - Grade-Oriented Infill District) to both a Direct Control District (DC) tied to plans.

The diagrams on this panel explains the concepts regarding the land use amendment.

Direct Control (DC) Tied to Plans:

- DC will be based on a site specific design proposal;
- Development Permit must conform with the plans that were submitted along with the land use application;
- If the property owner wishes to build a different project or sell the site, the existing stock land use will apply;
- A DC tied to plans provides certainty to the community on the project's design.



Comprehensive Project Overview

Engagement Summary

On March 26, 2018, residents from the community of Bridgeland-Riverside were invited to a community information session hosted at the Bridgeland-Riverside Community Association Hall. The purpose of this community information session was to provide information and to solicit community feedback on the proposed developments located on 438 8 Street and 230 7A Street NE.

The community information session was attended by approximately 25 residents. The format of this information session was an informal discussion with the project’s planners, architects, and land owners. Residents had the opportunity to provide feedback using comment forms and sticky notes provided by the applicant. There were a total of **50 comments received** and have been categorized into the following themes;

- 1. **Traffic, Parking & City Servicing**
- 2. **Bridgeland-Riverside ARP Alignment & Zoning**
- 3. **Building Design**
- 4. **Massing & Height**
- 5. **Adjacent Privacy, Lighting & Green Space**
- 6. **Planning Context & Community Heritage**

The following is a breakdown of the number of received comments that fall under the six main themes;

- 1. Traffic, Parking & City Servicing: **7 Comments**
- 2. Bridgeland-Riverside ARP Alignment & Zoning: **12 Comments**
- 3. Building Design: **5 Comments**
- 4. Massing & Height: **10 Comments**
- 5. Adjacent Privacy, Lighting & Green Space: **3 Comments**
- 6. Planning Context & Community Heritage: **9 Comments**

In addition to these comments, there were **4 comments that are categorized as miscellaneous**. These comments did not fit under any of the five main themes.

Of the 50 comments received, 4 comments were of support, and 46 comments were of concern or opposition. The following is a breakdown of support comments vs. concern or opposition comments categorized under the six main themes.

Comment Comparison Table

	Traffic, Parking & City Servicing	Bridgeland-Riverside ARP Alignment & Zoning	Building Design	Height & Massing	Adjacent Privacy, Lighting & Green Space	Planning Context & Community Heritage	Misc
No. Comments of Support			3	1			
No. Comments of Concern or Opposition	7	12	2	9	3	9	4



Comprehensive Project Overview

THE SEARCH FOR THE ‘MISSING MIDDLE’

“...what every city is missing is something in the middle:

the kind of not-single-family, not-condo-tower-apartment style of housing that provides enough room for a couple of parents and kids, has some kind of ground-level space outside and doesn’t cost a fortune.”

[The Globe and Mail, A search for the ‘missing middle’ of housing, May 23rd 2018]

“Multi-generational living arrangements... are the fastest growing type of household in the country.”

[CTV News, Why more Canadians are embracing multi-generational households, April 5th 2018]

“Ms. Maser says ... peak millennials are purchasing two-storey detached homes in the suburbs as a way to increase affordability over more central areas.”

[The Globe and Mail, Sweet Spot, June 5th 2018]

“Nearly one in five Calgary households are overspending on their housing. They’re paying more than 30 per cent of their income every month towards shelter, which means they’re making decisions between eating or having a roof over their head.”

[Calgary Herald, Calgary lays out eight-year plan for \$320M affordable housing investment, April 30th 2018]

“Twenty-four per cent of those surveyed said they didn’t qualify for a mortgage at all; in Alberta, this rose to 32 per cent, and 50 per cent of 25- to 30-year-olds in Alberta – more than any other province – said they didn’t believe they’d be able to afford to buy a home in the next five years.”

[Globe and Mail, Millennials’ demand for affordability driving housing agenda, November 23rd 2017]

“Rental properties in Montreal, Quebec City and Halifax are performing well, for a variety of reasons. Quebecers have long looked on rentals favourably, and they’re eager to move into units centrally located in search of the live-work-play lifestyle. ... renting offers a significant cost advantage over owning a condo in that market.”

[PWC Canada, Emerging Trends in Real Estate 2018]

“But what every city is missing is something in the middle: the kind of not-single-family, not-condo-tower-apartment style of housing that provides enough room for a couple of parents and kids, has some kind of ground-level space outside and doesn’t cost a fortune.”

[Globe and Mail, A search for the ‘missing middle’ of housing, May 24th 2018]

“For a lot of people, a one-bedroom is basically fine for them and then they have a kid. All of a sudden, they need something bigger. It’s really hard and there is not much out there.”

[CBC News, Affordable housing solutions for ‘missing middle’ explored in Museum of Vancouver workshop, February 1st 2018]

“Back-to-backs, brownstones, bungalow courts, clustered housing, plexes, maisonettes, row-houses, stacked towns and six-packs. Throughout the world, these low-density multi-housing forms provide affordable homes for millions of households. However, in British Columbia, most of our housing is either single-family homes or apartments. As a result, architects and planners are increasingly referring to these other “gentle density” housing solutions as “the missing middle”.”

[Vancouver Sun, ‘Missing middle’ competition offers residential housing solutions, May 12th 2018]

“A looming affordability crisis is poised to hit seniors across the country as the baby boom generation makes its long-predicted shift into its golden years, squeezing the supply of retirement home places and pushing up rents, according to a new report from the rating agency DBRS Ltd.”

[Financial Post, A looming housing affordability crisis is poised to hit seniors across Canada, May 13th 2018]

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CONSULTANTS

NOTES

PROJECT
Bridgeland 4

DATE

SCALE:
NTS

PROJECT LOCATION

230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

OLD RM-4 SOLUTION

- 1 Lack of privacy
- 2 Circulation issues
- 3 Lack of amenity space
- 4 Blank side elevation

Livable volume is approximately 40,150 ft3
\$650,000 - \$720,000

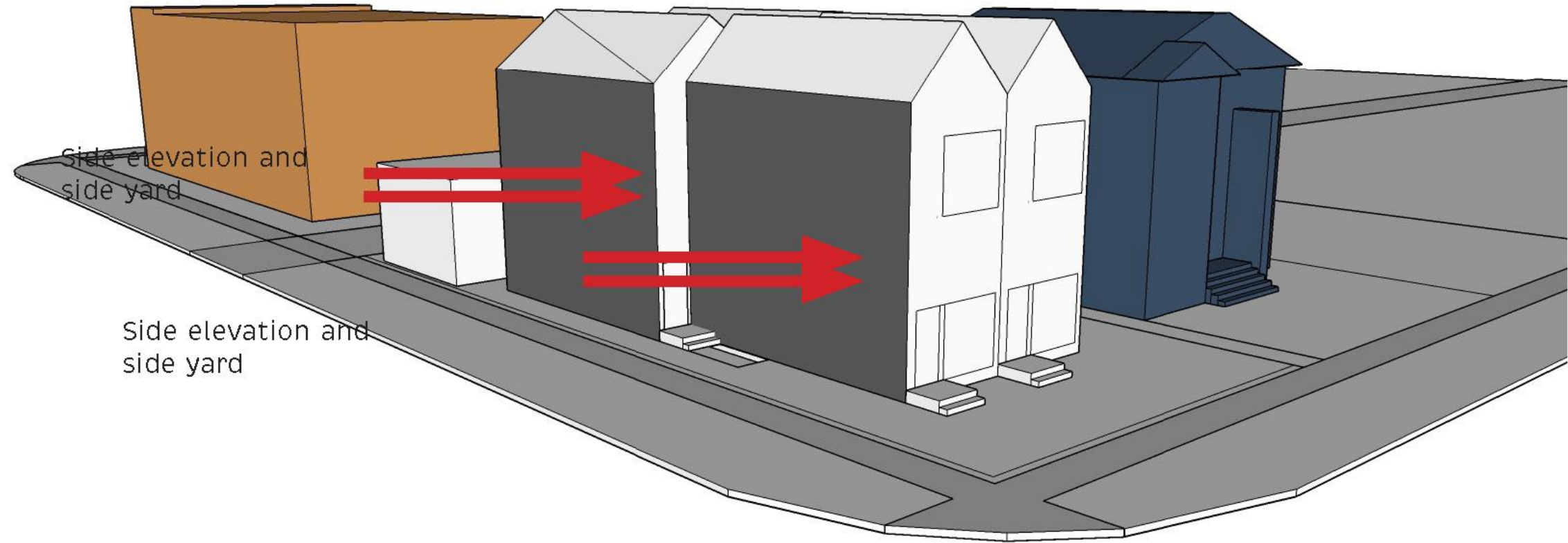


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DATE

SCALE
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

4-SHEET TITLE

Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

DUPLEX WITH GARAGE SUITES

- 1 Lack of unit variation
- 2 Unaffordable for most people
- 3 Blank side elevation

Livable volume is approximately 57,004 ft
\$950,000 - \$1,400,000



2016 /// 18

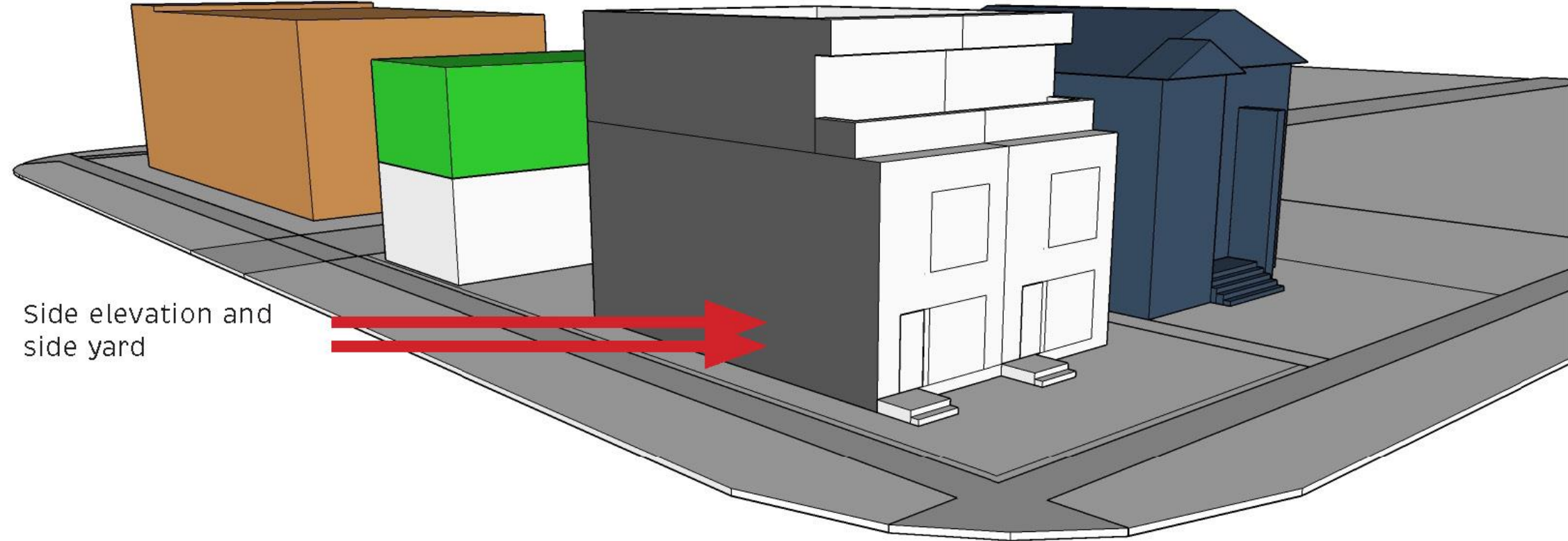
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DATE



Side elevation and
side yard

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CONSULTANTS

NOTES

PROJECT
Bridgeland 4

DATE

SCALE
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

4-INCH TITLE

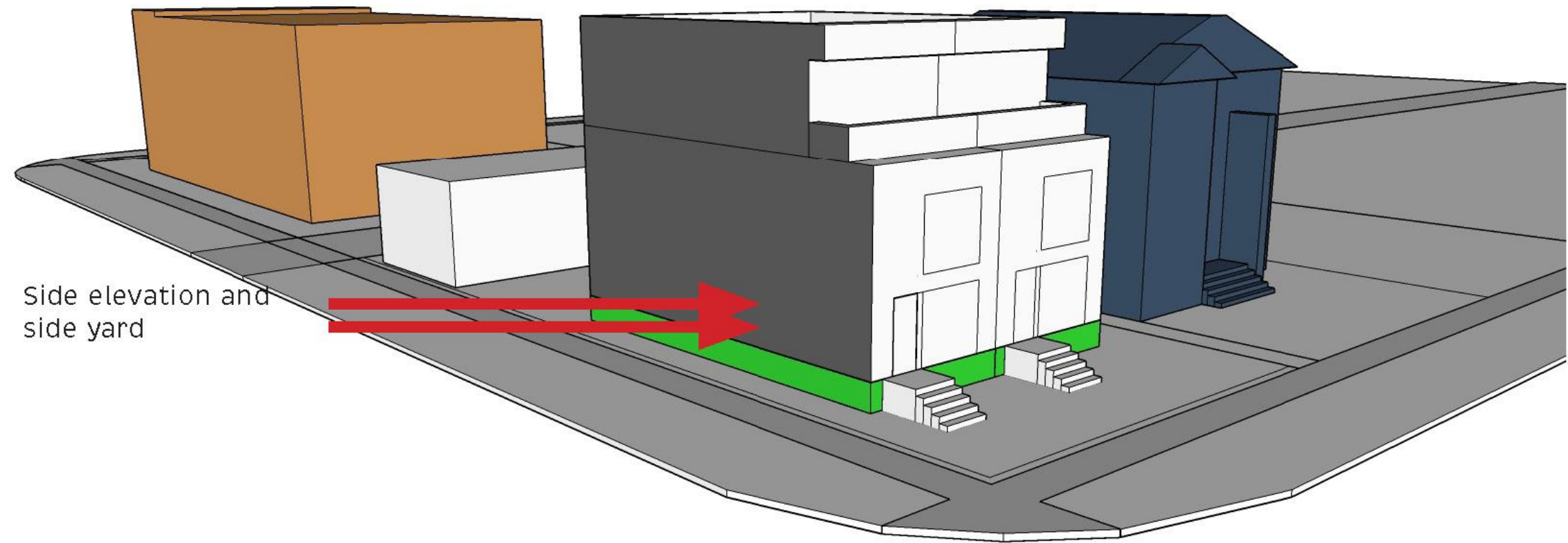
Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

DUPLEX WITH BASEMENT SUITES

- 1 Basement suites are “secondary” solutions
- 2 Unaffordable for most people

Livable volume is approximately 49,252 ft3
\$900,000 - 1,250,000



Side elevation and
side yard

2016 /// 18

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REVISED: 01/18

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DATE

SCALE:
NTS

PROJECT LOCATION

230 7A Street NE
438 8 Street NE

4-1117 TITLE

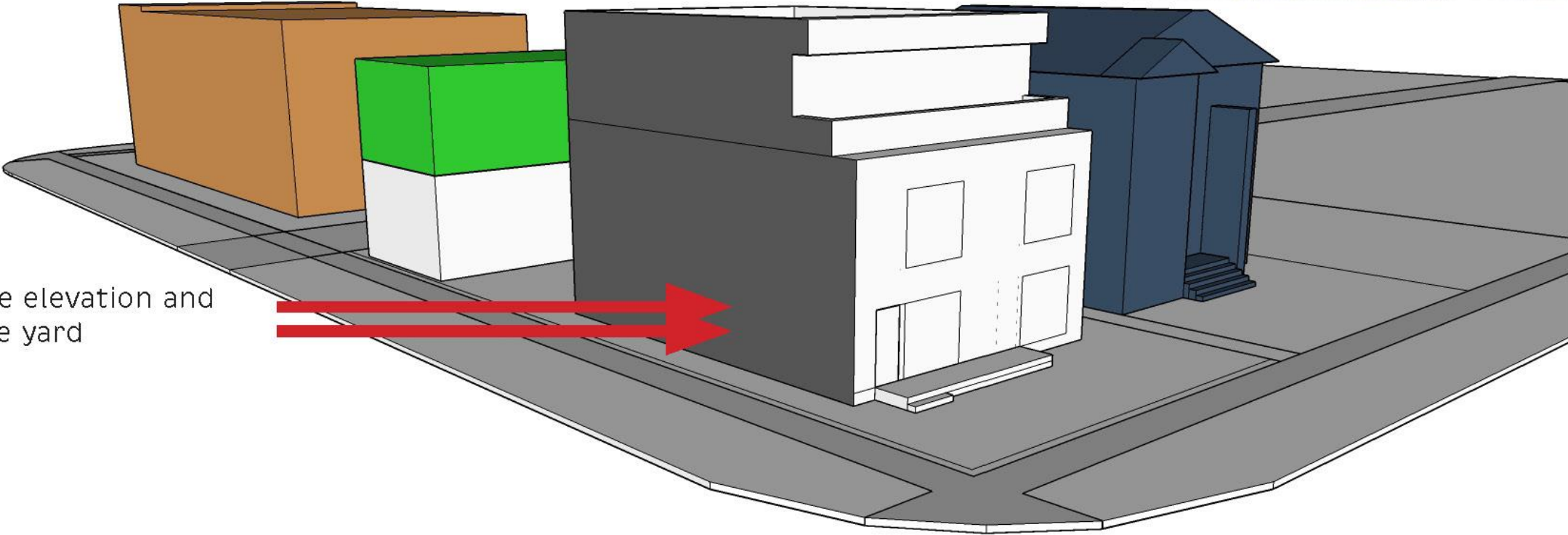
Comprehensive Project Overview

BUILDER GRADE SOLUTIONS TO DENSITY

LARGE SINGLE FAMILY

- 1 Unaffordable for most people
- 2 Highest price point

Livable volume is approximately 57,004 ft3
\$1,200,000 - \$1,850,000 and up



Side elevation and side yard

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Bridgeland 4

DATE

SCALE
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

BASEMENT SUITES

- 1 Basement suites are not great solutions
 - 2 Generally not well lit
 - 3 Not great for families
 - 4 Generally not well sound proofed
 - 5 Ownership by tenant is not an option
 - 6 Limitations - not barrier free
- Students, short term rentals, low income individuals

GARAGE SUITES

- 1 Small
 - 2 Generally a 1 bedroom / studio layout not conducive to families
 - 3 Ownership by tenant is not an option
 - 4 Limitations - not barrier free
- Students, short term rentals, low income individuals



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PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

NEW SOLUTION TO DESNISTY

NEW STACKED TOWNHOUSE



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DATE: 12/15/2016

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CONTRACT/DATE

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PROJECT
Bridgeland 4

DATE

SCALE:
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

#PROJECT TITLE

Comprehensive Project Overview

NEW SOLUTION TO DENSITY

OLD RM-4 SOLUTION
Livable volume is approximately 40,150 ft3
650,000 - \$720,000

NEW MC-G SOLUTION
Livable volume is approximately 101,322 ft3
\$650,000 - \$720,000

DUPLEX WITH GARAGE SUITES
Livable volume is approximately 57,004 ft
\$950,000 - \$1,250,000

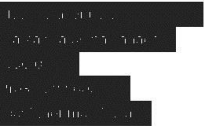
DUPLEX WITH BASEMENT SUITES
Livable volume is approximately 49,252 ft3
\$950,000 - 1,250,000

LARGE SINGLE FAMILY
Livable volume is approximately 57,004 ft3
\$1,250,000 - \$1,850,000 and up

NEW PROPOSED STACKED SUITED TOWNHOUSE
Livable volume is approximately 61,235 ft3
\$375,000 - \$800,000

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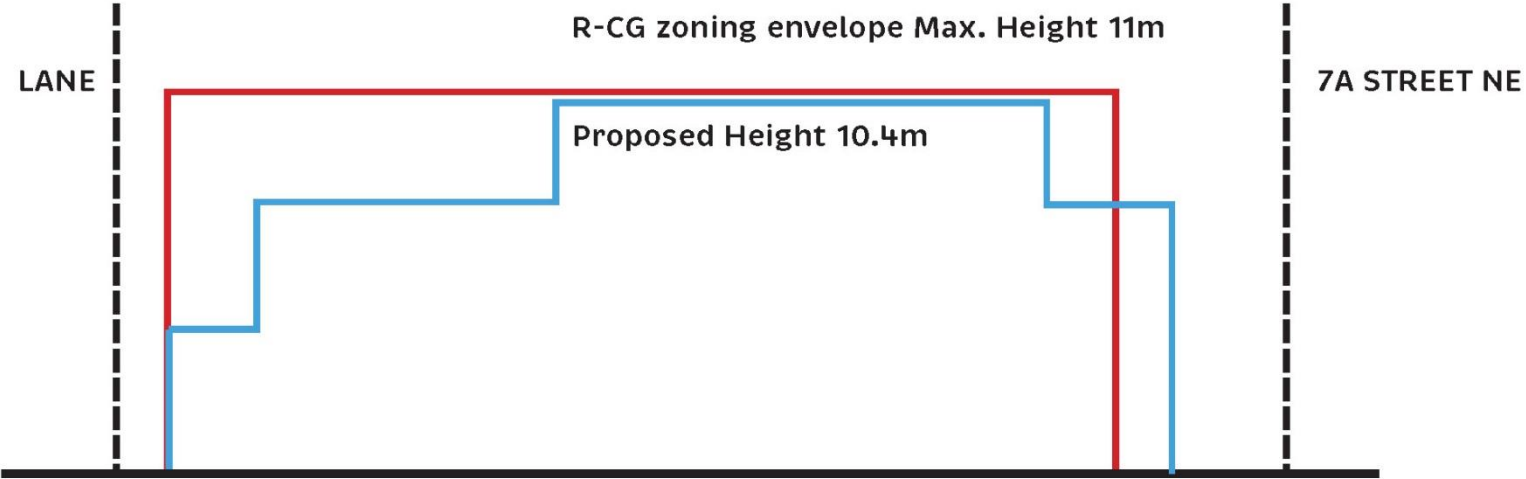
SCALE
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

MASSING 230 7A STREET



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Bridgeland 4

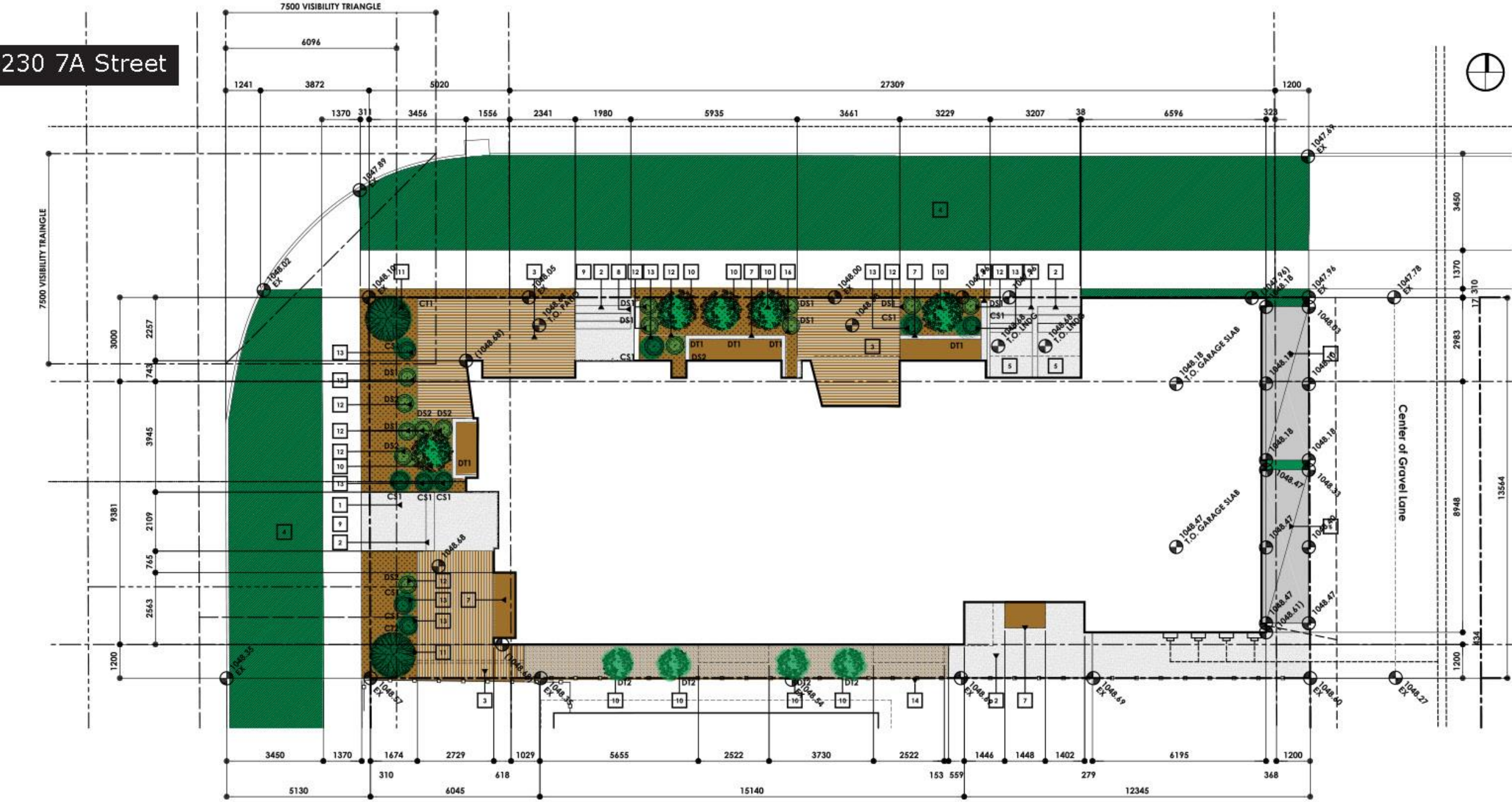
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SCALE:
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

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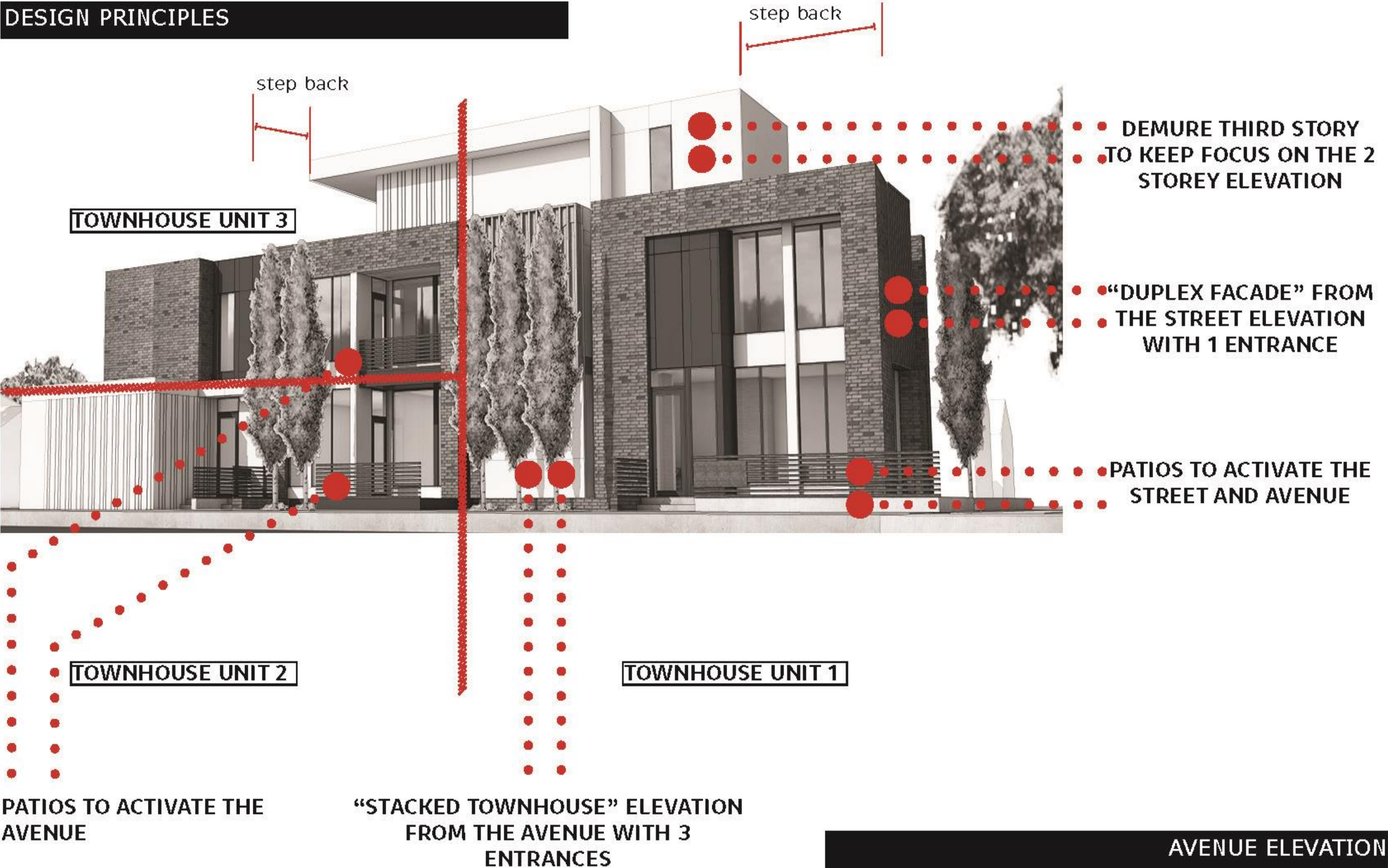


LEGEND NOTES:

1	New Concrete Walk / Pad	8	New Concrete Landscape Wall	00.00	Spot Elevations @ property line For Geodetic Elevations + 1000.00	Existing Deciduous Boulevard Tree	NEW Grass	NEW Wood Decking
2	New Concrete Steps / Landing	9	Existing Sidewalk to Remain	(00.00)	Spot Elevations @ building For Geodetic Elevations + 1000.00	New Exterior Pot Light 100W (See Elevations A-400)	NEW Concrete Apron	NEW Rock Mulch
3	New Wood Patio with Concrete Perimeter	10	New Deciduous Tree		Existing Trees / Shrubs to Remain	New Deciduous Tree	NEW Broom Finish Concrete	NEW Bark Mulch / Planting
4	Existing Boulevard Grass	11	New Coniferous Tree		Existing Trees / Shrubs to be Removed	New Coniferous Tree		
5	New Covered Entry Porch	12	New Deciduous Shrub			New Shrub		
6	New Concrete Apron	13	New Coniferous Shrub					
7	New Concrete Window Well	14	New Fence					

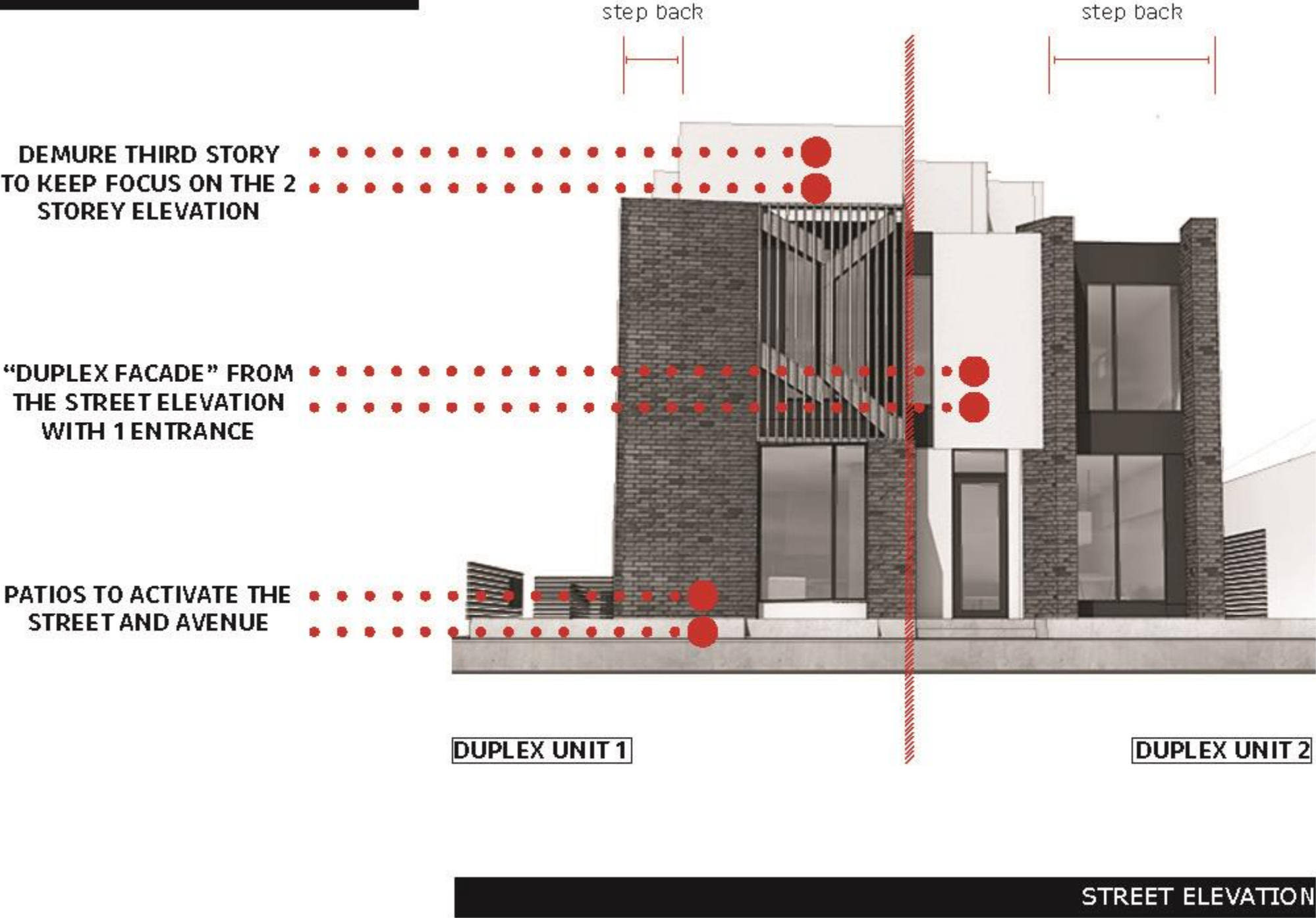
Comprehensive Project Overview

DESIGN PRINCIPLES



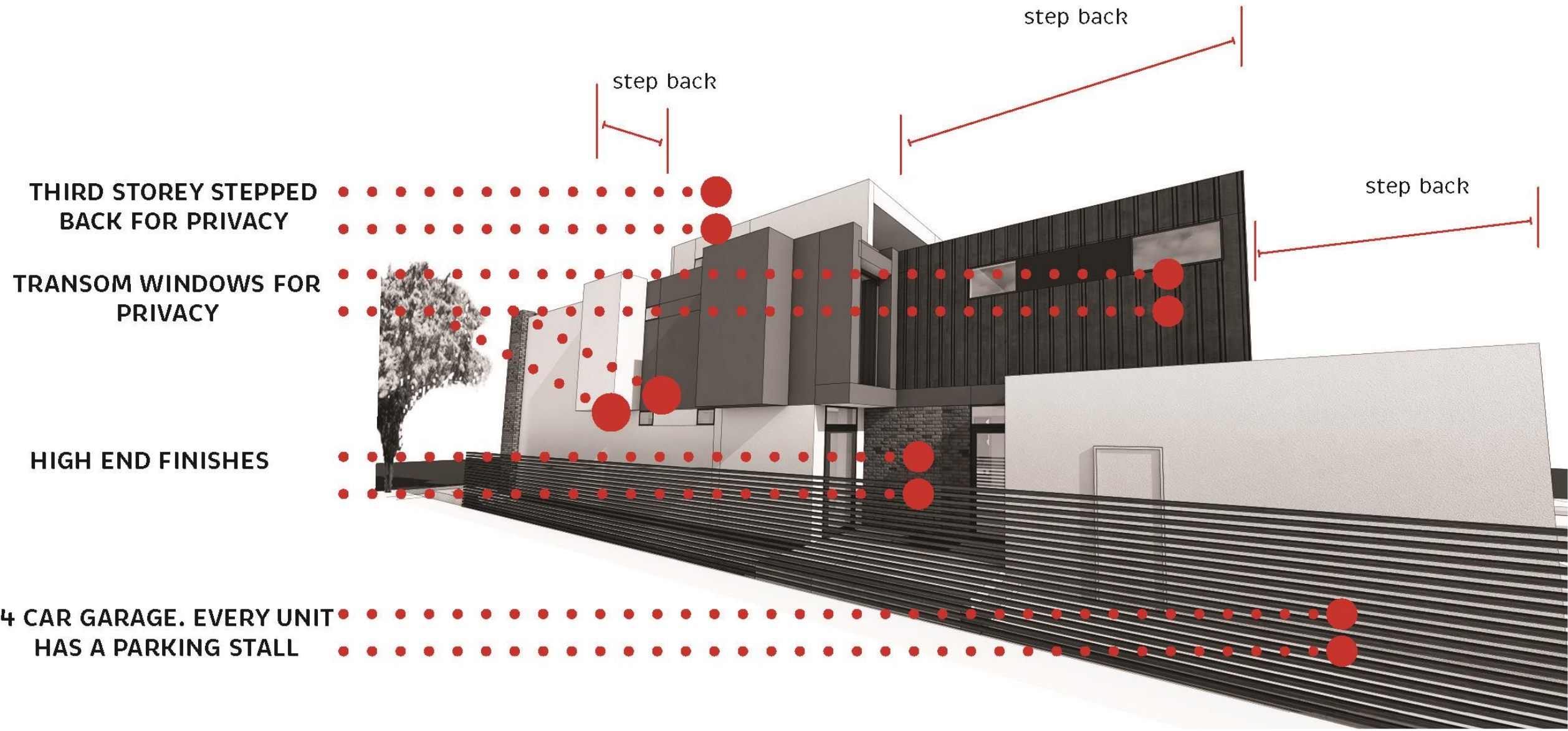
Comprehensive Project Overview

DESIGN PRINCIPLES



Comprehensive Project Overview

DESIGN PRINCIPLES



SIDE YARD ELEVATION

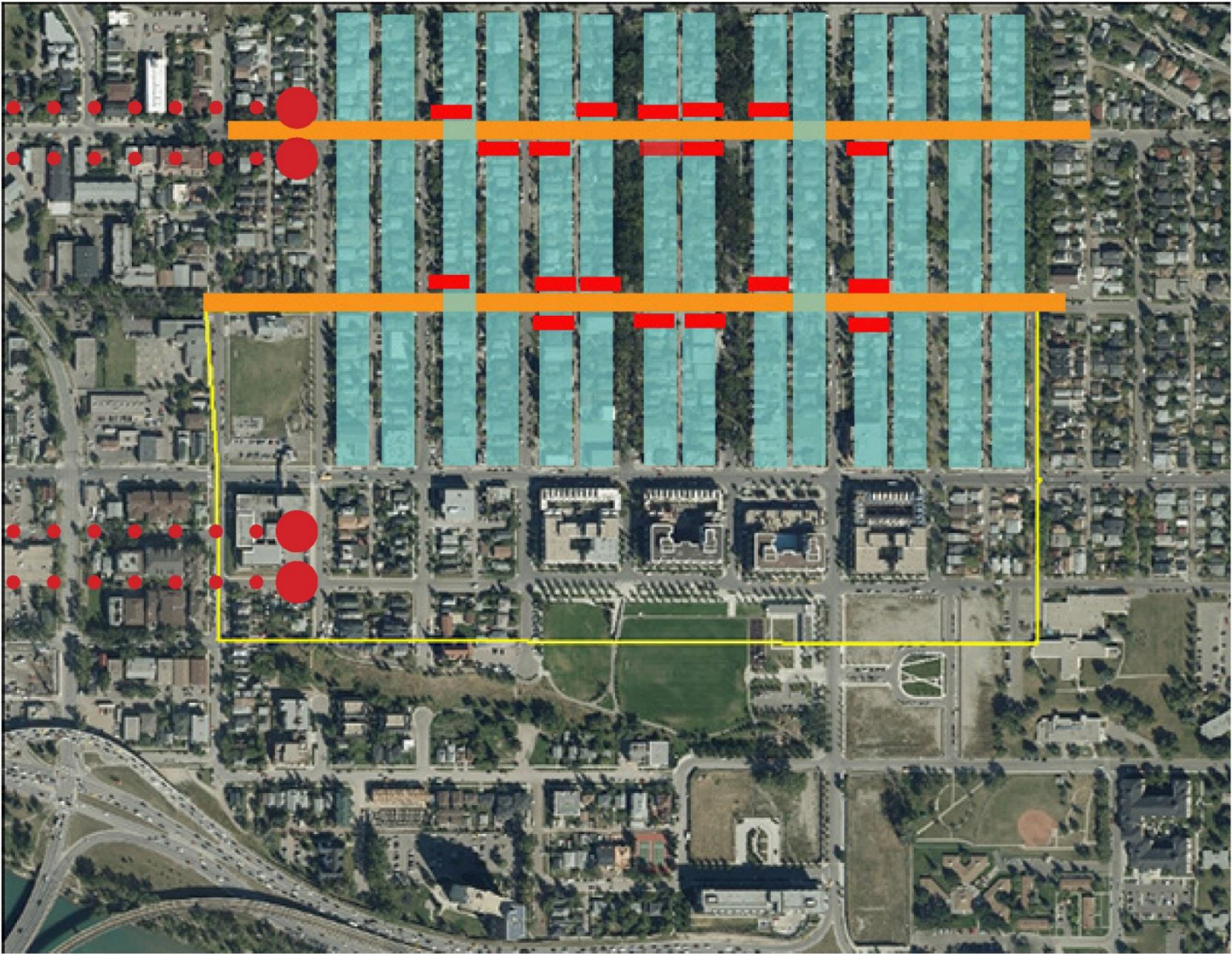
Comprehensive Project Overview

Urban Design - Activating the Street AND avenue

BRIDGELAND

GOOD URBAN DESIGN
ADDRESSES THE AVENUE
AND THE STREET

“MAIN STREETS” INITIATIVE
AREA



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NOTES

PROJECT
Bridgeland 4

DATE

SCALE:
NTS

PROJECT LOCATION
230 7A Street NE
438 8 Street NE

#SHEET TITLE

Comprehensive Project Overview

MATERIALS

Charcoal Metal Panel

Natural Concrete

Charcoal brick

Glass: large vertical windows

Copper Screen

Warm Grey Stucco

Off-white Stucco and Metal Cladding



438 8TH STREET



230 7A STREET

Charcoal Metal Panel

Natural Concrete

Light Brick

Glass: large vertical windows

Warm Grey Stucco

Off-white Stucco

Off-white Metal Cladding

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Calgary Alberta Canada
T2C 0L7
403.282.6082
davignonmartin.ca

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PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Comprehensive Project Overview



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PROJECT LOCATION

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438 8 Street NE

SHEET TITLE

Comprehensive Project Overview



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438 8 Street NE

PROJECT TITLE

Comprehensive Project Overview



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CALGARY, ALBERTA, CANADA
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403-253-6801
davignomartin.ca

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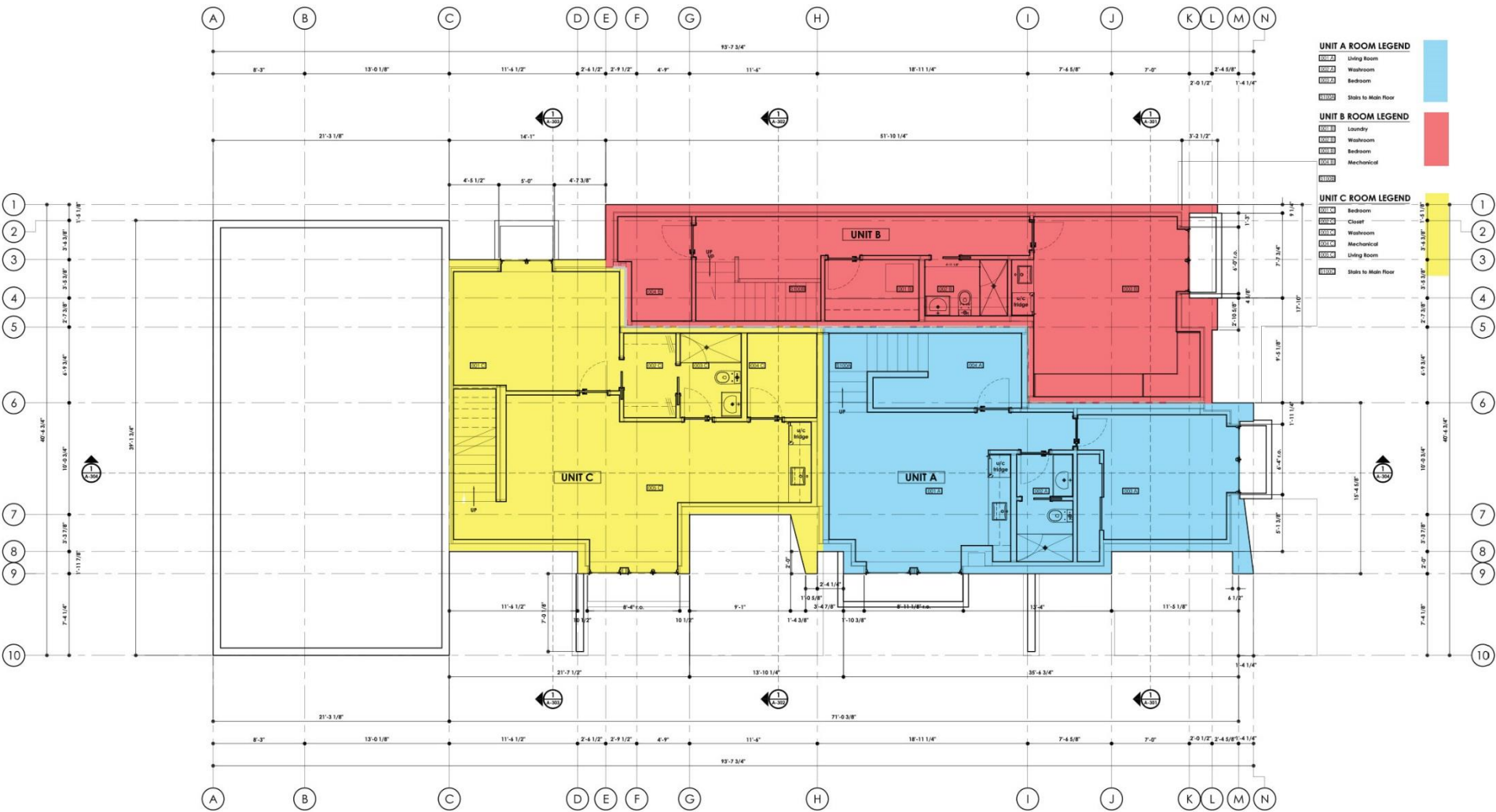
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Comprehensive Project Overview

BASEMENT



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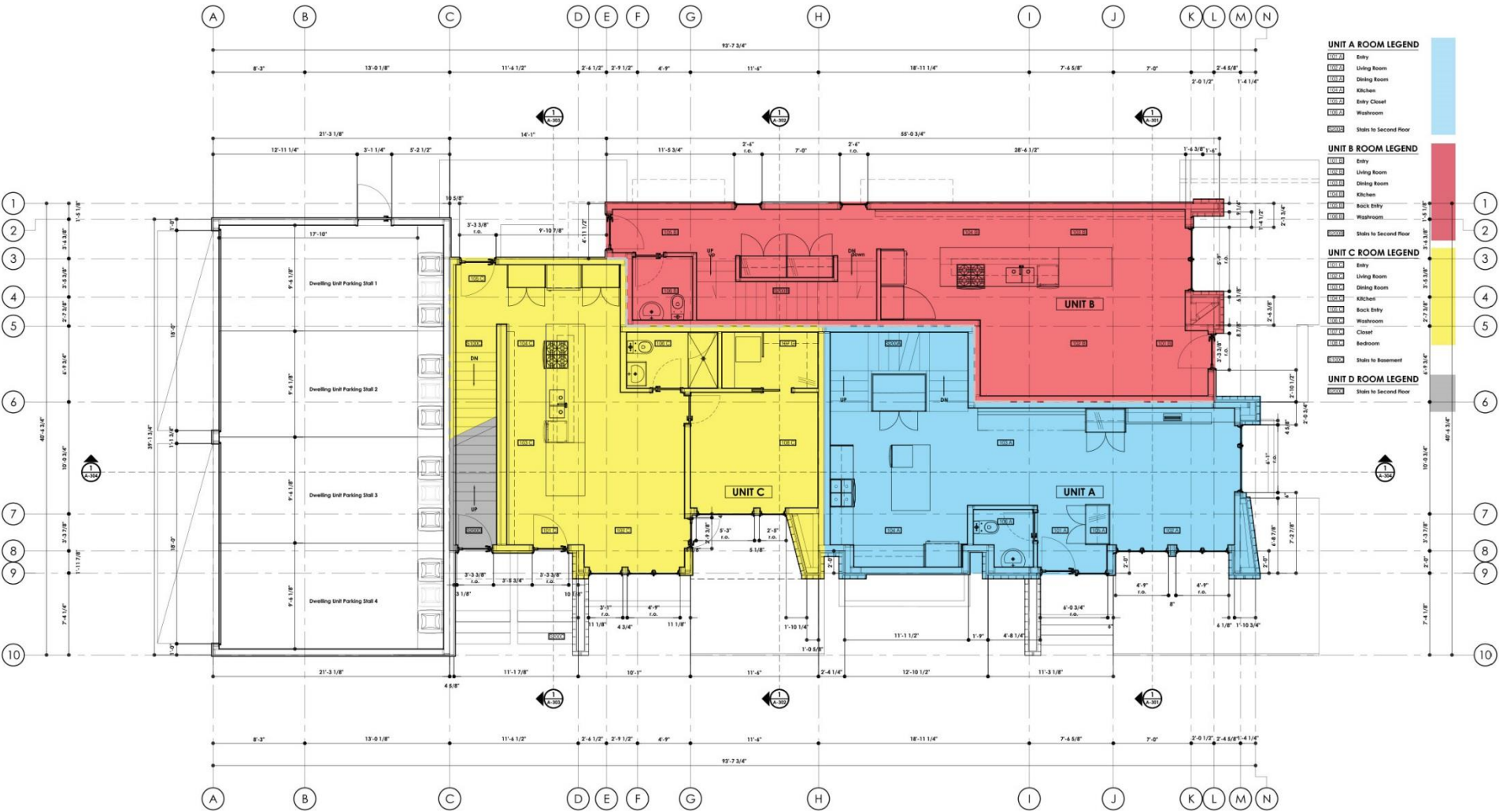
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Comprehensive Project Overview

MAIN FLOOR



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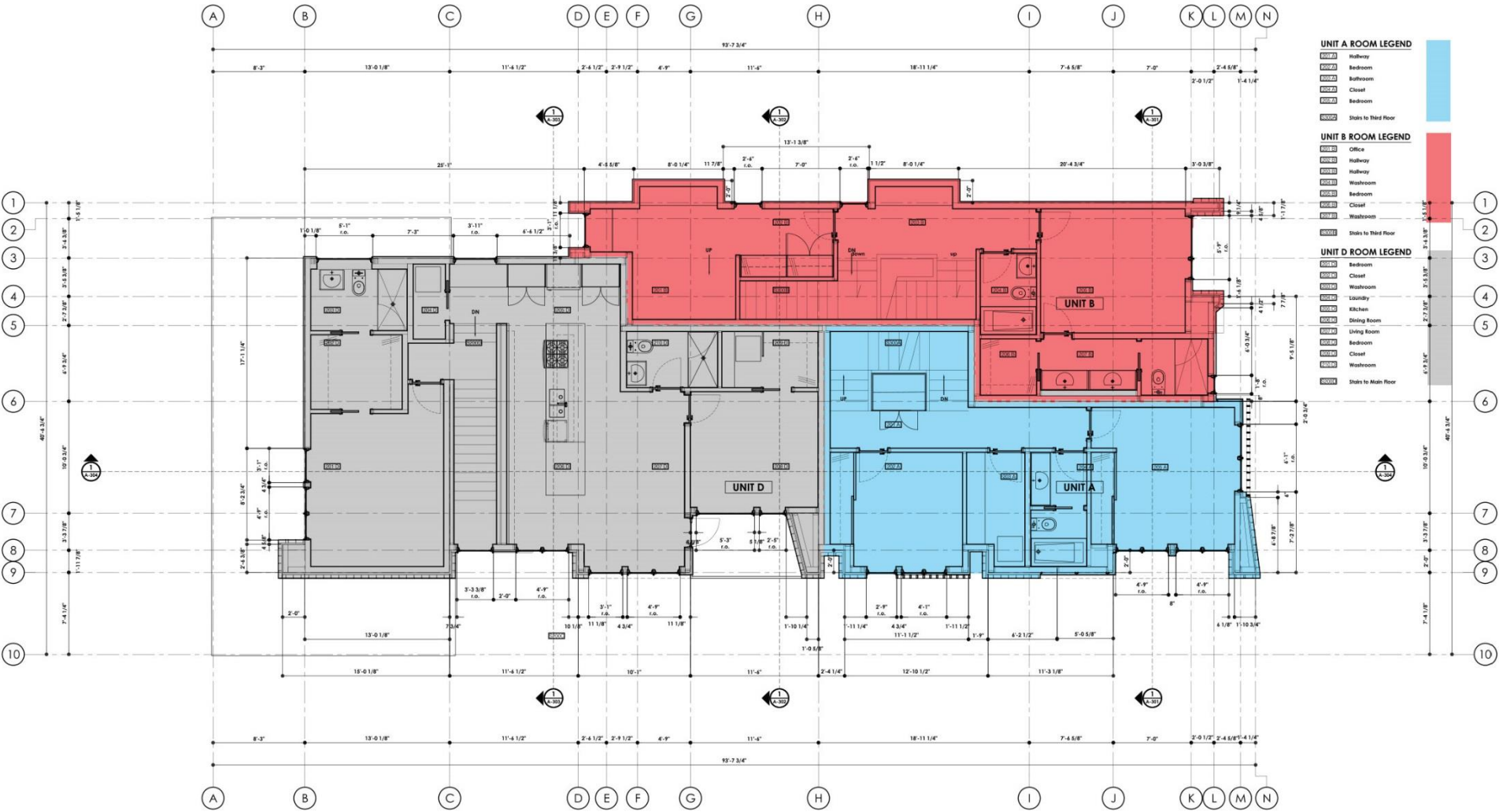
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SHEET TITLE

Comprehensive Project Overview

SECOND FLOOR



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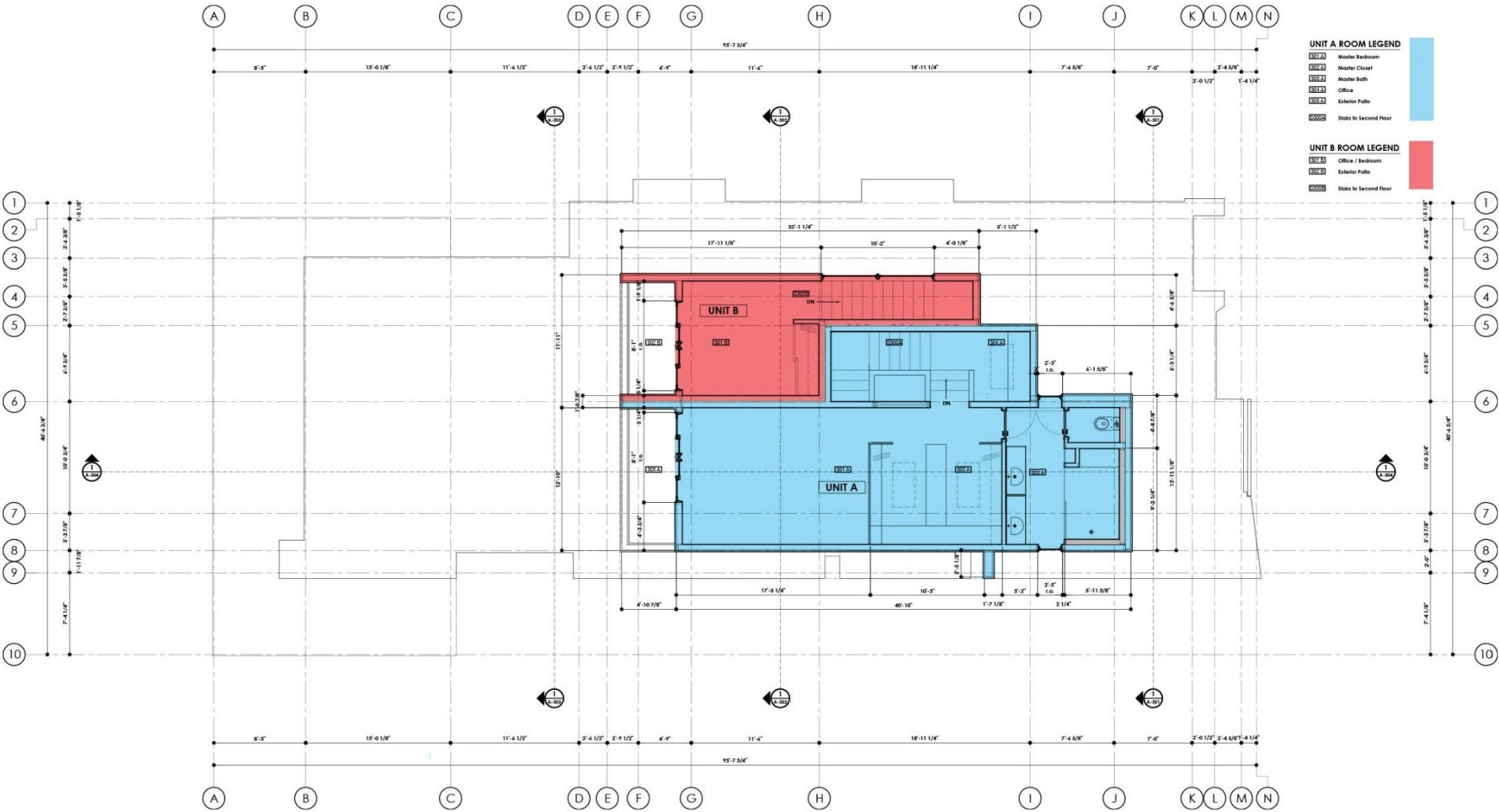
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PROJECT LOCATION
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438 8 Street NE

SHEET TITLE

Comprehensive Project Overview

THIRD FLOOR



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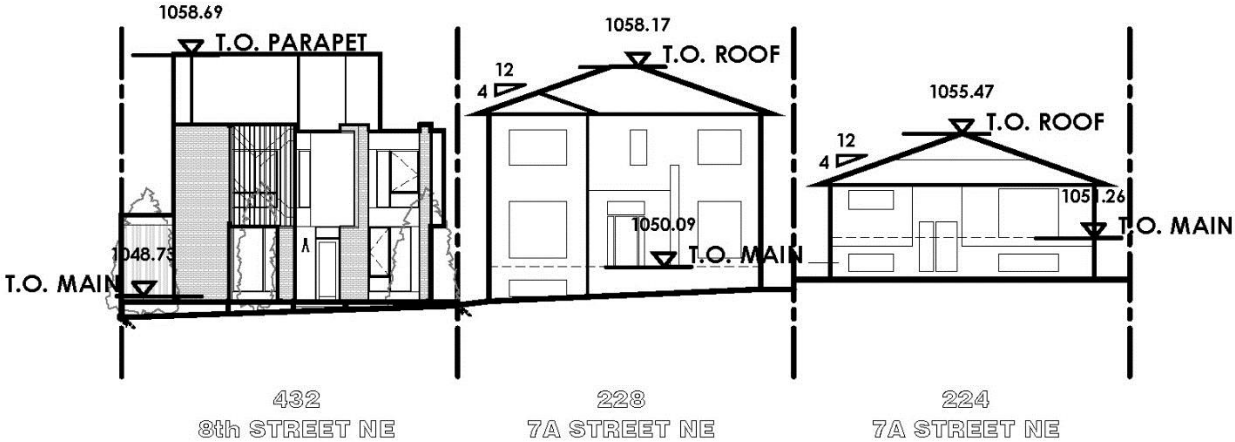
PROJECT LOCATION

230 7A Street NE
438 8 Street NE

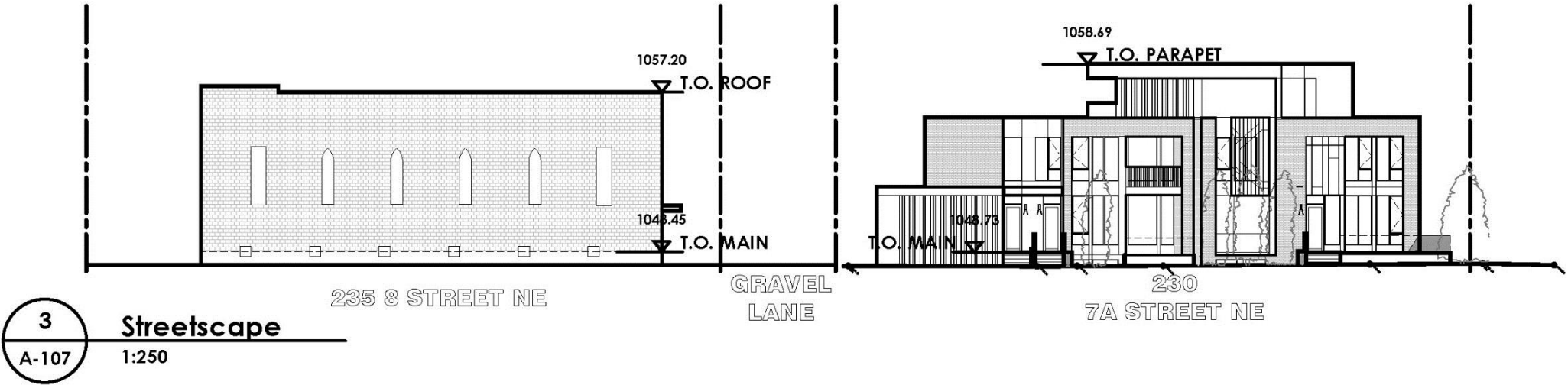
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Comprehensive Project Overview

230 7A Street Streetscapes



2 Streetscape
A-107 1:250



3 Streetscape
A-107 1:250

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PROJECT LOCATION
230 7A Street NE
438 8 Street NE

SHEET TITLE

Planning & Development Report to
Calgary Planning Commission
2018 July 18

ISC: UNRESTRICTED
CPC2018-0896
Page 1 of 5

Land Use Amendment in Thorncliffe (Ward 4) at 6327 Tregillus Street NW, LOC2018-0047

EXECUTIVE SUMMARY

This land use amendment application was submitted by Adele Margaret Caton on behalf of the landowners Chiu Ki Ma, Nuey Ling Wong and Wai Man Wong. The application proposes to change the land use designation of this property from Residential – Contextual One Dwelling (R-C1) District to Residential – Contextual One / Two Dwelling (R-C2) District to allow for the subdivision of the subject parcel with two separate land titles.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectare \pm (0.15 acres \pm) located at 6327 Tregillus Street NW (Plan 5799JK, Block 2, Lot 2) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One / Two Dwelling (R-C2) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

The applicant submitted a subdivision application (SB2017-0309) in 2017, which is currently on hold pending the review and approval of this land use amendment application. The purpose of the subdivision application is to split the current legal parcel, which contains a legal non-conforming Semi-detached Dwelling, into two separate legal parcels.

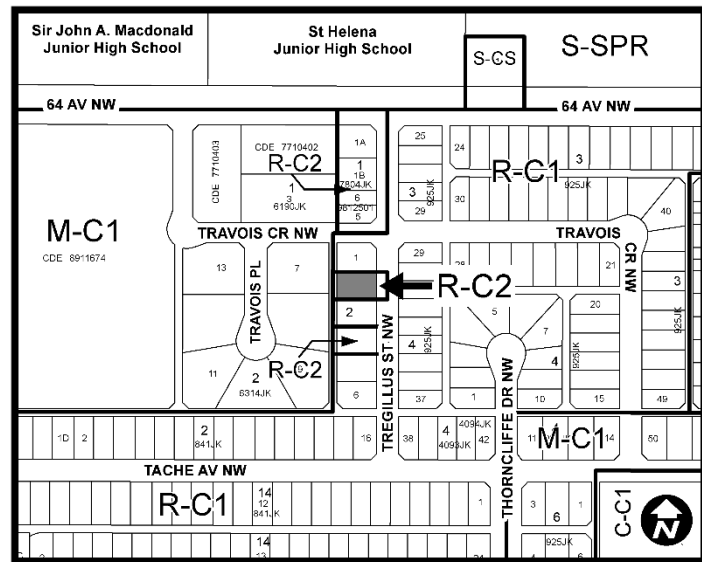
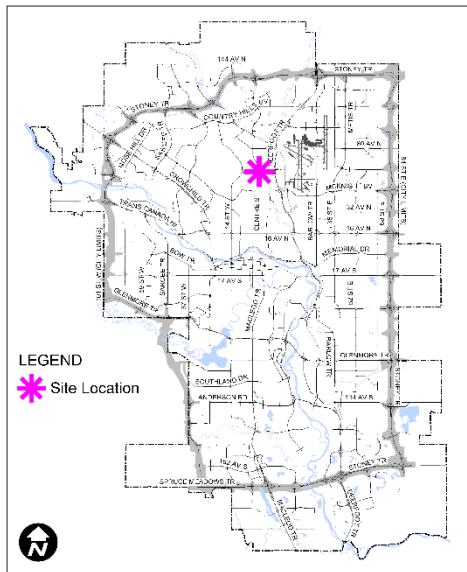
With the conversion of Land Use Bylaw 2P80 to 1P2007, the land use designation on the subject site was converted from (R-1) Residential Single-Detached District (2P80) to Residential – Contextual One Dwelling (R-C1) District (1P2007). A Semi-detached Dwelling is not an allowable use in the Residential – Contextual One Dwelling (R-C1) District.

**Planning & Development Report to
Calgary Planning Commission
2018 July 18**

**ISC: UNRESTRICTED
CPC2018-0896
Page 2 of 5**

**Land Use Amendment in Thorncliffe (Ward 4) at 6327 Tregillus Street NW,
LOC2018-0047**

Location Maps



Land Use Amendment in Thorncliffe (Ward 4) at 6327 Tregillus Street NW, LOC2018-0047

Site Context

The subject site is located in the northwest community of Thorncliffe, south-west of Centre Street N and 64 Avenue NW.

Surrounding development consists of low-density residential in the form of single detached and semi-detached dwellings with multi-residential development further to the west of the site. All existing development along the west side of Tregillus Street NW, which includes the subject site, consists of Semi-detached Dwellings designated either as Residential – Contextual One Dwelling (R-C1) or Residential – Contextual One / Two Dwelling (R-C2).

The site is approximately 21 metres (70 feet) by 30 metres (100 feet) in size and is developed with a semi-detached dwelling with vehicular access to 4 parking stalls from the rear lane. As identified in *Figure 1*, Thorncliffe has experienced a population decline from its peak in 1977.

Figure 1: Community Peak Population

Thorncliffe	
Peak Population Year	1977
Peak Population	11,379
2017 Current Population	8,474
Difference in Population (Number)	-2905
Difference in Population (Percent)	-26%

Source: *The City of Calgary 2017 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Thorncliffe](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposed land use amendment allows for the existing building to conform to Land Use Bylaw 1P2007 and allows for future potential redevelopment with the same built form as what is currently existing on the site. It is not the applicant's intention to redevelop the site at present.

Land Use District

The proposed Residential – Contextual One / Two Dwelling (R-C2) District is a low density residential district compatible with the existing surrounding low and multi-residential districts. The proposed district allows for a maximum of two residential units in the form of Duplex Dwellings or Semi-detached Dwellings and does not include Secondary or Backyard Suites.

Infrastructure

Transportation Networks

Vehicular access is available from the existing rear lane with 4 parking pads. A traffic impact assessment was not required as part of this application.

**Planning & Development Report to
Calgary Planning Commission
2018 July 18**

**ISC: UNRESTRICTED
CPC2018-0896
Page 4 of 5**

**Land Use Amendment in Thorncliffe (Ward 4) at 6327 Tregillus Street NW,
LOC2018-0047**

The subject site is situated within walking distance from several bus routes along 64 Avenue NW and 4 Street NW as well as bus rapid transit on Centre Street N.

Utilities and Servicing

Water, sanitary and storm sewer mains are available for the existing semi-detached dwelling and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online on the Planning and Development Map.

Following Calgary Planning Commission, notifications for the Public Hearing of Council will be posted on-site and mailed out to adjacent landowners.

The Thorncliffe/Greenview Community Association was circulated and expressed full support for the application. No objections against the application have been received by Administration.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory, 2014)

The site is located within the 'City, Town' area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. While the SSRP makes no specific reference to this site, the proposal is consistent with policies on Land Use Patterns.

Municipal Development Plan (Statutory, 2009)

The subject parcel is located within the 'Residential - Developed – Established area of the *Municipal Development Plan (MDP)*. The proposal is in line with the policies and intent of the MDP for Thorncliffe.

Local Area Plan

There is no local community plan for Thorncliffe.

Social, Environmental, Economic (External)

The proposed land use district allows for the existing housing type and the proposed subdivision of the subject site.

An environmental site assessment was not required for this application.

Planning & Development Report to
Calgary Planning Commission
2018 July 18

ISC: UNRESTRICTED
CPC2018-0896
Page 5 of 5

**Land Use Amendment in Thorncliffe (Ward 4) at 6327 Tregillus Street NW,
LOC2018-0047**

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no risks associated with the approval of this land use amendment application.

REASONS FOR RECOMMENDATION:

The proposed land use amendment conforms to the applicable policies of the *Municipal Development Plan* and allows for the existing semi-detached dwelling to conform to *Land Use Bylaw 1P2007*. The approval of this land use amendment would allow for the subdivision of the subject site into two legal parcels.

ATTACHMENT

1. Applicant's Submission

Applicant's Submission

Re: 6325/6327 Tregillus St NW Calgary, T2K 3T4

We bought this full duplex (semi-detached) in 1974. The zoning is R-CI.

We would now like to subdivide, split title on this semi-detached building with 2 living units. We have been advised by the city that in order to do so we must submit a Land Use Redesignation Application to change the use to R-C2. Once we have approval for that, we will proceed with our application to subdivide and split title.

There is no intention to redevelop the existing land or improvements.

Transportation Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0932
Page 1 of 7

9 Avenue SE Bridge Replacement - Inglewood (Ward 9) & East Village (Ward 7), 9 Avenue SE over Elbow River

EXECUTIVE SUMMARY

This report presents information on the 9 Avenue SE bridge replacement project in Inglewood and East Village. The existing bridge is past its useful service life and has numerous structural and functional deficiencies. A feasibility study was undertaken in 2016 and recommended replacement of the existing bridge.

The scope of the project includes the construction of a temporary bridge and roadway south of the existing alignment, removal of the existing bridge, construction of a new flood resilient bridge, and integration with the surrounding public realm.

The design of the replacement bridge is integrated to the context and prominence of the location and will accommodate all multi-modal needs. Public engagement has been a constant input throughout the design process.

Construction is anticipated to take place from Q4 2018 – Q4 of 2020.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission **RECEIVE AND ACCEPT** the 9 Avenue SE Bridge Replacement Project Report for information.

PREVIOUS COUNCIL DIRECTION

Council previously approved the plan for Investing in Mobility (2015-2024) in May 2014. Replacement of the 9 Avenue SE Bridge is included in Investing in Mobility under Lifecycle and Asset Management as a High Priority project. The approved budget for this project is \$23M, which includes \$12M from CMLC.

Transportation Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0932
Page 2 of 7

9 Avenue SE Bridge Replacement - Inglewood (Ward 9) & East Village (Ward 7), 9 Avenue SE over Elbow River

LOCATION MAP – AREA PLAN



9 Avenue SE Bridge Replacement - Inglewood (Ward 9) & East Village (Ward 7), 9 Avenue SE over Elbow River

BACKGROUND

The 9 Avenue SE Bridge over the Elbow River is located in Calgary's inner city and provides a gateway between the City's oldest neighbourhood, Inglewood, and the Downtown East Village. The Fort Calgary grounds and plaza connecting to the RiverWalk pathway system are located to the northwest. The historic Deane House property and a regional pathway are located to the northeast. A public City park, Statue Park, is located directly southeast of the bridge site. A vacant lot and the 7th Street SE connection to 9th Avenue SE are located southwest of the site. The CPR main line is located south of the bridge, while the Elbow River Traverse pedestrian bridge is located to the north.

The existing bridge is a single span steel truss bridge that was constructed in 1909, originally to accommodate a new streetcar system. The bridge was converted to vehicle use following removal of the streetcar system in the 1950's and has remained an important transportation connection within Calgary, with approximately 21,000 vehicles per day as of 2014. The bridge currently includes three (3) lanes of traffic (1 west bound, 2 east bound) with substandard width sidewalks on each side (1.8 metres south and 2.4 metres north).

Due to its deteriorated condition, the bridge is inspected annually by the City. There are also several functional deficiencies including vehicle weight restriction, narrow lane widths, vehicle height restriction, and insufficient flood clearance. A feasibility study was undertaken in 2016 and determined that a complete rehabilitation of the existing structure was not feasible due to the high costs and functional restrictions that would remain.

The new bridge is designed for a 100 year service life and would eliminate the functional restrictions that currently exist. The new bridge will be designed to accommodate up to four lanes of traffic and include 3.3 metre wide pathways on each side. The new bridge span will be raised in order to withstand a 1/100 year flood.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

Design and Aesthetics

Context sensitivity is an important aspect of the design. The existing site consists of a mix of park, river, historical buildings and railway elements. The design of the bridge aims to tie these diverse elements together while improving the main function of crossing the river. The design reflects the site's context and seeks compatible aesthetic outcomes representative of the bridge's historic function and the siting of the bridge as a gateway between the newly redeveloping Downtown East Village and the historic Inglewood community. The location and alignment of the new bridge provides for maximum opportunity for integration with the future plans in the area.

A steel arch bridge was developed as means to provide a design solution that represents a simple, yet distinct structure to the area. The use of exposed steel arches creates a prominent landmark at a scale appropriate for the site, uses a form that will be easily interpreted by the bridge users and features the use of structural steel reminiscent of the original bridge.

9 Avenue SE Bridge Replacement - Inglewood (Ward 9) & East Village (Ward 7), 9 Avenue SE over Elbow River

The selected bridge type is a single span steel through arch bridge that includes up to four (4) vehicle lanes for traffic and 3.3 metre multi-use pathways on each side. The bridge design will conform to the City's Access Design Standards and Sustainability guidelines. To minimize the environmental impact at the site and reflect the form of the existing bridge, the new bridge is being designed with no intermediate piers or permanent structures within the Elbow River.

The bridge is a key component of the design and functionality of the area including roads and pathways. The location and alignment of the new bridge provides for maximum opportunity to integrate with the future development plans in the area. Key design criteria includes:

- Safety;
- Accommodation of all modes of transportation;
- Accessibility for all users;
- Connectivity with existing and future pathways;
- Design and construction to current engineering standards;
- Sustainability;
- Cost and schedule, and;
- Aesthetics and context sensitive design;
- Commemoration of heritage.

Architectural support to the engineering team during development of bridge aesthetics and contextual sensitivity landed on a design alternative that is respectful of the old steel truss including form (through-type bridge), scale (similar length and height), and choice of materials (coated steel).

While the new 9 Avenue SE Bridge will remain respectful of the natural environment of this site and be functional for pedestrians, cyclists, transit and vehicles, the bridge will serve to further define the character of the site and will be a landmark between Inglewood and Downtown East Village.

A critical design requirement on all bridges is pedestrian safety and accessibility; this will be achieved by incorporating the following site-specific features:

- Pathway grades under 5 percent;
- Maximum 3:1 pathway side slopes;
- Keeping pedestrians safely separated from the Elbow River, CP Rail Line and vehicles through the use of railings and barriers;
- Way finding;
- Sufficient roadway and pathway lighting; and
- Maintaining clear sightlines.

The new bridge will also incorporate Crime Prevention Through Environmental Design (CPTED) guidelines in relation to the overall design and performance requirements.

9 Avenue SE Bridge Replacement - Inglewood (Ward 9) & East Village (Ward 7), 9 Avenue SE over Elbow River

Public realm integration is proposed at the Deane House, Statue Park, and Riverwalk Plaza to remediate impacts and tie the new bridge into the community and pathway network. The project will enhance and expand new landscaped areas at the bridge encouraging people to gather, pause to view the river and learn about the history of the old bridge through interpretive pieces and plaques developed using salvaged elements from the old bridge where possible.

Stakeholder Engagement, Research and Communication

Two public engagement sessions were held in March 2017 during the initial preliminary design phase with approximately 150 residents in attendance. For this engagement opportunity, The City presented different design forms and asked residents to share their thoughts on each of the conceptual bridge types based on the visual graphics and general information provided for each type. The engagement opportunity was also available online on the City's engage portal and 260 comments were received. The feedback from this in-person and online engagement was used to help determine the design of the new 9 Avenue Bridge.

Another public open house session was held in May 2018 during the detailed design phase with approximately 80 local residents attending. The open house included information on the final bridge design and construction staging, and requested input from the residents in developing the content of an interpretive plan to commemorate the old bridge.

In addition to open house sessions, public engagement has also included meetings with local community associations, both the Ward 7 and 9 Councillors, and adjacent businesses and residents throughout the design phase.

Information on the project is currently available on the City of Calgary's website. Project updates are posted regularly and the site will continue to be updated during construction with information for the public.

Pathway Connectivity, Accessibility

The proposed bridge includes 3.3 metre wide multi-use pathways along both sides of the bridge that will connect to the adjacent pathways, sidewalks, and RiverWalk. Pathway circulation is highlighted in Attachment 1 – Figure 1.

A minimum of 3 metre vertical clearance will be provided between the existing river pathway and the new bridge, and all pathway grading will meet current accessibility standards for mobility.

Visual sightline opportunities to other proposed key features such as the Elbow River are also considered. In addition, the design for the structure provides a unique experience for the user with entry features that tie to the adjacent landscaping and pathways in an exciting and pleasing way.

9 Avenue SE Bridge Replacement - Inglewood (Ward 9) & East Village (Ward 7), 9 Avenue SE over Elbow River

Heritage Commemoration

An interpretive plan that tells the bridge users the story of the original bridge has been developed. The story will be told through a combination of information plaques, photos, graphics and other interpretive elements. The existing truss will be salvaged and kept at a City yard for reuse of the old bridge elements where possible. A total of seven (7) locations on the project site near the bridge have been identified to feature interpretive elements. See Figure 2 – Attachment 1.

Strategic Alignment

The project is in alignment with the Calgary Transportation Plan – Appendix B – Principles and design considerations for river crossings:

- Principle 1: Demonstrated need for the crossing; and
- Principle 2: Advanced planning for appropriate site based on all relevant factors.

The project is in alignment with a number of the City's Action Plan Priorities (2015-2018) including:

N1. Keep communities safe by meeting and maintaining standards for crime prevention, fire response and enforcement. Crime Prevention through Environment Design is incorporated into the project. Pathways will include improved lighting and sightlines to increase pedestrian safety.

N2. Build resiliency to flooding. The new bridge is designed to improve the flood resiliency of the area and will be located above the waterline in a 1/100 year flood.

N3. Enhance The City's capacity and resiliency to prepare for and respond to pandemic, natural disasters and emergency situations. The new bridge and roadway will be able to accommodate emergency vehicles and will be a critical route during flood events.

N9. Provide great public spaces and public realm improvements across the city to foster opportunity for well used public spaces and places for citizen connections and urban vitality. The project will maintain and include new landscaped areas at the bridge encouraging people to gather, pause to view the river and learn about the history of the old bridge through the interpretive plan.

Social, Environmental, Economic (External)

The new bridge aligns with the City's sustainability policies and will result in long term social, environmental, and economic benefits. The new bridge promotes healthy lifestyles by significantly improving the pedestrian and cycling infrastructure and their connections to the adjacent pathways and sidewalks.

The new bridge is designed to last 100 years and incorporates the use of durable, sustainable materials which will reduce initial and ongoing costs and provide long term value to the City.

Transportation Report to
Calgary Planning Commission
2018 July 26

ISC: UNRESTRICTED
CPC2018-0932
Page 7 of 7

9 Avenue SE Bridge Replacement - Inglewood (Ward 9) & East Village (Ward 7), 9 Avenue SE over Elbow River

REASON(S) FOR RECOMMENDATION(S):

In 2016, the City undertook a feasibility study of the existing 9 Avenue SE bridge over the Elbow River and determined that due to its overall deteriorating condition and functional deficiencies, the structure had reached the end of its service life and required replacement.

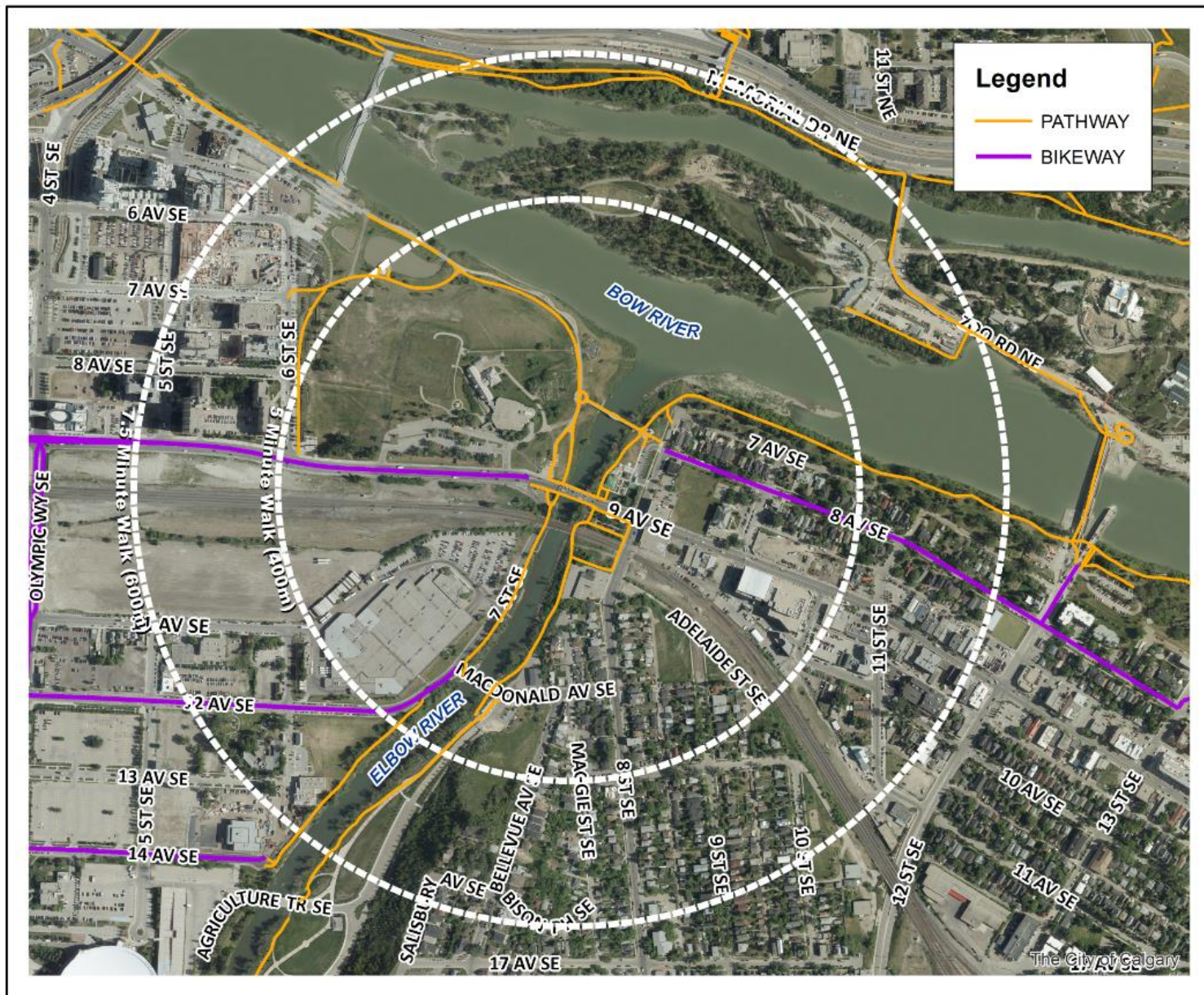
The analysis conducted at the project outset confirmed that replacement of the existing bridge would provide best value to The City.

ATTACHMENTS

1. Figures 1 to 5
2. Urban Design Review Panel Comments and Responses

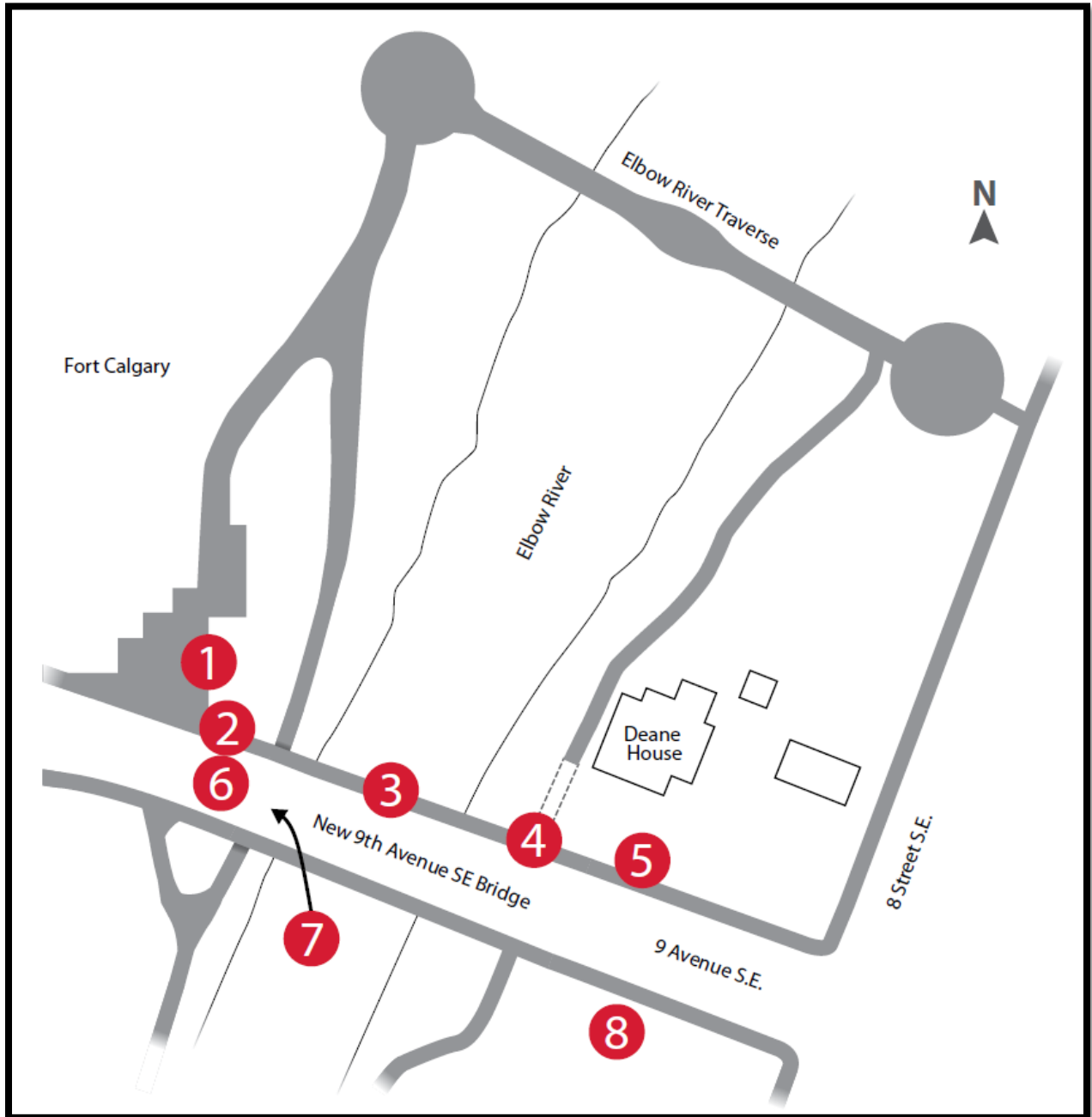
Figures 1 – 5

Figure 1: Site Plan and Pathway Connectivity



Figures 1 – 5

Figure 2: Proposed Heritage Interpretation Locations



Figures 1 – 5

Figure 3: Bridge elevation – river pathway user



Figures 1 – 5

Figure 4: Pathway perspective at night with lighting



Figure 5: Pathway user looking east



Urban Design Review Panel Comments

Date:	April 25, 2018	
Time:	1:45 pm	
Panel Members:	Present:	Absent:
	Chad Russill (chair)	Janice Liebe
	Robert LeBlond	Bruce Nelligan
	Terry Klassen	Jack Vanstone
	Chris Hardwicke	Yogeshwar Navagrah
	Eric Toker	Gary Mundy
	Glen Pardoe	
Advisor:	David Down, Chief Urban Designer	
Application number:	M2018-	
Municipal address:	9 th Avenue bridge	
Community:	Inglewood	
Project description:	Replacement of the 9 th Avenue bridge	
Review:	first	
File Manager:	Evan Fer	
City Wide Urban Design:	Afrah Rayes	
Applicant:	City of Calgary	
Architect:		
Owner:		
Ranking:		

Summary

Note: As this project is not a typical building project, the panel's primary comments are noted below in the summary section.

The Applicant presented the current design plans and 60% roadway designs for the bridge, abutments and 9th Avenue approaches on each side of the Elbow River.

Although UDRP generally supports the overall intent to improve the bridge crossing and overall experience for all modes of users, there was some concern about the design inasmuch as the retention of the character and feel of the old bridge. It was understood and recognized that the rehabilitation of the existing bridge was considered, but the Panel was not convinced that the heritage aspects of the bridge had been adequately reviewed and considered prior to moving ahead with the proposed design.

Specifically, UDRP discussed with the Applicant, the following:

- 1) Abutment Walls and Underpasses – The two bridge abutments are substantial in size and could be used as a canvass for additional art installations. There was also some suggestion from the Panel that the landscape experience that follows the bike paths from grade to under the bridge could be tied into the abutment wall areas to create more of an experience when passing through the area. The Panel suggested exploring opportunities to extend the length of the bridge structure to provide a wider passage for bicycles and pedestrians on one or perhaps both sides of the river, and to enhance the River Walk experience. It is understood the Applicant is considering this, however the current package does not reflect the approach adequately.
- 2) Context of Project – There are a number of other projects underway in the area, including River Walk. Although it is understood that the bridge project is a stand-alone exercise, the Panel suggested that the presentation was lacking context inasmuch as its relationship to other projects underway in the area. Given that the bridge is an important piece of civic infrastructure the Panel requests a range of visualizations of the bridge in context from significant viewpoints such as Fort

Urban Design Review Panel Comments

Calgary, Inglewood, the confluence of the Elbow and the Bow, and the MacDonald Avenue Bridge.

- 3) Character – The existing bridge is 110 years old and has a very unique industrial “stitched together” feel to it. The Panel felt that the proposed replacement structure seemed to lack character, and the design looked a bit impersonal and needed to be a bit more visually welcoming as the gateway to the Inglewood Community, or to Downtown (depending upon which way a person is travelling).
- 4) Heritage Value – The Panel felt that despite the statements by the Applicant regarding the issues with re-habilitating the bridge (ie. it would cost the same, but would not provide anything beyond a 30 year lifespan, and would not improve safety or flood mitigation), there should be more effort made to retain the original structure, in whole or in part. There was also some suggestion that the design as proposed does not adequately represent the original structure and therefore misses the point on the heritage aspect of the program.
- 5) Bridge Deck Design – The Panel was concerned about the sidewalk areas on the bridge deck and the raised areas adjacent. The thought was that these would have the propensity to collect garbage and dirt, requiring considerable resources to maintain. The Panel encouraged the Applicant to review how best to accommodate the necessary structural elements in this area.
- 6) 9th Avenue Cross Sections – The Panel noted the need to ensure that the Applicant be aware of and liaise with the CMLC regarding the bike network study that is underway, as there may be changes to how bikes are accommodated on 9th Avenue west of the bridge, and these changes could affect the laning and design of the roadway west of the bridge. As well, questions were raised about the adequacy of the lane widths through the curve on 9th Avenue vis a vis the passage of transit vehicles (3.3 metre lanes).
- 7) Pedestrian Realm – The Panel suggested that while it applauded the scale and coverage of the pedestrian facilities in the area, there may be merit to celebrate the connection on one side (North) as opposed to trying to accommodate both sides. Given immediate context primarily at the SW quadrant and influence of 7th Street, pedestrian connectivity on this side may not be justified.

Applicant Response

May 29, 2018

- 1) Art installations for the abutment are being considered. The new bridge will be longer to provide a wider pathway under the bridge, and it is understood that CMLC will be extending the RiverWalk in this area. The river pathway reconstruction is not within the scope of this project, and therefore not shown in this package.
- 2) The project team is coordinating with the other projects in the area, however the bridge replacement project is in the detailed design phase while the other projects in the area (Mainstreets, RiverWalk, Greenline etc.) are currently in conceptual stages and designs or visuals are not available.
- 3) At the time of construction of the original Parker Camelback bridge, the riveted gusset plate design and construction techniques utilized the most innovative technology offered in the early 1900's – a bridge that was contemporary to its era. This resulted in what UDRP has referred to as a “stitched together” feel.

Urban Design Review Panel Comments

The proposed design of the new arch bridge is implementing advanced construction techniques that are reflective of today's contemporary times. While different visually, the knife plated pin connections of the hanger detailing is symbolic to the riveted gusset plate connections, but reinterpreted and celebrated in a contemporary expression.

The selection of a through superstructure for the new bridge recognizes and celebrates the importance of the crossing location. The arch symbolizes a natural "gateway" structure, thus creating an entry threshold to the adjacent Inglewood and Ramsay communities.

- 4) The condition of the old truss bridge is poor and not suitable or safe for re-use. Retaining the original structure would not improve the functional deficiencies or meet flood resiliency requirements, and the substantial initial and ongoing costs would not provide good value to Calgarians.

The proposed bridge design was not intended to copy or imitate the old bridge. The old truss was state of the art 100 years ago, however this style of bridges are now becoming obsolete. The proposed design represents a modern design for the next 100 years.

A Heritage Interpretation plan is being developed to honor the old truss and will include reuse of the old bridge where possible.

- 5) The team is reviewing options to mitigate build up of dirt or garbage in these areas.
- 6) The team is coordinating with CMLC on the bike strategy west of the new bridge.

3.3m lane widths were determined to be adequate for this corridor, even though they are slightly below the 3.5m standard. 3.3m lane widths were selected and have been used successfully in Calgary and other major City's in North America as an effective traffic calming measure, which is a key principle of the City's complete streets policy.

- 7) As the new bridge is designed for 100 years and future expansion would be difficult with this type of bridge, the team believes accommodating users on both sides of the bridge is important to allow for flexibility in the future to accommodate different demands. The plan is also consistent with the Mainstreets plan and future connectivity with the east river pathway and RiverWalk.