



AGENDA

CALGARY PLANNING COMMISSION

April 16, 2026, 1:00 PM
IN THE COUNCIL CHAMBER

Members

Manager C. Lee, Chair
Director T. Mahler, Vice-Chair
Commissioner C. Boechler
Commissioner L. Campbell-Walters
Commissioner R-M. Damiani
Commissioner C. Hardwicke
Commissioner N. Hawryluk
Commissioner B. Montgomery
Commissioner M. Pink
Commissioner S. Remtulla
Commissioner S. Small
Commissioner K. Wagner

SPECIAL NOTES:

*Members of the public are encouraged to follow Council and Committee meetings using the live stream:
[Calgary.ca/WatchLive](https://calgary.ca/WatchLive)*

Commission Members may be participating remotely.

1. CALL TO ORDER
2. OPENING REMARKS
3. CONFIRMATION OF AGENDA
 - 3.1 DECLARATIONS - CONFLICT OF INTEREST
4. CONFIRMATION OF MINUTES
 - 4.1 Minutes of the Regular Meeting of the Calgary Planning Commission, 2026 March 26
5. CONSENT AGENDA

5.1 DEFERRALS AND PROCEDURAL REQUESTS

None

5.2 Land Use Amendment in Killarney/Glengarry (Ward 8) at 3627 Kildare Crescent SW, LOC2026-0016, CPC2026-0302

5.3 Land Use Amendment in Killarney/Glengarry (Ward 8) at 2823 – 30 Street SW, LOC2026-0009, CPC2026-0290

6. POSTPONED REPORTS

(including related/ supplemental reports)

None

7. ITEMS FROM OFFICERS, ADMINISTRATION AND COMMITTEES

7.1 DEVELOPMENT ITEMS

7.1.1 Development Permit in Medicine Hill (Ward 6) at 1152 Na'a Drive SW, DP2025-04914, CPC2026-0230

7.2 PLANNING ITEMS

7.2.1 Land Use Amendment in Upper Mount Royal (Ward 8) 1911 – 11 Street SW, LOC2025-0151, CPC2026-0204

7.2.2 Land Use Amendment in Banff Trail (Ward 7) at 2471 – 23 Street NW, LOC2025-0171, CPC2026-0250

7.2.3 Land Use Amendment in Saddle Ridge (Ward 5) at #1000, 4715 – 88 Avenue NE, LOC2025-0147, CPC2026-0010

7.2.4 Outline Plan, Policy and Land Use Amendment in Stonegate Landing (Ward 5) at multiple addresses, LOC2024-0171, CPC2026-0020

7.3 MISCELLANEOUS ITEMS

None

8. URGENT BUSINESS

9. CONFIDENTIAL ITEMS

9.1 ITEMS FROM OFFICERS, ADMINISTRATION AND COMMITTEES

None

9.2 URGENT BUSINESS

10. BRIEFINGS

None

11. ADJOURNMENT



MINUTES

CALGARY PLANNING COMMISSION

**March 26, 2026, 1:00 PM
ENGINEERING TRADITIONS COMMITTEE ROOM**

PRESENT: Manager C. Lee, Chair
Director T. Mahler, Vice-Chair
Commissioner C. Boechler
Commissioner L. Campbell-Walters
Commissioner C. Hardwicke
Commissioner N. Hawryluk
Commissioner B. Montgomery
Commissioner S. Small
Commissioner K. Wagner

ABSENT: Commissioner R-M. Damiani
Commissioner M. Pink
Commissioner S. Remtulla

ALSO PRESENT: A/Principal Planner S. Jones
Legislative Specialist A. Lennox
Legislative Specialist C. Nelson

1. CALL TO ORDER

Chair Lee called the meeting to order at 1:00 p.m.

ROLL CALL

Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, Commissioner Wagner, Director Mahler, and Manager Lee

Absent from Roll Call: Commissioner Damiani, Commissioner Pink, and Commissioner Remtulla

2. OPENING REMARKS

Chair Lee provided opening remarks and a traditional land acknowledgment.

3. CONFIRMATION OF AGENDA

Commissioner Small declared a conflict of interest and abstained from discussion and voting with respect to Report CPC2026-0166.

Commissioner Small left the meeting at 1:03 p.m. and returned at 1:08 p.m. after the vote was declared.

Moved by Commissioner Wagner

That the agenda for today's meeting be amended by adding Item 8.1, File Report LOC2025-0178 (CPC2026-0123), CPC2026-0319, as an Item of Urgent Business.

For: (7): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, and Commissioner Wagner

MOTION CARRIED

Moved by Commissioner Wagner

That the Agenda for today's meeting be amended by moving Items 7.2.3 Land Use Amendment in Hillhurst (Ward 7) at 207 and 211 – 14 Street NW, LOC2025-0195, CPC2026-0256 and 7.2.11 Land Use Amendment in Killarney/Glengarry (Ward 8) at multiple addresses, LOC2025-0234, CPC2026-0166 onto the Consent Agenda.

For: (7): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, and Commissioner Wagner

MOTION CARRIED

Moved by Director Mahler

That the Agenda for the 2026 March 26 Regular Meeting of the Calgary Planning Commission be confirmed, **as amended**.

For: (7): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, and Commissioner Wagner

MOTION CARRIED

3.1 DECLARATIONS - CONFLICT OF INTEREST

Commissioner Campbell-Walters declared a conflict of interest with respect to Item 5.2.

Commissioner Small declared a conflict of interest with respect to Items 7.2.3, 7.2.8, and 7.2.11.

Commissioner Boechler declared a conflict of interest with respect to Item 7.2.6.

4. CONFIRMATION OF MINUTES

4.1 Minutes of the Regular Meeting of the Calgary Planning Commission, 2026 March 12

Moved by Commissioner Boechler

That the Minutes of the 2026 March 12 Regular Meeting of the Calgary Planning Commission be confirmed.

For: (8): Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, Commissioner Wagner, and Director Mahler

MOTION CARRIED

5. CONSENT AGENDA

Commissioner Small declared a conflict of interest and abstained from discussion and voting with respect to Reports CPC2026-0256 and CPC2026-0166.

Commissioner Small left the meeting at 1:09 p.m. and returned at 1:15 p.m. after the vote was declared.

Moved by Commissioner Hardwicke

That the Consent Agenda be approved as follows, as corrected:

5.1 DEFERRALS AND PROCEDURAL REQUESTS

None

5.3 Land Use Amendment in Killarney/Glengarry (Ward 8) at 3036 and 3040 – 29 Street SW, LOC2025-0179, CPC2026-0252

Revised Attachment 1 was distributed with respect to Report CPC2026-0252.

7.2.3 Land Use Amendment in Hillhurst (Ward 7) at 207 and 211 – 14 Street NW, LOC2025-0195, CPC2026-0256

7.2.11 Land Use Amendment in Killarney/Glengarry (Ward 8) at multiple addresses, LOC2025-0234, CPC2026-0166

For: (6): Director Mahler, Commissioner Boechler, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, and Commissioner Wagner

MOTION CARRIED

5.2 Land Use Amendment in Hillhurst (Ward 7) at 1601 Bowness Road NW, LOC2025-0248, CPC2026-0219

Commissioner Campbell-Walters declared a conflict of interest and abstained from discussion and voting with respect to Report CPC2026-0219.

Commissioner Campbell-Walters left the meeting at 1:15 p.m. and returned at 1:22 p.m. after the vote was declared.

Moved by Commissioner Hawryluk

That with respect to Report CPC2026-0219, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.07 hectares ± (0.17 acres ±) at 1601 Bowness Road NW (Plan 5151O, Block 19, Lots 57 and 58) from Mixed Use – General (MU-1f1.0h13) District to Residential – Grade-Oriented Infill (R-CG) District.

For: (4): Director Mahler, Commissioner Hardwicke, Commissioner Hawryluk, and Commissioner Montgomery

Against: (2): Commissioner Boechler, and Commissioner Wagner

MOTION CARRIED

Commission then dealt with Item 7.2.1

Commission returned to this Item following Item 7.2.1. in order to record Commissioner Small's vote.

Commissioner Campbell-Walters left the meeting at 1:34 p.m. and returned at 1:36 p.m. after the vote was declared.

Moved by Commissioner Hawryluk

That with respect to Report CPC2026-0219, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.07 hectares ± (0.17 acres ±) at 1601 Bowness Road NW (Plan 5151O, Block 19, Lots 57 and 58) from Mixed Use – General (MU-1f1.0h13) District to Residential – Grade-Oriented Infill (R-CG) District.

For: (6): Director Mahler, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

Against: (1): Commissioner Boechler

MOTION CARRIED

Commission then dealt with Item 7.2.2.

6. POSTPONED REPORTS

None

7. ITEMS FROM OFFICERS, ADMINISTRATION AND COMMITTEES

7.1 DEVELOPMENT ITEMS

None

7.2 PLANNING ITEMS

7.2.1 Land Use Amendment in Westwinds (Ward 5) at 3633 Westwinds Drive NE, LOC2025-0210, CPC2026-0199

Commission dealt with this Item following the initial vote for Item 5.2.

A presentation entitled "LOC2025-0210 / CPC2026-0199 Land Use Amendment" was distributed with respect to Report CPC2026-0199.

Moved by Commissioner Boechler

That with respect to Report CPC2026-0199, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 8.44 hectares \pm (20.86 acres \pm) located at 3633 Westwinds Drive NE (Plan 0412252, Block 6, Lot 4) from Commercial – Regional 1 f0.2 (C-R1 f0.2) District to Commercial – Regional 3 f0.25h18 (C-R3 f0.25h18) District.

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

Commission then returned to Item 5.2

7.2.2 Outline Plan and Land Use Amendment in Cityscape (Ward 5) at 10011 – 52 Street NE, LOC2025-0113, CPC2026-0271

This Item was dealt with following the second vote on Item 5.2.

The following documents were distributed with respect to Report CPC2026-0271:

- Revised Attachment 1; and
- A presentation entitled "LOC2025-0113 / CPC2026-0271 Outline Plan and Land Use Amendment".

Jack Moddle and Joel van Huizen (applicants) answered questions of Commission with respect to Report CPC2026-0271.

Moved by Commissioner Boechler

That with respect to Report CPC2026-0271, the following be approved:

That Calgary Planning Commission:

1. As the Council-designated Approving Authority, approve the proposed outline plan located at 10011 – 52 Street NE (Portion of SE1/4 Section 22-25-29-4) to subdivide 7.89 hectares \pm (19.50 acres \pm) with conditions (Attachment 3).

That Calgary Planning Commission recommend that Council:

2. Give three readings to the proposed bylaw for the redesignation of 0.36 hectares \pm (0.89 acres \pm) located at 10011 – 52 Street NE (Portion of SE1/4 Section 22-25-29-4) from Residential – Low Density Mixed Housing (R-G) District to a Direct Control (DC) District, to accommodate low-density residential development, with guidelines (Attachment 2).

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

7.2.3 Land Use Amendment in Hillhurst (Ward 7) at 207 and 211 – 14 Street NW, LOC2025-0195, CPC2026-0256

This Item was dealt with at the Consent Agenda.

7.2.4 Land Use Amendment in Haskayne (Ward 1) at 6600 and 6815 – 133 Street NW, LOC2025-0070, CPC2026-0221

The following documents were distributed with respect to Report CPC2026-0221:

- Revised Cover Report;
- Revised Attachment 1; and
- A presentation entitled "LOC2025-0070 / CPC2026-0221 Land Use Amendment"

Jack Moddle and Elton Ma (applicants) answered questions of Commission with respect to Report CPC2026-0221.

Moved by Commissioner Montgomery

That with respect to **Revised** Report CPC2026-0221, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 3.91 hectares ± (9.67 acres ±) located at 6600 and 6815 – 133 Street NW (Portion of Plan 5126JK, Block D; Portion of Plan 1911645, Block 1, Lot 1) from Commercial – Community 1 (C-C1) District to Multi-Residential – Low Profile (M-1) District and Special Purpose – Community Service (S-CS) District.

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

7.2.5 Land Use Amendment in Manchester (Ward 9) at 5325 – 1A Street SW, LOC2025-0066, CPC2026-0025

The following documents were distributed with respect to Report CPC2026-0025:

- Revised Attachment 1; and
- A presentation entitled "LOC2025-0066/ CPC2026-0025 Land Use Amendment".

Amaan Hameed, Gilberto Ancheta, Lenin Cadang and Mandy Pontalera (applicants) answered questions of Commission with respect to Report CPC2026-0025.

Moved by Commissioner Small

That with respect to Report CPC2025-0025, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.06 hectares ± (0.14 acres ±) located at 5325 – 1A Street SW (Plan 5454AC, Block 12, Lots 32 and 33) from Industrial Redevelopment (I-R) District to Commercial – Corridor 2 f3.5h18 (C-COR 2 f3.5 h18) District.

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

7.2.6 Land Use Amendment in Beltline (Ward 8) at 310 – 15 Avenue SW, LOC2025-0242, CPC2026-0258

Commissioner Boechler declared a conflict of interest and abstained from discussion and voting with respect to Report CPC2026-0258.

Commissioner Boechler left the meeting at 2:34 p.m. and returned at 2:44 p.m. after the vote was declared.

A presentation entitled "LOC2025-0242 / CPC2026-0258 Land Use Amendment" was distributed with respect to Report CPC2026-0258.

Moved by Commissioner Montgomery

That with respect to Report CPC2026-0258, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.26 hectares ± (0.64 acres ±) located at 310 – 15 Avenue SW (Condominium Plan 2511573, Units 1 to 307) from Direct Control (DC) District and Centre City Multi-Residential High Rise District (CC-MH) to Centre City Multi-Residential High Rise Support Commercial District (CC-MHX).

For: (7): Director Mahler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

7.2.7 Land Use Amendment in Beltline (Ward 8) at multiple addresses, LOC2025-0254, CPC2026-0233

A presentation entitled "LOC2025-0254 / CPC2026-0233 Land Use Amendment" was distributed with respect to Report CPC2026-0233.

Moved by Commissioner Small

That with respect to Report CPC2026-0233, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.18 hectares ± (0.44 acres ±) located at 1302 – 14 Avenue SW and 721 – 13 Avenue SW (Plan 2311640, Block 97, Lot 55; Plan A1, Block 91, Lots 10, 11, a portion of Lot 9 and a portion of Lot 12) from Centre City Multi-Residential High Rise District (CC-MH) and Direct Control (DC) District to Direct Control (DC) District to accommodate heritage density transfer, with guidelines (Attachment 2).

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

7.2.8 Policy and Land Use Amendment in Downtown Commercial Core (Ward 7) at 728 – 4 Avenue SW, LOC2025-0137, CPC2026-0145

Commissioner Small declared a conflict of interest and abstained from discussion and voting with respect to Report CPC2026-0145.

Commissioner Small left the meeting at 2:54 p.m. and returned at 3:15 p.m. after the vote was declared.

A clerical correction was noted in the header of Report CPC2026-0145, in the title, by deleting the words "Eau Claire" and by substituting with the words "Downtown Commercial Core".

The following documents were distributed with respect to Report CPC2026-0145:

- Revised Cover Report;
- Revised Attachment 1; and
- A presentation entitled "LOC2025-0137 / (CPC2026-0145) Policy and Land Use Amendment".

Brian Horton (applicant) answered questions of Commission with respect to Report CPC2026-0145.

Moved by Commissioner Boechler

That with respect to **Revised** Report CPC2026-0145, the following be approved:

That Calgary Planning Commission recommend that Council:

1. Give three readings to the proposed bylaw amendment to the Eau Claire Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw for the redesignation of 0.20 hectares \pm (0.49 acres \pm) located at 728 – 4 Avenue SW (Plan A1, Block 13, Lots 32 to 38 and a portion of Lot 31) from Direct Control (DC) District to Direct Control (DC) District to accommodate a mixed-use high-rise development with guidelines (Attachment 3).

For: (7): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, and Commissioner Wagner

MOTION CARRIED

By Unanimous Consent, Commission modified the afternoon recess to 10 minutes.

Committee recessed at 3:05 p.m. and reconvened at 3:15 p.m. with Manager Lee in the Chair.

ROLL CALL

Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, Commissioner Wagner, Director Mahler, and Manager Lee

Absent from Roll Call: Commissioner Damiani, Commissioner Pink, and Commissioner Remtulla

7.2.9 Land Use Amendment in Burns Industrial (Ward 09) at 6010 – 12 Street SE, LOC2025-0119, CPC2025-0852

A presentation entitled "LOC2025-0119 (CPC2025-0852) Land Use Amendment" was distributed with respect to Report CPC2025-0852.

Moved by Commissioner Campbell-Walters

That with respect to Report CPC2025-0852, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.62 hectares \pm (1.55 acres \pm) located at 6010 – 12 Street SE (Plan 1213072, Block 9, Lot 11) from Industrial – Commercial (I-C) District to Direct

Control (DC) District to accommodate Place of Worship – Small and Place of Worship – Medium, with guidelines (Attachment 2).

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

7.2.10 Land Use Amendment in Maple Ridge (Ward 11) in 10411 – 15 Street SE, LOC2025-0214, CPC2026-0237

A presentation entitled "LOC2025-0214 / CPC2026-0237 Land Use Amendment" was distributed with respect to Report CPC2026-0237.

Moved by Commissioner Small

That with respect to Report CPC2026-0237, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 3.10 hectares ± (7.66 acres ±) located at 10411 – 15 Street SE (Plan 1711612, Block 2, Lot 1) from Special Purpose – Urban Nature (S-UN) District to Direct Control (DC) District to accommodate an additional use of Storage Yard, with guidelines (Attachment 2).

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

7.2.11 Land Use Amendment in Killarney/Glengarry (Ward 8) at multiple addresses, LOC2025-0234, CPC2026-0166

This item was dealt with at the Consent Agenda.

7.2.12 Land Use Amendment in Mahogany (Ward 12) at multiple addresses, LOC2025-0180, CPC2026-0148

A presentation entitled "LOC2025-0180 / CPC2026-0148 Land Use Amendment" was distributed with respect to Report CPC2026-0148.

Moved by Commissioner Boechler

That with respect to Report CPC2026-0148, the following be approved:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 6.96 hectares ± (17.20 acres ±) located at 1236, 1251 and 1325 Mahogany Boulevard SE (Plan 2510331, Block 144, Lot 5; Plan 2510331, Block 148,

Lots 1 and 3) from Multi-Residential – Low Profile (M-1d75) District and Direct Control (DC) District to Mixed Use – General (MU-1f3.5h26) District.

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

7.3 MISCELLANEOUS ITEMS

None

8. URGENT BUSINESS

8.1 File Report LOC2025-0178 (CPC2026-0123), CPC2026-0319

Moved by Commissioner Wagner

That with respect to Report CPC2026-0319, the following be approved:

That Calgary Planning Commission rescind the recommendation of Report CPC2025-0123.

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

Moved by Commissioner Wagner

That with respect to Report CPC2026-0123, the following be approved, **after amendment:**

That Calgary Planning Commission **file Report CPC2026-0123.**

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

9. CONFIDENTIAL ITEMS

9.1 ITEMS FROM OFFICERS, ADMINISTRATION AND COMMITTEES

None

9.2 URGENT BUSINESS

None

10. BRIEFINGS

None

11. ADJOURNMENT

Moved by Commissioner Small

That this meeting adjourn at 3:50 p.m.

For: (8): Director Mahler, Commissioner Boechler, Commissioner Campbell-Walters, Commissioner Hardwicke, Commissioner Hawryluk, Commissioner Montgomery, Commissioner Small, and Commissioner Wagner

MOTION CARRIED

The following Item has been forwarded to the 2026 May 12 Public Hearing Meeting of Council:

REPORTS FOR PUBLIC HEARING

CALGARY PLANNING COMMISSION REPORTS

- Land Use Amendment in Hillhurst (Ward 7) at 1601 Bowness Road NW, LOC2025-0248, CPC2026-0219
- Land Use Amendment in Killarney/Glengarry (Ward 8) at 3036 and 3040 – 29 Street SW, LOC2025-0179, CPC2026-0252
- Land Use Amendment in Westwinds (Ward 5) at 3633 Westwinds Drive NE, LOC2025-0210, CPC2026-0199
- Outline Plan and Land Use Amendment in Cityscape (Ward 5) at 10011 – 52 Street NE, LOC2025-0113, CPC2026-0271
- Land Use Amendment in Hillhurst (Ward 7) at 207 and 211 – 14 Street NW, LOC2025-0195, CPC2026-0256
- Land Use Amendment in Haskayne (Ward 1) at 6600 and 6815 – 133 Street NW, LOC2025-0070, CPC2026-0221
- Land Use Amendment in Manchester (Ward 9) at 5325 – 1A Street SW, LOC2025-0066, CPC2026-0025
- Land Use Amendment in Beltline (Ward 8) at 310 – 15 Avenue SW, LOC2025-0242, CPC2026-0258
- Land Use Amendment in Beltline (Ward 8) at multiple addresses, LOC2025-0254, CPC2026-0233
- Policy and Land Use Amendment in Downtown Commercial Core (Ward 7) at 728 – 4 Avenue SW, LOC2025-0137, CPC2026-0145
- Land Use Amendment in Burns Industrial (Ward 09) at 6010 – 12 Street SE, LOC2025-0119, CPC2025-0852
- Land Use Amendment in Maple Ridge (Ward 11) in 10411 – 15 Street SE, LOC2025-0214, CPC2026-0237

- Land Use Amendment in Killarney/Glengarry (Ward 8) at multiple addresses, LOC2025-0234, CPC2026-0166
- Land Use Amendment in Mahogany (Ward 12) at multiple addresses, LOC2025-0180, CPC2026-0148

The next Regular Meeting of the Calgary Planning Commission is scheduled to be held on 2026 April 16 at 1:00 p.m.

CONFIRMED BY COMMITTEE ON

CHAIR

CITY CLERK

UNCONFIRMED

Land Use Amendment in Killarney/Glengarry (Ward 8) at 3627 Kildare Crescent SW, LOC2026-0016

RECOMMENDATION:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.06 hectares \pm (0.15 acres \pm) located at 3627 Kildare Crescent SW (Plan 732GN, Block 4, Lot 35) from Direct Control (DC) District to Residential – Grade-Oriented Infill (R-CG) District.

HIGHLIGHTS

- This application seeks to redesignate the subject site to allow for a subdivision and redevelopment with Single Detached Dwellings, in addition to the building types already listed in the existing district (e.g. single detached, semi-detached and duplex dwellings).
- The proposal represents an appropriate density increase of a residential site, allows for development that may be compatible with the character of the existing neighbourhood and is in keeping with the applicable policies of the *Municipal Development Plan* (MDP) and the *Westbrook Communities Local Area Plan* (LAP).
- What does it mean to Calgarians? The proposed Residential – Grade-Oriented Infill (R-CG) District would allow for greater housing choice within the community and more efficient use of existing infrastructure and nearby amenities.
- Why does it matter? The proposed R-CG District would allow for more housing options that may better accommodate the evolving needs of different age groups, lifestyles and demographics.
- Two development permit applications (DP2025-07355 and DP2025-07356) proposing Single Detached Dwellings with detached garages, and one subdivision application (SB2026-0029) have been submitted and are under review.
- There is no previous Council direction related to this proposal.

DISCUSSION

This land use amendment application, in the southwest community of Killarney/Glengarry, was submitted by Horizon Land Surveys on behalf of the property owner, 2781718 Alberta Ltd. (Dave Aurora), on 2026 February 5. As outlined in the Applicant Submission (Attachment 2), the purpose of this land use amendment is to facilitate development of two Single Detached Dwellings with garages accessed from the rear lane. The existing Direct Control (DC) District is based on the R-2 Residential Low Density District of the previous Land Use Bylaw 2P80 and requires a minimum lot width of 11 metres for single detached dwellings, which would preclude this form of development on the subject parcel, which is approximately 17 metres wide. A subdivision application and two development permit applications have been submitted and are under review. The subdivision application (SB2026-0029) proposes to divide the existing 17.06 metre-wide lot into two 8.53 metre-wide lots, each intended to accommodate a Single Detached Dwelling (DP2025-07355 and DP2025-07356).

The 0.06 hectare (0.15 acre) midblock site is located on the south side of Kildare Crescent SW and is currently developed with a single detached dwelling and garage that is accessed from Kildare Crescent SW. Surrounding development is characterized by a mix of single detached

Land Use Amendment in Killarney/Glengarry (Ward 8) at 3627 Kildare Crescent SW, LOC2026-0016

dwellings, semi-detached dwellings, rowhouse buildings and multi-residential developments along 37 Street SW. Nearby amenities include Holy Name School, Killarney School, A. E. Cross School and neighbourhood-scale commercial developments on 26 Avenue SW and 37 Street SW.

A detailed planning evaluation of the application, including location maps and site context is provided in the Background and Planning Evaluation (Attachment 1).

ENGAGEMENT AND COMMUNICATION

- Outreach was undertaken by the Applicant
- Public/interested parties were informed by Administration

Applicant-Led Outreach

As part of the review of the proposed land use amendment application, the applicant was encouraged to use the [Applicant Outreach Toolkit](#) to assess which level of outreach with the public/interested parties and respective community association was appropriate. In response, the applicant delivered postcards to neighbours within a 100-metre radius of the site. The Applicant Outreach Summary can be found in Attachment 3.

City-Led Outreach

In keeping with Administration's practices, this application was circulated to the public/interested parties, notice posted on-site and published [online](#). Notification letters were also sent to adjacent landowners.

Administration did not receive any comments from the Killarney/Glengarry Community Association. Administration followed up on 2026 March 10 but did not receive a response. One comment was received from the public with concerns regarding the number of units and potential impacts on parking.

Administration considered the relevant planning issues specific to the application and has determined the proposal to be appropriate. The number of units and potential impacts on parking are being reviewed and determined through the submitted development permits.

Following Calgary Planning Commission, notifications for a Public Hearing of Council for the land use amendment will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

IMPLICATIONS

Social

The proposed land use application enables the continuation of development in the community of Killarney/Glengarry while providing additional housing types to support Calgary's growing population.

**Land Use Amendment in Killarney/Glengarry (Ward 8) at 3627 Kildare Crescent
SW, LOC2026-0016**

Environmental

This application does not include any actions that specifically address the objectives of the *Calgary Climate Strategy – Pathways to 2050*. Further opportunities to align future development on this site with applicable climate strategies are being explored and encouraged through the development permit review.

Economic

This application enables a more efficient use of land as well as existing infrastructure, services, and amenities in the surrounding community.

Service and Financial Implications

No anticipated financial impact.

RISK

There are no known risks associated with this project.

ATTACHMENTS

1. Background and Planning Evaluation
2. Applicant Submission
3. Applicant Outreach Summary

Department Circulation

General Manager (Name)	Department	Approve/Consult/Inform

Background and Planning Evaluation

Background and Site Context

The subject site is located in the southwest community of Killarney/Glengarry on the south side of Kildare Crescent SW. The midblock site is approximately 0.06 hectares (0.15 acres) in area and measures approximately 17 metres wide and 37 metres deep. It is currently developed with a single detached dwelling and detached garage accessed from Kildare Crescent SW.

Surrounding development is characterized by a mix of low density residential uses including single detached dwellings, semi-detached dwellings and rowhouse buildings. Nearby amenities include Holy Name School (a two-minute walk), Killarney School (a seven-minute walk), A. E. Cross School (an eight-minute walk) and neighbourhood-scale commercial developments on 26 Avenue SW (an eight-minute walk) and 37 Street SW (a five-minute walk).

The site is in close proximity to transit routes on 26 Avenue SW, 33 Avenue SW/ Richmond Road SW and 37 Street SW. Richmond Road SW and 37 Street SW are both part of the Primary Transit Network. Specifically, the subject site is approximately 220 metres (a four-minute walk) away from both southbound and northbound Route 9 (Dalhousie Station/Chinook Station) bus stops.

Community Peak Population Table

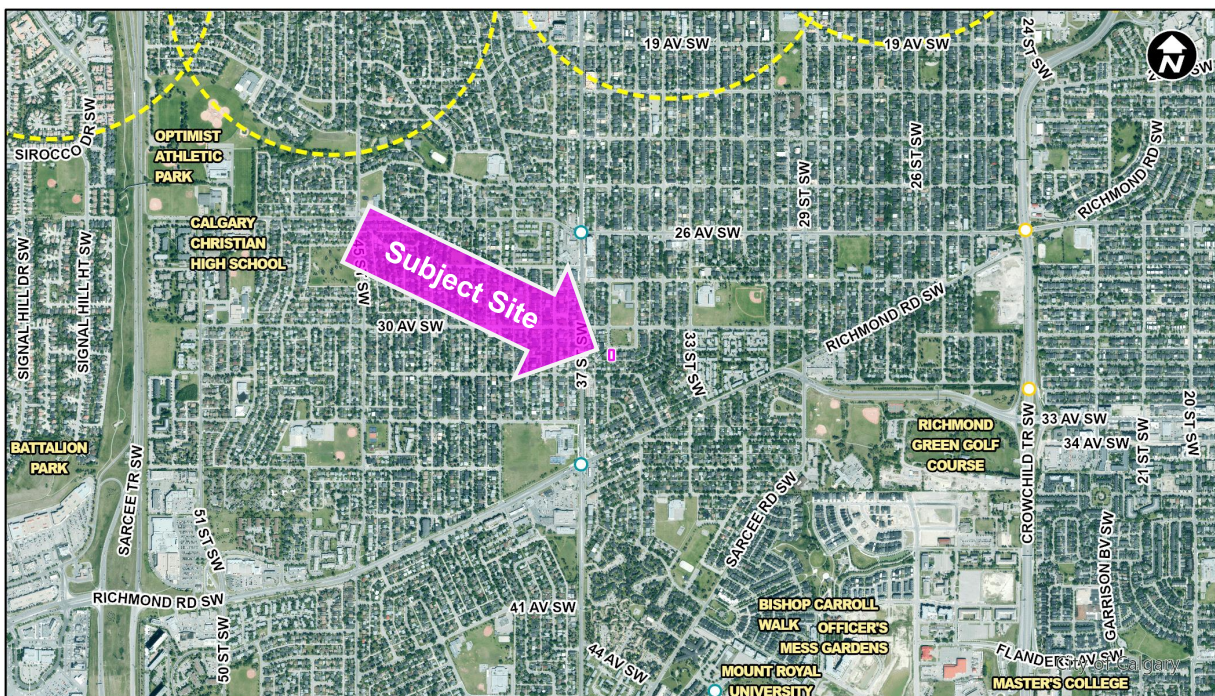
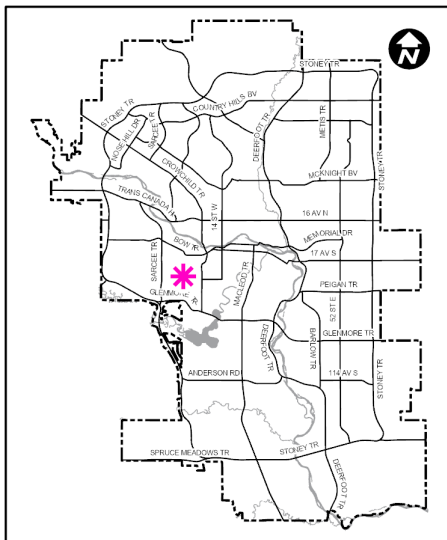
As identified below, the community of Killarney/Glengarry reached its peak population in 2019.

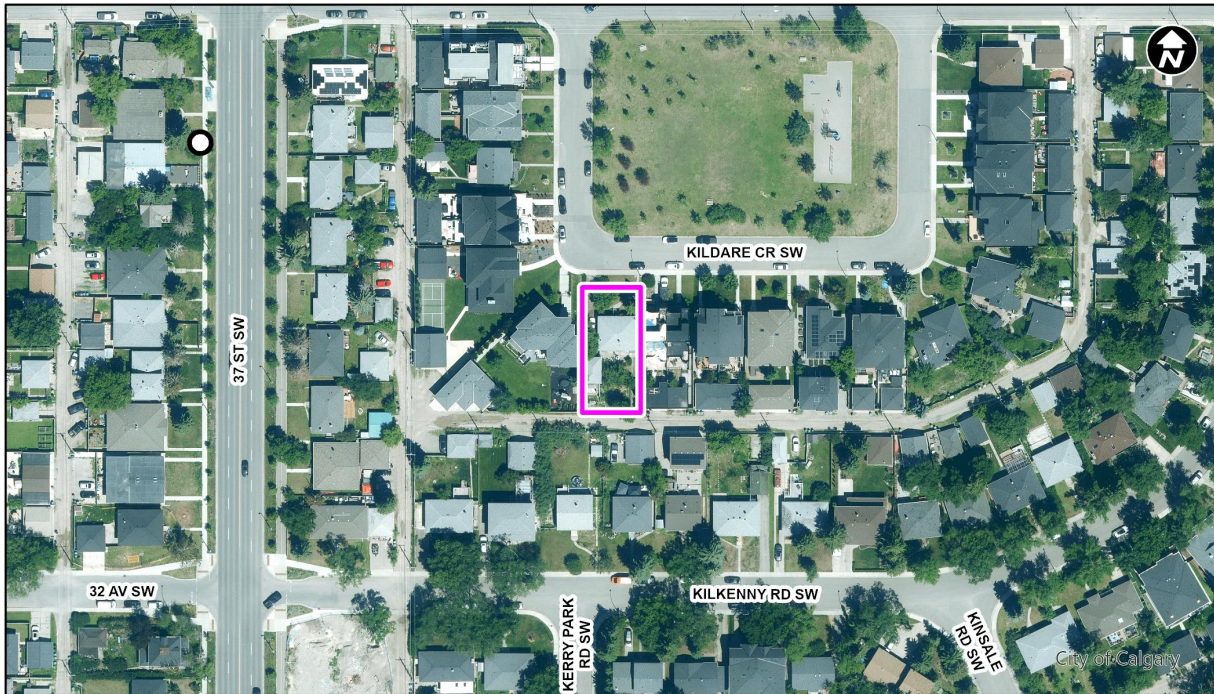
Killarney/Glengarry	
Peak Population Year	2019
Peak Population	7,685
2019 Current Population	7,685
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: *The City of Calgary 2019 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Killarney - Glengarry Community Profile](#).

Location Maps





Previous Council Direction

None.

Planning Evaluation

Land Use

The existing Direct Control (DC) District ([Bylaw 28Z91](#)) is based on the R-2 Residential Low Density District of the previous Land Use Bylaw 2P80. The DC District accommodates development in the form of single detached, semi-detached and duplex dwellings. It establishes a minimum lot width of 11 metres for parcels containing single detached dwellings, which would not allow for a subdivision to allow for two single detached dwellings, each on a separate lot. A land use amendment is required to enable this.

The proposed Residential – Grande-Oriented Infill (R-CG) District allows for a range of low-density housing forms such as single detached, semi-detached, duplex dwellings, townhouses and rowhouses. The R-CG District allows for a maximum building height of 11 metres and a maximum density of 75 dwelling units per hectare. Based on the subject site parcel area, this would allow up to four dwelling units.

Secondary suites (one backyard suite or one secondary suite per dwelling unit) are also allowed in the R-CG District and do not count towards allowable density. The parcel would require 0.5 parking stalls per dwelling unit and per secondary suite.

Development and Site Design

The rules of the proposed R-CG District would provide guidance for the future redevelopment of the site including the number of units, building design, landscaping and parking. Given the location and context of this site, the development permit process will also include a review of ways to mitigate shadowing and privacy concerns.

Transportation

Pedestrian and vehicular access to the site is available via Kildare Crescent SW. The subject site is a midblock parcel with lane access. Kildare Crescent SW is classified as a Residential Street and has less than 5,000 vehicle trips per day. The subject site is approximately 220 metres (a four-minute walk) away from both southbound, and northbound Route 9 (Dalhousie Station/Chinook Station) bus stops. The subject parcel is not located in a residential parking zone and there is unrestricted on-street parking on Kildare Crescent SW. Direct vehicular access for any future development is anticipated to be provided from the adjacent lane. A Transportation Impact Analysis was not required in support of this application.

Environmental Site Considerations

No environmental concerns were identified.

Utilities and Servicing

Water and sanitary mains are available to service the subject site. Details of the site servicing will be reviewed at the development permit stage.

Legislation and Policy

South Saskatchewan Regional Plan (2014)

Administration's recommendation aligns with the policy direction of the [South Saskatchewan Regional Plan](#), which directs population growth in the region to cities and towns, and promotes the efficient use of land.

Municipal Development Plan (Statutory – 2009)

The subject site is located in the Developed Residential – Inner City area on Map 1: Urban Structure of the [Municipal Development Plan](#) (MDP). The applicable MDP policies encourage redevelopment and modest intensification of inner-city communities to make more efficient use of existing infrastructure, public amenities and transit and deliver small and incremental benefits to climate resilience. The proposal to redesignate the site to the R-CG District to accommodate subdivision of the existing parcel to accommodate development of two Single Detached Dwellings aligns with the applicable policies of the MDP.

Calgary Climate Strategy (2022)

This application does not include any specific actions that address the objectives of the [Calgary Climate Strategy – Pathways to 2050](#). Further opportunities to align future development on this site with applicable climate strategies are being explored and encouraged through the development permit review.

Westbrook Communities Local Area Plan (Statutory – 2022)

The subject site is identified as being within a 'Neighbourhood Local' area on Map 3: Urban Form of the [Westbrook Communities Local Area Plan](#) (LAP), with a 'Limited' building scale modifier on Map 4: Building Scale, which allows for development up to three storeys. The applicable LAP policies encourage a range of housing types across the plan area while emphasizing that Neighbourhood Local areas, particularly those with a Limited building scale, remain residentially oriented and support privacy and context-sensitive built form. The proposed development of two Single Detached Dwellings generally aligns with the applicable policies of the LAP.

Applicant Submission

Company Name (if applicable):

Horizon Land Surveys

Applicant's Name:

Lei Wang

Date:

Feb. 2nd, 2026

LOC Number (office use only):

On behalf of the landowner, please accept this application to redesignate a +/-0.062 hectare site from Direct Control to R-CG to allow two singles to be built on site. A subdivision for two singles and Development permit will also be submitted to make clear the intention of the application.

The subject site, 3627 Kildare Crescent SW, is a mid-block lot located in the community of Killarney/Glengarry along Kildare Crescent SW. The lot is currently developed with a single detached dwelling built in 1952. Surrounding houses includes single, semi-detached and park space directly to the north of the site.

The site is approximately 0.062 hectares in size. A rear lane exists to the south of the site. Vehicle access to the parcel is available and will be via the rear lane. The site is very close distance to public schools and public transit along 37 Street.

The lot is under Direct Control 28Z91 zoning. The DC bylaw only allows one single or semi-detached to be built on site. The intention is to build two singles with the same maximum height, density, lot coverage, etc. allowed under the current zoning. To make clear of the intention, a subdivision for two singles and development permit will also be filed.

The subject parcel is located within the Residential-Developed-Inner City area of the Municipal Development Plan. The applicable policies encourage redevelopment of inner-city communities that is similar in scale and built form to existing development, including a mix of housing such as townhouses and row housing. The Municipal Development Plan also calls for a modest intensification of the inner city, an area serviced by existing infrastructure, public amenities and transit. The proposal is in keeping with the relevant policies of the MDP as the rules of the R-CG provide for development forms that may be sensitive to existing residential development in terms of height, built form and density.

Housing diversity and choice policies encourage the provision of a wide range of housing types, tenures and densities that may meet affordability, accessibility, life cycle and lifestyle needs of different groups; an adaptation of the City's existing housing stock to enable changing households to remain in the same neighbourhood for longer time periods through allowing accessory suites in low density areas. So we sincerely hope city can support our application.

Applicant Outreach Summary



Community Outreach on Planning & Development Applicant-led Outreach Summary

Please complete this form and include with your application submission.

Project name: 3627 Kildare Crescent SW

Did you conduct community outreach on your application? YES or NO

If no, please provide your rationale for why you did not conduct outreach.

Outreach Strategy

Provide an overview of your outreach strategy, summary of tactics and techniques you undertook (Include dates, locations, # of participants and any other relevant details)

Before undertaking the project, our office completed the "Community Outreach Assessment". The project's impact score is "1A". So we are implementing a direct approach to reach to community association, local residents within a 100 meters radius and also Ward Councilor Office.

On October 7th, 2025, our staff did post card deliver to residents within a 100 meters radius. During the process, our staff spoke with residents at home.

We have also contacted community association and councilor's office for comments.

Stakeholders

Who did you connect with in your outreach program? List all stakeholder groups you connected with. (Please do not include individual names)

Immediate neighbour, local residents, community association and ward councillor office

calgary.ca/planningoutreach



Community Outreach for Planning & Development Applicant-led Outreach Summary

What did you hear?

Provide a summary of main issues and ideas that were raised by participants in your outreach.

Since the proposal is not increasing density, but just to seek for two singles, most residents are quite understandable. Some express concerns asking for assurance that only two singles are being proposed.

How did stakeholder input influence decisions?

Provide a summary of how the issues and ideas summarized above influenced project decisions. If they did not, provide a response for why.

File subdivision application for two singles.

File Development Permit application for two singles.

How did you close the loop with stakeholders?

Provide a summary of how you shared outreach outcomes and final project decisions with the stakeholders that participated in your outreach. (Please include any reports or supplementary materials as attachments)

Continue engage with local residents, Councillor office and community association.

calgary.ca/planningoutreach

**Land Use Amendment in Killarney/Glengarry (Ward 8) at 2823 – 30 Street SW,
 LOC2026-0009**

RECOMMENDATION:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.06 hectares \pm (0.15 acres \pm) located at 2823 – 30 Street SW (Plan 732GN, Block 9, Lot 54 and portion of Lot 53) from Direct Control (DC) District to Residential – Grade-Oriented Infill (R-CG) District.

HIGHLIGHTS

- This application seeks to redesignate the subject site to allow for a subdivision and redevelopment with Single Detached Dwellings, in addition to the building types already listed in the existing district (e.g. single detached, semi-detached and duplex dwellings).
- The proposal represents an appropriate density increase of a residential site, allows for development that may be compatible with the character of the existing neighbourhood and is in keeping with the applicable policies of the *Municipal Development Plan* (MDP) and the *Westbrook Communities Local Area Plan* (LAP).
- What does it mean to Calgarians? The proposed Residential – Grade-Oriented Infill (R-CG) District would allow for greater housing choice within the community and more efficient use of existing infrastructure and nearby amenities.
- Why does it matter? The proposed R-CG District would allow for more housing options that may better accommodate the evolving needs of different age groups, lifestyles and demographics.
- Two development permit applications (DP2026-00867 and DP2026-00868) proposing Single Detached Dwellings with detached garages, and one subdivision application (SB2026-0041) have been submitted and are under review.
- There is no previous Council direction related to this proposal.

DISCUSSION

This land use amendment application, in the southwest community of Killarney/Glengarry, was submitted by Horizon Land Surveys on behalf of the property owner, Ian Lau Scott, on 2026 January 27. As outlined in the Applicant Submission (Attachment 2), the purpose of this land use amendment is to facilitate development of two Single Detached Dwellings with detached garages accessed from the rear lane. The existing Direct Control (DC) District is based on the R-2 Residential Low Density District of the previous Land Use Bylaw 2P80 and requires a minimum lot width of 11 metres for Single Detached Dwellings, which would preclude this form of development on the subject parcel, which is approximately 17 metres wide. A subdivision application and two development permit applications have been submitted and are under review. The subdivision application (SB2026-0041) proposes to divide the existing 17.08 metre-wide lot into two 8.54 metre-wide lots, each intended to accommodate a Single Detached Dwelling (DP2026-00867 and DP2026-00868).

The 0.06 hectare (0.15 acre) midblock site is located on the west side of 30 Street SW and is currently developed with a single detached dwelling with driveway access from 30 Street SW and a garage that is accessed from the rear lane. Surrounding development is characterized by

**Land Use Amendment in Killarney/Glengarry (Ward 8) at 2823 - 30 Street SW,
LOC2026-0009**

a mix of single detached dwellings, semi-detached dwellings and rowhouse buildings. Nearby amenities include Killarney School, neighbourhood-scale commercial developments on 26 Avenue SW and Richmond Road SW, the Killarney/Glengarry Community Association and Richmond Green Park.

A detailed planning evaluation of the application, including location maps and site context is provided in the Background and Planning Evaluation (Attachment 1).

ENGAGEMENT AND COMMUNICATION

- Outreach was undertaken by the Applicant
- Public/interested parties were informed by Administration

Applicant-Led Outreach

As part of the review of the proposed land use amendment application, the applicant was encouraged to use the [Applicant Outreach Toolkit](#) to assess which level of outreach with the public/interested parties and respective community association was appropriate. In response, the applicant delivered postcards to neighbours within a 100-metre radius of the site. The Applicant Outreach Summary can be found in Attachment 3.

City-Led Outreach

In keeping with Administration's practices, this application was circulated to the public/interested parties, notice posted on-site and published [online](#). Notification letters were also sent to adjacent landowners.

Administration did not receive any comments from the Killarney/Glengarry Community Association (CA). Administration followed up with the CA on 2026 February 27 but did not receive a response. One comment was received from the public with concerns regarding the number of units, building design, parking, privacy and offsite impacts.

Administration considered the relevant planning issues specific to the application and has determined the proposal to be appropriate. The number of units, building design, parking, privacy and offsite impacts are being reviewed and determined through the submitted development permits.

Following Calgary Planning Commission, notifications for a Public Hearing of Council for the land use amendment will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

IMPLICATIONS

Social

The proposed land use application enables the continuation of development in the community of Killarney/Glengarry while providing additional housing types to support Calgary's growing population.

**Land Use Amendment in Killarney/Glengarry (Ward 8) at 2823 - 30 Street SW,
LOC2026-0009**

Environmental

This application does not include any actions that specifically address the objectives of the *Calgary Climate Strategy – Pathways to 2050*. Further opportunities to align future development on this site with applicable climate strategies are being explored and encouraged through the development permit review.

Economic

This application enables a more efficient use of land as well as existing infrastructure, services, and amenities in the surrounding community.

Service and Financial Implications

No anticipated financial impact.

RISK

There are no known risks associated with this project.

ATTACHMENTS

1. Background and Planning Evaluation
2. Applicant Submission
3. Applicant Outreach Summary

Department Circulation

General Manager (Name)	Department	Approve/Consult/Inform

Background and Planning Evaluation

Background and Site Context

The subject site is located in the southwest community of Killarney/Glengarry on the west side of 30 Street SW. The midblock site is approximately 0.06 hectares (0.15 acres) in area and measures approximately 17 metres wide and 37 metres deep. It is currently developed with a single detached dwelling with driveway access from 30 Street SW and a detached garage accessed from the rear lane to the west of the site.

Surrounding development is characterized by a mix of low density residential uses including single detached dwellings, semi-detached dwellings and rowhouse buildings. Nearby amenities include Killarney School (a four-minute walk), neighbourhood-scale commercial developments on 26 Avenue SW (a six-minute walk) and Richmond Road SW (a seven-minute walk), the Killarney/Glengarry Community Association (a four-minute walk) and Richmond Green Park (an eight-minute walk).

The site is in close proximity to transit routes on both 33 Avenue SW/ Richmond Road SW (which is part of the Primary Transit Network) and 26 Avenue SW. Specifically, the subject site is approximately 190 metres (a three-minute walk) away from both eastbound and westbound Route 6 (Killarney/26 Ave SW) bus stops.

Community Peak Population Table

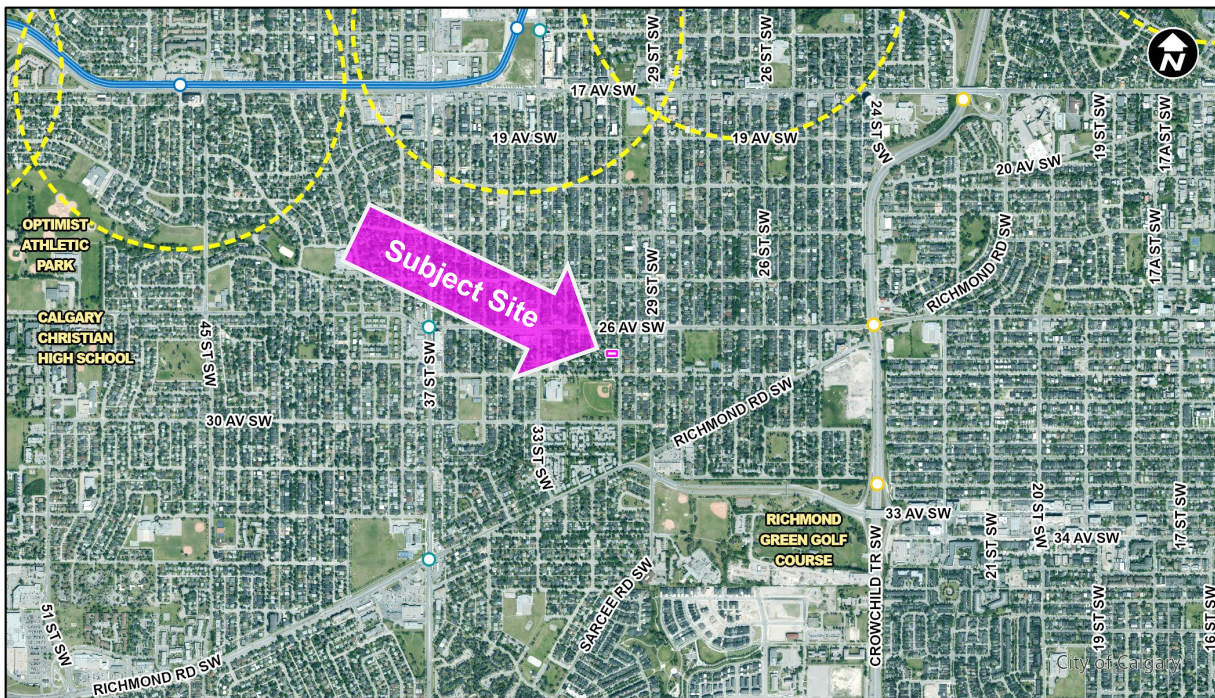
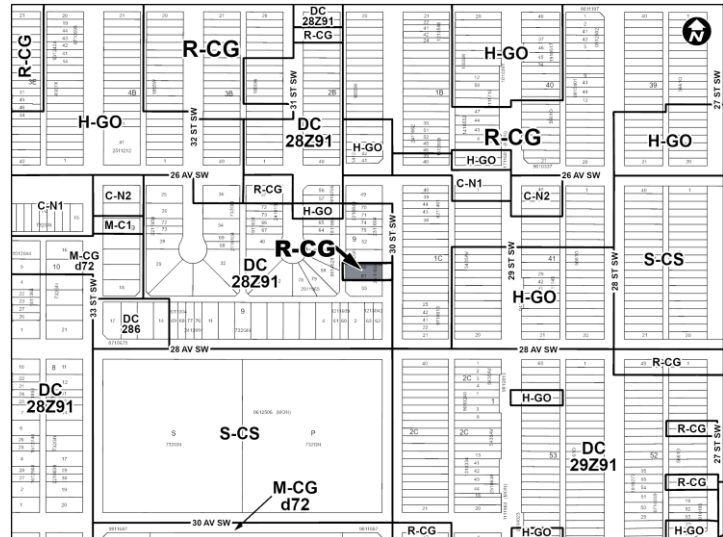
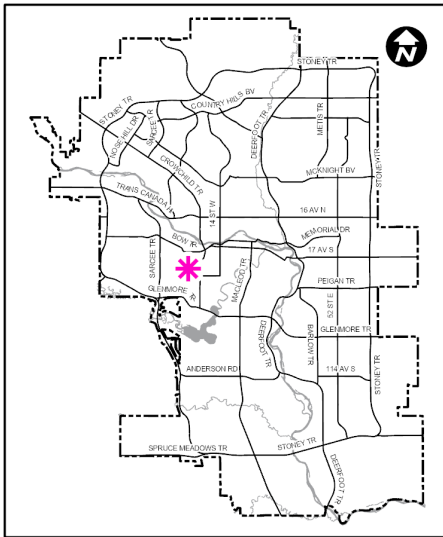
As identified below, the community of Killarney/Glengarry reached its peak population in 2019.

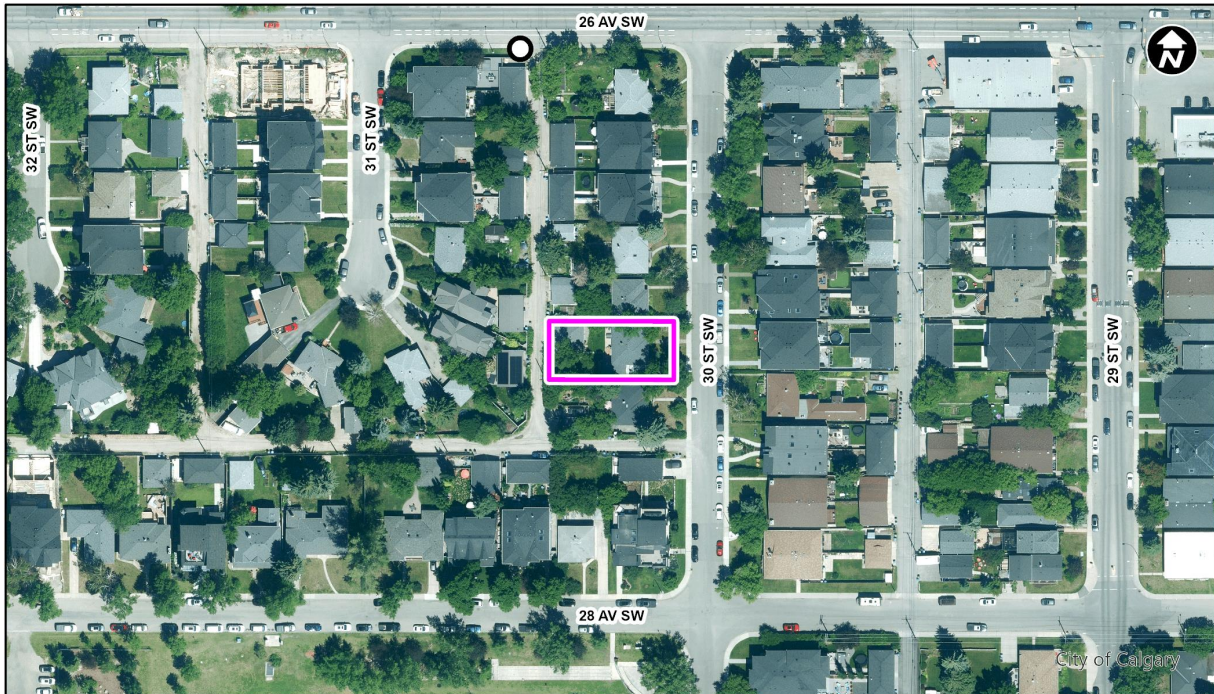
Killarney/Glengarry	
Peak Population Year	2019
Peak Population	7,685
2019 Current Population	7,685
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: *The City of Calgary 2019 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Killarney - Glengarry Community Profile](#).

Location Maps





Previous Council Direction

None.

Planning Evaluation

Land Use

The existing Direct Control (DC) District ([Bylaw 28Z91](#)) is based on the R-2 Residential Low Density District of the previous Land Use Bylaw 2P80. The DC District accommodates development in the form of single detached, semi-detached and duplex dwellings. It establishes a minimum lot width of 11 metres for parcels containing single detached dwellings, which would not allow for a subdivision to allow for two single detached dwellings, each on a separate lot. A land use amendment is required to enable this.

The proposed Residential – Grade-Oriented Infill (R-CG) District allows for a range of low-density housing forms such as single detached, semi-detached, duplex dwellings, townhouses and rowhouses. The R-CG District allows for a maximum building height of 11 metres and a maximum density of 75 dwelling units per hectare. Based on the subject site parcel area, this would allow up to four dwelling units.

Secondary suites (one backyard suite or one secondary suite per dwelling unit) are also allowed in the R-CG District and do not count towards allowable density. The parcel would require 0.5 parking stalls per dwelling unit and per secondary suite.

Development and Site Design

The rules of the proposed R-CG District would provide guidance for the future redevelopment of the site including the number of units, building design, landscaping and parking. Given the location and context of this site, the development permit process will also include a review of ways to mitigate shadowing and privacy concerns.

Transportation

Pedestrian and vehicular access to the site is available via 30 Street SW. The subject site is a mid block parcel with lane access. 30 Street SW is classified as a Residential Street and has less than 5,000 vehicle trips per day. The subject site is approximately 190 metres (a three-minute walk) away from both eastbound, and westbound Route 6 (Killarney/26 Ave SW) bus stops. The subject parcel is not located in a residential parking zone, and there is unrestricted on-street parking on 30 Street SW. Direct vehicular access for any future development is anticipated to be provided from the adjacent lane. A Transportation Impact Analysis was not required in support of the land use re-designation application.

Environmental Site Considerations

No environmental concerns were identified.

Utilities and Servicing

Water and sanitary utilities exist adjacent to the site within public road rights-of-way, no storm utilities are immediately available adjacent to the site. Servicing requirements will be determined at the time of development permit.

Legislation and Policy

South Saskatchewan Regional Plan (2014)

Administration's recommendation aligns with the policy direction of the [South Saskatchewan Regional Plan](#), which directs population growth in the region to cities and towns, and promotes the efficient use of land.

Municipal Development Plan (Statutory – 2009)

The subject site is located in the Developed Residential – Inner City area on Map 1: Urban Structure of the [Municipal Development Plan](#) (MDP). The applicable MDP policies encourage redevelopment and modest intensification of inner-city communities to make more efficient use of existing infrastructure, public amenities and transit and deliver small and incremental benefits to climate resilience. The proposal to redesignate the site to the R-CG district to accommodate subdivision of the existing parcel to accommodate development of two Single Detached Dwellings aligns with the applicable policies of the MDP.

Calgary Climate Strategy (2022)

This application does not include any specific actions that address the objectives of the [Calgary Climate Strategy – Pathways to 2050](#). Further opportunities to align future development on this site with applicable climate strategies are being explored and encouraged through the development permit review.

Westbrook Communities Local Area Plan (Statutory – 2022)

The subject site is identified as being within a 'Neighbourhood Local' area on Map 3: Urban Form of the [Westbrook Communities Local Area Plan](#) (LAP), with a 'Limited' building scale modifier on Map 4: Building Scale, which allows for development up to three storeys. The applicable LAP policies encourage a range of housing types across the plan area while emphasizing that Neighbourhood Local areas, particularly those with a Limited building scale, remain residentially oriented and support privacy and context-sensitive built form. The proposed development of two Single Detached Dwellings generally aligns with the applicable policies of the LAP.

Applicant Submission

Company Name (if applicable):

Horizon Land Surveys

Applicant's Name:

Lei Wang

Date:

Jan. 23rd, 2026

LOC Number (office use only):

On behalf of the landowner, please accept this application to redesignate a +/-0.062 hectare site from Direct Control to R-CG to allow two singles to be built on site. A subdivision for two singles and Development permit will also be submitted to make clear the intention of the application.

The subject site, 2823 30 Street SW, is along 30 Street SW in the community of Killarney/Glengarry. The site is currently built with a single detached dwelling built in 1953. Surrounding houses are mostly single detached although there are many R-CG, H-GO or commercial zoning lots along 26 Ave.

The site is approximately 0.062 hectares in size. A rear lane exists to the west of the site. Vehicle access to the parcel is available and will be via the rear lane. The site is very close distance to public transit along 26 Ave. Killarney School and Killarney Glengarry Community Association is also in close proximity.

The lot is under Direct Control 28Z91 zoning. The DC bylaw only allows one single or semi-detached to be built on site. The intention is to build two singles with the same maximum height, density, lot coverage, etc. allowed under the current zoning. To make clear of the intention, a subdivision for two singles and development permit will also be filed.

The subject parcel is located within the Residential-Developed-Inner City area of the Municipal Development Plan. The applicable policies encourage redevelopment of inner-city communities that is similar in scale and built form to existing development, including a mix of housing such as townhouses and row housing. The Municipal Development Plan also calls for a modest intensification of the inner city, an area serviced by existing infrastructure, public amenities and transit. The proposal is in keeping with the relevant policies of the MDP as the rules of the R-CG provide for development forms that may be sensitive to existing residential development in terms of height, built form and density.

Housing diversity and choice policies encourage the provision of a wide range of housing types, tenures and densities that may meet affordability, accessibility, life cycle and lifestyle needs of different groups; an adaptation of the City's existing housing stock to enable changing households to remain in the same neighbourhood for longer time periods through allowing accessory suites in low density areas. So we sincerely hope city can support our application.

Applicant Outreach Summary



Community Outreach on Planning & Development Applicant-led Outreach Summary

Please complete this form and include with your application submission.

Project name: 2823 30 Street SW

Did you conduct community outreach on your application? YES or NO

If no, please provide your rationale for why you did not conduct outreach.

Outreach Strategy

Provide an overview of your outreach strategy, summary of tactics and techniques you undertook (Include dates, locations, # of participants and any other relevant details)

Before undertaking the project, our office completed the "Community Outreach Assessment". The project's impact score is "1A". So we are implementing a direct approach to reach to community association, local residents within a 100 meters radius and also Ward Councillor Office.

On Jan. 14th, 2026, our staff did post card deliver to residents within a 100 meters radius. During the process, our staff spoke with residents at home.

On Jan. 23rd, 2026, we contacted community Association through email with regards to the project. A follow up email was send on Feb. 2nd.

On Jan. 23rd, we contacted Councillor office for the project. During online meeting with councillor on March 2nd, we briefly discussed the project with Councillor.

Stakeholders

Who did you connect with in your outreach program? List all stakeholder groups you connected with. (Please do not include individual names)

Immediate neighbour, local residents, community association and ward councillor office

calgary.ca/planningoutreach



Community Outreach for Planning & Development Applicant-led Outreach Summary

What did you hear?

Provide a summary of main issues and ideas that were raised by participants in your outreach.

Since the proposal is not increasing density, but just to seek for two singles, most residents are quite understandable. Some express concerns asking for assurance that only two singles are being proposed.

How did stakeholder input influence decisions?

Provide a summary of how the issues and ideas summarized above influenced project decisions. If they did not, provide a response for why.

File subdivision application for two singles.

File Development Permit application for two singles.

How did you close the loop with stakeholders?

Provide a summary of how you shared outreach outcomes and final project decisions with the stakeholders that participated in your outreach. (Please include any reports or supplementary materials as attachments)

Continue engage with local residents, Councillor office and community association.

calgary.ca/planningoutreach

Development Permit in Medicine Hill (Ward 6) at 1152 Na'a Drive SW, DP2025-04914

RECOMMENDATION:

That Calgary Planning Commission APPROVE Development Permit DP2025-04914 for a New: Office, Retail and Consumer Service, Self Storage Facility (1 building) at 1152 Na'a Drive SW (Plan 2510905, Block 3, Lot 12) with conditions (Attachment 2).

HIGHLIGHTS

- This application proposes a five-storey self storage building with additional street-facing commercial units on the ground floor.
- The proposed development would integrate with adjacent land uses and aligns with the planning policies of the *Municipal Development Plan (MDP)* and the *Canada Olympic Park and Adjacent Lands Area Structure Plan (ASP)*.
- What does this mean to Calgarians? A Self Storage Facility with at-grade commercial opportunities would provide storage options in proximity to the higher density multi-residential developments and home-based businesses in the area.
- Why does this matter? Providing a self-storage and at-grade commercial opportunities in the developing area of Medicine Hill may help respond to evolving household and business needs within proximity of Canada Olympic Park (Winsport).
- The design elements and materials were carefully chosen to respond to the cultural and historical context of Medicine Hill, the proposal is in alignment with the discretionary use rules of Land Use Bylaw 1P2007 and Direct Control (DC) District (Bylaw 35D2025), subject to minor relaxations.
- There is no previous Council direction related to this proposal.

DISCUSSION

This application, located in the southwest community of Medicine Hill, was submitted by Axiom Architecture Inc. on behalf of the landowner, Trinity Hills Storage Corp., on 2025 August 25. In 2015, Council approved amendments to the ASP, directing significant development permit applications in Medicine Hill to be brought to Calgary Planning Commission (CPC) for decision due to the unique attributes of the land and the prominent gateway location of the community. The site is situated along the Trans-Canada Highway, east of Canada Olympic Park and is highly visible from the highway, thus meeting the criteria identified for CPC development permit review. As indicated in the Applicant Submission (Attachment 3), the intent of this application is to allow for a self-storage building with an associated storefront retail (The Nest) and additional office spaces at-grade. The parcel fronts onto Na'a Drive SW to the south, which is identified as a Main Street in the ASP, from which vehicular access will be available. The parcel is approximately 0.81 hectares (2.00 acres) in size and is currently vacant.

The development permit application proposes a five storey building, which is approximately 11,000 square metres. The principal pedestrian-level access to the building will be provided along Na'a Drive SW. Additional details can be viewed in the Development Permit Plans (Attachment 4).

Development Permit in Medicine Hill (Ward 6) at 1152 Na' a Drive SW, DP2025-04914

As part of the review process, this application was reviewed by the Office of Urban Design (OUD) and by the Urban Design Review Panel (UDRP) on 2025 June 25 (Attachment 5). In response, the applicant enhanced the public realm along Na'a Drive SW with landscaping features below the windows of the commercial units to articulate the street edge and to highlight pedestrian access points. To minimize the negative visual impacts of the parking and loading area near Na'a Drive SW, an additional layered landscaping buffer was provided. The major highlight of the project design is a large scale buffalo mural proposed on the two most prominent building façades along the Trans-Canada Highway. UDRP appreciated this design element and indicated that this public art feature will be a landmark for the area. The UDRP noted the prominence of the mural feature and its strong recognition of the Indigenous history and traditions of the area. A subsequent review of the plans was completed by the OUD and the proposal is deemed to have satisfied the direction provided by both the OUD and UDRP.

ENGAGEMENT AND COMMUNICATION

- Outreach was undertaken by the Applicant
- Public/interested parties were informed by Administration

Applicant-Led Outreach

As part of the review of the proposed development permit application, the applicant was encouraged to use the [Applicant Outreach Toolkit](#) to assess which level of outreach with interested members of the public and the relevant community association was appropriate. In response, the applicant incorporated the feedback received from interested parties during the land use amendment stage into their project design. Through the development permit review process, the applicant furthered engagement with the Blackfoot Indigenous Community which culminated in the cultural and historical elements within the proposed development design. The applicant also amended their design drawings in response to the development permit circulation comments received from the Bowness Community Association (CA) and the Paskapoo Slopes Preservation Society (PSPS). Additional information can be found in the Applicant Outreach Summary (Attachment 6).

City-Led Outreach

In keeping with Administration's standard practices, the application was circulated to external partners, notice posted on site and published [online](#).

Administration received five letters of opposition from the public. The letters cited the following areas of concern:

- the five-storey building proposed is too high and would impact views of the Paskapoo Slopes from the edge of the Trans-Canada Highway;
- a storage facility is not good use of urban design and could also create potential competition for the other storage facilities located in the vicinity;
- the proposal is a square building with two tone cladding and uninspiring, a storage facility and office space is unnecessary for the area and would negatively impact the character and walkability of the area; and
- preference for a use that meets the needs of the community, such as a restaurant or cafe, child or youth facility (daycare, play centre and after-school activity space), a

Development Permit in Medicine Hill (Ward 6) at 1152 Na' a Drive SW, DP2025-04914

community park, health and wellness services (clinic, fitness studio and physiotherapy office).

There is currently no community association for the area, but the adjacent Bowness CA was notified of the proposed development. The Bowness CA responded on 2025 October 1 indicating opposition and concerns about potential lighting pollution from the proposed development. A subsequent letter was submitted by the CA on 2025 December 3 acknowledging the applicant's response to their concerns through which they also provided recommendations to the applicant on how to further reduce light pollution. The Bowness CA comments can be found in Attachment 7.

The PSPS was also notified in accordance with the circulation request they expressed at the land use amendment stage. The PSPS responded with a comment letter on 2025 October 3 and indicated their opposition for the proposed development. The PSPS comments can be found in Attachment 8.

Administration considered the relevant planning issues specific to the proposed development and has determined that the proposal is appropriate. The building and site design, location of at-grade commercial uses and landscaping align with the policy direction of the ASP. The development concept also responds to the concerns heard regarding functionality, massing, materiality and lighting. In addition, Council reviewed the self storage land use proposal during the land use stage and Public Hearing and approved the use on the subject site.

Following Calgary Planning Commission, Commission's decision will be advertised in accordance with the *Municipal Government Act*.

IMPLICATIONS

Social

This development permit will allow for a greater diversity of businesses in the area that respond to evolving household needs and supports compact development of complete communities.

Environmental

The plans include measures addressing the zero carbon neighbourhoods theme of the *Calgary Climate Strategy – Pathways to 2050*. Additional details are provided in Attachment 1.

Economic

The proposed development permit would enable the operation of a Self Storage Facility comprising of an approximately 385 square-metre floor area of at-grade commercial opportunities close to a Main Street while providing employment and business opportunities within the community. It may also support compact urban development that makes more efficient use of existing infrastructure and services.

Service and Financial Implications

No anticipated financial impact.

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CPC2026-0230
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Development Permit in Medicine Hill (Ward 6) at 1152 Na' a Drive SW, DP2025-04914

RISK

There are no known risks associated with this proposal.

ATTACHMENTS

1. Background and Planning Evaluation
2. Conditions of Approval
3. Applicant Submission
4. Development Permit Plans
5. Urban Design Review Panel Comments
6. Applicant Outreach Summary
7. Community Association Responses
8. Paskapoo Slopes Preservation Society Responses

Department Circulation

General Manager (Name)	Department	Approve/Consult/Inform

Background and Planning Evaluation

Background and Site Context

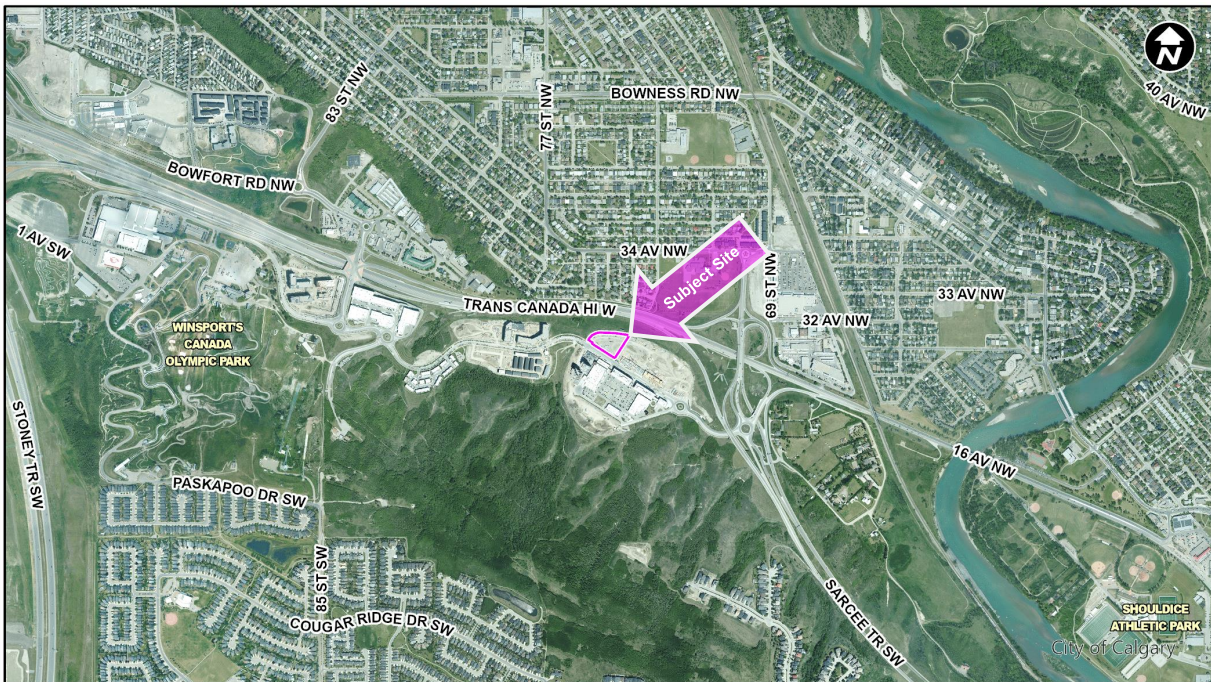
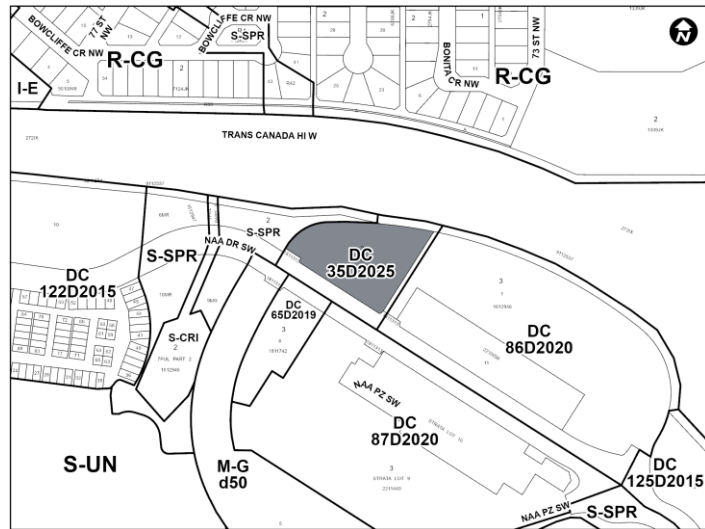
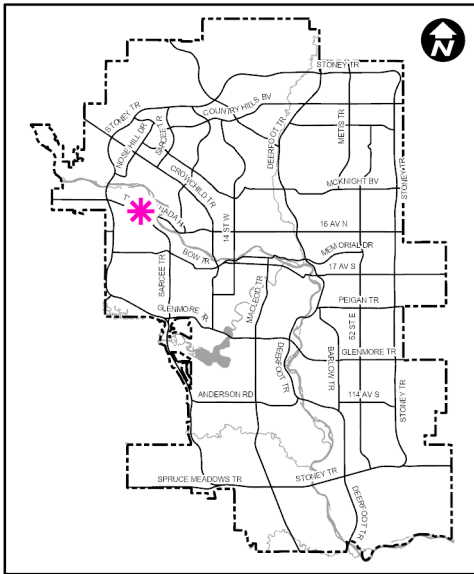
The proposed development is in the southwest community of Medicine Hill, south of the Trans-Canada Highway and west of Sarcee Trail. The parcel is irregularly shaped with complex topography and is approximately 0.81 hectares (2.00 acres) in size. Currently, the site is vacant and undeveloped. The site is proposed to be accessed from the south via Na'a Drive SW.

Surrounding development is characterized by a mixed-use commercial plaza across the street to the south, which is comprised of multi-residential towers, a grocery store, dental office and other retail services. To the east is a developing multi-residential site. Other developing multi-residential sites and developed commercial sites such as Canada Olympic Park (Winsport), and other retail/restaurant establishments are located to the west. A naturalized municipal reserve parcel is directly adjacent to the north and west of the subject site.

Community Peak Population Table

As of the 2019 City of Calgary Civic Census, there is no population data for the subject area as this is a new residential area.

Location Maps





Previous Council Direction

None.

Planning Evaluation

Land Use

The subject site is designated as a Direct Control (DC) District ([Bylaw 35D2025](#)), which was approved by Council on 2025 February 04. The DC District references the Commercial – Corridor 1 (C-COR1) District which is intended to accommodate a range of commercial uses with an additional discretionary use of a Self Storage Facility and includes specific requirements for ground floor retail and commercial uses close to the street and public sidewalk. The DC District also allows a maximum building height of 20.0 metres and a maximum building floor area of approximately 20,000 square metres through a maximum floor area ratio (FAR) of 2.0. The building height proposed in this application is approximately 16.48 metres (five storeys). The application also proposes a building floor area of approximately 11,387.49 square metres (an FAR of 1.4).

Section 6 of the DC District incorporates the rules of the base C-COR1 District in Bylaw 1P2007 where the DC District does not provide for specific regulations. Many of these rules can be relaxed if they meet the test for relaxation of Bylaw 1P2007. The configuration of the site area including the condition of the site topography resulted in the proposed development not being able to meet some of the base district rules. A full list of bylaw relaxations is referenced at the end of this attachment.

Development and Site Design

This application proposes a self storage facility building including an at-grade associated storefront retail unit and other office units along Na'a Drive SW. The proposed development also provides onsite amenity for landscaping and circulation management. Key aspects of the development are described below.

Site and Building Design

The proposed building is five storeys and provides one retail unit and 10 office units on the ground floor of the building. These units have direct access from Na'a Drive SW (a Main Street, as indicated in the *Canada Olympic Park and Adjacent Lands Area Structure Plan (ASP)*) and will provide commercial opportunities and desired street activation to achieve the policy intent of the ASP. Access to individual storage units will be from within the site, at the west side of the building, near Na'a Drive sidewalk. A slope-adaptive approach is implemented to manage the topographic challenges of the site, whereby the south and west-facing elevations of the building read like a four-storey building at grade. The five-storey built form then becomes apparent with a gradient drop of approximately 4.0 metres in both the east and north directions respectively.

Building Massing and Building Materials

For visual interest, the building design uses varied materials and colours, façade articulation and bold design elements to transcend conventional self storage architecture. To address glare and dark sky impacts at night, warm-coloured lighting installations are proposed on the building exterior including illuminated signages that are dimmable. Material selections and massing respond to the visibility and prominence of the site to create a more human-scaled composition and minimize a monolithic appearance. The building is also stepped back from the street while window bands and local artwork are used to break up large wall surfaces.

Public Realm and Outdoor Amenity Spaces

To enhance and activate the public realm, direct pedestrian access will be provided for the building entrance and the ground floor units to connect the site with Na'a Drive SW. The site includes clearly marked pedestrian walkways, wayfinding signage and landscape buffers which define the building edges. This enables spatial definition of pedestrian zones and encourages the use of common areas, such as the seating provided on the west side of the building. The loading docks and parking are located towards the side area of the building to minimize their visibility from pedestrian-facing zones. The site's only vehicular entrance from Na'a Drive SW is to be located further to the west of the building.

Landscaping

Public and private landscaping has been designed to soften the edges of the site and provide visual interest within the interior of the site using layered plantings and low water vegetative species. Proposed private tree planting along Na'a Drive SW will complement the existing public trees that are already planted and will assist in enhancing the public realm and promote traffic calming on the street. Along the edges of the parking area, there will be a retaining wall (ranging between 0.20 metre to 0.45 metres in front of the parking stalls) that is intended to accommodate the sloping terrain of the site. Drought-resistant trees and shrubs are proposed on the edge of the site to mask the visual impact of the retaining wall and to complement the street trees. Also, natural vegetation will be preserved along the boundary of the adjacent Municipal Reserve to maintain ecological continuity. Overall, the proposed landscaping elements are anticipated to complement the built form and contribute positively to the slope-adaptive site design.

Commercial Frontage

The front façade incorporates quality architectural materials, varied massing treatments, art mural and commercial signage to visibly articulate the entrances of the commercial units. Some of these treatments include the use of canopies, transparent glazing and colour contrasts to enhance the visual experience. The building design also creates a continuous street wall with strong urban edges using step back above-grade and the placement of landscaping.

The proposed storefront retail unit associated with the self storage use (The Nest) will occupy approximately 97 square metres of floor space adjacent to Na'a Drive SW. This storefront retail functions as a home organizational and moving goods store (with product lines on display for sale to storage customers and the general public). The balance of the street-facing office units proposed along Na'a Drive SW will occupy approximately 288 square metres of floor space. These units are expected to house micro-businesses, similar to those in the applicant's self-storage developments in Calgary. For example, a shipping logistics company that uses several storage units to hold inventory, a home staging/furniture sales company, a therapist, etc.

Cultural Context

The Paskapoo Slopes are known to contain First Nations artifacts of Blackfoot origin that have a significant archeological history. A former buffalo kill site is in the vicinity, which was one of the largest in the Calgary area. In alignment with updated ASP policies and the outline plan, which were approved in 2015 (LOC2014-0080), Traditional Knowledge Keepers and Indigenous Elders have been engaged in the design of the Medicine Hill community. These engagements informed the ASP policies which contain specific design guidance that is currently reflected in the site design, public art, and landscaping of developments in the Medicine Hill community.

The cultural and historical significance of the area is represented in the proposed development through a large buffalo mural proposed on the two most prominent building façades along the Trans-Canada Highway. The image illustrates the intimate connection the Blackfoot People have with the land as well as the significance of their relationship with the plains bison. Additional cultural symbology incorporated into the proposed façade design include motifs, logos and colours that create a unifying element that reinforces the Blackfoot Indigenous context. The Indigenous themes and representations are also proposed to be supplemented through the public realm design, with the installation of a storytelling panel adjacent to Na'a Drive SW.

The vision of the cultural elements included in the proposed development design were reviewed and endorsed by the Blackfoot Traditional Knowledge Keeper, Elder Duane Mistaken Chief.

Urban Design Review Panel

The applicant presented the proposed development to the Urban Design Review Panel (UDRP) on 2025 June 25. The Panel endorsed the proposal but suggested improvements to the site's northern and western interfaces with the Trans-Canada highway, providing more articulated frontage along Na'a Drive SW and having more purposeful landscaping. Administration worked with the applicant after UDRP review to revise the development permit drawings in response to both UDRP and Office of Urban Design (OUD) comments. The applicant made changes to the proposed development by enhancing the public realm along Na'a Drive SW with layered landscaping features below the windows of the commercial units to articulate the street edge and to highlight access points into the units. Other changes were also made to minimize the visual impacts of the parking and loading area near Na'a Drive SW, using additional landscaping buffers to reduce the amount of impervious surfaces on the site.

Transportation

The site fronts onto Na'a Drive SW, which is a collector road that connects to the Trans-Canada Highway on the west and Sarcee Trail on the east. Pedestrian connectivity in the neighbourhood is provided through the sidewalks along Na'a Drive SW.

Transit service to Winsport, Crestmont, Market Mall, University of Calgary and Brentwood Station is provided by local bus stops within a short walking distance from the subject site. Stops for east and westbound Route 108 (Paskapoo Slopes) are located on Na'a Drive SW approximately 120 metres away (a two-minute walk).

Vehicle access to the subject site would be provided from Na'a Drive SW. There are currently no parking restrictions on Na'a Drive SW.

Environmental Site Considerations

There are no environmental concerns to note.

Utilities and Servicing

Public water, sanitary and storm sewer mains exist in the adjacent public rights-of-way and have sufficient capacity to support this development.

Legislation and Policy

South Saskatchewan Regional Plan (2014)

Administration's recommendation aligns with the policy direction of the [South Saskatchewan Regional Plan](#), which directs population growth in the region to cities and towns, and promotes the efficient use of land.

Municipal Development Plan (Statutory – 2009)

Map 1: Urban Structure of the [Municipal Development Plan](#) (MDP) identifies the subject site as being within the Developing Residential Area and is referenced as a Planned Greenfield Community with Area Structure Plan. The MDP supports the development of complete communities including a mix of uses to ensure a compact urban form that efficiently utilizes land and infrastructure and supports local commercial and other services. This also includes resilient neighbourhoods that feature architectural and natural elements that contribute to a local identity and sense of place.

The proposed development design integrates well with its surrounding commercial and multi-residential context, which aligns with the policies of the MDP.

Calgary Climate Strategy (2022)

Administration has reviewed this application in relation to the objectives of the [Calgary Climate Strategy – Pathways to 2050](#) programs and actions. The applicant has proposed the installation of an electric vehicle charging station in line with the actions set out in Program Pathway F: Zero Emissions Vehicles – Accelerate the Transition to Zero Emissions Vehicle.

Canada Olympic Park and Adjacent Lands Area Structure Plan (Statutory – 2005)

Map 2: Land Use Concept of [Canada Olympic Park and Adjacent Lands Area Structure Plan](#) (ASP) identifies the subject site as being along the 'Main Street' portion of the 'Commercial District'. The Commercial District is intended to accommodate a wide range of commercial uses that will serve as a draw for residents from surrounding communities and provide everyday

services and amenities for local residents. The applicable policies support predominantly mixed use development that includes residential, office and other commercial or institutional uses preferably located above at grade retail uses. The ASP also indicates that buildings that are adjacent to the Main Street shall be designed to enhance the public realm with entryways to ground-floor residential and commercial units along the Main Street having individual, direct access to the sidewalk.

The proposed development will promote activity along the Main Street, support the needs of households, home-based businesses and small retailers in the area. The Self Storage Facility may also make higher density living and business operations in the area more appealing. The proposal aligns with the ASP policies.

Land Use Bylaw 1P2007

Administration highlights the following relaxations to the Land Use Bylaw 1P2007 in the table below. Administration has reviewed each relaxation individually and considers each relaxation to be acceptable for the reasons outlined in the table below.

Bylaw Relaxations			
Regulation	Standard	Provided	Administration's Rationale for Supporting a Relaxation
783 Building Façade	(1) The length of the building façade that faces the commercial street must be a min. of 80.0% of the length of the property line it faces. (2) In calculating the length of the building façade, the depth of any required rear or side setback areas referenced in sections 788 and 789 will not be included as part of the length of the property line.	Plans indicate the length of the building façade is 55.43m (-33.72m) or 49.74% or (30.26%) of the property line it faces.	The parcel configuration, the site grading condition, including the singular street frontage limits the ability to meet this bylaw rule. The relaxed façade rule is not expected to hinder the DC District's goal of creating pedestrian-friendly frontages and entrances near sidewalks and streets.
785 Use Area	(1) Unless otherwise referenced in subsection (3), the max. use area for uses on the ground floor of buildings in the Commercial – Corridor 1 District is 465.0m ² .	Plans indicate a use area of 2423.51m ² (+1958.51m ²).	The physical condition of the site limits the ability to meet this bylaw rule, which anticipates multiple storefront units (CRUs) on the ground floor facing a Na'a Drive SW. The gradient drop of approximately 4.0 metres in both the east and north directions require the second floor to be treated like a ground floor instead.

			<p>The relaxation is justified as the location requirement for ground floor commercial uses, stipulated in Section 9 of the DC District, remains unaffected.</p>
<p>790 Landscaping In Setback Areas</p>	<p>(4) Where a setback area shares a property line with a parcel designated as a commercial, industrial or special purpose district, the setback area: (a) be a soft surfaced landscaped area.</p>	<p>Plans indicate a portion east setback area is not soft surfaced landscaped.</p>	<p>Due to the Alberta Building Code requirement for the provision of a fire safety exit door out of the east side of the building, a sidewalk to allow people to get to Na'a Drive SW and away from the building safely is warranted.</p> <p>The proposed relaxation is considered minimal, as the eastern setback continues to provide sufficient soft landscaping to effectively buffer the adjacent property.</p>

Conditions of Approval

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning

1. Submit a complete digital set of the amended plans in PDF format and a separate PDF that provides a point-by-point explanation as to how each of the Prior to Release conditions were addressed and/or resolved. The submitted plans must comprehensively address the Prior to Release conditions as specified in this document. Ensure that all plans affected by the revisions are amended accordingly (**red-clouded for easy identification**). To arrange the digital submission, please contact your File Manager directly.
2. Amend landscape plan (drawing sheet A1.2) and correctly indicate the direction of the arrow pointing to the primary entrance for the retail unit (The Nest) from Na'a Drive SW.
3. Protection fencing along the boundary of the MR shall be installed prior to the commencement of any stripping and grading operations. This fencing is to be inspected and approved by the Parks Development Inspector Annie Rodrigues at 403-804-9397.

Utility Engineering

4. Follow the submission requirements outlined in Section 2 of the *Code of Practice for Erosion and Sediment Control* and either submit the required ESC Plan or a Written Notice and the ESC Plan. All submissions should be sent to ESC@Calgary.ca.

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Instruction Manual for Erosion and Sediment Control in Calgary* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Certified Professional in Erosion and Sediment Control (CPESC), Professional Engineer (P. Eng.), Professional Licensee (P.L. Eng), or Professional Agrologist (P. Ag.). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

The following City of Calgary Erosion and Sediment Control Documents can be obtained on the [ESC Approvals web page](#):

- a. *Erosion and Sediment Control Plan Application*
- b. *Instruction Manual for Erosion and Sediment Control in Calgary*
- c. *Standard Specifications Erosion and Sediment Control*

- d. *Erosion and Sediment Control Guidelines*
- e. *Code of Practice for Erosion and Sediment Control*

If you have any questions, contact 3-1-1. A Service Request (SR) will be created for the Stormwater Pollution Prevention Team.

- 5. Submit a Development Site Servicing Plan for review and acceptance from Utility Specialists, as required by Section 5 (2) of the Utility Site Servicing Bylaw 33M2005. Contact [Utility Specialists](#) for additional details. For further information, refer to the following:

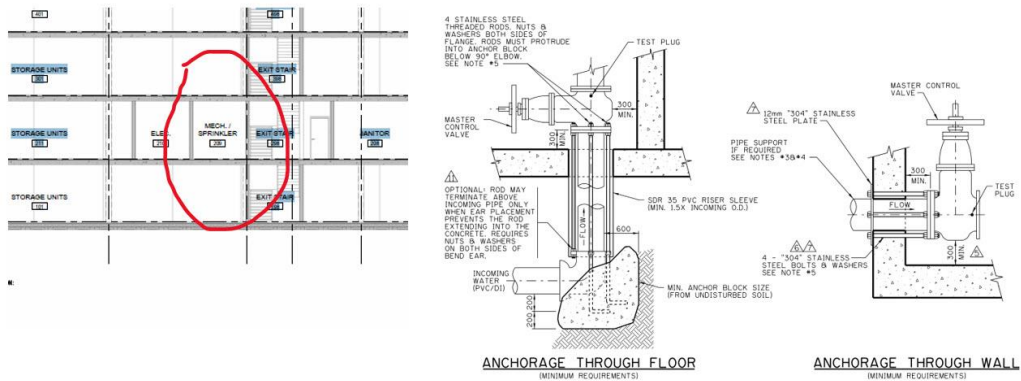
Development Site Servicing Plan Webpage

<https://www.calgary.ca/uep/water/specifications/water-development-resources/development-site-servicing-plans.html>

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/pd/Pages/Permits/carl-building-development-permit-search.aspx>

- 6. Amend floor plans to indicate the water metre room location is in accordance with the approved DSSP.



Mobility Engineering

- 7. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads:

- a. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel.

8. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Roads:

Street lighting upgrading adjacent to site frontage (If required).

Permanent Conditions

The following permanent conditions shall apply:

Planning

9. All rules of Land Use Bylaw 1P2007 apply, subject to any relaxations approved by the Development Authority in this development permit.
10. The development shall be completed in its entirety, in accordance with the approved plans and conditions. The stamped and signed plans are a legal document.
11. No changes to the approved plans shall take place unless authorized by the Development Authority. If changes to the development occur or are proposed, a new development permit or revised plan application may be required.
12. A development completion permit must be issued for the development before the use is commenced or the development occupied. A development completion permit is independent from the requirements of City of Calgary Building Regulations inspections and permission for occupancy. Request a development completion permit inspection by visiting inspections.calgary.ca or call 403-268-5311.
13. All roof top mechanical equipment shall be screened as shown on the approved plans.
14. All areas of soft landscaping must be watered as identified on the approved plans.
15. Parking and landscaping areas must be separated by a 150mm (6 inch) continuous, poured in place, concrete curb or equivalent material to the satisfaction of the Development Authority, where the height of the curb is measured from the finished hard surface.
16. All electrical servicing for freestanding light standards must be provided from underground.

17. Each parking stall, where located next to a sidewalk, must have a properly anchored wheel stop. The wheel stop must be 100mm in height and 600mm from the front of the parking stall.
18. Barrier free parking stall(s) shall be clearly designated, signed and located near to or adjoining a barrier-free path of travel leading to the nearest barrier-free entrance.
19. A lighting system to meet a minimum of 10 LUX for uncovered parking areas with limited public access and 22 LUX for shopping areas with uncovered parking areas with a uniformity ratio of 4:1 on pavement shall be provided.
20. Light illumination from the proposed development shall minimize any negative impacts at night, such that:
 - (a) Exterior light fixtures shall not allow light to escape from sides and must prevent upward glare. The color temperature of all exterior lighting fixtures shall be limited to between of 2,700 - 3,000 Kelvin (K); and
 - (b) Any signage with lighting, including halo-illuminated letters must be backlit and dimmable, with no upward light spill and the colour temperature must not exceed 2700K. The luminance shall also be limited to a maximum of 40 candela per square meter (cd/m²).
21. When the main floor of each building is constructed, submit the geodetic elevation to Geodetic.Review@Calgary.ca
22. A letter of confirmation from a certified electrical engineer or Master Electrician shall be provided to the Development Authority, prior to the issuance of the Development Completion Permit, certifying that all Electric Vehicle Parking Stalls identified on the approved plans have been completed, are fully operational, and support [level 2 EVSE installation](#) (charger).
23. The community name approved by Council is "Medicine Hill". Signs and any future drawings or plans associated with the subject parcel must not include "Trinity" or "Trinity Hills" as a name or phrase.
24. Any damage to public parks, boulevards or trees resulting from development activity, construction staging or materials storage, or construction access will require restoration at the developer's expense. The disturbed area shall be maintained until planting is established and approved by the Parks Development Inspector. Contact the Development Inspector Annie Rodrigues at 403-804-9397 for an inspection.

Any surface or subterranean damage to public parks resulting from the installation of building construction tie-backs or other construction practices requires remediation at the developer's expense, to the satisfaction of the Director, Parks. All materials associated with the encroachments must be removed and any subterranean and surface disturbances to the parcel must be remediated. All site remediations must be approved by the Parks Development

Inspector. Contact the Development Inspector Annie Rodrigues at 403-804-9397 for an inspection.

25. In order to ensure the integrity of existing public trees and roots, no grade changes are permitted in the boulevard within the drip lines of the trees.
26. In order to ensure the integrity of existing public trees and roots, there shall be a minimum 3 metre separation, ideally the full length of the canopy, between the trunk and any new/proposed structures, (i.e. driveways and walkways).
27. Tree protection information given as per the approved development permit does not constitute Tree Protection Plan approval. Tree Protection Plan approval must be obtained separately through Urban Forestry. Visit www.calgary.ca, call 311, or email tree.protection@calgary.ca for more information.
28. Point source drainage from the development site onto the adjacent park / municipal reserve is not permitted, as such drainage will compromise the integrity of the site.
29. All impacts to pathways including the regional and local pathways required for project execution shall adhere to the Pathway Closure and Detour Guidelines. A permit is required for all activities within 5m of a pathway. Coordinate with Calgary Parks, Pathways - pathways@calgary.ca prior to the start of construction regarding proposed changes and impacts to the existing pathway system in the area.
30. Any landscape rehabilitation on public parks shall be performed and inspected in accordance with Parks Development Guidelines and Standard Specifications - Landscape Construction (current edition). Applicant is to contact the Parks Development Inspector Annie Rodrigues at 403-804-9397.
31. A 1.2 m high chain-link fence (or suitable equivalent) shall be installed (fully within private property) and remain on the property line along all shared boundaries with adjacent municipal reserve.
32. There shall be no construction access through the adjacent park / municipal reserve lands.
33. Public trees located on the park / municipal reserve adjacent to the development site shall be retained and protected unless otherwise authorized by Urban Forestry. Prior to construction, install a temporary fence around the extent of the branches ("drip line") and ensure no construction materials are stored inside this fence.
34. Stormwater or other drainage from the development site onto the adjacent municipal reserve is not permitted. Any drainage from private lots onto the adjacent municipal reserve upon development completion of the subject site must be resolved to the satisfaction of the Director, Parks and any damage resulting from unauthorized drainage will require restoration at the developer's expense. Resolution of drainage issues must be approved by the Parks

Development Inspector. Contact the Development Inspector Annie Rodrigues at 403-804-9397 for an inspection.

35. Under the Historical Resources Act, developments on lands within Alberta that have been assigned a Historic Resource Value (HRV) may require Historical Resources Act approval. The parcel of land for this project has an HRV 5a,p in the provincial Listing of Historic Resources (<https://www.alberta.ca/listing-historic-resources>), meaning the project likely requires Historical Resources Act approval. Prior to any site work commencing, provide documentation of Historical Resources Act approval or proof of exemption from the province to the Development Authority. More information can be found at: <https://www.alberta.ca/apply-historical-resources-act-approval-development-project>
36. There are existing boulevard trees along Na'a Dr. within 6m of the development site. Existing street trees have not yet been given a Final Acceptance Certificate (FAC) and are not assets of Parks Urban Forestry yet. If this development will impact these street trees, please coordinate with the area developer for tree protection and tree removal accordingly.

Should the trees be given a FAC prior to any ground disturbance, the applicant will be required to provide compensation to the City of Calgary for any Public Trees that are removed or damaged. Applicants that are unfamiliar with tree protection or tree appraisal are advised to consult an arborist. Tree Protection Plan approval must be obtained separately through Urban Forestry. Tree protection information given as per the approved development permit does not constitute Tree Protection Plan approval.
37. Any tree planting in the City boulevard shall be performed and inspected in accordance with Parks' *Development Guidelines and Standard Specifications - Landscape Construction (current edition)*. Applicant is to contact the Parks Development Inspector Annie Rodrigues at 403-804-9397 to arrange an inspection.
38. Plant all public trees in compliance with the approved Public Landscaping Plan (if applicable). If there are conflicts between proposed boulevard trees and the development, the Applicant is to contact the Parks Development Inspector Annie Rodrigues at 403-804-9397 to resolve these issues or submit a new Line Assignment Landscape Construction Drawing.

Utility Engineering

39. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

For all soil disturbing projects, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions. The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

40. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
 - a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment and Protected Areas and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Climate and Environment (Contaminated Sites Section) must be immediately notified (311).
41. Stormwater runoff must be contained and managed in accordance with the Stormwater Management & Design Manual all to the satisfaction of the Coordinator, Utility Specialist.
42. The grades indicated on the approved Development Site Servicing Plan must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.
43. No trees, shrubs, buildings, permanent structures, or unauthorized grade changes are permitted within the utility rights-of-way.

Mobility Engineering

44. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the applicant's expense, upon completion of the foundation. Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at roadsia@calgary.ca
45. The developer shall be responsible for the cost of public work and any damage during construction in City road rights-of-way, as required by the Manager, Transportation Planning. All work performed on public property shall be done in accordance with City standards.

Applicant Submission



Michael Miller, Architect, AAA, B.Arch., M.R.A.I.C., Principal

Development Permit Applicant Statement

Project: Bluebird Self Storage Facility w/ Retail and Office Spaces

Location: 1152 Na'a Drive SW

Applicant: Axiom Architecture Inc.

Date: March 24, 2026

Introduction & Purpose of Application

This application is for a Development Permit to construct a new self-storage building located at **1152 Na'a Drive SW**. The proposed development is intended to provide a self-storage facility with additional office space and is designed to enhance the surrounding area by providing modern, efficient, and accessible space for businesses and community use.

The intent of this application is to ensure compliance with the **City of Calgary Land Use Bylaw**, relevant Area Redevelopment Plans (ARP) or Area Structure Plans (ASP), and to align with the **Municipal Development Plan and Calgary Climate Strategy goals for sustainable growth**.

Site Context and Surrounding Area

The subject site is located within the **Medicine Hills** area and is currently zoned **Direct Control – 35D2025 (C-COR1)**. It is surrounded by:

- **North:** Trans-Canada Highway (Hwy 1) and Residential Neighborhood
- **South:** Residential Apartments and Mixed-use Commercial Buildings
- **East:** Residential Apartments/Condos
- **West:** Green Space and Residential Apartments

This location was selected due to the high population density of the Medicine Hill area. The building provides services to residents, takes advantage of existing transit routes, encourages walkability. The inclusion of storefront retail of the self-storage facility, strengthens the mixed-use character of the corridor, supports nearby residential development, and contributes to a more active and pedestrian-friendly streetscape.

As the building is located near major corridors, the building acts as a visual gateway into Medicine Hill and to the City of Calgary. The development has been designed to minimize impacts on surrounding properties by incorporating appropriate setbacks, landscaping buffers, and traffic flow considerations.

Project Description

The proposed industrial building will be 122,348.88sqft (11,366.58sqm), consisting of 5 storeys of Self Storage with additional office space facing Na'a Drive SW.

Key design elements include:

- **Architectural Style:**
 - The building uses varied materials and colours, façade articulation, and bold design elements to transcend conventional self-storage architecture.
 - Rhythmic patterns, modern textures, and integrated lighting create a refined, urban character, that pays respect to indigenous visual cues, resourced from the history of Medicine Hill.

Calgary Office: 5917 - 1A Street SW, Calgary, Alberta, T2H 0G4, Phone: 403-253-2853, Fax: 403-253-3078
Red Deer Office: Suite 100, 550 Laura Ave, Red Deer County, Alberta, T4E 0A5, Phone: 403-358-3311

- **Cultural and Historical Aspects:**
 - The design of the building incorporates a large mural composition on two facades that references the natural and cultural history of the site.
 - Throughout the process, the mural design was developed in consultation with the Indigenous Relations Office (Lorna Crowshoe) at the City of Calgary.
 - Specifically, the image speaks to the intimate connection the Blackfoot People have with the land as well as the significance of their historical relationship with the plains bison, a vision that was reviewed and endorsed by the Blackfoot Knowledge Keeper, Duane Mistaken Chief.
 - Cultural symbology presented throughout the building façade design creates a unifying element that reinforces local site context. The cultural design brings a visual framework to life and contributes to an important gateway experience to our city from the west, presenting a strong, artistic component at a scale that has significant design impact.
 - The cultural elements are consistent with and speak the same design language established throughout the development of the broader site. This project continues the story of the bison, and the Blackfoot People as told through the vehicle of public realm design.
- **Height:**
 - 17.3m (56'-2 ½") – within the maximum height [20.0m (65'-7 13/32")] allowed by the Land Use Bylaw.
- **Building Placement:**
 - Designed to enhance the street frontage and encourage pedestrian activity.
- **Parking & Access:**
 - Vehicle access is provided via Na'a Drive SW.
 - Parking stalls: 21 stalls, meeting City of Calgary minimum standards.
 - Bicycle parking: 2 stalls to encourage alternative transportation, meeting City of Calgary minimum standards.
- **Landscaping:**
 - Native, drought-tolerant plantings to align with the City's sustainability goals.
 - Green buffers along property edges to reduce visual impact.
- **Buffalo Pound Historic Plaque:**
 - A historic plaque to be installed within the public realm of development. (Final location to be determined)
 - Plaque to tell a brief story about the buffalo pound as represented by the Mural along the North and East sides of the building.

Alignment with City of Calgary Policies & Bylaws

This project has been designed to **comply with applicable policies and regulations**, including:

- **Land Use Bylaw 1P2007:** Ensuring conformance with setback, height, and parking requirements.
- **Municipal Development Plan (MDP):** Supporting mixed-use, compact, and sustainable growth.

- **Calgary Climate Strategy:** Incorporating durable and low-maintenance materials that reduce life-cycle environmental impact. The exterior envelope is composed of durable insulated concrete and weather-resistant materials. The design considers energy-efficient systems, including LED lighting and HVAC controls, supporting reduced energy consumption.

By aligning with these policies, this development will contribute to a vibrant, economically sustainable, and environmentally conscious community.

Community Benefits and Considerations

The proposed development will provide several benefits to the surrounding area, including:

- Increased local employment opportunities.
- Modern, accessible space for businesses and community services.
- Improved streetscape and pedestrian experience through enhanced landscaping and building placement.
- Support for public transit use with nearby bus routes and walkable design.
- Integration of low-impact development features, such as efficient stormwater management.

Potential impacts, such as traffic, noise, and light pollution, have been carefully considered and mitigated through thoughtful design, landscaping, and site planning.

Conclusion

This proposed development represents a responsible, sustainable investment in the community and aligns with the City of Calgary's long-term vision for growth and development.

We respectfully request approval of this Development Permit so that we may proceed with delivering a high-quality building that will enhance the local area, support economic growth, and meet community needs.

Thank you for your consideration.

Submitted by:

Scott Bundziak

Email: ScottB@AxiomArch.ca

Phone: (403) 505-7897

Axiom Architecture Inc.

100 – 550 Laura Ave,

Red Deer County, AB T4E 0A5

Development Permit Plans

STOREWEST - MEDICINE HILL STORAGE

LOT: 12 BLOCK: 3 PLAN: 251 0905
1152 NA'A DRIVE S.W., CALGARY, ALBERTA

U 2026-02-24 ISSUED FOR 'DART' DECISION MEETING

Axiom
Architecture Inc.
Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311 www.AxiomArch.ca



ARCHITECTURAL
PROJECT # 25-010

CLIENT



DEVELOPER



DRAWING LIST (DEVELOPMENT PERMIT)		
ARCHITECTURAL (AXIOM ARCHITECTURE INC.)		
DWG #	DRAWING DESCRIPTION	OVERALL REV U
A0.1	OVERALL CONTEXT PLAN	U
A0.2	ENLARGED CONTEXT PLAN AND BYLAW INFORMATION	U
A0.3	FLOOD & AVA MAPS	U
A1.1	SITE PLAN	U
A1.2	LANDSCAPE & EXISTING TREE PLAN	U
A1.3	SITE & LANDSCAPE DETAILS	U
A1.4	SITE PHOTOS	U
A1.5	ADJACENT CONTEXT SITE PHOTOS	U
A1.6	SHADOW STUDY (SPRING EQUINOX & SUMMER SOLSTICE)	U
A1.7	SHADOW STUDY (AUTUMN EQUINOX & WINTER SOLSTICE)	U
A1.8	SHADOW STUDY (WINTER SOLSTICE)	U
A2.1	MAIN FLOOR PLAN & BUILDING CODE INFORMATION	U
A2.2	SECOND FLOOR PLAN	U
A2.3	THIRD FLOOR PLAN	U
A2.4	FOURTH & FIFTH FLOOR PLAN	U
A2.5	ROOF PLAN	U
A3.1	EXTERIOR ELEVATIONS	U
A3.2	EXTERIOR ELEVATIONS	U
A3.3	EXTERIOR BUILDING RENDERINGS	U
A3.4	EXTERIOR BUILDING RENDERINGS	U
A3.5	EXTERIOR BUILDING RENDERINGS	U
A4.1	BUILDING SECTIONS	U
A5.1	GLAZING COMPLIANCE EXTERIOR ELEVATION	U



OVERALL CONTEXT PLAN
SCALE: N.T.S.

CLIENT:

EAGLE BUILDERS

MECHANICAL / ELECTRICAL:

KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0232
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:

Grubb Engineering

ARCHITECTURAL:

Axiom Architecture Inc.
Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311
www.AxiomArch.ca

AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 N/A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

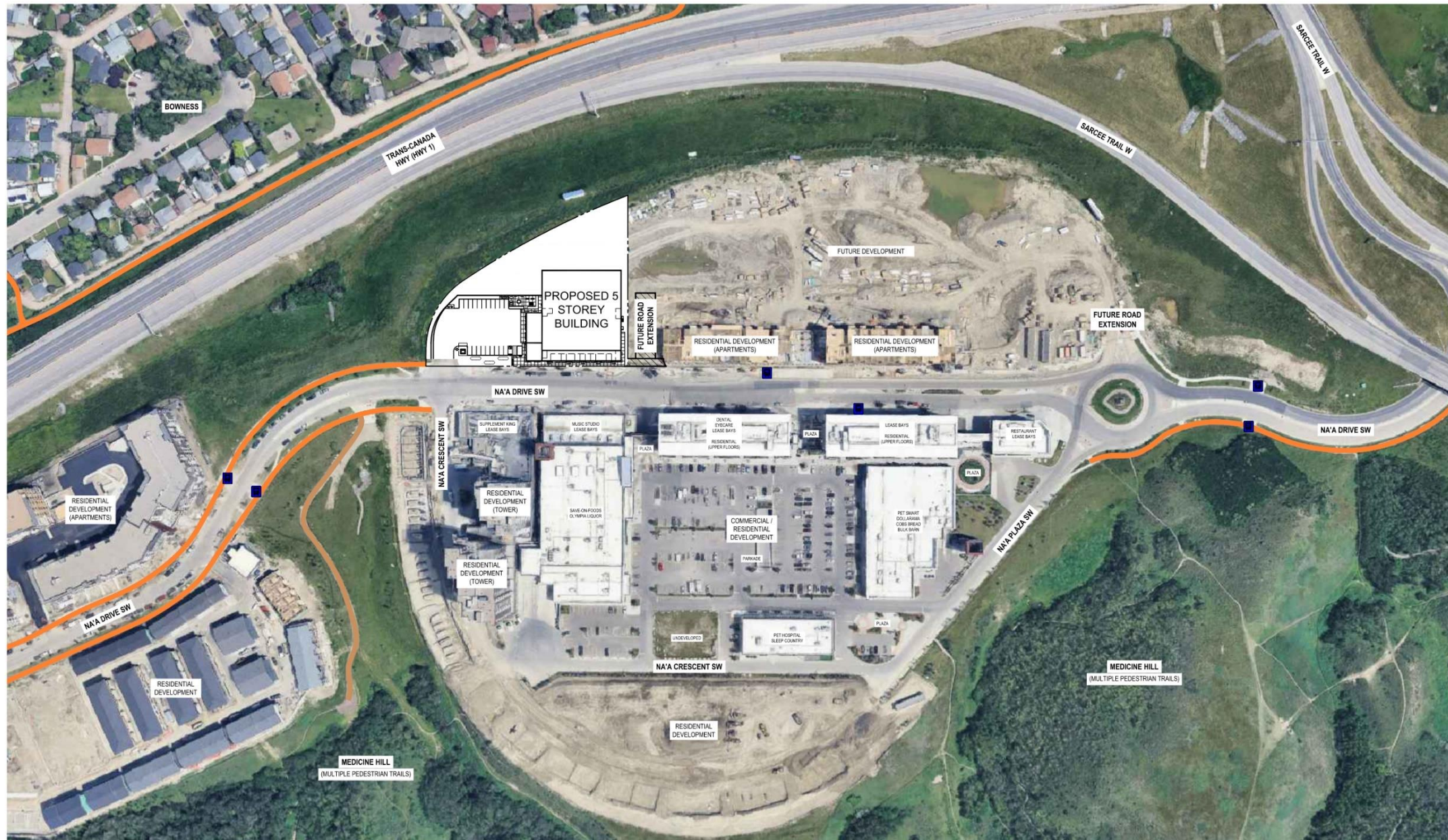
ISSUE	REV.	DATE	DESCRIPTION
U	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

DRAWING TITLE:

OVERALL CONTEXT PLAN

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A0.1

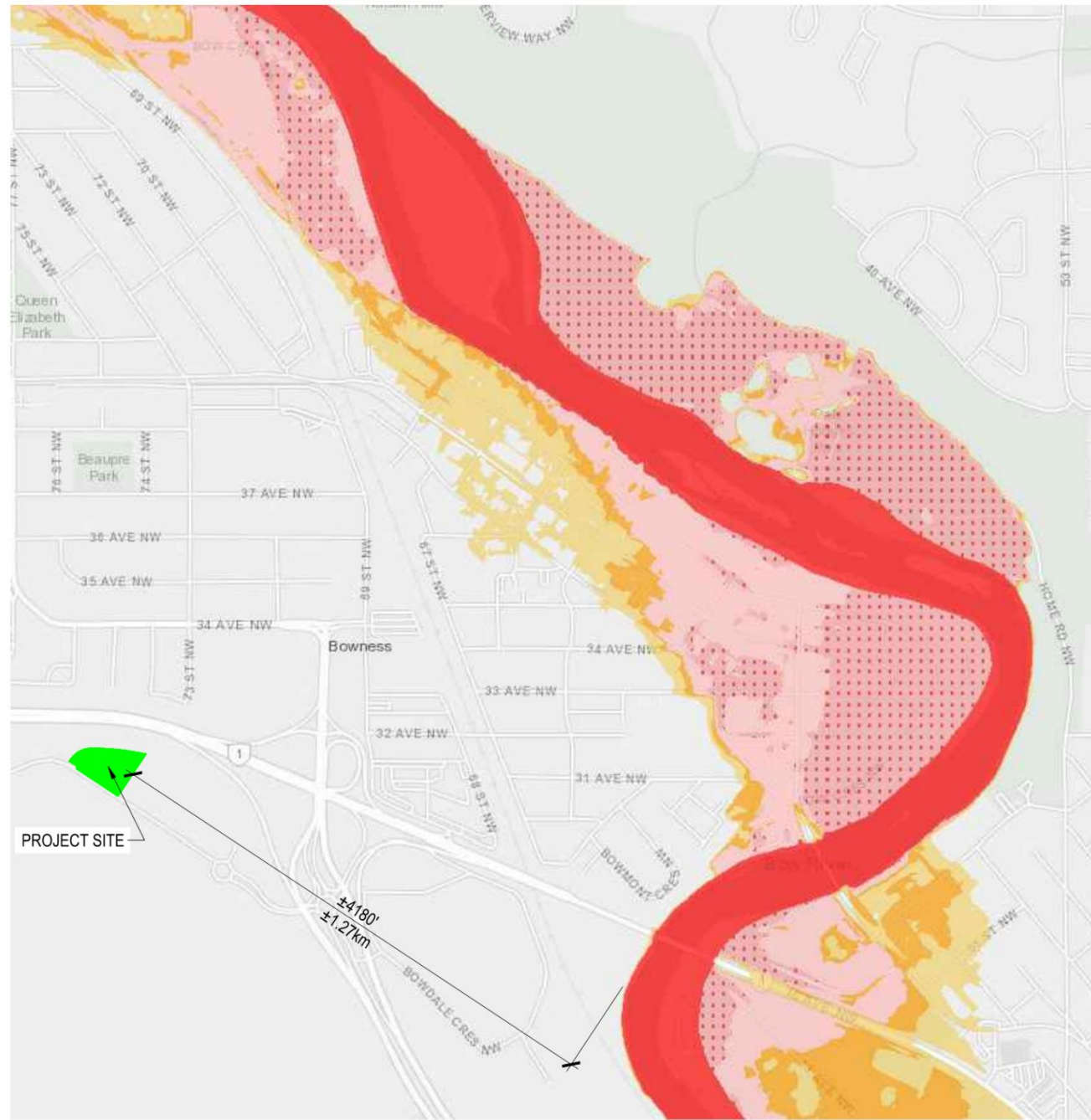


BYLAW INFORMATION		LAND USE BYLAW # 35D2025 & 1P2007
ZONING:		
DIRECT CONTROL - 35D2025 (C-COR1)		
SITE SIZE:		
8,099.59 sqm (87,163.24 sqft) (2.00 acres)		
SITE COVERAGE:		
REQUIRED:	AS DETERMINED BY DEVELOPMENT AUTHORITY	
PROVIDED:	33.41% COVERAGE	
BUILDING AREAS:		
MAIN FLOOR	2,165.21 sqm (23,306.11 sqft)	
SECOND FLOOR / FOOTPRINT	2,705.75 sqm (29,124.44 sqft)	
THIRD FLOOR	2,165.21 sqm (23,306.11 sqft)	
FOURTH FLOOR	2,165.21 sqm (23,306.11 sqft)	
FIFTH FLOOR	2,165.21 sqm (23,306.11 sqft)	
GROSS FLOOR AREA	11,366.58 sqm (122,348.88 sqft)	
F.A.R.	1.40 (2.0 MAXIMUM)	
BUILDING HEIGHT:		
REQUIRED:	MAX. 20.0m (65'-7" 13/32")	
PROVIDED:	17.13 m (56'-2" 1/2")	
FRONT YARD SETBACK: (N/A DRIVE S.W.)		
REQUIRED:	MAX. 3.0m (9'-10" 1/8")	
PROVIDED: (S)	3.0m (9'-10" 1/8")	
REAR YARD SETBACK:		
REQUIRED:	3.0m (9'-10" 1/8")	
PROVIDED: (N)	21.0m (68'-10" 11/16")	
SIDE YARD SETBACKS:		
REQUIRED:	3.0m (9'-10" 1/8")	
PROVIDED: (E)	3.68m (12'-0" 7/8")	
PROVIDED: (W)	36.37m (119'-4")	
LANDSCAPING REQUIREMENTS:		
SEE LANDSCAPE BYLAW INFORMATION ON A1.2 FOR MORE INFORMATION		
PARKING SPACES:		
REQUIRED:	SELF STORAGE: N/A OFFICE: 1 STALL / 100 sqm (281.81 sqm / 100 = 2.8 [3] STALLS) RETAIL: 1 STALL / 250 sqm (67.08 sqm / 250 = 0.39 [1] STALLS)	
TOTAL REQUIRED:	4 STALLS	
PROVIDED:	21 STALLS (20 REGULAR & 1 BARRIER FREE)	
STALLS LABELED AS VISITOR (V), STAFF (S) & OFFICE (O)		
LOADING SPACES:		
REQUIRED:	1 STALL / 9,300 sqm OF G.F.A. (11,366.58 sqm / 9,300 = 1.2 [2] STALLS)	
PROVIDED:	3 STALLS	
BICYCLE PARKING:		
REQUIRED: (CLASS 1)	MINIMUM FOR EACH DWELLING UNIT AND LIVE WORK UNIT IS 0.5 STALLS PER UNIT WHEN TOTAL NUMBER OF UNITS EQUALS OR EXCEEDS 20.	
REQUIRED: (CLASS 2)	MINIMUM FOR EACH DWELLING UNIT AND LIVE WORK UNIT IS 2.0 STALLS PER UNIT WHEN TOTAL NUMBER OF UNITS IS EQUAL TO OR LESS 20.	
MINIMUM FOR EACH DWELLING UNIT AND LIVE WORK UNIT IS 0.1 STALLS PER UNIT WHEN TOTAL NUMBER OF UNITS IS GREATER THAN 20.		
ALL OTHER USES IS 5% OF THE NUMBER OF MOTOR VEHICLE PARKING STALLS		
REQUIRED:	5% OF 21 MOTOR VEHICLE STALLS = (1.05) 2 STALLS	
PROVIDED:	2 STALLS	
COMMERCIAL USE:		
REQUIRED:	MIN. 10% OF THE GROUND FLOOR G.F.A. MUST CONTAIN COMMERCIAL USES (10% OF 2,705.75sqm [29,124.44sqft] = 270.58sqm [2,912.50sqft])	
PROVIDED:	281.81sqm [3,033.38sqft]	
REQUIRED:	MIN. 60% OF THE LENGTH OF THE FACADE LOCATED ON THE GROUND FLOOR AND FRONTING N/A DRIVE SW MUST CONTAIN COMMERCIAL USES (60% OF 55.37m = 33.22m)	
PROVIDED:	46.10m	
RULES FOR FACADES:		
SEE ADDITIONAL INFORMATION ON A5.1.		
REQUIRED:	THE FACADE OF A BUILDING ON THE GROUND FLOOR FACING N/A DRIVE SW MUST PROVIDE WINDOWS OF TRANSPARENT AND UNOBSCURED GLASS THAT OCCUPY A MIN. 50% OF THE FACADE BETWEEN THE HEIGHT OF 0.6m AND 2.4m (50% OF 99.67sqm [1,072.84sqft] = 49.84sqm [536.47sqft])	
PROVIDED:	55.73 sqm [599.84sqft] (55.9%) UNOBSCURED GLASS	
RULES FOR SELF STORAGE FACILITY:		
SEE ADDITIONAL INFORMATION ON A3.3.		
INDIVIDUAL ACCESS TO EACH SELF STORAGE UNIT MUST BE ENTIRELY INTERNAL TO THE BUILDING.		
REQUIRED:	THE FACADE OF A BUILDING LOCATED ABOVE THE GROUND FLOOR AND FACING A STREET MUST PROVIDE WINDOWS WITH UNOBSCURED GLASS THAT OCCUPY A MINIMUM OF 15% OF THE FACADE. THE PERIMETER WINDOWS ABOVE THE GROUND FLOOR MUST BE ABUTTED BY INTERNAL CIRCULATION CORRIDORS THAT ACCESS THE SELF STORAGE UNITS, OR BY COMMON AREAS (15% OF 439.46sqm [4,703.31sqft] = 65.92sqm [709.56sqft])	
PROVIDED:	71.21sqm [766.50sqft] (16.2%) UNOBSCURED GLASS	
REQUIRED:	A SELF STORAGE FACILITY MUST NOT EXCEED 90% OF THE GROUND FLOOR G.F.A. OF A BUILDING (90% OF 2,705.75sqm [29,124.44sqft] = MAX. 2,435.18sqm [26,212.06sqft])	
PROVIDED:	2,165.21sqm [23,306.13sqft]	

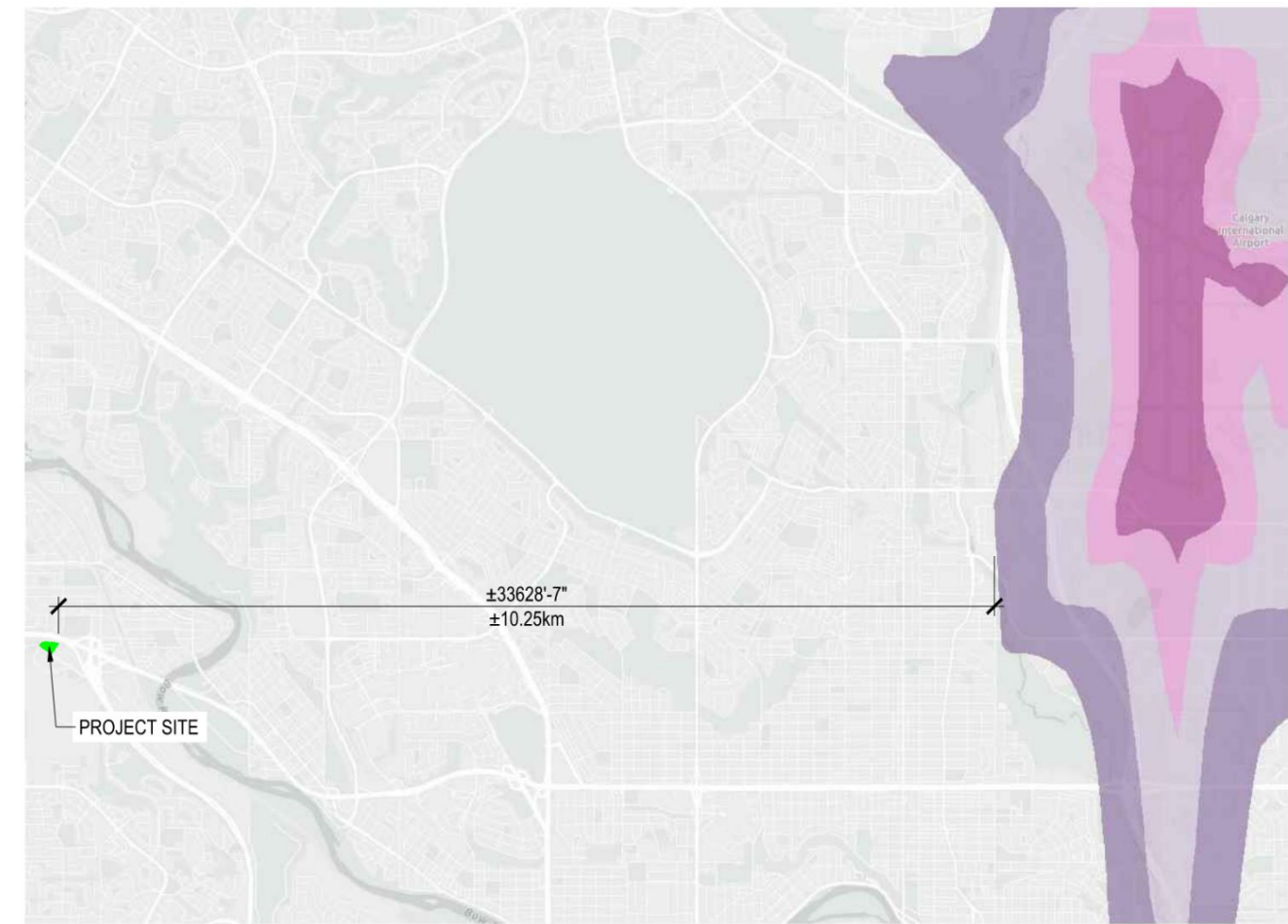
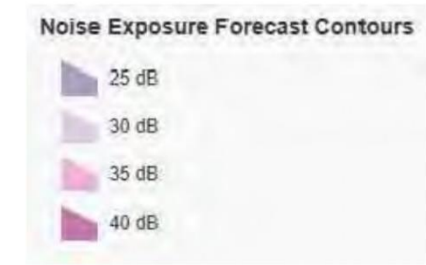
ENLARGED CONTEXT PLAN
SCALE: N.T.S.

- SYMBOL LEGEND**
- CITY OF CALGARY BUS STOP
 - TRAIL
 - REGIONAL OFF STREET PATHWAY

<p>CLIENT:</p> <p>EAGLE BUILDERS</p>	<p>MECHANICAL / ELECTRICAL:</p> <p>KB ENGINEERING LIMITED RED DEER COUNTY, ALBERTA, CANADA PHONE: 587.377.0232 EMAIL: KEON@KBENGINEERING.CA</p>	<p>STRUCTURAL:</p> <p>Grubb Engineering</p>	<p>ARCHITECTURAL:</p> <p>Axiom Architecture Inc. Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5 PH: 403-358-3311 www.AxiomArch.ca</p> <p>AXIOM ARCHITECTURE INC. (RED DEER) PERMIT No. AC 13659 ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA</p>	<p>STAMPS:</p> <p>ISSUED FOR 'DART' DECISION MEETING</p>	<p>PROJECT INFORMATION:</p> <p>STOREWEST - MEDICINE HILL STORAGE</p> <p>LOT: 12 BLOCK: 3 PLAN: 251 0905 CIVIC ADDRESS: 1152 N/A DRIVE S.W. CALGARY, ALBERTA</p>	<p>REVISIONS:</p> <table border="1"> <thead> <tr> <th>ISSUE</th> <th>REV.</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td>U</td> <td>-</td> <td>2026-02-24</td> <td>ISSUED FOR 'DART' DECISION MEETING</td> </tr> <tr> <td>T</td> <td>-</td> <td>2026-02-20</td> <td>RESUBMITTED FOR DTR-3 COMMENTS</td> </tr> <tr> <td>S</td> <td>-</td> <td>2026-02-13</td> <td>LOCKER LAYOUT ISSUED FOR REVIEW</td> </tr> <tr> <td>R</td> <td>-</td> <td>2026-02-11</td> <td>ISSUED FOR SUBCONSULTANT INFORMATION</td> </tr> <tr> <td>Q</td> <td>-</td> <td>2026-01-14</td> <td>ISSUED FOR SUBCONSULTANT INFORMATION</td> </tr> <tr> <td>P</td> <td>-</td> <td>2026-01-12</td> <td>RESUBMITTED FOR DTR-2 COMMENTS</td> </tr> </tbody> </table>	ISSUE	REV.	DATE	DESCRIPTION	U	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING	T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS	S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW	R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION	Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION	P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS	<p>DRAWING TITLE:</p> <p>ENLARGED CONTEXT PLAN AND BYLAW INFORMATION</p>	<p>JOB NUMBER: 25-010 DATE: APR. 16, 2025 DRAWN BY: HKB</p> <p>A0.2</p>
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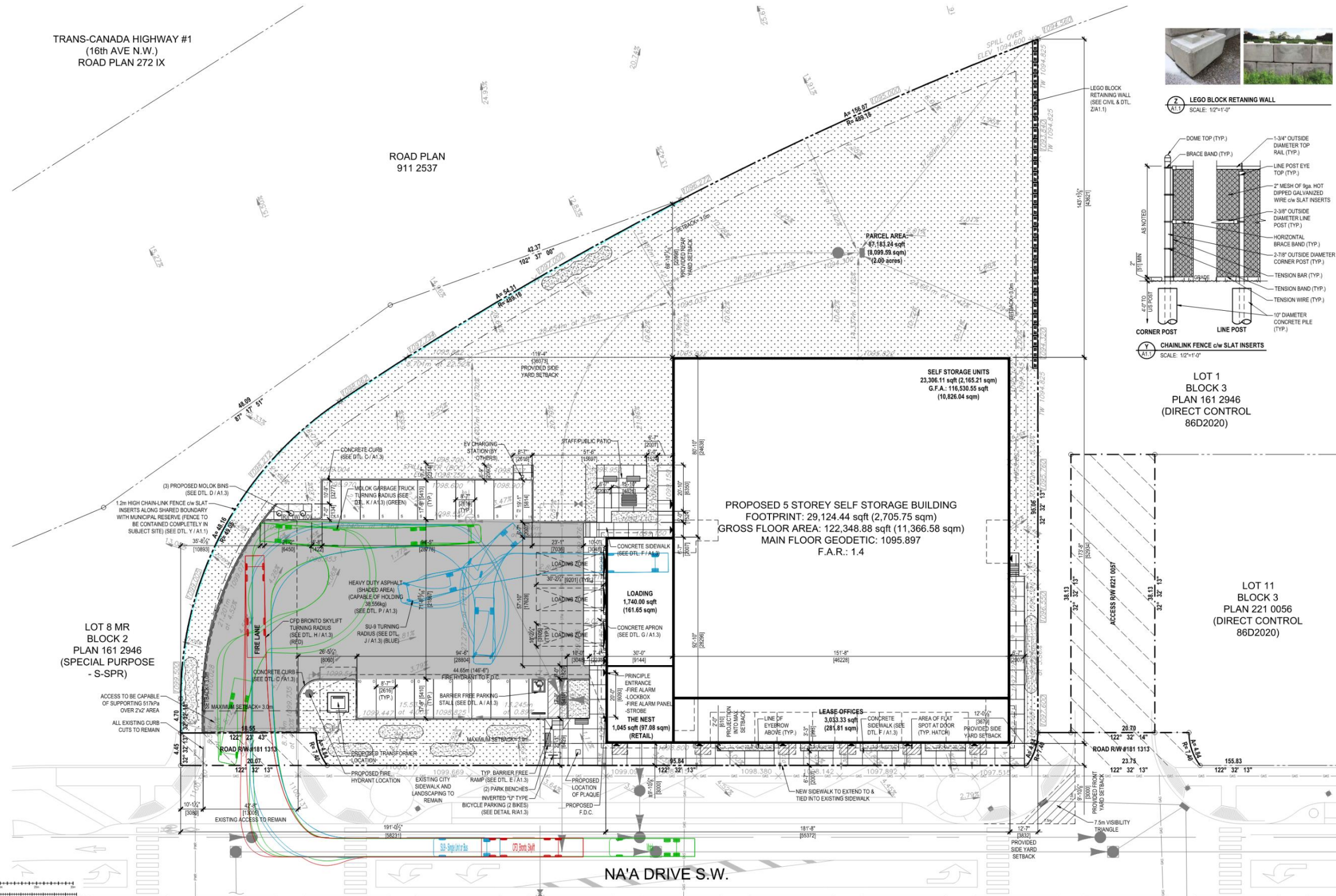
- Design Flood**
- Floodway
 - Flood Fringe
 - High Hazard Flood Fringe
 - Protected Flood Fringe
 - Overland Flow (Flood Fringe)
 - Flood Berm
- Larger Floods**
- 1:200 Flood
 - 1:500 Flood



FLOOD MAP
SCALE: N.T.S.

AVAP MAP
SCALE: N.T.S.

<p>CLIENT:</p>	<p>MECHANICAL / ELECTRICAL:</p>	<p>STRUCTURAL:</p>	<p>ARCHITECTURAL:</p> <p>AXIOM ARCHITECTURE INC. (RED DEER) PERMIT No. AC 13659 ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA</p>	<p>STAMPS:</p> <p style="text-align: center;">ISSUED FOR 'DART' DECISION MEETING</p>	<p>PROJECT INFORMATION:</p> <p>STOREWEST - MEDICINE HILL STORAGE</p> <p>LOT: 12 BLOCK: 3 PLAN: 251 0905 CIVIC ADDRESS: 1152 N/A DRIVE S.W. CALGARY, ALBERTA</p>	<p>REVISIONS:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>ISSUE</th> <th>REV.</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td>U</td> <td>-</td> <td>2026-02-24</td> <td>ISSUED FOR 'DART' DECISION MEETING</td> </tr> <tr> <td>T</td> <td>-</td> <td>2026-02-20</td> <td>RESUBMITTED FOR DTR-3 COMMENTS</td> </tr> <tr> <td>S</td> <td>-</td> <td>2026-02-13</td> <td>LOCKER LAYOUT ISSUED FOR REVIEW</td> </tr> <tr> <td>R</td> <td>-</td> <td>2026-02-11</td> <td>ISSUED FOR SUBCONSULTANT INFORMATION</td> </tr> <tr> <td>Q</td> <td>-</td> <td>2026-01-14</td> <td>ISSUED FOR SUBCONSULTANT INFORMATION</td> </tr> <tr> <td>P</td> <td>-</td> <td>2026-01-12</td> <td>RESUBMITTED FOR DTR-2 COMMENTS</td> </tr> </tbody> </table>	ISSUE	REV.	DATE	DESCRIPTION	U	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING	T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS	S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW	R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION	Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION	P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS	<p>DRAWING TITLE:</p> <p style="text-align: center;">FLOOD & AVA MAPS</p>	<p>JOB NUMBER: 25-010 DATE: APR. 16, 2025 DRAWN BY: KBK</p> <div style="text-align: center; font-size: 2em; font-weight: bold; border: 2px solid black; padding: 10px; margin: 10px auto; width: 80px;">A0.3</div>
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SITE PLAN
SCALE: 1"=20'-0"

CLIENT:

EAGLE BUILDERS

MECHANICAL / ELECTRICAL:

STRUCTURAL:

ARCHITECTURAL:

Axiom Architecture Inc.
Suite 110 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311
www.AxiomArch.ca

AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13655
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 NA'A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

REV.	DATE	DESCRIPTION
Y	2026-03-25	ISSUED FOR CPC MEETING
X	2026-03-18	ISSUED FOR BUILDING PERMIT
W	2026-03-17	ISSUED FOR SUBCONSULTANT COORDINATION
V	2026-03-03	ISSUED FOR BUILDING PERMIT
U	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
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Q	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

DRAWING TITLE:

SITE PLAN

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A1.1

BYLAW INFORMATION		LAND USE BYLAW # 35D/2025 & 1P/2007
LANDSCAPING REQUIREMENTS:		
REQUIRED:	WHERE A SETBACK AREA SHARES A PROPERTY LINE WITH A PARCEL DESIGNATED AS A COMMERCIAL, INDUSTRIAL OR SPECIAL PURPOSE DISTRICT, THE SETBACK AREA MUST BE A SOFT SURFACED LANDSCAPED AREA. MAY HAVE A SIDEWALK ALONG THE LENGTH OF THE BUILDING; AND MUST PROVIDE A MINIMUM OF 1.0 TREES AND 2.0 SHRUBS FOR EVERY 35.0 sqm, OR FOR EVERY 90.0 sqm, WHERE IRRIGATION IS PROVIDED BY A LOW WATER IRRIGATION SYSTEM.	
	WHERE A SETBACK AREA SHARES A PROPERTY LINE WITH N/A DRIVE SW, THE SETBACK AREA MAY BE SOFT SURFACED LANDSCAPED AREA OR HARD SURFACED LANDSCAPED AREA. MUST PROVIDE A MINIMUM OF 1.0 TREES AND 2.0 SHRUBS FOR EVERY 35.0 SQM OF LANDSCAPED AREA PROVIDED, AND PROVIDE TREES PLANTED IN A LINEAR ARRANGEMENT ALONG THE LENGTH OF THE SETBACK AREA.	
	ALL SOFT SURFACED LANDSCAPED AREAS MUST BE IRRIGATED BY AN UNDERGROUND IRRIGATION SYSTEM, UNLESS A LOW WATER IRRIGATION SYSTEM IS PROVIDED.	
	A MINIMUM OF 25% OF ALL TREES MUST BE CONIFEROUS.	
	WESTNORTH SETBACKS: 485.97sqm / 50sqm = (8.3) 10 TREES & (18.6) 19 SHRUBS EAST SETBACK: 288.19sqm / 50sqm = (5.8) 6 TREES & (11.5) 12 SHRUBS SOUTH SETBACK (N/A DRIVE S.W.): 353.61sqm / 35sqm = (10.1) 11 TREES & (20.2) 21 SHRUBS TOTAL: 27 TREES & 52 SHRUBS	
PROVIDED:	WESTNORTH SETBACKS: 19 TREES & 20 SHRUBS EAST SETBACK: 11 TREES & 12 SHRUBS SOUTH SETBACK (N/A DRIVE S.W.): 11 TREES & 22 SHRUBS (4 PROVIDED TREES & 2 SHRUBS HAVE BEEN PLACED ADJACENT TO THE SETBACK) INTERIOR OF SITE: 12 TREES & 38 SHRUBS TOTAL: 53 TREES & 92 SHRUBS	

TRANS-CANADA HIGHWAY #1
(16th AVE N.W.)
ROAD PLAN 272 IX

NOTE: --- LINE INDICATES THE LIMIT OF CONSTRUCTION DISTURBANCE. MUNICIPAL RESERVE/ CONSTRUCTION FENCING TO BE PROVIDED AROUND DISTURBANCE. FENCING TO BE INSTALLED PRIOR TO COMMENCEMENT OF ANY STRIPPING OR GRADING OPERATIONS. THIS FENCING IS TO BE INSPECTED AND APPROVED BY THE PARKS DEVELOPMENT INSPECTOR ANNIE RODRIGUES AT 403-604-6387.

ROAD PLAN
911 2537

**LANDSCAPING INFORMATION - HARDINESS ZONE 3b
CALGARY, ALBERTA**

NOTES
1) AREAS OF ANNUAL PLANTINGS OR FLOWER BEDS SHALL BE BY THE OWNER

DECIDUOUS SHRUBS

SYMBOL	PROVIDED	SIZE (MATURE)	PROVIDED
A1	RED OSIER DOGWOOD (CORNUS SERICEA NORSKYI)	600mm (2'-0") HEIGHT OR SPREAD	23
F	HIGH-BUSH CRANBERRY (VIBURNUM TRILOBUM)	600mm (2'-0") HEIGHT OR SPREAD	6

TOTAL NUMBER OF DECIDUOUS SHRUBS

CONIFEROUS SHRUBS

SYMBOL	PROVIDED	SIZE (MATURE)	PROVIDED
H	JUNIPER (JUNIPER COMMUNIS)	600mm (2'-0") HEIGHT OR SPREAD	16
J1	UPRIGHT JUNIPER (THUJA OCCIDENTALIS HOLMSTRUP)	900mm (3'-0") HEIGHT OR SPREAD	31
K	MUGO PINE (PINUS MUGO)	600mm (2'-0") HEIGHT OR SPREAD	16

TOTAL NUMBER OF CONIFEROUS SHRUBS

DECIDUOUS TREES

SYMBOL	PROVIDED	SIZE (MATURE)	PROVIDED
T6	SWEDISH COLUMNAR ASPEN (POPULUS TREMULA STRICTA)	MIN. 75mm (3") CAL/PER HEIGHT	26

TOTAL NUMBER OF DECIDUOUS TREES

CONIFEROUS TREES

SYMBOL	PROVIDED	SIZE (MATURE)	PROVIDED
T7A	COLORADO BLUE SPRUCE (PICEA PUNGENS)	MIN. 2.0m (6'-6 3/4") HEIGHT	13
T7B	COLORADO BLUE SPRUCE (PICEA PUNGENS)	MIN. 3.0m (9'-10 1/8") HEIGHT	14

TOTAL NUMBER OF CONIFEROUS TREES

EXISTING PUBLIC TREES

SYMBOL	EXISTING PROVIDED	SIZE (MATURE)	EXISTING
EX1	QUAKING ASPEN (POPULUS TREMULOIDES)	EXISTING - MEDIUM	19
EX2	SIBERIAN CRABAPPLE (MALUS ADSTRINGENS)	EXISTING - MEDIUM	3

TOTAL NUMBER OF EXISTING PUBLIC TREES (TO REMAIN)

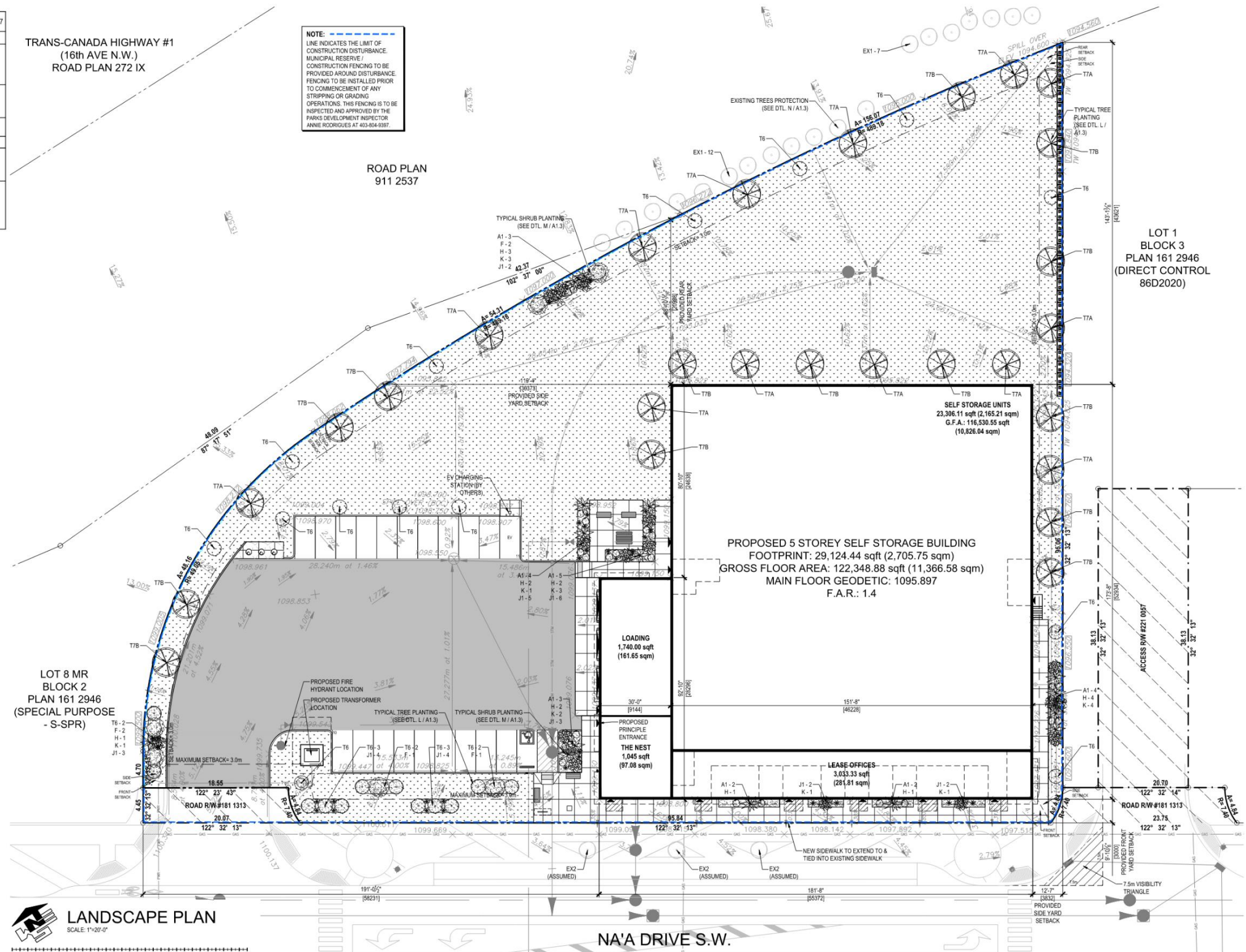
REGULATIONS

MINIMUM SIZE OF TREES AND SHRUBS
DECIDUOUS TREES - MIN. CAL/PER 50mm (50% MIN. 75mm)
CONIFEROUS TREES - MIN. HT. 2.0m (50% MIN. 3.0m)
DECIDUOUS SHRUBS - MIN. HT. 600mm
CONIFEROUS SHRUBS - MIN. SPREAD 600mm

TREES AND SHRUBS TO BE WATERED WITH LOW WATER IRRIGATION SYSTEM. IRRIGATION TO BE CONFINED TO TREE AND SHRUB LOCATIONS ONLY

HATCHING DEPICTS SOODED AREAS (AREAS TO USE DROUGHT TOLERANT GRASS) (SOIL DEPTH MIN. 300mm)

HATCHING DEPICTS MULCHED AREAS (SOIL DEPTH MIN. 600mm IN PLANTING BEDS)



LANDSCAPE PLAN
SCALE: 1"=20'-0"

CLIENT: **EAGLE BUILDERS**

MECHANICAL / ELECTRICAL:

STRUCTURAL:

ARCHITECTURAL:

Axiom Architecture Inc.
Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 6A5
Ph: 403-358-3311 www.AxiomArch.ca

AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

PROJECT INFORMATION:
STOREWEST - MEDICINE HILL STORAGE

LOT 12
BLOCK 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 N/A DRIVE S.W.
CALGARY, ALBERTA

StoreWest

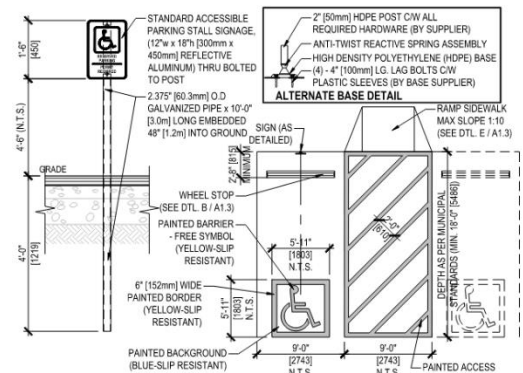
REVISIONS:

ISSUE	REV.	DATE	DESCRIPTION
Y	-	2026-03-25	ISSUED FOR CPC MEETING
X	-	2026-03-18	ISSUED FOR BUILDING PERMIT
W	-	2026-03-17	ISSUED FOR SUBCONSULTANT COORDINATION
V	-	2026-03-03	ISSUED FOR BUILDING PERMIT
U	-	2026-02-24	ISSUED FOR DART DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

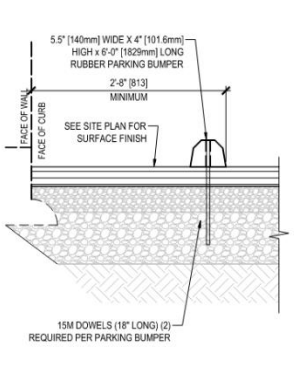
DRAWING TITLE:
LANDSCAPE & EXISTING TREE PLAN

JOB NUMBER: 25-010
DATE: APR. 14, 2025
DRAWN BY: KSK

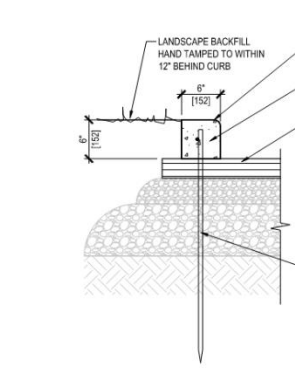
A1.2



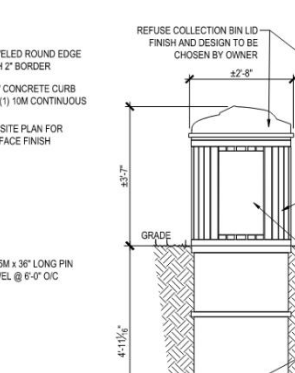
A BARRIER FREE PARKING STALL & SIGN
SCALE: 1/2"=1'-0"



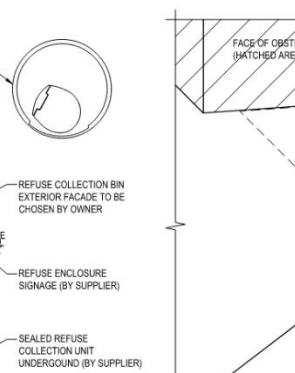
B RUBBER WHEEL STOP
SCALE: 1"=1'-0"



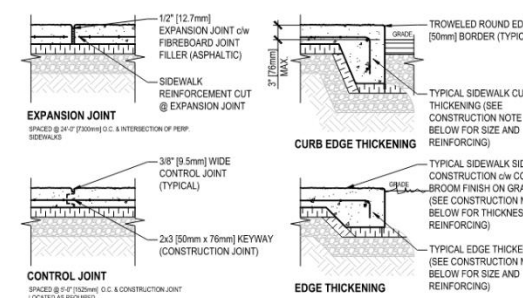
C PINNED CONCRETE CURB
SCALE: 1"=1'-0"



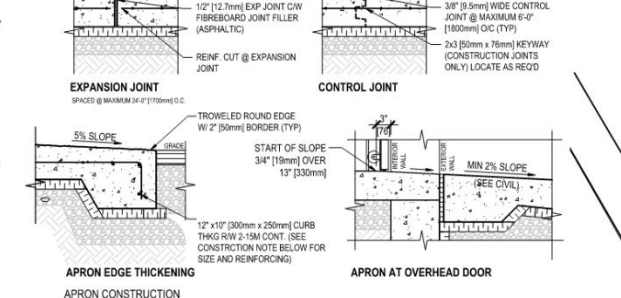
D SEMI-UNDERGROUND GARBAGE/RECYCLE UNIT
SCALE: 1/2"=1'-0"



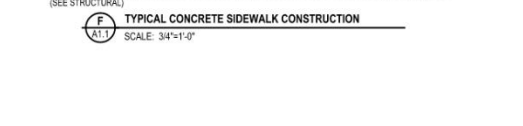
E BARRIER FREE SIDEWALK RAMP
SCALE: 1/4"=1'-0"



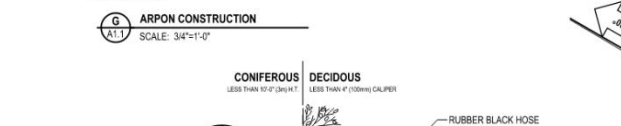
EXPANSION JOINT
CONTROL JOINT
CURVED EDGE THICKENING



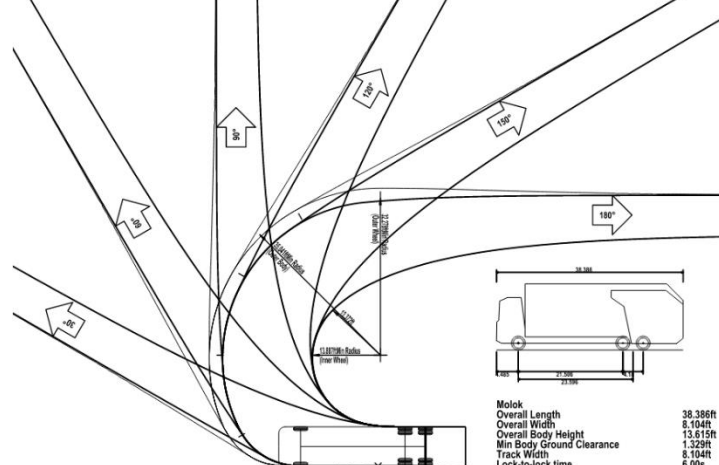
APRON CONSTRUCTION
APRON AT OVERHEAD DOOR



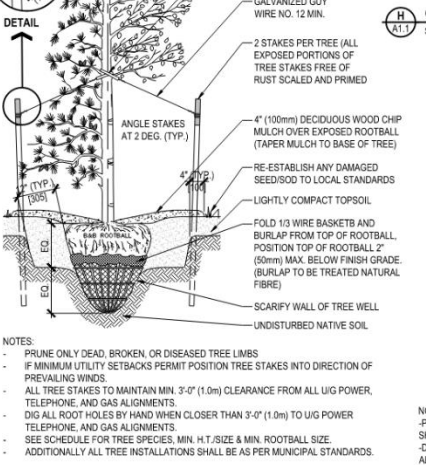
F TYPICAL CONCRETE SIDEWALK CONSTRUCTION
SCALE: 3/4"=1'-0"



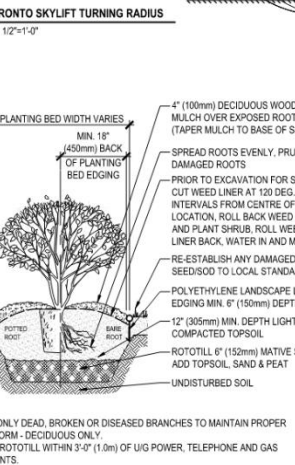
G APRON CONSTRUCTION
SCALE: 3/4"=1'-0"



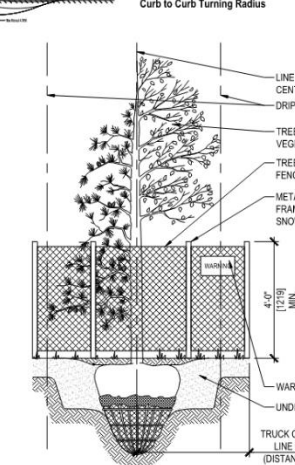
K MOLOK GARBAGE TRUCK TURNING RADIUS
SCALE: 1/2"=1'-0"



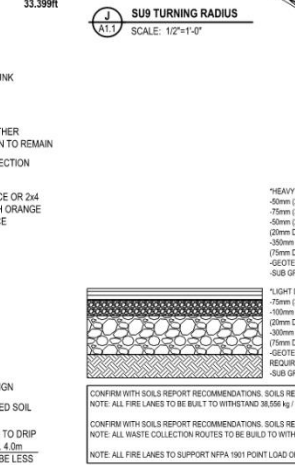
L STANDARD TREE PLANTING
SCALE: 3/8"=1'-0"



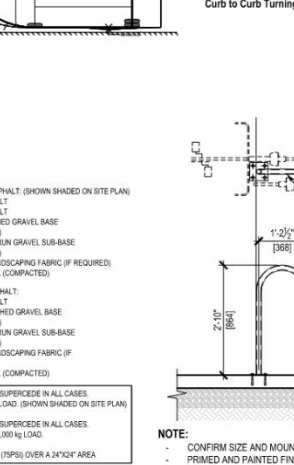
M STANDARD SHRUB PLANTING
SCALE: 3/8"=1'-0"



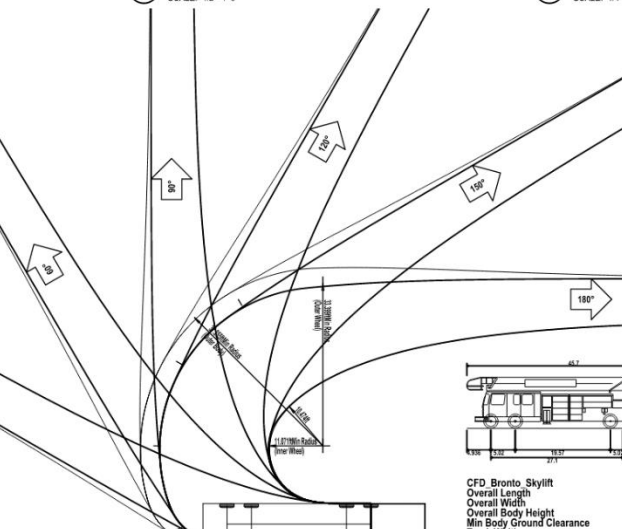
N TREE PROTECTION ZONE DETAIL
SCALE: 3/8"=1'-0"



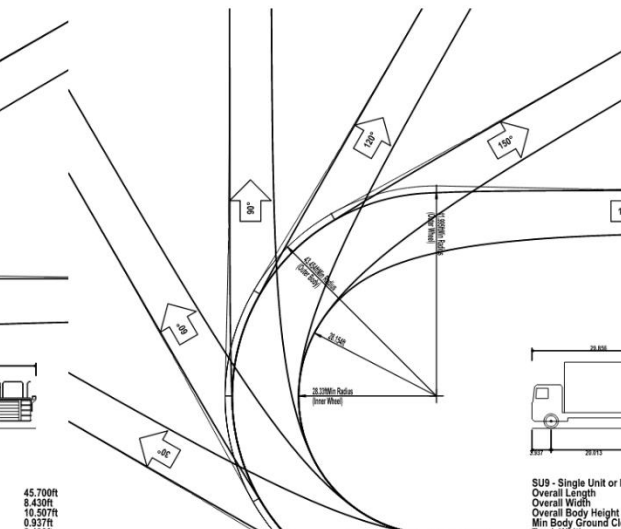
P PAVEMENT DETAIL / INFORMATION
SCALE: 1"=2'-0"



R U-RACK BIKE RACK
SCALE: 1/2"=1'-0"



H CFD BRONTO SKYLIFT TURNING RADIUS
SCALE: 1/2"=1'-0"



J SUB TURNING RADIUS
SCALE: 1/2"=1'-0"

CLIENT:

EAGLE BUILDERS

MECHANICAL / ELECTRICAL:

KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0232
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:

Grubb Engineering

ARCHITECTURAL:

Axiom Architecture Inc.
Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311 www.AxiomArch.ca

AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT NO. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 N/A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

U	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
T	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
O	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

DRAWING TITLE:

SITE & LANDSCAPE DETAILS

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KSK

A1.3



CLIENT:

EAGLE BUILDERS

MECHANICAL / ELECTRICAL:

KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0232
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PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 NAVA DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

ISSUE	REV.	DATE	DESCRIPTION
U	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

DRAWING TITLE:

SITE PHOTOS

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A1.4



13 ADJACENT RESIDENTIAL
SCALE: N.T.S.



14 ADJACENT RESIDENTIAL
SCALE: N.T.S.



15 ADJACENT COMMERCIAL
SCALE: N.T.S.



16 ADJACENT COMMERCIAL
SCALE: N.T.S.



17 ADJACENT COMMERCIAL/RESIDENTIAL
SCALE: N.T.S.



18 ADJACENT COMMERCIAL/RESIDENTIAL
SCALE: N.T.S.



19 ADJACENT COMMERCIAL/RESIDENTIAL
SCALE: N.T.S.



20 ADJACENT COMMERCIAL/RESIDENTIAL
SCALE: N.T.S.



21 ADJACENT RESIDENTIAL
SCALE: N.T.S.



22 ADJACENT COMMERCIAL
SCALE: N.T.S.



23 ADJACENT COMMERCIAL
SCALE: N.T.S.



24 ADJACENT COMMERCIAL
SCALE: N.T.S.

CLIENT:

EAGLE BUILDERS

MECHANICAL / ELECTRICAL:

KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.317.0232
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:

Grubb Engineering

ARCHITECTURAL:

Axiom Architecture Inc.
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AXIOM ARCHITECTURE INC (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 NAVA DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

U	REV.	DATE	DESCRIPTION
U	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS


DRAWING TITLE:

ADJACENT CONTEXT SITE PHOTOS


JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A1.5




 SHADOW STUDY (MAR. 21, 9am)
SCALE: 1" = 60'-0"




 SHADOW STUDY (MAR. 21, 12pm)
SCALE: 1" = 60'-0"



 SHADOW STUDY (MAR. 21, 4pm)
SCALE: 1" = 60'-0"




 SHADOW STUDY (JUN. 21, 9am)
SCALE: 1" = 60'-0"



 SHADOW STUDY (JUN. 21, 12pm)
SCALE: 1" = 60'-0"



 SHADOW STUDY (JUN. 21, 4pm)
SCALE: 1" = 60'-0"

CLIENT:



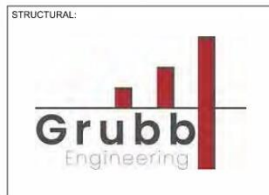
EAGLE BUILDERS

MECHANICAL / ELECTRICAL:



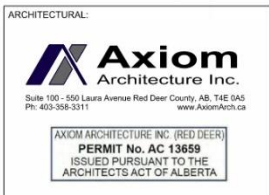
KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.317.5232
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:



Grubb Engineering

ARCHITECTURAL:



Axiom Architecture Inc.
Suite 100 - 100 Laura Avenue Red Deer County, AB, T4E 0A5
PH: 403-358-3311
www.AxiomArch.ca
AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

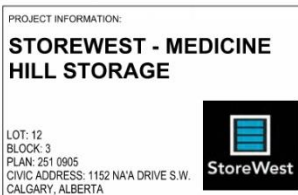
STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 351 0905
CIVIC ADDRESS: 1152 NA'A DRIVE S.W.
CALGARY, ALBERTA



REVISIONS:

Q	REV.	DATE	DESCRIPTION
Q	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS


Drawing Title:

SHADOW STUDY (SPRING EQUINOX & SUMMER SOLSTICE)


JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A1.6




 SHADOW STUDY (SEPT. 21, 9am)
SCALE: 1" = 60'-0"




 SHADOW STUDY (SEPT. 21, 12pm)
SCALE: 1" = 60'-0"



 SHADOW STUDY (SEPT. 21, 4pm)
SCALE: 1" = 60'-0"



 SHADOW STUDY (DEC. 21, 12pm)
SCALE: 1" = 60'-0"

CLIENT:




EAGLE BUILDERS

MECHANICAL / ELECTRICAL:




KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 403.377.0232
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:



Grubb Engineering

ARCHITECTURAL:



Axiom Architecture Inc.
Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311 www.AxiomArch.ca


AXIOM ARCHITECTURE, INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE



LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 NA'A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

Q	REV	DATE	DESCRIPTION
Q	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS


Drawing Title:


SHADOW STUDY (AUTUMN EQUINOX & WINTER SOLSTICE)

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A1.7



 SHADOW STUDY (DEC. 21, 9am)
SCALE: 1" = 100'-0"

 SHADOW STUDY (DEC. 21, 4pm)
SCALE: 1" = 100'-0"

CLIENT:




EAGLE BUILDERS

MECHANICAL / ELECTRICAL:




KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0252
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:



Grubb Engineering

ARCHITECTURAL:



Axiom Architecture Inc.
Suite 109 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311 www.AxiomArch.ca


AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE



LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 NA'A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

Q	REV.	DATE	DESCRIPTION
Q	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

Drawing Title:

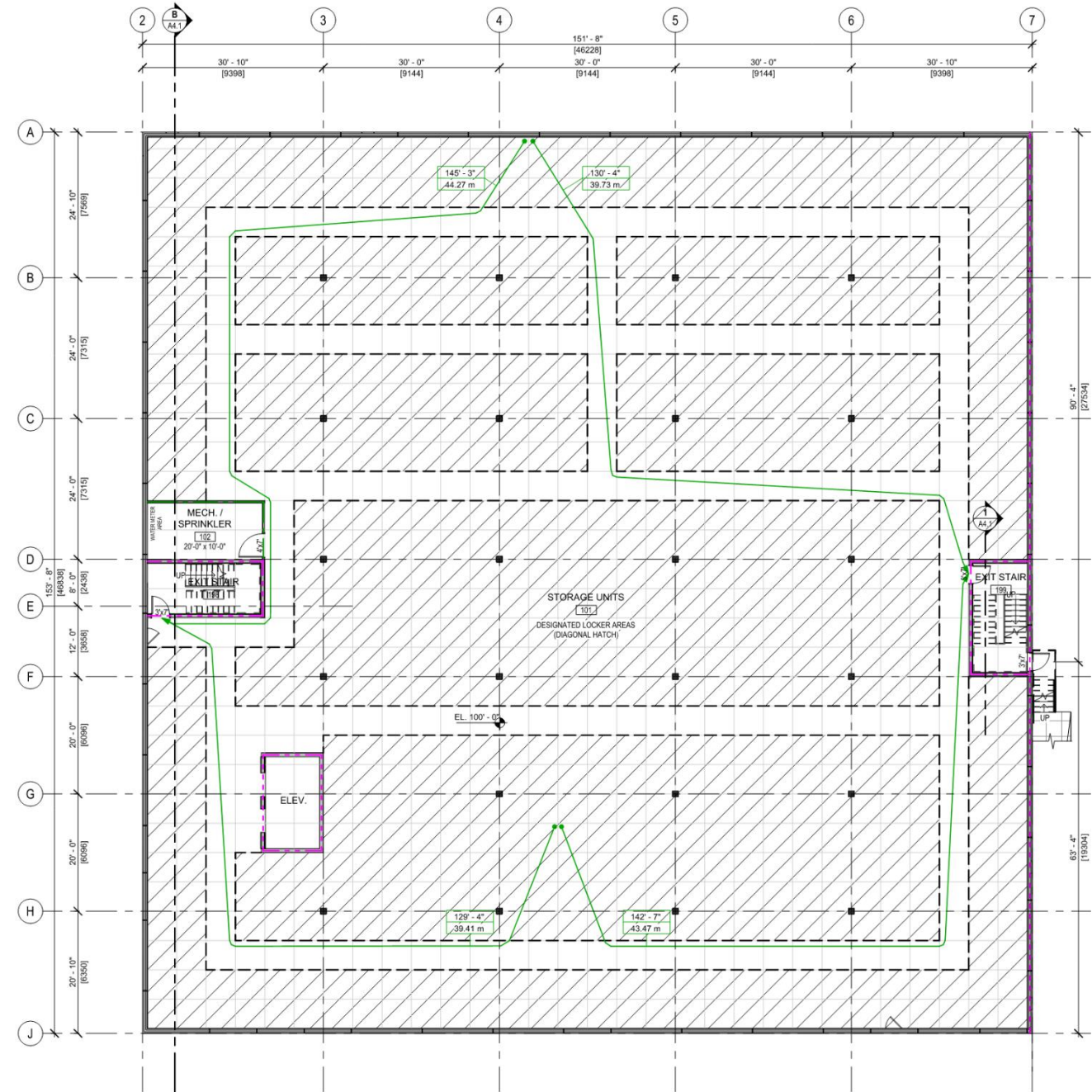
SHADOW STUDY (WINTER SOLSTICE)

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

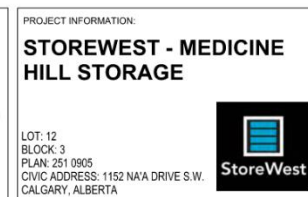
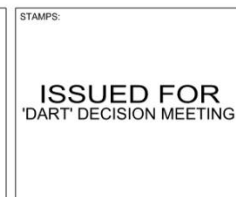
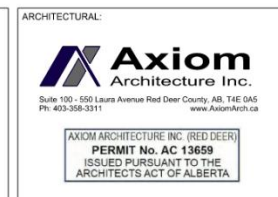
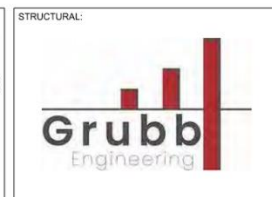
A1.8

NATIONAL BUILDING CODE (AS EDITION) 2023		
Key	N.B.C (AE) Sentence	Code
BUILDING USE, CLASSIFICATION & DESIGN		
BC.001	3.2.2.1	OCCUPANCIES 3.2.2.76 - GROUP F DIV 2 - MEDIUM HAZARD INDUSTRIAL (BUILDING CLASSIFICATION) 3.2.2.65 - GROUP D - OFFICE
BC.002	3.2.2.76	5 STOREY NON-COMBUSTIBLE CONSTRUCTION NO MAXIMUM AREA FACING 2 STREETS (FIRELANE REQUIRED)
BC.003	3.2.2.76 & 3.2.5.12	SPRINKLERED
BC.004	3.2.4.1	FIRE ALARM REQUIRED
BC.005	3.2.5.8	STANDPIPE REQUIRED
BC.006	3.2.5.7	SPRINKLERED BUILDING NOTE: FIRE HYDRANT TO BE LOCATED WITHIN 45 m (150'-0") OF FIRE DEPARTMENT CONNECTION FOR THE AUTOMATIC SPRINKLER SYSTEM. FIRE DEPARTMENT CONNECTION TO BE LOCATED WITHIN 15m (50'-0") OF THE PRINCIPAL ENTRANCE. FIRE FIGHTING WATER PROVIDED BY MUNICIPALITY WATER SUPPLY IS TO BE CAPABLE OF BEING DELIVERED AT A RATE OF NOT LESS THAN 3,800 L/MIN AND A RESIDUAL PRESSURE OF NOT LESS THAN 140 kPa. CONTACT CONSULTANT IF A PRIVATE WATER SUPPLY IS TO BE PROVIDED. FIRE HYDRANT AND AUTOMATIC SPRINKLER SYSTEM TO BE DESIGNED, CONSTRUCTED, INSTALLED & TESTED IN CONFORMANCE WITH NFPA 13.
FIRE SEPARATIONS AND RATINGS		
BC.101	3.1.3.1(1)	OCCUPANCY SEPARATIONS 22 (2) = 1 HOUR REQUIRED
BC.102	3.2.2.76	LOADBEARING WALLS AND COLUMNS = 2 HOUR RATING (BE OF NON-COMBUSTIBLE CONSTRUCTION AND HAVE A FIRE-RESISTANCE RATING NOT LESS THAN THAT REQUIRED FOR THE SUPPORTED ASSEMBLY)
BC.103	3.2.2.76	FLOORS (FIRE SEPARATION) = 2 HOUR
BC.104	3.3.1.22	JANITORS ROOMS (FIRE SEPARATION) = WAIVED (SPRINKLERED) (14 HOUR PROVIDED)
BC.105	3.4.4.1	EXIT (FIRE SEPARATION) = 2 HOURS
BC.106	3.5.3.1 & TABLE 3.5.3.1	ELEVATOR SHAFT (FIRE SEPARATION) = 2 HOURS
BC.107	3.6.2.1	MECHANICAL ROOMS (FIRE SEPARATION) = 1 HOUR
BC.108	3.6.3.1 & TABLE 3.6.3.1	VERTICAL SHAFT (FIRE SEPARATION) = 1 HOUR
BC.109	3.3.5.6	LOADING BAYS = 1.5 HOUR
BC.110	TABLE 3.1.8.4	FIRE PROTECTION RATINGS OF CLOSURES: F.R.R. OF FIRE SEPARATION = MINIMUM RATING OF CLOSURE 1 HOUR = 34 HOUR 1.5 HOUR = 1 HOUR 2 HOUR = 1.5 HOUR
ADDITIONAL INFORMATION		
BC.201	3.4.2.5	MAXIMUM 45m TRAVEL DISTANCE TO EXIT (GROUP F DIV 2) MAXIMUM 40m TRAVEL DISTANCE TO EXIT (GROUP D)
BC.202	3.6.4.4	ROOF ACCESS REQUIRED: YES
OCCUPANT LOAD		
BC.301	3.1.17.1	OFFICE AREA (GROUP D): 20 PERSON (STIPULATED) STORAGE AREA (GROUP F DIV 2): 20 PERSON (STIPULATED) TOTAL OCCUPANCY: 40 PERSONS (STIPULATED)
SPATIAL SEPARATION		
BC.401	TABLE 3.2.3.1-E & 3.2.3.3.1-D & 3.2.3.7	NORTH WALL 1 GROUP F DIV 2 - SPRINKLERED WALL AREA= 815.68 sqm OPENINGS AREA= 0 sqm L.D.: 20.99m OPENING % ALLOWED: 100% OPENING % PROVIDED: 0% COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION COMBUSTIBLE OR NON-COMBUSTIBLE CLADDING NORTH WALL 2 GROUP F DIV 2 - SPRINKLERED WALL AREA= 49.51 sqm OPENINGS AREA= 13.54 sqm L.D.: 40.75m OPENING % ALLOWED: 100% OPENING % PROVIDED: 27.34% COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION COMBUSTIBLE OR NON-COMBUSTIBLE CLADDING
BC.402	TABLE 3.2.3.1-E & 3.2.3.3.1-D & 3.2.3.7 & 3.2.3.7	EAST WALL 1 GROUP F DIV 2 - SPRINKLERED WALL AREA= 794.13 sqm OPENINGS AREA= 2.14 sqm L.D.: 3.83m OPENING % ALLOWED: 13.2% OPENING % PROVIDED: 0.27% 2 HOUR SPATIAL SEPARATION REQUIRED COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION NON-COMBUSTIBLE CLADDING EAST WALL 2 GROUP D - SPRINKLERED WALL AREA= 37.54 sqm OPENINGS AREA= 12.91 sqm L.D.: 3.68m OPENING % ALLOWED: 54.4% OPENING % PROVIDED: 34.40% 34 HOUR SPATIAL SEPARATION REQUIRED COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION COMBUSTIBLE OR NON-COMBUSTIBLE CLADDING
BC.403	TABLE 3.2.3.1-E & 3.2.3.3.1-D & 3.2.3.7 & 3.2.3.7 & 3.2.3.10	SOUTH WALL 1 GROUP D - SPRINKLERED WALL AREA= 296.1 sqm OPENINGS AREA= 134.68 sqm L.D.: 17.85m (STREET) OPENING % ALLOWED: 100% OPENING % PROVIDED: 45.48% COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION COMBUSTIBLE OR NON-COMBUSTIBLE CLADDING SOUTH WALL 2 GROUP F DIV 2 - SPRINKLERED WALL AREA= 439.46 sqm OPENINGS AREA= 94.22 sqm L.D.: 23.95m (STREET) OPENING % ALLOWED: 100% OPENING % PROVIDED: 21.44% COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION COMBUSTIBLE OR NON-COMBUSTIBLE CLADDING

NATIONAL BUILDING CODE (AS EDITION) 2023		
Key	N.B.C (AE) Sentence	Code
BC.404	TABLE 3.2.3.1-E & 3.2.3.3.1-D & 3.2.3.7	WEST WALL 1 GROUP D - SPRINKLERED WALL AREA= 56.9 sqm OPENINGS AREA= 18.3 sqm L.D.: 58.03m OPENING % ALLOWED: 100% OPENING % PROVIDED: 32.16% COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION COMBUSTIBLE OR NON-COMBUSTIBLE CLADDING WEST WALL 2 GROUP F DIV 2 - SPRINKLERED WALL AREA= 94.74 sqm OPENINGS AREA= 42.27 sqm L.D.: 58.03m OPENING % ALLOWED: 100% OPENING % PROVIDED: 44.62% COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION COMBUSTIBLE OR NON-COMBUSTIBLE CLADDING WEST WALL 3 GROUP F DIV 2 - SPRINKLERED WALL AREA= 552.28 sqm OPENINGS AREA= 41.31 sqm L.D.: 36.37m OPENING % ALLOWED: 100% OPENING % PROVIDED: 7.48% COMBUSTIBLE OR NON-COMBUSTIBLE CONSTRUCTION COMBUSTIBLE OR NON-COMBUSTIBLE CLADDING
BARRIER-FREE DESIGN		
BC.501	3.8.2.1	BARRIER FREE ACCESS REQUIRED: YES
BC.502	3.8.2.2	BARRIER FREE ENTRANCE REQUIRED: YES
BC.503	3.8.2.3	BIF ACCESS REQUIRED TO FLOOR ABOVE FIRST FLOOR: YES
BC.504	3.8.2.7	BARRIER FREE DOOR OPERATORS AT ENTRANCE: YES
BC.505	3.8.2.8	BARRIER FREE WIR REQUIRED: YES
BC.506	3.8.2.5	BARRIER FREE PARKING: YES
TOTAL STALLS REQUIRED: 1		



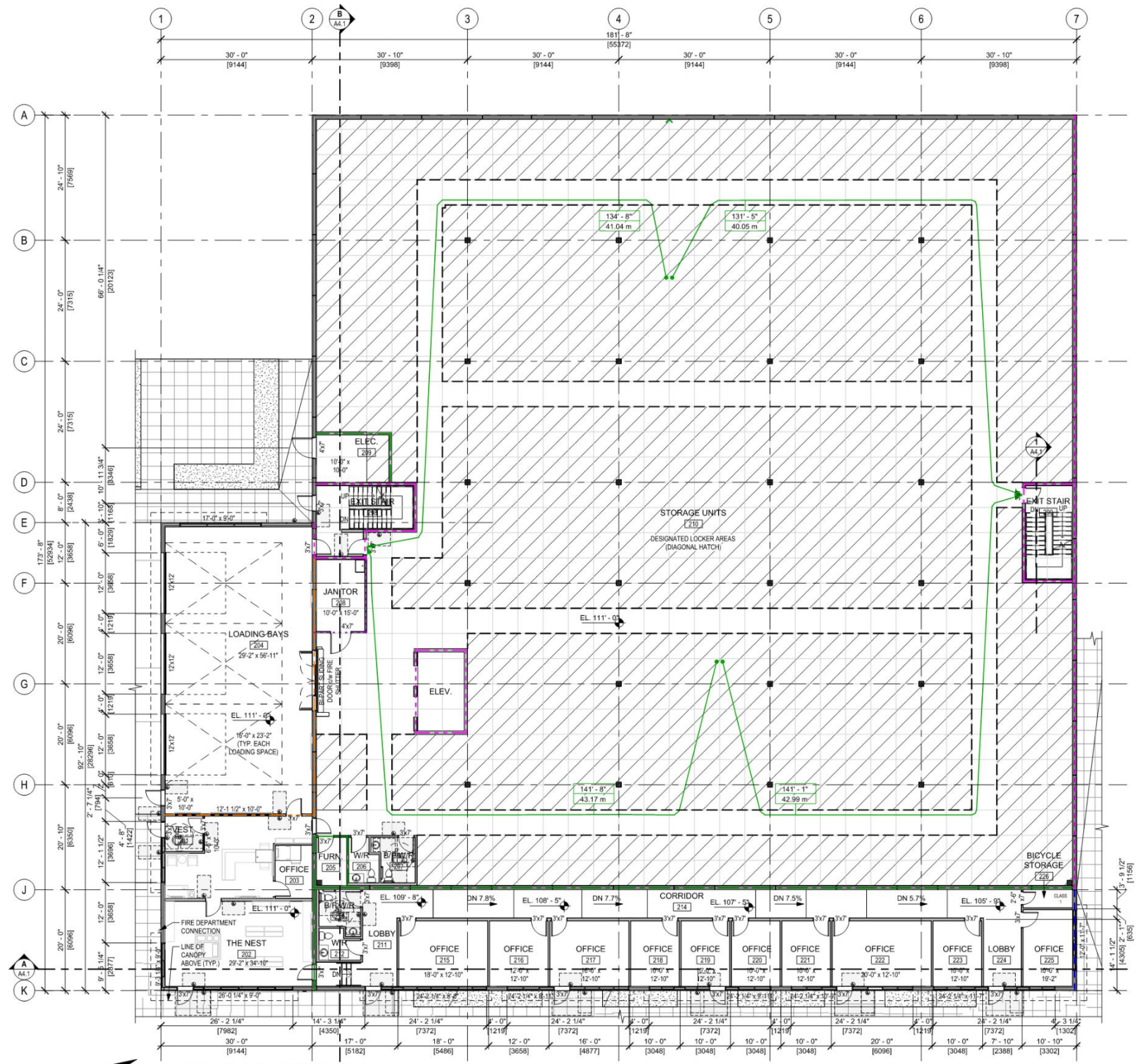
MAIN FLOOR PLAN
SCALE: 3/32" = 1'-0"



ISSUE	REV.	DATE	DESCRIPTION
Q	-	2026-02-24	ISSUED FOR DART DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
O	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS



JOB NUMBER:	25-010
DATE:	APR 16, 2025
DRAWN BY:	KBK
A2.1	



SECOND FLOOR PLAN
SCALE: 3/32" = 1'-0"

CLIENT:

EAGLE BUILDERS

MECHANICAL / ELECTRICAL:

KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0292
EMAIL: KEDM@ENGINEERING.CA

STRUCTURAL:

Grubb Engineering

ARCHITECTURAL:

Axiom Architecture Inc.
Suite 100 - 550 Laska Avenue Red Deer County, AB, T4E 0A5
Ph: 403-338-3311 www.AxiomArch.ca

AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0505
CIVIC ADDRESS: 1152 N/A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

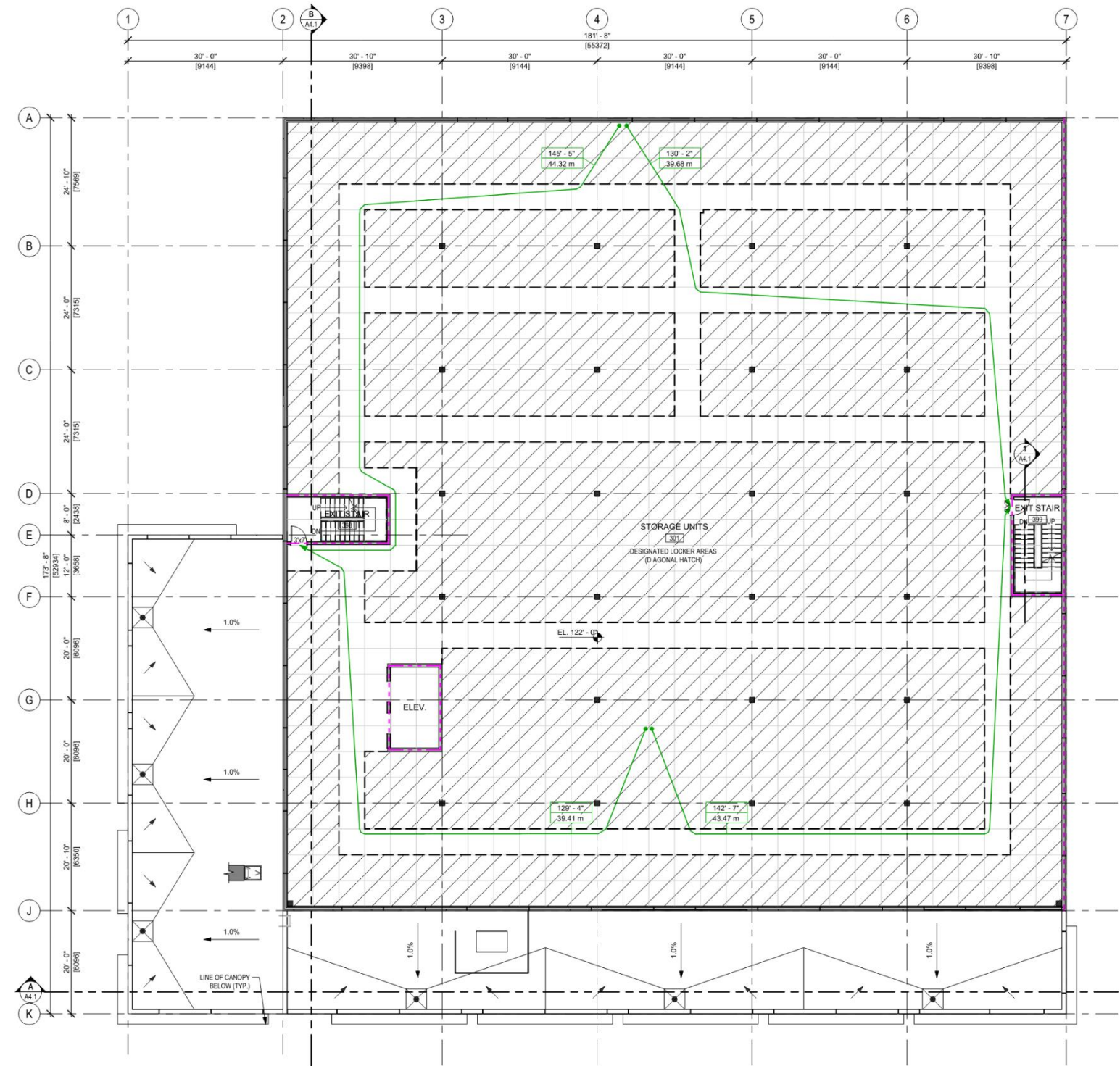
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P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

Drawing Title:

SECOND FLOOR PLAN

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A2.2



THIRD FLOOR PLAN
SCALE: 3/32" = 1'-0"

CLIENT:

EAGLE BUILDERS

MECHANICAL / ELECTRICAL:

KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0232
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:

Grubb Engineering

ARCHITECTURAL:

Axiom Architecture Inc.
Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311
www.AxiomArch.ca
AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

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STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 N/A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

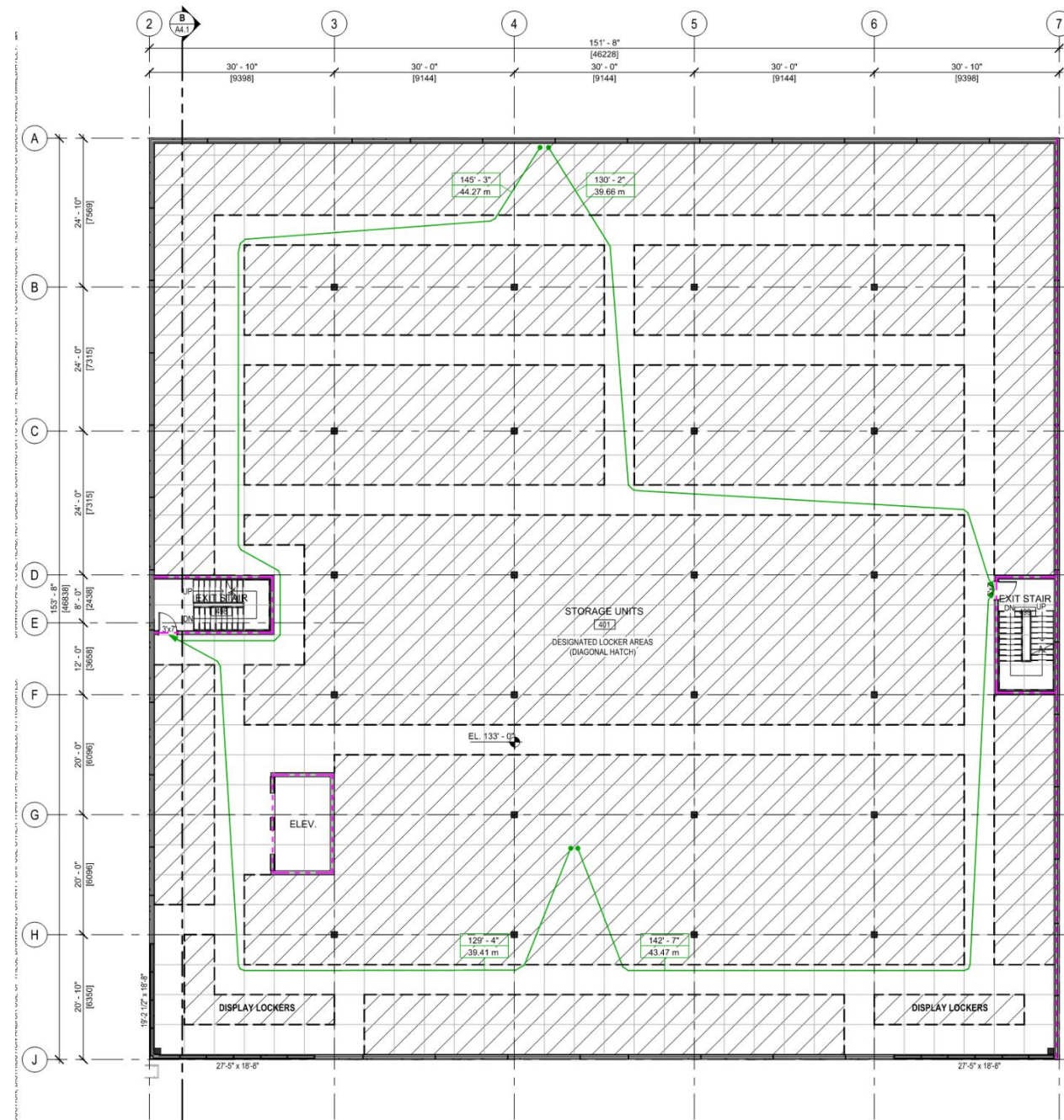
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P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

Drawing Title:

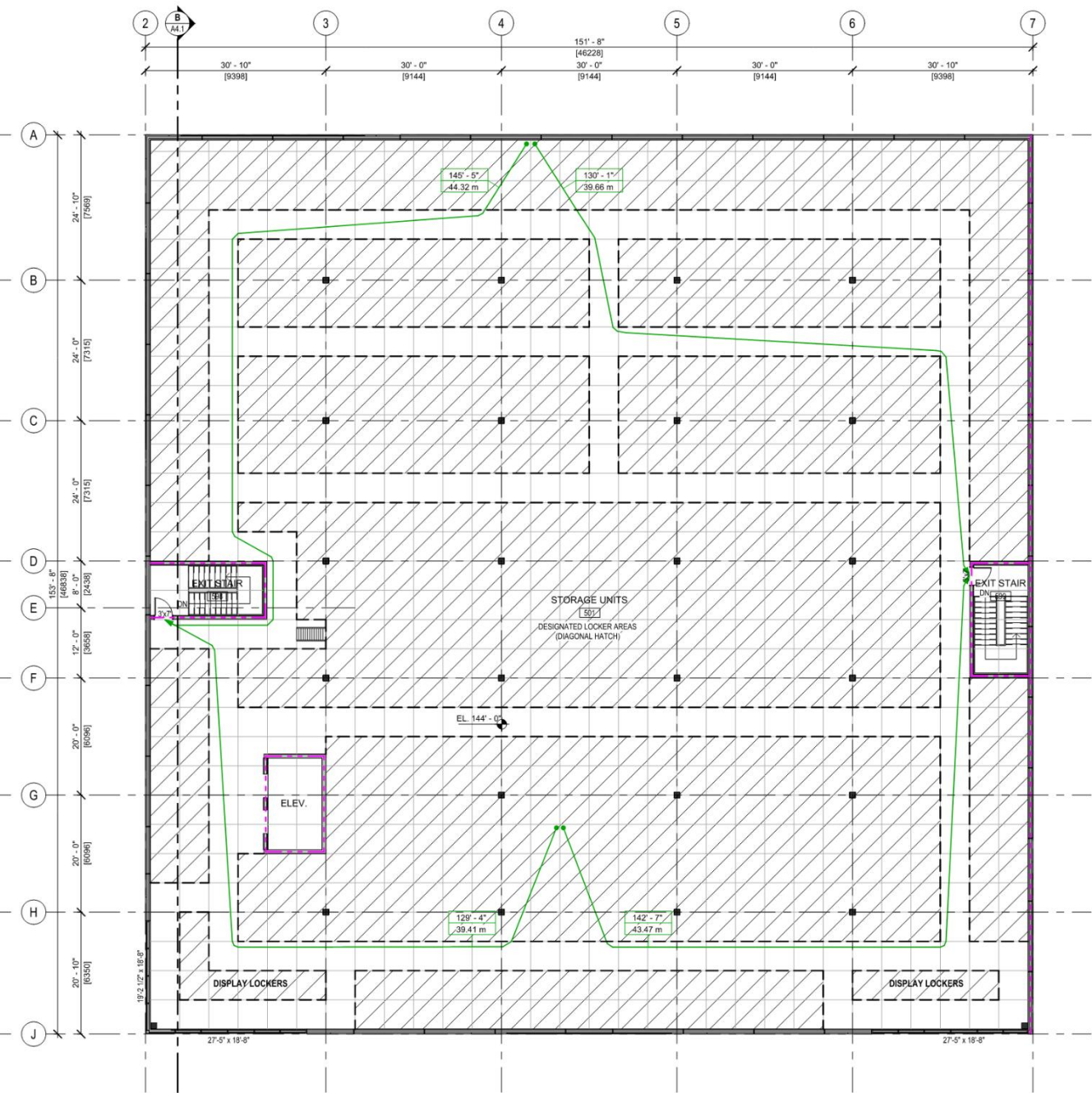
THIRD FLOOR PLAN

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A2.3



FOURTH FLOOR PLAN
SCALE: 3/32" = 1'-0"



FIFTH FLOOR PLAN
SCALE: 3/32" = 1'-0"

CLIENT:

EAGLE BUILDERS

MECHANICAL/ELECTRICAL:

KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 581.377.0232
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:

Grubb Engineering

ARCHITECTURAL:

Axiom Architecture Inc.
Suite 100 - 550 Luana Avenue Red Deer County, AB, T4E 0A5
Ph: 403-368-3311
www.AxiomArch.ca

AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 N/A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

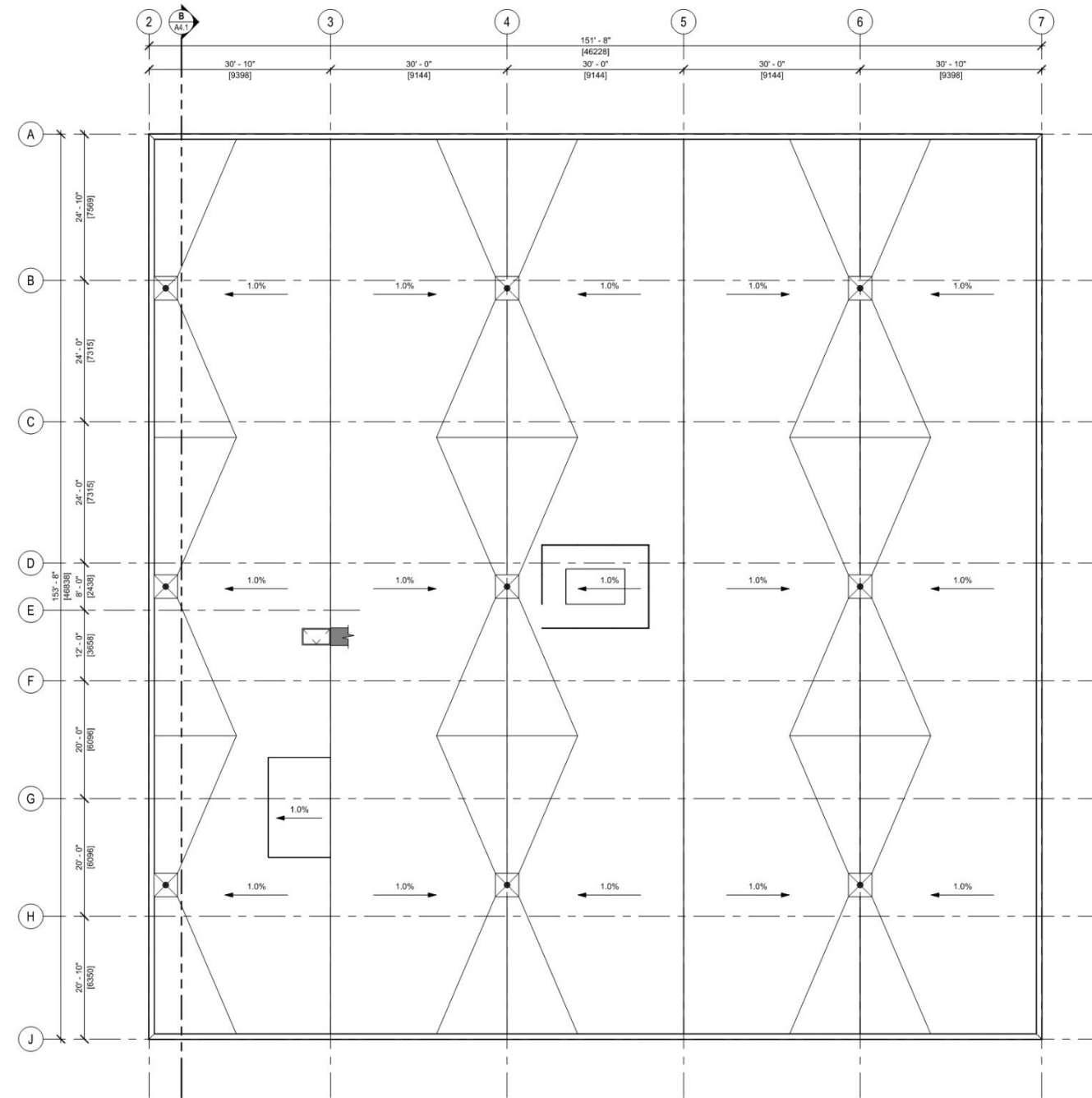
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T	-	2026-02-20	RESUBMITTED FOR DTR-J COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-J COMMENTS

Drawing Title:

FOURTH & FIFTH FLOOR PLAN

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A2.4



 **ROOF PLAN**
SCALE: 3/32" = 1'-0"

CLIENT:




EAGLE BUILDERS

MECHANICAL / ELECTRICAL:




KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 867.377.0232
EMAIL: KECON@KBENGINEERING.CA

STRUCTURAL:



Grubb Engineering

ARCHITECTURAL:



Axiom Architecture Inc.
Suite 101 - 350 Laura Avenue Red Deer County, AB, T4E 6A5
Ph: 403-358-3311 www.AxiomArch.ca

AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA


STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 N/A DRIVE S.W.
CALGARY, ALBERTA



REVISIONS:

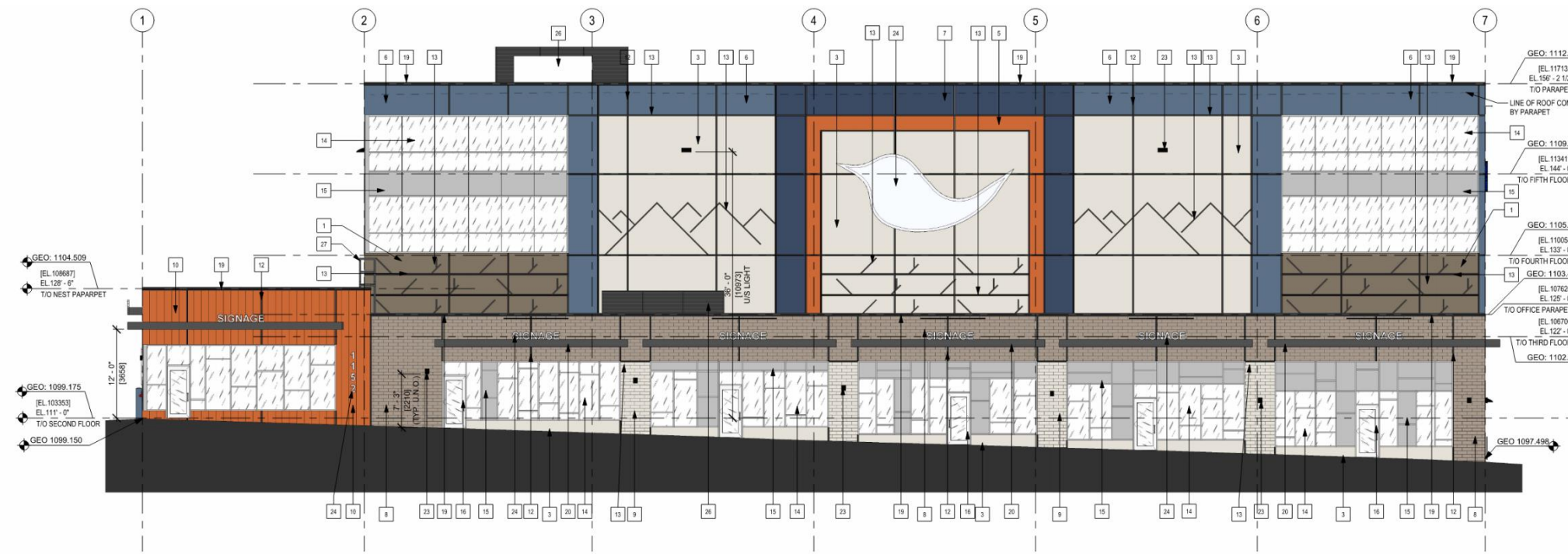
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Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

Drawing Title:

ROOF PLAN

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A2.5



SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



WEST ELEVATION
SCALE: 1/8" = 1'-0"

KEYNOTE LEGEND	
TAG	DESCRIPTION
1	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE COASTAL FOG 0213)
2	PRIMED & PAINTED PRECAST CONCRETE PANELS w/ REVEAL PATTERN (COLOUR: CLOVERDALE BIG SPENDER 0212)
3	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE BONAIRE 0103)
4	PRIMED & PAINTED PRECAST CONCRETE REVEAL (COLOUR: CLOVERDALE SUBWAY 0336) (SEE PAGE A5.3)
5	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE CASA DE ORO 1026)
6	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE BLUE DEPTHS 0626)
7	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE QUIET PEACE 0592)
8	STONE PATTERNED PRIMED & PAINTED PRECAST CONCRETE PANELS (RECKLI 272 08) (COLOUR: CLOVERDALE BIG SPENDER 0212)
9	STONE PATTERNED PRIMED & PAINTED PRECAST CONCRETE PANELS (RECKLI 272 08) (COLOUR: CLOVERDALE BONAIRE 0103)
10	WOOD PATTERNED PRIMED & PAINTED PRECAST CONCRETE PANELS (RECKLI 272 08) (COLOUR: CLOVERDALE CASA DE ORO 1026)
11	ETCHED & SEALED PRECAST CONCRETE BUFFALO MURAL (RECKLI PHOTO ENGRAVING)
12	PRECAST CONCRETE PANEL JOINT (COLOUR TO MATCH ADJACENT PANEL)
13	PRECAST CONCRETE PANEL REVEAL (COLOUR TO MATCH ADJACENT PANEL)
14	CLEAR GLAZING IN PREFINISHED CLEAR ANODIZED ALUMINUM FRAME
15	SPANDREL PANEL IN PREFINISHED CLEAR ANODIZED ALUMINUM FRAME
16	PREFINISHED CLEAR ANODIZED ALUMINUM FULL GLAZED DOOR
17	PRIMED & PAINTED METAL MAN DOOR (COLOUR TO MATCH ADJACENT WALL COLOUR)
18	PREFINISHED CLEAR GLAZING OVERHEAD DOOR
19	PREFINISHED METAL CAP FLASHING (COLOUR: BLACK)
20	POWDER COATED STEEL CANOPY (COLOUR: BLACK)
21	POWDER COATED STEEL STAIRS & PIPE GUARDRAIL (COLOUR: BLACK)
22	CONCRETE FILLED PIPE BOLLARD (COLOUR: CLOVERDALE BLUE DEPTHS 0626)
23	EXTERIOR DOWNLIGHTING (ELECTRICAL TO CONFIRM FIXTURES AND LIGHTING)
24	EXTERIOR ILLUMINATED BUILDING SIGNAGE (BY OTHERS UNDER SEPARATE PERMIT)
25	FIRE DEPARTMENT CONNECTION w/ SIGN (AS PER CITY OF CALGARY REQUIREMENTS)
26	ROOF SCREEN FOR MECHANICAL UNITS (SEE ROOF PLAN FOR LOCATIONS) (BLACK LOUVERS)
27	PRIMED & PAINTED ROOF ACCESS LADDER (COLOUR TO MATCH BLACK)
28	AFTER HOURS ACCESS KEYPAD (BY OTHERS)
29	FIRE DEPARTMENT LOCKBOX (AS PER CITY OF CALGARY REQUIREMENTS)
30	MECHANICAL WALL GRILLES & PENETRATIONS (SEE NOTE BELOW)

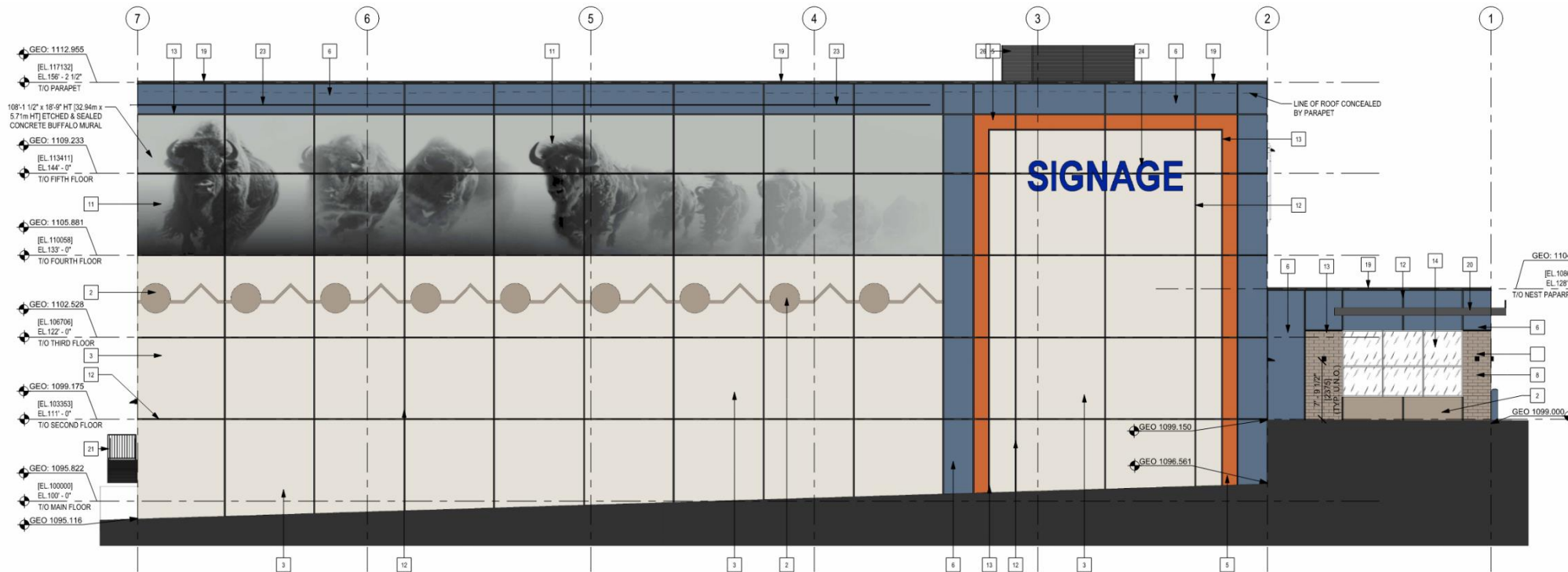
NOTE: ALL MECHANICAL WALL GRILLES & PENETRATIONS ARE TO BE PAINTED SIMILAR TO ADJACENT WALL COLOUR

LIGHTING LEGEND

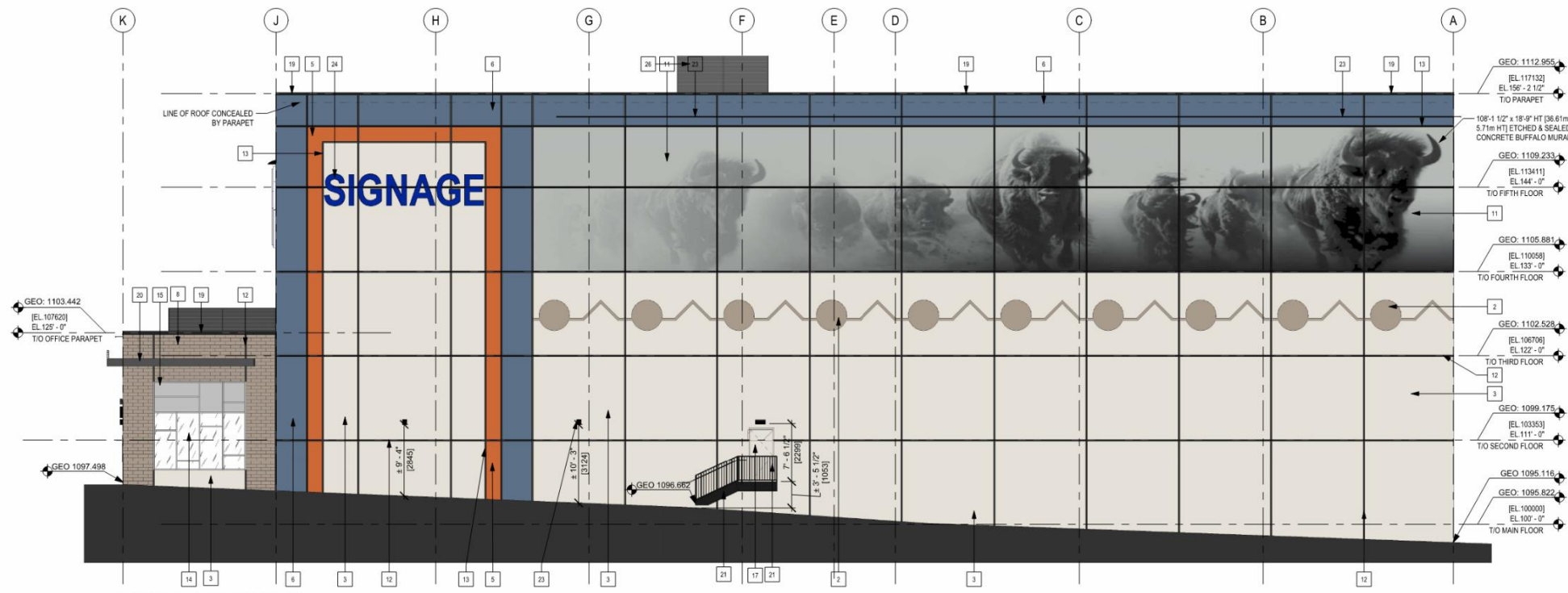


NOTE: EXTERIOR DOWNLIGHTING (ELECTRICAL TO CONFIRM FIXTURES AND LIGHTING). LIGHTING TO MEET DARK SKY PRINCIPLES. EXTERIOR LIGHT FIXTURES SHALL CAST LIGHT DOWNWARD AND WILL NOT ALLOW LIGHT TO ESCAPE FROM SIDES AND WILL PREVENT UPWARD GLARE AND ALL SIGNAGES WITH LIGHTING, INCLUDING FASCIA SIGNS SHALL DIM OR BE SHIELDED FROM CASTING LIGHT UPWARDS.

<p>CLIENT:</p> <p>EAGLE BUILDERS</p>	<p>MECHANICAL / ELECTRICAL:</p>	<p>STRUCTURAL:</p>	<p>ARCHITECTURAL:</p> <p>Axiom Architecture Inc. Suite 100 - 553 Laura Avenue Red Deer County, AB, T4E 0A5 Ph: 403-358-3311 www.AxiomArch.ca</p> <p>AXIOM ARCHITECTURE INC. (RED DEER) PERMIT No. AC 13659 ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA</p>	<p>STAMPS:</p>	<p>PROJECT INFORMATION:</p> <p>STOREWEST - MEDICINE HILL STORAGE</p> <p>LOT: 12 BLOCK: 3 PLAN: 251 0905 CIVIC ADDRESS: 1152 NAVA DRIVE S.W. CALGARY, ALBERTA</p>	<p>REVISIONS:</p> <table border="1"> <thead> <tr> <th>REV.</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td>Y</td> <td>2025-03-25</td> <td>ISSUED FOR CPC MEETING</td> </tr> <tr> <td>X</td> <td>2025-03-18</td> <td>ISSUED FOR BUILDING PERMIT</td> </tr> <tr> <td>W</td> <td>2025-03-17</td> <td>ISSUED FOR SUBCONSULTANT COORDINATION</td> </tr> <tr> <td>V</td> <td>2025-03-03</td> <td>ISSUED FOR BUILDING PERMIT</td> </tr> <tr> <td>U</td> <td>2025-02-24</td> <td>ISSUED FOR DART DECISION MEETING</td> </tr> <tr> <td>T</td> <td>2025-02-20</td> <td>RESUBMITTED FOR DTR-3 COMMENTS</td> </tr> <tr> <td>S</td> <td>2025-02-13</td> <td>LOCKER LAYOUT ISSUED FOR REVIEW</td> </tr> <tr> <td>R</td> <td>2025-02-11</td> <td>ISSUED FOR SUBCONSULTANT INFORMATION</td> </tr> <tr> <td>Q</td> <td>2025-01-14</td> <td>ISSUED FOR SUBCONSULTANT INFORMATION</td> </tr> <tr> <td>P</td> <td>2025-01-12</td> <td>RESUBMITTED FOR DTR-2 COMMENTS</td> </tr> </tbody> </table>	REV.	DATE	DESCRIPTION	Y	2025-03-25	ISSUED FOR CPC MEETING	X	2025-03-18	ISSUED FOR BUILDING PERMIT	W	2025-03-17	ISSUED FOR SUBCONSULTANT COORDINATION	V	2025-03-03	ISSUED FOR BUILDING PERMIT	U	2025-02-24	ISSUED FOR DART DECISION MEETING	T	2025-02-20	RESUBMITTED FOR DTR-3 COMMENTS	S	2025-02-13	LOCKER LAYOUT ISSUED FOR REVIEW	R	2025-02-11	ISSUED FOR SUBCONSULTANT INFORMATION	Q	2025-01-14	ISSUED FOR SUBCONSULTANT INFORMATION	P	2025-01-12	RESUBMITTED FOR DTR-2 COMMENTS	<p>Drawing Title:</p> <p>EXTERIOR ELEVATIONS</p>	<p>JOB NUMBER: 25-010 DATE: APR. 16, 2025 DRAWN BY: KKB</p> <p>A3.1</p>
							REV.	DATE	DESCRIPTION																																
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P	2025-01-12	RESUBMITTED FOR DTR-2 COMMENTS																																							
<p>SCALE: 1/8" = 1'-0"</p>																																									



NORTH ELEVATION
SCALE: 1/8" = 1'-0"



EAST ELEVATION
SCALE: 1/8" = 1'-0"

KEYNOTE LEGEND	
TAG	DESCRIPTION
1	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE COASTAL_FOG 0213)
2	PRIMED & PAINTED PRECAST CONCRETE REVEAL (COLOUR: CLOVERDALE BIG SPENDER 0212)
3	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE BONAIRE 0020)
4	PRIMED & PAINTED PRECAST CONCRETE REVEAL (COLOUR: CLOVERDALE SUBWAY 0536) (SEE PAGE A5.3)
5	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE CASA DE ORO 1026)
6	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE BLUE DEPTHS 0626)
7	PRIMED & PAINTED PRECAST CONCRETE PANELS (COLOUR: CLOVERDALE QUIET PEACE 0592)
8	STONE PATTERNED PRIMED & PAINTED PRECAST CONCRETE PANELS (RECKLI 2172 08) (COLOUR: CLOVERDALE BIG SPENDER 0212)
9	STONE PATTERNED PRIMED & PAINTED PRECAST CONCRETE PANELS (RECKLI 2172 08) (COLOUR: CLOVERDALE BONAIRE 0020)
10	WOOD PATTERNED PRIMED & PAINTED PRECAST CONCRETE PANELS (RECKLI 2172 08) (COLOUR: CLOVERDALE CASA DE ORO 1026)
11	ETCHED & SEALED PRECAST CONCRETE BUFFALO MURAL (RECKLI PHOTO-ENGRAVING)
12	PRECAST CONCRETE PANEL JOINT (COLOUR TO MATCH ADJACENT PANEL)
13	PRECAST CONCRETE PANEL REVEAL (COLOUR TO MATCH ADJACENT PANEL)
14	CLEAR GLAZING IN PREFINISHED CLEAR ANODIZED ALUMINUM FRAME
15	SPANDREL PANEL IN PREFINISHED CLEAR ANODIZED ALUMINUM FRAME
16	PREFINISHED CLEAR ANODIZED ALUMINUM FULL GLAZED DOOR
17	PRIMED & PAINTED METAL MAN DOOR (COLOUR TO MATCH ADJACENT WALL COLOUR)
18	PREFINISHED CLEAR GLAZING OVERHEAD DOOR
19	PREFINISHED METAL CAP FLASHING (COLOUR: BLACK)
20	POWDER COATED STEEL CANOPY (COLOUR: BLACK)
21	POWDER COATED STEEL STAIRS & PIPE GUARDRAIL (COLOUR: BLACK)
22	CONCRETE FILLED PIPE BOLLARD (COLOUR: CLOVERDALE BLUE DEPTHS 0626)
23	EXTERIOR DOWNLIGHTING (ELECTRICAL TO CONFIRM FIXTURES AND LIGHTING)
24	EXTERIOR ILLUMINATED BUILDING SIGNAGE (BY OTHERS UNDER SEPARATE PERMIT)
25	FIRE DEPARTMENT CONNECTION SIGN (AS PER CITY OF CALGARY REQUIREMENTS)
26	ROOF SCREEN FOR MECHANICAL UNITS (SEE ROOF PLAN FOR LOCATIONS) (BLACK LOUVERS)
27	PRIMED & PAINTED ROOF ACCESS LADDER (COLOUR TO MATCH BLACK)
28	AFTER HOURS ACCESS KEYPAD (BY OTHERS)
29	FIRE DEPARTMENT LOCKBOX (AS PER CITY OF CALGARY REQUIREMENTS)
30	MECHANICAL WALL GRILLES & PENETRATIONS (SEE NOTE BELOW)
NOTE: ALL MECHANICAL WALL GRILLES & PENETRATIONS ARE TO BE PAINTED SIMILAR TO ADJACENT WALL COLOUR	

LIGHTING LEGEND



NOTE: EXTERIOR DOWNLIGHTING (ELECTRICAL TO CONFIRM FIXTURES AND LIGHTING). LIGHTING TO MEET DARK SKY PRINCIPLES. EXTERIOR LIGHT FIXTURES SHALL CAST LIGHT DOWNWARD AND WILL NOT ALLOW LIGHT TO ESCAPE FROM SIDES AND WILL PREVENT UPWARD GLARE AND ALL SIGNAGES WITH LIGHTING, INCLUDING FASCIA SIGNS SHALL DIM OR BE SHIELDED FROM CASTING LIGHT UPWARDS.

CLIENT:

MECHANICAL / ELECTRICAL:

STRUCTURAL:

ARCHITECTURAL:

STAMPS:

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

REVISIONS:

REV.	DATE	DESCRIPTION
Y	2026-03-25	ISSUED FOR CPC MEETING
X	2026-03-18	ISSUED FOR BUILDING PERMIT
W	2026-03-17	ISSUED FOR SUBCONSULTANT COORDINATION
V	2026-03-03	ISSUED FOR BUILDING PERMIT
U	2026-02-24	ISSUED FOR DART DECISION MEETING
T	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
R	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION
Q	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

Drawing Title:

EXTERIOR ELEVATIONS

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A3.2



SOUTHWEST VIEW



SOUTHEAST VIEW



NORTHEAST VIEW



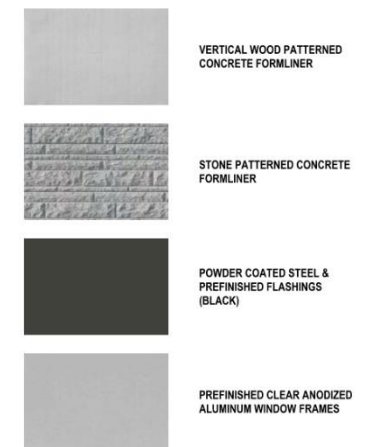
NORTHWEST VIEW

WALL COLOURS



NOTE: ALL WALLS CONSIST OF PAINTED PRECAST CONCRETE ONLY. PLEASE SEE FEATURE ELEMENTS BELOW FOR CAST IN FORMLINER PATTERNS.

FEATURE ELEMENTS



CLIENT:

MECHANICAL / ELECTRICAL:

STRUCTURAL:

ARCHITECTURAL:

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

REVISIONS:

Q	REV.	DATE	DESCRIPTION
Q	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW
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Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

Drawing Title:

EXTERIOR BUILDING RENDERINGS

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

A3.3



STREETSCAPE - STORAGE OFFICE (THE NEST)



STREETSCAPE - OFFICES

WALL COLOURS



NOTE: ALL WALLS CONSIST OF PAINTED PRECAST CONCRETE ONLY. PLEASE SEE FEATURE ELEMENTS BELOW FOR CAST IN FORMLINER PATTERNS.

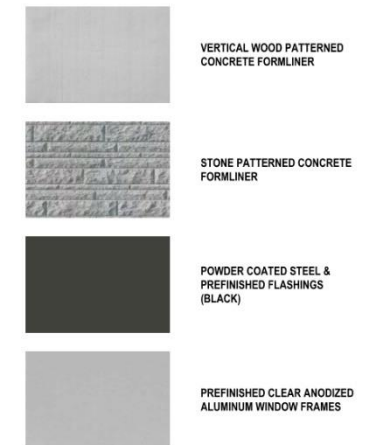


STORAGE LOADING AREA



STREETSCAPE - SIGNAGE TOWER

FEATURE ELEMENTS



CLIENT:

EAGLE BUILDERS

MECHANICAL / ELECTRICAL:

KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0232
EMAIL: KCON@ENGINEERING.CA

STRUCTURAL:

Grubb Engineering

ARCHITECTURAL:

Axiom Architecture Inc.
Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311
www.AxiomArch.ca
AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 NAVA DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

Q	REV.	DATE	DESCRIPTION
Q	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING
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Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

Drawing Title:

EXTERIOR BUILDING RENDERINGS

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KKB

A3.4



ETCHED & SEALED CONCRETE BUFFALO MURAL



ETCHED & SEALED CONCRETE BUFFALO MURAL - NIGHT

WALL COLOURS

-  CLOVERDALE - COASTAL FOG (0213)
-  CLOVERDALE - BIG SPENDER (0212)
-  CLOVERDALE - BONAIRE (0020)
-  CLOVERDALE - CASA DE ORO (1026)
-  CLOVERDALE - BLUE DEPTHS (0626)
-  CLOVERDALE - QUIET PEACE (0592)

NOTE: ALL WALLS CONSIST OF PAINTED PRECAST CONCRETE ONLY. PLEASE SEE FEATURE ELEMENTS BELOW FOR CAST IN FORMLINER PATTERNS.







VISUAL IMPACT - TRANS CANADA HIGHWAY (DISTANCE OF 150m)



VISUAL IMPACT - TRANS CANADA HIGHWAY (DISTANCE OF 150m)

FEATURE ELEMENTS

-  VERTICAL WOOD PATTERNED CONCRETE FORMLINER
-  STONE PATTERNED CONCRETE FORMLINER
-  POWDER COATED STEEL & PREFINISHED FLASHINGS (BLACK)
-  PREFINISHED CLEAR ANODIZED ALUMINUM WINDOW FRAMES

CLIENT:




EAGLE BUILDERS

MECHANICAL / ELECTRICAL:




KB ENGINEERING LIMITED
RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0292
EMAIL: KEON@KBENGINEERING.CA

STRUCTURAL:



Grubb Engineering

ARCHITECTURAL:



Axiom Architecture Inc.
PERMIT No. AC 13659
Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311
www.AxiomArch.ca


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STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE



LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 NAVA DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

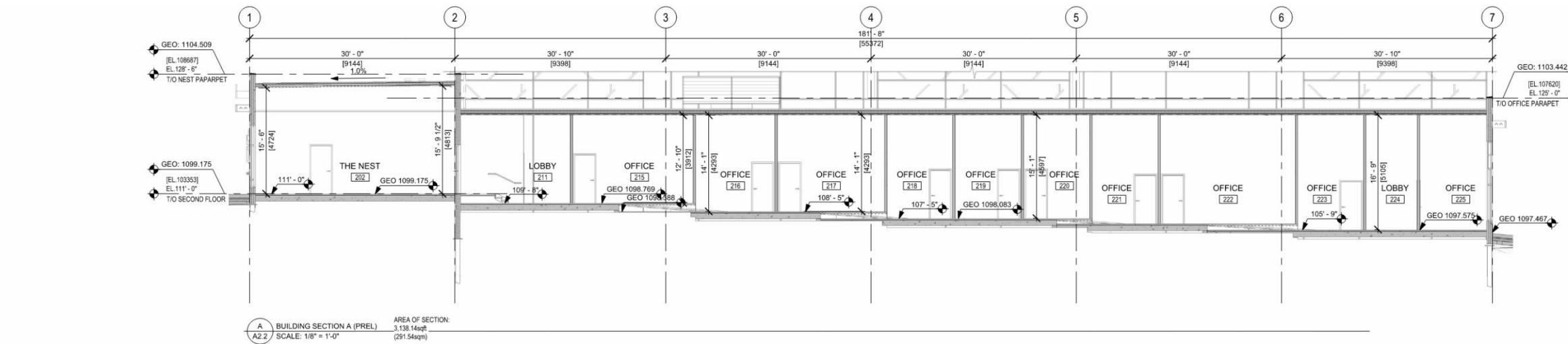
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T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS
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Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION
P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS

Drawing Title:

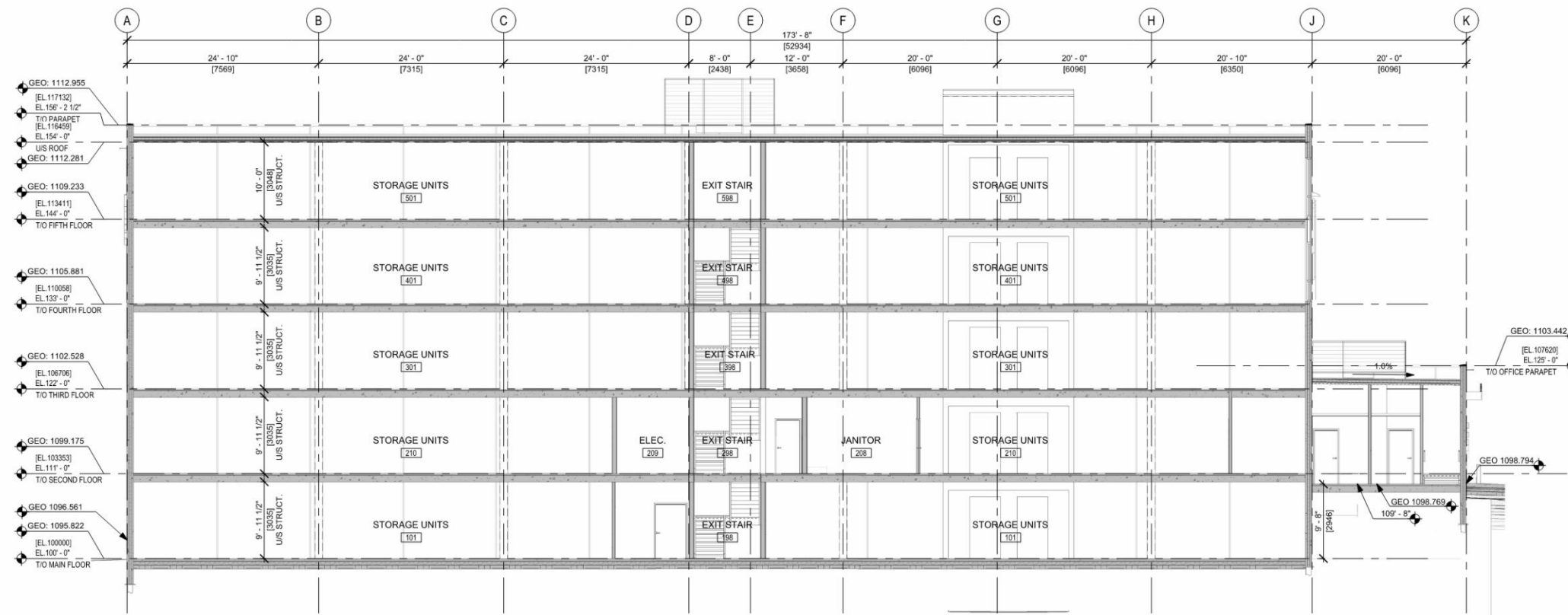
EXTERIOR BUILDING RENDERINGS

JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBK

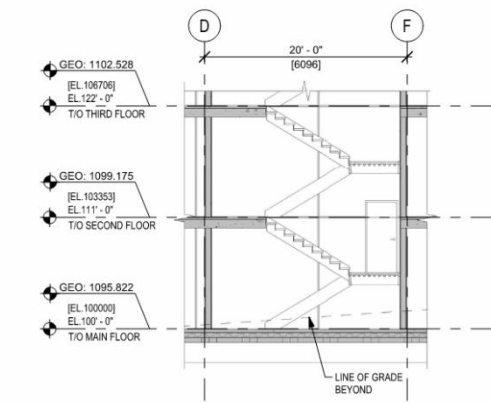
A3.5



A BUILDING SECTION A (PREL)
A2.2 SCALE: 1/8" = 1'-0"
AREA OF SECTION:
3,138.14sqft
(291.54sqm)



B BUILDING SECTION B (PREL)
A2.1 SCALE: 1/8" = 1'-0"
AREA OF SECTION:
8,926.75sqft
(829.32sqm)



1 STAIR SECTION (PREL)
A2.1 SCALE: 1/8" = 1'-0"

<p>CLIENT:</p> <p>EAGLE BUILDERS</p>	<p>MECHANICAL / ELECTRICAL:</p> <p>KB ENGINEERING LIMITED RED DEER COUNTY, ALBERTA, CANADA PHONE: 587.377.0232 EMAIL: KEON@KBENGINEERING.CA</p>	<p>STRUCTURAL:</p> <p>Grubb Engineering</p>	<p>ARCHITECTURAL:</p> <p>Axiom Architecture Inc. Suite 105 - 550 Laura Avenue Red Deer County, AB, T4E 0A5 Ph: 403-358-3311 www.AxiomArch.ca</p> <p>AXIOM ARCHITECTURE INC. (RED DEER) PERMIT No. AC 13659 ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA</p>	<p>STAMPS:</p> <p>ISSUED FOR 'DART' DECISION MEETING</p>	<p>PROJECT INFORMATION:</p> <p>STOREWEST - MEDICINE HILL STORAGE</p> <p>LOT: 12 BLOCK: 3 PLAN: 251 0905 CIVIC ADDRESS: 1152 N/A DRIVE S.W. CALGARY, ALBERTA</p>	<p>REVISIONS:</p> <table border="1"> <thead> <tr> <th>Q</th> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td>Q</td> <td>-</td> <td>2026-02-24</td> <td>ISSUED FOR 'DART' DECISION MEETING</td> </tr> <tr> <td>T</td> <td>-</td> <td>2026-02-20</td> <td>RESUBMITTED FOR DTR-3 COMMENTS</td> </tr> <tr> <td>S</td> <td>-</td> <td>2026-02-13</td> <td>LOCKER LAYOUT ISSUED FOR REVIEW</td> </tr> <tr> <td>R</td> <td>-</td> <td>2026-02-11</td> <td>ISSUED FOR SUBCONSULTANT INFORMATION</td> </tr> <tr> <td>Q</td> <td>-</td> <td>2026-01-14</td> <td>ISSUED FOR SUBCONSULTANT INFORMATION</td> </tr> <tr> <td>P</td> <td>-</td> <td>2026-01-12</td> <td>RESUBMITTED FOR DTR-2 COMMENTS</td> </tr> </tbody> </table>	Q	REV	DATE	DESCRIPTION	Q	-	2026-02-24	ISSUED FOR 'DART' DECISION MEETING	T	-	2026-02-20	RESUBMITTED FOR DTR-3 COMMENTS	S	-	2026-02-13	LOCKER LAYOUT ISSUED FOR REVIEW	R	-	2026-02-11	ISSUED FOR SUBCONSULTANT INFORMATION	Q	-	2026-01-14	ISSUED FOR SUBCONSULTANT INFORMATION	P	-	2026-01-12	RESUBMITTED FOR DTR-2 COMMENTS	<p>Drawing Title:</p> <p>BUILDING SECTIONS</p>	<p>JOB NUMBER: 25-010 DATE: APR. 16, 2025 DRAWN BY: KBK</p> <p>A4.1</p>
Q	REV	DATE	DESCRIPTION																																	
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RULES FOR FACADES AND SELF STORAGE FACILITY

GLAZING REQUIREMENTS AS PER DIRECT CONTROL - 35D2025

- THE FACADE OF A BUILDING ON THE GROUND FLOOR FACING NA'A DRIVE SW MUST PROVIDE WINDOWS OF TRANSPARENT AND UNOBSCURED GLASS THAT OCCUPY A MIN. 50% OF THE FACADE BETWEEN THE HEIGHT OF 0.6m AND 2.4m
- THE FACADE OF A BUILDING LOCATED ABOVE THE GROUND FLOOR AND FACING A STREET MUST PROVIDE WINDOWS WITH UNOBSCURED GLASS THAT OCCUPY A MINIMUM OF 15% OF THE FACADE

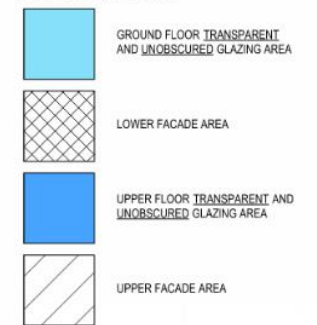
GROUND FLOOR GLAZING

- FACADE AREA: 99.67sqm (1,072.84sqft)
- MIN. REQUIRED GLAZING: 49.84sqm (536.47sqft) (50%)
- PROVIDED GLAZING: 55.73 sqm (599.84sqft) (55.9%)

UPPER FLOOR GLAZING

- FACADE AREA: 439.46sqm (4,730.31sqft)
- MIN. REQUIRED GLAZING: 65.92sqm (709.56sqft) (15%)
- PROVIDED GLAZING: 71.21sqm (766.50sqft) (16.2%)

BYLAW GLAZING LEGEND



STREET FACING GLAZING ELEVATION (SOUTH)

SCALE: 1/8" = 1'-0"

CLIENT:

MECHANICAL / ELECTRICAL:

RED DEER COUNTY, ALBERTA, CANADA
PHONE: 587.377.0232
EMAIL: KEONG@KBENGINEERING.CA

STRUCTURAL:

ARCHITECTURAL:

Suite 100 - 550 Laura Avenue Red Deer County, AB, T4E 0A5
Ph: 403-358-3311 www.AxiomArch.ca

AXIOM ARCHITECTURE INC. (RED DEER)
PERMIT No. AC 13659
ISSUED PURSUANT TO THE ARCHITECTS ACT OF ALBERTA

STAMPS:

ISSUED FOR 'DART' DECISION MEETING

PROJECT INFORMATION:

STOREWEST - MEDICINE HILL STORAGE

LOT: 12
BLOCK: 3
PLAN: 251 0905
CIVIC ADDRESS: 1152 NA'A DRIVE S.W.
CALGARY, ALBERTA

REVISIONS:

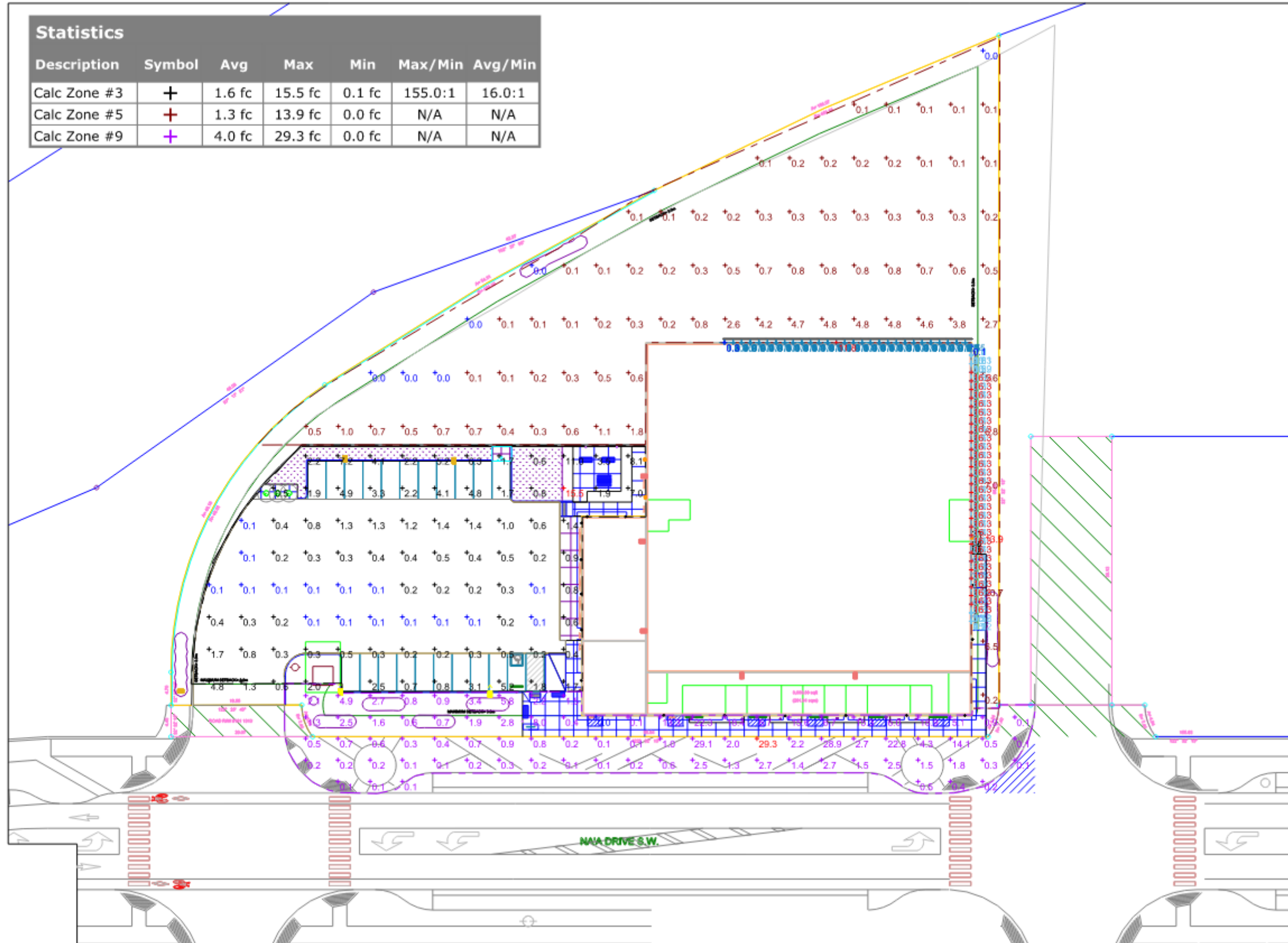
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ISSUE	REV.	DATE	DESCRIPTION

Drawing Title:

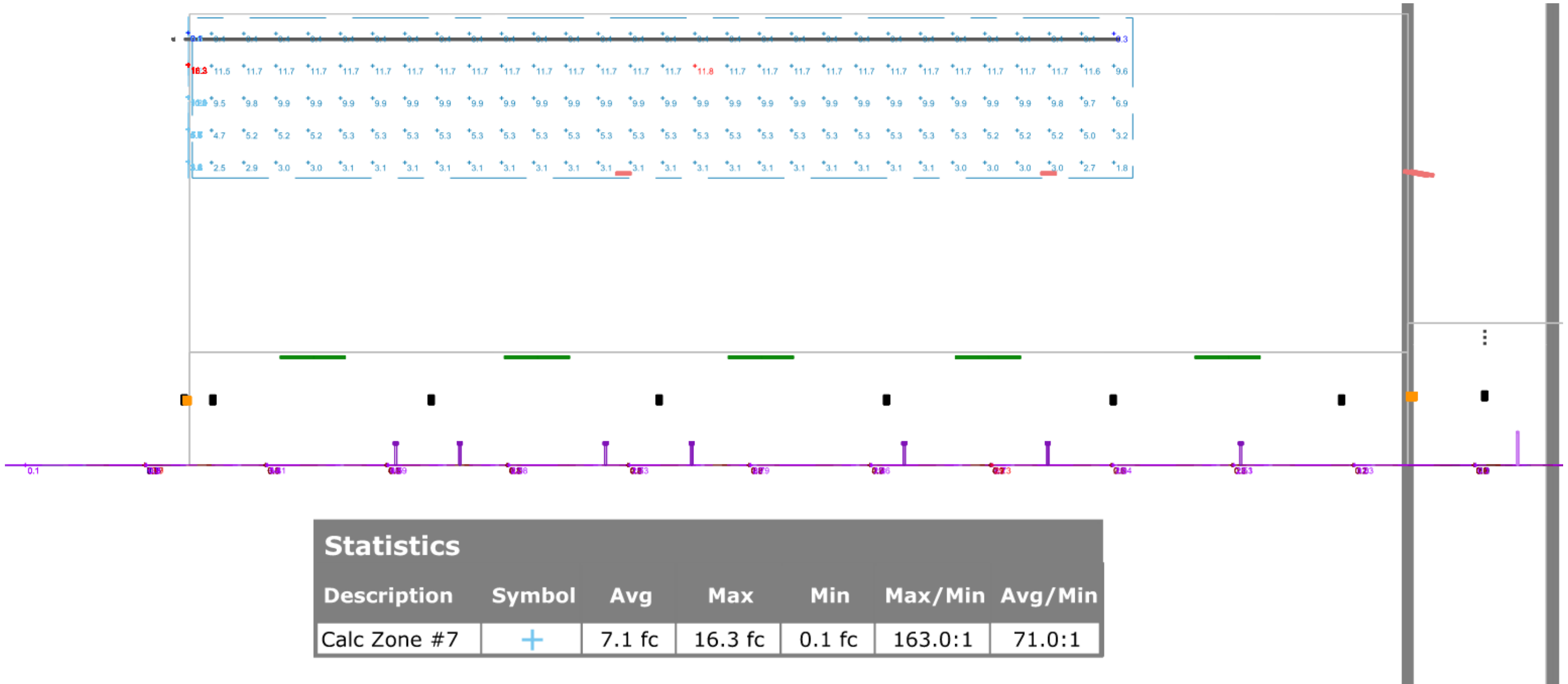
GLAZING COMPLIANCE EXTERIOR ELEVATION

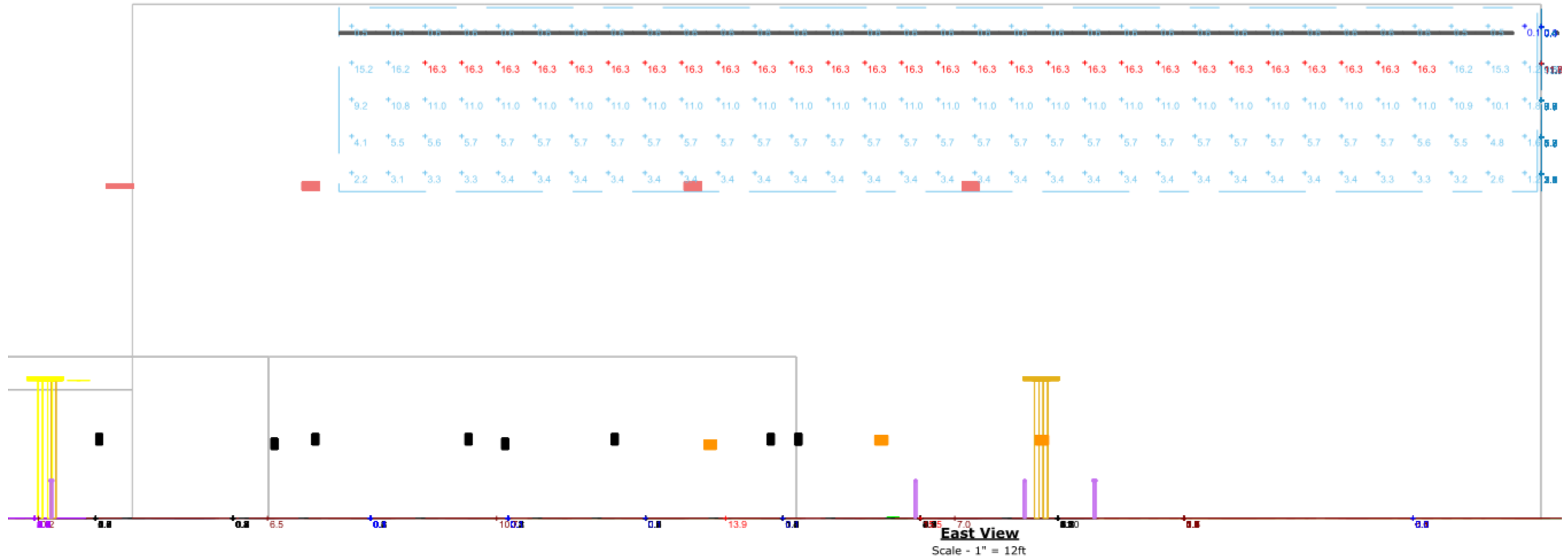
JOB NUMBER: 25-010
DATE: APR. 16, 2025
DRAWN BY: KBC

A5.1












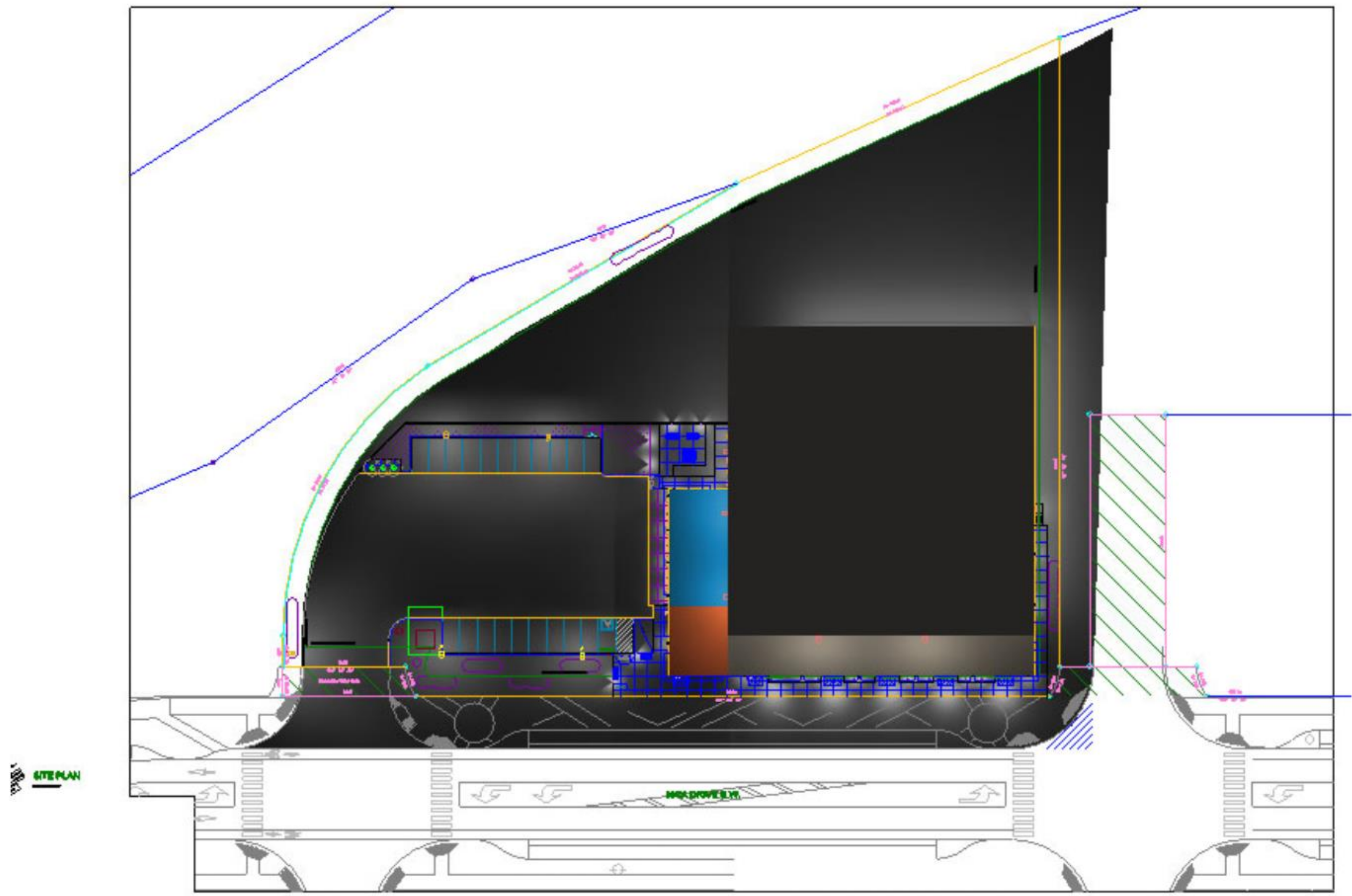
Plan View
Scale - 1" = 50ft

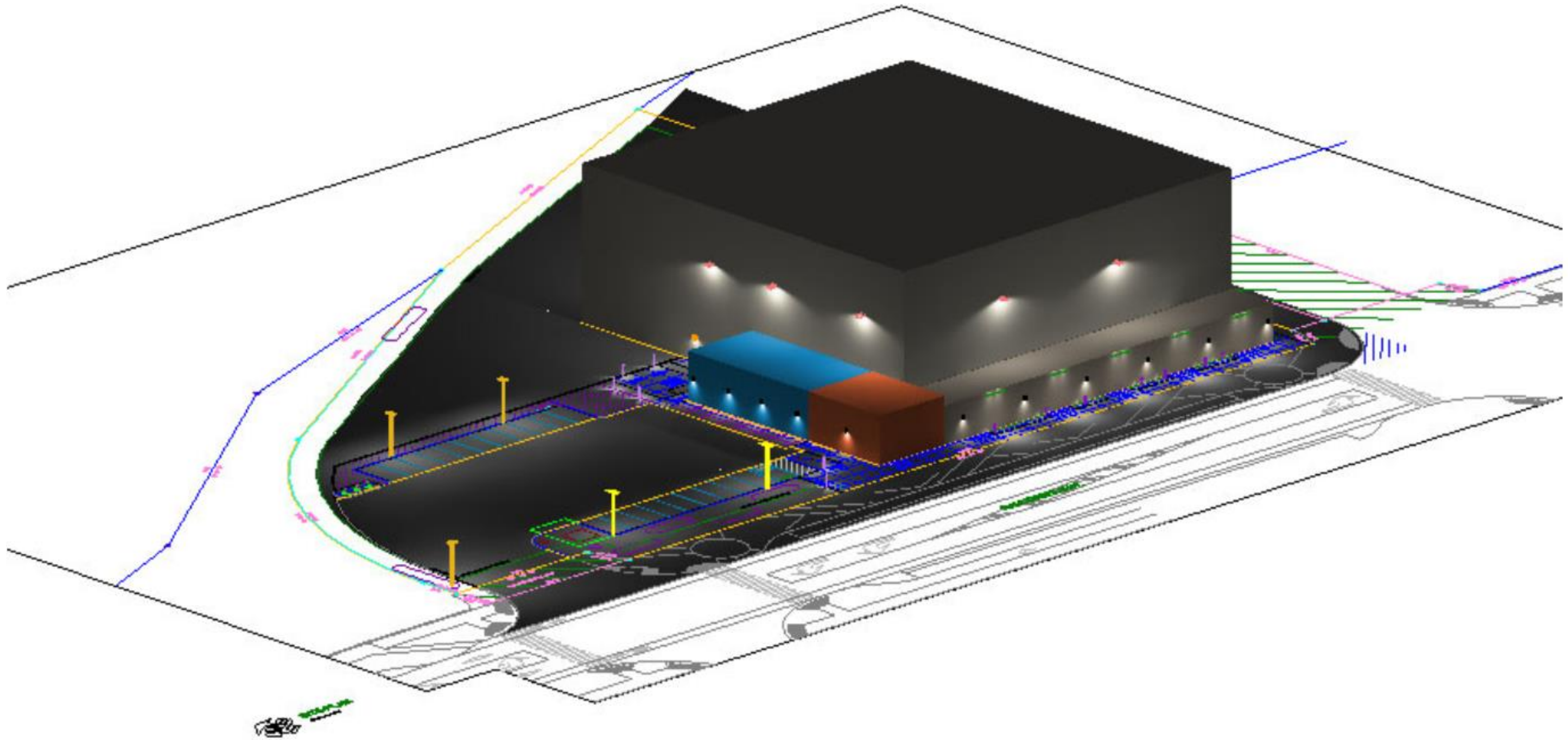


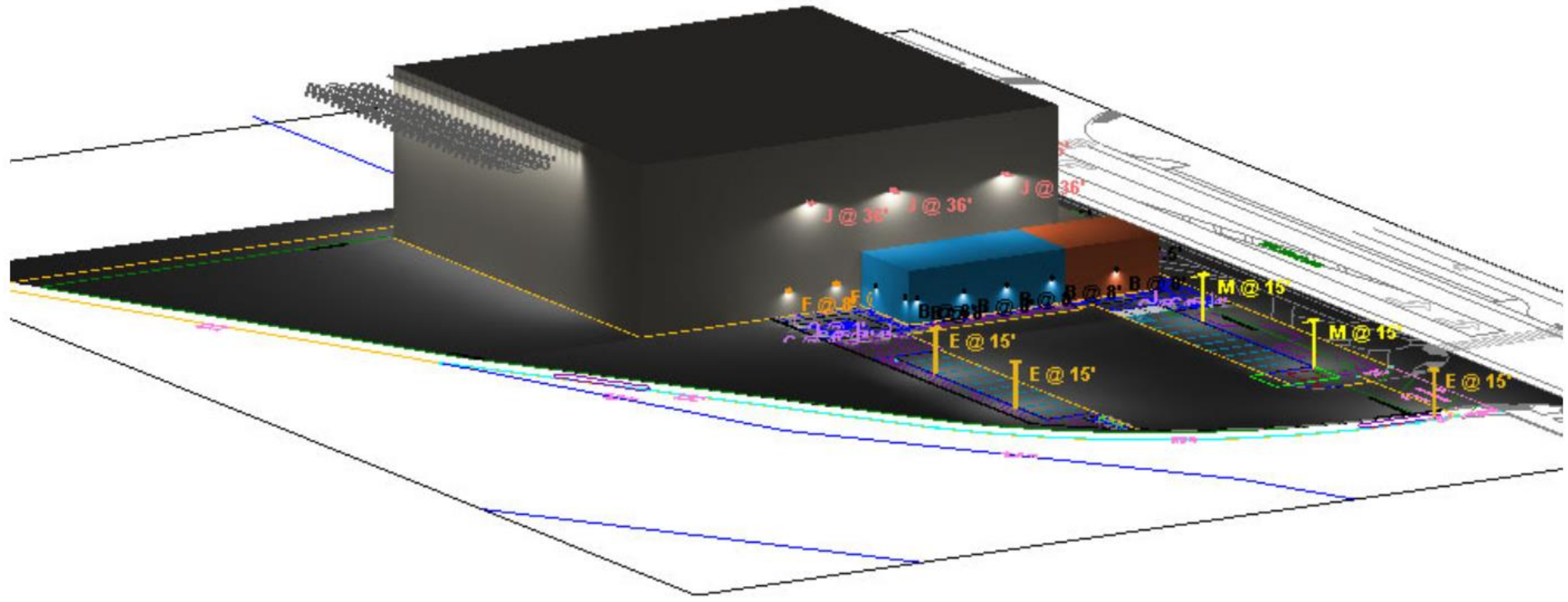


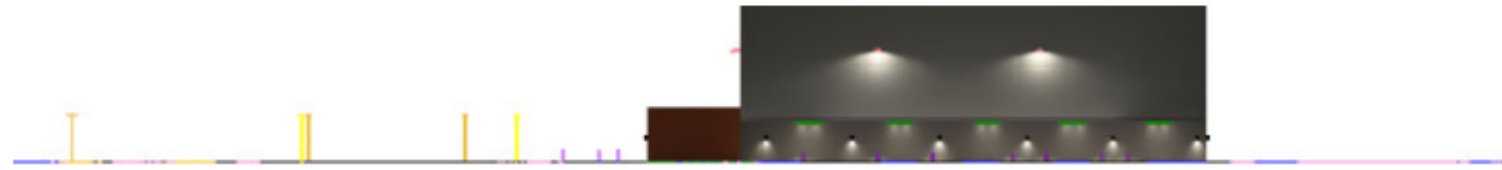
Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Calc Zone #7	+	7.1 fc	16.3 fc	0.1 fc	163.0:1	71.0:1

Schedule									
Symbol	Label	QTY	Description	Part Number	Lamp	Lumens per Lamp	LLF	Wattage	
	A	61	Linear Sign Lighting 12W/30K 24V	ONOFF 1200X38X36MM	807801-1200-3000K-S30	1477	0.4	11.5	
	B	15	4in Cylinder - Down 14W 30K	IP-228030-B-14W3K-S60D	14W 3000K 60Deg	1200	0.9	14	
	C	6	Bollard 18W 30K 40in	IP55-116577-B-3-18W-830-L	116577-III-18W830-L	1530	0.9	18	
	E	3	Set to 60W/3000K/T4-Visor-Back	FL05-100W-3P-3CCT-UD		8783	0.9	60	
	F	3	Wallpack Small - 15W 30K Type 3	AWP05-40W-3CCT-4P-UD		2237	0.9	15.2234	
	J	5	Set to 100W/4000K/T3-Visor-Front	FL05-100W-3P-3CCT-UD		9736	0.75	96.5165	
	K	7	Bollard 8W 30K 22in	IP55-116577-B-1-8W-830-L	116577-III-18W830-L	680	0.9	18	
	G	10	Linear Sign Lighting 24W/30K 24V	1200X38X36MM	807801-1200-3000K-S30	2953	0.9	23.5	
	M	2	Set to 60W/3000K/T4-Visor-Front	FL05-100W-3P-3CCT-UD		5399	0.9	56.6041	









View #6



View #8



View #9



View #10

Urban Design Review Panel Comments

Date	June 25, 2025	
Time	3:00	
Panel Members	Present Boris Karn (a/chair) Rick Gendron Rasool Ghodoosi Dehnavi Ben Klumper Ryan Martinson	Distribution Jeff Lyness (chair) Kathy Oberg (chair) Noorullah Hussain Zada Andrea Kennedy Maria Landry Bao-Nghi Nhan Brendan Stevenson
Advisor	Joyce Tang, Chief Urban Designer /a	
Application number	PE2025-00772	
Municipal address	1024 Na'A Dr SW	
Community	Medicine Hill	
Project description	Self-storage facility	
Review	first	
File Manager	Quadri Adebayo	
Urban Design	Sonny Tomic	
Applicant	Storewest	

*Based on the applicant's response to the Panel's comments, the Chief Urban Designer will determine if further review will include the Panel or be completed internally only by Urban Design.

Summary

This project is a 5-storey mixed use self-storage facility located in the Medicine Hill neighbourhood. Despite its large size, the scale and massing of the proposed building has been well managed on site and addresses the street in an appropriate and commendable manner (e.g. street-facing CRUs). Additionally, the large and well considered public art mural proposed on the two most prominent and publicly visible sides of the building will be a landmark for the area. The Panel appreciates and fully supports the robust engagement process with Indigenous communities that led to the proposed mural motif and overall design approach. The Panel appreciates the applicant attending the Urban Design Review Panel at this early stage of the development process and is supportive of this unique application, with some potential refinements noted below that could be integrated prior to formal Development Permit submission.

The Panel encourages the applicant to consider increasing the size of the mural while simplifying or removing some of the other façade motifs, logos, and colours. Also, the Panel suggests that additional softscape / landscaping on the sidewalk adjacent to Na'a Drive and within the loading area be considered. Finally, further articulation to the individual entrances for the CRUs and storage would be appropriate.

Applicant Response

PE2025-00772
UDRP Comments

06/25/2025

Urban Design Element	
Place Recognize and enhance the unique and emerging identity of a place by responding to surrounding context, local policy, and community objectives through the contribution of innovative architecture and public realm.	
Site	Does the site planning show innovation in addressing site constraints and challenges?
	Does the design respect existing topography, landscape, and archaeology?
	Does the site design accommodate people of all abilities?
Architecture	Is the project visually interesting and unique?
	Does the architecture respond to landmark and gateway opportunities presented by the site?
	Does the design reflect any distinctive social, cultural or historical aspects of the site and community?
Public Realm	Does the project contribute to the creation of a high quality, connected public realm?
UDRP Commentary	The proposed development will be a landmark in the area because of its large mural installation on two sides of the building. This is a very exciting and well-thought through installation that recognizes the Indigenous history and traditions of the area. The Panel encourages the applicant to consider making the mural installation larger and reduce / simplify some of the other motifs to help bring this public art installation into greater focus. Additionally, it is recommended that the applicant place an easily viewable interpretive plaque on site so the public can learn more about the mural and its meaning.
Applicant Response	Making the Buffalo Mural larger has been considered. Due to the overall cost of providing the Mural, the process of applying the Mural and the precast concrete panel sizes the Buffalo Mural is to remain the size as designed. Reducing the other motifs has been considered. With these items not being provided on the same sides as the Mural we believe there is still a great focus on the Mural especially from the Trans-Canada Highway. Additional motifs around the building are also to remain to maintain links to the Indigenous history in the area and the Bluebird brand identity. A viewable/intractable plaque on the site is to be provided to allow the public to learn more about the meaning the mural and the Indigenous history. Options being considered include a pedestal or a plaque that has been cast in to the precast wall. These items are being discussed and worked on to see what can be provided as we go along with the subsequent design stages.
Scale Ensure appropriate transitions between building masses and adjacent places and spaces; define street and open space edges and bring human scale through articulation, materials, details and landscaping.	
Site	Does the arrangement of buildings and spaces on the site address street edges well?
	Is the scale and placement of buildings and structures appropriate for the street and public space size and type?
	Are large service and surface parking areas modulated and screened by structures and landscaping?
Architecture	Are design strategies employed to reduce the impact of building height and bulk?
	Are street walls well defined and of appropriate height to street width and type?
	Are human scaled elements and details included to enhance street character?
Public Realm	Are public spaces well edged and framed by structures and/or landscaping?
	Does the design include detail which will enhance street character and encourage use of the public realm?
UDRP Commentary	The building creates a legible street wall, with good scale along the street frontage. The overall size of the building is masked well by the varied grade of the site. The Panel recommends that the applicant explore increasing the size of the public realm / sidewalk by moving the building further north to allow for more opportunities for trees and other street furniture to be incorporated.
Applicant Response	The Direct Control (DC35D2025) Bylaw requires a <u>maximum</u> 3.0m setback from NA'A Drive. The building cannot be pushed North to increase sidewalk due to already being set at the maximum allowed setback.
Amenity Ensure that public sidewalks and gathering spaces are generously proportioned, comfortable, safe, fully accessible, and framed by permeable facades which allow for activation throughout the year.	
Site	Are equitable, inviting access and varied movement options provided for all ages and abilities?
	Does the design work with sun orientation and seasonal climate variation?
	Does the site plan safely accommodate all travel modes?
	Are service and utility requirements located appropriately to lessen visual impact?
Architecture	Does the building(s) meet or exceed expectations for universal access design?
	Does the architecture create a pleasant street edge which feels safe to users?

Public Realm	Does the public realm design prioritize pedestrians and cyclists over vehicle access?
	Is the public realm visually interesting, comfortable, and safe during all seasons?
	Are the public spaces designed for people of all abilities and ages?
	Do the public spaces meet or exceed expectations for universal access design?
UDRP Commentary	The Panel recommends that the applicant explore additional amenity in front of the building near the offices and commercial retail units. Incorporating a boulevard with trees as a buffer between the sidewalk and the street will help to create a more pleasant pedestrian environment and give the frontage of the building a softer look and feel. Incorporating CRUs into this development at the street level is fully supported for helping to animate the street, especially given the primary use of the site is passive in nature.
Applicant Response	Due to the Direct Control (DC35D2025) Bylaw requires a maximum 3.0m setback from NA'A Drive there is minimal room to incorporate big buffers. However, the long sidewalk in front of the entrances has been broken up with landscape areas under the storefront windows. This helps to provide a more pleasant feeling as well as help direct and highlight the entrances for the pedestrians.
Legibility Create logical, permeable networks of streets and pathways that connect within and between neighbourhoods and public places; design well-defined community and building entrances with distinctive, memorable attributes.	
Site	Does the project provide a permeable, fine-grained and functional urban structure of blocks and streets?
	Does the project provide legible, accessible, continuous walking and cycling connections within the site that connect to adjacent systems and destinations?
	Does the proposed network consider future expansion into surrounding areas?
	Are large parking areas designed with clear, safe, direct pedestrian connections?
Architecture	Are buildings designed with clearly marked and differentiated entries to facilitate wayfinding?
Public Realm	Are the public routes and spaces configured to facilitate easy and safe navigation with clear paths and appropriately placed wayfinding elements?
UDRP Commentary	The Panel commends the proposed art installation on the building's prominent blank facades and recommends that the applicant explore simplifying / refining the proposed signage and architectural accents on the building to further emphasize the presence of the public art installation. Additionally, the individual entrances to the CRUs and the storage facility could be articulated more clearly through colour or architectural features / design details. The Panel suggests that the grounding of the building be done without the black banding and use a softer treatment instead (e.g. landscaping). All doorways to the CRUs should also have an accessible landing area.
Applicant Response	Providing landscape areas under the windows along Na'a Drive helps to provide a more pleasant feeling as well as help direct and highlight the entrances for the pedestrians. Barrier-Free design items for doorways complying with the N.B.C.- 2023 A.E. have been considered for access to publicly accessible areas. Additionally, the removal of the large black banding around the helps make the building feel more grounded.
Vibrancy Ensure that new developments are configured and designed to animate streets and public spaces with varied sizes and types of grade-oriented uses.	
Site	Will the building placement and orientation together with the arrangement and variety of uses activate the adjacent streets and public spaces?
	Will the project contribute to creating greater economic, employment and/or residential diversity in the neighbourhood?
Architecture	Does the building articulation, materials and details contribute to the vibrancy of the streets and public spaces?
	Is there a variety of residential and/or commercial unit types and sizes?
Public Realm	Do outdoor spaces provide varied experiences and accommodate people with diverse abilities?
UDRP Commentary	The proposed development has a variety of potential users which can activate the street, while also providing a useful service to the nearby residents. The scale of the building is suitable for the street and surrounding area. The Panel suggests that the parking area be buffered with more landscaping on the side adjacent to the street and that the hard surface area of the turnaround/loading area be reviewed further to determine if impervious surface could be reduced or broken up with soft scaping.
Applicant Response	The stalls closest to Na'a Drive have been pushed back into the site to provide a larger landscape buffer. This larger landscape buffer has allowed for the use of layered landscaping to screen the parking lot. Reviewing further providing landscaping in the middle of the parking

	area is not feasible. By pushing these stalls back, we have reduced the turnaround area for trucks to access the loading bays which in turn has reduced the amount of impervious surface on the site.
Resilience Ensure that projects provide opportunities, through their site layout, spatial configuration, materials, and sustainable design features for responsible operation and continuous adaptation to change over time.	
Site	Is the project designed to respond to change (economic, social, demographic or other) over time?
	Does the plan meet/exceed climate resilience/sustainable design expectations?
	Are active travel modes prioritized, and active lifestyle choices encouraged?
Architecture	Does the building show indication of sustainable design practices and materials?
	Is a range of uses accommodated; does the design anticipate future change?
	Is the building designed to endure over time with reasonable maintenance?
Public Realm	Are public spaces adaptable for multiple uses over short and medium term?
	Does the public realm design respond to climate resilience / sustainability expectations?
UDRP Commentary	Further sustainable design practices could be explored through the development process. Treatments like LED lighting, solar panels on the roof, and incorporating soft scaping within the large impervious surface that is used for the turnaround/loading area are some considerations the Panel suggests. These are by no means exhaustive.
Applicant Response	Items like building mounted LED lighting are incorporated to further explore sustainable design practices. Providing landscaping in the middle of the parking area is not feasible as it interferes with truck access to the loading bays. However, this has reduced the amount of impervious surface on the site as a larger landscape buffer is being provided along Na'a Drive.

Applicant Outreach Summary

2025 December 9



Community Outreach on Planning & Development Applicant-led Outreach Summary

Please complete this form and include with your application submission.

Project name: Medicine Hill - Bluebird Self Storage

Did you conduct community outreach on your application? YES or NO

If no, please provide your rationale for why you did not conduct outreach.

Outreach Strategy

Provide an overview of your outreach strategy, summary of tactics and techniques you undertook (Include dates, locations, # of participants and any other relevant details)

The outreach strategy began in the land use process and involved multiple rounds of engagement with stakeholders and neighbouring community associations (West Springs/Cougar Ridge, Bowness, Coach Hill/Patterson Heights and Valley Ridge) in 2024. Hosted a virtual meeting Bowness Community Association. Worked with Indigenous relations Office, hosted virtual meeting with Lorna Crowshoe, Issue Strategist - All to positive effect. Attended Joint Advisory Committee meeting. Responded in writing to matters raised during meeting to file manager. Arising out of the land use redesignation was the obligation to present the building design to the UDRP - which which garnered positive feedback and endorsement of the design. At DP, repeated questions from land use arose from the stakeholders listed below, which were self apparent as pre-land use or market-driven practicalities that are outlined in the "What Did You Hear?" section on the next page.



Affected Parties

Who did you connect with in your outreach program? List all groups you connected with. (Please do not include individual names)

West Springs/Cougar Ridge, Bowness, Coach Hill/Patterson Heights Community Associations
Indigenous Relations Office & Traditional Knowledge Keepers
Paskapoo Slopes Joint Advisory Committee which includes Paskapoo Slopes Preservation Society, Calgary River Valleys, and neighbouring communities and property owners.

calgary.ca/planningoutreach



Community Outreach for Planning & Development Applicant-led Outreach Summary

What did you hear?

Provide a summary of main issues and ideas that were raised by participants in your outreach.

The self-storage development itself exists as a result of the land use decision that was passed into law by Council, so there was not much to be gained in debating whether or not the property could be an restaurant or a park. As a rule, the questions that were posed by the Paskapoo at the land use stage were oriented toward the DP process, and at the DP stage, the questions were referenced the land use process. It was outside the ordinary, and somewhat difficult to answer these stakeholders questions with practical responses.

As designed, the development meets or exceeds the rules that were established in the DC district, including provisions for a maximum height. On the concept of retail space and street activation - it is important to note, many years ago, the Medicine Hill community developer registered protective restrictive covenants on title for their long term tenants against most retail and commercial uses in the area, so there is a limited number of uses that are available for CRUs (commercial/ retail units) in the community of Medicine Hills.

Here is the list of currently enforceable restricted uses:

1. Pet-Related Uses

- Pet sales
- Pet grooming
- Pet boarding
- Pet daycare
- Pet training
- Veterinary services
- Pet food sales (except incidental)
- Pet accessories retail (except incidental)

2. Size / Area Restrictions

- Retail stores over 40,000 sq ft (without consent)
- Grocery or supermarket over 40,000 sq ft (without consent)
- Any retail use over 10,000 sq ft (without consent)
- Any single use over 7,500 sq ft (without consent)

3. Tobacco / Vape / Cannabis

- Smokeshops
- Tobacco stores
- E-cigarette / vape stores
- Cannabis retail

4. Financial / High-Risk Uses

- Pawn shops
- Second-hand stores
- Military surplus stores
- Payday loan services
- Cheque-cashing services
- Other high-risk financial services

5. Personal Services Restrictions

- Tattoo parlors
- Body piercing shops
- Massage therapy uses exceeding permitted limits
- Medical aesthetic clinics exceeding permitted limits
- Dental clinics (unless within exceptions)
- Wellness clinics exceeding permitted size or categories

6. Entertainment / Recreation Uses

- Bingo halls
- Gaming facilities
- Pool halls
- Billiard rooms
- Bowling alleys
- Amusement arcades
- Virtual reality gaming facilities
- Trampoline parks
- Recreation centres
- Theatres
- Cinemas
- Live entertainment venues
- Nightclubs

7. Firearms / Weapons

- Gun stores
- Ammunition sales
- Firearms ranges

8. Automotive Uses

- Motorcycle sales
- Recreational vehicle sales
- Automobile sales
- Truck sales
- Auto repair shops
- Auto parts stores

9. Adult-Oriented Uses

- Adult bookstores
- Adult movie stores
- Adult entertainment venues

10. Food & Beverage Restrictions

- Liquor stores (except where specifically permitted)
- Bars as a primary use
- Lounges as a primary use
- Bakeries over 2,000 sq ft (unless permitted)
- Bulk food stores beyond permitted limits
- Retailers with bulk food sections exceeding allowable percentages

11. Restaurant / Drive-Through Restrictions

- Drive-through food or beverage service
- Quick-serve fast food with no seating

12. Lodging / Hospitality

- Hotels
- Motels

13. Industrial / Storage

- Self-storage facilities
- Industrial manufacturing uses
- Warehousing uses

14. Market / Auction / Recycling Uses

- Flea markets
- Auction houses
- Recycling depots

15. Large Format / Big Box Retail

- Large discount club stores beyond allowable limits
- Mattress stores
- Bedding stores
- Sleep-themed retailers
- Large furniture stores selling bedding categories
- Any expansion of restricted uses beyond permitted limits

Further, our company conducted a survey of CRU space for lease in Trinity Hills, and found 22,000 sq ft vacant, just in the immediate surrounding area around the subject property.

The micro-business offices at street level on Na'a Drive that have been designed into the new building are extremely popular in our other developments in Calgary, where there are waiting lists to gain access due to the flexibility and accessible cost of having an office/ commercial storefront on a main street.

Some of the users that are currently operating at our Mahogany location are: A Mechanical contractor; a home-staging company/ furniture sales; custom engineering blueprint-maker; a Therapist who also uses another office as her waiting room; a Mortgage Broker; a drop-ship company using several storage units to hold inventory; a Realtor; and the accounting office for a pizzeria chain. These micro-business companies benefits from the exposure and flexibility we are able to offer.

On height restrictions: The community developer also established rules not to impede sightlines for the grocery store and other retailers, which have been observed and satisfied in the design process, and in consultation with the original developer of the community.

How did input influence decisions?

Provide a summary of how the issues and ideas summarized above influenced project decisions. If they did not, provide a response for why.

At the land use stage, one comment from the Paskapoo Slopes group was to refrain from having too many windows on the building, out of concern for the danger to local birdlife. Our building has a mural that is spread over two hundred feet of wall space - an effective and well-received image that refers to the cultural significance of Medicine Hill also helps to mitigate danger to local birdlife.

How did you close the loop ?

Provide a summary of how you shared outreach outcomes and final project decisions with those who participated in your outreach. (Please include any reports or supplementary materials as attachments)

Our design process culminated at the UDRP - and those in attendance for our presentation were overwhelmingly supportive of the design, and their few comments on simplifying the Na'a Drive side of the building were put into action by our design team.

calgary.ca/planningoutreach



Community Outreach Assessment Tool

The City has created this optional assessment tool which is designed to help you navigate the decision to undertake outreach and provides guidance on high-level outreach considerations based on the impact of your proposed project and the community complexity.

When filling this out, it can be helpful to do some initial research into past projects in the area as this will help inform your answers for more accurate results. Also, ensure when you are answering these that you are putting yourself in the shoes of the community. Remember that how you answer today, is reflective of the context of today and isn't a guarantee that no issues will arise in the future as you move through your process.

COMMUNITY IMPACT	1 Point	2 Points	3 Points	Enter Values
How similar is your project to what already exists in the community?	Similar projects exist in the community and is a low level of change for community.	Community is not that familiar with this project type and will be a moderate change.	Major change or redevelopment being proposed and likely a big change for the community.	2 <input type="text"/>
What is the duration of your project (to occupancy)?	Under 2 years	3 - 5 years	5 + years	1 <input type="text"/>
How broadly will this project impact the surrounding community?	Likely to affect immediate neighbours.	Could have an impact within a few blocks.	Likely to have an impact community - wide.	1 <input type="text"/>
How common is redevelopment within the community?	Redevelopment is common in community.	Moderate redevelopment has occurred.	Little to no redevelopment has occurred.	2 <input type="text"/>
TOTAL				6
COMMUNITY COMPLEXITY	1 Point	2 Points	3 Points	Enter Values
How do you anticipate the community will react to this project?	Little attention - project unlikely to be a public issue.	Anticipate there will be some attention. Disagreement or differing opinions are expected.	Anticipate this to be a highly sensitive issue.	1 <input type="text"/>
How inclined will the community be to accept this project?	Likely minimal to no issues anticipated.	Some issues anticipated.	Many issues are anticipated.	1 <input type="text"/>
What level of influence does the community have over project decisions?	No decisions open for input, willing to inform the community of project details.	Willing to listen to community and learn about their ideas and respond where possible	Willing to collect input to influence specific project decisions.	1 <input type="text"/>
TOTAL				3

IMPACT SCORE
4 to 6 1
7 to 9 2
10 to 12 3

COMPLEXITY SCORE
3 to 5 A
6 to 9 B

COMMUNITY IMPACT	COMMUNITY COMPLEXITY	
	3A	3B
	2A	2B
	1A	1B

Your Project's Score:
1 A

Outreach Approach Assessment	
Direct approach (1A, 1B)	Your project is likely of low impact to the community and is not proposing a major change or disruption. For higher complexity, you may have to put a bit more effort in developing content to educate and inform The community about your project details. Consider choosing 2 - 3 tactics suited for a targeted audience*.
Moderate approach (2A, 3A)	Your project is of medium to high impact for the community, but is not very complex and likely little attention and/or issues are expected. Given the level of impact consider expanding your reach beyond the immediate neighbours to inform them of the project details and collect input if needed. Consider choosing 4 - 6 tactics for a range of targeted and broader audiences*.
Comprehensive approach (2B, 3B)	Your project is of medium to high impact for the community, and of higher complexity. There are likely issues that will need to be mitigated and addressed and extra effort will be needed to educate and inform the community about your project. Consider a broader approach with the community and be open to an iterative process with multiple tactics where input could help inform better decisions*.

*For an overview of outreach tactics and techniques you could consider, click [here](#).

Community Association Responses

2025 October 1

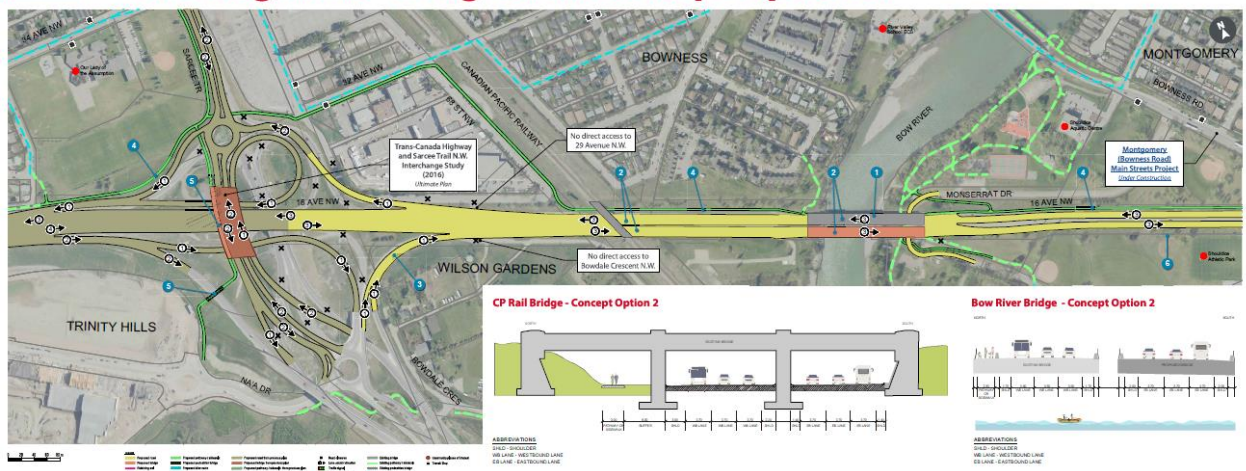
The storage Facility proposes high lumen lighting, contributing to the light pollution from the Trinity Hills development that is already in existence. Given these properties sit at an elevation much higher than Bowness, a majority of residents who live just below the Trinity Hills area will be subject to this light pollution if the DP do not include the following:

- lower lumen lighting fixtures for facade lighting and mural lighting.
- sign lighting that has a lumen output adjustment based on ambient light combined with lower lumen output selections.
- These are options available on nearly all lighting products via manufacturers at time of purchase.
- All sign lighting should be static. No moving or changing images changing the frequency or intensity of lumen output.
- When images change, colours or pixels change, this automatically changes the intensity of lumen output on the sign light creating a flashing effect.
- options for backlit sign lighting.
- options for backlit awning sign lighting.
- options for lighting placed on arms that cast light back towards the building signage vs those that cast light outwards or upwards towards residential buildings.

We like that the building has murals on it. Much nicer than additional advertising space or just a plain building.

We have attached the West Segment Mobility plan where a pedestrian bridge to Trinity Hills was proposed from Bowness. We see the green spaces on the north and west side of the development as potential tie in locations for a pedestrian bridge or other types of walking infrastructure for Bowness and Trinity Hills.

Calgary **West Segment - Long-Term Concept Option 2** Provides six lanes on 16 Avenue N.W.



Location Map

Benefits

- Retains existing CP Rail bridge
- Retains existing Bow River bridge and pathway connection on north side
- Constructs new Bow River bridge for eastbound traffic
- Repurposes existing bridges where possible to avoid cost of widening or replacing those bridges
- Avoids impacts to critical water and sanitary sewer pipes along 16 Avenue N.W.

Trade-Offs

- Potential grading impacts to properties south of 16 Avenue N.W. and west of the CP Rail bridge.
- Shifts 16 Avenue N.W. slightly to the south
- Removes some trees to the south of 16 Avenue N.W. near the Bow River bridge
- Removes direct access to 29 Avenue N.W. and Bowdale Crescent N.W.

Design Features

Social and Healthy Lifestyle

- 1 Widens existing north pathway over the Bow River bridge

Mobility and Functionality

- 2 Provides additional core lane in each direction for six core lanes of travel along 16 Avenue N.W.
- 3 Makes merge from northbound Sarcee Trail to eastbound 16 Avenue N.W. longer to improve safety

Character and Identity

- 4 Connects Shouldice Park, Bowness and Montgomery with continuous pathway north of 16 Avenue N.W.
- 5 Connects Trinity Hills, Wilson Gardens, and Bowness with continuous pathway over 16 Avenue N.W. at Sarcee Trail
- 6 Creates a visual transition to Montgomery in the gateway zone with trees, street furniture, banners, displays or other elements

2025 December 3

The linear lights above the bison mural (as shown on drawing A3.5) should be dimmable according to ambient light to reduce light pollution the darker it gets.

Previous comments submitted by our committee seem to have been taken into account given the updated lighting schedule in this DP.

Paskapoo Slopes Preservation Society Response

2025 October 3

The application was circulated to our organization as we are part of the Joint Advisory Committee established by City Council for the Paskapoo Slopes. The building mass is of concern because of the size and prominence as viewed from the TransCanada Highway and Bowness. The visual and aesthetic requirements under the Area Structure Plan have not been satisfied by either the structure of the building or by sufficient landscaping and plantings along the northern side of the building. In addition, the retail space along Na'a Drive is not planned or designed in a way that will activate the street as expected under the ASP. There is only one bench and one small bike rack shown in the plans which is also inadequate if the space is really intended to function as a vibrant and welcoming place for people. This development does not appear to contribute to engaging community use and enjoyment of the street as was intended.

Hugh Magill, President, Paskapoo Slopes Preservation Society

**Land Use Amendment in Upper Mount Royal (Ward 8) 1911 – 11 Street SW,
LOC2025-0151**

RECOMMENDATION:

That Calgary Planning Commission recommend that Council:

Refuse and abandon the proposed bylaw for the redesignation of 0.06 hectares ± (0.14 acres ±) located at 1911 – 11 Street SW (Plan 179R, Block 43, Lot 7) from Direct Control (DC) District to Direct Control (DC) District to establish a maximum parcel width and minimum building setback, with guidelines (Attachment 2).

HIGHLIGHTS

- This application seeks to redesignate the subject site from a Direct Control (DC) District to a DC District to allow for a subdivision and development of two single detached dwellings.
- Administration is recommending refusal of the proposed DC District because it adds residential permitted uses which is not supported by the Heritage Guideline Areas of the *West Elbow Communities Local Area Plan* (LAP). In addition, the subdivision desired by the applicant could be supported through an standard low density district.
- What does this mean to Calgarians? Refusal of this proposal will ensure the land use is consistent with the LAP.
- Why does this matter? The LAP guides future development within the Plan area, helping to manage and direct change in a way that supports the long-term vision for West Elbow communities. Refusal of this land use amendment would ensure the policy in the LAP are able to be implemented.
- Two development permits for two single detached dwellings (DP2025-05363 and DP2025-05364) and an associated subdivision application (SB2025-0380) were submitted in late 2025 and are under review.
- There is no previous Council direction related to this proposal.

DISCUSSION

This land use amendment application in the southwest neighbourhood of Upper Mount Royal was submitted by Horizon Land Surveys on behalf of the landowner, 1317958 Alberta Ltd. (Amir Wasef) on 2025 July 18. Two development permits (DP2025-05363 and DP2025-05364) have been submitted at this time; the intent is for two detached dwellings on the site. A subdivision application (SB2025-0380) has also been submitted in support of this intent and is currently under review.

The 0.06 hectare (0.14 acre) site is located on 11 Street SW, south of Cameron Avenue SW, one and a half blocks south of 17 Avenue SW. The surrounding development is single detached dwellings to the south and low-rise residential buildings to the north. The proposed DC District would allow for the subdivision of the site for a single detached dwelling on each future parcel.

As indicated in the Applicant Submission (Attachment 3), the subject parcel is subject to a restrictive covenant (RC) that restricts uses on the property to a maximum of one dwelling unit, a minimum parcel width of 15.0 metres and a setback of 7.6 metres. The proposed DC District

**Land Use Amendment in Upper Mount Royal (Ward 8) 1911 - 11 Street SW,
LOC2025-0151**

adds permitted residential uses, implements a maximum parcel width of 7.5 metres and increases the setback depth. Administration is recommending refusal of this proposal because it is not in alignment with the Heritage Guideline Area policies of the LAP and the subdivision of the lot can be accomplished with a standard land use district.

A detailed planning evaluation of the application, including location maps and site context, is provided in the Background and Planning Evaluation (Attachment 1).

ENGAGEMENT AND COMMUNICATION

- Outreach was undertaken by the Applicant
- Public/interested parties were informed by Administration

Applicant-Led Outreach

As part of the review of the proposed land use amendment application, the applicant was encouraged to use the [Applicant Outreach Toolkit](#) to assess which level of outreach with the public/interested parties and respective community association was appropriate. As part of the outreach strategy, the applicant reached out to the Mount Royal Community Association (CA), the Ward 8 Councillor and door-knocked and delivered a post card to residents within a 100 metre radius of the subject parcel. The Applicant Outreach Summary can be found in Attachment 4.

City-Led Outreach

In keeping with Administration's practices, this application was circulated to the public/interested parties notice posted on-site and published [online](#). Notification letters were also sent to adjacent landowners.

Administration received 46 responses from the public. Forty-five responses were in opposition and one was in support. The responses in opposition raised the following areas of concerns:

- land use appropriateness;
- heritage character of the area, design fit;
- landscape context pertaining to width and depth of lot;
- building setbacks contextual to the street;
- increased density on lots traditionally with lower density; and
- restrictive covenants limiting number of dwellings.

The CA provided comments on 2025 August 27 after an internal planning and development meeting and after speaking with the applicant (Attachment 5). The CA opposes the application based on a lack of adherence to the LAP heritage policies for additional dwellings on a parcel, reduced front yard setback and reduced parcel width, and utilizing the planning system to circumvent the restrictive covenant on title which limits development of this nature on the parcel.

Administration considered the relevant planning issues to the application and determined the proposal to be inappropriate as the proposal is not in alignment with the Heritage Guideline

**Land Use Amendment in Upper Mount Royal (Ward 8) 1911 - 11 Street SW,
LOC2025-0151**

Area policies of the LAP and the proposed DC District is not required to allow for a future subdivision.

Following Calgary Planning Commission, notifications for a Public Hearing of Council for the land use amendment will be posted on-site and mailed to adjacent landowners. In addition, Commission’s recommendation and the date of the Public Hearing will be advertised.

IMPLICATIONS

Social

The proposal would allow for a range of housing types that may accommodate the housing needs of different age groups, lifestyles and demographics.

Environmental

This application does not include any specific actions that address the objectives of the *Calgary Climate Strategy – Pathways to 2050*. Further opportunities to align development of this site with applicable climate strategies is being explored and encouraged through the review of the development permit.

Economic

The proposed land use could allow for a more efficient use of land, existing infrastructure and services and provide more compact housing in the community.

Service and Financial Implications

No anticipated financial impact.

RISK

There are no known risks associated with this proposal.

ATTACHMENTS

1. Background and Planning Evaluation
2. Proposed Direct Control District
3. Applicant Submission
4. Applicant Outreach Summary
5. Community Association Response

Department Circulation

General Manager (Name)	Department	Approve/Consult/Inform

Background and Planning Evaluation

Background and Site Context

The subject site is a mid-block parcel located in the southwest neighbourhood of Upper Mount Royal at 1911 – 11 Street SW. The site is approximately 15 metres wide by 39 metres deep and 0.06 hectares (0.14 acres) in size. There is currently a single detached dwelling on the site with a garage accessed from the lane located directly north of the property.

Surrounding development is characterized solely by residential development. To the south of the site the parcels are primarily designated as Direct Control (DC) District ([Bylaw 1Z99](#)) and are typically developed as single detached dwellings. To the north of the subject site, the lots are designated as the Multi-Residential – Contextual Medium Profile (M-C2) District and developed as low-rise apartment development.

The site is about 200 metres (a three-minute walk) from 17 Avenue SW, which is identified as a Neighbourhood Main Street and part of the Primary Transit Network in the *Municipal Development Plan* (MDP). Bus routes are accessed on 17 Avenue SW to the north and to the west of the site on 14 Avenue SW.

Mount Royal School is located approximately 400 metres (a four-minute walk) west of the site. Thomson Family Park is approximately 500 metres (a seven-minute walk) to the north of the site.

Community Peak Population Table

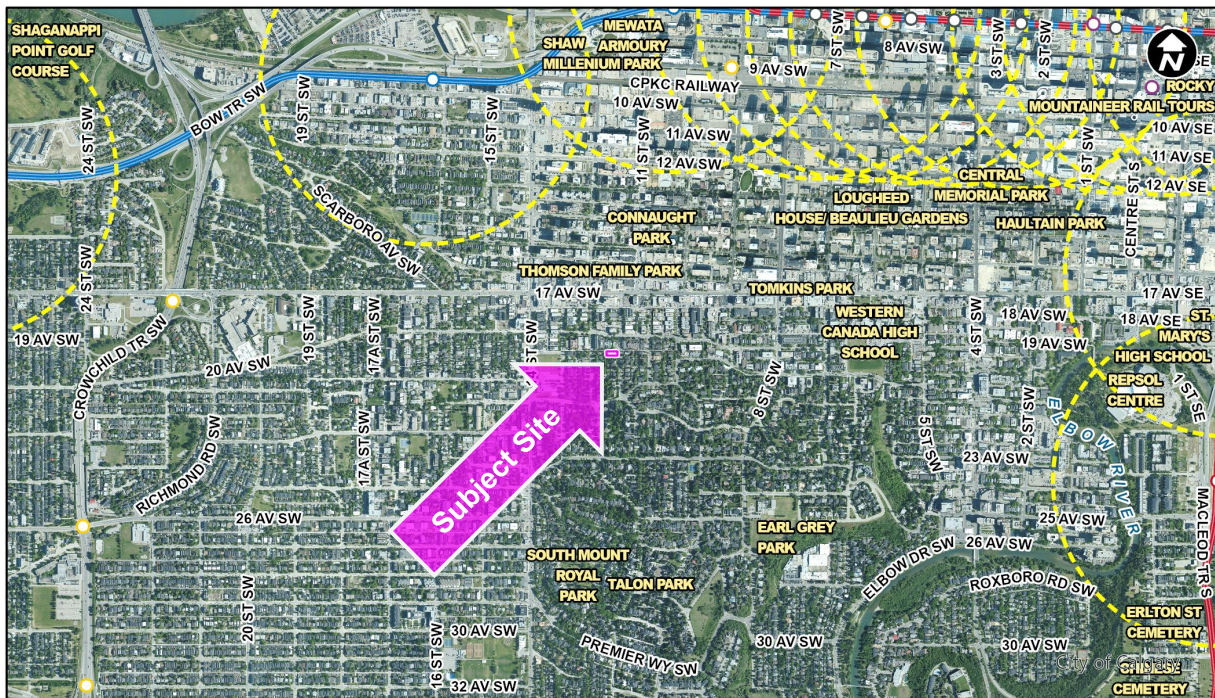
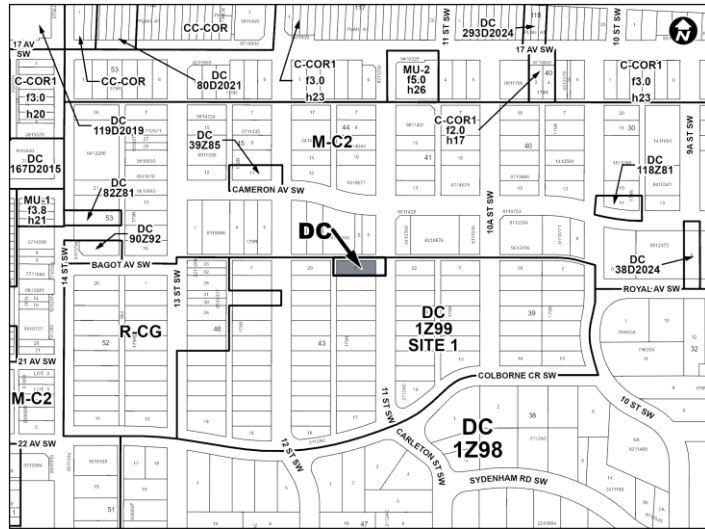
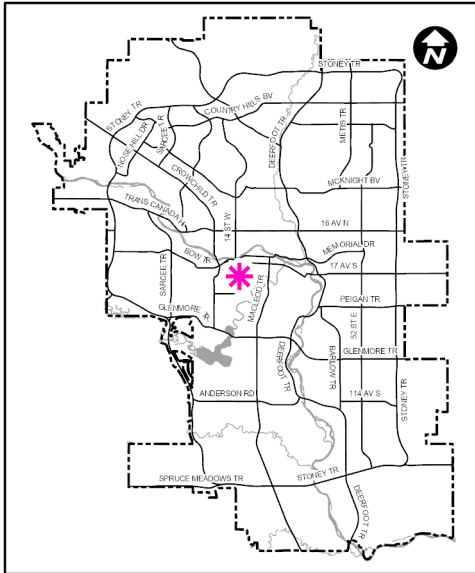
As identified below, the community of Upper Mount Royal reached its peak population in 1969.

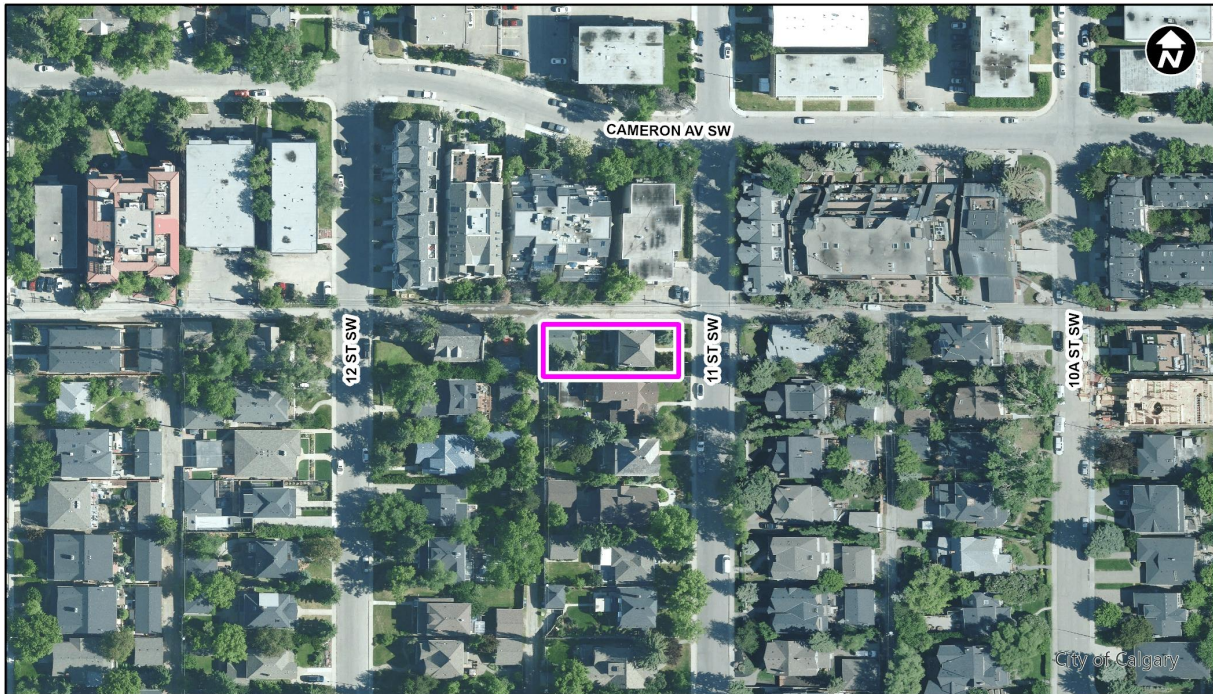
Upper Mount Royal	
Peak Population Year	1969
Peak Population	3,147
2019 Current Population	2,478
Difference in Population (Number)	- 669
Difference in Population (Percent)	-21.3%

Source: *The City of Calgary 20219 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Upper Mount Royal Community Profile](#).

Location Maps





Previous Council Direction

None.

Planning Evaluation

Land Use

The site is currently designated as a DC District ([Bylaw 1Z99 Site 2](#)), which is based on the R-2 Residential Low Density District of the previous 2P80 Land Use Bylaw. This DC District allows for duplex dwellings, semi-detached dwellings and single-detached dwellings, with a minimum six metre front yard setback and a minimum 15 metre parcel width. A maximum building height of 10 metres and a minimum area of 600 square metres is also stipulated under the existing DC District.

The proposed DC District is based on the Residential – Grade-Oriented Infill (R-CG) District of the current 1P2007 Land Use Bylaw. It moves residential building forms, such as Duplex Dwelling, Semi-detached Dwelling and Single Detached Dwelling from discretionary uses to permitted uses. It also includes a maximum parcel width of 7.5 metres and increase the minimum front building setback to 7.5 metres. This increase in setback is a result of a block plan analysis performed by the applicant to determine what the average front setback is along 11 Street SW.

Pursuant to Section 20 of the Land Use Bylaw 1P2007 (LUB), DC Districts must only be used for the purpose of providing for developments that due to their unique characteristics, innovative ideas or unusual site constraints, require specific regulation unavailable in other land use districts. Furthermore, DC Districts must not be used in substitution of any other land use district in the LUB that could be used to achieve the same result either with or without relaxations.

As the applicant's intent is to create a district with a maximum parcel width and increased minimum front setback along with an increased list of permitted uses, the proposed DC District does align with Section 20 of the LUB in that there are no low density residential stock districts which accommodate the proposal. However, Administration is not supportive of the use of the DC District as it is not needed to accommodate the intended development. A standard low density district, such as the R-CG District the DC District is based on, already allows for the subdivision of the site, and a single detached dwelling may be considered as a discretionary use on each future parcel.

The proposed DC District includes a rule that allows the Development Authority to relax Section 6 of the DC District Bylaw. Section 6 incorporates the rules of the base district in Bylaw 1P2007 where the DC District does not provide for specific regulation. In a standard district, many of these rules can be relaxed if they meet the test for relaxation of Bylaw 1P2007. The intent of this DC District rule is to ensure that rules of Bylaw 1P2007 that regulate aspects of development that are not specifically regulated in this DC District can also be relaxed in the same way that they would be in a standard district. The DC District also allows sections 8 and 9, the building setback areas and setback from the front property line to be relaxed as well. This is to ensure that changes to the average front setback can be accommodated over time as homes may be added or removed.

Development and Site Design

The rules of the proposed DC District would provide guidance for the future development of the site including appropriate uses, building height and massing, landscaping, parcel coverage and parking. Details regarding siting and massing, parking, site access, landscaping and waste and recycling management are being evaluated through the development permits.

Transportation

The site is about 200 metres (a three-minute walk) from 17 Avenue SW, identified as part of the Primary Transit Network in the MDP. Route 2 (Mount Pleasant/Killarney) has stops along 17 Avenue SW. Routes 7 (Marda Loop) and 22 (Richmond Rd SW) are 400 metres away (a six-minute walk) on 14 Street SW offering further transit connections.

Vehicular access is currently from the rear lane and there is on-street parking along 11 Street SW. Pedestrian access to the site is available from the sidewalks on 11 Street SW.

The nearest on-street bikeway is north of the subject site on 11 Street SW, starting at 16 Avenue SW which is 350 metres (a five-minute walk) away.

Environmental Site Considerations

No environmental concerns were identified.

Utilities and Servicing

Water, sanitary and storm services are available to the site. Details of site servicing, as well as appropriate stormwater management would be considered as part of the development permit review process.

Legislation and Policy

South Saskatchewan Regional Plan (2014)

Administration's recommendation aligns with the policy direction of the [South Saskatchewan Regional Plan](#), which directs population growth in the region to cities and towns, and promotes the efficient use of land.

Municipal Development Plan (Statutory – 2009)

The site is within the Developed Residential – Inner City area as identified on Map 1: Urban Structure of the [Municipal Development Plan](#) (MDP). These areas are comprised of residential communities that were primarily subdivided and developed prior to the 1950s.

The applicable MDP policies encourage redevelopment and modest intensification of inner-city communities to make more efficient use of existing infrastructure, public amenities and transit and deliver small and incremental benefits to climate resilience. MDP policies encourage the provision of greater housing choices, covering a mix of built forms and ownership tenures, in locations close to job markets and in areas well served by the Primary Transit Network, and within or near existing residential communities.

Calgary Climate Strategy (2022)

This application does not include any specific actions that address the objectives of the [Calgary Climate Strategy – Pathways to 2050](#). Further opportunities to align development of this site with applicable climate strategies are being explored and encouraged through the review of the development permits.

West Elbow Communities Local Area Plan (Statutory – 2025)

The site falls within the Neighbourhood Local urban form area within the [West Elbow Communities Local Area Plan](#) (LAP), and under the Limited (up to 3 Storeys) building scale. These areas should be primarily residential and consider local built form context. The LAP encourages a wide variety of building forms. There are no policies regarding the parcel width. The area also falls within the Heritage Guideline Areas and is subject to these policies, which state that land use redesignations that allow for permitted use dwelling units should not be supported in Heritage Guideline Areas.

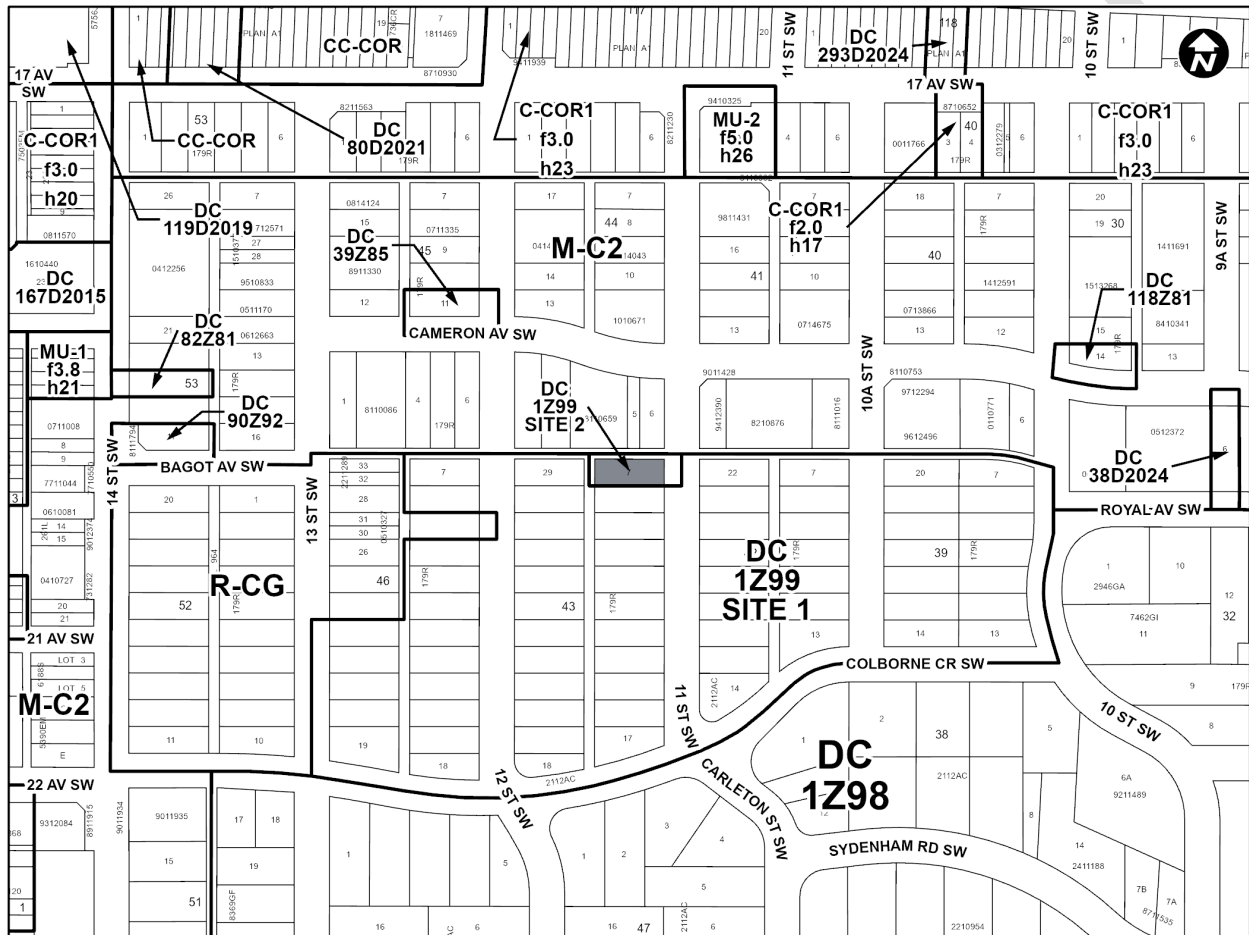
Section 4.2(9) of the LAP also provides specific direction in cases where there is conflict between the policy direction of the LAP and a restrictive covenant affecting this site, noting that: "Some parcels in the Plan Area may have registrations on the certificate of title, called restrictive covenants, which may restrict development. These restrictions may include, but are not limited to, restricting development to one or two-unit dwellings. Where the restrictive covenant is not in alignment with the goals and objectives of this Plan, The City of Calgary supports the direction of this Plan."

The policies in the LAP support a range of housing types and intensification in existing neighbourhoods. However, the proposed DC District is not in alignment with the LAP as the DC District adds permitted uses, which would not allow the Heritage Guideline Area Policies to be implemented.

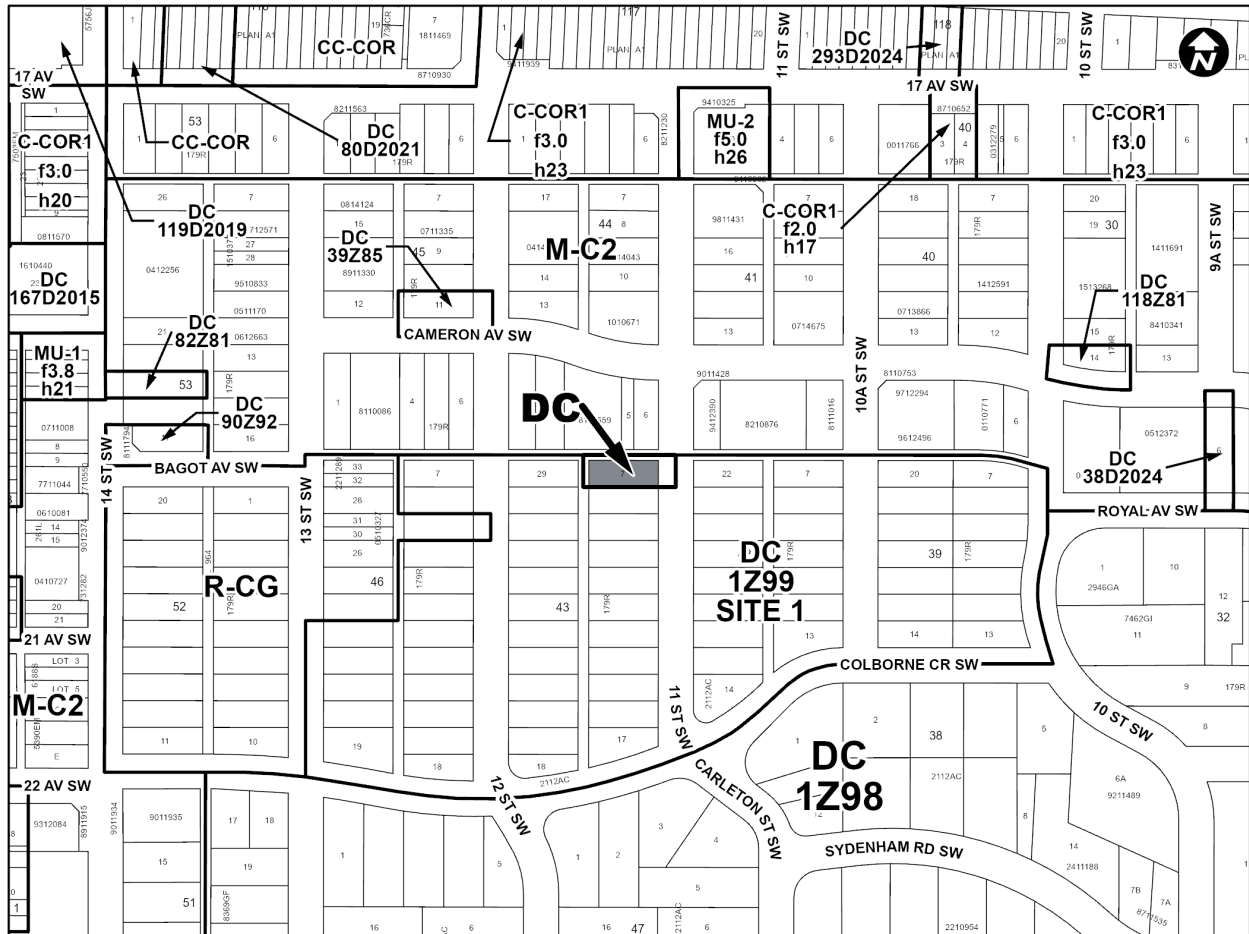
The application was also circulated to the Heritage Planning Team, who noted that no permitted use dwellings should be allowed, and that front setbacks should be informed by the existing heritage assets on the block.

Proposed Direct Control District

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and replacing it with that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".



SCHEDULE B



DIRECT CONTROL DISTRICT

Purpose

- 1 This Direct Control District Bylaw is intended to establish:
 - (a) a maximum parcel width; and
 - (b) a minimum building setback to align with adjacent development.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District Bylaw.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District Bylaw, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

4 The **permitted uses** of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District with the addition of the following **uses**:

- (a) **Duplex Dwelling;**
- (b) **Semi-detached Dwelling;** and
- (c) **Single Detached Dwelling.**

Discretionary Uses

5 The **discretionary uses** of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District with the exclusion of the following **uses**:

- (a) **Duplex Dwelling;**
- (b) **Semi-detached Dwelling;** and
- (c) **Single Detached Dwelling.**

Bylaw 1P2007 District Rules

6 Unless otherwise specified, the rules of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 apply in this Direct Control District.

Maximum Parcel Width

7 The maximum **parcel width** is 7.5 metres.

Building Setback Areas

8 The minimum depth of all **setback areas** must be equal to the minimum **building setback** required in Section 9 of this Direct Control District Bylaw and Sections 535, 538, 539 and 540 of Bylaw 1P2007.

Building Setback from Front Property Line

9 The minimum **building setback** from a **front property line** is 7.5 metres.

Relaxations

10 The **Development Authority** may relax the rules contained in Sections 6, 8 and 9 of this Direct Control District Bylaw in accordance with Sections 31 and 36 of Bylaw 1P2007.

Applicant Submission

Company Name (if applicable):

Horizon Land Surveys

LOC Number (office use only):

Applicant's Name:

Lei Wang

Date:

July 13th, 2025

On behalf of the landowner, please accept this application to redesignate a +/-0.060 hectare site from Direct Control (1Z99 Site 2) to Direct Control based on R-CG to allow for:

- a maximum building height of 11 metres
- flexible parcel dimensions and building setbacks that allow a diversity of grade-oriented housing
- add minimum density of 40 units per hectare

The subject site, 1911 11 Street SW, is located in the community of Upper Mount Royal along 11 Street SW. The lot is currently developed with a single detached dwelling built in 1915. Immediately to the north of the sites are multi-residential lots in Lower Mount Royal. The lot is surrounded in other directions by single detached dwellings.

The site is approximately 0.060 hectares in size. Lane exists to the north and west of the site. Vehicle access to the parcel is available and will be via the rear lane. The site is within 200 meters of both 14 Street and 17 Ave main street where many commercial, recreational and social facilities are located.

The existing house is built in 1915. Despite many renovations and improvements over the years, the existing house is close to the end of its life. The current owner plans to demo the existing house and build two units on site. However caveat 3286U originally imposed by Canadian Pacific Railway limit the use of the land to one single only. In addition, the caveat also requires the new house to be at a distance of 25 feet from street or avenue in front and rear of the said lot. Thus the proposal is to replace the current direct control bylaw with new direct control based on R-CG by setting up minimum density and also apply R-CG setback rule.

The lot is categorized as "neighbourhood local" under West Elbow Communities Local Area Plan with a building scale of up to 3 storeys. The LAP identifies six goals and

objectives including "housing for all" which encourage provision of non-market housing and mixed market housing that meets the diverse and changing needs, life stages, and financial abilities of individuals and also provide and enable inclusive, diverse and equitable housing options across the plan area. 4.2 (p) of the LAP states that where the restrictive covenant is not in alignment with the goals and objectives of the plan, the City of Calgary supports the direction of the plan.

The subject parcel is located within the Residential-Developed-Inner City area of the Municipal Development Plan. The applicable policies encourage redevelopment of inner-city communities that is similar in scale and built form to existing development, including a mix of housing such as townhouses and row housing. The Municipal Development Plan also calls for a modest intensification of the inner city, an area serviced by existing infrastructure, public amenities and transit. The proposal is in keeping with the relevant policies of the MDP as the rules of the R-CG provide for development forms that may be sensitive to existing residential development in terms of height, built form and density.

Housing diversity and choice policies encourage the provision of a wide range of housing types, tenures and densities that may meet affordability, accessibility, life cycle and lifestyle needs of different groups; an adaptation of the City's existing housing stock to enable changing households to remain in the same neighbourhood for longer time periods through allowing accessory suites in low density areas. So we sincerely hope city can support our application.

Applicant Outreach Summary



Community Outreach on Planning & Development Applicant-led Outreach Summary

Please complete this form and include with your application submission.

Project name: 1911 11 Street SW

Did you conduct community outreach on your application? YES or NO

If no, please provide your rationale for why you did not conduct outreach.

Outreach Strategy

Provide an overview of your outreach strategy, summary of tactics and techniques you undertook (Include dates, locations, # of participants and any other relevant details)

Before undertaking the project, our office completed the "Community Outreach Assessment". The project's impact score is "1A". So we are implementing a direct approach to reach to community association, local residents within a 100 meters radius and also Ward Councilor Office.

On June 26th, 2025, our staff did post card deliver to residents within a 100 meters radius. During the process, our staff did door knocking and spoke with residents at home. The main concerns our office got are with regards to increased traffic, increased density, height, shadow effects, and safety. We believe those can be properly dealt with at the development permit stage.

We have spoke with previous councillor and current councillor with regards to the project. On Aug. 11th, 2025, we attended planning committee meetings held by the community association.

Stakeholders

Who did you connect with in your outreach program? List all stakeholder groups you connected with. (Please do not include individual names)

Immediate neighbour, local residents, community association and ward councillor office

calgary.ca/planningoutreach



Community Outreach for Planning & Development Applicant-led Outreach Summary

What did you hear?

Provide a summary of main issues and ideas that were raised by participants in your outreach.

additional dwelling units and non-compliance with existing Restrictive Covenant; community character and heritage guidelines, in particular the setback requirement and landscaping; and impact of additional density on traffic.
The use of direct control to deal with restrictive covenant

How did stakeholder input influence decisions?

Provide a summary of how the issues and ideas summarized above influenced project decisions. If they did not, provide a response for why.

A concurrent DP application has been submitted to clearly show the intention of two singles. The heritage setback rule and landscaping rule can be easily incorporated in DP.

A subdivision for two singles has also been submitted to provide further clarity

How did you close the loop with stakeholders?

Provide a summary of how you shared outreach outcomes and final project decisions with the stakeholders that participated in your outreach. (Please include any reports or supplementary materials as attachments)

Continue engage with local residents, Councillor office and community association.

calgary.ca/planningoutreach



#130 Business Centre NW
Calgary, AB, T3B 5W5

Phone 403-719-0272
Fax 403-775-4171

www.horizonsurveys.ca
info@horizonsurveys.ca

Dear Homeowners,

Horizon Land Surveys is applying land use amendment at 1911 11 Street SW from the current zoning Direct Control 1Z99 Site 2 to Direct Control based on R-CG to accommodate two singles.



Our client truly would like to build houses with attractive urban design. And we certainly welcome any feedbacks from local residents and listen to your concerns. So if you have any, please call or email us at bernard@horizonsurveys.ca. You can also check city's D-map below for proposed development on site: <https://dmap.calgary.ca/?p=DP2025-05364>.

Thank You.

Yours truly,

Horizon Land Surveys Team

Community Association Response



To: **Adam Kaddoura**
Adam.Kaddoura@calgary.ca
DP.Circ@calgary.ca
File Manager
City of Calgary Planning Department
Cc: PDC, Cllr. Courtney Walcott

Re: **LOC 2025-0151 (1911 – 11 Street SW)**

August 27, 2025

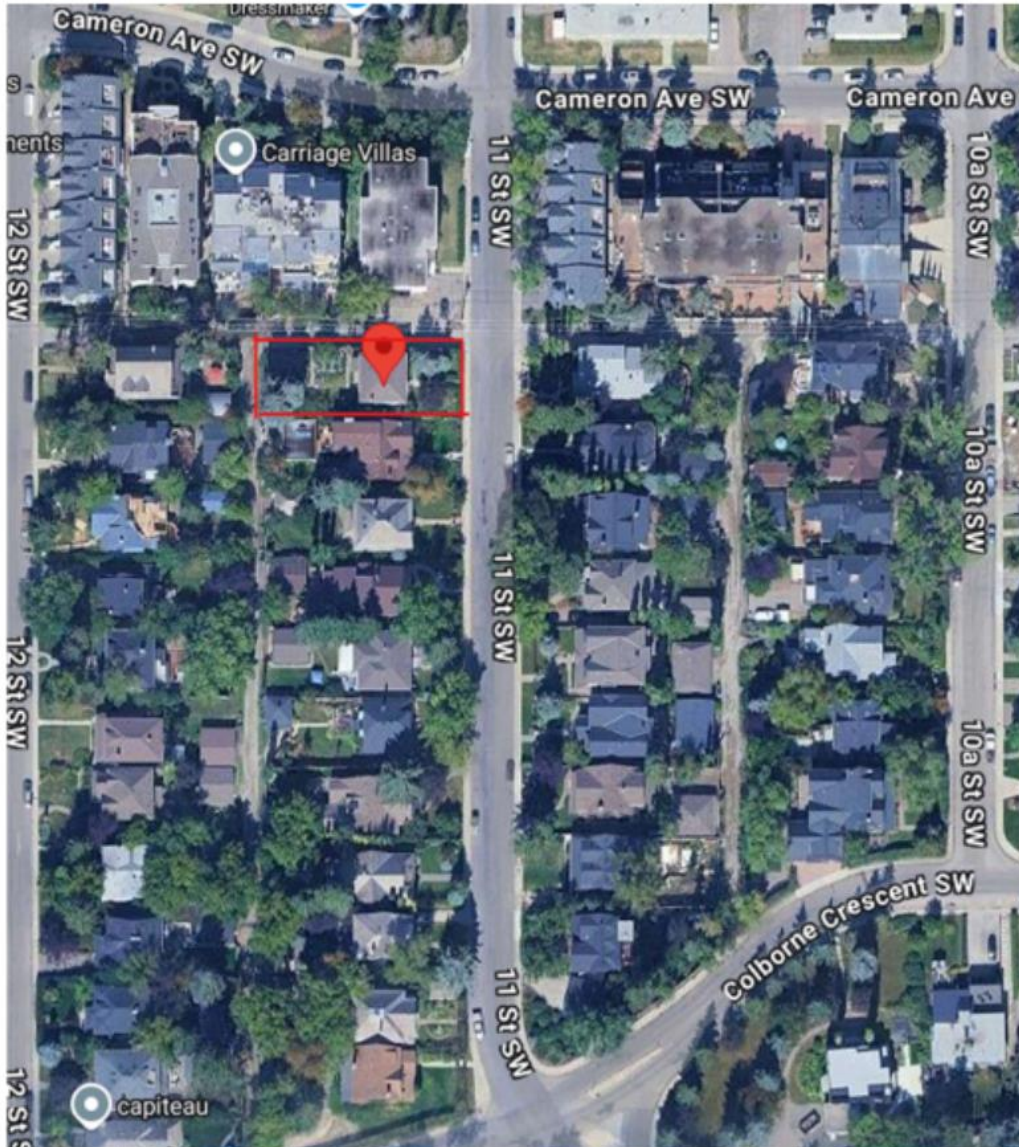
Adam,

I am writing on behalf of the Mount Royal Community Association Planning and Development Committee (PDC) regarding the above-mentioned land use redesignation application. The PDC held a very well attended community meeting on Tuesday, August 11, 2025. An agent for the Owner attended the meeting but he was not clear or forthcoming on what the intent of the proposed land use change was intended to achieve. It did "slip" that they wanted to get around the current restrictive covenant registered on the property.

Background – Existing Land Use

1. The existing land use designation for this property is a DC (Direct Control) District. Direct control districts were previously used by the City of Calgary to define parcels or groups of parcels that had distinct characteristics, including potential for increased density if in a transportation corridor, heritage preservation or unique lot configurations for example. With the blanket upzoning that came into effect recently, direct control districts were not affected or changed to the blanket (R-CG) land use designation and as a result, the direct control districts in Mount Royal were not affected.
2. The existing Direct Control District, Amendment No.98/123, Bylaw No.1299 was approved by council in January 1999. The purpose of the district **is to preserve the unique characteristics of a special area of the city**. It is very clear that previous councils agreed that this area of Calgary should be protected. It first adopted the Upper Mount Royal ARP designed to protect the special character of the neighbourhood. More recently, it adopted the West Elbow LAP which also included this area as a special heritage district with very clear guidelines and principles to guide any redevelopment.
3. For this site, referred to as Site 02, The DC District permitted uses include **modest single-detached dwellings, modest duplex dwellings, modest semi-detached dwellings and accessory buildings (garages)**. **Discretionary uses include duplex dwellings, semi-detached dwellings, single detached dwellings, and accessory dwelling units (within principal building)**. Note: the use of modest could now be considered as contextual as outlined in the land use bylaw.

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Background – Existing Restrictive Covenant

1. The existing parcel has a restrictive covenant on the property title. The Document is registered as 3286U. This is the 1907 restrictive covenant placed on the property title. The building covenant notes that only a single dwelling house shall be erected on the property and shall have a setback minimum of 25'-0" (7.62m) from the street or avenue and the rear. Image of a portion of the restrictive covenant below:

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of the First Part,

AND

THE CANADIAN PACIFIC RAILWAY COMPANY,

of the Second Part ;

Whereas the Part 7 of the First Part, by an Agreement bearing date the *Thirty first* day of *May*, A.D. 1907, purchased from the Parties of the Second Part Lots numbered *Three (3), Four (4) and Five (5)* in Block numbered *Eighty three* in the C.P.R. Addition to Calgary, according to a map or plan of said addition filed in the Land Titles Office at Calgary as No. *179 R.*

And Whereas the Part 7 of the First Part, for *himself and his* legal representatives and assigns, covenanted in the said Agreement with the Parties of the Second Part, their successors and assigns as follows:-

(a) That *he* would not erect or maintain on any of said lots any building for any purpose whatsoever, save for the purpose of a private residence and out-buildings incidental thereto.

(b) That *he* would not erect or cause or suffer or permit to be erected or placed upon the said land any dwelling-house that would cost less than Two Thousand Dollars (\$2000).

(c) That *he* would not erect or cause or suffer or permit to be erected or placed upon any of the aforesaid lots more than one such dwelling-house, and further, that any house or other building that should be erected or placed on the aforesaid lots should be at a distance of at least Twenty-Five Feet (25) from the street or avenue in front and rear of the said lots, and that *he* would also insert similar covenants to the above in all Agreements for Sale made by *him* for re-sale of the said land or any part thereof.

And Whereas the purchase money due on said lots has been paid in full, and the Part 7 of the First Part *is* entitled to a Transfer of said lots from said Parties of the Second Part and has agreed to hold the said lots subject to the conditions and limitations aforesaid after the title to the said lots has been vested in him.

Now therefore, this Agreement Witnesseth that, in consideration of the delivery by the Parties of the Second Part to the Part 7 of the First Part of a transfer of said lots free from said conditions and limitations, *he* the Part 7 of the First Part, for *himself and his* heirs, executors, administrators and assigns, do hereby covenant with the Parties of the Second Part, their successors and assigns, that *he* and they will hold the said lots subject to the said conditions and limitations, and will observe and perform the said covenants after Certificate of Title for said lots has issued to *him*, as if the same were expressed in said Certificate of Title.

The rights under this Agreement shall accrue as often as any of the said conditions are broken, and under no circumstances shall any condonation, waiver or release of any breach of any of the said conditions be pleaded in bar to any action to enforce any right under this Agreement, and this condition runs with the title to the said lots, and the Part 7 of the First Part executing this Agreement agrees on behalf of *himself and his* heirs, executors, administrators and assigns, to this condition.

In Witness Whereof the Part 7 of the First Part have hereunto set *his* hand and seal on the day and year first above written.

Proposed Land Use

- 1 The applicant is proposing a new direct control district to replace the existing direct control district based on the (R-CG) land use that was used for blanket upzoning. The applicant noted at the August 11, 2025, meeting that they only want to build a semi-detached development. The committee found this disingenuous as the RC-G opens the potential for townhouse and row house development. There is no guarantee that the current owner will keep the property or keep their word about their proposed intentions.
2. The link to the land use bylaw section which deals with (R-CG) is noted below:

<https://www.calgary.ca/planning/land-use/online-land-use-bylaw.html?part=5&div=11>

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The R-CG land is noted as a low-density residential district and is described as a grade-oriented infill district. Along with single and semi-detached developments, the district allows for a wide variety of low-density multi-family building types including cottage housing cluster, row housing, townhouses, and backyard suites (garage suites) as discretionary uses.

The recently approved West Elbow Local Area Plan designates this part of Mount Royal as a Heritage Guideline Area.

Excerpt from West Elbow Local Area Plan:

"2.6.2 Heritage Guideline Areas To recognize and celebrate the unique history and resulting built form that is seen in parts of the West Elbow Communities, Heritage Guideline Areas with concentrated groupings of residential heritage assets have been identified. These residential heritage assets are privately owned buildings, typically constructed prior to 1945, that significantly retain their original form, scale, massing, window/door pattern, and architectural details or materials. Concentrations of residential heritage assets that help define the Heritage Guideline Areas are identified on Map 5. These guidelines apply to block faces where more than 25% of the buildings have been identified as heritage assets. Buildings that qualify as residential heritage assets may be listed on the Inventory of Evaluated Historic Resources, but it is not a requirement."



September 1, 2025

Re: LOC2025-0151 (1911 11th St. SW)

Dear Mr. Kaddoura;

I am writing a follow-up note to our Mount Royal Community Association Planning and Development Committee (PDC) response of August 27 to the above noted proposed land use re-designation.

We had talked on the telephone in August and you then forwarded the draft DC that was written by the applicant, Horizon Surveys. I indicated I would review it and offer up my comments as one of "planner to planner." In addition, I would like to include these comments as an addendum to our earlier PDC comments submitted by my co-chair.

There have been a recent number of similar style re-designations that have gone to Council, including three at the July Public Hearing. Horizon has been party to a number of these applications. While others may have proceeded, I wanted to examine this one with fresh eyes, especially as it needs to match Section 29 of the LUB. In my planner mind, it does not meet those criteria and as a result, I have not attempted to edit or modify the present draft DC.

The LUB states that a DC "...must only be used... due to unique characteristics, innovative ideas or unusual site constraints...unavailable in other land use districts." There has been no identification of such limiting factors, and in fact discards the existing DC that did indeed recognize the unique characteristics of the area. It appears the intent of the draft DC is to skirt a restrictive covenant, which is another legal agreement between private parties. I would suggest that such logic does not withstand planning or legal principles.

The LUB also states that a DC "...must not be used: (a) in substitution of any other land use district in this Bylaw that could be used to achieve the same result...." The draft DC mimics the RCG District and therefore does not require the special DC approach. The City and Council decided to leave the present DC districts untouched during the blanket upzoning in established areas for an especially important reason; those districts did indeed capture the special characteristics of each of the carefully crafted zones.

The recently approved West Elbow LAP also recognized this area of Mount Royal as having special characteristics associated with its heritage nature and crafted policies that aligned with the previous ARP and indeed support the character of 11th St. This draft DC is at best a crass mechanism to skirt both the restrictive covenant and the guidelines of the LAP. The applicant should request a simple land use change to RCG, which of course, the MRCA would also oppose. However, such a process at least attempts to keep the integrity of the LUB in place.

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Lastly, while the applicant indicates it would only build two single detached infills or a semi-detached house, we need to remind ourselves that we are examining a land use change and that all uses allowed in that district could be ultimately applied for, regardless of what the applicant initially "promises".

In closing, the MRCA continues to oppose this application and has held meetings with residents (and exceptionally large numbers) who also unanimously oppose the erosion and eating away of the edge of our heritage district. The MRCA requests that Administration recommends refusal of this application.

Please feel free to call me at 403-607-8486 if you want to chat further.

Yours truly

Roy Wright RPP, MCIP

Co-chair PDC

CC; PDC

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403-437-0520 | info@mrca.ca | mrca.ca

Land Use Amendment in Banff Trail (Ward 7) at 2471 – 23 Street NW, LOC2025-0171

RECOMMENDATIONS:

That Calgary Planning Commission:

1. Forward this report (CPC2026-0250) to the 2026 May 12 Public Hearing Meeting of Council; and

That Calgary Planning Commission recommend that Council:

2. Give three readings to the proposed bylaw for the redesignation of 0.11 hectares \pm (0.27 acres \pm) located at 2471 – 23 Street NW (Plan 9110GI, Block 4, Lot 21) from Multi-Residential – High Density Low Rise (M-H1f3.6h20d280) to Direct Control (DC) District to accommodate multi-residential development, with guidelines (Attachment 2).

HIGHLIGHTS

- This application seeks to redesignate the subject parcel to a Direct Control (DC) District based on the Multi-Residential – High Density Medium Rise (M-H2) District to address unusual site constraints created by the irregular shape of the subject site.
- This proposal supports the transit-oriented development vision for the Banff Trail community and is in keeping with the relevant policies of the *Municipal Development Plan* (MDP) and the *South Shaganappi Communities Local Area Plan* (LAP).
- What does this mean to Calgarians? This proposal would allow for more housing options within the community and more efficient use of existing infrastructure, public amenities and transit.
- Why does this matter? By providing new housing options close to the existing Banff Trail Light Rail Transit (LRT) Station, it may increase population diversity near existing transit infrastructure while accommodating the evolving needs of different age groups, lifestyles and demographics.
- A development permit for a multi-residential development has been submitted and is under review.
- There is no previous Council direction related to this proposal.

DISCUSSION

This application, located in the northwest community of Banff Trail, was submitted on 2025 August 25 by Modern Office of Design + Architecture on behalf of the landowner, Banff Trail 2471 Development Corporation. This application proposes a redesignation to a DC District to enable the development of a multi-residential development, as indicated in the Applicant Submission (Attachment 3). A development permit (DP2025-05477) for a multi-residential development with 64 units was submitted on 2025 September 23 and is under review.

The approximately 0.11 hectare (0.27 acre) site is located at the corner of 24 Avenue NW and 23 Street NW and is currently developed with a single detached dwelling with a detached garage. The site is approximately 400 metres northeast of the Banff Trail LRT Station (a seven-

Land Use Amendment in Banff Trail (Ward 7) at 2471 - 23 Street NW, LOC2025-0171

minute walk) away. The site is also within the Core Zone of the Banff Trail LRT Station where these areas are envisioned to be developed with higher building scales.

A detailed planning evaluation of the application, including location maps and site context, is provided in the Background and Planning Evaluation (Attachment 1).

ENGAGEMENT AND COMMUNICATION

- Outreach was undertaken by the Applicant
- Public/interested parties were informed by Administration

Applicant-Led Outreach

As part of the review of the proposed land use amendment application, the applicant was encouraged to use the [Applicant Outreach Toolkit](#) to assess which level of outreach with the public/interested parties and respective community association was appropriate. In response, a consultant on behalf of the applicant conducted a virtual meeting with the Banff Trail Community Association (CA) and hosted an in-person open house at the community association on 2025 October 30. In addition, the applicant delivered 250 post cards to nearby properties advertising the open house and project. The Applicant Outreach Summary can be found in Attachment 4.

City-Led Outreach

In keeping with Administration's practice, this application was circulated to the public/interested parties, notice posted on site and published [online](#). Notification letters were also sent to adjacent landowners.

Administration received two letters of opposition from the public, noting areas of concern including increased parking and traffic, shadowing and impacts to utilities.

The CA provided a response stating it did not have any comments on the proposed land use (Attachment 5).

Administration considered the relevant planning issues specific to the application and has determined the proposal to be appropriate based on providing additional housing choice in an area designated as a Core Zone for the Banff Trail Station Area and applying site specific development rules to address the surrounding context. A multi-residential development will provide opportunities for more people to live in an established community supported by nearby amenities including the Banff Trail LRT and the University of Calgary. The future building and site design, including landscaping, parking, waste and recycling will be considered at the development permit stage.

Following Calgary Planning Commission, notification for a Public Hearing of Council will be posted on-site and mailed to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Land Use Amendment in Banff Trail (Ward 7) at 2471 - 23 Street NW, LOC2025-0171

IMPLICATIONS

Social

The proposed DC District would allow for higher density development to accommodate the housing needs of diverse demographics, income levels, and lifestyles. The development of these lands would promote more efficient use of land and infrastructure, support surrounding uses and amenities and introduce additional housing units near public transit.

Environmental

This land use amendment does not include any actions that specifically address the objectives of the *Calgary Climate Strategy – Pathways to 2050*. Opportunities to align the future development on this site with applicable climate strategies are being explored through the development permit application.

Economic

The proposal would contribute to Calgary’s housing supply and provide for greater housing options, while making more efficient use of existing infrastructure and services. It would also enhance transit ridership and support local business and employment opportunities within surrounding communities.

Service and Financial Implications

No anticipated financial impact.

RISK

There are no known risks associated with this proposal.

ATTACHMENTS

1. Background and Planning Evaluation
2. Proposed Direct Control District
3. Applicant Submission
4. Applicant Outreach Summary
5. Community Association Response

Department Circulation

General Manager (Name)	Department	Approve/Consult/Inform

Background and Planning Evaluation

Background and Site Context

The subject site is located in the northwest community of Banff Trail at the corner of 24 Avenue NW and 23 Street NW. The site is approximately 0.11 hectares (0.27 acres) in size and is approximately 38 metres wide by 30 metres deep. The parcel is currently developed with a single detached dwelling with a detached garage.

Surrounding development is generally characterized by a mix of low density residential, commercial, special purpose and direct control districts. Parcels surrounding the development are designated as Residential – Grade-Oriented Infill (R-CG) District. Directly east of the site is designated as a Direct Control (DC) District ([Bylaw 104D2021](#)) that allows mixed use and multi-residential development. Parcels located to the north and west of the site are designated as special purpose districts including Special Purpose – Community Institution (S-CI) District and Special Purpose – Recreation (S-R) District. While parcels to the south are designated as Commercial – Corridor 2 (C-COR2) and a variety of DC Districts surrounding the Banff Trail Transit Station Area.

The site is located near public transit, recreational and institutional opportunities. The parcel has a frontage along 23 Street NW which provides direct access to the Banff Trail Light Rail Transit (LRT) Station which is approximately 400 metres southwest (a seven-minute walk) away. The University of Calgary Campus and McMahon Stadium are approximately 650 metres (an 11-minute walk) north and west of the site.

Community Peak Population Table

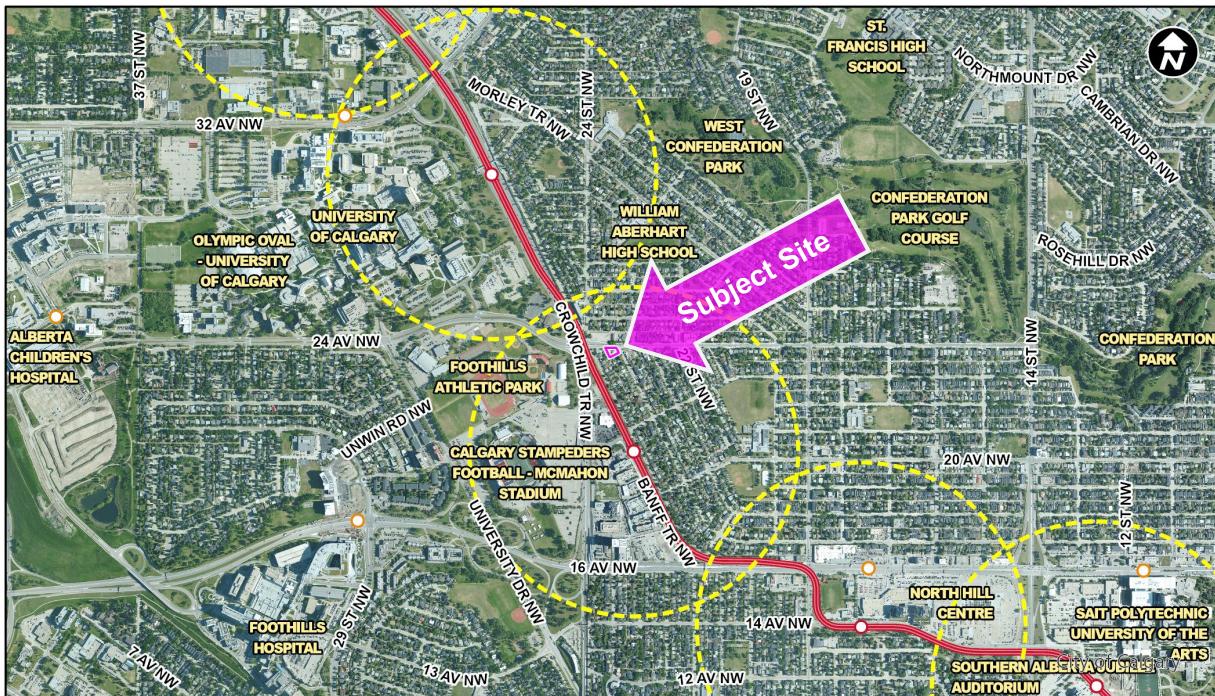
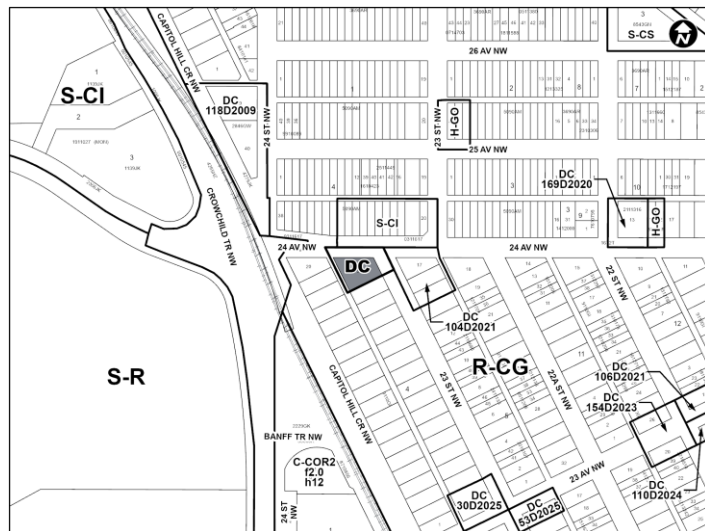
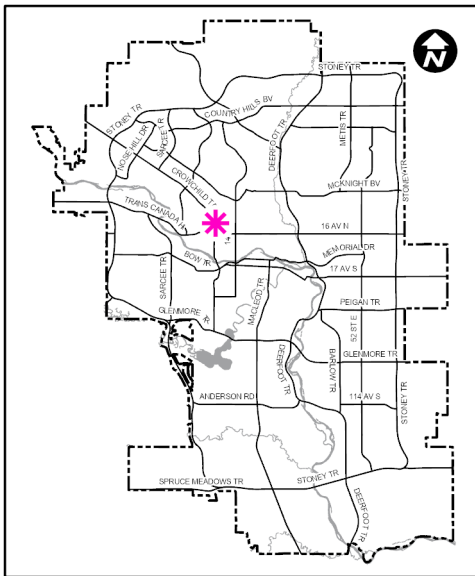
As identified below, the community of Banff Trail reached its peak population in 1968.

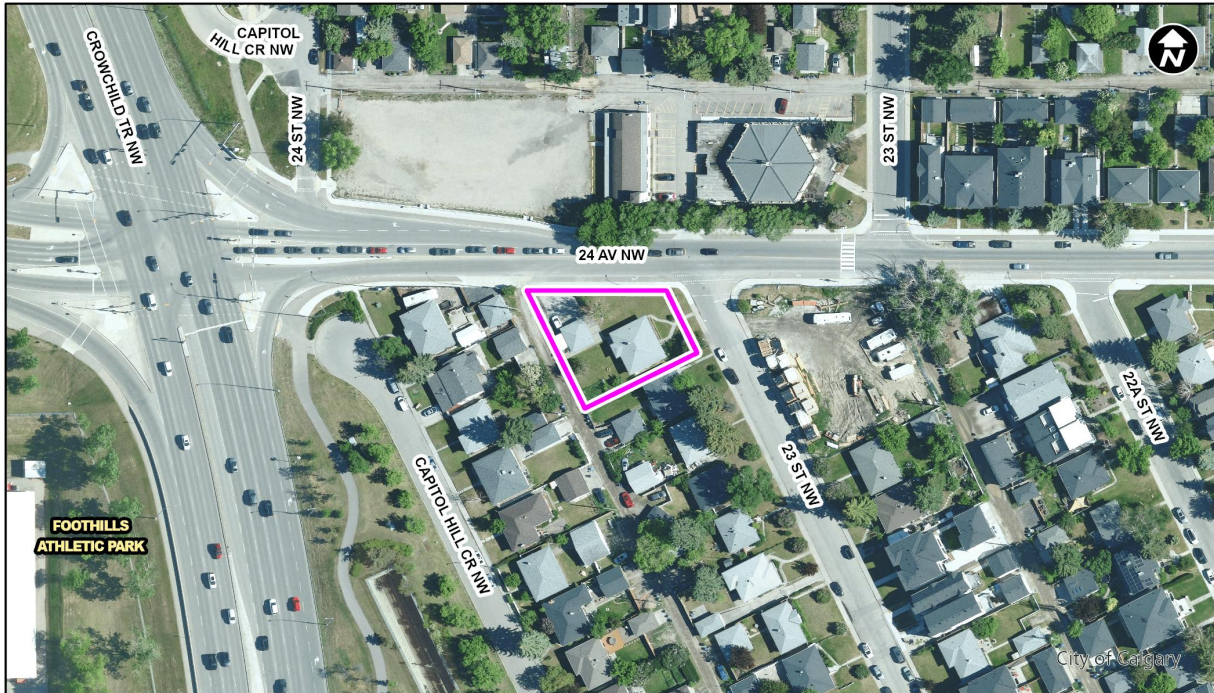
Banff Trail	
Peak Population Year	1968
Peak Population	4,883
2019 Current Population	4,153
Difference in Population (Number)	- 730
Difference in Population (Percent)	- 14.9%

Source: *The City of Calgary 2019 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Banff Trail Community Profile](#).

Location Maps





Previous Council Direction

None.

Planning Evaluation

Land Use

The existing Multi-Residential – High Density Low Rise (M-H1) District accommodates multi-residential development in variety of forms and is typically located near community nodes, transit and transportation corridors. The current M-H1 District has modifiers that allow a maximum floor area ratio (FAR) of 3.6, a maximum building height of 20 metres (approximately five to six storeys) and a maximum density of 280 units per hectare.

The proposed Direct Control (DC) District is based on the Multi-Residential – High Density Medium Rise (M-H2) District. The proposed DC District will allow for a maximum building height of 26.0 metres (approximately six to eight storeys) and a minimum density of 150 units per hectare, with no maximum density. The intent of the DC District is to establish specific rules for setbacks and building height and exclude specific uses on the site. The setback rules allow for no minimum building setback with a street or a lane and a minimum 3.0 metre building setback from a property line shared with another parcel. Additionally, the DC District excludes Secondary Suite, Backyard Suite, Duplex Dwelling, Semi-detached Dwelling and Single Detached Dwelling uses.

Pursuant to Section 20 of the Land Use Bylaw 1P2007, this application for a DC District has been reviewed by Administration and the use of a Direct Control District is necessary to provide for the applicant's proposed development due to unusual site constraints caused by the triangular shape of the parcel along with the reduced frontage along 23 Street NW. The use of a

DC District with allow for the applicant's intended built form, which could not be achieved through the use of a standard land use district in the Land Use Bylaw.

The proposed DC District includes a rule that allows the Development Authority to relax Section 6 of the DC District Bylaw. Section 6 incorporates the rules of the base district in Bylaw 1P2007 where the DC District does not provide for specific regulation. In a standard district, many of these rules can be relaxed if they meet the test for relaxation of Bylaw 1P2007. The intent of this DC District rule is to ensure that rules of Bylaw 1P2007 that regulate aspects of development that are not specifically regulated in this DC District can also be relaxed in the same way that they would be in a standard district. The DC District also includes a rule that allows the Development Authority to relax Sections 8, 9(2) and 9(3) of the DC District Bylaw. This is to allow for flexibility for building setbacks and chamfers at the development permit approval stage.

Development and Site Design

A development permit (DP2025-05477) for a multi-residential building is currently being reviewed by Administration. The review of the development permit will include a review of the proposed land use, maximum building height, building massing, landscaping and parking provisions to ensure alignment with applicable policies and design guidelines. Based on the site's location within a Transit Station Core Zone, additional considerations will be addressed through the development permit process including interface with adjacent developments, pedestrian connectivity and transit supportive site planning.

Transportation

Pedestrian access is available from the existing sidewalks on 23 Street NW and 24 Avenue NW. Vehicle access to the site is available from the rear lane. On-street parking is restricted along 23 Street northbound and southbound (2 hours, 7:00 a.m. – 11:00 p.m., Monday to Friday) and no on-street parking is allowed along 24 Avenue NE both directions adjacent to the proposed development.

Existing on-street bikeways, which are part of the current Always Available for All Ages and Abilities (5A) Network, are situated along 22 Street NW, 20 Avenue NW, 24 Avenue NW, Capitol Hill Crescent NW and an existing pathway along Crowchild Trail NW. These bikeways facilitate access to and from the site by supporting alternative modes of transportation.

The subject site is well served by public transit and has direct access to Banff Trail LRT Station, approximately 400 metres southwest of the site (a seven-minute walk). The subject site is approximately 700 metres south (a 12-minute walk) from Route 65 (Market Mall/Downtown West) located along Morley Trail NW eastbound at 24 Street NW. In addition, it is approximately 900 metres east (a 15-minute walk) from a bus stop located along University Drive NW northbound at 24 Avenue NW with access to Route 9 (Dalhousie Station/Chinook Station), Route 19 (16 Avenue North), Route 20 (Heritage Station/Northmount Dr N), Route 90 (Bridgeland/University of Calgary) and Route 104 (Sunnyside/University of Calgary).

Vehicular access to the proposed development is anticipated to be from the rear lane. A parking study was required as part of this application.

Environmental Site Considerations

No environmental concerns were identified.

Utilities and Servicing

Water and sanitary sewer mains are available to service the subject site. Details of the site servicing, as well as appropriate stormwater management will be considered and reviewed as part of any development permit application.

Legislation and Policy

South Saskatchewan Regional Plan (2014)

Administration's recommendation aligns with the policy direction of the [South Saskatchewan Regional Plan](#), which directs population growth in the region to cities and towns, and promotes the efficient use of land.

Municipal Development Plan (Statutory – 2009)

The subject site is located within the Developed – Residential Inner City area as identified on Map 1: Urban Structure in the [Municipal Development Plan](#) (MDP). The proposal contributes to achieving applicable MDP policies that encourage redevelopment and modest intensification of inner-city communities to make more efficient use of existing infrastructure, public amenities and transit, while delivering modest and incremental benefits to climate resilience.

Calgary Climate Strategy (2022)

This application does not include any specific actions that address objectives of the [Calgary Climate Strategy – Pathways to 2050](#). Further opportunities to align development of this site with applicable climate strategies are being explored and encouraged through the review of the development permit.

South Shaganappi Communities Local Area Plan (Statutory – 2025)

The [South Shaganappi Communities Local Area Plan](#) (LAP) designates this site as part of the Neighbourhood Connector urban form category, as referenced in (Map 3: Urban Form). Neighbourhood Connector areas are predominantly residential, with provisions for a variety of work-live units and home-based businesses. Additionally, the site falls within the Mid building scale designation, which permits development of up to 12 storeys, as outlined in Map 4: Building Scale.

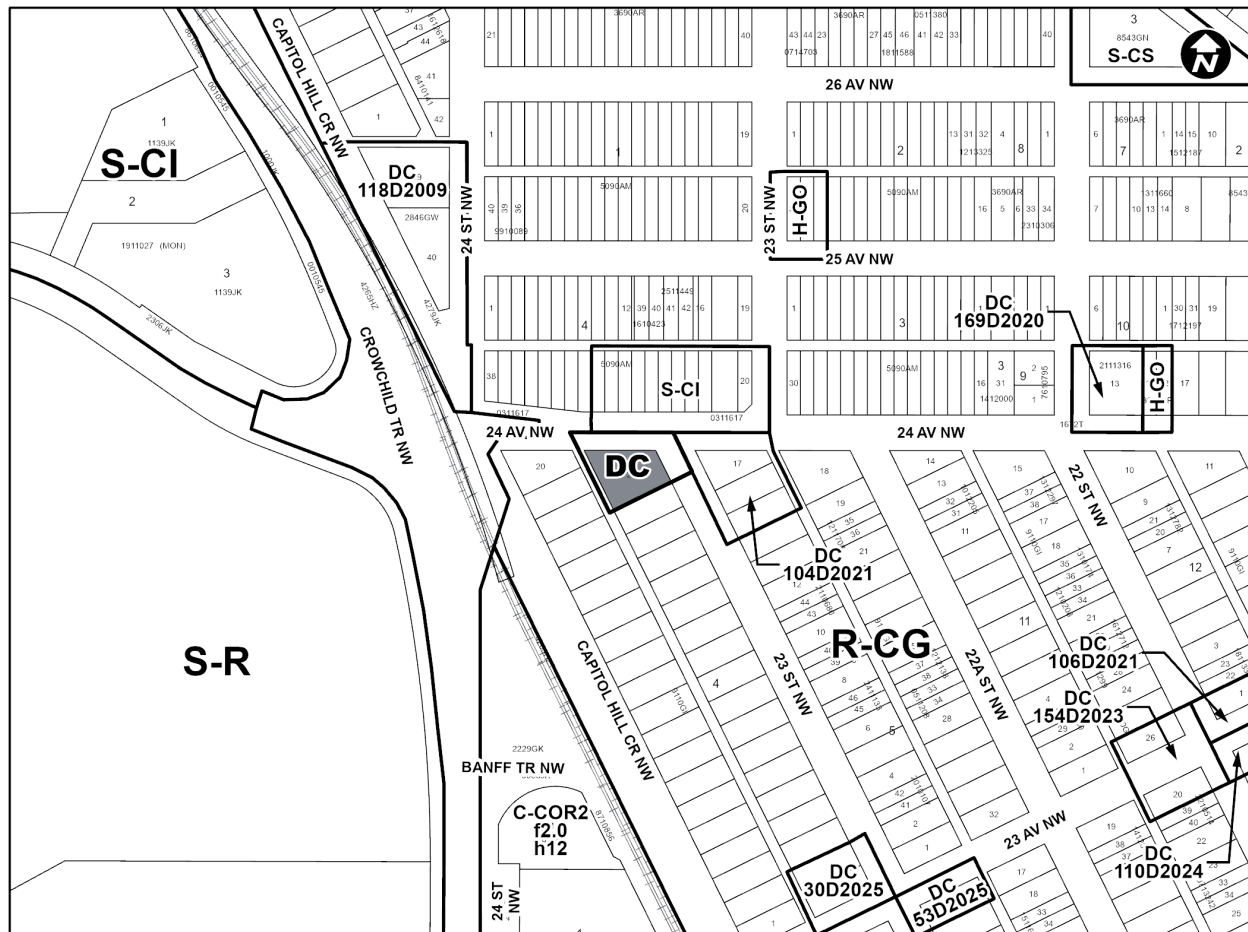
The subject site is within the Banff Trail Core Zone (Figure 9: Banff Trail Station Area). The Banff Trail LRT Station connects the University of Calgary, Foothills Athletic Park, and the Banff Trail community. These areas are envisioned to accommodate mixed-use development that supports transit infrastructure and promotes a walkable, well-connected neighbourhood. As such, the proposed land use amendment is in alignment with the applicable policies of the LAP.

Proposed Direct Control District

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and replacing it with that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".

SCHEDULE A



SCHEDULE B**DIRECT CONTROL DISTRICT****Purpose**

- 1 This Direct Control District Bylaw is intended to:
- (a) establish specific rules for setbacks and building height; and
 - (b) exclude specific uses on the site.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District Bylaw.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District Bylaw, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The **permitted uses** of the Multi-Residential – High Density Medium Rise (M-H2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District with the exclusion of:
- (a) **Secondary Suite.**

Discretionary Uses

- 5 The **discretionary uses** of the Multi-Residential – High Density Medium Rise (M-H2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District with the exclusion of:
- (a) **Backyard Suite;**
 - (b) **Duplex Dwelling;**
 - (c) **Semi-detached Dwelling;** and
 - (d) **Single Detached Dwelling.**

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Multi-Residential – High Density Medium Rise (M-H2) District of Bylaw 1P2007 apply in this Direct Control District.

Setback Area

- 7 The depth of all **setback areas** must be equal to the minimum **building setback** required in Section 8 of this Direct Control District Bylaw.

Building Setbacks

- 8 (1) The minimum **building setback** from a **property line** shared with a **street** or a **lane** is zero metres.
- (2) The minimum **building setback** from a **property line** shared with another **parcel** is 3.0 metres.

Building Height

- 9 (1) Unless otherwise referenced in subsections (2) and (3), the maximum **building height** is 26.0 metres.
- (2) Where a **parcel** shares a **property line** with a **lane**, the maximum **building height** is reduced to 14.0 metres from **grade** within 3.0 metres of that shared **property line**.
- (3) Where a **parcel** shares a **property line** with another **parcel**, the maximum **building height** is reduced to 11.0 metres from **grade** within 5.0 metres of that shared **property line**.

Relaxations

- 10 The **Development Authority** may relax the rules contained in Section 6, 7, 8, 9(2) and 9(3) of this Direct Control District Bylaw in accordance with Sections 31 and 36 of Bylaw 1P2007.

Applicant Submission

Company Name (if applicable):

Modern Office of Design + Architecture

LOC Number (office use only):

Applicant's Name:

Brogan Gordon-Cooper

Date:

March 3, 2026

Our hopes are to allow redevelopment at 2471 23 Street NW for a six (6) storey multifamily building, complete with a parkade level below grade. This parcel is located in the community of Banff Trail and located within a block of Crowchild Trail NW. The NB Banff Trail (C-Train) station is about 500m away if accessed via city paths along Capitol Hill Crescent NW, but only 400m if accessed from 23 Street NW. The South Shaganappi Local Area Plan (LAP), Map 4, shows a "Mid" building scale on this parcel, which allows up to 12 stories. We are looking to keep out development as "Low" building scale and will keep it as a 6-storey building. Map 3 shows the parcel as a "Neighbourhood Connector" and a residential building would align with that. The building will consider the local built form context by adhering to all height and chamfer height bylaw rules as we are directly adjacent to low density residential development. The building will be oriented towards the street and as it is at the north end of the block, the shadowing effects will be minimal. The parkade will be accessed off the lane. The main entrance will also be located off a street. The current zoning on this parcel is Multi-Residential - High Density Low Rise (M-H1) District with a permitted maximum floor area ratio (FAR) of 3.6, a height maximum of 20m and a density modifier of 280 units/ ha. The height unfortunately is too low to allow for a six-storey development.

The parcel across the street at 2464-2468 23 Street NW was successfully changed to a Direct Control 104d2021 zoning based on a mixed use - general (MU-1) district with a permitted FAR of 3.9, a height maximum of 23 m from grade and a minimum density modifier of 150 unit/ha. In light of the success of the zoning, we are also pursuing a DC zoning but instead based on M-H2 as it allows for the higher FAR and density modifier while aligning with the intent of the LAP.

INTENT STATEMENT:

The proposed DC based on M-H2 zoning allows for a higher FAR of 5.0 and a minimum density of 150 units/ha. We are requesting a height modifier to a maximum of 26m along with modifications to the building setbacks and height chamfers to better support the 6-storey mass. This DC will also exclude specific uses that conflict with the intent of the LAP.

Applicant Outreach Summary

2026 March 02



Outreach Summary

O2 implemented a comprehensive engagement strategy to share details of the proposed land use amendment application, receive feedback from interested stakeholders and outline potential revisions to the proposed application. Community Outreach has remained a continuous effort throughout the land use application process to ensure information was updated and project updates were shared.

The following engagement tactics were implemented:

- Postcard Mailers
- Notice Posting
- Meeting with the Community Association
- Open House event with adjacent neighbours and the broader community

Postcard Mailers

250 postcard mailers were hand delivered to directly adjacent and nearby properties in advance of the in-person open house to notify neighbours of the upcoming engagement and provide them with the email address of the project team to answer any required questions.

Public Notice Posting

A public notice sign was posted on the subject site in compliance with the City's notice posting standards. The posting included contact information for the project team, enabling the public to provide direct feedback on the proposed redesignation to the applicant.

Coordination with Community Associations

Project information was shared with the Banff Trail Community Association (BTCA) via a summary memo and contact details. A digital meeting was held on September 16, 2025, to present the proposed land use change and architectural concept.

Open House with the wider community

An in-person open house was held on October 30, 2025, from 5–7 pm at the Banff Trail Community Association. The session provided an overview of the proposed development and invited community feedback. Members of the project team were present to answer questions. A total of 10 residents attended.

What We Heard

BTCA – What We Heard

- What sizes of units will be available in the building?
- Why was this land use district chosen?
- Why is the designation going for six storeys and not twelve?
- What are the potential traffic impacts and parking?
- Will bicycle parking be provided?
- How long will it take before building starts?
- That commercial use on the ground floor would benefit the community.

Public Open House – What We Heard

- General acknowledgement that 24th Avenue NW and the surrounding community are experiencing change through increased density and a diversity of built forms.
- Recognition that the site is well suited for additional density due to its proximity to the LRT, community amenities, and educational institutions.
- Recognition that the site is well suited for additional density due to its size and being a corner block.
- Concerns that the proposed height could create privacy impacts for properties to the south and west.
- Concerns about potential increases in traffic and parking congestion resulting from the development.
- Concerns about the materials and aesthetic of building finishes being 'cheap'
- Questions on how many dwelling units
- Concerns of tree canopy loss



How the Application Responds to Comments

Appropriateness of the Proposed District:

In response to questions regarding the suitability of the Multi-Residential – High Density Low Rise (M-H2) District, the project team confirms that this designation is appropriate and strongly supported by City policy. The M-H2 District is intended to accommodate multi-residential development and includes a limited range of supporting commercial uses that can serve local residents. This aligns with the South Shaganappi Communities Local Area Plan (LAP), which identifies this section of 24 Avenue NW as a Neighbourhood Connector and part of a Community Corridor—areas that encourage a diversity of housing types and opportunities for small-scale, locally serving uses. While commercial uses are not currently present along this street, the LAP is a forward-looking policy that anticipates gradual evolution over time. The M-H2 designation provides flexibility for future adaptation while primarily supporting residential development, making it a well-suited land use for the site.

Height and Massing:

Public feedback has raised concerns regarding the proposed six-storey height and overall massing. In the project team's professional opinion, the site is well-suited for intensification due to its location: it is a corner lot with lane access, approximately 350 metres from the Banff Trail LRT station, and within walking distance of schools, the University of Calgary, recreation facilities, and employment areas.

The Local Area Plan (LAP) designates the site within a 'Mid' building scale modifier area, which supports buildings up to twelve storeys. The proposed six-storey height is well below this maximum and was carefully chosen to ensure contextual appropriateness with the surrounding built form. Similar mid-rise developments in West Hillhurst, Banff Trail, and Killarney have demonstrated how thoughtfully designed six-storey buildings can provide additional housing choice while fitting seamlessly within established neighbourhoods. The LAP also includes strong urban design policies to ensure that future development is compatible with surrounding buildings. These policies will guide the Development Permit stage to ensure appropriate transitions and respect for the character of the neighbourhood.

Traffic and Parking:

The site is well connected to existing infrastructure to support future residents. It is adjacent to Crowchild Trail SW, providing strong road connectivity to the broader network. The BRT runs along 16th Ave NW with direct service to Downtown, Mount Royal University, Marda Loop, and Westhills. The site is also 350 m from the Banff Trail LRT station, offering enhanced transit options. Off-street cycling routes connect the site to the citywide network, including the Confederation Park trail network. Given these attributes, it is our professional opinion that the transit-oriented nature of the proposed development will help reduce reliance on personal vehicles by providing residents with multiple high-quality mobility options.

As a corner lot with rear lane access, the site allows for safe vehicle entry and efficient integration of underground parking. Any future development must meet vehicle and bicycle parking requirements under the MH-2 district and comply with the City's Mobility Engineering standards. Existing on-street parking along 23rd Street NW will also provide supplementary capacity.

As part of the circulation review, mobility engineers confirmed that a Transportation Impact Assessment (TIA) or related study is not required at the land use stage. Should more detailed analysis be needed based on future density or access design, it will be addressed at the Development Permit stage when more detailed plans are available.

Building Materials:

Design and materials are generally addressed through the development permit process, which ensures that new buildings meet relevant urban design guidelines and standards. At this stage of planning, the focus of the engagement is on land use and overall development objectives, rather than specific architectural details. The project architect has provided renderings that illustrate material concepts and demonstrate how the building responds to the surrounding context, which will inform discussions and decisions during the development permit review. At the Development Permit stage, the project team will continue to refine the building's design.

Concern about the impact on existing trees:

There are currently two trees on site which will need to be removed to accommodate a new six-storey building. However, the proposed landscape plan introduces planting five new trees. Future development will be required to comply with all landscaping requirements outlined in the Land Use Bylaw, including tree planting standards.

Community Association Response

2025 October 01

The BTCA has reviewed and does not have any comments on this proposed land use.

**Land Use Amendment in Saddle Ridge (Ward 5) at #1000, 4715 – 88 Avenue NE,
 LOC2025-0147**

RECOMMENDATION:

That Calgary Planning Commission recommend that Council:

Give three readings to the proposed bylaw for the redesignation of 0.31 hectares ± (0.77 acres ±) located at #1000, 4715 – 88 Avenue NE (Condominium Plan 2312116, Units 3 to 38) from Commercial – Neighbourhood 2 (C-N2) District to Multi-Residential – High Density Low Rise (M-H1) District.

HIGHLIGHTS

- This application seeks to redesignate the subject site to enable six storey multi-residential development.
- The proposal allows for an appropriate building form and set of uses within the site and is in keeping with the applicable policies of the *Municipal Development Plan (MDP)* and *Saddle Ridge Area Structure Plan (ASP)*.
- What does this mean to Calgarians? This application would provide greater housing options in a developing area while enabling more efficient use of existing infrastructure and nearby amenities.
- Why does this matter? The proposal would allow for more housing options that may better accommodate the evolving needs of different age groups, lifestyles and demographics.
- No development permit has been submitted at this time.
- There is no previous Council direction regarding this proposal.

DISCUSSION

This land use amendment application in the northeast community of Saddle Ridge was submitted by Dino Kasparis on behalf of the landowners, 2239959 Alberta Ltd, 2348852 Alberta Ltd. (Ramandeep Arora) and Inner Space Developments Ltd. on 2025 July 14. The subject site forms part of a larger bareland condominium site located at the northeast corner of Guru Nanak Gate NE and 87 Avenue NE. The subject site comprises the southwestern portion of the condominium site and is approximately 0.31 hectares in area. The site is currently vacant and undeveloped. The ASP also identifies the subject site to be conceptually located within a Neighbourhood Activity Centre (NAC).

Parcels located directly adjacent to the subject site have all been designated for multi-residential and commercial development of varying sizes and intensities. The northern portions of the condominium site are designated with Commercial – Community 1 (C-C1) District and Commercial – Neighbourhood 1 (C-N1) District and are currently developed with two-storey commercial buildings. To the east, a portion of the site is designated Multi-Residential – Medium Profile Support Commercial (M-X2) District and is currently being used for parking.

No development permit has been submitted at this time; however, as noted in the Applicant Submission (Attachment 2), their intent is to develop a six storey multi-residential development that will be comprised of approximately 96 residential units.

**Land Use Amendment in Saddle Ridge (Ward 5) at #1000, 4715 - 88 Avenue NE,
LOC2025-0147**

A detailed planning evaluation of the application, including location maps and site context, is provided in the Background and Planning Evaluation (Attachment 1).

ENGAGEMENT AND COMMUNICATION

- Outreach was undertaken by the Applicant
- Public/interested parties were informed by Administration

Applicant-Led Outreach

As part of the review of the proposed land use amendment application, the applicant was encouraged to use the [Applicant Outreach Toolkit](#) to assess which level of outreach with the public/interested parties and the respective community association was appropriate. In response, the applicant mailed notifications to adjacent businesses and all residential homes within a 250-metre radius of the subject site, reached out to the Ward 5 Councillor's Office and contacted the Saddle Ridge Community Association (CA). The applicant also provided a dedicated phone line and email for feedback on the project and undertook in-person engagement with the local community during the Vaisakhi festival at Prairie Winds Park. The Applicant Outreach Summary can be found in Attachment 3.

City-Led Outreach

In keeping with Administration's practices, this application was circulated to the public/interested parties, notice posted on-site and published [online](#). Notification letters were also sent to adjacent landowners.

Administration received 10 letters of opposition from the public that included concerns about traffic, density and parking. No comments were received from the CA. Administration contacted the CA to follow up and no response was received.

Administration considered the relevant planning issues specific to the application and has determined the proposal to be appropriate. The building and site design, number of units and on-site parking will be reviewed and determined at the development permit stage.

Following Calgary Planning Commission, notifications for a Public Hearing of Council will be posted on-site and mailed to adjacent landowners. In addition, Commission's recommendations and the date of the Public Hearing will be advertised.

**Land Use Amendment in Saddle Ridge (Ward 5) at #1000, 4715 - 88 Avenue NE,
LOC2025-0147**

IMPLICATIONS

Social

The proposal would facilitate higher density development in the form of low-rise multi-residential housing within a developing community. This land use district and resulting form of development is currently not generally found in the surrounding area, and would introduce an alternative housing option that supports a broader range of housing choices by accommodating different age groups, household sizes, lifestyles, and demographic needs. Locating this form of development within a NAC provides opportunities to support a more complete and diverse community, while contributing to the mixed-use character envisioned for this area.

Environmental

This application does not include any actions that specifically address the objectives of the *Calgary Climate Strategy – Pathways to 2050*. Further opportunities to align future development on this site with applicable climate strategies are being explored and encouraged within the development permit review.

Economic

The proposed land use district would allow for a more efficient use of land and infrastructure by housing residents within Calgary’s developing areas. Future development will provide more housing options in the community, while supporting local businesses in the area.

Service and Financial Implications

No anticipated financial impact.

RISK

There are no known risks associated with this proposal.

ATTACHMENTS

1. Background and Planning Evaluation
2. Applicant Submission
3. Applicant Outreach Summary

Department Circulation

General Manager (Name)	Department	Approve/Consult/Inform

Background and Planning Evaluation

Background and Site Context

The subject site is located in the northeast community of Saddle Ridge and consists of a portion of a bare land condominium parcel located south of 88 Avenue NE, which is approximately 0.31 hectares (0.77 acres) in size. The site is located at the northeast corner of the intersection of 87 Avenue NE and Guru Nanak Gate NE and is approximately 57 metres wide by 55 metres deep. The site is currently vacant and will have access from the condominium roads. The northern portion of the condominium parcel is developed with two existing two-storey commercial buildings.

This parcel was part of an area that received outline plan and subsequent land use approvals in 2019 February ([LOC2017-0042](#)). At the time of outline plan approval, this site was envisioned to form part of a neighbourhood gateway along Guru Nanak Gate NE. In 2021, a land use amendment application ([LOC2020-0097](#)) was approved by Council, which redesignated the subject portion of the parcel from Multi-Residential – Medium Profile Support Commercial (M-X2) District to Commercial – Neighbourhood 2 (C-N2) District to allow for small-scale commercial developments. The proposed Multi-Residential – High Density Low Rise (M-H1) District proposal will facilitate multi-residential development, which was originally envisioned under the previous M-X2 District. The applicant intends to accommodate a multi-residential development of approximately 96 dwelling units where a portion of the units will be comprised of non-market rental housing.

Surrounding development is characterized by multi-residential development to the west, small-scale commercial development to the north, institutional and park uses to the south, and low-density residential development to the east across Saddlepeace Crescent NE. The portion of the condominium parcel immediately to the east of the subject site is designated M-X2 District, which is intended for multi-residential development with support commercial uses and maximum building height of 16 metres, but is currently being used for parking.

The Gobind Sarvar School, a private high school, is located directly to the south of the subject site as well as a large open space amenity and connection to the greater regional pathway network. New low-density residential development is located to the north across 88 Avenue NE. An existing Always Available for All Ages and Abilities (5A) Network pathway is provided along 88 Avenue NE, extending between Métis Trail NE and Saddleland Drive NE. The Saddletowne LRT Station is approximately 2 kilometres (a 25-minute walk) southeast of the subject site.

Community Peak Population Table

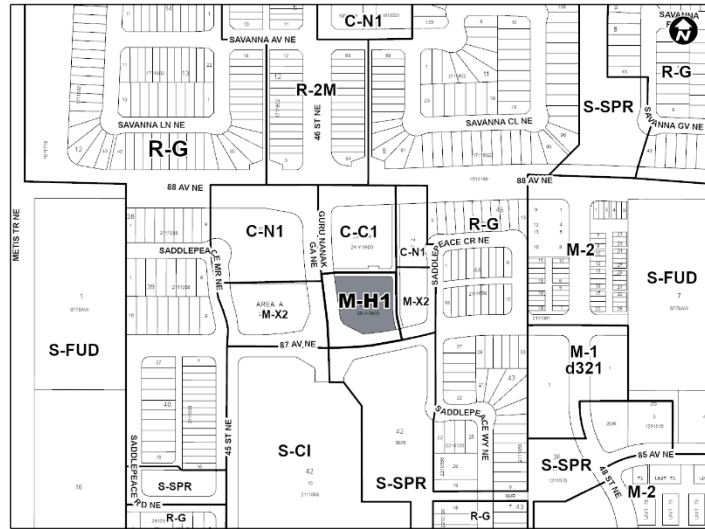
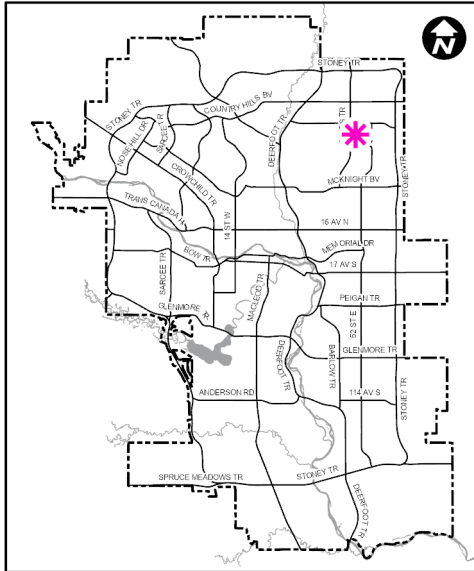
As identified below, the community of Saddle Ridge reached its peak population in 2019.

Saddle Ridge	
Peak Population Year	2019
Peak Population	22,321
2019 Current Population	22,321
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: *The City of Calgary 2019 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Saddle Ridge Community Profile](#).

Location Maps





Previous Council Direction

None.

Planning Evaluation

Land Use

The subject site forms part of a bare land condominium (Plan 2312116) with multiple land use district designations. The portion of the parcel subject to this land use amendment is currently designated Commercial – Neighbourhood 2 (C- N2) District. The existing C-N2 District is intended for small-scale commercial developments with limited auto-oriented uses with buildings that are in keeping with the scale of nearby residential areas. It allows for a maximum floor area ratio (FAR) of 1.0 and a maximum building height of 10 metres.

The proposed Multi-Residential – High Density Low Rise (M-H1) District allows for high-density multi-residential development and is intended to be located at community nodes, and transit and transportation corridors and nodes. There is no maximum density requirement but a minimum density of 150 units per hectare is required, which equates to up to 46 dwelling units for the subject site. It allows for a maximum building height of 26.0 metres and an FAR of 4.0 (approximately 12,400 square metres).

Administration supports the proposal as it will provide for development in a variety of forms in an appropriate location where future development can complement the surrounding context.

Development and Site Design

The applicable rules of the proposed M-H1 District will provide guidance for the future development of this site including appropriate uses, building height and massing, landscaping and parking. Given the specific context of this corner site forming the neighbourhood gateway along Guru Nanak Gate NE, and with adjacency to institutional and park uses to the south,

additional items that will be considered through the development permit process include, but are not limited to:

- providing engaging facades and appropriate interfaces with outward facing streets (87 Avenue NE and Guru Nanak Gate NE), as well as within the site;
- ensuring strong, safe and efficient pedestrian connectivity within the site for residents and for those utilizing the retail services of the site;
- ensuring the future development integrates with the surrounding context in terms of built form; and
- providing a high-quality landscaped environment and mitigating any potential conflict between pedestrian and vehicular users of the site.

Transportation

Pedestrian access to the site is available from the existing sidewalk on Guru Nanak Gate NE and 87 Avenue NE. The Always Available for All Ages and Abilities (5A) Network provides year-round walking, wheeling and cycling options to Calgarians. The site is served by the 5A Network, with existing pathways located along 88 Avenue NE and along Métis Trail NE between Airport Trail NE and 48 Avenue NE.

The area is currently served by Route 100 (Airport) and by Route 59 (Savanna), with bus stops located at 88 Avenue NE, approximately 120 metres (a two-minute walk) north of the subject site and at 46 Street NE, approximately 190 metres (a three-minute walk) north of the subject site. Route 100 takes riders to destinations including Calgary Airport (YYC) and the Saddletowne LRT Station. Route 59 provides a clockwise loop through the residential area, also providing connectivity to the LRT Station.

Details regarding vehicular access will be determined at the development permit stage with primary site access from the condominium roads and/or vehicular access easements. No independent access shall be permitted to and from 88 Avenue NE and Saddlepeace Crescent NE.

A Trip Generation Memo comparing trips anticipated from the proposed land use proposal (M-H1 District) and the trips contemplated under the existing land use was submitted with the application. The memo was reviewed and accepted by Administration. A Transportation Impact Assessment (TIA) was not required for this application.

Environmental Site Considerations

No environmental concerns were identified.

Utilities and Servicing

Sanitary, storm, and water servicing infrastructure is available to service the development area. The proposed amendments in this application did not affect the provision of previously approved servicing infrastructure. Details of the site servicing, as well as appropriate stormwater management will be considered and reviewed as part of any development permit application.

Legislation and Policy

South Saskatchewan Regional Plan (2014)

Administration's recommendation aligns with the policy direction of the [South Saskatchewan Regional Plan](#), which directs population growth in the region to cities and towns and promotes the efficient use of land.

Calgary International Airport Vicinity Protection Area (2009)

A portion of the subject site is located within the 0-25 Noise Exposure Forecast (NEF) contours of the [Airport Vicinity Protection Area](#) (AVPA) land use regulations. Multi-residential uses are allowable within this area. Future development permits will be circulated to NAV Canada and reviewed against the applicable regulations to ensure alignment and compliance.

Municipal Development Plan (Statutory – 2009)

The subject site is located within the “Developing Residential - Planned Greenfield with Area Structure Plan” area as identified on Map 1: Urban Structure of the [Municipal Development Plan](#) (MDP). General policies in the MDP for residential areas in the Planned Greenfield area include support for densification by adding additional population on underdeveloped parcels in the community. The MDP specifies that the *Saddle Ridge Area Structure Plan* (ASP), which was in existence prior to the adoption of the MDP, is recognized as appropriate policy to provide specific direction for development of the local community.

The proposal introduces higher density and supports a compact urban form by allowing for a variety of dwelling types within this developing community. The application aligns with the applicable MDP policies.

Calgary Climate Strategy (2022)

This application does not include any specific actions that address the objectives of the [Calgary Climate Strategy – Pathways to 2050](#). Further opportunities to align future development of this site with applicable climate strategies will be explored and encouraged at subsequent development approval stages, including accommodating solar energy systems and electric system designs that can accommodate electric vehicle charging.

Saddle Ridge Area Structure Plan (Statutory – 2012)

The subject parcels are located within the area covered by the [Saddle Ridge Area Structure Plan](#) (ASP) which identifies the subject lands as part of the Cell D residential area. The ASP encourages low to medium density residential development including a mix of housing types in this area. There are specific policies for the design of the Cell D residential area, including policies on building form, design and site servicing that will be implemented at a future development permit stage.

The ASP also identifies the subject site to be located within a Neighbourhood Activity Centre (NAC), which is conceptually located in the Cell D area in this general location. The NAC is intended to accommodate a range of multi-residential and mixed-use development, including townhouses and apartments. Recent development on the condominium parcel and in the vicinity has introduced a mix of non-residential uses, reinforcing the ASP’s intent to establish a mixed-use and functional neighbourhood activity centre for Cell D.

In summary, this application aligns with applicable ASP policies as it provides for the addition of more multi-residential uses to meet the changing needs of the community and contributes to the site’s location as a NAC.

Applicant Submission

2025 October 24



APPLICANT SUBMISSION FORM
LAND USE AMENDMENT
4715 - 88 AV NE (SW)

Executive Summary

The subject site is a 0.31-hectare land parcel situated at the southwest portion of #1000 - 4715 88 Avenue NE in the residential community of Saddle Ridge. The landowners and development proponents of the subject parcel are seeking to develop a multi-unit residential building that will be comprised of approximately 96 dwelling units and a portion of units will be comprised of non-market rental housing.

The proponents would like the City's support to amend the land use designation to M-H1 District to accommodate a multi-residential built form with an option to incorporate supporting commercial uses.

This development will be financed jointly between private investment and the Canadian Mortgage Housing Corporation (CMHC) and aligns with Council's Housing strategy in terms of addressing the crisis for affordable housing within the city.

Background & Context

The community of Saddle Ridge is characterized by a mixture of single and semi-detached homes and multi-residential built forms. Within a 400-metre radius of the subject site, the predominant land use in the area is a mixture of low density, multi-residential, commercial and institutional designations. The parcels located directly adjacent to the subject site have all been designated for commercial development of varying sizes and intensities. The Gobind Savar School, a private high school is located directly to the south subject site as well as a large open space amenity and connection to the greater regional pathway network.

The subject site along with the parcels located directly adjacent to the subject site forms the largest Neighbourhood Activity Centre (NAC) in Saddle Ridge.

Overall, the subject site is in an excellent location for medium density residential development as it is located within a NAC and is near major roadways such as 88 Avenue NE, 52 Street NE, and Metis Trail NE. Its proximity to public transit, community amenities, commercial uses and open space make it a viable candidate for an MH-1 designation.

Development Vision

The future development is envisioned to be one multi-unit residential building (with flexibility for ground floor commercial uses) a shared entryway, to contain a total of 96 dwelling units, 5 to 6 storeys in height. The development will contain both underground and surface parking facilities that will consist of 1 vehicle parking stall assigned for each dwelling unit.

Policy Review

Municipal Development Plan – (Statutory – 2009)

The subject parcel is located within the Developing Residential - Planned Greenfield with Area Structure Plan (ASP) area as identified on Map 1: Urban Structure in the Municipal Development Plan (MDP). Policies for the Planned

AUTHOR: DINO KASPARIS, OLYMPIA PLANNING



**APPLICANT SUBMISSION FORM
LAND USE AMENDMENT
4715 - 88 AV NE (SW)**

Greenfield Area recognize the ASP as appropriate to provide direction for development of the local community. The density targets proposed are in alignment with the MDP.

The applicable MDP policies encourage development and intensification in this community to occur in a form and nature that provides a variety of housing types and encourages concentration of residential density in areas adjacent to open spaces, parks, wetlands, and sports fields, especially where the area is served by transit services and community amenities. Regardless of land use typology, new development shall incorporate appropriate densities, a mix of land uses and a pedestrian-friendly environment to support an enhanced Base or Primary Transit Network.

In terms of housing Section 2.3.1: Housing encourages a wide variety and choice of housing forms, tenures, and affordability throughout our city to accommodate the needs of current and future Calgarians. The proposed M-H1 land use would allow for the provision of both new market and non-market housing opportunities in a developing community. In terms of location, affordable housing projects should be located near parks, schools, recreation facilities, and commercial nodes. The proposal aligns with the applicable policy directives of the MDP. The proposed M-H1 District intends to provide Multi-Residential development in Developing Areas. It provides for medium density development in a variety of forms and could have a density modifier applied to the district for development to be contextually sensitive to its surroundings.

The proposed development is in close vicinity to an array of small and medium scale commercial uses, open space amenities and other residential forms. Base public transit service is located adjacent to the site along 88 Avenue NE and provides a direct connection to the larger and primary transit network, the international airport and major commercial uses located nearby in the Saddle Ridge area.

Saddle Ridge Area Structure Plan (Statutory – 1984)

The subject site is located within the Saddle Ridge Area Structure Plan (ASP). The site is located specifically within Cell D of the ASP. The Saddle Ridge ASP speaks to residential development in this area being designed as an integrated neighbourhood that contains both low to medium density residential development including a mix of housing types. The ASP also provides specific policies for design of the Cell D neighbourhood, including policies on building form, design and site servicing that will be implemented through the development permit stage.

The ASP also identifies the site as part of a Neighbourhood Activity Centre (NAC). The NAC should contain a variety of multi-residential housing which includes higher density development, transit stops, local commercial uses, public open space, and mixed-use development with retail at grade. The NAC shall be comprised of a mixture of land uses that reach a minimum intensity of 100 people and jobs per gross developable hectare. The proposal is consistent with the applicable policies of the ASP as it represents additional medium scale multi-unit residential development. The future development shall compliment the surrounding development in the area as it exceeds the minimum intensity requirements established and there is already a strong mix of land use types to fulfill the policy intent of the ASP.

Proposed Applicant-Led Stakeholder Outreach

The project team is committed to being good neighbours and engaging with the communities we build in and serve. The proposed outreach strategies considered for this project relied on the guidance of the Applicant Outreach Toolkit made available by the City of Calgary. The project team approach to applicant led stakeholder engagement is designed

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**APPLICANT SUBMISSION FORM
LAND USE AMENDMENT
4715 - 88 AV NE (SW)**

to provide opportunities for stakeholders to learn more about the development vision for the site early on and to share their thoughts with the intent of maintaining a respectful, inclusive, and transparent dialogue.

Some Proposed Outreach Strategies:

- A dedicated project phone line, voice mail and email
- Postcard mailers to be circulated within a 200-metre radius of the parcel
- On-site notice signage
- In-person and virtual meetings

Engineering Considerations

Pedestrian access to the subject site is available from Guru Nanak Gate and 87 Avenue NE. Primary vehicular access will remain along 87 Avenue NE. The area is served by Calgary Transit bus service. Base public transit service is provided along 88 Avenue NE, as Route #100 has a bus stop located less than 200 metres from the property, providing a direct connection to Saddletowne LRT station and the greater primary transit network.

The subject site has exceptional access to arterial roads such as 88 Avenue NE. The nearest major skeletal roadway to the parcel is Metis Trail NE which is located less than 400 metres to the west of the subject site.

Water, sanitary and stormwater mains are available to service the site. The specific servicing arrangements and stormwater management shall be reviewed and evaluated in detail by the project team as part of the future development permit application.

Conclusion

Based on the subject site's corner location, parcel area shape and supporting policy direction, an amendment to propose an MH-1 District to accommodate a new multi-residential development will provide greater housing choices for current and future residents of Saddle Ridge. For the reasons outlined above, we respectfully request that DART, Planning Commission and Council support this land use application. Thank you for your time and consideration.



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Applicant Outreach Summary

2025 October 24



WHAT WE HEARD REPORT: OUR APPLICANT OUTREACH SUMMARY – LOC2025-0147

Summary

Our best practices for outreach was designed to inform, share information on the development vision and provide communication channels for interested stakeholders to share their queries and thoughts early in the review process with the intent of maintaining a respectful and transparent dialogue.

In support of the Land Use Amendment for this project, Olympia has undertaken a proactive applicant-led outreach program that was uniquely designed by the Community Assessment Outreach Tool provided by Engage Calgary. A variety of outreach strategies were implemented since August 2025 to commence a dialogue with interested parties and groups outlined in the below section, Our Community Outreach Strategy + Process.

Our Community Outreach Strategy + Process



Mailing Campaign

Mailers were sent to 190 homes within 200 metres of the project site providing notification of the project and an invitation to engage the project team.



Notice Letter

A notice letter was sent to the Saddle Ridge Residents Community Association and Ward Councilor's Office providing a project overview, reasons for the application of Land Use Amendment and to welcome any questions and continued dialogue.



Phone, Voicemail & Email

A phone line equipped with voicemail, email address and a QR code was shared with stakeholders to provide a direct communication channel to reach the project team.



Distanced + Face-to-Face Meetings

The project team held virtually meetings with Ward Councilor and interested stakeholders to share project information, project updates and continue conversations.

Comment Themes + Our Responses

Stakeholder Feedback + What We Heard

To date, through our Inbound engagement strategy, the project team has received a total of 10 responses from community members providing feedback in the form of phone calls, emails and text messages.

The project team notified the Ward 5 Office and the Saddleridge Residents Community Association (SRCA) to share an overview of the Land Use Amendment. The Ward 5 Office began meeting with the project team in May 2025 and provided feedback correspondence.

The project team is pleased to keep the communication lines open throughout the entire review process and will update the City Administration, Planning Commission and Council on any subsequent feedback that is received.

Project feedback received thus far has been categorized into 3 comment themes. Each common theme highlighted begins with an outline of what the project team has heard and subsequently provides the project team's response.

Comment Themes

- **Density**
- **Parking**
- **Traffic Congestion**

Density

What We Heard

Some respondents were concerned that, if approved, the land use amendment would introduce more density to the Saddle Ridge community that is perceived to be already "crowded" with a lot of existing development. These respondents felt that this would bring negative social impacts to the community and consume scarce community resources.

Our Response

The project team acknowledges the perspective of residents relating to intensification in the immediate area. The City of Calgary's Municipal Development Plan (MDP) and Saddle Ridge Area Structure Plan (ASP) encourages more housing options and intensification provided it is done modestly to be contextually sensitive to the established context. The Saddle Ridge ASP indicates the site is located within the Cell D NAC policy area that is envisioned to contain higher densities and act as a destination for the community at large. The number of dwelling units on the site would be controlled by the Calgary International Airport Zoning Regulation, parcel area size, surrounding context and mandatory landscape plan requirements set out the land use bylaw.

Engineering studies were submitted for City review to determine if upgrades to existing service connections are necessary to accommodate future development.

Further discussion regarding building and site design will be essential to refine the concept for these future developments to ensure density is balanced with community needs.

Parking

What We Heard

Some respondents were concerned about the parking supply and potential risk of parking overflow onto surrounding areas.

Our Response

In response to this concern, future redevelopment on these sites would need to supply the minimum number of motor vehicle parking stalls, bicycle and visitor stalls entirely within the boundaries of the private land prescribed in Calgary's land use bylaw. The developer is committed to supply a minimum 1 stall per dwelling across two levels of underground parkade and surface parking facility located within the subject condo plan area.

The final number of parking stalls will be determined based on the final assessed dwelling unit count and detailed site plan and building design proposed on the development permit submission. The project team also acknowledges parking requirements may change dramatically over time and may impact the future design of the development based on the future Calgary zoning code.

Traffic Impact

What We Heard

Some respondents shared concern over potential negative impacts from increased residential traffic upon the existing street network in the immediate area due to a new multi-unit residential development.

Our Response

The project team acknowledges the concerns related to potential traffic impact resulting from any approved development proposal. A traffic generation review memorandum has been prepared by Bunt & Associates and submitted to the City for review. The City determined the current network has sufficient capacity to support the future development. No additional studies are required to amend the results of The *Guru Nanak TIA* (2017) and no further improvements to the current network are required to facilitate the development. The developer is committed to improving the current transportation network, if it was deemed necessary.

Our Philosophy to Applicant-Led Outreach

Since no single design solution can satisfy all stakeholders involved, the project team cannot integrate everything suggested by all neighbours. As a result, our promise is to continue to be open and transparent about how we reach our conclusions while cultivating a constructive dialogue and we are pleased to abide by Council's visions for the areas and communities we serve.

Outline Plan, Policy and Land Use Amendment in Stonegate Landing (Ward 5) at multiple addresses, LOC2024-0171

RECOMMENDATIONS:

That Calgary Planning Commission:

1. Forward this report (CPC2026-0020) to the 2026 May 12 Public Hearing Meeting of Council;
2. As the Council-designated Approving Authority, approve the proposed outline plan located at 12331, 12414, 13030, 13440, 13440R, 13601 and 13621 – 36 Street NE; 1300, 1450, 1495, 1500 and 1570 Barlow Crescent NE; 3699 and 4440 Country Hills Boulevard NE; 12939 Métis Trail NE and 4200 – 128 Avenue NE (Portion of NE1/4 Section 28-25-29-4; Portion of NW1/4 Section 27-25-29-4; Portion of W1/2 Section 34-25-29-4; Portion of SE1/4 Section 33-25-29-4; Plan 9212612 Lot B; Plan 1310593 Block 5 Lot 5; Plan 1611320 Block 5 Lot 6; Plan 0913802 Block 4 Lot 2; Plan 0913802 Block 5 Lot 2; Plan 1811898 Block 5 Lots 8 and 9; Plan 0614349 Block A; Portion SW1/4 Section 27-25-29-4; Plan 1711309 Block 5 Lot 7) to subdivide 194.81 hectares ± (481.38 acres ±) with conditions (Attachment 2).

That the Calgary Planning Commission recommend that Council:

3. Give three readings to the proposed bylaw for the amendments to the Municipal Development Plan (Attachment 8);
4. Give three readings to the proposed bylaw for the amendments to the Northeast Industrial Area Structure Plan (Attachment 7); and
5. Give three readings to the proposed bylaw for the redesignation of 194.81 hectares ± (481.38 acres ±) located at 12331, 12414, 13030, 13440, 13440R, 13601 and 13621 – 36 Street NE; 1300, 1450, 1495, 1500 and 1570 Barlow Crescent NE; 3699 and 4440 Country Hills Boulevard NE; 12939 Métis Trail NE and 4200 – 128 Avenue NE (Portion of NE1/4 Section 28-25-29-4; Portion of NW1/4 Section 27-25-29-4; Portion of W1/2 Section 34-25-29-4; Portion of SE1/4 Section 33-25-29-4; Plan 9212612 Lot B; Plan 1310593 Block 5 Lot 5; Plan 1611320 Block 5 Lot 6; Plan 0913802 Block 4 Lot 2; Plan 0913802 Block 5 Lot 2; Plan 1811898 Block 5 Lots 8 and 9; Plan 0614349 Block A; Portion SW1/4 Section 27-25-29-4; Plan 1711309 Block 5 Lot 7) from Industrial – General (I-G) District, Industrial – Business f0.5h27 (I-B f0.5h27) District, Commercial – Regional 3 f0.23h18 (C-R3 f0.23h18) District, Commercial – Regional 3 f0.3h18 (C-R3 f0.3h18) District, Commercial – Regional 3 f0.5h18 (C-R3 f0.5h18) District, and Special Purpose – City and Regional Infrastructure (S-CRI) District to Industrial – General (I-G) District, Industrial – Commercial (I-C) District, Commercial – Regional 3 f0.5h18 (C-R3 f0.5h18) District, Commercial – Community 2 f2.0h24 (C-C2f2.0h24) District, Commercial – Neighbourhood 1 (C-N1) District, Residential – Low Density Mixed Housing (R-G) District, Residential – Low Density Mixed Housing (R-Gm) District, Multi-Residential – At Grade Housing (M-G) District, Multi-Residential – Low Profile (M-1) District, Multi-Residential – High Density Low Rise (M-H1) District, Mixed Use – General (MU-1f3.5h24) District, Special Purpose – City and Regional Infrastructure (S-CRI) District and Special Purpose – School, Park and Community Reserve (S-SPR) District.

Outline Plan, Policy and Land Use Amendment in Stonegate Landing (Ward 5) at multiple addresses, LOC2024-0171

HIGHLIGHTS

- This application proposes an outline plan, land use amendments and policy amendments to the *Municipal Development Plan* (MDP) and the *Northeast Industrial Area Structure Plan* (ASP) in the community of Stonegate Landing to establish a subdivision framework and a residential, commercial, mixed-use, industrial and special purpose land use pattern including a variety of transit supportive land uses near a future LRT station. Amendments to the ASP include policy direction for phasing of development in alignment with updated transportation network planning and capital funding.
- The proposal would maintain industrial and commercial land uses while introducing residential and mixed uses into the plan area to allow for more housing choice in the northeast quadrant of the city.
- What does this mean to Calgarians? Approval of the land uses and outline plan with associated policy amendments would enable development adjacent to a future LRT station, create increased housing choice and diversity and maintain industrial and commercial opportunities in the area.
- Why does this matter? The proposed land uses and outline plan would facilitate the development of two neighbourhoods that would contribute to Calgary's economic health by providing housing, jobs and services for residents, as well as commercial and employment opportunities near to existing and future mobility options.
- In August 2021, the *Calgary International Airport Vicinity Protection Area Regulation* (AVPA) was amended by the Province of Alberta in response to a joint request made by the City of Calgary and the Calgary Airport Authority. The purpose of the *AVPA Regulation* amendment was to modernize the Noise Exposure Forecast (NEF) contour areas to properly reflect the noise impacts that have changed through advancements in technology and airport operations, and to support continued development in Calgary through appropriate regulation. The NEF contours no longer prohibit residential development on a portion of the subject lands (Attachment 9).
- On 2025 January 28, a Notice of Motion directed Administration to prepare amendments to the MDP and *Northeast Industrial ASP* to enable residential land uses within Stonegate Landing through a Comprehensive Planning Overlay and to bring those amendments directly to the Public Hearing Meeting of Council on 2025 March 04. Council directed that subsequent policy amendments and a land use and outline plan return to a Public Hearing of Council no later than 2026 March 31. On 2026 February 24 Council approved a deferral request, directing the *Northeast Industrial ASP* amendments, land use amendments and outline plan for the Stonegate Landing Development area to proceed to the 2026 April 16 Calgary Planning Commission meeting and the 2026 May 12 Public Hearing Meeting of Council for consideration.

DISCUSSION

This application, located in the northeast community of Stonegate Landing, was submitted by B&A Studios, on behalf of landowners, Qualico Developments West on 2024 June 28. The approximately 194.81-hectare (481.38-acre) plan area is located in the northeast quadrant of the city and is currently vacant and undeveloped. The subject site consists of two areas that are separated by 128 Avenue NE. The northern portion is bounded by Stoney Trail NE, 128 Avenue NE to the south, Barlow Crescent NE to the west and Métis Trail NE to the east. The second

Outline Plan, Policy and Land Use Amendment in Stonegate Landing (Ward 5) at multiple addresses, LOC2024-0171

portion of the subject lands are bounded by 128 Avenue NE to the north, Country Hills Boulevard NE to the south, 36 Street NE to the west and Métis Trail NE to the east.

As noted in the Applicant Submission (Attachment 3), the land use and outline plan with policy amendments is being pursued to provide an updated vision for the subject lands including housing and employment opportunities, a school site, open space, future LRT alignment, a fire station site and regional pathways.

A detailed planning evaluation of the application, including location maps and site context, is provided in the Background and Planning Evaluation (Attachment 1).

ENGAGEMENT AND COMMUNICATION

- Outreach was undertaken by the Applicant
- Public/interested parties were informed by Administration

Applicant-Led Outreach

As part of the review of the proposed policy amendment application, the applicant was encouraged to use the [Applicant Outreach Toolkit](#) to assess which level of outreach with the public/interested parties and the respective community association was appropriate. In response, the applicant engaged with adjacent landowners and interested members of the public. The Applicant Outreach Summary can be found in Attachment 10.

City-Led Outreach

In keeping with Administration's practices, this application was circulated to the public/interested parties, notice posted on site and published [online](#). Notification letters were also sent to adjacent landowners.

Currently, there is no community association for the Stonegate Landing area. Administration contacted the adjacent Skyview Ranch Community Association, but no response was received.

Two letters of objection were received from the public stating concerns regarding traffic impacts.

Administration considered the relevant planning issues specific to the application and has determined the proposal to be appropriate. The proposal enables new residential development in a developing community and is in alignment with the *Municipal Development Plan* (MDP).

Following Calgary Planning Commission, notifications for a Public Hearing Meeting of Council for the land use amendment will be posted on site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

IMPLICATIONS

Social

The City of Calgary's Housing Strategy has an objective to increase housing supply to meet demand and increase affordability through amending and streamlining planning policy. The MDP identifies sustainability principles including creating a range of housing opportunities and

Outline Plan, Policy and Land Use Amendment in Stonegate Landing (Ward 5) at multiple addresses, LOC2024-0171

choices, mixing land uses and providing transportation services in an efficient manner. The proposal is in alignment with the policies of the MDP and Housing Strategy.

Environmental

Administration has reviewed this application in relation to the objectives of the *Calgary Climate Strategy – Pathways to 2050* programs and actions. The proposal would align with the objective of ‘Zero Carbon Neighbourhoods’ by supporting higher density mixed-use development near primary transit networks, and providing alternative travel options such as public transit, walking and cycling, which would reduce greenhouse gas emissions. Further opportunities to align development of this site with applicable climate strategies would be explored and encouraged through the review of development permits.

Economic

The subject lands are close to highways and YYC Calgary International Airport that offer ideal site selection criteria for future industrial development. Converting a portion of these lands from industrial to residential will reduce the city’s future industrial land supply but will also increase the residential tax base and provide an opportunity to utilize existing infrastructure in Stonegate Landing.

Service and Financial Implications

Full build out of the ASP will require future investments in new interchanges and mobility network improvements, to be confirmed through a NE Network Study. The network improvements include, but are not limited to, the widening of Métis Trail and the Deerfoot/128 Avenue interchange, which are both included in the 10-year Capital Infrastructure Needs Assessment for Roads and Pathways and in the Off-site Levies Bylaw 1H2024. Funding for the future network improvements will be required and assessed through the Growth Application process and/or the Business Plan and Budget process.

RISK

There is a risk of losing strategically located industrial land supply, which contributes to the city’s annual tax revenue. The northeast quadrant of the city has a large inventory of vacant industrial lands, with over 1,200 hectares and accounting for a 45% share of Calgary’s total vacant lands in city-wide industrial areas.

ATTACHMENTS

1. Background and Planning Evaluation
2. Proposed Outline Plan Conditions of Approval
3. Applicant Submission
4. Proposed Outline Plan
5. Proposed Land Use Amendment Map
6. Proposed Outline Plan Data Sheet
7. Proposed ASP Amendments
8. Proposed MDP Amendments
9. Provincially approved changes to NEF Contours
10. Applicant Outreach Summary

**Planning and Development Services Report to
Calgary Planning Commission
2026 April 16**

**ISC: UNRESTRICTED
CPC2026-0020
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**Outline Plan, Policy and Land Use Amendment in Stonegate Landing (Ward 5) at
multiple addresses, LOC2024-0171**

Department Circulation

General Manager (Name)	Department	Approve/Consult/Inform

Background and Planning Evaluation

Background and Site Context

The subject site is located within the northeast community of Stonegate Landing and comprised of two site areas that are divided by 128 Avenue NE. The site consists of 18 parcels, totaling approximately 194.81 hectares (481.38 acres) of undeveloped land. The site is well served by transportation options with access to major transportation routes including Deerfoot Trail NE, Stoney Trail NE, Country Hills Boulevard NE, Barlow Trail NE, Airport Trail NE, and Métis Trail NE.

The surrounding land use is characterized by residential, commercial and industrial development. The subject site is ideally located south of Stoney Trail NE, which is designated as Special Purpose – Transportation and Utility Corridor (S-TUC) District. Residential communities are located east of the site, including Redstone and Skyview Ranch, and consist of a mix of special purpose, commercial, multi-residential and low-density residential development.

YYC Calgary International Airport is located southwest of the site and is designated as Special Purpose – City and Regional Infrastructure (S-CRI). Parcels surrounding the southern and western boundaries consist of industrial and commercial developments designated as Industrial – Commercial (I-C) and Industrial – General (I-G) Districts.

On 2025 March 04 Council approved amendments to the *Municipal Development Plan* (MDP) and *Northeast Industrial Area Structure Plan* (ASP) providing policy direction to allow for further planning of non-industrial uses in Stonegate Landing. The current proposal includes an outline plan, land use amendments and associated comprehensive policy amendments to the Northeast Industrial ASP and map amendments to the MDP.

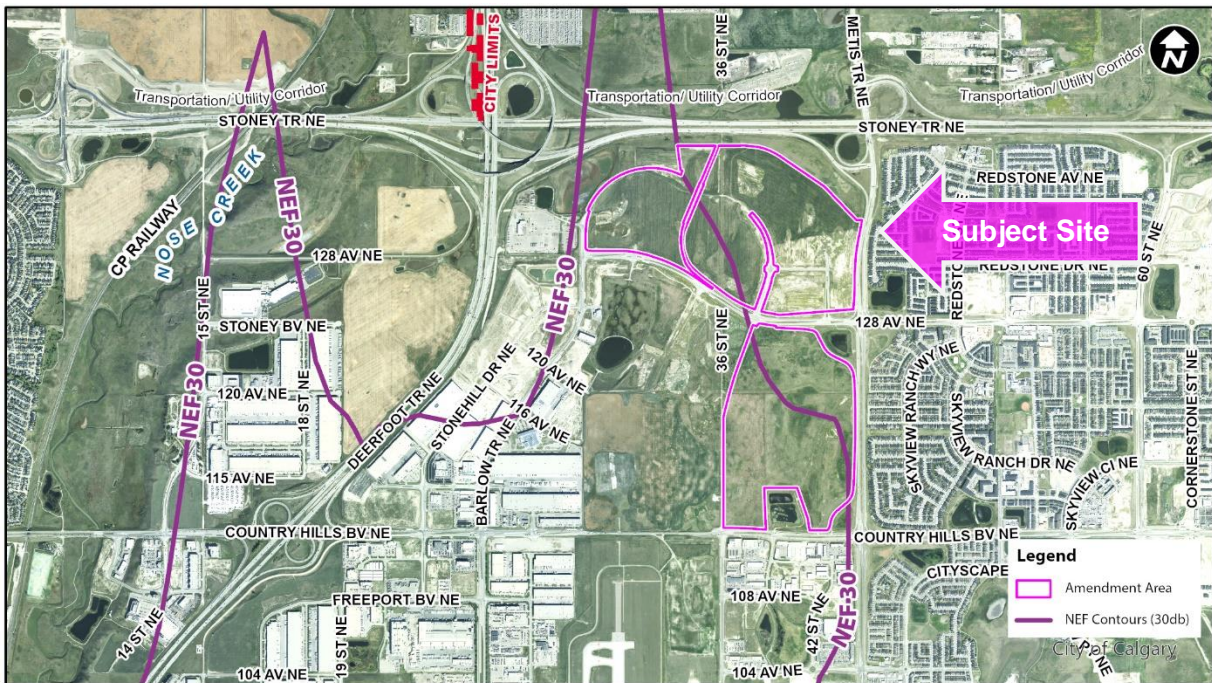
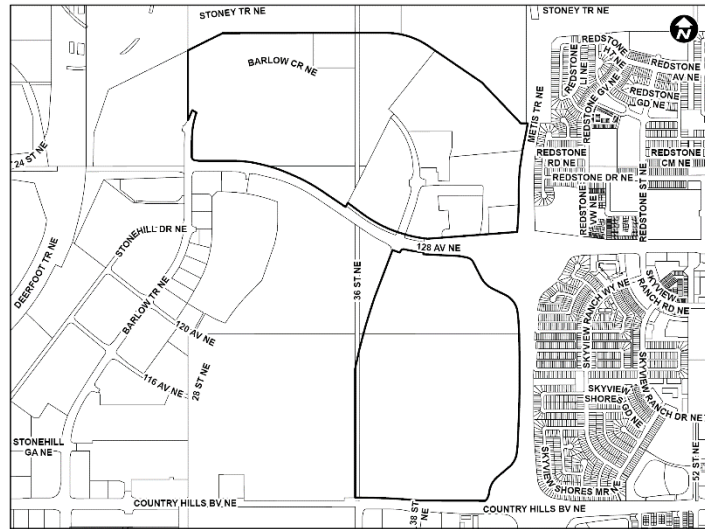
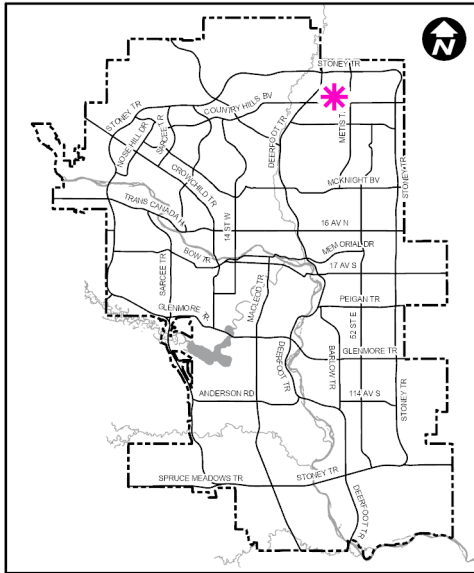
The development proposal for this site complements other development in the area. Notable features of the proposal include:

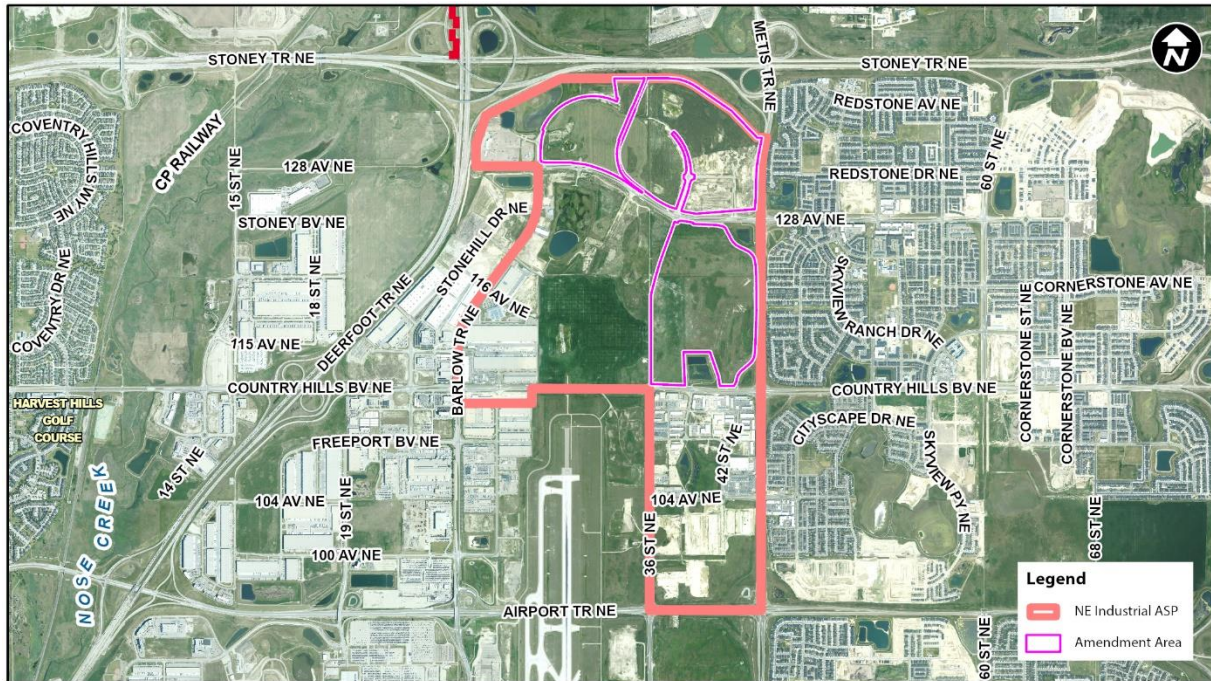
- medium and low density residential, including apartments, townhouses, rowhouses, semi-detached and single-detached homes;
- two Neighbourhood Activity Centres (NACs) that provide transit-supportive mixed-use, local commercial services and a school site;
- multi-residential and mixed-use development within easy walking distance of a future Blue Line LRT Station;
- a fire station site, supporting the needs of a safe and complete community;
- preservation of an environmentally significant wetland, along with a system of neighbourhood parks and pathways;
- a shared school site to accommodate a K-9 Calgary Board of Education School and a K-12 FrancoSud Francophone school with associated playfields; and
- a block-based grid street network that provides strong walking and wheeling connectivity.

Community Peak Population Table

There is no population data available for Stonegate Landing.

Location Maps





Previous Council Direction

On 2025 January 28, Administration was directed to prepare amendments to the MDP and the *Northeast Industrial ASP* to support the additional use of residential within the Stonegate Landing lands through a Comprehensive Planning Overlay and to bring those amendments directly to the Public Hearing Meeting of Council on 2025 March 04.

On 2025 March 04, Council provided three readings to the proposed Bylaws 27P2025 and 28P2025 for amendments to the MDP and *Northeast Industrial ASP* to allow for further planning of non-industrial uses in Stonegate Landing.

On 2026 February 24, Council approved a deferral request, directing the *Northeast Industrial ASP* amendments, land use amendments and outline plan for the Stonegate Landing Development area to proceed to the 2026 April 16 Calgary Planning Commission meeting and 2026 May 12 Public Hearing Meeting of Council for consideration.

Planning Evaluation

The amendment to the MDP and the *Northeast Industrial ASP* provide comprehensive policy direction for residential, commercial, mixed-use and industrial development.

Proposed MDP Amendments

The existing land use typology for the subject lands shown in the MDP (Map 1: Urban Structure) is Standard Industrial and Developing Residential – Planned Greenfield with Area Structure Plan (ASP). Map amendments to the MDP are required to identify areas within the proposed land use and outline plan area as Developing Residential – Planned Greenfield with (ASP).

Proposed Northeast Industrial ASP Amendments

The proposed policy amendments to the *Northeast Industrial ASP* introduce residential and mixed-use land uses into the plan area. The proposed ASP amendments establish policy direction for a residential, commercial, mixed-use and industrial development including a variety of transit-supportive land uses near a future LRT station. Neighbourhood parks, a school site and future fire station site are identified in the ASP policies. Amendments to the *Northeast Industrial ASP* also include policy direction for phasing of development in alignment with updated transportation network planning and capital funding.

Land Use

The existing land uses include commercial, industrial and special purpose districts.

This application proposes several residential, mixed use, commercial, industrial and special purpose districts, including:

- Residential – Low Density Mixed Housing (R-G) District;
- Residential – Low Density Mixed Housing (R-Gm) District;
- Multi-Residential – At Grade Housing (M-G) District;
- Multi-Residential – Low Profile (M-1) District;
- Multi-Residential – High Density Low Rise (M-H1) District;
- Mixed Use – General (MU-1 f3.5h24) District;
- Commercial – Neighbourhood 1 (C-N1) District;
- Commercial – Community 2 (C-C2f2.0h24) District;
- Commercial – Regional 3 (C-R3f0.5h18) District;
- Industrial – General (I-G) District;
- Industrial – Commercial (I-C) District;
- Special Purpose – School, Park and Community Reserve (S-SPR) District; and
- Special Purpose – City and Regional Infrastructure (S-CRI) District;

Residential – Low Density Mixed Housing (R-G) District

The proposed R-G District is intended for low-density neighbourhoods in master-planned communities in suburban greenfield locations. The district is designed to support a variety of low-density residential building forms including single detached dwellings, duplex dwellings, and rowhouse buildings, along with secondary suites and backyard suites.

Residential – Low Density Mixed Housing (R-Gm) District

The proposed R-Gm District has the same intent as the R-G District but does not allow for permitted single detached dwellings. Like the R-G District, the maximum building height is 12 metres. This district is placed in locations that provide appropriate built form as a gateway into the community.

Multi-Residential – At Grade Housing (M-G) District

The proposed M-G District is intended for multi-residential development with higher numbers of dwelling units and traffic generation than low density residential dwellings. Development must be designed to provide all units with direct pedestrian access to grade, meet a minimum density of 35 units per hectare, contain at least three or more units and may be built to a maximum of 13 metres in height.

Multi-Residential – Low Profile (M-1) District

The proposed M-1 District is intended for multi-residential development of low height and medium density. It is intended for areas in proximity or adjacent to low density residential development. This district allows for a maximum building height of 14 metres. There is no listed maximum floor area ratio (FAR). It lists a minimum density of 50 units per hectare and a maximum of 148 units per hectare.

Multi-Residential – High Density Low Rise (M-H1) District

The proposed M-H1 District is intended for multi-residential development with high density. The M-H1 District is intended to be located at community nodes, transit and transportation corridors and nodes. Density is measured by floor area ratio (FAR) to provide flexibility in building form and dwelling unit size and number. The M-H1 District requires that multi-residential development achieves a minimum density of 150 units per hectare. The maximum FAR is 4.0 and the maximum height of 26.0 metres (approximately six to eight storeys). This district includes a limited range of support commercial multi-residential uses, restricted in size and location within the building.

Mixed Use – General (MU-1f3.5h24) District

The proposed Mixed Use – General (MU-1f3.5h24) District is intended for locations along commercial streets where residential and commercial uses are integrated, with active uses supported at grade fronting the street. It facilitates a blend of residential and commercial development within single or multiple buildings across an area and establishes parcel-specific maximum building heights in response to local context. The proposed MU-1f3.5h24 District allows for a maximum height of 24.0 metres (about six storeys) and a maximum floor area ratio (FAR) of 3.5, enabling a flexible built form which optimizes density within a Transit Oriented Development (TOD) context. The district may also support small-scale commercial uses. The proposed density and height are considered appropriate and will allow for more housing choice within the community and more efficient use of future infrastructure

Commercial – Neighbourhood 1 (C-N1) District

The proposed C-N1 District is intended for small scale street-oriented commercial development with opportunities for residential uses on the upper floors. The district provides for development that has limited use sizes and types. The maximum floor area ratio is 1.0 and the maximum height is 10 metres.

Commercial – Community 2 (C-C2f2.0h24) District

The proposed C-C2 District is intended to accommodate larger scale commercial development with opportunities for commercial uses to be combined with office and residential uses in the same development. Development controls include size limitations for specific uses and rules for the location of uses within buildings. The proposed C-C2f2.0h24 District allows for a maximum height of 24.0 metres (about six storeys) and a maximum floor area ratio (FAR) of 2.0.

Commercial – Regional 3 (C-R3f0.5h18) District

The proposed C-R3 District is intended to be applied on sites in the southern portion of the plan. The district is characterized by comprehensively planned and designed development of multiple buildings on multiple parcels. The proposed C-R3f0.5h18 District allows for a maximum height of 18.0 metres and a maximum floor area ratio (FAR) of 0.5.

Industrial – General (I-G) District

The proposed I-G District is intended to accommodate a wide variety of light and medium general industrial uses and a limited number of commercial uses, with no restriction on building height and floor area ratio (FAR) of 1.0.

Industrial – Commercial (I-C) District

The proposed Industrial – Commercial (I-C) District is intended for locations on the perimeter of industrial areas, along major streets and expressways, to accommodate light industrial uses and small-scale commercial uses that are compatible with and complement light industrial uses. The I-C District allows for a maximum building height of 12.0 metres and a maximum FAR of 1.0. It would facilitate the development of light industrial and small-scale commercial opportunities on the site, which are compatible with the adjacent mixture of both industrial and special purposes uses.

Special Purpose – School, Park and Community Reserve (S-SPR) District

The proposed S-SPR District is intended to provide for schools, parks, open space and recreational facilities with parcels of various sizes and use intensities. This district is to be used for land dedicated as Municipal School Reserve or other forms of Municipal Reserve (MR) pursuant to the MGA.

Special Purpose – City and Regional Infrastructure (S-CRI) District

The proposed S-CRI District is intended to provide for city and regional infrastructure necessary for the proper servicing of the development, as well as uses operated by Federal, Provincial, and Municipal levels of government. The S-CRI District is proposed for a stormwater pond, an abandoned well setback site, a fire station site, a LRT park and ride site and transit bus loop site. The stormwater pond and abandoned well setback site will be designated as Public Utility Lots (PUL) pursuant to the Municipal Government Act (MGA). Together, the S-CRI District sites comprise 14.30 hectares ± (35.33 acres ±) of the proposed redesignation area.

Subdivision Design

Housing

A variety of housing options are possible in the plan area, including low density housing forms, such as single-detached, semi-detached, duplex, rowhouse and apartments. Laned and laneless parcels are supported to allow for a variety of building forms as well as the ability to intensify over time through secondary and backyard suites. Multi-residential housing forms, such as townhouses and apartments are located within the northwest portion of the plan, which is within the Transit Station Planning Area (associated with the Neighbourhood Activity Centre (NAC) located to the north). Non-industrial land uses are located between housing and industrial uses throughout the plan area. In the limited locations where housing and industrial uses are adjacent, transition and buffering will be designed at the development permit stage. The proposed land use framework of this plan area meets the requirements of the MDP and ASP.

Pathway and Street Network

An interconnected system of streets, pathways and walkway provide mobility choices to future residents while keeping road dedications within MGA requirements and working with the boundary conditions of the neighbourhood.

Transit Station Planning Area

The purpose of this area is to provide for Transit-Oriented Development (TOD) surrounding an LRT station within the Plan area. This station will be situated adjacent to a residential neighbourhood and an industrial/business park. Accordingly, multi-unit residential, mixed-use development, smaller format commercial as well as transit-supportive uses such as a park and ride and transit drop-off areas are planned close to the station. This development pattern complies with the restrictions within the Airport Vicinity NEF contours. Pedestrian connections provide community linkages within the Transit Station Planning Area.

Neighbourhood Activity Centres

The Neighbourhood Activity Centres (NAC) are designed comprehensively to create unique and attractive gathering spaces. Mixed-uses and a future LRT station are in close proximity to the north NAC. Neighbourhood commercial, mixed-uses, and a school site are next to the south NAC.

Schools and Open Space Network

The outline plan includes school sites with shared playfields plus neighbourhood parks designed with passive and active and recreational amenities. The 7.25-hectare shared school site will accommodate a K-9 Calgary Board of Education school and a K-12 FrancoSud Francophone school. Four neighbourhood parks are proposed, ranging in size from 0.59 hectares to 0.41 hectares. A regional pathway network provides connectivity throughout the outline plan area and to adjacent communities.

Density and Intensity

At build-out, the proposed outline plan is anticipated to have a total of 2,977 units and a residential density of 32.6 units per hectare (13.2 units per acre). The anticipated intensity of the plan area is 54 people and jobs per gross developable hectare.

The *Municipal Development Plan* (MDP) sets out minimum density and intensity targets for new communities at a density of 20 units per gross developable hectare (eight units per acre) and an intensity of 60 people and/or jobs per hectare. The anticipated intensity of the residential areas is 86.4 people and jobs per gross developable hectare, and the anticipated intensity of the non-residential areas is 24 people and jobs per gross developable hectare. If the non-residential areas develop with more employee intensive uses, the non-residential intensity will increase.

Transportation

The subject lands are bounded by Stoney Trail NE to the north, Métis Trail NE to the east, and Country Hills Boulevard NE to the south, with 128 Avenue NE bisecting the site.

A Transportation Impact Assessment (TIA) was submitted in support of the application and reviewed by Administration. The TIA evaluated both local and regional transportation network requirements associated with the proposed land use. The analysis confirms that Phase 1 land use (Phases 1A and 1B) can proceed without the need for upgrades to the regional transportation network, subject to required local infrastructure. Policies directing approvals of subsequent phasing (Phase 2-4) of development in alignment with updated transportation network planning and capital funding are included with the proposed ASP amendments (Attachment 7). The regional and local transportation requirements are summarized below.

Local Road Network

Access to the Phase 1 development areas will be provided primarily from 128 Avenue NE, with internal traffic distribution through the planned local road network. For Phase 1A, internal traffic circulation will be accommodated via Stone Ridge Boulevard NE, Stone Haven Drive NE, 32 Street NE, and Iron Grove NE/Shale Gate NE. For Phase 1B, access and traffic circulation will be provided via Stone Ridge Road NE and Stone Ridge Way NE.

Note that portions of the existing roads were constructed in association with previous land use designations, based on primarily industrial and commercial uses, and using superseded design standards. With this application, the road cross-sections were modified and retrofitted to meet current proposed land uses, including the addition of residential and school uses, and meeting current design standards.

Regional Road Network

For the proposed Phases 2 through 4, the TIA identifies the need for regional infrastructure upgrades, including interchanges at Deerfoot Trail/128 Avenue NE, 128 Avenue NE/Métis Trail NE and Country Hills Boulevard/Métis Trail NE, as well as the widening of Métis Trail NE, Country Hills Boulevard NE and 128 Avenue NE to six lanes. It also identifies the need to construct 36 Street NE to four lanes. Approval of land use redesignations for Phases 2 through 4 are included at this time. Subdivision and development approvals will proceed as transportation network planning, functional planning studies and the funding framework are completed.

A partial interchange at Deerfoot Trail/128 Avenue NE was identified in the *Northeast Industrial ASP* and has subsequently been included for funding consideration in the City's 10 Year Capital Plan.

In July 2013, Métis Trail NE was reclassified from a Skeletal Road to an Arterial Street between McKnight Boulevard NE and Stoney Trail NE, based on the Northeast Network Planning Study (June 2012). As part of the reclassification, several previously planned interchanges were removed, including interchanges at Métis Trail and 128 Avenue NE and at Métis Trail Country Hills Boulevard NE. Lands previously reserved for an interchange at Métis Trail and Country Hills Boulevard NE were later confirmed as surplus, and the associated road right-of-way was closed and consolidated with adjacent parcels.

Since completion of 2012 Network Planning Study, the area has experienced significant growth. The existing network planning framework no longer reflects current conditions. As a result, prior to approval of Phase 2 and subsequent Phases of development, the following are required to occur:

1. Completion of an updated Network Planning Study by The City in the northeast area to confirm regional infrastructure needs and timing for the broader mobility system.
2. Completion of Functional Planning Studies, funded by the applicant, for the interchanges at: 128 Avenue NE/Métis Trail NE; and Country Hills Boulevard/Métis Trail NE. These studies will evaluate interchange functionality and type, including public engagement, utilities, stormwater management, land requirements, staging, and cost estimates. These studies will replace previous outdated analyses and reflect current growth and network demands.
3. Update the Off-site Levies Bylaw to incorporate the Country Hills Boulevard/Métis Trail NE interchange. The 128 Avenue NE/Métis Trail NE interchange is currently included within the Off-site Levies Bylaw.

4. Securing and approval of funding for detailed design and construction of the required interchanges and associated roadway widening, including Deerfoot Trail/128 Avenue NE, 128 Avenue NE/Métis Trail NE, and Country Hills Boulevard NE/Métis Trail NE.
5. Submission of a Traffic Analysis Memo prior to approval of subsequent subdivision and development (Phases 2 to 4) to confirm infrastructure triggers, based on updated traffic volumes, development staging and plans at the time of application.

Active Modes/Always Available for All Ages and Abilities (5A) Network

The proposed transportation network incorporates wide sidewalks, multi-use pathways, and regional pathways along local, collector, and major roadways to support walking, cycling, and transit accessibility. The road network includes regional pathways along: Métis Trail NE, 128 Avenue NE, Country Hills Boulevard NE, 36 Street NE; and along the eastern and northern boundaries of the site. Multiple walkway connections are provided to connect the development to adjacent regional pathway systems and the broader Always Available for All Ages and Abilities (5A) Network.

Roadways surrounding the school site include a 3.5m mono-multiuse pathway on one side and a 2.0 metre wide sidewalk on the other side.

Traffic calming measures, including curb extensions, roundabouts, and smart channelized right-turn treatments are included to reduce vehicle speeds and enhance pedestrian and cyclist safety.

Transit

The MDP identifies 128 Avenue NE as part of the Primary Transit Network and provides access to Deerfoot Trail NE / Highway 2. Surrounding Primary Transit Network roadways include Airport Trail NE, Country Hills Boulevard NE and 60 Street NE. A future Blue Line LRT station is planned within the subject lands at a location north of 128 Avenue NE.

Transit bus routes are planned for 36 Street NE / Stone Haven Drive NE, as well as Stone Ridge Way NE and 32 Street NE. These transit routes will take riders to and from the future LRT station.

Environmental Site Considerations

A Phase I Environmental Site Assessment, Preliminary Natural Site Assessment, Historical Resources Overview Assessment and Geotechnical Evaluation Reports were submitted and accepted by Administration as part of the application review process.

Utilities and Servicing

Water and sanitary servicing will be provided by connecting to the existing water and sanitary mains at 128 Avenue NE located in the plan area. The proposed development is supported by existing major capital infrastructure, which is also serving other planned developments. The timing and phasing of the subject lands will need to be assessed at the subdivision stage to determine when new off-site water and sanitary infrastructure investments are required to support the development. Stormwater will be managed through existing area ponds; however, the lands ultimately discharge to Nose Creek which has release rate targets. Revisions to the Staged Master Drainage Plan may require Water Act approval from Alberta Environment and Protected Areas to accommodate these obligations.

Legislation and Policy

South Saskatchewan Regional Plan (2014)

Administration's recommendation aligns with the policy direction of the [South Saskatchewan Regional Plan](#), which directs population growth in the region to cities and towns, and promotes the efficient use of land.

Calgary International Airport Vicinity Protection Area (2009)

The Calgary International Airport Vicinity Protection Area (AVPA) identifies the subject site as being located within the 25-30, 30-35 and 35-40 Noise Exposure Forecast (NEF) of the AVPA. The [AVPA Regulation](#) was created to ensure that only compatible land uses are developed near airport flight paths. The AVPA Regulation establishes prohibitive uses in certain locations, identified within NEF areas. The proposed policy amendments are in alignment with the 2021 changes to the AVPA Regulation and align with allowing residential in the 25-30 NEF area where residences were previously prohibited. Future outline plan, land use amendment and development permit applications would be circulated to the Airport Authority and reviewed against the applicable regulations to ensure compliance.

Rocky View/Calgary Intermunicipal Development Plan (2012)

The site is within the Policy Area on (Map 1: Plan Area) of the [Rocky View County/City of Calgary Intermunicipal Development Plan](#) (IDP). The application was circulated to Rocky View County for their review and no concerns were identified. The proposal is consistent with the goals of the Rocky View County/City of Calgary IDP.

Municipal Development Plan (Statutory – 2009)

The site is located within the "Developing Residential – Planned Greenfield with Area Structure Plan (ASP)" area as identified on Map 1: Urban Structure in the [Municipal Development Plan](#) (MDP). The applicable policies promote housing diversity and choice through a wide range of housing types and densities to create diverse neighbourhoods. The proposed outline plan, land use proposal and policy amendment accommodate residential, commercial, mixed-use and industrial development that meets minimum density, and intensity targets applicable to this area. The proposed outline plan and land use proposal aligns with the applicable policies of the MDP.

Within Section 1.1.1, the MDP identifies sustainability principles including creating a range of housing opportunities and choices, mixing land uses and providing transportation services in a safe, effective, affordable and efficient manner that ensures reasonable accessibility to all areas of the city for all residents. The proposal is in alignment with the policies of the MDP by integrating residential uses into the plan area to assist in achieving a complete community.

Within (Map 2: Primary Transit Network) it shows 128 Avenue NE running through the site area being a part of the Primary Transit Network and the location of the proposed future alignment of the Blue Line LRT. Section 2.2.2 describes developing a transit-supportive land use framework that consists of elements including density, diversity, design and distance to help promote transit-oriented development. The proposal is in alignment with the policies of the MDP in supporting a transit-supportive land use framework by introducing a variety of land uses in close proximity to future transit including residential land uses which promotes future transit ridership.

In consideration of the policies and sustainability principles outlined above, locating residential uses on the subject lands may be considered suitable based upon creating a complete community near to a future LRT station, promoting transit-oriented development and proposing residential near the Primary Transit Network and existing communities.

Transit Oriented Development Policy Guidelines (2004)

The [Transit Oriented Development Policy Guidelines](#) provide direction for the development of areas within 600 metres of an existing BRT or LRT station or an existing future LRT station. The Guidelines encourage development that creates a higher density, walkable, mixed-use environment within station areas in order to optimize use of transit infrastructure, create mobility options for Calgarians, and benefit local communities. The proposed plan complies with the intent of these guidelines.

Calgary Climate Strategy (2022)

This application does not include any specific actions that address the objectives of the [Calgary Climate Strategy – Pathways to 2050](#). Further opportunities to align development of this site with applicable climate strategies will be explored and encouraged at subsequent development approval stages.

Northeast Industrial Area Structure Plan (Statutory – 2007)

The proposal seeks to amend the [Northeast Industrial Area Structure Plan](#) (ASP) as current policy states that residential development is prohibited within the Northeast Industrial Area. The subject site is within two land use policy categories, including Business/Industrial and Gateway Commercial, that currently enables light industrial and retail commercial uses.

The proposed ASP policy amendment introduces opportunities for neighbourhood and residential land uses into the plan area while providing alignment with the proposed MDP amendments. The ASP area is envisioned to redevelop over time with connectivity to the surrounding communities. The updated ASP vision is a vibrant and successful business and residential area that provides for a complete community with employment, housing, education and recreational opportunities (Attachment 7).

Outline Plan Conditions of Approval

These conditions relate to Recommendation 1 for the Outline Plan where Calgary Planning Commission is the Approving Authority. Attachment for Council's reference only.

The following Conditions of Approval shall apply:

Planning

1. The developer shall pay cash-in-lieu of land for reserve dedication in the amount of 4.065 hectares, to the satisfaction of the City of Calgary.
2. The balance of reserves owing in the amount of 9.077 hectares shall be deferred by caveat pending future subdivision of the residual lands.
3. Compensation for dedication of reserves in excess of 10% is deemed to be \$1.00.
4. Existing buildings that are to be removed must be removed prior to endorsement of the legal plan of subdivision for the area where the building is located.
5. With each tentative plan of subdivision, the developer shall submit a density phasing plan indicating the intended phasing of subdivision within the outline plan area and the projected number of dwelling units within each phase, demonstrating compliance with the minimum required densities.
6. Prior to approval of the affected tentative plan of subdivision, the proposed community and street names shall be submitted to the City for review and approval by City Council.
7. All existing utilities within the road closure area shall be protected by easement or relocated at the developer's expense.
8. Prior to submission of the tentative plan of subdivision, the developer shall enter into negotiations with Real Estate and Development Services for the purchase of the closed road right-of-way. Please contact the Coordinator, Real Estate Sales at realestateinquiries@calgary.ca to commence negotiations. Provide documentation to show that negotiations have commenced.

Provide a PDF of the registered road plan from the Land Title Office, with a plan number. Contact landadmin@calgary.ca at the City of Calgary Real Estate and Development Services at to obtain authorization to register the road closure plan at the Land Titles Office.
9. The developer is responsible for all costs associated with the closure including all necessary physical construction, removal, rehabilitation, utility relocation, etc.
10. The closed road right-of-way is to be consolidated with the adjacent lands

11. The developer shall provide payment in lieu of reserve dedication for 4.065 ha of municipal reserve. A land appraisal report performed by an independent appraiser, contracted by The City of Calgary, shall be prepared to determine the value of the payment in lieu of reserve.
12. The developer shall provide payment in lieu of reserve dedication. An appraisal report performed by an independent appraiser, contracted by The City of Calgary, shall be prepared to determine the value of the payment in lieu of reserve. The cost of the land appraisal is \$6346.00 and will be coordinated through the Real Estates & Development Services Team.
13. Rehabilitate all portions of the Municipal School Reserve/Municipal Reserve/Environmental Reserve/Public Utility Lot/Transportation Utility Corridor/private (add details) lands along the boundaries of the plan area that are damaged as a result of this development, all to the satisfaction of the Director, Calgary Parks.
14. Construct all regional pathway routes within and along the boundaries of the plan area according to Calgary Parks Development Guidelines and Standard Specifications: Landscape Construction (current version), including setback requirements, to the satisfaction of the Director, Calgary Parks.
15. Plant all public trees in compliance with the approved Landscape Construction Drawing for Boulevard and Median Tree Line Assignment.
16. Prior to endorsement of the tentative plan of subdivision, landscape construction drawings that are reflective of the subject tentative plan of subdivision for the proposed Municipal Reserve lands are to be submitted to the Coordinator, Landscape Construction Approvals for review and approval prior to construction.
17. All proposed parks (Municipal Reserve/Environmental Reserve) and Regional/Local Pathways and Trails must comply with the Calgary Parks and Open Spaces DGSS - Development Guidelines and Standard Specifications: Landscape Construction (current edition).
18. Calgary Parks and Open Spaces does not support point source drainage directed towards Municipal Reserve (MR)/Municipal School Reserve (MSR) or Environmental Reserve (ER) extents. All drainage and storm related infrastructure catering to private property shall be entirely clear of MR/ER/MSR areas.
19. All shallow utility alignments, including street light cables, shall be set back 1.5 metres from the street tree alignment on all road cross sections in accordance with Section 4.1.3 of Calgary Calgary Parks and Open Spaces *Development Guidelines and Standard Specifications: Landscape Construction (current edition)*.
20. The developer, at its sole cost and expense, shall be responsible for the construction of the Municipal Reserve (MR)/ Municipal School Reserve (MSR) parcels within the boundaries of the plan area according to the approved

Landscape Construction Drawings and the Calgary Parks and Open Spaces Development Guidelines and Standard Specifications: Landscape Construction (current version).

21. At the tentative plan of subdivision stage, cross-sections illustrating the interface between the proposed Environmental Reserve (ER) parcel (4.22 ha, northwest corner of the plan) and the proposed Public Utility Lot (PUL - Pond 6) shall be submitted for review to confirm that proposed grading and construction do not encroach into the ER. Where the cross-sections demonstrate potential encroachment, revisions to the anticipated parcel boundaries or design may be required to ensure compliance with ER requirements.
22. With the submission of Landscape Construction Drawings, the developer shall include a detailed Restoration Plan including a maintenance schedule for each Environmental Reserve proposed to be affected by any construction. The Plan should indicate how it will be rehabilitated and restored. The restored area(s) shall be maintained by the developer until it is established and approved by Parks prior to Final Acceptance Certificate.
23. The developer shall minimize stripping and grading within the Environmental Reserve. Any proposed disturbance within the ER, including that for roadways, utilities, and storm water management infrastructure, shall be approved by Calgary Parks prior to stripping and grading.
24. The developer shall install and maintain a temporary construction fence on the private property line with the adjacent Environmental Reserve to protect public lands prior to the commencement of any stripping and grading related to the site and during all phases of construction. Contact the Parks Development Inspector to approve the location of the fencing prior to its installation.
25. Prior to approval of the related Stripping and Grading Permit or Engineering Construction Drawings, whichever submitted first, the developer shall install Environmental Reserve (ER) protection measures around the wetlands/ravines/Environmentally Significant Areas to be retained in order to prevent excessive overland drainage and siltation onto said areas during all phases of construction, in accordance with The City's Guidelines for Erosion and Sediment Control, to the satisfaction of the Director of Calgary Parks. Contact the Parks Development Inspector Annie Rodrigues (403-804-9397) to approve the location prior to commencement of Stripping and Grading activities.
26. Pursuant to Part 4 of the Water Act (Alberta), the applicant shall promptly provide a copy of the Water Act approval from Alberta Environment to The City of Calgary Parks department.
27. Until receipt of the Water Act approval by the applicant from Alberta Environment, the wetland(s) shall not be developed or disturbed in anyway and shall be protected in place.

28. Prior to Endorsement of the affected tentative plan of subdivision, the developer shall submit detailed Engineering Construction Drawings and Landscape Construction Drawings for the proposed wetland/storm pond to both Water Resources and Parks for review.
29. No disturbance of Environmental reserve lands is permitted without written permission from the Parks Planner for this area.
30. Prior to the approval of a stripping and grading permit, a Development Agreement or a subject area Tentative Plan, Parks requires details pertaining to the total limit of disturbance resulting from the proposed development in its entirety.
31. Any development or grading related to permanent disturbance which results from storm water infrastructure within lands designated as environmental reserve, requires approval from the Director of Parks.
32. Prior to the approval of the affected tentative plan of subdivision, the developer shall confirm fencing requirements adjacent to ER parcels to the satisfaction of the Director, Calgary Parks.
33. Prior to approval of the first tentative plan of subdivision or stripping and grading permit (whichever comes first), it shall be confirmed that grading of the development site will match the existing grades of adjacent parks and open space (MR and/or ER), with all grading confined to the private property, unless otherwise approved by Parks.
34. Prior to approval of the tentative plan of subdivision or stripping and grading permit (whichever comes first), an onsite meeting shall be arranged to confirm that the surveyed boundaries of the environmental reserve area meet Parks approval. A plan illustrating the surveyed ER boundaries must be provided to Parks in advance of the onsite meeting.
35. Prior to endorsement of the legal plan of subdivision, a Wetland Management Plan is to be submitted for Wetlands E6 and H, as per requirements stated in the Staged Master Drainage Plan.
36. A Wetland Monitoring Plan for Wetlands E6 and H must be submitted and approved prior to development permit for stripping and grading of the development site.
37. Proposed municipal reserve amenities will require further refinement and negotiation at the tentative plan of subdivision and Landscape Construction Drawing process. These proposed amenities include, but are not limited to, half basketball courts, community gardens, and skate/seating walls.
38. Proposed 'South Central MR Concept' will require further refinement and design at the tentative plan of subdivision and Landscape Construction Drawing process.

Utility Engineering

39. This subject plan area is within the boundary of the Nose Creek drainage catchment and subject to stormwater volume control measures. Based on the Watershed Management Plan stormwater discharge is limited to an average annual runoff volume is limited to 200mm. Low Impact Development and stormwater source control is recommended.
40. Servicing arrangements shall be to the satisfaction of the Manager, Development Engineering.
41. The Master Drainage plan has been approved. The staged master drainage report is accepted in principal. Prior to the first tentative plan the SMDP will need to be finalized.
42. Primary water network has been conditionally approved. Prior to the first tentative plan the applicant will need to finalize the water network.
43. One operating pipeline (License 7634) has been identified adjacent to the site. Prior to the first tentative plan the applicant shall provide documentation from the operator (Alberta Products Pipeline Ltd.) confirming that they do not object to the proposed lot size and distance to the existing pipeline right of way and have specified acceptable surface uses on the pipeline right of way. All documentation shall be reviewed to the satisfaction of The City of Calgary.
44. Execute a Development Agreement. Contact the Infrastructure Strategist, Development Commitments for further information at email Valerie.Gibbons@calgary.ca.
45. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:
 - a) Install the offsite/onsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.
 - b) Construct the underground utilities and surface improvements within and along the boundaries of the plan area
 - c) Construct the onsite and offsite storm water management facilities (wet pond, wetlands, etc.) to service the plan area according to the most current City of Calgary Standard Specifications Sewer Construction, Stormwater Management and Design Manual and Design Guidelines for Subdivision Servicing.
 - d) Construct a wood screening fence, chain link fence, sound attenuation fence, whichever may be required, along the boundary of the plan area.
 - e) Construct the MR/ER/PUL within the plan area.

- f) Construct the multiuse pathways and regional pathways within and along the boundaries of the plan area, to the satisfaction of the Director of Parks Development.
46. Make satisfactory cost sharing arrangements with WAM Stoney Industrial GP for part cost of the existing underground utilities and surface improvements installed/constructed in 128 AV NE that was paid for and/or constructed by WAM Stoney Industrial GP under Northeast Industrial, Phase 2, DA2009-0004.
 47. Make satisfactory cost sharing arrangements with WAM Stoney Industrial GP for part cost of the existing underground utilities and surface improvements installed/constructed in Barlow CR NE and 128 AV NE that was paid for and/or constructed by WAM Stoney Industrial GP under Northeast Industrial, Phase 1, DA2010-0109.
 48. Make satisfactory cost sharing arrangements with WAM Stoney Industrial GP for part cost of the existing storm pond constructed in 28-25-29-4-NE that was paid for and/or constructed by WAM Stoney Industrial GP under Northeast Industrial, Phase 1, DA2010-0109.
 49. Make satisfactory cost sharing arrangements with the City of Calgary Transportation Infrastructure for part cost of the existing storm pond (Pond I) constructed in 27-25-29-4-SW.
 50. Make repayment arrangements with the City of Calgary for part cost of the existing storm pond (WP207) constructed in 27-25-29-4-NW, which was installed and financed by the City of Calgary.
 51. Make repayment arrangements with the City of Calgary for part cost of the underground utilities and surface improvements in Country Hills BV NE adjacent to the site, which was installed by Shepard Development Corporation through their Jacksonport, Phase 1, DA2009-0031, and financed by Calgary Roads - Program 204 (#432576).
 52. Prior to approval each subdivision or development permit, provide a letter of confirmation that the density proposed equals to or less than the anticipated density in the LOC and approved Sanitary servicing study.
 53. Prior to the approval of the first tentative plan, provide a phasing plan with anticipated timeline for the water network so we can evaluate the needs for future infrastructure to support a larger regional area.
 54. Prior to approval of each tentative plan/ development permit, submittal and acceptance of a geotechnical slope stability assessment will be required.

Prior to approval of each tentative plan/ development permit submittal and acceptance of a geotechnical deep fills report will be required.
 55. The existing ponds will need to be modified to accommodate the development. Prior to the first tentative plan an updated storm pond report will be required

Mobility Engineering

56. At the Phase 1 (1A and 1B) Tentative plan stage, equivalent to 2241 residential units, the following regional infrastructure network improvements, local infrastructure network improvements, and technical memorandum are required.
- a. No capital funded regional network infrastructure upgrades required at Phase 1.
 - b. Developer funded local Improvements:
 - Construct internal roadways and intersections associated with the phasing.
 - Construct sidewalks and pathways associated with the road cross-sections.
 - Regional pathways completed to the satisfaction of Development Engineering.
 - Install traffic signals along with associated pavement marking, signage and geometric changes at:
 - 128 Avenue/Barlow Trail/Stone Ridge Way NE intersection.
 - 128 Avenue/36 Street/Stone Haven Drive NE intersection.
 - 128 Avenue/Stone Ridge Blvd NE intersection.

At the Initial Tentative Plan associated with Phase 2 Development, a Technical Memorandum is required. The memorandum shall be prepared using updated traffic counts and assumptions on surrounding developments and the supporting roadway network to confirm the capacity available on the regional road network including the accommodation of active modes is provided. Recommendations of the Technical memorandum shall be implemented through at the Tentative plan stage and / or the City led capital infrastructure program.

57. At the Phase 2 (2A and 2B) initial Tentative plan stage, equivalent to 6817 residential units, the following regional infrastructure network improvements, and local infrastructure network improvements will be required, subject to the findings of the Northeast Network Planning study, Technical Memorandum, and / or local Transportation Impact Assessment.
- a. Regional network capital funded infrastructure upgrades required:
 - Construct the interchange at Deerfoot Trail/128 Avenue NE, ramps and associated roadways (Deerfoot to Barlow Trail NE/Stone Ridge Way NE). Note that interchanges are subject to the findings of the Northeast Network Planning study. Further note that the widening of 128 AV NE to six (6) lanes will require additional right of way for 128 AV NE between Deerfoot Trail and Barlow Trail NE.
 - Widen Métis Trail NE to a six (6) lane cross section.
 - b. Developer funded Improvements:
 - Widen Country Hills Boulevard to six (6) lane cross-section
 - Construct internal roadways and intersections associated with the phasing
 - Regional pathways completed to the satisfaction of Development Engineering.

Construct 36 Street NE/Stone Spire Drive NE, including but not limited to the following:

- Construct north approach: Three northbound and two southbound through lanes, dual left turn lanes and a right turn lane (smart right turn)
- Construct south approach: Three northbound and two southbound through lanes, one left turn and one right turn lane (smart right turn)
- Construct east approach: Four (4) lane cross-section
- Install traffic signals along with associated pavement marking and signage
- Construct sidewalks and pathways associated with the road cross-sections

Construct 36 Street NE/120 Avenue NE intersection, including but not limited to the following:

- Construct north approach: Two northbound and two southbound through lanes, dual left turn lanes and a right turn lane (smart right turn)
- Construct south approach: Two northbound and two southbound through lanes, one left turn and one right turn lane (smart right turn)
- Construct east approach: Four (4) lane cross-section and one left turn lane
- Install traffic signals along with associated pavement marking and signage
- Construct sidewalks and pathways associated with the road cross-sections

Construct 36 Street NE/114 Avenue NE intersection, including but not limited to the following:

- Construct north approach: Two northbound and two southbound through lanes, one left turn and one right turn lane (smart right turn)
- Construct south approach: Two northbound and two southbound through lanes, one left turn and one right turn lane (smart right turn)
- Construct east approach: Four (4) lane cross-section and one left turn lane
- Install traffic signals along with associated pavement marking and signage
- Construct sidewalks and pathways associated with the road cross-sections

58. At the Phase 2 (2A and 2B) initial Tentative Plan stage, equivalent to 6817 residential units, the following local infrastructure network improvements and technical memorandum are required.

Developer funded Improvements:

Construct Country Hills Blvd NE/36 Street NE intersection, including but not limited to the following:

- Construct north approach: Two northbound and two southbound through lanes, dual left turn lanes and a right turn lane (smart right turn) and associated geometric changes

- Construct south approach: Two northbound lanes, one right turn lane (smart right turn) and associated geometric changes
- Construct east approach: Dual left turn lanes and associated geometric changes
- Construct west approach: Modifications to right turn island (smart right turn) and associated geometric changes
- Construct sidewalks and pathways associated with the road cross-sections

Construct Country Hills Blvd NE/38 Street NE intersection, including but not limited to the following:

- Construct north approach: Two northbound lanes, dual southbound left turn lanes and a shared southbound through with right turn lane and associated geometric changes
- Construct south approach: One left turn lane, a shared through and right turn lane and associated geometric changes.
- Construct east approach: Dual left turn lanes and associated geometric changes
- Construct west approach: Right turn island (smart right turn) and associated geometric changes
- Construct sidewalks and pathways associated with the road cross-sections.

Construct the roundabout at the intersection of 38 Street and 120 Avenue NE.

At the Initial Tentative Plan associated with Phase 3 Development, a Technical Memorandum is required. The memorandum shall be prepared using updated traffic counts and assumptions on surrounding developments and the supporting roadway network to confirm the capacity available on the regional road network including accommodation of active modes. The memorandum shall confirm the need for and timing of the interchanges at Métis Trail NE /128 Avenue NE and Country Hills Boulevard NE/128 Avenue NE. Subject to confirmation, secure funding for functional planning, detailed design, and construction of the required interchanges and associated roadway.

59. At the Phase 3 initial Tentative Plan stage, equivalent to 3679 residential units, the following regional infrastructure network improvements, and local infrastructure network improvements will be required.
- a. Regional network capital funded infrastructure upgrades required
 - Construct the interchanges at Métis Trail NE/128 Avenue NE and Country Hills Blvd NE/128 Avenue NE, ramps and associated roadways
 - Update the offsite levy bylaw to incorporate the Country Hills BV / Métis Trail NE interchange.
 - Secure funding for design and construction of two interchanges: Country Hills BV / Métis Trail NE; and 128 AV / Métis Trail NE.
 - b. Developer funded Improvements:
 - Construct internal roadways and intersections associated with the phasing

- Construct sidewalks and pathways associated with the roads (cross sections).
 - Construct regional pathways completed to the satisfaction of Development Engineering.
 - Construct 128 Avenue/36 Street/Stone Haven Drive NE, including but not limited to the following:
 - Construct north approach.
 - Construct south approach: Two northbound and two southbound lanes, dual left turn lanes, one free flow right turn lane and associated geometric changes
 - Construct east approach: Add 3rd westbound lane and associated geometric changes
 - Construct west approach: 3rd eastbound through lane. Add free flow right turn lane and associated geometric changes
 - Construct sidewalks and pathways associated with the road cross-sections
60. At the Phase 4 (4 and 4B) initial Tentative Plan stage, equivalent to 3772 Residential Units, the following regional infrastructure network improvements, and local infrastructure network improvements will be required.

Developer funded Improvements:

- a. Construct internal roadways and intersections associated with the phasing.
 - b. Widen 128 Avenue NE to six (6) lanes east of Barlow Trail/ Stone Ridge Way NE
 - c. Construct 128 Avenue/Barlow Trail/Stone Ridge NE, including but not limited to the following:
 - Construct north approach: Improve to dual northbound left turn and one northbound through lane and associated geometric changes
 - d. Construct 128 Avenue/29 Street/Stone Ridge Road NE, including but not limited to the following:
 - Construct east approach: Westbound right turn lane, modifications to right turn island (smart right turn) and associated geometric changes.
 - Construct six (6) lanes along 128 AV NE.
 - e. Construct the Right in Right out intersection design at 40 Street and Country Hills Boulevard NE.
 - f. Construct the roundabout at 32 Street and Iron Grove NE
 - g. Multi-Use pathways and Regional pathways completed to the satisfaction of Development Engineering.
61. In conjunction with the Phase 2 Tentative Plan submission, or otherwise when deemed appropriate by Development Engineering, the Northeast Industrial Area Structure Plan shall be updated to reflect the intention to upgrade the intersections of Métis Trail & 128 AV NE and Métis Trial & Country Hills

Boulevard NE to interchange designs. Note that any proposed Transportation amendments to the Area Structure Plan are subject to the findings of the Northeast Network Planning study.

62. In conjunction with the Phase 2 Tentative Plan submission, or otherwise when deemed appropriate by Development Engineering, a Functional Planning study shall be accepted for the interchange designs at Métis Trail & Country Hills Boulevard NE, and at Métis Trail & 128 Avenue NE the latter including the LRT alignment and land requirements west of the interchange. The Functional Planning study shall be funded by the Developer.
63. In conjunction with the Initial Tentative Plan, or otherwise when deemed appropriate by Development Engineering, the Developer shall construct all necessary off-site transportation network connections to support the subdivision, to the satisfaction of the Manager, Development Engineering.
64. In conjunction with the Initial Tentative Plan, or otherwise when deemed appropriate by Development Engineering, the Developer shall dedicate land for the surface improvements to the satisfaction of the Manager, Development Engineering, as per the Functional Planning study, including but not limited to:
 - a. Dedicate full LRT right-of-way (ROW) for the LRT track infrastructure connecting from the existing LRT ROW.
 - b. Dedicate boundary half of 36 Street NE ROW
 - c. Dedicate ROW for Collectors and Primary Collectors, associated intersections and local widening.
 - d. Dedicate ROW for local widening:
 - Country Hills Boulevard for the Outline Plan area
 - Stone Ridge Road NE
 - 128 Avenue/32 Street NE
 - 128 Avenue/36 Street/Stone Haven Drive NE
 - 36 Street/Stone Spire Drive NE
 - 36 Street/120 Avenue NE
 - 36 Street/114 Avenue NE
 - 36 Street/Country Hills Boulevard NE
 - Country Hills Boulevard/38 Street NE
 - Country Hills Boulevard/40 Street NE
 - Stone Ridge Blvd/Shale Gate/Onyx Place
 - Stone Heaven Drive NE (between 128 Ave NE and Shale Gate NE)
 - Stone Ridge Blvd, between Shale Gate NE and the existing Stone Ridge ROW
 - Stone Ridge Blvd NE/Onyx Green NE (may require turning lane)
 - e. Dedicate 8.0m boulevard along north side of 128 Avenue to accommodate a minimum of 3.0 MUP/Regional pathway
65. In conjunction with the applicable Tentative Plan of subdivision, the Developer shall register a road plan for all roads and intersections within the phase to the satisfaction of the Manager, Development Engineering.

66. In conjunction with the applicable Tentative Plan of subdivision, the developer shall dedicate and construct to its ultimate cross section as a modified primary collector as per the approved outline plan to the satisfaction of the Manager, Development Engineering for the following:
- a. 32 Street NE between 128 Ave NE and Iron Grove NE
 - b. Stone Haven Drive NE between Shale Gate NE and 32 Street NE
 - c. Shale Gate NE between Stone Haven Drive NE and Stone Ridge Blvd NE
 - d. Stone Spire Drive NE between 36 Street NE and Shale Mews NE
 - e. 120 Ave NE between 36 Street NE and 38 Street NE
 - f. 114 Ave NE between 36 Street NE and 38 Street NE
 - g. 38 Street NE between Country Hills Boulevard and 120 Ave NE

The roadways and ancillary works to support the roadways shall be designed and constructed at the Developer's sole expense.

67. In conjunction with the Applicable Tentative Plan of subdivision, the Developer shall dedicate land and construct turn lanes at all intersections as shown on the Outline Plan or identified as part of the Transportation Impact Assessment.
68. In conjunction with the applicable Tentative Plan of subdivision, collector standard roads (and below) shall be built to their full width to the satisfaction of the Manager, Development Engineering.

The roadways, intersections and ancillary works to support the roadways shall be designed and constructed at the Developer's sole expense.

69. In conjunction with each Tentative plan of subdivision, two connections from the affected tentative plan to the Regional Transportation network must be constructed and open to the public.
70. In conjunction with the applicable Tentative Plan of subdivision, Construction Drawings and turning templates shall be submitted and approved to the satisfaction of the Manager, Development Engineering for all roadways within the plan area, as well as boundary roads. Construction drawing review may require changes to proposed right-of-way to meet the approved design.
71. In conjunction with the applicable tentative plan of subdivision, the Developer shall register road plans for Collector standard roadways within the subject lands to the satisfaction of the Manager, Development Engineering, that provide continuous active modes and vehicle routing through the community with at least two points of public access around the Tentative Plan boundary to the arterial road network.

The continuous collector road network is required to ensure that efficient Transit routing through the plan area can be accommodated, while the two points of access to the plan area ensures residents will have two routes into and out of the area, in the event of emergency or road closures, and the ensure availability of capacity at the plan area access points.

72. In conjunction with the applicable tentative plan of subdivision, all roads and intersections within the plan area shall be located, designed, and constructed at the Developer's sole expense to the satisfaction of the Manager, Development Engineering.
73. In conjunction with the applicable Tentative plan(s) of subdivision, intersections shall be designed to the satisfaction of Development Engineering. Local widening and lane transitions may be required on roadways stemming from the intersection. Geometrical adjustments to the roads may require additional right of way.
74. In conjunction with the applicable Tentative plan stage, an appropriate road right of way will be provided for the potential accommodation of Traffic Signals at the following intersections:
- Stone Spire Drive & 38 ST NE.
 - Stone Ridge BV NE & Shale GA NE.
 - 32 ST NE & Shale CO NE.
75. In conjunction with the applicable Tentative Plan of subdivision, the developer shall provide a Letter of Credit for pedestrian-actuated crossing signals that are agreed upon by the developer and the Manager, Development Engineering. Pedestrian-actuated crossing signals shall be considered to the satisfaction of the Manager, Development Engineering:
- a. Stone Ridge Road NE/Public Access Easement for Transit access
 - b. Stone Ridge Way NE and LRT Crossing
 - c. Approach legs at Iron Grove NE and 32 Street Roundabout
 - d. Stone Ridge Drive NE and 38 Street/Onyx Link NE
 - e. Stone Spire Drive NE and 38 Street NE
 - f. 38 Street NE and Iron Heath NE
 - g. 38 Street NE and 120 Ave NE
 - h. Approach legs at 120 Ave NE and 38 Street NE Roundabout
 - i. Other locations as determined at the Tentative plan stage.
- The Developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit, and is required to submit payment in support of the proposed Tentative Plan applications.
76. In conjunction with the applicable tentative plan of subdivision that proposes midblock crossings, the Developer shall design and construct infrastructure at its sole expense, as follows:
- Curb bump outs shall be built, at a minimum, at all proposed mid-block crossing locations to the satisfaction of the Director, Transportation Planning.
 - Approach grades for all proposed mid-block crossing locations shall be no more than 4 per cent.
77. Prior to approval of the applicable Tentative Plan of subdivision, the design and right-of-way requirements for roundabouts shall be designed and constructed to the satisfaction of the Manager, Development Engineering. Submit scaled

- (1:500) drawings showing the geometry and vehicle templating of all proposed roundabouts. The design requirements shall include, but not be limited to:
- a. Bus stops adjacent to roundabouts should be located outside the curb flares influence zone and along the curb lanes where there is on-street parking;
 - b. A fastest path analysis.
 - c. A sight line analysis for roundabouts
 - d. Truck and transit sweep paths through all roundabouts (including articulated buses). Transit buses shall not be required to mount the central truck apron in order to navigate the roundabout.
 - e. The inscribed circle diameter (ICD).
 - f. Sight lines through centre of circle must be clear (i.e. no objects to block vehicle's sight lines), and chevron patterns shall be embedded upon the concrete pad on the inner side of the roundabout.
 - g. All roundabouts in the plan area shall include bike ramps to facilitate cycling access through the roundabouts.
 - h. No accesses are permitted within the functional area of roundabouts.
78. At the applicable Tentative Plan stage(s) of subdivision, Active Modes and Pathways will be provided to the satisfaction of Development Engineering. These will include (but may not be limited to):
- a. Plans shall indicate the regional pathway along the south boulevard of 128 AV NE, including a future connection toward the east.
 - b. Plans shall indicate the regional pathway on the west side of Métis Trail NE.
 - c. A regional pathway is required along 120 Ave east of 36 Street NE and a pathway connection to Métis Trail along the south side of M-1 site.
 - d. To the satisfaction of Development Engineering, provide a 3.0m pathway alignment from 38 ST NE to 40 ST NE (in lieu of the previously proposed 114 AV NE connection).
 - e. Pathways connecting near corner knuckles may be located away from the knuckle, with street crossings designed to the satisfaction of Development Engineering.
 - f. Indicate 3.0m Multi-use pathways on both sides of Stone Haven Drive between Shale Gate NE and 32 Street NE.
79. In conjunction with the applicable Tentative plan, the First access/intersection along collectors, primary collectors (RI/RO, all turns, limited turns, etc.) shall be located 120m from the arterial streets or otherwise located to the satisfaction of the manager of Development Engineering.
80. At the applicable Tentative Plan stage(s) of subdivision, confirm that the residential lane located north of the Future Regional Retail site does not connect to Stone Ridge BV NE. To avoid a dead end, the lane may include a turnaround or a connection to Shale Gate NE (to shorten the dead-end portion of the lane).
81. In conjunction with the applicable Tentative Plan, curb extensions to be designed and constructed at the Developer's sole expense and to the satisfaction of the Manager, Development Engineering. Curb extensions are required at all mid-block crossings, including those of residential streets, along collector roadways to replace parking lanes, and wherever possible at all other intersection crossings.

82. In conjunction with the applicable tentative plan of subdivision, 3.0m residential lane corner cuts shall be provided from parcels adjacent to lane and street intersections.
83. Vehicular access to the future LRT station shall be provided to the satisfaction of Development Engineering. At the applicable Tentative Plan stage, a public roadway connector from Stone Ridge Boulevard NE to the S-CRI site (LRT station) will be investigated.
84. In conjunction with the applicable tentative plan of subdivision, Stone Ridge BV NE and its intersection with Iron Crescent NE shall be designed to accommodate Transit buses and their turning movements. TAC standard turning templates will be required to confirm an appropriate design. Although a standard Transit route is not currently planned on Stone Ridge BV NE, it may be needed as an alternate route under certain circumstances.
85. In conjunction with the applicable tentative plan of subdivision, the Developer shall provide signage within the road right-of-way or on city public land, indicating specific accesses to the plan area which are interim/temporary in nature and subject to change as phasing progresses. Signage shall be designed and located to the satisfaction of the Manager, Development Engineering. All work will be at the Developer's expense.
86. At each tentative plan of subdivision, if the developer intends to use any road subject to load restrictions (as indicated on the City of Calgary Load Bans Map), they must obtain a valid load ban permit before using that road. For more details, please refer to the link below: <https://www.calgary.ca/roads/permits/overweight-load-bans.html>
87. In conjunction with the applicable tentative plan of subdivision, public access easement agreement will be required to be registered for all private roads located in subject lands. All private roads must conform to City of Calgary design standards. Public access easement will also be required for pathways located on private parcels.
88. At the applicable Tentative plan stage(s) of subdivision, turning templates will be required to confirm the proposed geometry for corner knuckles on roads with the plan area. Adjustments to the road rights of way may be required in response to this investigation.
89. In conjunction with the Applicable Tentative Plan of subdivision, Transit routes and bus stop pad locations will be finalised. Where the approved Outline plan disagrees with the Tentative plan, the Tentative plan shall prevail. We note that no Transit routes are currently planned in the area east of 36 ST and south of 128 AV NE.
90. In conjunction with the Applicable Tentative Plan of subdivision, roadways and intersections adjacent to the school site shall be designed to accommodate Transit buses and their turning movements. These roads include Stone Spire DR NE, 38 ST NE, 40 ST NE, and 120 AV NE. Although a Transit route is not

currently planned adjacent to the school, the geometry of these roads including intersections must be designed for potential Transit access.

91. At the applicable Tentative plan stage, Stone Ridge BV NE and its intersection with Iron Crescent NE shall be designed to accommodate Transit buses and their turning movements. TAC standard turning templates will be required to confirm an appropriate design. Although a standard Transit route is not currently planned on Stone Ridge BV NE, It may be needed as an alternate route under certain circumstances.
92. In conjunction with each Tentative Plan for subdivision, Transit service shall be provided to the satisfaction of the Director, Transit and the Manager, Development Engineering.
93. In conjunction with the Applicable Tentative Plan of subdivision or Development Permit for the staged construction of the road network, transit zone locations shall be confirmed as per the Outline plan or otherwise provided to the satisfaction of the Manager, Development Engineering. Transit zones are typically located:
 - a. Where commercial areas are concentrated;
 - b. To service High schools and post-secondary institutions.
 - c. Where the grades and site lines are compatible to install bus zones; and
 - d. Where pedestrian walkways, pathways, and roadway crossing opportunities are provided.
94. In conjunction with the applicable Development Permit for multi-family or commercial sites, Transit shelter(s) shall be provided as identified by the Manager, Development Engineering and shall be supplied and installed at the Developer's sole expense. The shelter(s) shall be installed by Transit upon receipt of satisfactory payment.
95. In conjunction with each Tentative Plan for subdivision, all pedestrian walkways identified for achieving Transit walking distances shall be 3 meters wide and be paved and lighted.
96. In conjunction with the Applicable Tentative Plan of subdivision, a restrictive covenant shall be registered against the specific lot(s) identified by the Manager, Development Engineering concurrent with the final instrument prohibiting the construction of front driveways over the bus loading area(s).
97. In conjunction with each Tentative Plan for subdivision, lane-less lots adjacent to transit loading areas shall be a minimum width of 11.0m to avoid conflicts with front driveways
98. In conjunction with each Tentative Plan for subdivision, Custom cross-sections shall be designed to meet or exceed the standards of the Design Guidelines for Subdivision Servicing.
99. In conjunction with the applicable Tentative Plan of subdivision, the cross-sections approved as part of the Outline Plan shall apply. However, if new

standards or priorities are introduced by the City prior to or during subsequent tentative plan submissions, the approved cross-sections may be required to be modified to meet the standards of the day.

100. In conjunction with the applicable Tentative Plan of subdivision, provide cross-sections for streets west of 36 ST NE (Ronmor lands). These cross sections may inform how the intersections are designed in the future.
101. In conjunction with the applicable Tentative Plan of subdivision, indicate that the 8.5m pavement width associated with Cross section `A' will only be applied to streets adjacent to lane-less lots with front driveways.
102. In conjunction with the applicable Tentative Plan of subdivision indicate that the 9.0m pavement width associated with Cross section `B' will only be applied to streets where the adjacent lots also abut a rear lane.
103. In conjunction with the applicable Tentative Plan of subdivision, indicate the following with respect to Cross section `S':
 - This road is an existing Arterial, not a Skeletal Road.
 - The existing cross section is 4 lanes not 6 lanes. Six lanes is a future condition.
 - The Transportation Impact Assessment recommends a 6 lane cross section, not an 8 lane cross section.
 - Add 3.0m Regional pathway on both sides as per the 5A pathway network (Always Available for All Ages and Abilities). The pathway location shall comply with clear zone requirements in the Alberta Transportation Roadside Design Guide.
104. In conjunction with the applicable Tentative Plan of subdivision, indicate the following with respect to Cross section `T':
 - The existing cross section is 4 lanes not 6 lanes.
 - The cross-section shows 3.0 m regional pathway both sides of 128 AV NE.
 - The south side pathway would be 'new' or 'proposed' given the current condition, which is a 1.4m separate sidewalk.
 - The north side pathway would be 'modified' or proposed as the current width varies between 2.5m wide and 3.5m wide.
105. In conjunction with the applicable Tentative Plan of subdivision, indicate the following with respect to Cross section `U,' adjacent to the site boundary:
 - A minimum boulevard width of 8.0m.
 - A minimum 2.5m Regional Pathway on the north boulevard per the 5A pathway network (Always Available for All Ages and Abilities).
 - A minimum 3.5m travel lane width per the Arterial classification in the Design Guidelines for Subdivision Servicing.
106. In conjunction with the applicable Tentative Plan of subdivision stage(s) add cross-sections where left-turn lanes are proposed and local widening is required, in accordance with the Conceptual Functional Plans. For example, cross

sections are required along 38 Street NE immediately north of Country Hills Boulevard. Additional right of way may be required, both on the north and south sides, to retain and match the existing median.

107. In conjunction with the applicable Tentative Plan of subdivision, provide a cross-section north of 128 Ave along Stone Ridge Blvd NE.
108. In conjunction with the applicable Tentative Plan of subdivision or Development Permit, access to each parcel within the subject lands shall be located and designed to the satisfaction of the Manager, Development Engineering.
109. In conjunction with the applicable Tentative Plan of subdivision, for low density residential lots adjacent to a lane, register a restrictive covenant to prevent vehicular access to the street.
110. In conjunction with the applicable Tentative Plan of subdivision, or Development Permit, access from 36 ST to the C-N1 parcel shall be limited to Right in access only. Transportation analysis will be required to support Right in / Right out access.
111. In conjunction with the applicable Tentative Plan of subdivision, provide an access easement agreement along the shared property line between the S-CRI parcel and the MU-1 parcel accessing Shale Court NE.
112. In conjunction with the applicable Tentative Plan of subdivision, no vehicular access shall be permitted to collector (and higher order) roadways for low density residential lots. Vehicular access shall be provided via rear lanes. Restrictive covenant shall be registered on all applicable titles concurrent with the registration of the final instrument.
113. In conjunction with the applicable Tentative Plan of subdivision, no vehicular access shall be permitted to residential streets with lane products. Vehicular access shall be provided via rear lanes. Restrictive covenant shall be registered on all applicable titles concurrent with the registration of the final instrument.
114. In conjunction with the applicable Tentative Plan of subdivision or Development Permit, no vehicular access shall be permitted from parcels to the following roadways:
 - a. Stoney Trail NE
 - b. Métis Trail NE
 - c. Country Hills BV NE
 - d. 128 Avenue NE other than the intersections and RI/RO shown on the OP
 - e. 36 Street NE other than the intersections and RI/RO shown on the OP
 - f. 32 Street other than access locations shown on the OP
 - g. Stone Haven Drive NE other than the RI/RO shown on the OP
 - h. Stone Ridge Blvd NE other than the intersections and RI/RO shown on the OP

A restrictive covenant regarding this access restriction shall be registered concurrent with the registration of the legal plan of subdivision.

115. In conjunction with the applicable tentative plan of subdivision, a restrictive covenant shall be registered against the specific lot(s) identified by the Manager, Development Engineering concurrent with the legal plan of subdivision prohibiting the construction of front driveways over the bus loading area(s).
116. At the applicable Tentative plan stage of subdivision or Development Permit stage, all turns vehicular access to the C-R3 sites on 114 AV NE shall be located a minimum of 120m from 36 ST NE.
117. At the applicable Tentative plan stage of subdivision or Development permit stage, vehicular access for the C-C2 lot between 120 Ave and Stone Spire Drive NE shall meet the following parameters:
- RI/RO access shall be located 120m from 36 Street arterial.
 - All turn access immediately east of the RI/RO does not meet the access spacing, review and locate the access in accordance with the access standards. Removal of the RI/RO access may be required.
 - Provide a cross section for the private street, meeting the intention of a complete street design.
 - A public access easement will be required for the private street.
118. At the applicable Tentative Plan stage of subdivision or Development Permit stage, for the MU-1 site located adjacent to Slate Green NE and Shale Mews NE, vehicular access shall be taken to Shale Mews NE (Collector), not Slate Green NE (Residential). A restrictive covenant regarding this access restriction shall be registered concurrent with the registration of the legal plan of subdivision.
119. At the applicable Tentative Plan stage of subdivision and Development Permit stage, vehicular access to the 'Future Regional Retail' parcel, east of Stone Haven DR NE, shall meet the following parameters:
- Access arrows to this site appearing on the Outline plan should be considered 'potential access locations.'
 - Access from Shale Gate NE shall be received with a road stub design, allowing for potential signalization.
 - All turns access from Shale Gate NE will require Transportation analysis to consider traffic signals, intersection geometry, and associated right of way requirements.
 - All turns access from Stone Haven DR NE shall be considered, in conjunction with Transportation analysis for the traffic signals, intersection location and geometry, and associated right of way requirements.
120. At the applicable Tentative Plan stage of subdivision or Development Permit stage, vehicular access to the M-G parcel north of Iron Link NE and west of Stone Ridge BV NE shall be designed, located and constructed to the satisfaction of Development Engineering. All turns access from Stone Ridge BV NE is available in alignment with Slate Heath NE. Conversion of Stone Ridge BV NE to a Collector standard will be investigated to support this access configuration.

121. At the applicable Tentative Plan stage of subdivision or Development Permit stage, vehicular access to the M-1 parcel north of Shale Court NE/Stone Heaven Drive NE and west of Slate Manor NE shall be designed, located and constructed to the satisfaction of Development Engineering. Since all turns access should be taken to a Collector standard road, conversion of Slate Manor NE to a Collector standard will be investigated to support this access configuration.
122. In conjunction with the Applicable Tentative Plan of subdivision and associated Construction Drawing submission, driveways shall not conflict with crosswalks/wheelchair ramp locations.
123. In conjunction with the Initial Tentative Plan of subdivision, the Transportation/Utility Corridor (TUC) shall be permanently and prominently signed in accordance with City Council's policy and it shall also be clearly identified on the Land Use Sign for the area.
124. In conjunction with the applicable Tentative Plan of subdivision or Development Permit, a noise attenuation study is required for any residential development adjacent to Arterial roadways and the LRT alignment. Arterial roads to consider include Métis Trail NE, 128 AV NE, Stoney Trail NE, and 36 ST NE. The study is to be completed by certified by a Professional Engineer with expertise in the subject of acoustics related to land use planning and submitted to Mobility Engineering for approval. Note that where sound attenuation is not required adjacent to Arterial roadways, a uniform screening fence shall be provided, in accordance with the 2020 Design Guidelines for Subdivision Servicing.
125. At the applicable Tentative Plan of subdivision stages, all noise attenuation features (noise walls, berms, etc.), screening fence, and ancillary facilities required in support of the development shall be constructed entirely within the development boundary (location of noise walls, berms, screening fence, etc) and associated ancillary works shall not infringe onto the road rights-of-way. Noise attenuation features and screening fences shall be at the Developer's sole expense.
126. In conjunction with the applicable Tentative Plan of subdivision, the Developer shall enter into a Development Agreement for the offsite surface improvements as necessary to service the proposed development to the satisfaction of the Approving Authority.
127. At each tentative plan of subdivision, if the developer intends to use any road subject to load restrictions (as indicated on the City of Calgary Load Bans Map), they must obtain a valid load ban permit before using that road. For more details, please refer to the link below: <https://www.calgary.ca/roads/permits/overweight-load-bans.html>
128. Prior to approval of Construction Drawings and Permissions to Construct Surface improvements: The developer shall provide signed copies of back sloping agreements for any back sloping that is to take place on adjacent lands (owned privately or owned by the City).

129. In conjunction with each Tentative Plan of subdivision, the Developer shall demonstrate that the plan area provides contiguous extension of development with the Outline Plan area, to the satisfaction of the Manager, Development Engineering. The intent is to ensure transportation connectivity for all modes within and adjacent to the plan area, as well as facilitate transit routing.
130. In conjunction with the applicable Tentative Plan of subdivision or Development Permit, the Developer shall register public access easement agreements for pedestrian access through lands as indicated on the Outline Plan, to the satisfaction of the Manager, Development Engineering.
131. In conjunction with the applicable Tentative Plan of subdivision, curb extensions to be designed and constructed at the Developer's sole expense and to the satisfaction of the Manager, Development Engineering. Curb extensions are required at all mid-block crossings, including those of residential streets, along collector roadways to replace parking lanes, and wherever possible at all other intersection crossings.
132. In conjunction with the applicable Tentative Plan of subdivision or Development Permit, all community entrance features must be located outside the public right-of-way.
133. In conjunction with the applicable Tentative Plan of subdivision, no retaining walls, stairs or similar structures will be permitted in road right of way.
134. In conjunction with the applicable tentative plan of subdivision, graveled and oiled turnarounds are required for all temporary dead-end streets. Post and cable fence is required where the temporary turnaround is anticipated to be required for a period greater than 1 year. Temporary oil and gravel bus turnaround / cul-de-sac with a minimum radius of 15.25 meters is required at the terminus of each construction phase. Where the developer intends to fence the turnaround, the minimum radius shall be increased to 16.25 meters. If road construction and/or construction phasing affects the operations of transit service, the Developer is required to provide an interim transit route replacement, to the satisfaction of the Director, Transit and the Manager, Development Engineering.
135. In conjunction with the applicable Tentative Plan, the Developer shall ensure that all pathways from MR lands have direct pedestrian connection by sidewalk or pathway to crossing locations at intersections to the satisfaction of the Manager, Development Engineering.

Applicant Submission



Applicant's Submission

January 26, 2026

On behalf of Qualico Communities, B&A has submitted an outline plan and land use amendment application for 194.81 hectares \pm (481.38 acres \pm) of land located in the northeast community of Stonegate Landing. This plan introduces 2 residential neighbourhoods and provides for commercial and industrial land uses in high demand locations.

The land is currently designated a mixture of industrial and commercial land use districts with the commercial land uses concentrated at the intersection of Metis Trail NE and 128 Avenue NE. The City of Calgary invested over \$90 million infrastructure delivering services to Stonegate but the lands have remained underutilized due to a lack of demand for product type. The 2021 revisions to the Noise Exposure Forecast (NEF) Contours in the Airport Vicinity Protection Area have opened up a portion of this land to residential uses and present an opportunity for an updated vision for the area and to provide additional housing supply to the northeast sector. The reimagined development plan unlocks the investment in the lands and provides for a complete community in a highly sought-after area. City Council approved amendments to the MDP and ASP in March 2025 to allow for the industrial to residential conversion. Additional ASP Amendments are proposed with this application to align with the proposed outline plan and land use amendment.

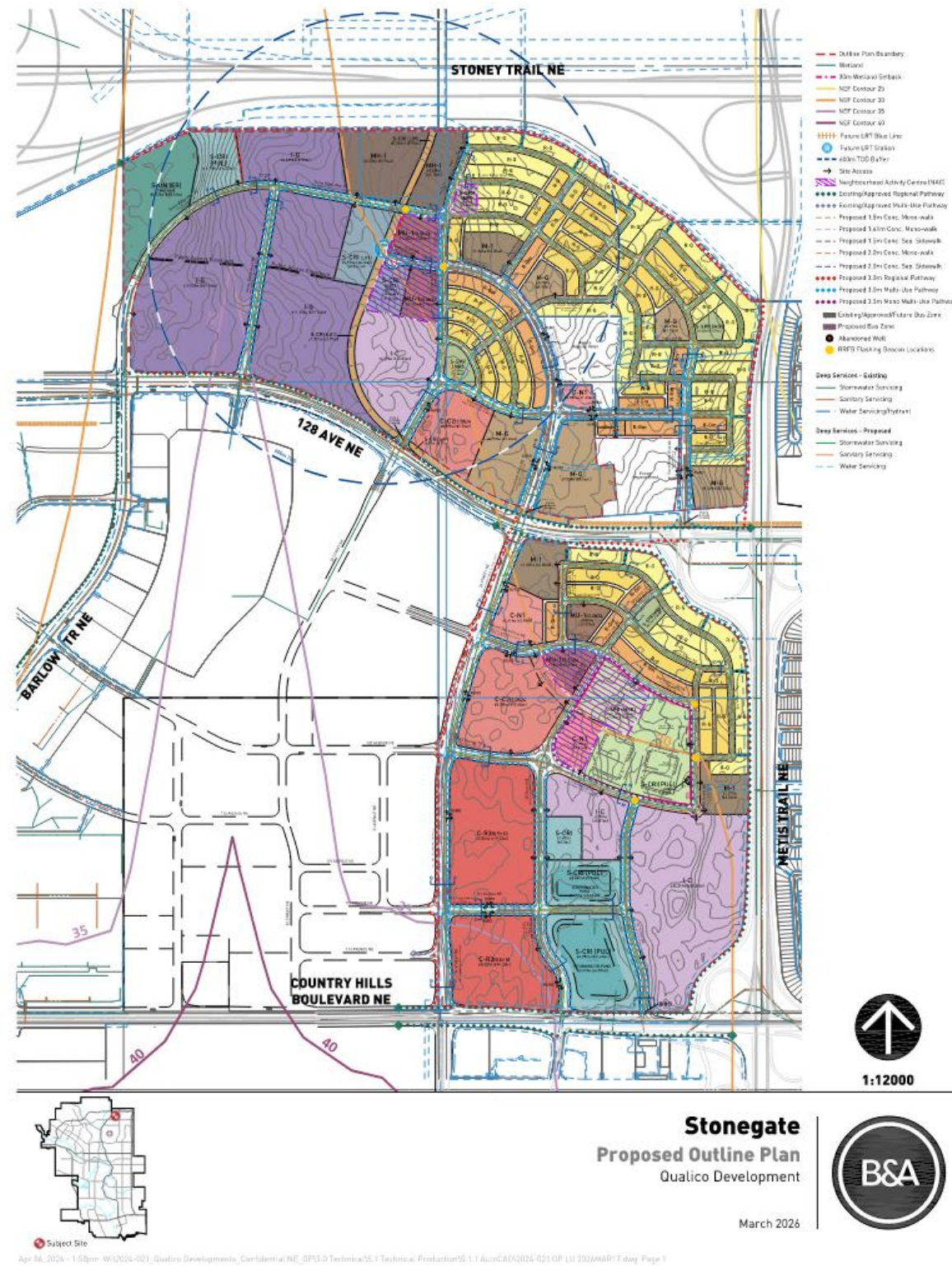
This application maintains industrial land uses in the northwest and southeast portions of the plan area and moves the regional commercial land uses south to provide better access to Country Hills Boulevard NE and a continuation of the existing commercial corridor. Residential land uses have been introduced to the plan area where they are now permitted to be located and appropriately positioned to leverage the future LRT.

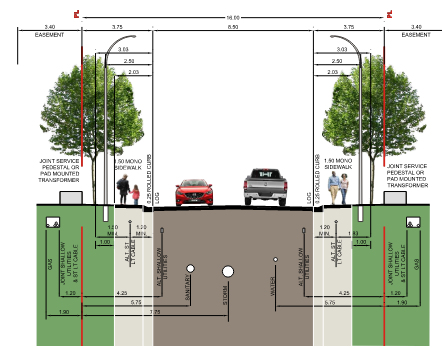
Two distinct residential neighbourhoods are being proposed, each with their own unique sense of place but integrated to provide a unified complete community. The northern neighbourhood includes features such as direct access to the Rotary Mattamy Greenway and the future LRT station, local commercial amenities and a variety of open spaces. The southern neighbourhood will provide for a neighbourhood commercial centre, a main street and a site for two future schools. A variety of residential land use districts have been included throughout to provide for a variety of housing types in the community. This is a unique opportunity to develop a complete community where residents can live, work and play.

Qualico is seeking support of the land use amendment, outline plan and ASP amendments to allow for a unique opportunity to provide a complete community where residents can live, work and play. The proposed development enhances and activates the adjacent industrial lands, supports the future LRT station, contributes additional residential housing supply to the northeast sector and incorporates new parks and open spaces, including a site for two future schools.

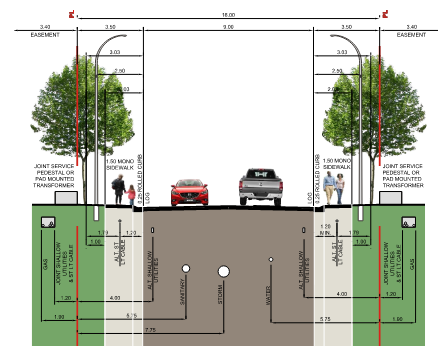
Proposed Outline Plan

Calgary Planning Commission is the Approving Authority for the Outline Plan. Attachment for Council's reference only.

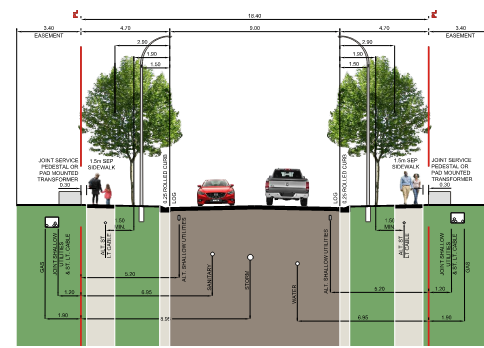




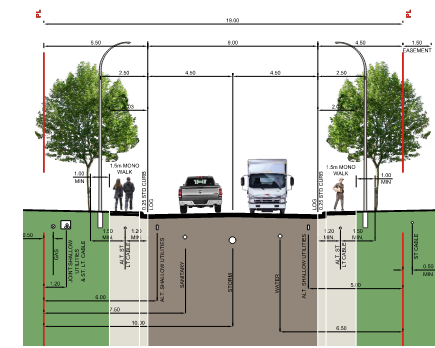
A - MODIFIED RESIDENTIAL STREET "M" (8.5m / 16.0m)
SCALE - 1:200



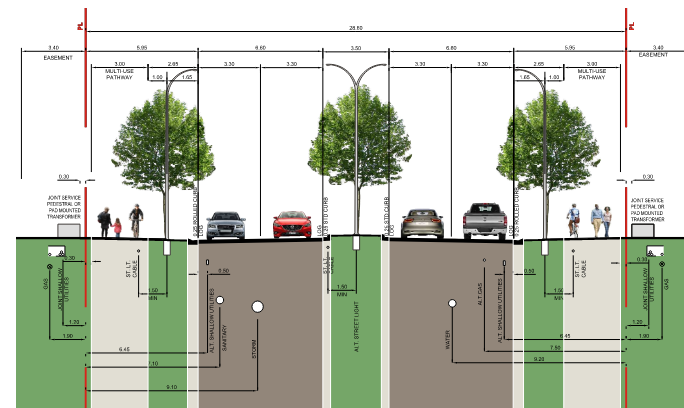
B - MODIFIED RESIDENTIAL STREET - "M-L" (9.0m / 16.0m)
SCALE - 1:200



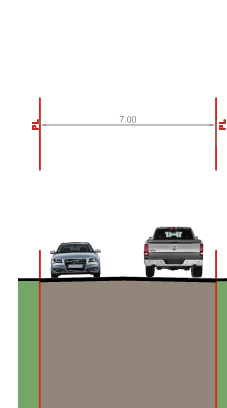
C - MODIFIED RESIDENTIAL STREET - "SW-L"
(9.0m / 18.4m)
SCALE - 1:200



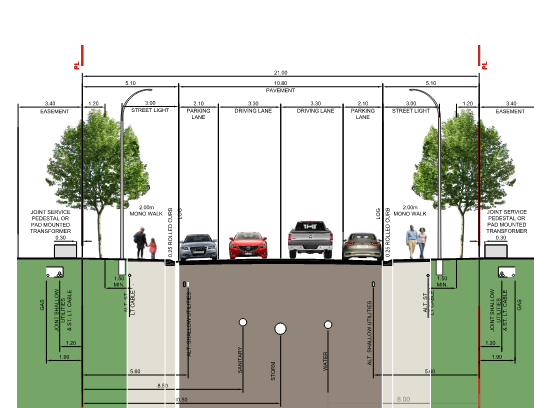
D - MODIFIED INDUSTRIAL STREET (9.0m / 19.0m)
SCALE - 1:200



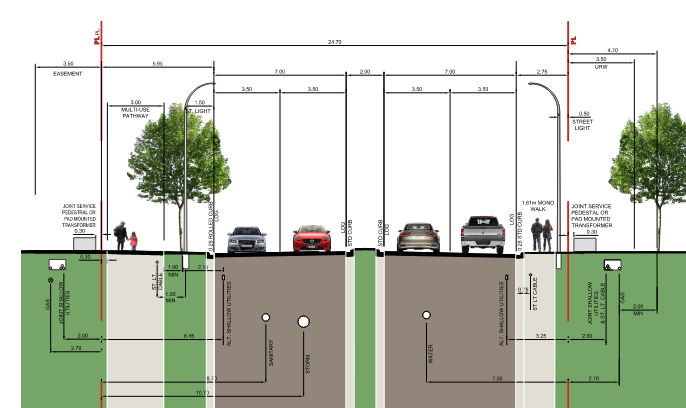
E - MODIFIED PRIMARY COLLECTOR STREET (206.6m / 28.6m)
SCALE - 1:200



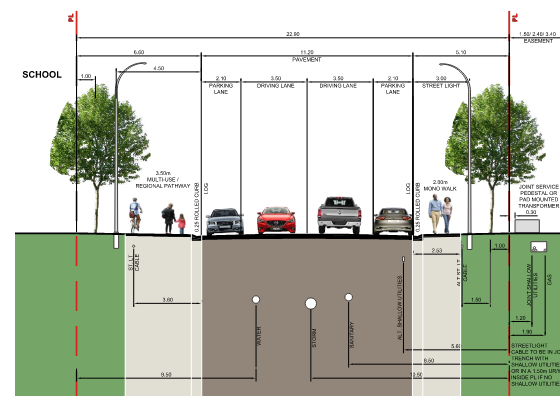
F - LANE (7.0m)
SCALE - 1:150



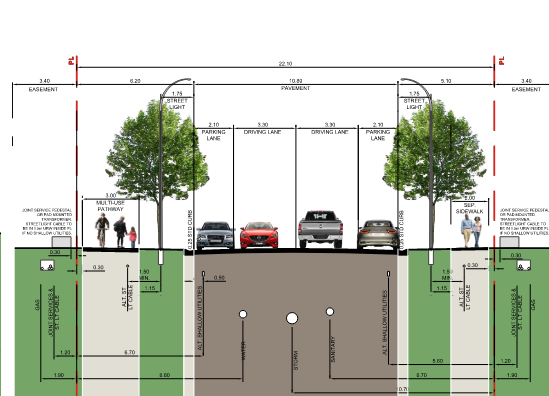
G - MODIFIED COLLECTOR STREET (10.8m / 21.0m)
SCALE - 1:200



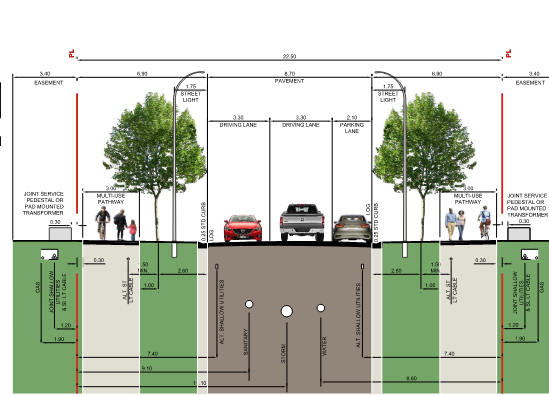
H - MODIFIED PRIMARY COLLECTOR (207.0m / 24.7m)
SCALE - 1:200



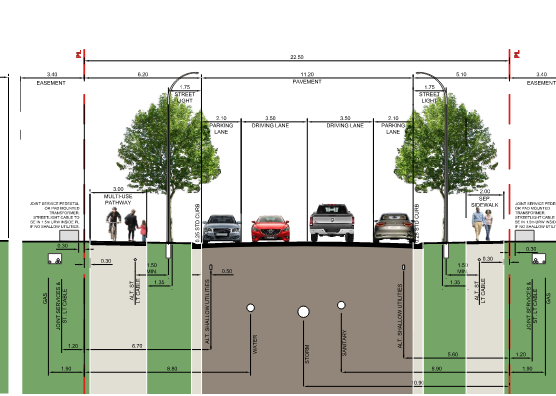
I - MODIFIED COLLECTOR STREET (11.2m / 22.9m)
SCALE - 1:200



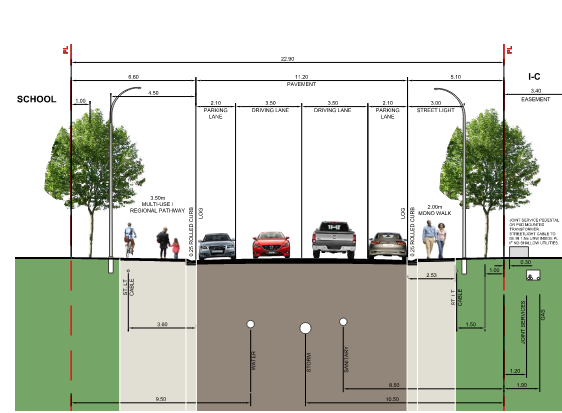
J - MODIFIED COLLECTOR STREET (10.8m / 22.1m)
SCALE - 1:200



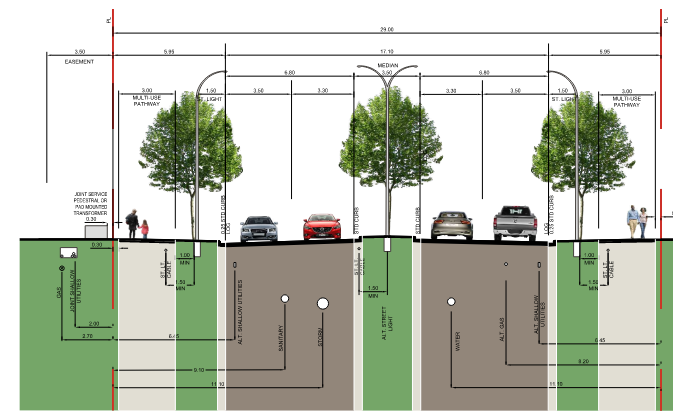
K - MODIFIED COLLECTOR STREET (8.7m / 22.5m)
SCALE - 1:200



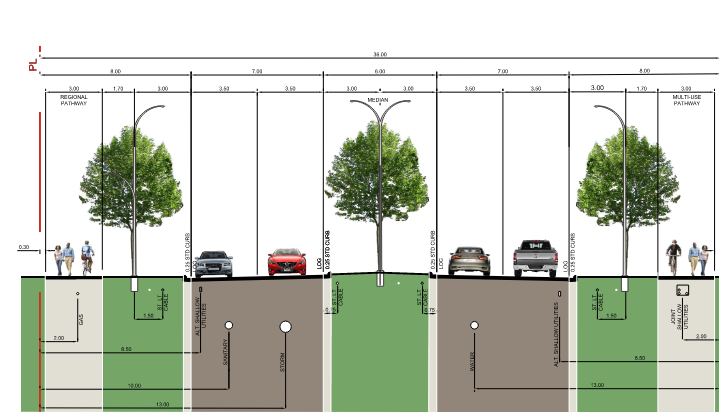
L - MODIFIED COLLECTOR STREET (11.2m / 22.5m)
SCALE - 1:200



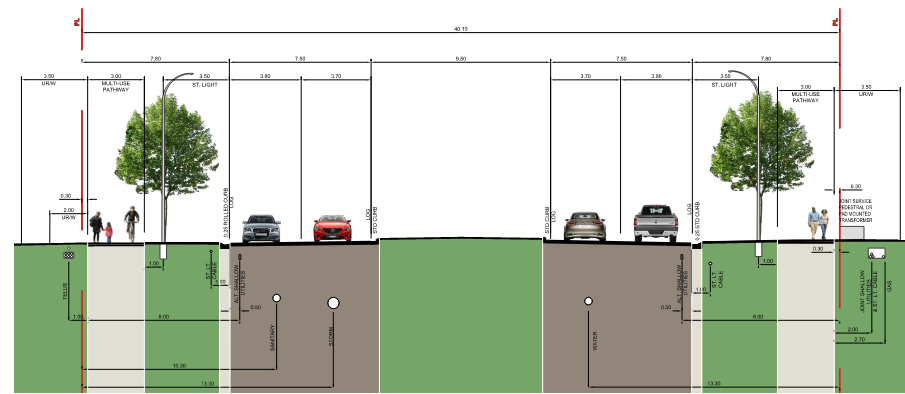
M - MODIFIED COLLECTOR STREET (11.20m / 22.9m)
SCALE - 1:200



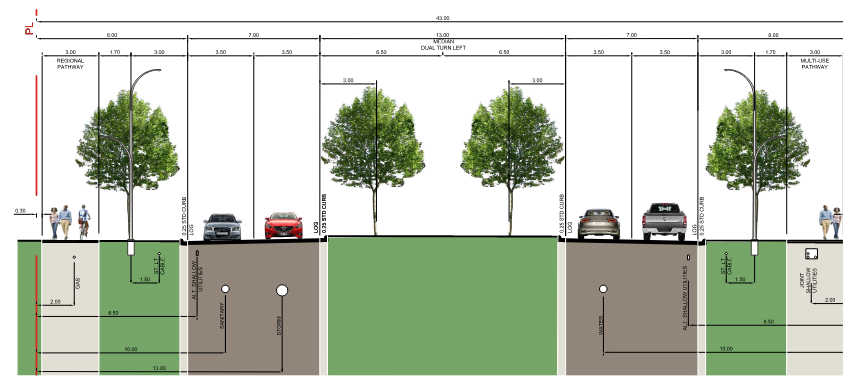
N - MODIFIED LOCAL ARTERIAL STREET (206.8m / 29.0m)
SCALE - 1:200



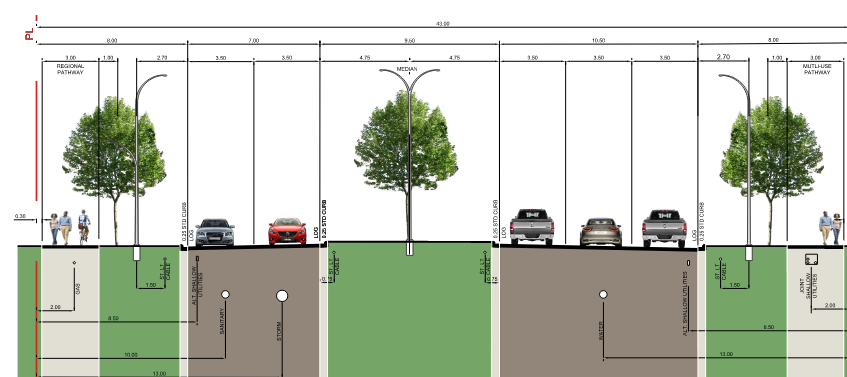
O - MODIFIED ARTERIAL STREET - 4 LANE (207.0m / 36.0m) (APPROVED LOC2018-0145)
(36 STREET NE)
SCALE - 1:200



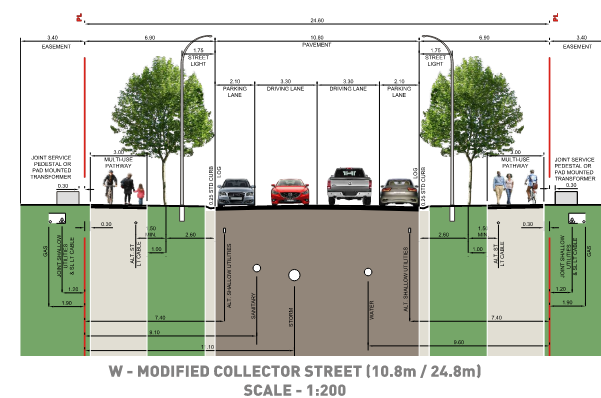
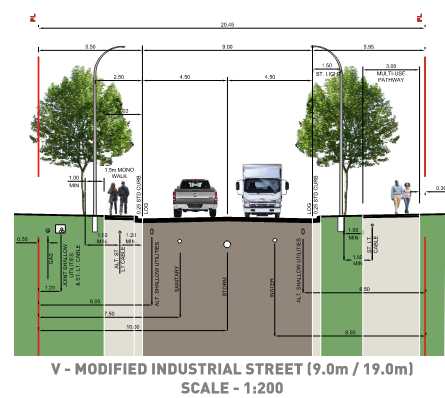
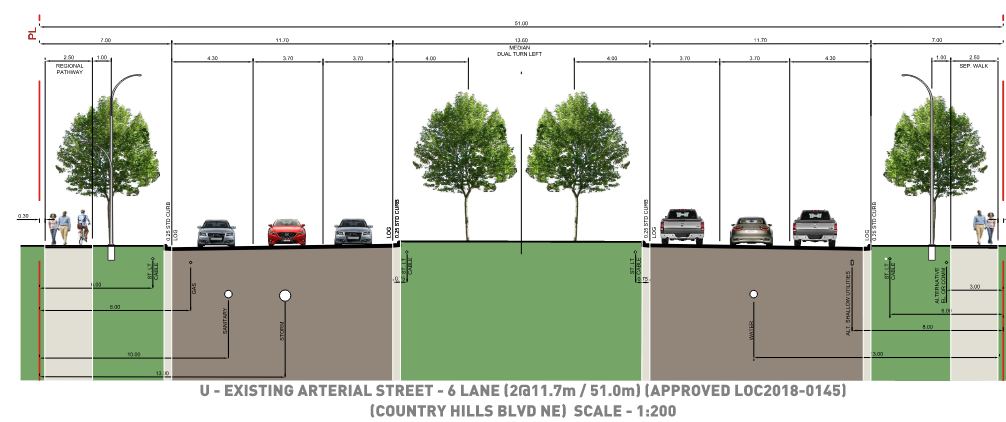
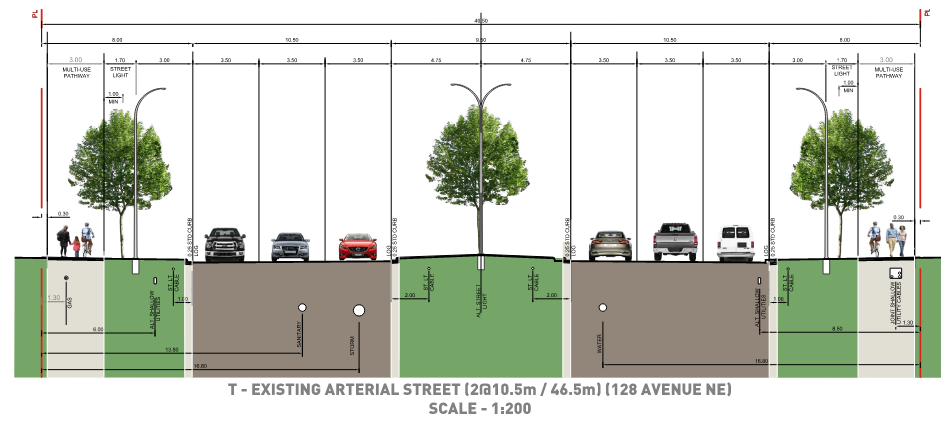
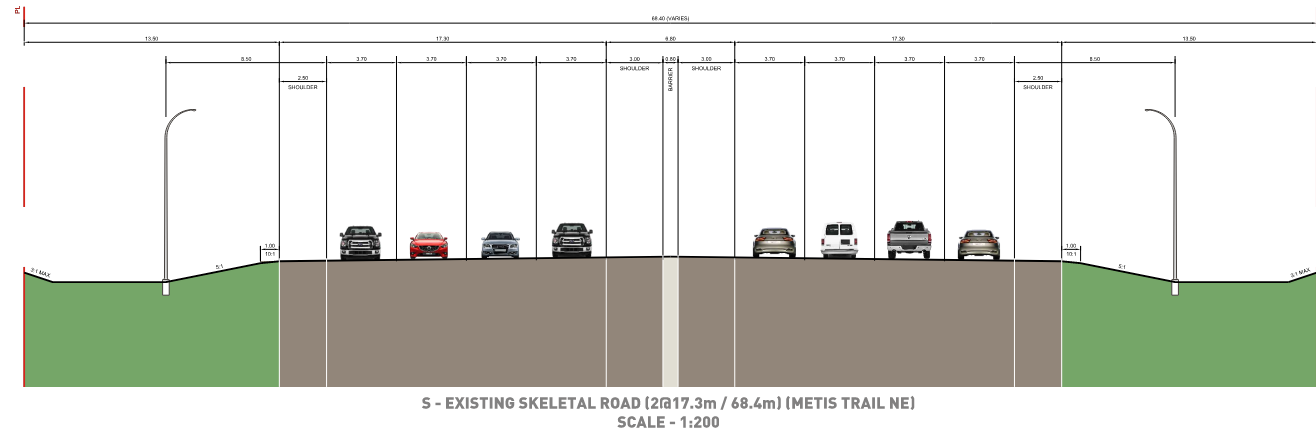
P - MODIFIED ARTERIAL STREET (207.5m / 40.1m)
SCALE - 1:200



Q - MODIFIED ARTERIAL STREET - 4 LANE w/ SLOTTED DUAL LEFT TURN (207.0m / 43.0m)
(APPROVED LOC2018-0145) (36 STREET NE)
SCALE - 1:200



R - MODIFIED ARTERIAL STREET - 5 LANE (207.0m & 10.5m / 43.0m)
(36 STREET NE) SCALE - 1:200



OUTLINE PLAN STATISTICS							
		Hectares (±)	Acres (±)	% of GDA			
Residential Area		91.36	225.75				
Industrial Area		103.45	255.63				
<i>less</i>							
S-UN(ER)	Wetland	4.22	10.42				
Gross Developable Area (GDA)		190.59	470.96	100%			
Land Use		Lot Width (m) / units per acre (upa)	Frontage (m)	Hectares (±)	Acres (±)	No. of Units	% of GDA
Residential				56.41	139.39		29.60%
R-G	Residential - Low Density Mixed Housing District			30.21	74.64		
	Anticipated number of laned lots based on 7.8m lot width	7.80 m	3,547.78 m	12.10	29.91	455 units	
	Anticipated number of laneless lots based on 10.0m lot width	10.00 m	4,853.16 m	18.10	44.73	485 units	
						940 units	
	<i>Maximum number of lots based on 6.0m lot width</i>	<i>6.00 m</i>				<i>1400 units</i>	
R-Gm	Residential - Low Density Mixed Housing District			5.64	13.93		
	Anticipated number of lots based on 6.1m lot width	6.10 m	1,642.91 m			269 units	
	<i>Maximum number of lots based on 6.0m lot width</i>	<i>6.00 m</i>				<i>274 units</i>	
M-G	Multi-Residential - At Grade Housing			9.41	23.24		
	Anticipated number of units based on 20upa	20 upa				465 units	
	<i>Maximum number of units based on 32upa</i>	<i>32 upa</i>				<i>744 units</i>	
M-1	Multi-Residential - Low Profile			4.17	10.31		
	Anticipated number of units based on 25upa	25 upa				258 units	
	<i>Maximum number of units based on 60upa</i>	<i>60 upa</i>				<i>619 units</i>	
M-H1	Multi-Residential - High Density Low Rise			4.91	12.14		
	Anticipated number of units based on 65upa	65 upa				789 units	
	<i>Maximum number of units based on 150upa</i>	<i>150 upa</i>				<i>1821 units</i>	
MU-1f3.5h24	Mixed Use - General District (50% Residential)			2.07	5.13		
	Anticipated number of units based on 50upa	50 upa				256 units	
	<i>Maximum number of units based on 148upa</i>	<i>148 upa</i>				<i>759 units</i>	
Total Frontage		10,044 m					
Total Units		Anticipated				2977 units	
		<i>Maximum</i>				<i>5616 units</i>	
Density (Residential Area)		Anticipated		32.6 upha	13.2 upa		
		<i>Maximum</i>		<i>61.5 upha</i>	<i>24.9 upa</i>		
Density (Overall)		Anticipated		32.6 upha	6.3 upa		
		<i>Maximum</i>		<i>61.5 upha</i>	<i>11.9 upa</i>		
Retail/Mixed Use				27.30	67.47	14.32%	
MU-1f3.5h24	Mixed Use - General District (50% Commercial)			2.07	5.13		
C-N1	Commercial - Neighbourhood 1 District			3.28	8.11		
C-C2f2.0h24	Commercial - Community 2 District			8.41	20.79		
C-R3f0.5h18	Commercial - Regional 3 District			13.53	33.44		
Industrial				46.69	115.36	24.69%	
I-G	Industrial - Business District			26.90	66.48		
I-C	Commercial District			19.78	48.88		
Public Service				14.30	35.33	7.50%	
S-CRI	Fire Station			1.01	2.50		
S-CRI(LRT)	Future LRT Area			4.56	11.26		
S-CRI(PUL)	5.0m Setback from Well			0.02	0.06		
S-CRI(PUL)	Ponds & Around Area/PUL			8.70	21.51		
Open Space				19.059	47.096	10.00%	
S-SPR(MR)	Special Purpose - School, Park and Community			1.825	4.510		
S-SPR(MR)	Cash in Lieu (To be Provided)			4.065	10.045		
S-SPR(MR)	Cash in Lieu (Previously Provided)			5.917	14.621		
S-SPR(MSR)	Special Purpose - School, Park and Community			7.252	17.920		
Roadways & Lanes				26.84	66.32	14.08%	

LAND USE STATISTICS			
FROM	TO	AREA	
		ha (±)	ac (±)
C-R3 f0.23h18		1.32	3.26
I-B f0.5h27	M-1	0.20	0.49
I-G		3.48	8.61
C-R3 f0.23h18		9.91	24.48
C-R3 f0.50h18	M-G	1.48	3.66
C-R3 f0.23h18		2.01	4.97
S-CRI	M-H1	0.08	0.20
I-G		3.29	8.13
C-R3 f0.23h18		0.64	1.57
I-B f0.5h27	MU-1	2.36	5.82
S-CRI	f3.5h24	0.08	0.19
I-G		2.99	7.40
C-R3 f0.23h18		30.07	74.31
I-B f0.5h27	R-G	1.85	4.56
I-G		9.62	23.76
C-R3 f0.23h18		4.30	10.63
C-R3 f0.30h18	R-GM	1.02	2.52
I-G		2.97	7.35
I-B f0.5h27		0.86	2.12
C-R3 f0.23h18		0.10	0.24
I-B f0.5h27	S-CRI	3.08	7.62
I-G		4.72	11.67
C-R3 f0.23h18		1.40	3.45
I-B f0.5h27	S-SPR(MR)	0.69	1.70
I-G		0.58	1.44
I-G	S-SPR(MSR)	8.36	20.66
I-B f0.5h27		5.16	12.75
S-CRI	I-C	0.05	0.12
I-G		16.50	40.77
I-B f0.5h27		25.20	62.27
S-CRI	I-G	0.18	0.44
I-G		3.74	9.23
C-R3 f0.23h18	C-N1	0.75	1.85
C-R3 f0.23h18		2.37	5.86
I-G	C-C2 f2.0h24	6.97	17.22
S-CRI		0.02	0.04
I-B f0.5h27		0.95	2.34
S-CRI	C-R3 f0.5h18	0.73	1.80
I-G		16.03	39.61
Total		176.08	435.11



Proposed Outline Plan Data Sheet

*Calgary Planning Commission is the Approving Authority for the Outline Plan.
Attachment for Council's reference only.*

	HECTARES	ACRES
GROSS AREA OF PLAN	194.81	481.38
LESS: ENVIRONMENTAL RESERVE	4.22	10.42
LESS: LAND PURCHASE AREA	0	0
NET DEVELOPABLE AREA	190.59	470.96

LAND USE (Residential)	HECTARES	ACRES	ANTICIPATED # OF LOTS	ANTICIPATED # OF UNITS (Multi Residential)
R-G	30.24	74.72	940	
R-Gm	5.64	13.94	269	
M-G	9.42	23.27		465
M-1	4.17	10.31		258
M-H1	4.91	12.14		789
Total Residential	54.38	134.38	1209	1512

LAND USE (Mixed Use)	HECTARES	ACRES	ANTICIPATED # OF RESIDENTIAL UNITS	ANTICIPATED COMMERCIAL AREA (SQ.M)
MU-1f3.5h24	2.07	5.13	256	36200
Total Residential	2.07	5.13	256	
Total Commercial	2.07	5.13		36200

LAND USE (Commercial / Industrial)	HECTARES	ACRES	ANTICIPATED # OF LOTS	ANTICIPATED AREA (SQ.M) IF AVAILABLE
C-N1	3.28	8.11	3	32800
C-C2f2.0h24	8.41	20.79	2	168600

LAND USE (Commercial / Industrial)	HECTARES	ACRES	ANTICIPATED # OF LOTS	ANTICIPATED AREA (SQ.M) IF AVAILABLE
C-R3f0.5h18	13.53	33.44	2	67650
I-G	26.90	66.48	3	269000
I-C	19.78	48.88	3	198600
Total Commercial	25.22	62.34	7	269059
Total Industrial	46.68	115.36	6	467600

	HECTARES	ACRES	% OF NET AREA
ROADS (Credit)	26.84	66.32	14.08
PUBLIC UTILITY LOT (S-CRI)	14.30	35.33	7.5

RESERVES	HECTARES	ACRES	% OF NET AREA
MR Credit (S-SPR)	1.83	4.51	1%
MR Cash in Lieu	9.98	24.67	5.2%
MSR (S-SPR)	7.25	17.92	3.8%

	UNITS	UPH	UPA
ANTICIPATED # OF RESIDENTIAL UNITS	2977		
ANTICIPATED DENSITY		32.6	13.2
ANTICIPATED INTENSITY		54	22

Proposed Amendments to the Northeast Industrial Area Structure Plan

1. The Northeast Industrial Area Structure Plan attached to and forming part of Bylaw 2P2007, as amended, is hereby further amended as follows:
 - (a) In Section 2.0 Planning Area, Subsection 2.1, Plan Area Map, in the first paragraph, delete the last two sentences and replace with the following:

“A portion of the Plan area is located within the 30 Noise Exposure Forecast (NEF) contour as prescribed in the Calgary International Airport Vicinity Protection Area Regulation. As such, residential development is prohibited within that portion of the Plan area.”
 - (b) Delete the existing Map 1 entitled ‘Plan Area’ and replace with the revised Map 1 entitled ‘Plan Area’ attached as Schedule A
 - (c) In Section 3.0 Vision, Subsection 3.1 Vision of the Future, delete the first paragraph and replace with the following:

“The Northeast Industrial Area, in the year 2030, has been transformed into a vibrant and successful business and residential area that provides for a complete community with employment, housing, education and recreational opportunities.”
 - (d) In Section 3.0 Vision, Subsection 3.1 Vision of the Future, in the fourth paragraph, delete the second sentence and replace with the following:

“Transit routes will link the residential, recreational and employment areas within the Plan area to other surrounding residential communities and employment areas.”
 - (e) In Section 3.0 Vision, Subsection 3.1, Vision of the Future, delete the sixth paragraph and replace with the following:

“In summary, the Northeast Industrial Area comprises a vibrant residential community and a prominent business area that convey a positive image of growth, prosperity and diversity. The area provides a strong employment, residential and recreational function, creates a quality environment in which to live, work and play and contributes to a thriving and healthy city.
 - (f) In Section 3.0 Vision, Subsection 3.2 Goals for the Area, add the following new bullet after the first bullet:

“• Residential Development
To provide for a complete well-connected residential community which provides for a variety of housing choices to meet the needs of residents.”

- (g) Delete the existing Map 2 entitled 'Planning Cells' and replace with the revised Map 2 entitled 'Planning Cells' as attached as Schedule B.
- (h) In Section 6.0 Land Use Concept, Subsection 6.1, Land Use Concept Map, in the first paragraph, replace "Gateway Commercial Area" with "Commercial Area".
- (i) In Section 6.0 Land Use Concept, Subsection 6.1, Land Use Concept Map, in the first paragraph, after the second bullet add the following:
- Residential Area
- (j) Delete the existing Map 3 entitled 'Land Use Concept' and replace with the revised Map 3 entitled 'Land Use Concept' as attached as Schedule C.
- (k) In Section 7.0 Land Use Areas, Subsection 7.1.2, Policies, after Policy (2), insert the following new policy:
- (g) Development should provide a transition between the Business/Industrial Area and adjacent residential areas that is complimentary to the form and scale and should include screening such as fences, berms or landscaping to provide separation between these areas."
- (l) In Section 7.2, replace any mention of "Gateway Commercial Area" with "Commercial Area".
- (m) In Section 7.2.2, Policies (1)(a), delete (ii) and replace with the following:
- (ii) secondary commercial uses, service commercial uses, office uses, institutional uses, recreational uses, dwelling units on a second storey or above, public uses, and similar and accessory uses to the above, may be allowed within the Commercial Area where determined to be compatible and appropriate."
- (n) Delete Section 7.2.2(2) Development within Gateway Commercial Area in its entirety.
- (o) In Section 7.3 Transit Station Planning Area, in subsection 7.3.1 Purpose, delete the paragraph and replace with the following:
- "The purpose of this area is to provide for transit-oriented development (TOD) surrounding an LRT station within the Plan area. This station will be situated adjacent to a residential neighbourhood and an industrial/business park. Accordingly, multi-unit residential, mixed-use development, smaller format commercial as well as transit supportive uses such as a park and ride and transit drop-off areas should be located closest to the station followed by larger format industrial uses and grade-oriented residential. This development pattern will need to recognize the restrictions within the NEF contours and that the industrial area will function as predominantly larger format industrial. Pedestrian connections

will provide connectivity and community linkages within the Transit Station Planning Area.”

- (p) In Section 7.3.2, Policy (1) (a), delete (iii), (iv), (v) and replace with the following:

“(iii) multi-unit residential developments;

(iv) mixed-use developments; and

(v) other uses consistent with a residential area and an industrial/business park.”

- (q) In Section 7.3.2, Policy (1), delete (d) and replace with the following:

“(d) Multi-unit residential, mixed-use, business and employment generating uses should locate in close proximity to the LRT station.”

- (r) In Section 7.3.2, Policy (2), Alignment of the LRT Line, delete the sentence and replace with the following:

“The exact alignment of the LRT line in relation to the adjacent multi-unit residential, mixed-use and industrial/business park should be determined through the Outline Plan and/or subdivision approval process.”

- (s) In Section 7.0 Land Use Areas, delete subsection 7.4 Comprehensive Planning Overlay in its entirety and replace with the following:

“7.4 Residential Areas

7.4.1 Purpose

The purpose of this area is to accommodate ground-oriented and multi-unit residential development that encourage a variety of residential typologies. These areas are intended to accommodate different household types and lifestyles, encourage social diversity and provide for the opportunity for aging in place.

7.4.2 Policies

(1) Neighbourhood Activity Centre

The Plan area will feature a Neighbourhood Activity Centre (NAC) within each Residential neighbourhood which is intended to be the focal point of these areas.

1. NACs should be located as shown on Map 3: Land Use Concept.

2. The NACs should be designed to create unique and attractive gathering spaces through the inclusion of publicly accessible open space and public amenities.
3. NACs should be located so that there is a maximum walking distance of 700 metres from the surrounding Neighbourhood Area to a NAC. This may be exceeded in cases where the neighbourhood is larger or an irregular shape due to natural features.

(2) Multi-Unit Residential Areas

Multi-Unit Residential Areas are intended to primarily be made up of developments in the form of apartment buildings (typically four storeys and greater) in order to allow for greater housing choices in proximity to existing and planned transit.

- (a) Multi-unit residential buildings are encouraged to provide separate and direct grade-oriented access for ground floor units.

(3) Ground-Oriented Residential Areas

Future development within Ground-Oriented Residential Areas is intended to provide a variety of lower-density building forms (three storeys and lower) with at-grade access.

(4) Open Space

An open space network will be provided to offer recreational opportunities for residents both within and outside the neighbourhood area.

- (a) Open spaces should be designed to be accessible to people of all ages and abilities and provide opportunities for both active and passive recreation.
- (b) Open spaces should be designed for year-round use and incorporate weather protection elements such as shading for the summer and wind breaks and solar access for winter.”
- (t) Delete the existing Map 4 entitled ‘Wetland Study Area’ and replace with the revised Map 4 entitled ‘Wetland Study Area’ as attached as Schedule D.
- (u) In Section 12.2 Noise Exposure Forecast Contours, subsection 12.2.1 Purpose, delete the fourth sentence and replace with the following:

“This contour encompasses most of the Northeast Industrial Area except for a portion in the northeast corner of the Plan.”
- (v) In Section 16.1.2, Policies, after policy (3) insert new policy (4) as follows:

“(4) Cells A and C Phasing of Development

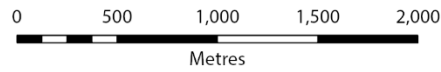
- (a) Phasing of subdivision and development approvals in Cells A and C should be as shown in Map 6 entitled 'Cell A and C Phasing'.
- (b) Subdivision and development of Phase 1 (Phases 1A and 1B) may proceed without upgrades to the regional transportation network.
- (c) In conjunction with the Phase 2 Tentative Plan submission, or otherwise when deemed appropriate by Development Engineering, the Northeast Industrial Area Structure Plan shall be updated to reflect the intention to upgrade the intersections of Metis Trail & 128 Avenue NE and Metis Trail & Country Hills Boulevard NE to interchange designs. Note that any proposed Transportation amendments to the Area Structure Plan are subject to the findings of the Northeast Network Planning study.
- (d) In conjunction with the Phase 2 Tentative Plan submission, or otherwise when deemed appropriate by Development Engineering, a Functional Planning study shall be completed for the interchange designs at Metis Trail & Country Hills Boulevard NE, and at Metis Trail & 128 Avenue NE the latter including the LRT alignment and land requirements west of the interchange.
- (e) Reassessment of phase limits may be considered without requiring an amendment to the Northeast Industrial Area Structure Plan, when subdivision approvals of the entire preceding phase area shown on Map 6 have been completed.”
- (w) In Section 16.1.2, Policies, at the end of the section insert new Map 6 entitled 'Cell A and C Phasing' as attached as Schedule E.
- (x) In Section 18.2 Land Use Definitions, after (7) insert the following land use definitions and renumber the subsequent definitions accordingly:
 - “(8) **Mixed-Use** means development of land, or building with two or more different types of uses, such as residential, office or retail. Mixed-use can occur vertically within a building, or horizontally.
 - (9) **Multi-unit Residential** means development which includes 3 or more dwelling units in the same building. This may include apartment buildings, townhomes and other compatible forms.
 - (10) **Neighbourhood Activity Centre (NAC)** means a centre providing opportunities for residential intensification, local jobs, retail services, and civic activities.”

SCHEDULE A














Northeast Industrial Area Structure Plan

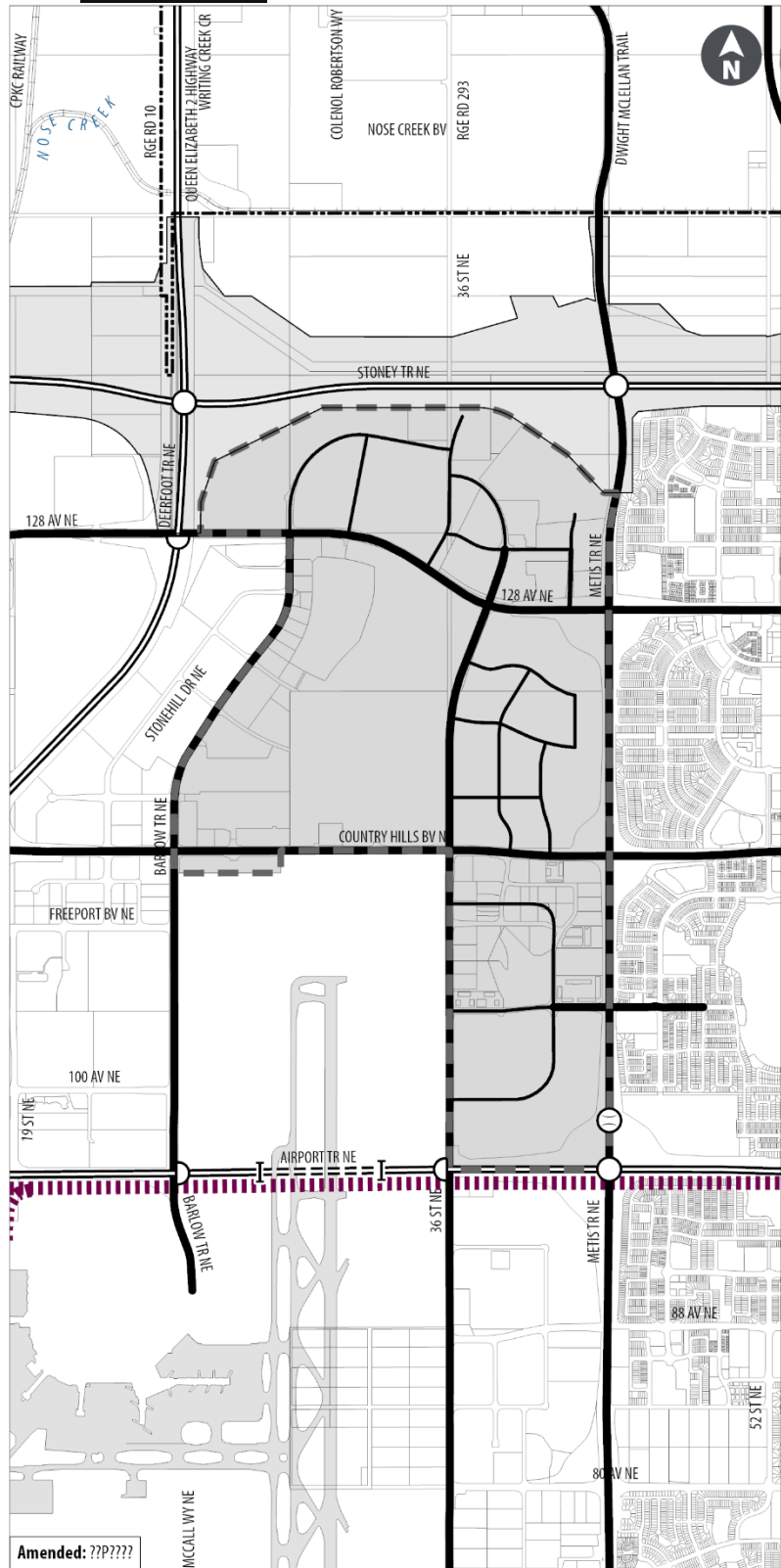
Map 1

Plan Area



Legend

-  Plan Area
-  City Limits
-  Transportation/
Utility Corridor
-  Expressway (Skeletal Road)
-  Expressway (Skeletal Road) Tunnel
-  Major Road (Arterial Street)
-  Minor Arterial
-  Internal Road
-  Full Interchange
-  Partial Interchange
-  Potential Pedestrian Overpass
-  Railway
-  Airport Transit Connection
(Technology TBD)



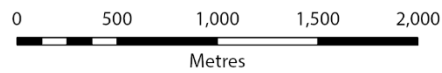
This map is conceptual only. No measurements of distances or areas should be taken from this map.

Amended: ???P????

Northeast Industrial Area Structure Plan

Map 2

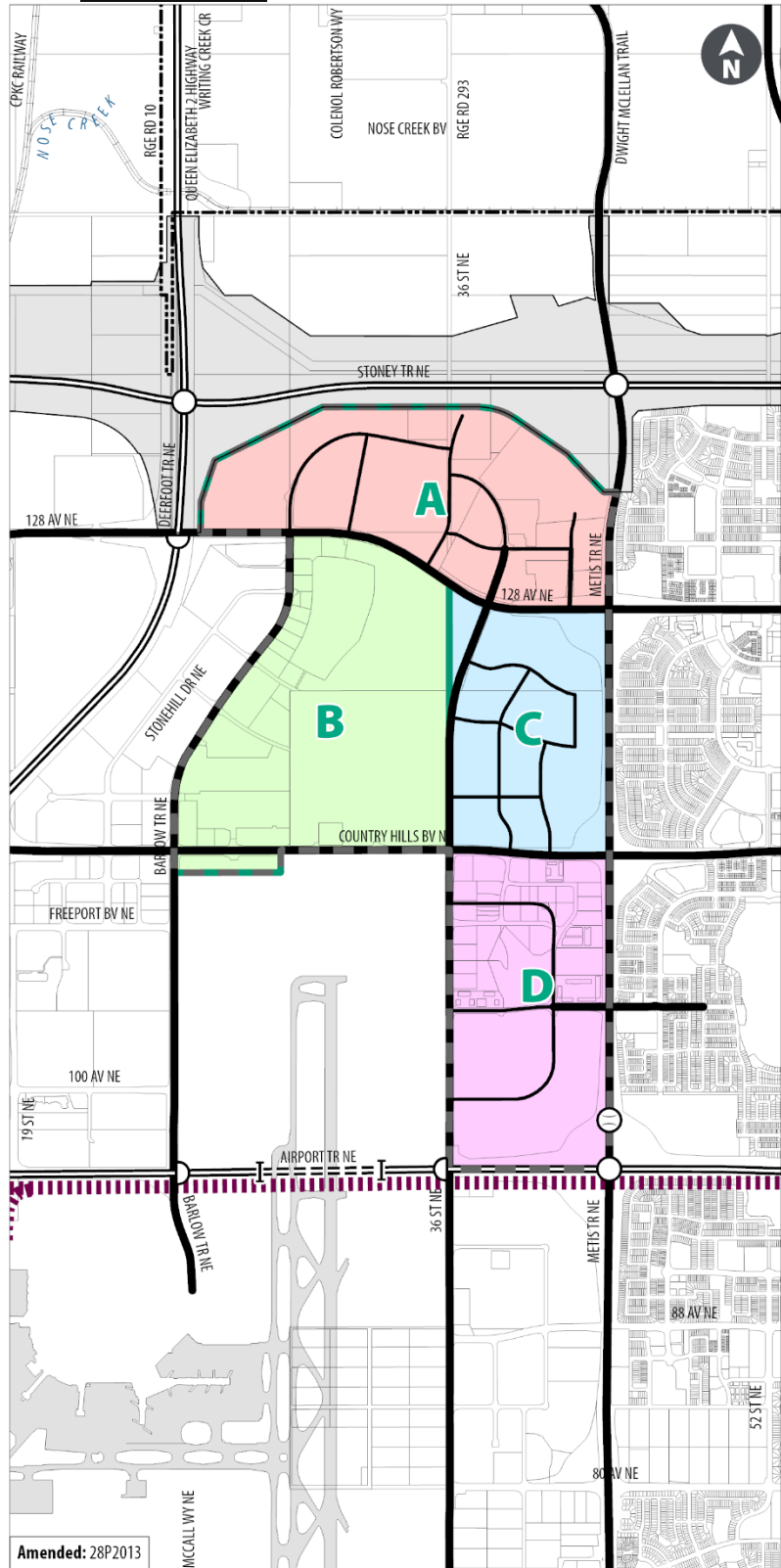
Planning Cells



Legend

- Plan Area Boundary
- City Limits
- Transportation/Utility Corridor
- Planning Cells
- Expressway (Skeletal Road)
- Expressway (Skeletal Road) Tunnel
- Major Road (Arterial Street)
- Minor Arterial
- Internal Road
- Full Interchange
- Partial Interchange
- Potential Pedestrian Overpass
- Railway
- Airport Transit Connection (Technology TBD)

SCHEDULE B



This map is conceptual only. No measurements of distances or areas should be taken from this map.

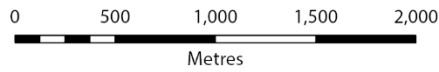
Amended: 28P2013

SCHEDULE C

Northeast Industrial Area Structure Plan

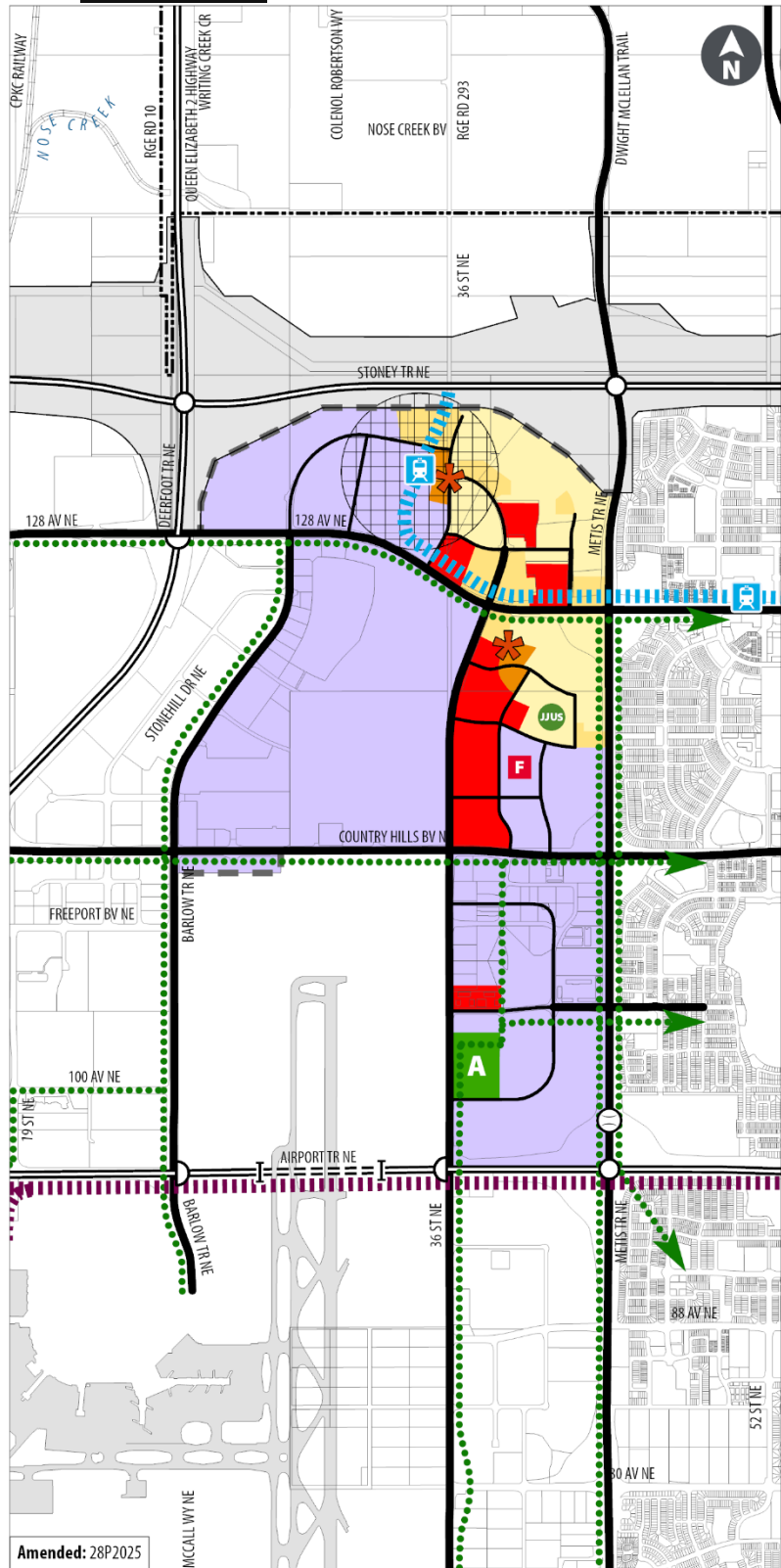
Map 3

Land Use Concept



Legend

- Plan Area Boundary
- City Limits
- Transportation/Utility Corridor
- Business/ Industrial Area
- Commercial Area
- Regional Athletic Park
- Mixed Use
- Multi-Unit Residential
- Ground-Oriented Residential
- Fire Station
- Joint Joint Use Site
- Neighbourhood Activity Centre
- Transit Station Planning Area
- Regional Pathway
- Expressway (Skeletal Road)
- Expressway (Skeletal Road) Tunnel
- Major Road (Arterial Street)
- Minor Arterial
- Internal Road
- Full Interchange
- Partial Interchange
- Potential Pedestrian Overpass
- Railway
- LRT Alignment
- LRT Station
- Airport Transit Connection (Technology TBD)



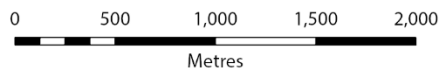
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Amended: 28P2025

Northeast Industrial Area Structure Plan

Map 4

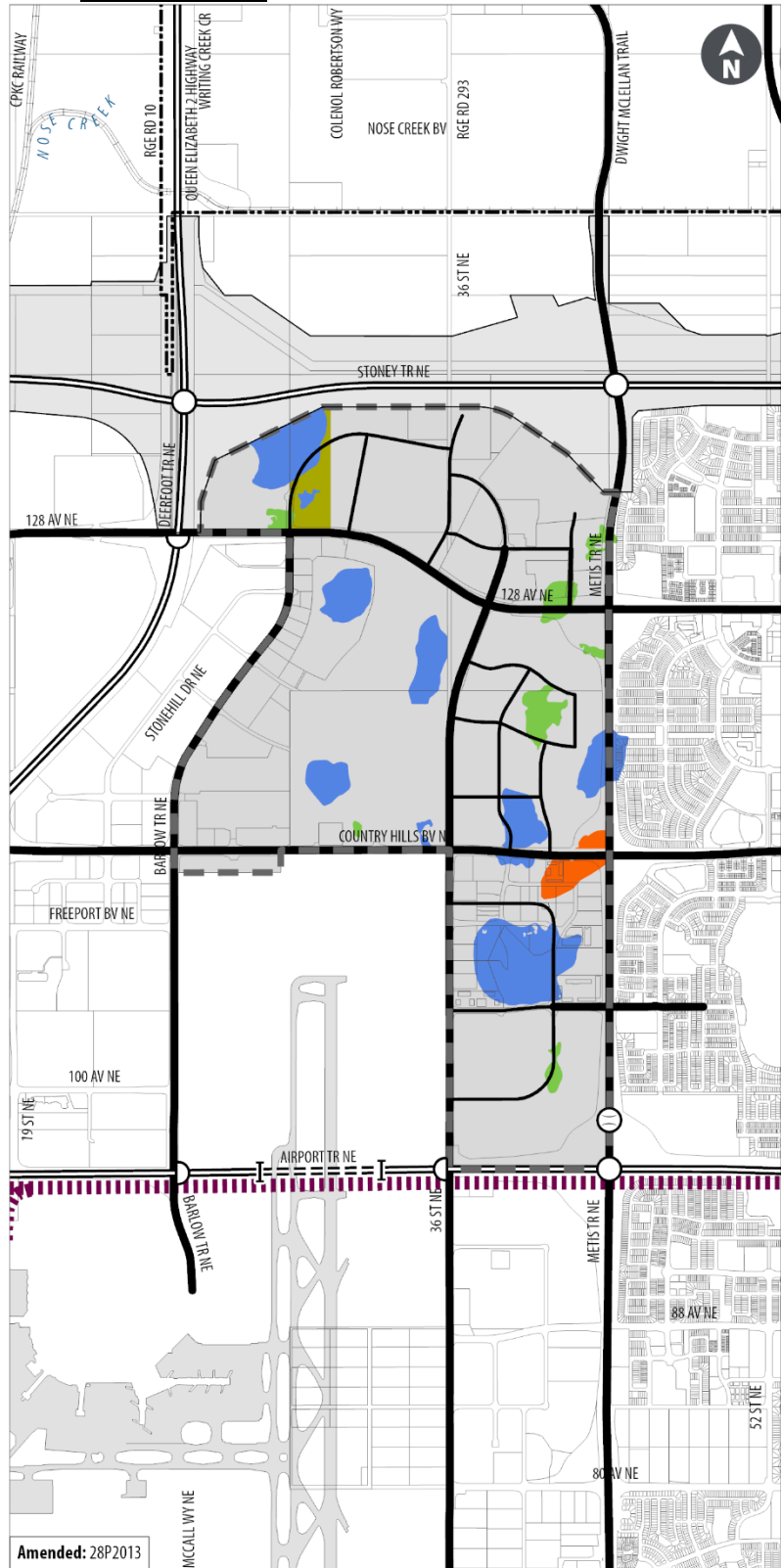
Wetland Study Area



Legend

- Plan Area
- City Limits
- Transportation/Utility Corridor
- Expressway (Skeletal Road)
- Expressway (Skeletal Road) Tunnel
- Major Road (Arterial Street)
- Minor Arterial
- Internal Road
- Full Interchange
- Partial Interchange
- Potential Pedestrian Overpass
- Railway
- Upland Grassland
- Airport Transit Connection (Technology TBD)
- Type 3 Seasonal
- Type 4 Semi-permanent
- Type 6 Alkali

SCHEDULE D



This map is conceptual only. No measurements of distances or areas should be taken from this map.

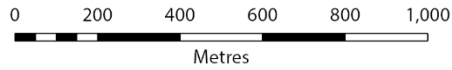
Amended: 28P2013

SCHEDULE E

Northeast Industrial Area Structure Plan

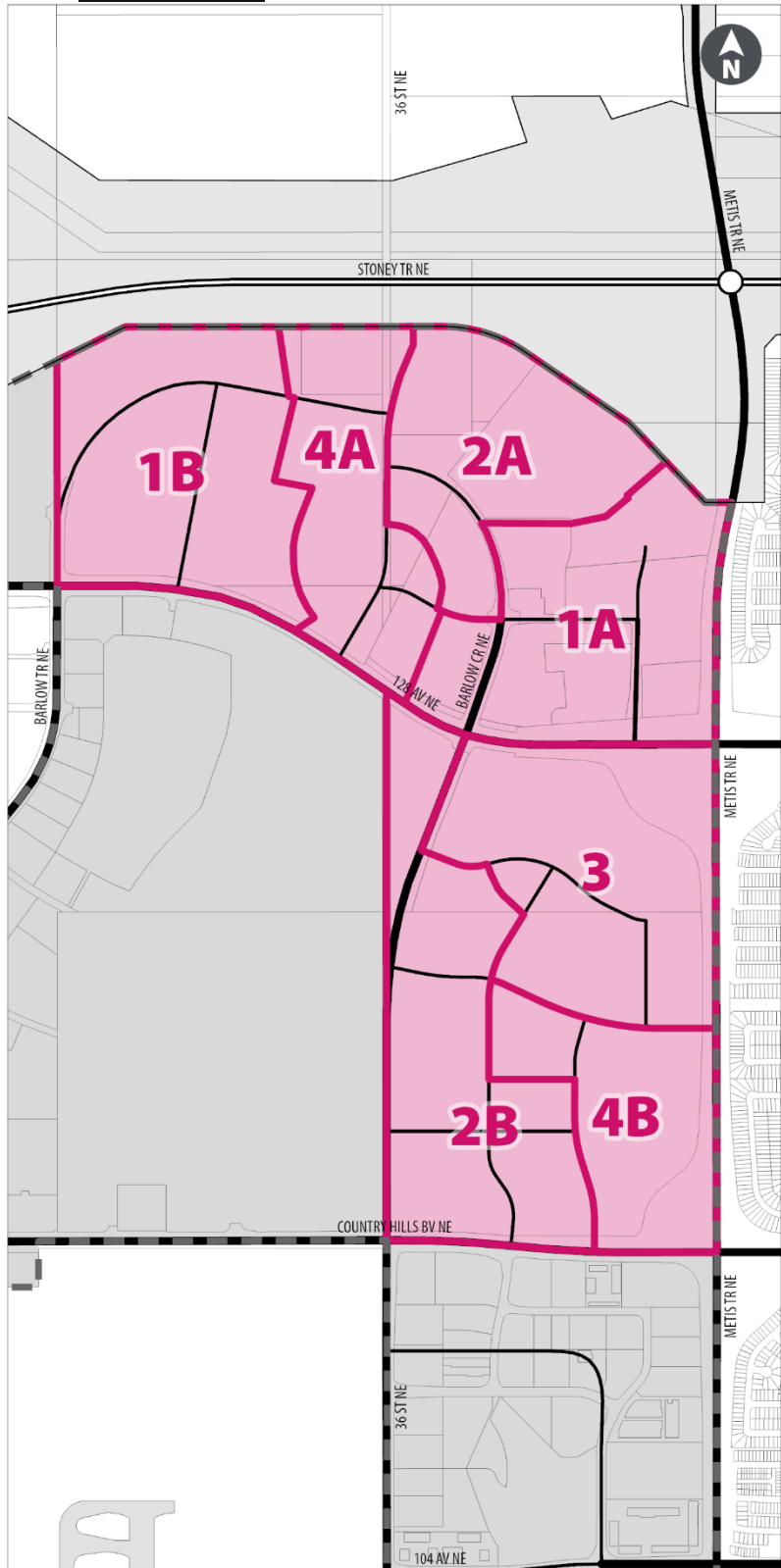
Map 6

Planning Cell A
and C Phasing



Legend

- Plan Area
- Transportation/Utility Corridor
- Expressway (Skeletal Road)
- Major Road (Arterial Street)
- Minor Arterial
- Internal Road
- Full Interchange



- Phase 1:** (1A & 1B): equivalent to 2241 residential units
- Phase 2:** (2A & 2B): equivalent to 6817 residential units
- Phase 3:** equivalent to 3679 residential units
- Phase 4:** (4A & 4B): equivalent to 3772 residential units

This map is conceptual only. No measurements of distances or areas should be taken from this map.

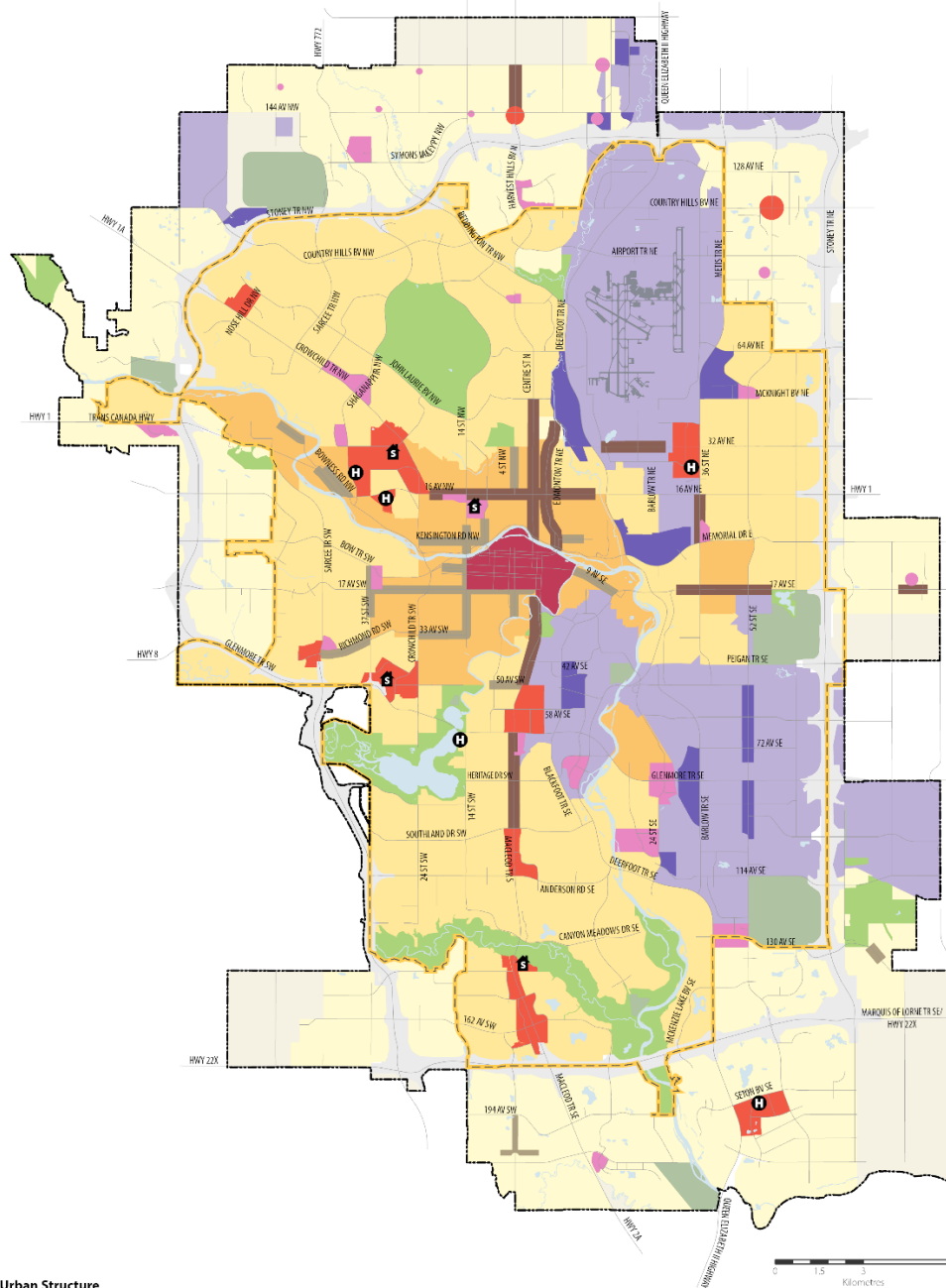
Proposed Amendment to the Municipal Development Plan

1. The Municipal Development Plan attached to and forming part of Bylaw 24P2009, as amended, is hereby further amended as follows:
 - (a) Delete the existing Municipal Development Plan: Volume 1, Map 1 entitled 'Urban Structure', and replace it with the revised Map 1 entitled 'Urban Structure' as shown in Schedule A.
 - (b) Delete the existing Municipal Development Plan: Volume 3 (Calgary Transportation Plan), Map 5 entitled 'Primary Goods Movement Network', and replace it with the revised Map 5 entitled 'Primary Goods Movement Network' as shown in Schedule B.

TEXT FOR DISCUSSION

SCHEDULE A

Map 1: Urban Structure



Urban Structure
(By Land Use Typology)

Activity Centres

- Greater Downtown
- Major Activity Centre
- Community Activity Centre

Main Streets

- Urban Main Street
- Neighbourhood Main Street

Developed Residential

- Inner City
- Established

Developing Residential

- Planned Greenfield with Area Structure Plan (ASP)
- Future Greenfield

Industrial

- Industrial - Employee Intensive
- Standard Industrial

Major Public Open Space

- Major Public Open Space
- Public Utility

Hospital

- H Hospital

University

- U University

Transportation/Utility Corridor

- Transportation/Utility Corridor

City Limits

- City Limits

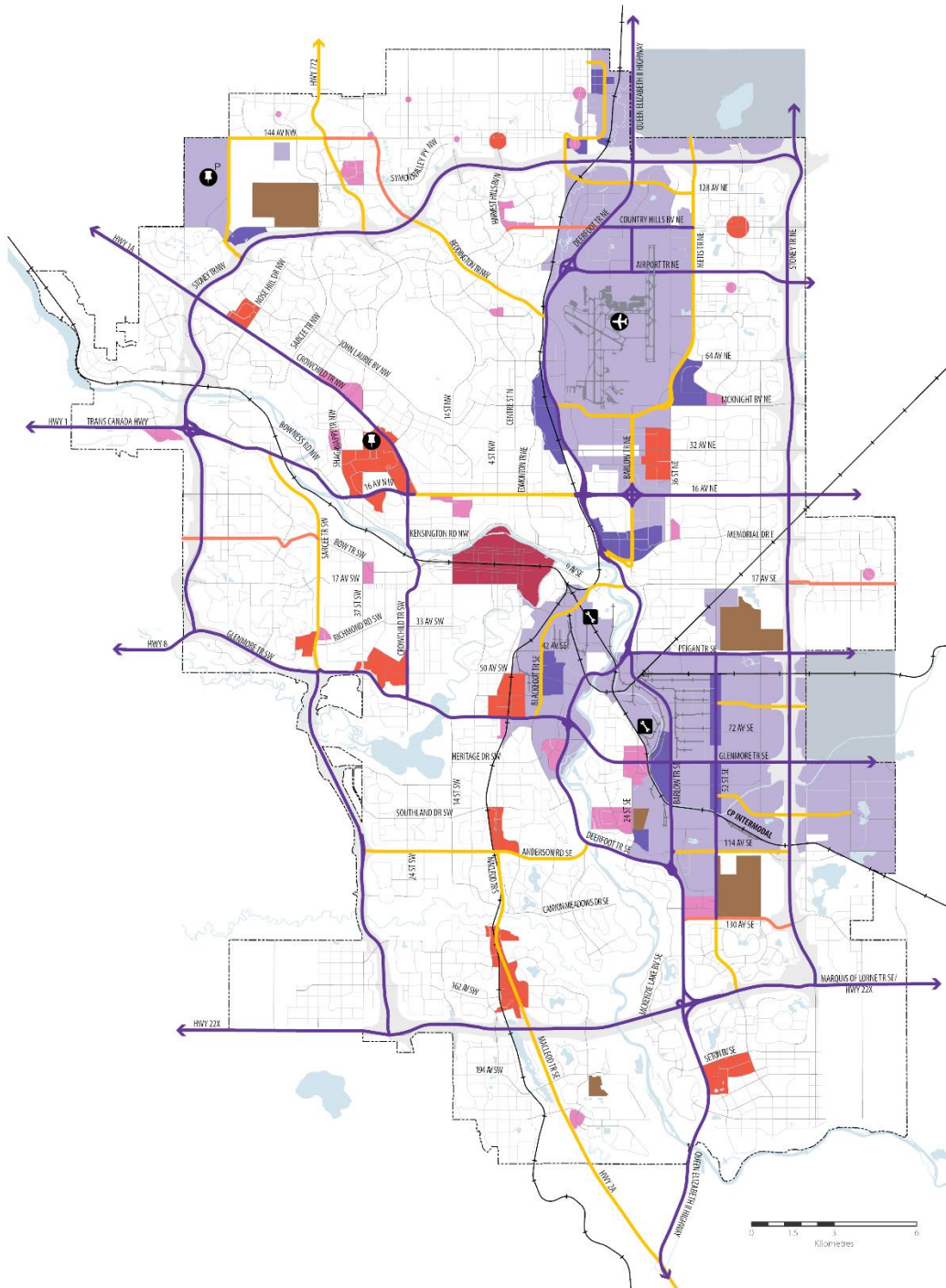
Balanced Growth Boundary

- Balanced Growth Boundary



SCHEDULE B

Map 1: Primary Goods Movement Network



Legend

Primary Goods Movement Network

- Main Goods Movement Corridor
- Supporting Goods Movement Corridor
- Emerging Goods Movement Corridor
- Air Cargo

- Rail Maintenance Yard
- Research Park
- Future Research Park
- Railway Tracks

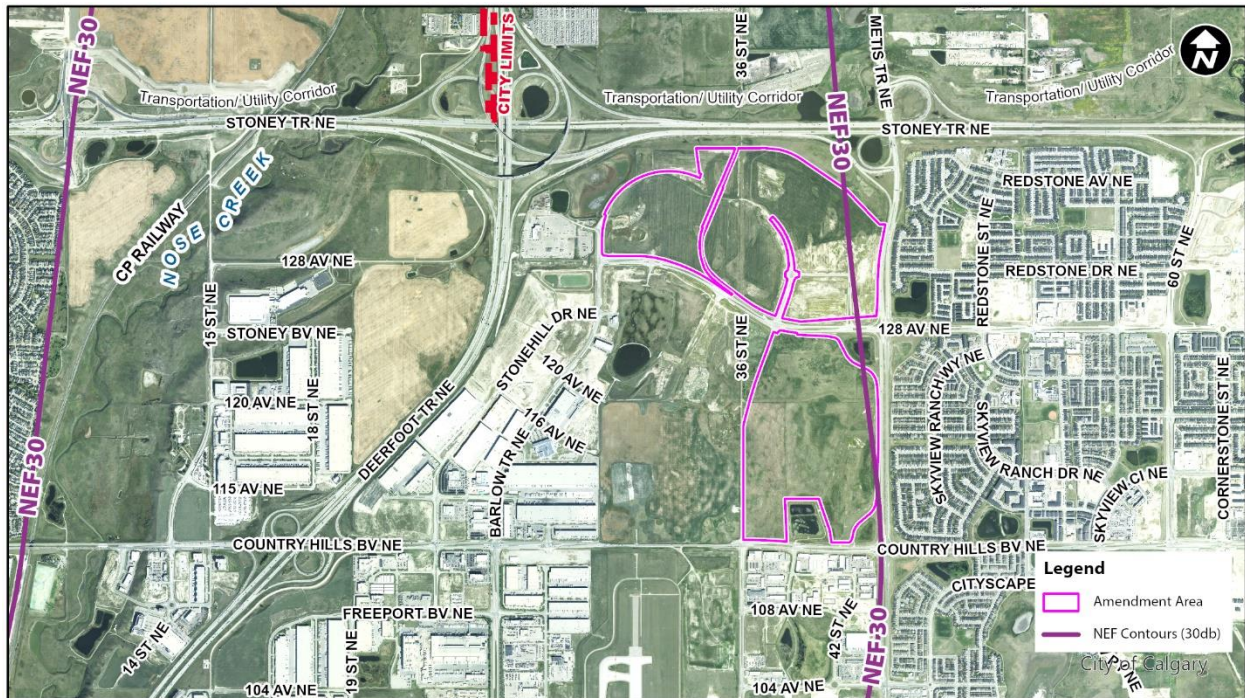
- Landfill
- County of Rocky View Industrial
- Transportation/Utility Corridor
- City Limits

Urban Structure

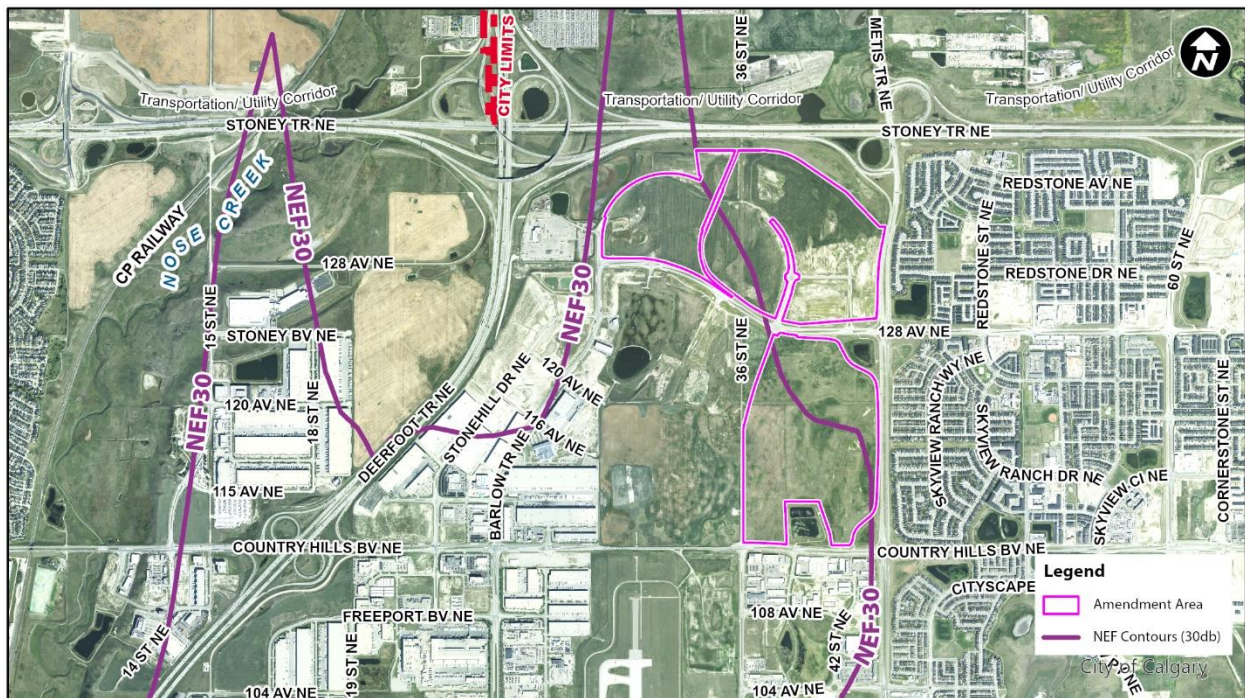
- Greater Downtown
- Major Activity Centre
- Community Activity Centre
- Industrial - Employee Intensive
- Standard Industrial

Provincially approved changes to NEF Contours

Previous NEF Contours (prior to August 2021)



Changes to NEF Contours approved by the Province of Alberta (August 2021)



Applicant Outreach Summary



STONEGATE Engagement Summary Report

QUALICO[®]
communities



STONEGATE

Engagement Summary Report

On behalf of **Qualico Communities**
Prepared by **B&A Studios**



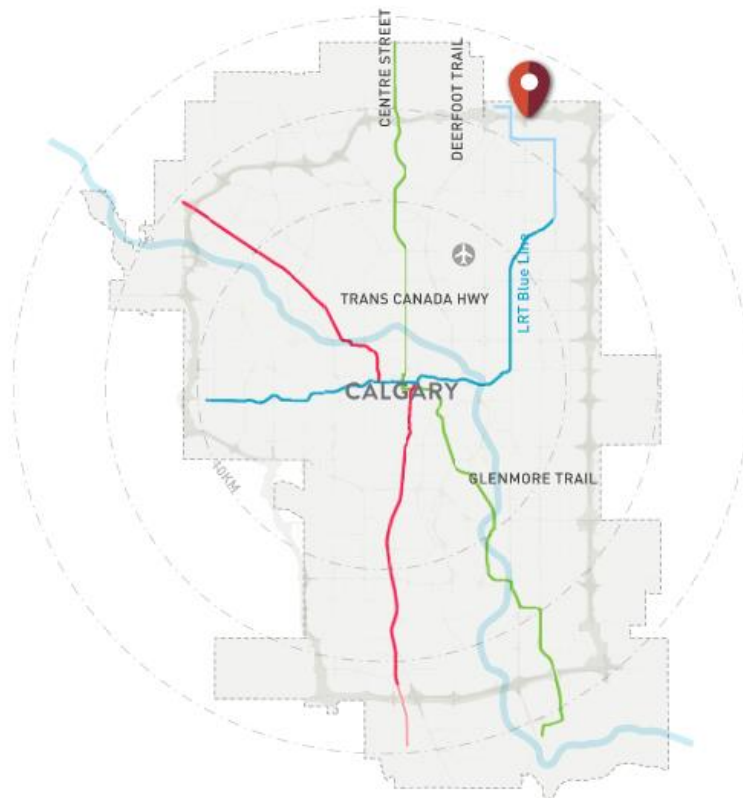
Table of Contents

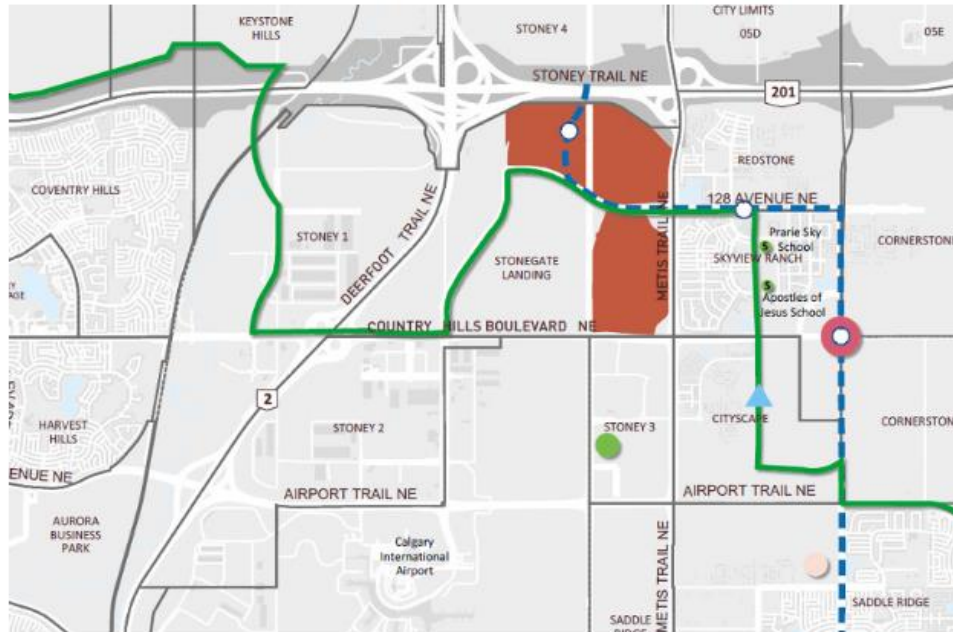
1.0	Project Background	01
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1.0 Project Background

Qualico is proposing an Outline Plan and Land Use Amendment for the Stonegate NE Lands—an approximately 487-acre site in northeast Calgary located south of Stoney Trail NE, west of Metis Trail NE, and north of Country Hills Boulevard NE.

The lands are currently designated as industrial and commercial land use districts and are located within the Northeast Industrial Area Structure Plan (ASP).





- Subject Lands
- Community Activity Centre
- Future Regional Athletic Park
- Rotary Mattamy Greenway
- Major Activity Centre
- S Schools
- Future Blue Line / LRT Stations
- ▲ Cityscape Interpretive Wetland

Originally approved in 2007 for industrial development, the site has remained largely undeveloped due to limited market demand. Recent amendments to the Airport Vicinity Protection Area (AVPA) regulations in 2021 now enable a more flexible approach to planning, including the introduction of residential uses.

This proposal seeks to realign the site's vision with Calgary's contemporary planning goals by providing a complete community with a balanced mix of residential, commercial, industrial, and recreational land uses. The site is partially serviced and well-positioned to support growth, with access to future Blue Line LRT infrastructure and more than \$90 million already invested in servicing and roadways.

2.0 Engagement Objectives

The engagement process aimed to inform, gather input, and maintain open communication with the community. Key objectives included:



Inform local residents and interested parties about the proposed land use changes



Provide accessible and multilingual information about the planning and City approval process



Invite feedback through both in-person and virtual channels



Address questions and key themes raised by attendees and participants



3.0 Outreach Overview

Promotion Channels	
Postcard	Delivered two weeks prior to the open house to approximately 3,200 households in Skyview Ranch and Redstone, including 800 homes closest to Metis Trail and the Country Hills Blvd strip mall. See Appendix F.
BOLD Signage	Installed near Generations Hall from May 6–20 to promote the event.
Community Association & Group Outreach	<ul style="list-style-type: none"> • Skyview Ranch Community Association • Redstone Homeowners Association • Cityscape Residents Calgary Facebook group (event created)
Email Campaign	Notices shared with community associations and interested stakeholders



Postcard Collateral from the promotion are included Appendix F.

Language Accessibility

Recognizing the language diversity in the surrounding communities, multilingual support was a key feature:

- ✦ **Skyview Ranch:** 49% speak languages other than English
- ✦ **Redstone:** 60% speak languages other than English
- ✦ **Cityscape:** 65% speak languages other than English

Two multilingual team members were onsite to support conversations in Urdu, Hindi, and Gujarati. Translators were clearly identified by name tags indicating the languages spoken and proactively engaged attendees.



Name Tag Collateral from the promotion are included Appendix C.

4.0 Open House Event



Tuesday, May 20, 2025



5:00 – 8:00 p.m.



Generations Multi-Purpose Hall - 120 Skyview Ranch Drive NE



13 community members in attendance

Event Setup

- ✦ Directional signage was placed outside the venue and within the lobby to guide attendees. See Appendix E.
- ✦ Open house boards detailing the proposal and planning context were displayed throughout the space. See Appendix A.
- ✦ Printed, hand-held versions of boards were available for attendees to take home. See Appendix D.
- ✦ Project team members wore name tags, with translators clearly labeled. See Appendix C.
- ✦ Photos of participants and event setup were taken to document the session. See Appendix B.



All the above collateral from the promotion are included Appendix at the end.

5.0 Feedback Summary

A small number of feedback forms were submitted at the event. While attendees completed checkboxes identifying topics of interest and their connection to the area, no written comments were provided. As a result, the feedback summary below is primarily informed by direct conversations with attendees, with key points documented by project team members during the open house.

How attendees heard about the event:	<ul style="list-style-type: none"> Most participants cited the postcard as their source of information
Connection to the area:	<ul style="list-style-type: none"> Majority were residents of Skyview Ranch, Redstone, or Cityscape Some participants expressed general interest in northeast Calgary development
Topics of interest:	<ul style="list-style-type: none"> Strong interest in commercial and employment areas Some participants expressed a desire for additional commercial space and local amenities Several attendees raised concerns about the strain on existing schools and suggested the inclusion of a school site to serve future residents Notable mentions of parks and recreation spaces, overall community design, and transportation and mobility

Key themes



Desire for More Amenities

Several attendees noted a strong desire for additional local services, such as full-service grocery stores and family-friendly restaurants.



Mixed Reactions to Additional Housing

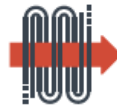
While some participants welcomed a broader range of housing options, others raised concerns about increased density and potential strain on infrastructure. Feedback reflected a preference for ensuring that any residential development is supported by amenities, transit access, and long-term serviceability.

Key themes continued...



Infrastructure & Design Feedback

Participants emphasized the need for enhanced stormwater drainage, increased green space, and a walkable, accessible community design.



Positive Responses

Some attendees expressed appreciation for the open house format, noting that the information was clear, the materials were informative, and project representatives were helpful and responsive.

6.0 Post-Event Communications

- + Follow-up email sent to attendees and Community Associations on May 22, 2025
- + Included digital copies of the open house boards (Appendix A)

7.0 Next Steps

Community feedback from this engagement will help inform refinements to the Outline Plan and Land Use Amendment prior to re-submission. The project team remains committed to transparency and ongoing communication as the project progresses.

Key milestones in the planning process include:

- + **Spring 2025** – Consider feedback and make improvements to the plan
- + **Spring 2025** – Re-submit application to the City of Calgary
- + **Fall 2025** – Anticipated Council decision



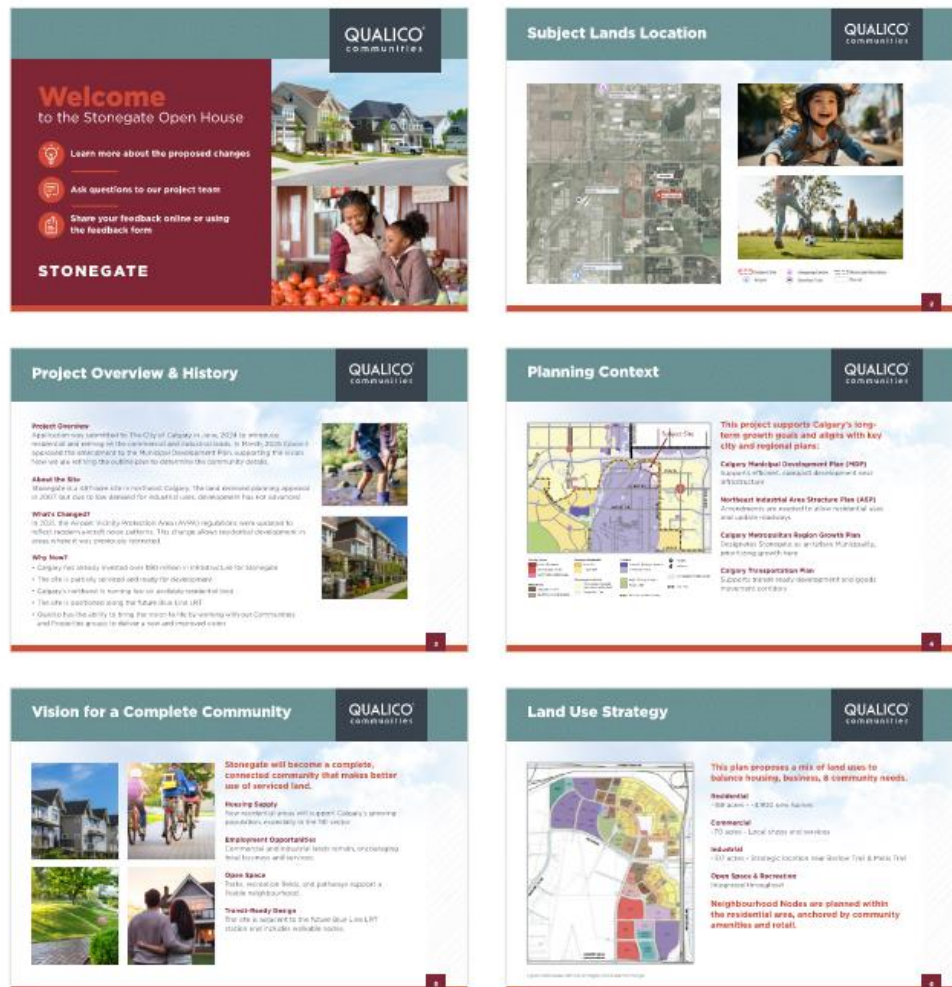
For questions or additional information, please contact:

Jane Geller, Engagement Specialist
jgeller@bastudios.ca

8.0 Appendices

The following appendices include select materials from the engagement process to support transparency and provide context for the engagement approach. These reflect the tools used to promote the session and share information with the community.

Appendix A: Open House Boards



Parks, Recreation & Open Space QUALICO COMMUNITIES

Stonegate will include a variety of open spaces to support healthy, active living:

- Public School Site:** Centrally located to facilitate active recreation.
- Green Fields:** High-visibility open space for community, sports and active living.
- Parks & Green Spaces:** Provide high quality green space for active living and recreation.
- Stamenway Plaza:** Open space to support active living and recreation.
- Neighborhood Plaza:** Open space to support active living and recreation.
- Walkway:** A connected network for walking and cycling to the area surrounding the community.

These spaces support both local residents and regional recreation needs.

Transportation & Mobility QUALICO COMMUNITIES

Stonegate is well-connected and designed for future mobility needs.

- Road Network:**
 - Access from Cooney Hill Blvd, Main Trail, and St. St. St.
 - Access to major thoroughfares for walking and cycling.
- Active Transportation:**
 - Walkway and bicycle network throughout.
 - Conveniently located to major transit and LRT stations.
- Transit-Oriented Development:**
 - Located on the parkway of the Blue Line LRT.
 - Supports higher density housing and commercial uses.

Phasing QUALICO COMMUNITIES

Stonegate is partially serviced, reducing the timeline for development.

- Qualico intends to proceed with development immediately, subject to approvals.
- Development will occur in logical phases over time.
- Industrial and residential components may proceed in tandem.
- Actual size and location of these phases will be determined on future market conditions and infrastructure requirements.

What's Next & How to Stay Involved QUALICO COMMUNITIES

- Please complete the feedback form or visit the online survey.
- Your comments will help refine the proposal before it is submitted to the City.
- Scan the QR code to complete the survey.
- For Questions or comments contact [Jane Geller, jstudies@jstudies.ca](mailto:Jane.Geller@jstudies.ca)

Next Steps

- Spring 2023:** Consider feedback & make improvements to the Plan.
- Spring 2023:** Re-submit application to the City of Calgary.
- Fall 2023:** Anticipate Council decision.

Appendix B: Photos of Event & Participants



Appendix C: Translator Name Tags



Appendix D: Hand-held Printouts of Boards



Appendix E: Directional Signage (Interior and Exterior)



Appendix F: Distributed Postcard

