



REVISED AGENDA

CALGARY PLANNING COMMISSION

November 21, 2019, 1:00 PM
IN THE COUNCIL CHAMBER
Members

Director M.Tita, Chair
Director R. Vanderputten, Vice-Chair
Councillor G. Chahal
Councillor E. Woolley
Commissioner H. Cameron
Commissioner P. Gedye
Commissioner L. Juan
Commissioner A. Palmiere
Commissioner K. Schmalz
Commissioner J. Scott
Mayor N. Nenshi

1. CALL TO ORDER
2. OPENING REMARKS
3. CONFIRMATION OF AGENDA
4. CONFIRMATION OF MINUTES

REVISED MATERIALS

4.1 *Minutes of the Regular Meeting of the Calgary Planning Commission, 2019 November 07*

5. CONSENT AGENDA

- 5.1 DEFERRALS AND PROCEDURAL REQUESTS
None
- 5.2 BRIEFINGS
None

- 5.3 Land Use Amendment in Renfrew (Ward 9) at 639 and 641 – 12 Avenue NE, LOC2019-0125, CPC2019-1031

6. POSTPONED REPORTS
(including related/ supplemental reports)

None

7. ITEMS FROM OFFICERS, ADMINISTRATION AND COMMITTEES

7.1 DEVELOPMENT ITEMS

- 7.1.1 Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978, CPC2019-1413

7.2 PLANNING ITEMS

- 7.2.1 Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE, LOC2019-0099, CPC2019-1129
- 7.2.2 Land Use Amendment in Stoney 3 (Ward 5) at 3730 – 108 Avenue NE, LOC2019-0131, CPC2019-1376
- 7.2.3 Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE, LOC2019-0105, CPC2019-1436
- 7.2.4 Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple addresses, LOC2018-0249, CPC2019-1375
- 7.2.5 Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP), CPC2019-1391
- 7.2.6 Land Use Amendment in Crestmont (Ward 1) at multiple addresses, LOC2019-0061, CPC2019-1398
- 7.2.7 Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085, CPC2019-1079
- 7.2.8 Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP), CPC2019-1333
- 7.2.9 Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses, LOC2016-0088, CPC2019-0695

7.3 MISCELLANEOUS ITEMS
None

8. URGENT BUSINESS

9. CONFIDENTIAL ITEMS

9.1 ITEMS FROM OFFICERS, ADMINISTRATION AND COMMITTEES

9.2 URGENT BUSINESS

10. ADJOURNMENT



INDEX FOR THE 2019 NOVEMBER 21 REGULAR MEETING OF CALGARY PLANNING COMMISSION

NOTE:
PLANS SUBMITTED TO THE COMMISSION, AS PART OF THE DEPARTMENTAL REPORTS,
ARE INCLUDED FOR THE SOLE PURPOSE OF ASSISTING THE COMMISSION IN MAKING
A DECISION AND ARE NOT TO BE USED FOR ANY OTHER PURPOSE.

ISC: Unrestricted



CONSENT AGENDA

ITEM NO.: 5.1

DEFERRALS AND PROCEDURAL REQUESTS

ITEM NO.: 5.2

BRIEFINGS

ITEM NO.: 5.3

Melanie Horkan

COMMUNITY:

Renfrew (Ward 9)

FILE NUMBER:

LOC2019-0125 (CPC2019-1031)

PROPOSED REDESIGNATION:

From: Residential – Contextual One / Two Dwelling
(R-C2) District

To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS:

639 and 641 – 12 Avenue NE

APPLICANT:

New Century Design

OWNER:

2193553 Alberta Ltd (William Yeung and Preetpaul Bal)

ADMINISTRATION RECOMMENDATION:

APPROVAL

DEVELOPMENT ITEMS

ITEM NO.:7.1.1

Ezra Wasser

COMMUNITY:

Haysboro (Ward 11)

FILE NUMBER:

DP2019-0978 (CPC2019-1413)

PROPOSED DEVELOPMENT:

New: Assisted Living, Multi-Residential Development,
Sign - Class A

MUNICIPAL ADDRESS:

8610 Horton Road SW

APPLICANT:

NORR Architects Engineers Planners

OWNER:

Chartwell Retirement Residences

ADMINISTRATION RECOMMENDATION:

APPROVAL

PLANNING ITEMS

ITEM NO.: 7.2.1

Giyan Brenkman

COMMUNITY:

Highland Park (Ward 4)

FILE NUMBER:

LOC2019-0099 (CPC2019-1129)

PROPOSED REDESIGNATION:

From: Residential – Contextual One / Two Dwelling
(R-C2) District

To: Multi-Residential – Contextual Grade Oriented
(M-CGd72) District

MUNICIPAL ADDRESS:

3827 Centre A Street NE

APPLICANT:

Inertia

OWNER:

Huafeng Zhang

ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 7.2.2

Courtney Stengel

COMMUNITY:

Stoney 3 (Ward 5)

FILE NUMBER:

LOC2019-0131 (CPC2019-1376)

PROPOSED REDESIGNATION:

From: Industrial – Business f1.0 (I-B f1.0) District

To: DC Direct Control District to accommodate the
additional uses of Cannabis Facility and General
Industrial – Light

MUNICIPAL ADDRESS:

3730 - 108 Avenue NE

APPLICANT:

CivicWorks Planning + Design

OWNER:

2060708 Alberta LTD (Eagle Crest Construction)

ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 7.2.3

Manish Singh

COMMUNITY:

Pineridge (Ward 10)

FILE NUMBER:

LOC2019-0105 (CPC2019-1436)

PROPOSED REDESIGNATION:

From: Special Purpose – Community Institution (S-CI)
District

To: Commercial – Neighbourhood 2 (C-N2) District

MUNICIPAL ADDRESS:

6927 Rundlehorn Drive NE

APPLICANT:

Zeidler Architecture

OWNER:

The Western Canadian District of The Christian and
Missionary Alliance

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.4

Brad Bevill
(Related to Item 7.2.5)

COMMUNITY:

Cityscape (Ward 05)

FILE NUMBER:

LOC2018-0249 (CPC2019-1375)

PROPOSED ROAD CLOSURE:

0.017 hectare ± (0.042 acre ±) portion of road

PROPOSED REDESIGNATION:

From: Residential – Narrow Parcel One Dwelling (R-1N) District, Residential – One Dwelling (R-1) District, Residential – One / Two Dwelling (R-2) District, Residential – Low Density Multiple Dwelling (R-2M) District, Multi-Residential – Low Profile (M-1) District, Multi-Residential – Medium Profile (M-2) District, Multi-Residential – High Density Medium Rise (M-H2) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – Urban Nature (S-UN) District, Special Purpose – City and Regional Infrastructure (S-CRI) District, Special Purpose – Future Urban Development (S-FUD) District, DC Direct Control District and Undesignated Road Right-of-Way

To: Residential – Low Density Mixed Housing (R-G) District, Special Purpose – City and Regional Infrastructure (S-CRI) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – Urban Nature (S-UN) District, DC Direct Control District to accommodate multi residential development and DC Direct Control District to accommodate low density development with Live Work

MUNICIPAL ADDRESS:

10010 and 10011 – 52 Street NE
5555 and 5299 Country Hills Boulevard NE

APPLICANT:

B&A Planning Group

OWNER:

Mattamy (Northpoint) Limited
The City of Calgary

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.5

Brad Bevill
(Related to Item 7.2.4)

COMMUNITY:

Cityscape (Ward 05)

FILE NUMBER:

LOC2018-0249(OP) (CPC2019-1391)

PROPOSED OUTLINE PLAN:

Subdivision of 74.68 hectares \pm (184.53 acres \pm)

MUNICIPAL ADDRESS:

10010 and 10011 – 52 Street NE
5555 and 5299 Country Hills Boulevard NE

APPLICANT:

B&A Planning Group

OWNER:

Mattamy (Northpoint) Limited
The City of Calgary

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.6

Lindsey Ganczar

COMMUNITY:

Crestmont (Ward 1)

FILE NUMBER:

LOC2019-0061 (CPC2019-1398)

PROPOSED REDESIGNATION:

From: DC Direct Control District

To: Residential – One Dwelling (R-1) District

MUNICIPAL ADDRESS:

Various Addresses

APPLICANT:

Stantec Consulting Ltd

OWNER:

Crestmont Developments Inc

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.7

Joseph Yun
(Related to Item 7.2.8)

COMMUNITY:

Springbank Hill (Ward 06)

FILE NUMBER:

LOC2018-0085 (CPC2019-1079)

PROPOSED POLICY AMENDMENTS:

Springbank Area Structure Plan

PROPOSED REDESIGNATION:

From: DC Direct Control District

To: Mixed-Use - General (MU-1f5.0h50) District,
Mixed-Use - General (MU-1f3.0h20) District, DC
Direct Control District, Multi-Residential – Low
Profile (M-1) District, Special Purpose – School,
Park and Community Reserve (S-SPR) District,
Special Purpose – City and Regional
Infrastructure (S-CRI) District and Special
Purpose – Urban Nature (S-UN) District

MUNICIPAL ADDRESS:

1880, 2188 and 2220 – 85 Street SW, 2027 – 81 Street
SW and 8361 and 8473 – 17 Avenue SW

APPLICANT:

O2 Designs

OWNER:

Aspen Springs GP Ltd

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.8

Joseph Yun
(Related to Item 7.2.7)

COMMUNITY:

Springbank Hill (Ward 06)

FILE NUMBER:

LOC2018-0085(OP) (CPC2019-1333)

PROPOSED OUTLINE PLAN:

Subdivision of 10.65 hectares \pm (26.32 acres \pm)

MUNICIPAL ADDRESS:

1880, 2188 and 2220 – 85 Street SW, 2027 – 81 Street
SW and 8361 and 8473 – 17 Avenue SW

APPLICANT:

O2 Designs

OWNER:

Aspen Springs GP Ltd

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 7.2.9

Kate van Fraassen

COMMUNITY:

Ramsay (Ward 9)

FILE NUMBER:

LOC2016-0088 (CPC2019-0695)

PROPOSED POLICY AMENDMENTS:

Amendments to the Ramsay Area Redevelopment Plan

PROPOSED REDESIGNATION:

From: Residential Contextual One / Two Dwelling
(R-C2) District

To: DC Direct Control District a broad range of
residential development including multi-
residential buildings

MUNICIPAL ADDRESS:

1105, 1107, 1109, 1111, 1113, 1115, 1117, 1121, 1123,
1125, 1129, 1131, 1133, 1135, 1137, 1139, 1141, 1143,
1145 and 1147 – 8 Street SE, 803 and 805 - 11 Avenue
SE and 1110 and 1120 Maggie Street SE

APPLICANT:

Mediated Solutions

OWNER:

Various Owners

ADMINISTRATION RECOMMENDATION:

REFUSAL



MINUTES

CALGARY PLANNING COMMISSION

**November 7, 2019, 1:00 PM
IN THE COUNCIL CHAMBER**

PRESENT: Director M. Tita, Chair
Director R. Vanderputten, Vice-Chair
Councillor E. Woolley
Commissioner P. Gedye
Commissioner L. Juan
Commissioner A. Palmiere
Commissioner K. Schmalz
Commissioner J. Scott

ALSO PRESENT: A/Principal Planner M. Beck
A/CPC Secretary J. Palaschuk

1. **CALL TO ORDER**

Director Tita called the meeting to order at 1:02 p.m.

2. **OPENING REMARKS**

Director Tita provided opening remarks at today's Meeting.

3. **CONFIRMATION OF AGENDA**

Moved by Commissioner Scott

That the Agenda for today's meeting be amended by pulling Item 5.3, Report CPC2019-1354 off the Consent Agenda.

MOTION CARRIED

Moved by Commissioner Scott

That the Agenda for the 2019 November 7 Regular Meeting of the Calgary Planning Commission be confirmed, **as amended**.

MOTION CARRIED

4. **CONFIRMATION OF MINUTES**

4.1 Minutes of the Regular Meeting of the Calgary Planning Commission, 2019
October 17

Moved by Commissioner Scott

That the Minutes of the 2019 October 17 Regular Meeting of the Calgary Planning Commission be amended by adding the following clerical corrections to Item 7.2.12, Report CPC2019-1236:

1. ADOPT, by bylaw, the proposed closure of 1.54 hectares \pm (3.8 acres \pm) of road (Plan **1911701**, Area 'A') adjacent to and between the NW1/4 Section and NW1/4 Section 15 within TWP 22 RGE 29 W4, with conditions (Attachment 1); and
2. Give three readings to the proposed closure bylaw.
3. ADOPT, by bylaw, the proposed redesignation of 130.5 hectares \pm (322.4 acres \pm) located at 19610 and 20706 – 72 Street SE (W1/2 Section 14-22-29-4; **Plan 1911701, Area 'A'**) from Special Purpose – Future Urban Development (S-FUD) District and Undesignated Road Right-of-Way to Residential – Narrow Parcel One Dwelling (R-1N) District, Residential – Low Density Mixed Housing (R-G) District, Residential – Low Density Mixed Housing (R-Gm) District, Multi-Residential – Medium Profile (M-2) District, Mixed Use - General (MU-1f2.0d154h16) District, Commercial – Community 2 (C-C2f0.5h10) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – City and Regional Infrastructure (S-CRI) **District**, and Special Purpose – Recreation (S-R) District; and
4. Give three readings to the proposed bylaw.

MOTION CARRIED

Moved by Commissioner Scott

That the Minutes of the 2019 October 17 Regular Meeting of the Calgary Planning Commission be confirmed, **as amended**.

MOTION CARRIED

5. **CONSENT AGENDA**

Moved by Commissioner Gedy

That the Committee Recommendations contained in the following reports be approved in an omnibus motion:

- 5.4 Land Use Amendment in Wolf Willow (Ward 14) at 2107 - 194 Avenue SE, LOC2019-0124, CPC2019-1344

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed redesignation of 0.007 hectares \pm (0.017 acres \pm) located at 2107 - 194 Avenue SE (Portion of Plan 0712785, Block 1, Lot 1) from Commercial – Community 1 (C-C1) District to Special Purpose – Recreation (S-R) District; and

2. Give three readings to the proposed Bylaw.

5.5 Proposed Street Name Change – 128 Avenue NW to Symons Valley Parkway NW, in multiple communities (Ward 2), SN2019-0009, CPC2019-1357

That Calgary Planning Commission recommend that Council:

1. ADOPT, by Resolution, the proposed street name of Symons Valley Parkway NW.

MOTION CARRIED

5.1 DEFERRALS AND PROCEDURAL REQUESTS

None

5.2 BRIEFINGS

None

5.3 Policy Amendment and Land Use Amendment in Tuxedo Park (Ward 7) at 140 - 25 Avenue NE, LOC2019-0064, CPC2019-1354

A clerical correction was noted on page 1 of 9 of the Cover Report, in Recommendation 1, by deleting the word "Attachment 2" and by substituting the word "Attachment 3".

Moved by Commissioner Schmalz

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendment to the North Hill Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. ADOPT, by bylaw, the proposed redesignation of 0.04 hectares ± (0.10 acres ±) located at 140 - 25 Avenue NE (Plan 2617AG, Block 12, Lot 1 and portion of Lot 2) from the Residential – Contextual One / Two Dwelling (R-C2) District to Residential – Grade - Oriented Infill (R-CG) District; and
4. Give three readings to the proposed bylaw.

MOTION CARRIED

6. POSTPONED REPORTS

None

7. ITEMS FROM OFFICERS, ADMINISTRATION AND COMMITTEES

7.1 DEVELOPMENT ITEMS

None

7.2 PLANNING ITEMS

Commissioner Schmalz declared a conflict of interest and abstained from discussion and voting with respect to Reports CPC2019-1387 and CPC2019-1386. Commissioner Schmalz left the Council Chamber at 1:07 p.m. and returned at 1:34 p.m. after the vote was declared.

7.2.1 Policy Amendment to the Keystone Hills Area Structure Plan, CPC2019-1387

A document entitled "Calgary Planning Commission Comments and Administration Follow-up" was distributed with respect to Report CPC2019-1387.

A clerical correction was noted on page 7 of 10 of the Cover Report, under Calgary Planning Commission Review, first sentence, by deleting the words "September 19" and by substituting the words "September 05".

A clerical correction was noted on page 7 of 10 of the Cover Report, under Calgary Planning Commission Review, in the second paragraph, first sentence, by add the words "(Attachment 3)" following the words "to the proposed policies".

A clerical correction was noted on page 10 of 10 of the Cover Report, under Attachments, by adding a new Attachment 3, to read as follows: "Attachment 3 - Calgary Planning Commission Comments and Administrative Follow-up."

Moved by Commissioner Palmiere

That with respect to Report CPC2019-1387, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendments to the Keystone Hills Area Structure Plan (Attachment 1); and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

7.2.2 Land Use Amendment in Livingston (Ward 3) for multiple proerties, LOC2018-0213, CPC2019-1386

A clerical correction was noted on all pages of the Cover Report, in the header, by deleting all instances of the word "proerties" and by substituting the word "properties".

Moved by Commissioner Palmiere

That with respect to Report CPC2019-0213, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed redesignation of 17.48 hectares (43.19 acres) located at 200 and 500 – 144 Avenue NW and 300 –

144 Avenue NE (Portion of SE1/4 Section 4-26-1-5; Portion of SW1/4 Section 3-26-1-5) from Special Purpose Future Urban Development (S-FUD) District to DC Direct Control District to accommodate mixed-use development, with guidelines (Attachment 2); and

2. Give three readings to the proposed bylaw.
3. ADOPT, bylaw, the proposed redesignation of 4.10 hectares (10.13 acres) located at 14224 Centre Street NE (Portion of NW1/4 Section 34-25-1-5) from Special Purpose Future Urban Development (S-FUD) District to DC Direct Control District to accommodate commercial development, with guidelines (Attachment 3); and
4. Give three readings to the proposed bylaw.

MOTION CARRIED

7.2.3 Land Use Amendment in Skyview Ranch (Ward 5) at 6011 Country Hills Boulevard NE, LOC2014-0165, CPC2019-1367

Item 7.2.3, Report CPC2019-1367 was heard in conjunction with Item 7.2.4, Report CPC2019-1133.

A clerical correction was noted on the header of Attachment 4 of Report CPC2019-1367, by adding the words "(MU-1 / North)" following the words "District Guidelines".

A clerical correction was noted on the header of Attachment 5 of Report CPC2019-1367, by adding the words "(MU-1 / South)" following the words "District Guidelines".

A clerical correction was noted on the header of Attachment 6 of Report CPC2019-1367, by adding the words "(M-G / S-R)" following the words "District Guidelines".

A clerical correction was noted on page 15 of 15 of the Cover Report, in Attachments, by adding the words " / North)" following the words "(MU-1" of Attachment 4 and " / South)" following the words "(MU-1" of Attachment 5.

Moved by Councillor Woolley

That with respect to Report CPC2019-1367, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed redesignation of 25.53 hectares \pm (63.08 acres \pm) located at 6011 Country Hills Boulevard NE (Portion of NW1/4 of Section 23-25-29-4) from Special Purpose – Future Urban Development (S-FUD) District, Commercial – Corridor 1 f8.0h80 (C-COR1f8.0h80) District and Special Purpose – School, Park, and Community Reserve (S-SPR) District to Multi-Residential – High Density Low Rise (M-H1) District, Multi-Residential – Medium Profile (M-2) District, Multi-Residential – At Grade Housing (M-G)

District, Residential – Low Density Mixed Housing (R-G) District, Commercial – Neighbourhood 2 (C-N2) District, Commercial – Community 2 f3.0h30 (C-C2f3.0h30) District, Special Purpose – City and Regional Infrastructure (S-CRI) District, Special Purpose – Urban Nature (S-UN) District, and Special Purpose – School, Park and Community Reserve (S-SPR) District; and

2. Give three readings to the proposed bylaw.
3. ADOPT, by bylaw, the proposed redesignation of 3.06 hectares \pm (7.56 acres \pm) located at 6011 Country Hills Boulevard NE (Portion of NW1/4 of Section 23-25-29-4) from Commercial – Corridor 1 f8.0h80 (C-COR1f8.0h80) District to DC Direct Control District to accommodate mixed-use commercial, with guidelines (Attachment 4); and
4. Give three readings to the proposed bylaw.
5. ADOPT, by bylaw, the proposed redesignation of 2.07 hectares \pm (5.12 acres \pm) located at 6011 Country Hills Boulevard NE (Portion of NW1/4 of Section 23-25-29-4) from Special Purpose – Future Urban Development (S-FUD) District, Commercial – Corridor 1 f8.0h80 (C-COR1f8.0h80) District and Special Purpose – School, Park, and Community Reserve (S-SPR) District to DC Direct Control District accommodate mixed-use commercial (park and ride), with guidelines (Attachment 5); and
6. Give three readings to the proposed bylaw.
7. ADOPT, by bylaw, the proposed redesignation of 0.95 hectares \pm (2.35 acres \pm) located at 6011 Country Hills Boulevard NE (Portion of NW1/4 of Section 23-25-29-4) from Special Purpose – Future Urban Development (S-FUD) District to DC Direct Control District accommodate medium density residential with added recreation uses, with guidelines (Attachment 6); and
8. Give three readings to the proposed bylaw.

MOTION CARRIED

- 7.2.4 Outline Plan in Skyview Ranch and Cityscape (Ward 5) at 6011 Country Hills Boulevard NE, LOC2014-0165 (OP), CPC2019-1133

Item 7.2.4, Report CPC2019-1133 was heard in conjunction with Item 7.2.3, Report CPC2019-1367.

A revised page 4 of 16 of the Cover Report was distributed with respect to Report CPC2019-1133.

Item 7.2.4, Report CPC2019-1133 was heard in conjunction with Item 7.2.3, Report CPC2019-1367.

A revised page 4 of 16 of the Cover Report was distributed with respect to Report CPC2019-1133. A clerical correction was noted on page 16 of 16

of the Cover Report, in Attachments, by deleting the words "Subdivision" for Attachment 5 and by substituting with the word "Outline".

Moved by Councillor Woolley

That with respect to Report CPC2019-1133, the following be approved:

That Calgary Planning Commission APPROVE the proposed outline plan located at 6011 Country Hills Boulevard NE (NW1/4 of Section 23-25-29-4) to subdivide 35.09 hectares \pm (86.71 acres \pm), with conditions (Attachment 1).

MOTION CARRIED

- 7.2.5 Land Use Amendment in Sage Hill (Ward 2) at 3655 Sage Hill Drive NW and 625 Sage Valley Boulevard NW, LOC2017-0404, CPC2019-1370

Item 7.2.5, Report CPC2019-1370 was heard in conjunction with Item 7.2.6, Report CPC2019-1369.

The following speakers addressed Commission with respect to Reports CPC2019-1370 and CPC2019-1369:

1. Kathy Oberg, B&A Planning
2. Patrick Wetter, B&A Planning
3. Darcy Murnaghan, Genesis Land Development Corporation

Moved by Commissioner Schmalz

That with respect to Report CPC2019-1370, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and,

1. ADOPT, by bylaw, the proposed redesignation of 20.85 hectares \pm (51.51 acres \pm) located at 3655 Sage Hill Drive NW and 625 Sage Valley Boulevard NW (SE1/4 Section 36,25,2,5; Plan 1810140, Block 1, Lot 4) from DC Direct Control District, Commercial – Regional 3 f0.3h18 (C-R3 f0.3h18) District and Commercial – Community 1 (C-C1) District to Residential – Low Density Mixed Housing (R-G) District, Residential – Low Density Mixed Housing (R-Gm) District, Multi-Residential – At Grade Housing (M-G) District, Multi-Residential – Medium Profile (M-2) District, Mixed Use – General (MU-1h22) District, Commercial – Community 1 (C-C1) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, and Special Purpose – Urban Nature (S-UN) District; and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

- 7.2.6 Outline Plan in Sage Hill (Ward 2) at 3655 Sage Hill Drive NW and 625 Sage Valley Boulevard NW, LOC2017-0404 (OP), CPC2019-1369

Item 7.2.6, Report CPC2019-1369 was heard in conjunction with Item 7.2.5, Report CPC2019-1370.

The following speakers addressed Commission with respect to Reports CPC2019-1369 and CPC2019-1370:

1. Kathy Oberg, B&A Planning
2. Patrick Wetter, B&A Planning
3. Darcy Murnaghan, Genesis Land Development Corporation

Moved by Commissioner Schmalz

That with respect to Report CPC2019-1219, the following be approved:

That Calgary Planning Commission APPROVE the proposed outline plan located at 3655 Sage Hill Drive NW and 625 Sage Valley Boulevard NW (SE1/4 Section 36-25-2-5; Plan 1810140, Block 1, Lot 4) to subdivide the 20.85 hectares \pm (51.51 acres \pm), with conditions (Attachment 1).

MOTION CARRIED

7.2.7 Land Use Amendment in Highland Park (Ward 4) at 4111 - 1 Street NE, LOC2019-0115, CPC2019-1337

The following documents were distributed with respect to Report CPC2019-1337:

- A revised page 2 of 9 of the Cover Report; and
- A letter from Thorncliff Greenview Community Association, dated 2019 November 03.

Moved by Commissioner Juan

That the letter from the Community Association with respect to Report CPC2019-1337 be included with the Report on to Council.

MOTION CARRIED

Moved by Commissioner Scott

That with respect to Report CPC2019-1337, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed redesignation of 0.23 hectares \pm (0.58 acres \pm) located at 4111 - 1 Street NE (Plan 9212522, Area A) from Residential – Contextual One / Two Dwelling (R-C2) District to Multi-Residential – High Density Low Rise (M-H1) District; and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

- 7.2.8 Land Use Amendment in Evergreen (Ward 13) at 167 Evermeadow Avenue SW, LOC2019-0056, CPC2019-1355

Moved by Commissioner Juan

That with respect to Report CPC2019-1355, the following be approved:

That Calgary Planning Commission recommends that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed redesignation of 0.04 hectares \pm (0.09 acres \pm) located at 167 Evermeadow Avenue SW (Plan 0511297, Block 9, Lot 80) from Residential – Narrow Parcel One Dwelling (R-1N) District to DC Direct Control District to accommodate the additional use Child Care Service, with guidelines (Attachment 2); and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

- 7.2.9 Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1603 – 37 Avenue SW, LOC2019-0137, CPC2019-1338

Moved by Commissioner Gedye

That with respect to Report CPC2019-1338, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendment to the South Calgary/Altadore Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. ADOPT, by bylaw, the proposed redesignation of 0.05 hectares \pm (0.12 acres \pm) located at 1603 – 37 Avenue SW (Plan 2869AC, Block A, Lots 23 and 24) from Residential – Contextual One / Two Dwelling (R-C2) District to Multi-Residential – Contextual Grade-Orientated (M-CGd80) District; and
4. Give three readings to the proposed bylaw.

MOTION CARRIED

- 7.2.10 Policy Amendment, Road Closure and Land Use Amendment in Bowness (Ward 1) at multiples properties, CPC2019-1339

Commission, by general consent, suspended Section 78(2)(b) of the Procedure Bylaw 35M2017, as amended, in order for Commission to finish the item prior to the afternoon recess.

Moved by Commissioner Scott

That with respect to Report CPC2019-1339, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendments to the Bowness Area Redevelopment Plan (Attachment 1); and
2. Give three readings to the proposed bylaw.
3. ADOPT, by bylaw, the proposed closure of 0.55 hectares \pm (1.36 acres \pm) of road (Plan 1911928, Area 'A') adjacent to 3412 - 69 Street NW and 6903 to 6935 - 33 Avenue NW and lane running east/west between 32 Avenue NW and 33 Avenue NW and a portion of 68 Street NW, with conditions (Attachment 3); and
4. Give three readings to the proposed closure bylaw.
5. ADOPT, by bylaw, the proposed redesignation of 0.97 hectares \pm (2.4 acres \pm) located at 3520 and 3532 - 69 Street NW (Plan 9812432, Block D, Lot 3; Plan 4608FO, Block C) from DC Direct Control District to Multi-Residential – High Density Medium Rise (M-H2h35) District; and
6. Give three readings to the proposed bylaw.
7. ADOPT, by bylaw, the proposed redesignation of 1.41 hectares \pm (3.48 acres \pm) located at 3439 - 69 Street NW (Plan 1339JK, Block 4) from DC Direct Control District to DC Direct Control District to accommodate commercial, residential and light-industrial development, with guidelines (Attachment 4); and
8. Give three readings to the proposed bylaw.
9. ADOPT, by bylaw, the proposed redesignation of 3.76 hectares \pm (9.29 acres \pm) located at various municipal address (Attachment 5) and the closed road from DC Direct Control District, Residential – Contextual One Dwelling (R-C1) District and Undesignated Road Right-of-Way to DC Direct Control District to accommodate commercial, residential and light-industrial development, with guidelines (Attachment 6); and
10. Give three readings to the proposed bylaw.

MOTION CARRIED

Commission recessed at 3:30 p.m and reconvened at 3:50 p.m. with Director Tita in the Chair.

- 7.2.11 Land Use Amendment in East Shepard Industrial (Ward 12) at 5315 Dufferin Boulevard SE, LOC2019-0117, CPC2019-1365

Moved by Commissioner Schmalz

That with respect to Report CPC2019-1365, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing and:

1. ADOPT, by bylaw, the proposed redesignation of 0.67 hectares \pm (1.65 acres \pm) located at 5315 Dufferin Boulevard SE (Plan 1811967, Block 2, Lot 7) from Industrial – General (I-G) District to Industrial – Commercial (I-C) District; and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

7.2.12 Land Use Amendment in Manchester Industrial at 6420 - 1A Street SW, LOC2019-0108, CPC2019-1356

David Jacobs, David Jacobs Consulting, addressed Commission with respect to Report CPC2019-1356.

Moved by Commissioner Palmiere

That with respect to Report CPC2019-1356, the following be approved:

That Calgary Planning Commission recommend Council hold a Public Hearing; and

1. ADOPT, by bylaw the proposed redesignation of 1.70 hectares \pm (4.21 acres \pm) located at 6420 - 1A Street SW (Plan 9212031, Block C, Lot 3A) from Commercial – Corridor 3 f1.0h12 (C-COR3 f1.0h12) District to DC Direct Control to accommodate the additional use of Place of Worship – Large and mixed-use development, with guidelines (Attachment 2); and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

7.2.13 Policy Amendment and Land Use Amendment in Bridgeland / Riverside (Ward 9) at 717, 721, and 723 McDougall Road NE, LOC2017-0405, CPC2019-1301

Moved by Commissioner Palmiere

That with respect to Report CPC2019-1301, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendment to the Bridgeland-Riverside Area Redevelopment Plan (Attachment 1); and
2. Give three readings to the proposed bylaw.
3. ADOPT, by bylaw, the proposed redesignation of 0.11 hectares \pm (0.27 acres \pm) located at 717, 721 and 723 McDougall Road NE (Plan 4301R, Block 1, Lots 23 to 26) from the Multi-Residential – Contextual

Low Profile (M-C1) District to Multi-Residential – Contextual Medium Profile (M-C2) District; and

4. Give three readings to the proposed bylaw.

MOTION CARRIED

7.2.14 Policy Amendment and Land Use Amendment in Inglewood (Ward 9) at 915 – 9 Avenue SE, LOC2018-0038, CPC2019-1174

Elika Tehrani, Hungerford Properties, addressed Commission with respect to Report CPC2019-1174.

Commission, by general consent, tabled Report CPC2019-1174, to the call of the Chair.

Report CPC2019-1174 was lifted from the table.

Moved by Councillor Woolley

That with respect to Report CPC2019-1174, Attachment 3 be amended, as follows:

1. Amend Section 10, new subsection (3) before “maximum **building height**” delete “The” and insert “Notwithstanding subsections (1) and (2), the”
2. Amend Section 10, insert new subsection (4) to read:

“(4) Where the height of a **building** is greater than 14.0 metres measured from **grade**, the façade of the **building** facing 9 Avenue SE and 8 Street SE must have a horizontal separation from the portion of the façade closest to **grade** such that:

 - (a) the horizontal separation has a minimum depth of 2.0 metres; and
 - (b) the horizontal separation occurs between a minimum of 7.5 metres and a maximum of 14.0 metres measured from **grade**.”;

and renumber the Section accordingly.
3. Insert new Section 11 entitled “Street Wall Stepback” to read
“**Street Wall Stepback**
11 Section 1338 of Bylaw 1P2007 does not apply to this Direct Control District Bylaw.”;
and renumber accordingly.

MOTION CARRIED

Moved by Director Vanderputten

That with respect to Report CPC2019-1174, Attachment 3 be amended in Section 8, as follows:

Floor Area Ratio

8 (1) Unless otherwise referenced in subsection (2), the maximum floor area ratio is 2.8.

(2) The maximum floor area ratio referenced in subsection (1) may be increased to 4.4 **where:**

(a) the building existing on the parcel, **as of the effective date of this Direct Control District Bylaw**, located at 921 – 9 Avenue SE and legally described as Plan A2, Block 10, Lot 11 is designated as a “Municipal Historic Resource”;

(b) a minimum of 5.0 per cent of the total number of units within the development are “universally accessible dwelling units”; and

(c) a “publicly accessible private open space” not less than 150 square metres with no dimension less than 3.0 metres facing 8 Street SE and 9 Avenue SE **is located within the development, and where this open space** area must be maintained for the life of the development.

MOTION CARRIED

Moved by Councillor Woolley

That with respect to Report CPC2019-1174, the following be approved, **as amended:**

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendments to the Inglewood Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. ADOPT, by bylaw, the proposed redesignation of 0.25 hectares \pm (0.61 acres \pm) located at 915 – 9 Avenue SE (Plan 9411558, Block 10, Lot 14) from Industrial – Commercial (I-C) District and Commercial – Corridor 2 f2.8h12 (C-COR2 f2.8h12) District to DC Direct Control District to accommodate a mixed-use development, with guidelines (Attachment 3) **as amended**; and
4. Give three readings to the proposed Bylaw.

MOTION CARRIED

Commissioner Palmiere declared a conflict of interest and abstained from discussion and voting with respect to Report CPC2019-1364.

Commissioner Palmiere left the Council Chamber at 4:44 p.m. and returned at 4:54 p.m. after the vote was declared.

7.2.15 Land Use Amendment in Inglewood (Ward 9) at 922 – 8 Avenue SE, LOC2019-0109, CPC2019-1364

Brian Horton, O2 Planning and Design, addressed Commission with respect to Report CPC2019-1364.

Moved by Commissioner Scott

That with respect to Report CPC2019-1364, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed redesignation of 0.04 hectares \pm (0.09 acres \pm) located at 922 – 8 Avenue SE (Plan A2, Block 4, Lot 11) from Residential – Contextual One / Two Dwelling District (R-C2) District to DC Direct Control District to accommodate Reading Place, with guidelines (Attachment 1); and
2. Give three readings to the proposed bylaw.

MOTION CARRIED

7.2.16 Policy Amendment and Land Use Amendment in Bridgeland - Riverside (Ward 9) at 520 - 5 Street NE, LOC2019-0116, CPC2019-1352

A clerical correction was noted on Attachment 2 of Report CPC2019-1352, be deleting clause b in its entirety.

Moved by Commissioner Schmalz

That with respect to Report CPC2019-1352, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendments to the Bridgeland-Riverside Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. ADOPT, by bylaw, the proposed redesignation of 0.07 hectares \pm (0.17 acres \pm) located at 520 - 5 Street NE (Condominium Plan 1511130) from Multi-Residential – Contextual Grade-Oriented (M-CGd111) District to Multi-Residential – Contextual Low Profile (M-C1) District; and
4. Give three readings to the proposed bylaw.

MOTION CARRIED

7.2.17 Policy Amendment and Land Use Amendment in Bridgeland - Riverside (Ward 9) at 612 - 4 Avenue NE, LOC2019-0083, CPC2019-1351

A clerical correction was noted on Attachment 2 of Report CPC2019-1351, be deleting clause b in its entirety.

Moved by Councillor Woolley

That with respect to Report CPC2019-1351, the following be approved:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. ADOPT, by bylaw, the proposed amendments to the Bridgeland-Riverside Area Redevelopment Plan (Attachment 2); and
2. Give three readings to the proposed bylaw.
3. ADOPT, by bylaw, the proposed redesignation of 0.38 hectares \pm (0.94 acres \pm) located at 612 - 4 Avenue NE (Plan D, Block 2, portions of Lots 22 and 23) from Multi-Residential – Contextual Grade-Oriented (M-CGd111) District to Multi-Residential – Contextual Medium Profile (M-C2) District; and
4. Give three readings to the proposed bylaw.

MOTION CARRIED

7.3 MISCELLANEOUS ITEMS

None

8. URGENT BUSINESS

None

9. ADJOURNMENT

Moved by Director Vanderputten

That this Meeting adjourn at 5:30 p.m.

MOTION CARRIED

THE FOLLOWING ITEMS HAVE BEEN FORWARDED TO THE 2019 DECEMBER 16 COMBINED MEETING OF COUNCIL:

- Policy Amendment and Land Use Amendment in Tuxedo Park (Ward 7) at 140 - 25 Avenue NE LOC2019-0064, CPC2019-1354
- Land Use Amendment in Wolf Willow (Ward 14) at 2107 - 194 Avenue SE, LOC2019-0124, CPC2019-1344
- Policy Amendment to the Keystone Hills Area Structure Plan, CPC2019-1387
- Land Use Amendment in Livingston (Ward 3) for multiple properties, LOC2018-0213, CPC2019-1386
- Land Use Amendment in Skyview Ranch (Ward 5) at 6011 Country Hills Boulevard NE, LOC2014-0165, CPC2019-1367
- Land Use Amendment in Sage Hill (Ward 2) at 3655 Sage Hill Drive NW and 625 Sage Valley Boulevard NW, LOC2017-0404, CPC2019-1370

- Land Use Amendment in Highland Park (Ward 4) at 4111 - 1 Street NE, LOC2019-0115, CPC2019-1337
- Land Use Amendment in Evergreen (Ward 13) at 167 Evermeadow Avenue SW, LOC2019-0056, CPC2019-1355
- Policy Amendment and Land Use Amendment in Altadore (Ward 8) at 1603 – 37 Avenue SW, LOC2019-0137, CPC2019-1338
- Policy Amendment, Road Closure and Land Use Amendment in Bowness (Ward 1) at multiples properties, CPC2019-1339
- Land Use Amendment in East Shepard Industrial (Ward 12) at 5315 Dufferin Boulevard SE, LOC2019-0117, CPC2019-1365
- Land Use Amendment in Manchester Industrial at 6420 - 1A Street SW, LOC2019-0108, CPC2019-1356
- Policy Amendment and Land Use Amendment in Bridgeland / Riverside (Ward 9) at 717, 721, and 723 McDougall Road NE, LOC2017-0405, CPC2019-1301
- Policy Amendment and Land Use Amendment in Inglewood (Ward 9) at 915 – 9 Avenue SE, LOC2018-0038, CPC2019-1174
- Land Use Amendment in Inglewood (Ward 9) at 922 – 8 Avenue SE, LOC2019-0109, CPC2019-1364
- Policy Amendment and Land Use Amendment in Bridgeland - Riverside (Ward 9) at 520 - 5 Street NE, LOC2019-0116, CPC2019-1352
- Policy Amendment and Land Use Amendment in Bridgeland - Riverside (Ward 9) at 612 - 4 Avenue NE, LOC2019-0083, CPC2019-1351

PLANNING MATTERS NOT REQUIRING PUBLIC HEARING:

- Proposed Street Name Change - 128 Avenue NW to Symons Valley Parkway NW, in Multiple Communities (Ward 2), SN2019-0009, CPC2019-1357

The next Regular Meeting of the Calgary Planning Commission is scheduled to be held 2019 November 21 at 1:00 p.m.

PLANNING MATTERS FOR PUBLIC HEARING:

CONFIRMED BY COMMISSION ON

CHAIR

ACTING CPC SECRETARY

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1031

Land Use Amendment in Renfrew (Ward 9) at 639 and 641 – 12 Avenue NE, LOC2019-0125

EXECUTIVE SUMMARY

This land use amendment application was submitted by New Century Design on behalf of the landowners 2193553 Alberta Ltd (William Yeung and Preetpaul Bal) on 2019 July 30. This application proposes to change the designation of the subject site from Residential – Contextual One / Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District to allow for:

- rowhouses in addition to building types already allowed (e.g. single detached, semi-detached, duplex dwellings and secondary suites);
- a maximum building height of 11 metres (an increase from the current maximum of 10 metres);
- a maximum of 4 dwelling units (an increase from the current maximum of 2 dwelling units); and
- the uses listed in the R-CG District.

The proposal is in keeping with applicable policies of the *Municipal Development Plan* (MDP). There is no local area plan for Renfrew.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.14 acres \pm) located at 639 and 641 – 12 Avenue NE (Plan 470P, Block 54, Lots 39 and 40) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Residential – Grade-Oriented Infill (R-CG) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

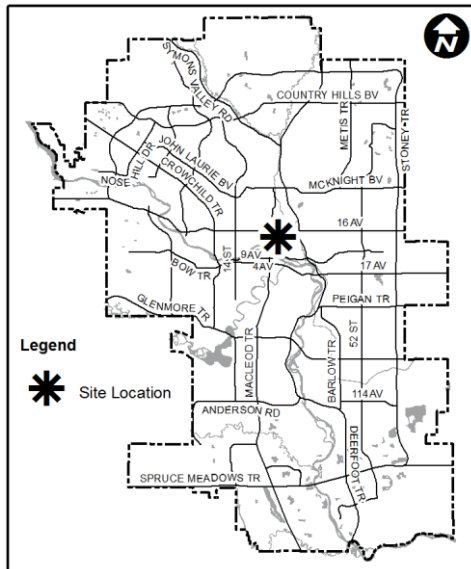
This land use amendment application was submitted by New Century Design on behalf of the landowners 2193553 Alberta Ltd (William Yeung and Preetpaul Bal) on 2019 July 30. No development permit application has been submitted at this time, however, as noted in the Applicant's Submission (Attachment 1), the applicant intends to pursue a development permit application for a three or four-unit rowhouse development in the future.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1031

Land Use Amendment in Renfrew (Ward 9) at 639 and 641 - 12 Avenue NE,
LOC2019-0125

Location Maps



Land Use Amendment in Renfrew (Ward 9) at 639 and 641 - 12 Avenue NE, LOC2019-0125

Site Context

The subject site, which consists of two separate parcels, is located at the southwest corner of 12 Avenue NE and 6 Street NE in the community of Renfrew. The site is approximately 0.06 hectares in size, with approximately 15 metres of frontage along 12 Avenue NE and 37 metres of frontage on 6 Street NE. A gravel residential lane exists to the south of the site. The site is currently undeveloped.

The surrounding area is characterized as a low-density residential area comprised of a mix of single and semi-detached dwellings. The site is located four blocks south of 16 Avenue NE and three blocks east of Edmonton Trail NE where commercial and local service developments exist.

As identified in *Figure 1*, the community of Renfrew has seen a decline in population since its peak in 1968.

Figure 1: Community Peak Population

Renfrew	
Peak Population Year	1968
Peak Population	8,019
2018 Current Population	6,524
Difference in Population (Number)	-1,495
Difference in Population (Percent)	-18%

Source: *The City of Calgary 2018 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Renfrew](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This proposal represents a modest increase in density on a corner site in an inner-city area, and allows for a range of housing forms that have the ability to be compatible with the scale and character of the existing neighbourhood. The proposal is consistent with applicable policies as discussed in the Strategic Alignment section of this report.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

**Land Use Amendment in Renfrew (Ward 9) at 639 and 641 - 12 Avenue NE,
LOC2019-0125**

Land Use

The existing Residential – Contextual One / Two Dwelling (R-C2) District is a residential designation applied to developed areas that is primarily for single detached, semi-detached and duplex dwellings. Single detached dwellings may include a secondary suite, but semi-detached dwellings may not. The R-C2 District allows for a maximum building height of 10 metres and a maximum of two dwelling units.

The proposed Residential – Grade-Oriented Infill (R-CG) District allows for two to three storey (11 metres maximum height) rowhouse developments where one façade of each dwelling unit must directly face a public street. The District allows for a maximum density of 75 units per hectare which would allow for up to four dwelling units on the subject site. The R-CG District also allows for a range of other low density housing forms including single detached, semi-detached and duplex dwellings. Backyard suites or secondary suites are also allowable in R-CG developments.

Development and Site Design

While a development permit application has not been submitted, the rules of the proposed Residential – Grade-Oriented Infill (R-CG) District will provide basic guidance for the site development including height and building massing, landscaping and parking. Given the specific context of this corner site, additional items that will be considered through the development permit process will include, but are not limited to:

- ensuring an engaging built interface along both 12 Avenue NE and 6 Street NE;
- emphasising individual at-grade entrances;
- provision of parking for the rowhouse development; and
- mitigation of shadowing, overlooking and privacy concerns.

Environmental

There are no known environmental concerns associated with the site or this proposal. As such, no Environmental Site Assessment was required.

Transportation

Pedestrian and vehicular access to the site is available from 12 Avenue NE, 6 Street NE and the rear lane. The area is served by Calgary Transit bus service Route 17 Renfrew / Ramsay with a bus stop directly across the street from the site on 12 Avenue NE. On-street parking adjacent to the site is unregulated on both 12 Avenue NE and 6 Street NE. However, both 12 Avenue NE and 6 Street NE are classified as Snow Routes with parking restrictions in effect during a snow ban.

**Land Use Amendment in Renfrew (Ward 9) at 639 and 641 - 12 Avenue NE,
LOC2019-0125**

Utilities and Servicing

Water, sanitary and storm mains are available and can accommodate potential redevelopment of the subject site without the need for off-site improvements at this time. Individual servicing connections as well as appropriate stormwater management will be considered and reviewed as part of a development permit.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to external stakeholders and notice posted on site. Notification letters were also sent to adjacent landowners and the application was advertised online.

The Renfrew Community Association provided a letter of support regarding the proposed land use amendment on 2019 August 30 (Attachment 2).

In response to the circulation process, Administration received four letters of opposition to the application. Letters highlighted concerns including perception that a rowhouse is out of character for Renfrew, potential for loss of views and sunlight, potential for drainage issues and concerns related to parking and traffic.

Administration has considered the relevant planning issues specific to the proposed redesignation and has determined the proposal to be appropriate for the site as the R-CG District is intended to be located adjacent to other low-density districts and will accommodate a modest density increase within neighbourhoods. Parking and design considerations will be reviewed at the development permit stage.

Following Calgary Planning Commission, notifications for the Public Hearing of Council will be posted on site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSRP) which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed application builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

**Land Use Amendment in Renfrew (Ward 9) at 639 and 641 - 12 Avenue NE,
LOC2019-0125**

Municipal Development Plan (Statutory, 2009)

The subject site is located within the Residential – Developed – Inner City area of the *Municipal Development Plan* (MDP). The applicable MDP policies encourage redevelopment of inner city communities in a manner that is similar in scale and built form to existing development, including a mix of housing such as townhouses and rowhousing. The MDP also calls for modest intensification of the inner city areas serviced by existing infrastructure, public amenities and transit.

The proposal is in keeping with relevant MDP policies as the intent and rules of the R-CG District allow for a modest increase in density in a form that would be sensitive to existing residential development in terms of height, scale and massing.

There is no local plan in place for the community of Renfrew.

North Hill Communities Local Growth Planning

Administration is currently undertaking the *North Hill Communities Local Growth Planning* initiative, which includes the community of Renfrew. This multi-community planning process does not prohibit applications from being submitted. The local area plan is anticipated by Q1 2020.

Social, Environmental, Economic (External)

The proposed R-CG District allows for a wider range of housing types than the existing R-C2 District and as such, the proposed change may better accommodate the housing needs of different age groups, lifestyles and demographics. Furthermore, the ability to develop up to four rowhouse units will allow for more efficient use of existing infrastructure and services.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed land use amendment does not trigger capital infrastructure investment and there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1031

**Land Use Amendment in Renfrew (Ward 9) at 639 and 641 - 12 Avenue NE,
LOC2019-0125**

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with applicable policies of the *Municipal Development Plan*. The proposed R-CG District was designed to be implemented in proximity or directly adjacent to low density residential development. The proposal represents a modest increase in density of inner-city parcels of land and allows for development that has the ability to be compatible with the character of the existing neighbourhood.

ATTACHMENT(S)

1. Applicant's Submission
2. Community Association Letter

Applicant's Submission

July 31, 2019

639 & 641 12 Av NE

This application is being made for the redesignation of two R-C2 lots forming the corner of 12th Av and 6th St NE, in Renfrew. We are applying to redesignate to R-CG with the intention of developing a rowhouse up to 4 units with potential secondary suites (to be determined through design development). This lot is relatively flat with a combined dimension of 50' x 120', and is primarily surrounded by low-density residential R-C2 properties.

There is currently no buildings existing on the lot as it has been cleared in the past. The lot is fronting onto 12 Av to the North, with 6 St to the East, and gravel lane access to the South. There are currently two mature coniferous trees on the site straddling the East property line next to the public boulevard.

This site is approx. 200m South of 16 Av NE, and 500m East of Edmonton Trail. Within 300m to the Northeast is the Renfrew Aquatic and Rec Centre, public green space, and the Colonel Macleod School. Within 500m to the Southeast is the Renfrew Community Centre, public park, and the Stanley Jones School. Several other public green areas, schools, places of worship, businesses, and food establishments are within 1000m.

Transit is fairly accessible to this lot, including several varied bus stops. Directly adjacent along 12 Av NE is the #17 bus route. 400m to the West along Edmonton Trail is the #4, #5, and #69 bus routes. 450m to the North along 16 Av NE is the #19 and #303 bus routes. The future Green Line station at 16th Av and Centre Street N is approx. 1.4km to the Northwest.

Due to an abundance of nearby amenities, schools, transit, and open space, this parcel is in a great location for rowhouses. With three or four row houses on this lot, we will be able to provide residences at a reasonable cost in an excellent neighbourhood.

Update November 7, 2019

Through the design phase, we have determined that the project will only include 4 rowhouse units with no secondary suites. Design finalization is still in progress with the intention of submitting a DP before public hearing on the LOC.

Community Association Letter

From: Renfrew Planning [<mailto:planning@renfrewyc.ca>]
Sent: Friday, August 30, 2019 10:38 AM
To: Horkan, Melanie <Melanie.Horkan@calgary.ca>
Subject: [EXT] Re: LOC2019-0125 - Circulation Package

Hi Melanie,

Below are the Renfrew Planning Committee's comments on behalf of the Board of the Renfrew Community Association:

This vacant lot is an excellent location for the R-CG land use. It is at the intersection of two collector streets with a bus stop. Typically, the Boys and Girls Club on the northeast corner of the intersection has little need for on-street parking during the evening. Consequently, there should be space nearby for residents to park and cross the street. Thus, from transportation and parking perspectives, this seems like a good location for rowhouses with secondary suites.

We met with the applicant to discuss early ideas. We would encourage the applicant to apply for a concurrent development permit to speed up the process and add certainty to the neighbourhood.

The applicant also expressed a desire to use a curb cut to avoid moving the power pole in the lane. We would prefer moving the pole to avoid a curb cut next to an existing lane. This would reduce the number of points where people driving and people walking can come in contact. As such, we ask that the applicant contact Enmax early in the design process to make possible moving the power pole.

Thanks,
Nathan Hawryluk
Director, Planning
Renfrew Community Association

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1413

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

EXECUTIVE SUMMARY

This development permit application was submitted by NORR Architects Engineers Planners on 2019 March 01 on behalf of the landowner, Chartwell Retirement Residences. The proposed development is located in the community of Haysboro. This application proposes an older adult community, which includes:

- a 26-storey tower with a 14-storey podium;
- 320 independent living units; and
- 64 assisted living residential units.

Subject to the conditions attached, the proposal represents an appropriate development outcome. The proposal is consistent with the intent of the Multi-Residential – High Density High Rise land use bylaw district, the policies of the *Municipal Development Plan*, the guidelines of the *Transit Oriented Development Policy Guidelines*, and the principles of the *Planning Principles for the Location of Care Facilities and Shelters*.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission **APPROVE** the proposed development permit application DP2019-0978 for New: Assisted Living, Multi-Residential Development, Sign - Class A (address signs - 4) at 8610 Horton Road SW (Plan 0213832; Block 3; Lot 4) on 0.56 hectares \pm (1.39 acres \pm), with conditions (Attachment 1)

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

On 2018 May 24, pre-application/enquiry PE2018-01104 was submitted by NORR Architects Engineers Planners. The City of Calgary Corporate Planning Applications Group (CPAG) that reviewed the proposal recognized the physical challenges of the site and context, as well as the transit-oriented development opportunities available given the proximity of the site to the Heritage light rail transit (LRT) Station transit hub and adjacency to the Heritage LRT Station pedestrian bridge. CPAG identified studies required to assure responsible development of the site, stated openness to flexibility with regard to land use bylaw rules to achieve the goals of applicable policy and guidelines, and encouraged convenient, safe, and comfortable access to the Heritage Station.

After the completion of the pre-application/enquiry file and prior to submission of the development permit application, on 2018 December 12, the proposal was reviewed by the Urban Design Review Panel. The panel endorsed the proposal and recommended further

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1413

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

review. Additional information on the panel's review is available in the Urban Design Review Panel section of this report.

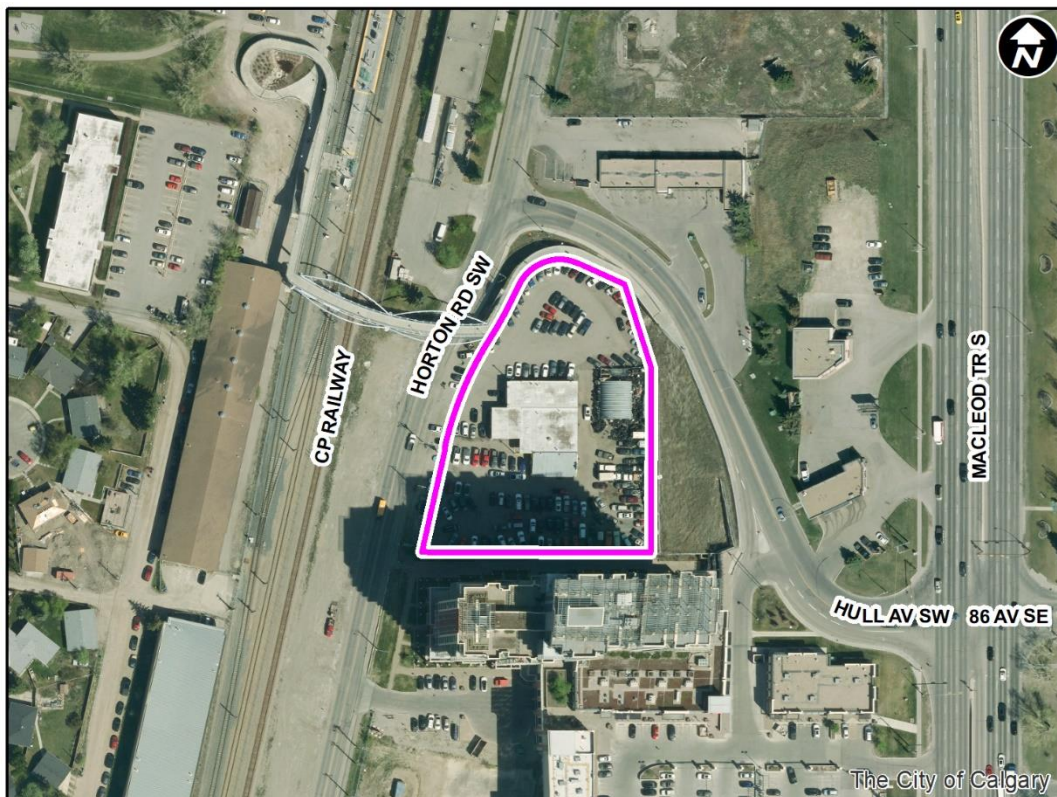
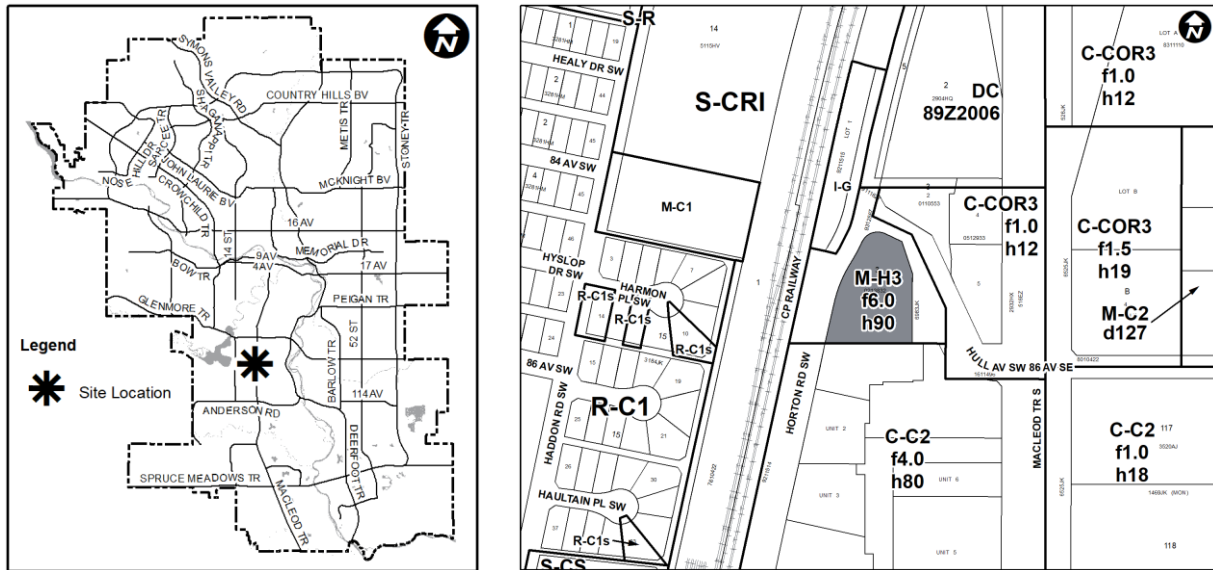
On 2019 March 01, Development Permit application DP2019-0978 was submitted by NORR Architects Engineers Planners. The Applicant's Submission is available in Attachment 2. Detailed team review documents were issued on this application. Working collaboratively with the applicant and the landowner, CPAG was able to move forward with a recommendation. All prior to decision conditions were addressed prior to sign up for the Calgary Planning Commission. Information on the application review is available in the Application Review section of this report.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1413

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

Location Maps



Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

Site Context

The site is located at the intersection of Horton Road SW and Hull Avenue SW in the community of Haysboro. The site is located near the intersection of Macleod Trail S and Heritage Drive S, where the boundaries of the communities of Haysboro, Kingsland, Fairview, and Acadia meet.

The site is approximately 0.56 hectares (1.39 acres) in size. The maximum north-south dimension of the site is approximately 95 metres, whereas the east-west dimension of the majority of the site ranges from approximately 35 metres to 75 metres. The site is flat, and level with Horton Road SW. The site is at the base of a slope that rises to the east. Until recently, the site was developed with a vehicle sales and servicing business.

To the west of the site, across Horton Road SW, is a Canadian Pacific Railway (CPR) corridor and Calgary Transit LRT corridor. The Heritage LRT Station is within the corridor, and there is a pedestrian bridge from Heritage Station to the east side of Horton Road SW. The landing of the pedestrian bridge in the road right-of-way adjacent to the northeast side of the site and a staircase descends from the pedestrian bridge, with a landing in the road right-of-way adjacent to the west side of the site.

To the northeast of the site, across Hull Avenue SW, is a Commercial – Corridor 3 f1.0h12 (C-COR3 f1.0h12) District site developed with an automotive service business building.

To the east of the site, Hull Avenue SW rises approximately 13 metres from its intersection with Horton Road SW to its intersection with Macleod Trail S. To the east of the site, across Hull Avenue SW, is a Commercial – Corridor 3 f1.0h12 (C-COR3 f1.0h12) District site developed with automotive service businesses buildings.

To the south of the site is a Commercial – Community 2 f4.0h80 (C-C2 f4.0h80) District that includes a commercial plaza development accessible from Macleod Trail S and two multi-residential development buildings (known as “London at Heritage Station”) accessible from Horton Road SW. Two additional multi-residential buildings were approved on the C-C2 f4.0h80 site, but development has not commenced. The commercial and residential development form one condominium corporation.

The main level of the commercial plaza to the south is located four floors above the main floor and lobby level of the multi-residential development buildings to the south. There is an elevator in the northeast corner of the C-C2 f4.0h80 site that provides access from the commercial plaza level to the multi-residential lobby level. The elevator does not provide access into any commercial nor multi-residential building. The elevator was intended to be publicly accessible but was closed to the public due to security concerns.

As identified in *Figure 1*, the community of Haysboro has seen the population generally decline since the population reached its peak in 1968. However, the population has increased since the population levelled off between the early 1980s and late 2000s.

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

Figure 1: Community Peak Population

Haysboro	
Peak Population Year	1968
Peak Population	8,044
2019 Current Population	7,165
Difference in Population (Number)	-878
Difference in Population (Percentage)	-10.9%

Source: The City of Calgary 2019 Civic Census

Additional demographic and socio-economic information can be obtained online through the [Haysboro](#) community profile at Calgary.ca.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This development permit application proposes development of a 26-storey tower with a 14-storey podium, 320 independent living units, and 64 assisted living units (Attachment 2).

Land Use

A land use redesignation application (LOC2007-0125) was submitted for the site by a previous landowner. On 2014 May 12, Council approved Bylaw 31D2014 which redesignated the site from an Industrial – General (I-G) District to a Multi-Residential – High Density High Rise f6.0h90 (M-H3 f6.0h90) District, with a maximum floor area ratio of 6.0 and a maximum height of 90 metres.

The M-H3 District is intended to provide for tall, high-density, high-intensity multi-residential development on strategic parcels, including landmark locations, transit and transportation corridors and nodes and employment concentrations. The M-H3 District also provides for supportive commercial uses, outdoor space for social interaction, and landscaping. The proposal is generally consistent with the intent of the M-H3 District.

Site, Building, and Landscaping Design

The proposed development is a 26-storey tower with a 14-storey podium, 320 independent living units, and 64 assisted living units. The proposed floor area ratio is 6.0 and the proposed height is 89.90 metres. The main floor of the proposed building is generally level with Horton Road SW.

The building is oriented north-south along the eastern edge of the property and the taller portion of the building is located at the northern portion of the parcel. The site layout reduces the impact of the proposed building's mass on the site to the south. The proposed building is located 17 metres from the building to the south. The proposed building is set back from Horton Road SW, with the primary entrance located approximately 28 metres from the public sidewalk along Horton Road SW.

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

Two walkways connect the primary entrance to the public sidewalk along Horton Road SW, near the landing of the staircase to the pedestrian bridge. A landscaped area that includes a seating area, surrounded by plantings is proposed near the primary entrance, to the north of the walkways to the primary entrance. Planting areas are proposed along the north, west, and south property lines. A rooftop amenity area is proposed atop the podium of the proposed building, accessed from the 15 floor.

A driveway loop with two access points to Horton Road SW is proposed south of the walkways to the primary entrance. Within the driveway loop are the underground parkade access ramp and a dog play area. Inside the parkade, 231 motor vehicle parking stalls and 80 bicycle parking stalls are proposed. A loading stall, a private shuttle parking stall, a service vehicle stall, four short-term motor vehicle parking stalls, and 32 bicycle parking stalls are proposed around the driveway loop.

As per the *City of Calgary Fire Department Access Standards*, two 6.0 metre-wide emergency access points are required for multi-residential developments with between 101 and 600 residential units. The proposed driveway loop addresses the requirements of the *City of Calgary Fire Department Access Standards*.

Administration requested a direct, comfortable, and safe pedestrian connection from the second floor of the proposed building to the pedestrian bridge and/or the landing of the pedestrian bridge. However, the applicant is unable to provide this pedestrian connection.

Administration and the applicant had several discussions with regard to creating a direct access from the pedestrian bridge to the proposed development. The applicant raised concerns that this would not be feasible for this project, indicating several reasons including security concerns and how the landowner operates its business. The safety and security of the residents are of high importance to the landowner. Administration further requested a confirmation of the feasibility of connections to the building. The applicant confirmed the feasibility of a separate connection from the second floor to the east could be accommodated if the use changes from residential to a different use.

As an alternative, the client intends to provide shuttle service for residents and the applicant is proposing entering an agreement with the owner of the property to the south ("London at Heritage Station"). This agreement will grant access for residents to the elevator in the northwest corner. Administration expressed concerns regarding the accessibility, monitoring, and maintenance of the door at the southeast corner of the proposed building, the pedestrian access to the elevator, the elevator lobbies, and the elevator itself. The pedestrian walkway to the lower elevator lobby was redesigned to be less steep, wider, and to provide more comfortable turning radii. Residents and employees also have access to the pedestrian bridge via the staircase with the landing on Horton Road SW or along the 1.2 metre-wide sidewalk on the south side of Hull Avenue SW.

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

Environmental

The site previously had battery recycling and automobile, sales, repair, and storage uses. Phase 1 and 2 Environmental Site Assessments were completed in support of the proposal and were reviewed by City of Calgary Environmental Development Review. A remedial action plan and a letter of commitment to implement the remedial action plan are required to ensure remediation in accordance with the recommendations of the reports.

Utilities and Servicing

Water, sanitary, and storm utilities are available for the development. A development site servicing plan is being completed for the development.

Transportation

The northern proposed driveway access is approximately 275 metres from Macleod Trail S via Horton Road SW and Hull Avenue SW, and approximately 350 metres from Heritage Drive SW Horton Road SW.

Pedestrian, cyclist, and vehicle access to the site are available from Horton Road SW. There is no access to Hull Avenue SW, which rises along a slope to the east. There is no lane.

The site is within 75 metres of the platform of the Calgary Transit Heritage LRT Station, which is adjacent to a bus terminal that includes a Calgary Transit MAX Teal bus rapid transit (BRT) station, and service from bus Routes 3, 20, 37, 39, 79, 80, 81, 410, and 502. Bus route 81 also has stops at the intersection of Hull Avenue SW/86 Avenue SE and Macleod Trail S, within 175 metres walking distance of the site.

A new 2.0 metre-wide sidewalk is proposed along Horton Road SW. No sidewalk exists on the west side of Horton Road SW nor along north side of Hull Avenue SW. The sidewalk along the south side of Hull Avenue SW cannot be expanded due to lane width requirements to the north and the slope retaining structure to the south.

Inside the parkade, 231 motor vehicle parking stalls and 80 bicycle parking stalls are proposed. A loading stall, a private shuttle parking stall, a service vehicle stall, four short-term motor vehicle parking stalls, and 32 bicycle parking stalls are proposed around the driveway loop.

A Transportation Impact Assessment and parking study were received in support of the application.

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

Application Review

On 2019 April 04, a detailed team review document was sent to the applicant. The detailed team review identified the following items which Administration requested the applicant address to ensure the functionality of the proposal, to integrate with the site's context, and to generally improve the proposal:

- Conforming to or providing rationale for land use bylaw discrepancies;
- Providing a direct, comfortable, and safe pedestrian connection from the second floor of the proposed building to the pedestrian bridge (to the west) and/or the landing of the pedestrian bridge (to the northeast);
- Changes to the driveway loop and surrounding area to delineate vehicle, pedestrian, and recreation areas and clarify functionality;
- Refinements to cladding on the main building and parkade entrance for less massive appearances, warmer palettes, and complementariness with nearby structures;
- Consideration of additional at-grade amenities for residents;
- Consideration of planting sustainability and tree coverage in winter;
- Conforming to City of Calgary Fire department standards;
- Conforming to City of Calgary Roads department standards, including sidewalk, curb, and gutter details for Horton Road SW;
- Replacing proposed wall mounted bicycle parking stalls for ease of use by residents and allowing for electric bicycles;
- Providing a bus stop pad; and
- Obtaining permission from the site to the south's owner to make an agreement regarding resident access through the site and to the elevator in the northwest corner of the site to the south.

On 2019 July 03, amended plans were received by Administration. The applicant addressed the items listed in the detailed team review. On 2019 August 13, a second detailed team review document was sent to the applicant, reiterating the direction to provide a direct, comfortable, and safe pedestrian connection from the second floor of the proposed building to the pedestrian bridge and/or the landing of the pedestrian bridge.

On 2019 October 23, amended plans were received by Administration.

City Wide Urban Design

The development permit application was reviewed by the City Wide Urban Design team within CPAG. The City Wide Urban Design team comments were as follows:

- The massing is good and simple, and its accents and horizontal elements reduce the vertical massing;
- The colour palette of the first through fourth floors is dark. Brighter colours would improve the residential feel of the building for residents;

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

- The driveway loop takes up a large part of the frontage area. Decreasing the driveway area could add space to the pedestrian plaza;
- The driveway and pedestrian areas should have clear demarcation to improve pedestrian safety;
- The diagonal northeast-to-southwest walkway crossing the driveway is confusing and does not connect to another walkway nor a destination at the southwest corner;
- Consider stormwater collection in site design; and
- Consider additional landscaping near the pedestrian bridge to improve the interface between the site and adjacent streets.

Urban Design Review Panel

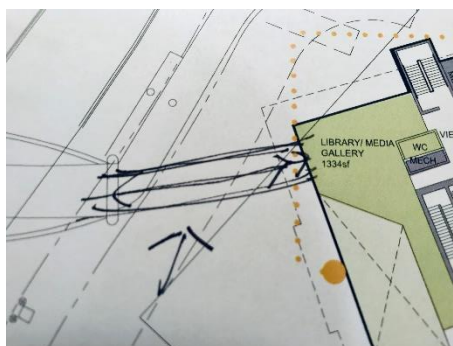
On 2018 December 12, the proposal was reviewed by the Urban Design Review Panel. The panel endorsed the proposal and recommended further review. The panel stated the following:

“The proposal to redevelop a difficult site near the Heritage LRT Station will offer an array of senior care options. Site challenges include establishing an active street-presence on a site below the significantly-sloped Hull Avenue SW and connection to the existing pedestrian bridge that crosses the busy Horton Road SW, the freight rail and the LRT lines to the Station.

The main areas of priority are outlined below and support the rated urban design elements –

- 1) Create direct connections for active seniors to streets and Station bridge that are accessible, safe, and convenient.
 - a) The discussion between Panel, Applicant and City revealed new information about a potential shared-use-public-access from Hull Ave to an elevator/stairwell in the neighbouring London Tower.
 - b) The Panel recommends functional design assessment of the bridge extending directly into what is currently shown as the Library/Media Gallery on the Level 02 Amenity Floor (see sketch notation below.)

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978



- 2) Without changing architectural massing, consider colour and explore options to break up the white masses as well as tying together the lower and upper masses (positive precedent image examples shown in the Applicant's UDRP submission).
- 3) Further design exploration should give consideration of the blank-south-facing-stepped-back-11-storey-wall facing the neighbouring London Tower, during day and dark.

The Panel supports the positive impact and demographic diversity the 383-unit density will contribute to the area."

The Urban Design Review Panel's comments and the applicant's response are contained in Attachment 3.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders, notice posted on-site, and advertised online.

The decision made by Calgary Planning Commission will be advertised in accordance with the *Municipal Government Act*. As this development permit is for a discretionary use, an appeal may be filed based on the decision on the entire permit, the decision to grant a relaxation of a land use bylaw rule, or any of the conditions of approval.

The application was circulated to the Haysboro, Kingsland, Fairview, and Acadia community associations. No response was received to the noted circulations.

Administration received letters of opposition from 10 households by the report submission date. Reasons for opposition included:

- Traffic and congestion;

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

- Noise;
- Blocked views;
- Lack of market demand for new housing;
- Aesthetics;
- Shadowing and blocking light;
- Impacts of construction;
- Property values;
- Air pollution;
- Fire safety; and
- Character of the area.

Engagement

On 2019 March 12, the applicant and landowner met with members of the Haysboro Community Association to discuss details of the proposal. Refer to the Engagement Summary in Attachment 4 for more information.

In 2019 April, the applicant and landowner held a meeting with members of the condominium board and property manager of the property to the south to discuss details of the proposal. The condominium board and property manager considered the proposed development as a good fit for the community of Haysboro as it is an aging community and the proposed development would promote long term residency. Refer to the Engagement Summary in Attachment 4 for more information.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The site is located within the City, Town area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan* (SSRP). While the SSRP makes no specific reference to this site, the proposal is consistent with policies on land use patterns.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed development builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The site is located within a Main Street - Urban Main Street area, as identified on Map 1: Urban Structure of the *Municipal Development Plan* (MDP). The MDP makes no specific reference to this site. The proposal is generally consistent with the city-wide policies regarding concentration of development, optimization of use of existing infrastructure, higher population densities in Main Streets and near higher-frequency and higher-capacity transit. The proposal is also

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

consistent with policies regarding housing choice, particularly with regard to special care facilities and aging in place.

Local Area Plan

There is no local area plan.

The Heritage Communities Local Growth Planning Project is currently under review by Administration as part of the Heritage Communities Local Growth Planning Project. The multi-community planning process does not prohibit applications from being submitted. The local area plan is anticipated to be ready in Q3 2020.

Transit Oriented Development Policy Guidelines (Statutory – 2004)

The *Transit Oriented Development Policy Guidelines* provide direction for development proposed within 600 metres of a transit station. Objectives for transit-oriented development areas include provision of transit supportive uses, high density development, and pedestrian-oriented design and planning near transit stations. The proposal is consistent with the guidelines regarding concentration of development near transit stations and underground parking.

Planning Principles for the Location of Care Facilities and Shelters (Non-Statutory – 2011)

The *Planning Principles for the Location of Care Facilities and Shelters* provides guidance for locating and developing care facilities, including seniors housing and assisted living developments. The proposal is generally consistent with the general principles and the principles for residential care and assisted living developments.

The Development Next to Freight Rail Corridor Policy (Non-Statutory – 2018)

The proposed development is outside both the rail proximity envelope for safety and the rail proximity envelope for noise, which are both measured 30.0 metres horizontally from the freight rail corridor, as per the *Development Next to Freight Rail Policy*. No further analysis is required.

Land Use Bylaw 1P2007

Administration identified the following proposed bylaw relaxations. After review, Administration is supportive of relaxations to each of the listed rules, and is satisfied that rationale provided by the applicant passes the tests of section 36 of *Land Use Bylaw 1P2007*.

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

Table 1: Bylaw Relaxations

Regulation	Standard	Provided
549 Projections Into Setback Areas	(1) Unless otherwise referenced in subsections (2), (3), (4), (5), (6), and (7), a building or air conditioning units must not be located in any setback area.	Plans indicate the north balconies as being setback 2.03 metres (-3.97 metres) from the north property line. Plans indicate the generator enclosure/mechanical intake as being setback 1.25 metres (-4.75 metres) from the west property line. Plans indicate the west balconies as being setback 5.05 metres (-0.95 metres) from the west property line. Plans indicate the east balconies as being setback 1.40 metres (-4.60 metres) when measured to the east property line. <i>Administration is supportive of the requested relaxation: It does not materially impact the interface with Horton Road SW nor Hull Avenue SW.</i>
	(5) Eaves may project a maximum of 0.6 metres, and window wells may project a maximum of 0.8 metres, into any setback area.	Plans indicate the north eaves as projecting 5.49 metres (+4.89 metres) for the 2 nd and 5 th floor eave, 5.17 metres (+4.57 metres) for the 15 th and 16 th floor eaves, and 5.68 metres (+5.08 metres) for the roof eave. Plans indicate the west eaves as projecting 2.54 metres (+1.94 metres) for the 2 nd , 5 th , and 15 th /16 th floor eave, and 2.40 metres (+1.80 metres) for the roof eave. Plans indicate the east eaves as projecting 4.69 metres (+4.09 metres) for the 2 nd and 5 th floor eaves, 2.38 metres (+1.78 metres) for the 15 th and 16 th floor eaves, and 2.98 metres (+2.38 metres) for the roof eave. <i>Administration is supportive of the requested relaxation: It does not materially impact the interface with Horton Road SW nor Hull Avenue SW.</i>
551 Specific Rules for Landscaped Areas	(3)(b) The maximum hard surfaced landscaped area is 40.0 per cent of the required landscaped area, in all other cases.	Plans indicate 1057.15m ² (+159.38m ²) or 47.10% (+7.10%) of hard landscaped area. <i>Administration is supportive of the requested relaxation: Sufficient soft surfaced landscaped area is provided.</i>
557 Amenity Space	(8)(b) Private amenity space must have no minimum dimensions of less than 2.0 metres.	Plans indicate amenity spaces as having dimensions of 1.95 metres (-0.05 metres).

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1413

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

		<i>Administration is supportive of the requested relaxation: The discrepancy is materially minimal.</i>
664 Building Setbacks	(1) Unless otherwise referenced in subsection (2), the minimum building setback from a property line shared with a street is 6.0 metres.	<p>Plans indicate the north balconies as being setback 2.03 metres (-3.97 metres) from the north property line.</p> <p>Plans indicate the generator enclosure/mechanical intake as being setback 1.25 metres (-4.75 metres) from the west property line.</p> <p>Plans indicate the west balconies as being setback 5.05 metres (-0.95 metres) from the west property line.</p> <p>Plans indicate the east balconies as being setback 1.40 metres (-4.60 metres) when measured to the east property line.</p> <p><i>Administration is supportive of the requested relaxation: It does not materially impact the interface with Horton Road SW nor Hull Avenue SW.</i></p>
666 Building Height	(2)(a) The maximum building height is 10.0 metres from grade within 4.0 metres of a property line shared with a street or with a parcel designated as a M-C2, M-2, M-H1, M-H2, M-H3, or M-X2 District.	<p>Plans indicate portions of the building as being located within 10.0 metres of grade within 4.0 metres of Hull Avenue SW.</p> <p><i>Administration is supportive of the requested relaxation: It does not materially impact the interface with Hull Avenue SW.</i></p>
Motor Vehicle Parking Stalls	385 motor vehicle parking stalls are required.	<p>Plans indicate 228 (-157) motor vehicle parking stalls being provided.</p> <p>Note: Three stalls were not counted because they do not meet the minimum motor vehicle parking stall dimensions.</p> <p><i>Administration is supportive of the requested relaxation: Transportation is satisfied with the proposed number of motor vehicle parking stalls.</i></p>
Bicycle Parking Stalls	160 bicycle parking stalls – class 1 are required.	<p>Plans indicate 81 (-79) bicycle parking stalls – class 1 being provided.</p> <p><i>Administration is supportive of the requested relaxation: Transportation is satisfied with the proposed number of bicycle parking stalls - class 1.</i></p>

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1413

Development Permit in Haysboro (Ward 11) at 8610 Horton Road SW, DP2019-0978

Social, Environmental, Economic (External)

The proposed development would provide additional housing for the growing demographic segments that want and need older-adult-oriented housing and ancillary services. The proposed development makes effective use of existing infrastructure.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to current or future operating budgets at this time.

Current and Future Capital Budget

The proposed development does not trigger capital infrastructure investment, and therefore there are no growth management concerns at this time.

Risk Assessment

There are no proposal-specific risks associated with this application.

REASON(S) FOR RECOMMENDATION(S):

The proposal provides high density residential development for a segment of the population that has specific housing needs, in a location that makes effective use of existing infrastructure.

ATTACHMENT(S)

1. Conditions of Approval
2. Applicant's Submission, Development Permit Renderings and Development Permit Plans
3. UDRP Comments and Applicant Response
4. Engagement Summary

Conditions of Approval

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a total of five (5) complete sets of Amended Plans (file folded and collated) to the Planning Generalist that comprehensively address the Prior To Release conditions of all Departments as specified below.

In order to expedite the review of the Amended Plans, please include the following in your submission:

- a. Three (3) of the plan sets shall highlight all of the amendments.
- b. Three (3) detailed written responses to the Conditions of Approval document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.

Please ensure that all plans affected by the revisions are amended accordingly.

2. Amend the plans by providing a detail of shrub plantings above parkade roof slab.
3. Amend the drawing titles of the floors to identify which are the assisted living floors and which are the independent living floors.
4. Amend the plans so that the pedestrian sidewalk that leads from Horton Road SW to the primary entrance has a legible, direct path from the landing of the staircase. The change was made on the landscape plan, but not on the site plan. Ensure that all plans match.
5. Amend the plans by correcting the note misprint on the landscape plan, wherein some notes were covered by a layer. It is noted that the misprint is not depicted on the plans submitted to the Calgary Planning Commission for review.

Development Engineering:

6. Amend the plans by:

Fire - Lockbox Location

- a. Indicate (note) a "Calgary Fire Department approved lockbox" at / near the buildings "principle entrance".

Fire – Fire Department Connection (Siamese) Location

Revise the location of the Siamese location such that it:

- a. Provides 2m operational clearance left and right of each port.

Note:

It appears that the fire department connection location shown does not provide a minimum 2m clearance on each side (which may be challenging for fire crews to connect to the ports). Revise as to show / dimension a 2m minimum clearance on each side.

Conditions of Approval

7. Provide a Remedial Action Plan that fully addresses the previously identified environmental concerns on site.

All reports are to be prepared by a qualified professional and will be reviewed to the satisfaction of City of Calgary Environmental & Safety Management.

Note:

For further details, contact the Environmental Specialist at 403-268-1933

8. Provide a signed letter of commitment confirming that the recommendations outlined in the Remedial Action Plan will be completed.

All information submitted will be reviewed to the satisfaction of City of Calgary Environmental & Safety Management.

Note:

For further details, contact the Environmental Specialist at 403-268-1933

9. Submit three (3) sets of the development site servicing plan details to Development Servicing, Inspections and Permits, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact developmentservicing2@calgary.ca for additional details.

For further information, refer to the following:

Design Guidelines for Development Site Servicing Plans

http://www.calgary.ca/PDA/pd/Documents/urban_development/publications/DSSP2015.pdf

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/pd/Documents/development/development-site-servicing-plan.pdf>

Note:

Records suggest that DSSP2019-0138 is “pending decision” as of August 30, 2019. For further details, contact the Engineering Consultant (directly) OR the Water Resources –Development Approvals Technologist at 403-268-5688.

10. Follow the submission requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control (Erosion and Sediment Control Reports and Drawings: Technical Requirements) and either submit the required (2) copies of an Erosion and Sediment Control (ESC) Report and Drawings or the required (2) copies of a Written Notice to Development Engineering, for review and acceptance by Water Resources. The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Guidelines for Erosion and Sediment Control* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Professional Engineer (P. Eng.), Professional Licensee (P.L. Eng), Professional Agrologist (P. Ag.) or Certified Professional in Erosion and Sediment Control (CPESC). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

Conditions of Approval

If you have any questions, contact 3-1-1. A Service Request (SR) will be created for the Erosion Control Team in Water Resources.

Note:

Records suggest that an ESC submission had been submitted to the City and review in late October, 2019. Further information (resubmission) was deemed required (as of October 23, 2019).

For further details, contact the Engineering Consultant (directly).

Transportation:

11. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Construction of two new driveway crossings on Horton Road SW.
 - b. Construction of new sidewalks adjacent to Horton Road SW.
 - c. Construction of new curb and gutter adjacent to Horton Road SW.
 - d. Construction of a Type C bus pad on Horton Road SW.
 - e. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel,
 - f. Rehabilitation of existing City Boulevard to be sodded.
12. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Roads

- a. Street lighting upgrading adjacent to Horton Road SW.
13. The site is within the catchment area for financial contributions for the pedestrian bridge over Horton Road SW to Calgary Transit Heritage Light Rail Transit Station. The applicant shall contact Heritage Station Inc. Westcorp Properties Inc. to enter into a cost sharing arrangement (\$107,500, 4.3% of 2,500,000) for the developer-funded transit-oriented infrastructure for the pedestrian bridge over Horton Road SW. Provide a letter from Heritage LRT Station Inc. stating that the necessary cost sharing arrangement has been entered into.
 14. Execute and register on title a mutual access easement agreement between 8610 Horton Road SW 0213832 and 8710 Horton Road SW for the purpose of pedestrian access. The agreement and registerable access right of way plan shall be to the satisfaction of the Director, Transportation Planning. A standard template for the agreement and an Instruction Document will be provided by the Transportation Corporate Planning Application Group

Conditions of Approval

Generalist. Submit an original copy of the executed agreement and the certificate of title(s), indicating the agreement is registered on title, for all affected parcels.

Parks:

No comments.

Permanent Conditions

The following permanent conditions shall apply:

Planning:

15. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
16. No changes to the approved plans shall take place unless authorized by the Development Authority.
17. A Development Completion Permit shall be issued for the development **before the use is commenced or the development occupied**. A Development Completion Permit is independent from the requirements of Building Permit occupancy. Call Development Inspection Services at 403-268-5311 to request a site inspection for the Development Completion Permit.
18. Upon completion of the main floor geodetic elevation of the building, the constructed main floor geodetic elevation must be submitted to and approved by the Development Authority prior to any further construction proceeding. Email confirmation to geodetic.review@calgary.ca.
19. Vehicular and soft landscaping areas shall be separated by a 150mm (6 inch) continuous, poured in place, concrete curb or equivalent material to the satisfaction of the Development Authority, where the height of the curb is measured from the finished hard surface.
20. The walls, pillars and ceiling of the underground parkade shall be painted white or a comparable light colour.
21. The light fixtures in the parkade shall be positioned over the parking stalls (not the drive aisles).
22. All stairwell doors and elevator access areas shall be installed with a transparent panel for visibility.
23. All electrical servicing for freestanding light standards shall be provided from underground.
24. A lighting system to meet a minimum of 10 LUX with a uniformity ratio of 4:1 on pavement shall be provided for any at-grade parking and passenger pick-up/drop-off areas.
25. Fascia signage shall be placed only in the designated sign area as indicated on the approved plans. Any damage to the building face, as a result of the sign installation or removal, shall be repaired to the satisfaction of the Development Authority.

Conditions of Approval

26. Loading and delivery shall take place in the designated loading stall as shown on the approved plans and shall, at no time, impede the safety of pedestrian movements and use of the parking lot.

Development Engineering:

27. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
- the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
28. The developer/project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

29. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.
30. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
31. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in

Conditions of Approval

accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.

32. No trees, shrubs, buildings, permanent structures or unauthorized grade changes are permitted within the utility rights-of-way.

Transportation:

33. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the applicant's expense, upon completion of the foundation. Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.
34. The approved driveway(s) required for this development must be constructed to the ramp grades as shown on the approved Development Permit plans. Negative sloping of the driveway within the City boulevard is not acceptable. If actual grades do not match the approved grades, the developer/owner shall be responsible for all costs to remove and reconstruct the entire driveway ramp in accordance with approved grades.
35. The approved driveway(s) required for this development must be constructed to the ramp grades as shown on the approved Development Permit plans. Negative sloping of the driveway within the City boulevard is not acceptable. If actual grades do not match the approved grades, the developer/owner shall be responsible for all costs to remove and reconstruct the entire driveway ramp in accordance with approved grades.

Parks:

No comments.

Applicant's Submission, Renderings, and Development Permit Plans



CALGARY PLANNING
COMMISSION
APPLICANT PACKAGE
DP2019-0978

CHARTWELL HAYSBORO
8610 HORTON RD. SW

2019-11-12

Applicant’s Submission, Renderings, and Development Permit Plans



The Owners of the site, Chartwell Retirement Residences, focus on developing and operating Senior’s retirement residences. They are a reputable service provider and have nominated this site for their first new-build development in Calgary. This landmark development has encompassed pride in place, and will seek to be a solid part of the local community. Ward 11 has the highest pre-retirement demographic in Calgary, providing existing Calgarians an opportunity for ageing in place in a unique and diverse environment.

Chartwell is a long term owner operator, with the intention of having a minimum 40 year projected ownership. This is reflected in the design methodology through high quality specifications, materials choices, site design and community interface in this proposal.

Chartwell Haysboro is situated notably alongside the Heritage commercial development, offering a continuum of care for retirement age residents and seniors within a contemporary, urban environment. Future residents with varying levels of mobility and care will be provided the opportunity and independence to interact within the building and greater neighborhood. Direct access is offered to nearby amenities near Macleod Trail, to mass transit options and adjacent residential neighbourhood green spaces.

Conceptualized as a vertical urban neighborhood, the building programming and site response based on function and scale in section, providing its most active amenity uses at its base, interfacing with the public realm. The abundance of these active interior amenities further encourages engagement of its populace with the surrounding community while passively encouraging security for the surrounding site and its neighbours.

The building has been sited to align with its eastern boundary, pulling the main and second floor podium in to alleviate massing at grade, while encouraging transparency and views for the residential neighbours bordering to the south. The higher tower is stacked to the north away from its southern neighbours, skewed in response to the adjacent pedestrian bridge. This siting acts to alleviate impact to neighbours while remaining within the site’s zoning height restrictions. It also enables the development of a forecourt facing Horton Rd (outlined further below in item 2)

Chartwell strives for safe, accessible movement of residents, visitors and staff by providing barrier-free accessibility to all interior common spaces, building edges, and exterior amenities on site. The public plaza or forecourt was designed with minimal elevation changes, lending itself to flexible uses such as emergency access, truck loading, vehicle drop-off, and resident amenities.

Safe navigation for pedestrians is prioritized with safety bollards and lighting that aid in vehicle wayfinding. Long term parking is primarily underground, further prioritizing and benefiting the walkability and urban appeal of Horton Road as a Main Street for existing and future residents. The site itself posed some significant challenges to navigate. Below, we outline some of the key challenges and our design approach to resolve these issues.

1. **Scale and proximity, adjacency to the existing London at Heritage tower development:** After conducting an initial volume study, the team redesigned building massing to alleviate impact of the building volume for existing neighbouring residents. This encouraged more daylight penetration to site on the eastern and western aspects. The offset tower building volume also lends to architectural visual interest that can be seen from Macleod Trail. Comfort of human scale is addressed by providing roof canopies and overhangs where the podium levels have been scaled back. These roof features wrap around the active edges of the building, responding to the pedestrian bridge and path up Hull Avenue.
2. **Site Access & reduced site frontage:** By pushing the building volume east, the created forecourt / plaza environment previously mentioned acts as a multi-functional space, maintaining the functionality of driveway site access while simultaneously creating a beautiful open area which all passers-by, residents, and neighbours can visually enjoy from various viewpoints – from the tower adjacent, on the pedestrian bridge, and along Horton Road. Instead of turning its back on Macleod Trail, the building articulates podium detailing along active street edges from east to west as pedestrians cross Horton Road, up Hull Avenue to Macleod Trail.
3. **Access to open spaces and daylighting, creating opportunities for residents to access outdoor space:** As part of the siting response previously discussed by situating a taller tower on the north side of the site provided an opportunity for an elevated outdoor roof amenity for residents. This provides for beautification of rooftop spaces for neighbouring residents to visually participate. Providing access to spectacular views and access to daylight, this is an investment important to the Owners they have maintained throughout the design process.
4. **Security of seniors as a sensitive population; creating a better public environment and interface on a prominent and diverse site:** In our discussion with the community association and neighbouring resident group, they advised of some general concerns by existing residents pertaining to the bottle depot and train tracks attracting transient visitors. Eyes on the street provided by the senior demographic that is largely on site through the day. The proposed forecourt plaza increases supervision via the operator’s reception direct visuals to the vehicle drop off and amenity area, aided by the use of the operator’s cctv monitoring to the current laneway north of the neighbouring heritage tower. Provision of night lighting with CPTED principles for safe connectivity to the surrounding public realm further contributes to the safety of the adjacent public realm.

SUMMARY

Applicant's Submission, Renderings, and Development Permit Plans



Applicant's Submission, Renderings, and Development Permit Plans



TRANSPORTATION

Applicant’s Submission, Renderings, and Development Permit Plans

NORR



NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12

- LEGEND
- COMMERCIAL HIGH RISE

COMMERCIAL MEDIUM RISE 80M

COMMERCIAL LOW RISE 12M

MULTI RESIDENTIAL MEDIUM RISE

MULTI RESIDENTIAL LOW RISE

R-C1 - RESIDENTIAL

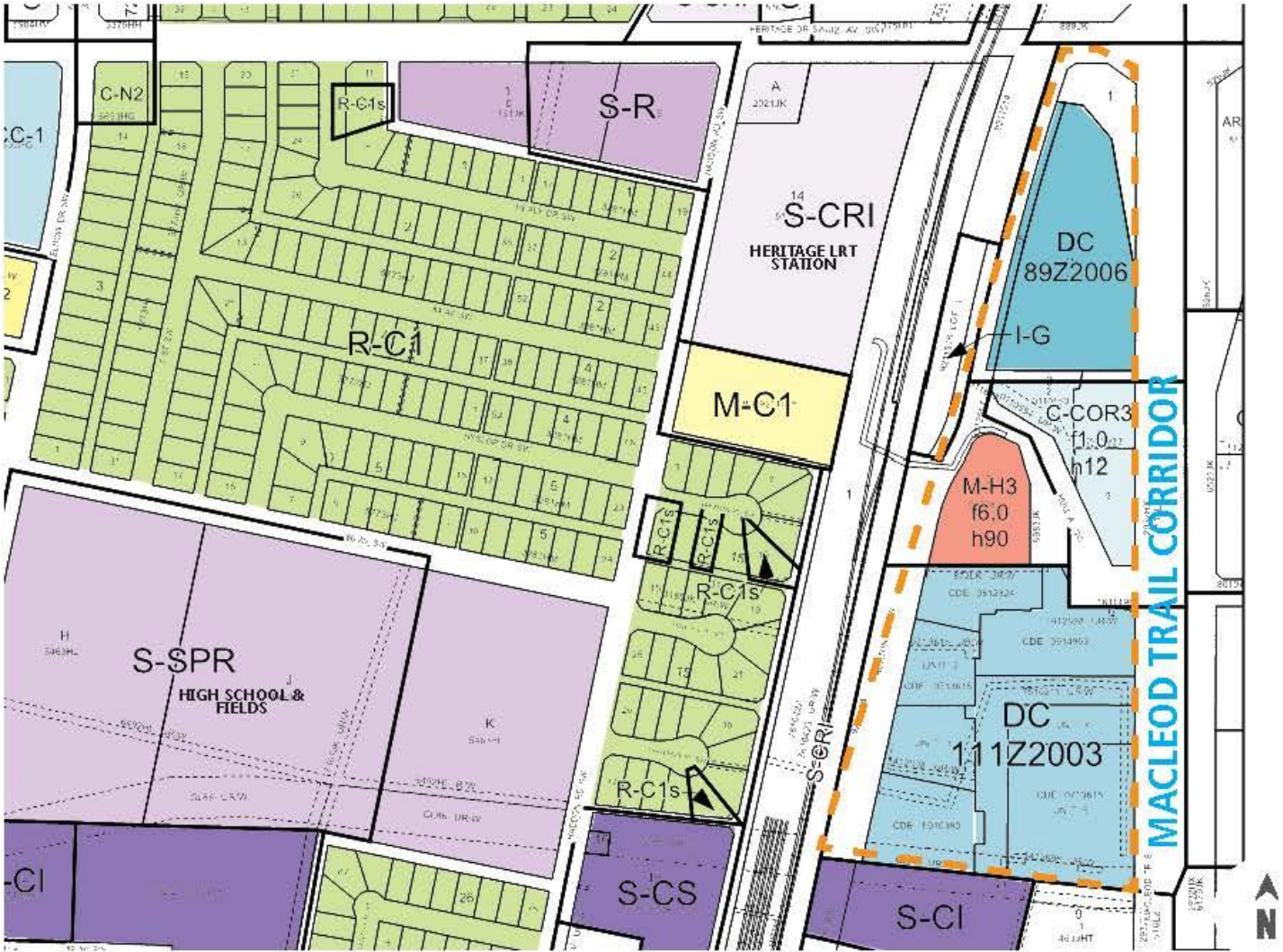
SPECIAL PURPOSE COMMUNITY

SPECIAL PURPOSE RECREATION

SPECIAL PURPOSE SCHOOL/PARK

SPECIAL PURPOSE CITY/REGIONAL

PROPOSAL SITE MH3 - MULTI-RESIDENTIAL



SITE ZONING

Applicant's Submission, Renderings, and Development Permit Plans

NORR



CHARTwell

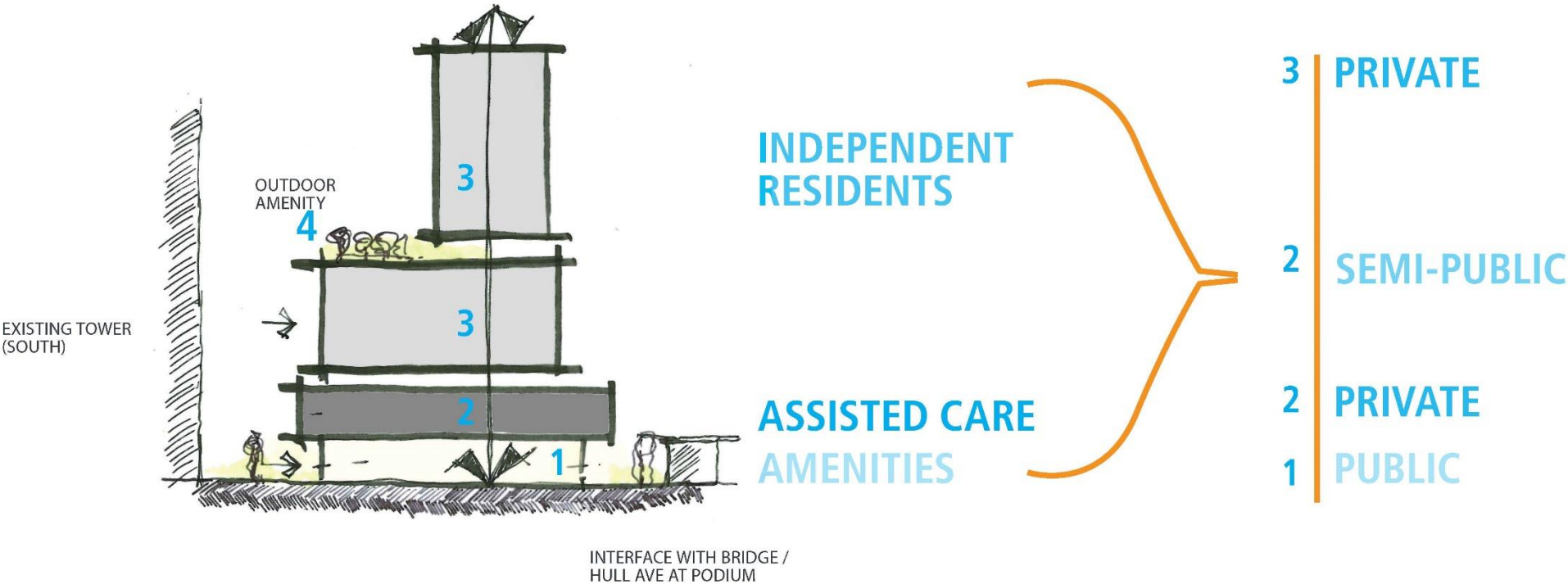
retirement residences

NCCA18-0065

CHARTWELL HAYSBORO | DP2019-0978

CALGARY PLANNING COMMISSION APPLICANT PACKAGE

2019-11-12



BUILDING PROGRAMMING

Applicant's Submission, Renderings, and Development Permit Plans

NORR



CHARTwell

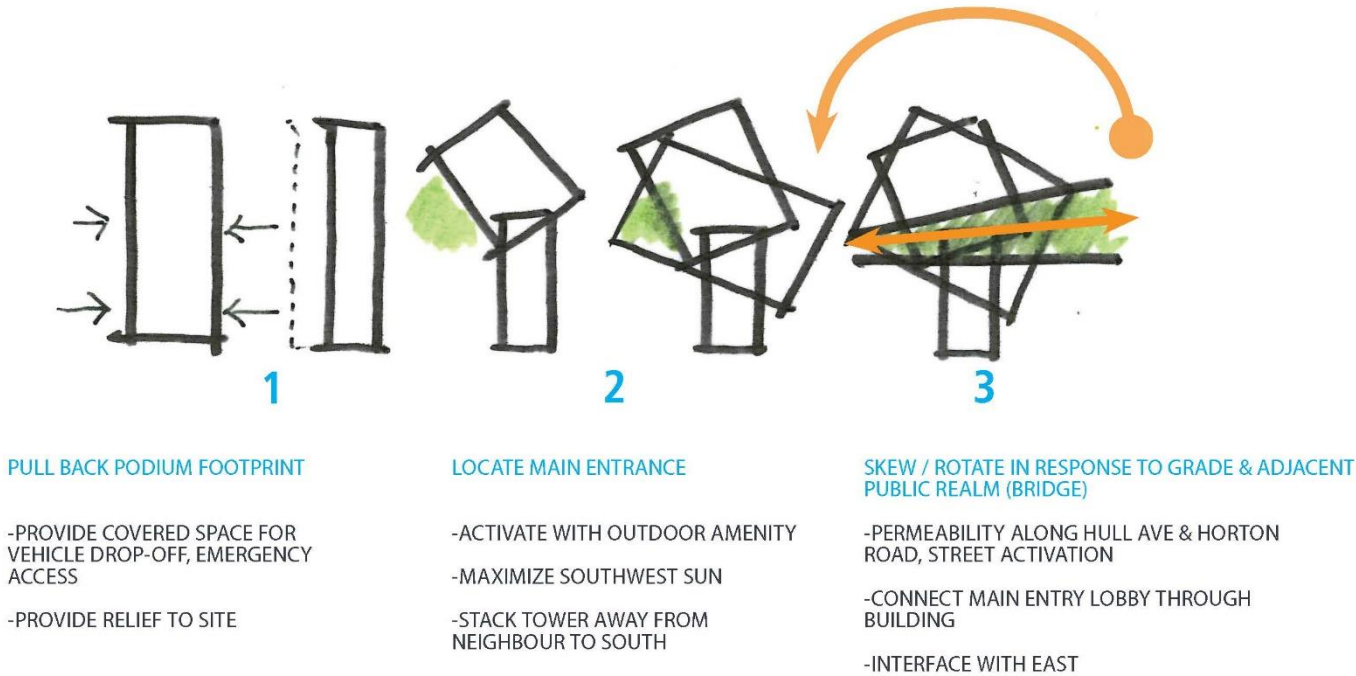
retirement residences

NCCA18-0065

CHARTWELL HAYSBORO | DP2019-0978

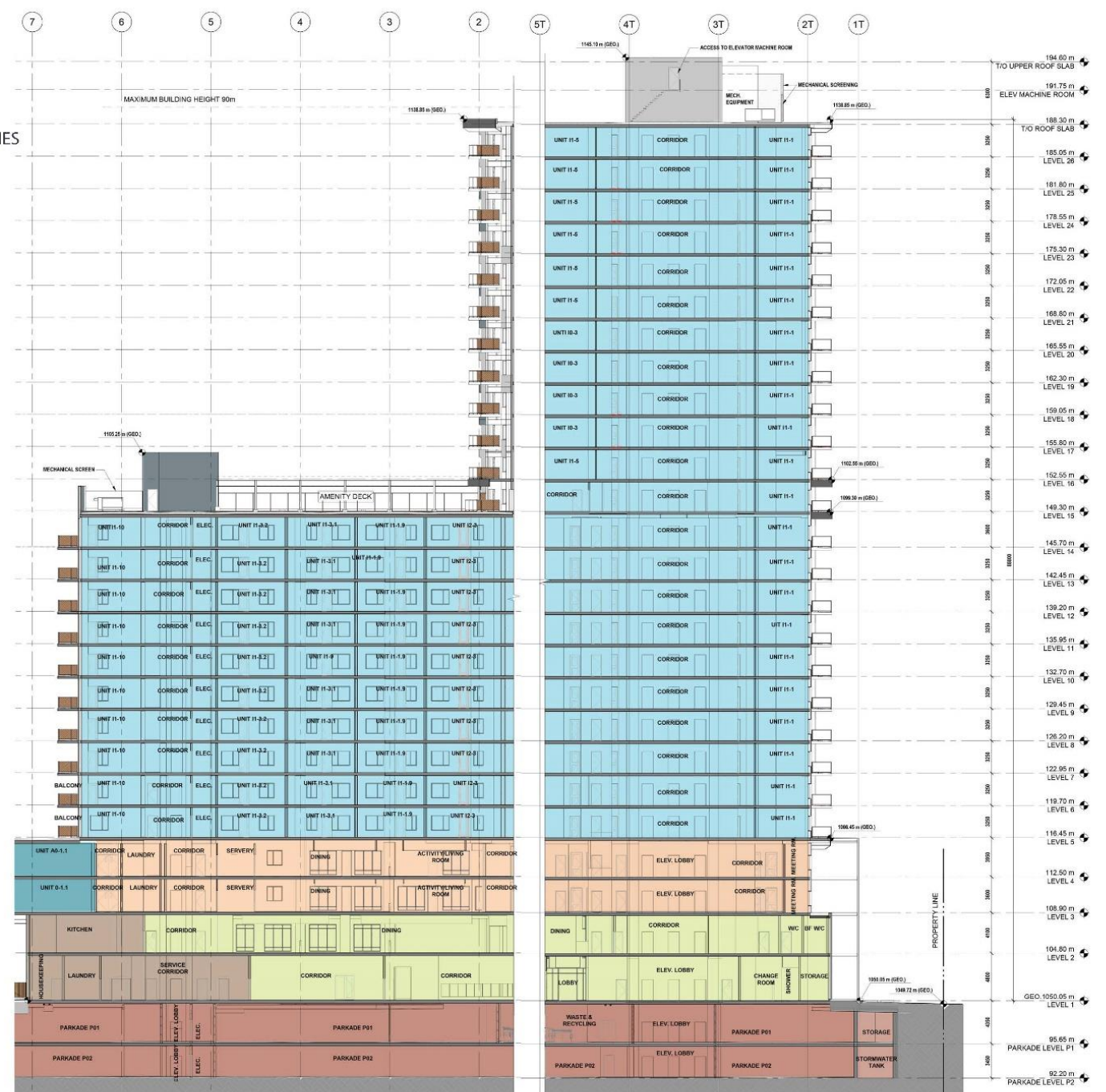
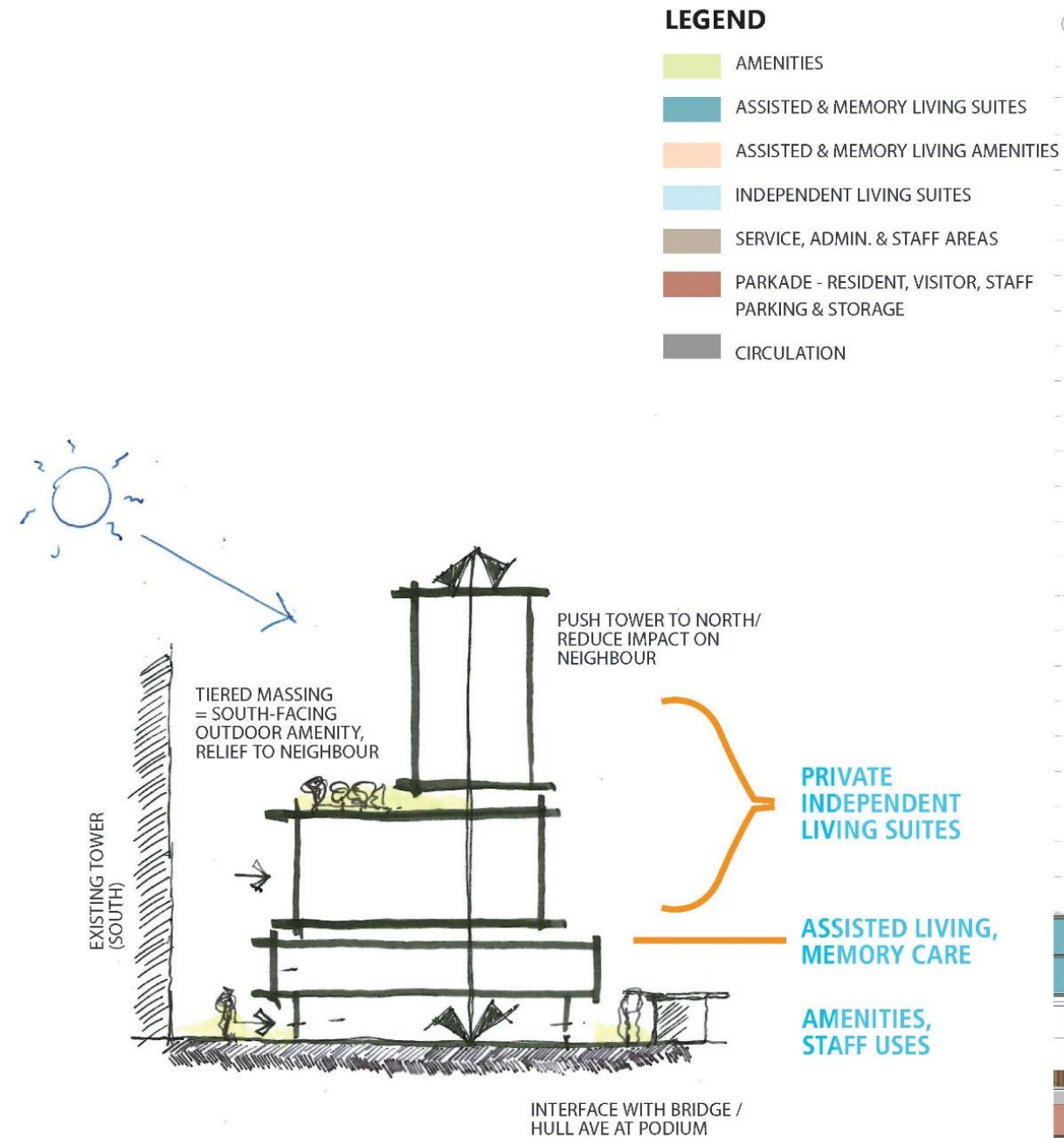
CALGARY PLANNING COMMISSION APPLICANT PACKAGE

2019-11-12



SITE RESPONSE & BUILDING VOLUME DIAGRAM

Applicant's Submission, Renderings, and Development Permit Plans



LONGITUDINAL SECTION - SCALED FROM ORIGINAL DP DRAWINGS - NTS

SITE MASSING & ADJACENCIES

Applicant's Submission, Renderings, and Development Permit Plans

NORR

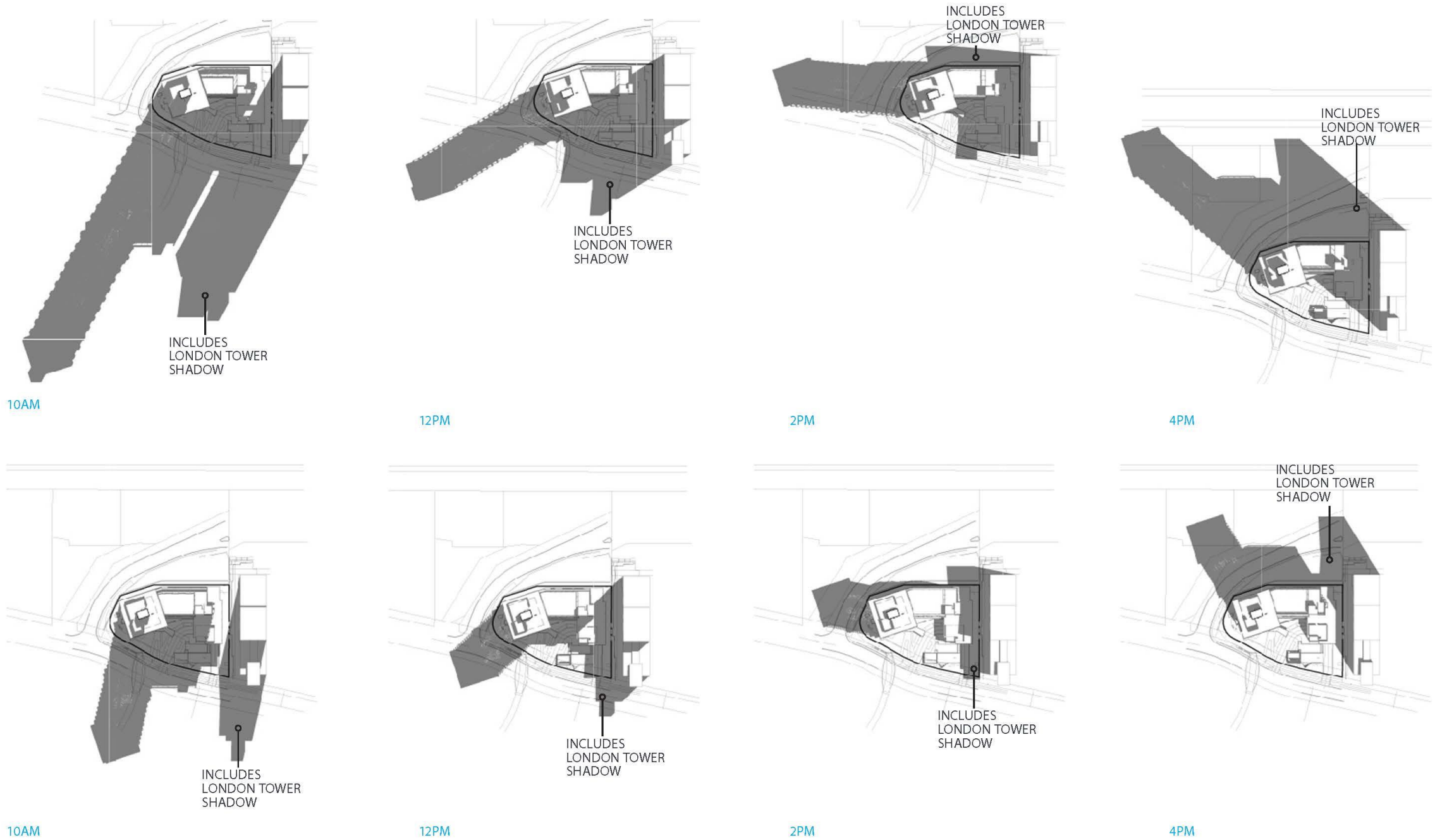


CHARTWELL
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12

SEPTEMBER / MARCH 21

JUNE 21



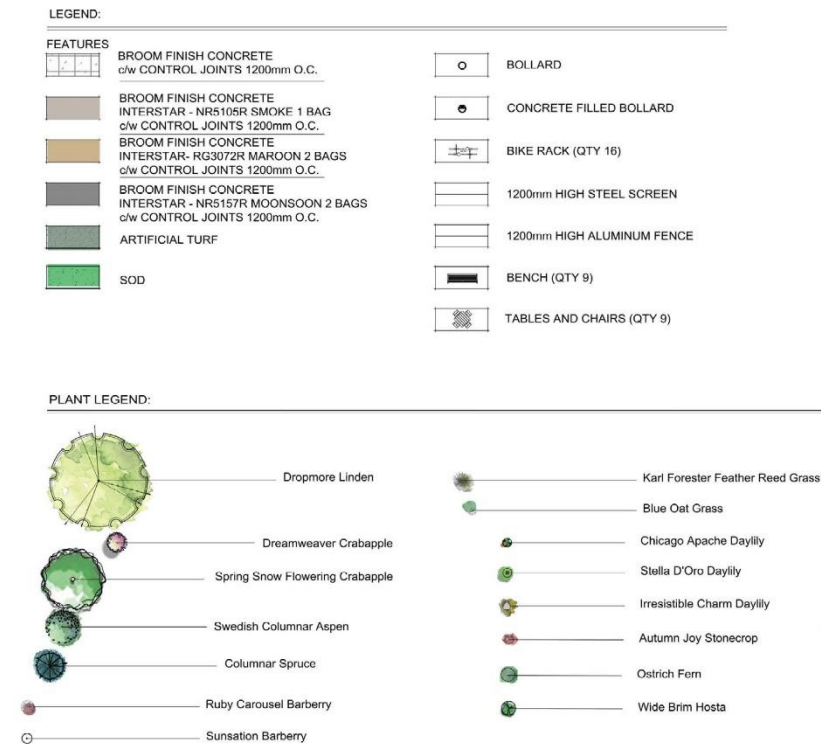
SUN STUDIES

Applicant's Submission, Renderings, and Development Permit Plans



CHARTwell
 retirement residences

NCCA18-0065
 CHARTWELL HAYSBORO | DP2019-0978
 CALGARY PLANNING COMMISSION APPLICANT PACKAGE
 2019-11-12



SCALED FROM ORIGINAL DP DRAWINGS - NTS

COLOURED LANDSCAPE PLAN

Applicant's Submission, Renderings, and Development Permit Plans

NORR

CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



Applicant’s Submission, Renderings, and Development Permit Plans

NORR

CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



SCALED FROM ORIGINAL DP DRAWINGS - NTS

NORTH ELEVATION

Applicant's Submission, Renderings, and Development Permit Plans

NORR

CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



SCALED FROM ORIGINAL DP DRAWINGS - NTS

EAST ELEVATION

Applicant's Submission, Renderings, and Development Permit Plans

NORR

CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



Applicant's Submission, Renderings, and Development Permit Plans

NORR

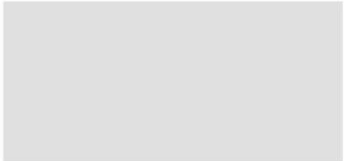
CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12




SCALED FROM ORIGINAL DP DRAWINGS - NTS * REFER TO ELEVATION MATERIAL LEGEND IN DRAWINGS

60A|64a




Aluminum Doors; Aluminum punched windows - Clear Glass w/ White Anodized Frame

63|65



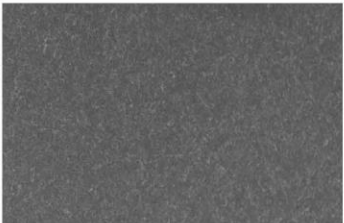
Clear Glazing in Aluminum Frames; back-painted infill panels (spandrel) white

60B|64B



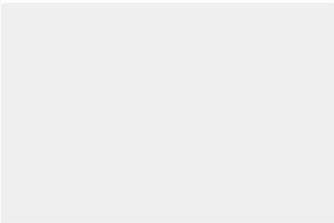
Aluminum Doors; Aluminum punched windows - Clear Glass w/ Charcoal anodized Frame

23



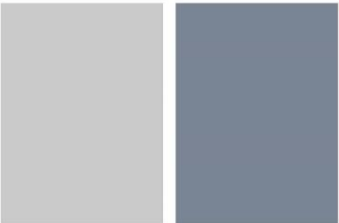
Fibre Cement Panel - Charcoal

22




Composite Metal Panel - White

21A/21B




Composite Metal Panel - Accent Light Grey / Blue Grey

20|40




Composite Metal Panel & Fascia- Charcoal Grey; Prefinished Metal Powder coated

31



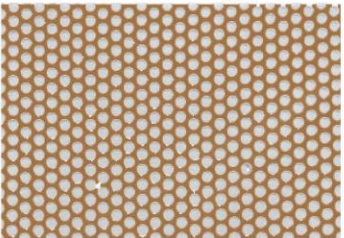
Metal Soffit Siding- Wood grain finish

24



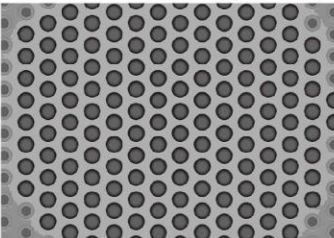
Composite Metal Panel Fascia- Medium Brown

51




Pre-finished Perforated metal screen- balcony insert

52




Pre-finished Perforated metal screen - Grey Mechanical

54



Privacy Screen - Balcony Light Grey Frame w/ Frosted Glass

53



Extruded Aluminum Beams - Dark Cherry Wood Tone

MATERIAL PALETTE

Applicant's Submission, Renderings, and Development Permit Plans

NORR



CHARTWELL

retirement residences

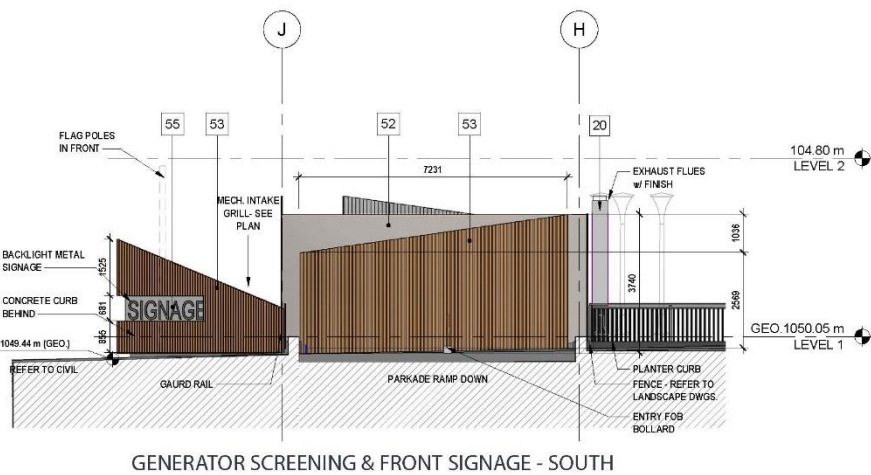
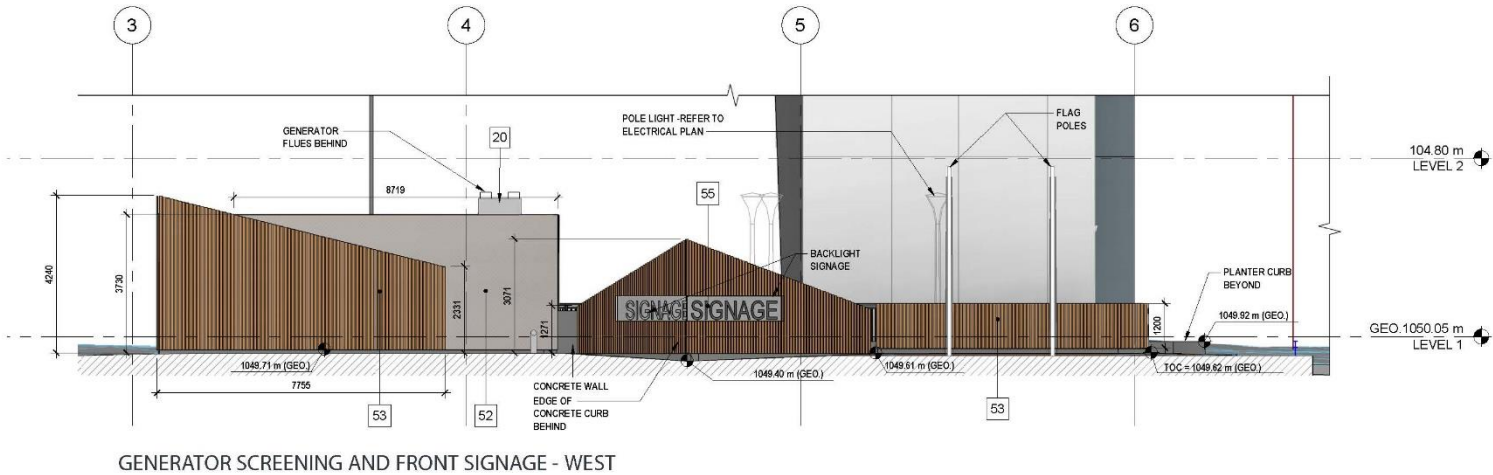
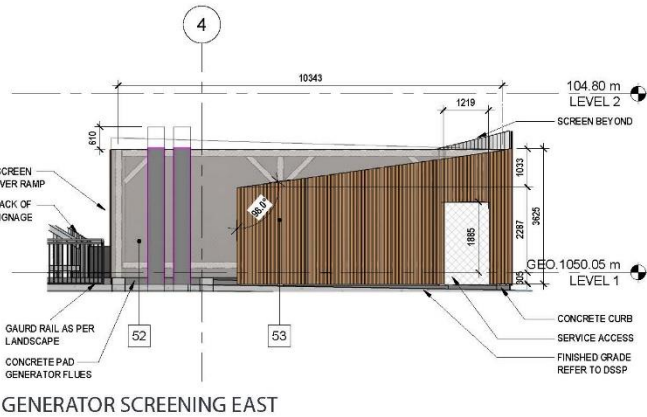
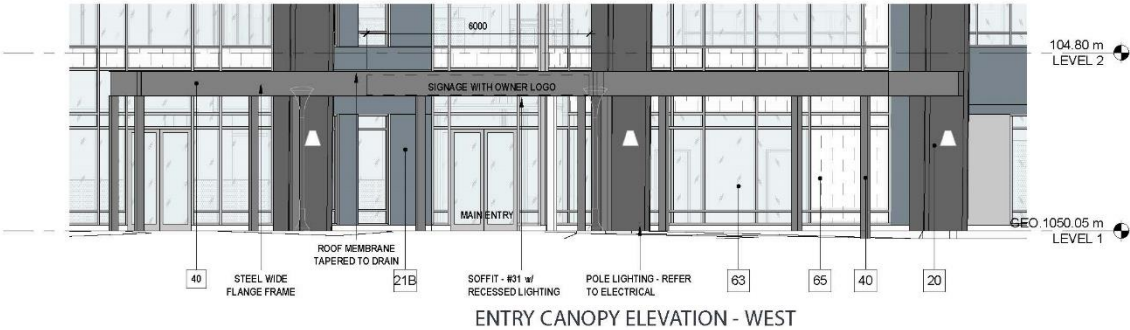
NCCA18-0065

CHARTWELL HAYSBORO | DP2019-0978

CALGARY PLANNING COMMISSION APPLICANT PACKAGE

2019-11-12

ELEVATION MATERIAL LEGEND		
TAG	CATEGORY	COLOUR
10	PAVING	LIGHT GREY
20	COMPOSITE METAL PANEL & FASCIA	CHARCOAL GREY SMOOTH MATTE
21A	COMPOSITE METAL PANEL	ACCENT 1 - MEDIUM GREY SMOOTH MATTE
21B	COMPOSITE METAL PANEL	ACCENT 2 - BLUE GREY SMOOTH MATTE
22	COMPOSITE METAL PANEL	WHITE SMOOTH MATTE
23	FIBRE CEMENT WALL BOARD - SMOOTH w/ MATCHING FASTENERS	MEDIUM GREY
24	COMPOSITE METAL PANEL FASCIA	MEDIUM BROWN TO MATCH #51
31	WOOD-LOOK METAL SOFFIT SIDING	DARK CHERRY WOOD TONE
40	PREFINISHED METAL POWDER COATED	CHARCOAL GREY
41	PREFINISHED METAL WALL LOUVRES	CHARCOAL GREY
50	CLEAR GLASS BALCONIES w/ ALUMINUM FRAME	CLEAR GLASS w/ LIGHT GREY FRAME
51	PREFINISHED PERFORATED METAL BALCONY INSERT	COPPER / CORTEN
52	PERFORATED METAL SCREEN	LIGHT GREY
53	1 1/2" BEAMS EXTRUDED ALUMINUM SIDING	DARK CHERRY WOOD TONE
54	PRIVACY SCREEN - BALCONY	LIGHT GREY FRAME w/ FROSTED GLASS
55	EMBOSSED METAL SIGNAGE	GREY
60A	ALUMINUM PATIO DOORS	WHITE ANODIZED FRAME w/ CLEAR GLASS INSERT
60B	ALUMINUM PATIO DOORS	CHARCOAL ANODIZED FRAME w/ CLEAR GLASS INSERT
63	GLAZING IN ALUMINUM FRAMES	CLEAR GLAZING w/ CLEAR ANODIZED ALUMINUM FRAMES
64A	ALUMINUM PUNCHED WINDOWS	WHITE ANODIZED FRAMES w/ CLEAR GLAZING
64B	ALUMINUM PUNCHED WINDOWS	CHARCOAL ANODIZED FRAMES w/ CLEAR GLAZING
65	BACK-PAINTED GLASS INFILL PANELS w/ BACKPAN INSULATED ASSEMBLY	WHITE
66	PREFINISHED METAL EQUIPMENT & SCREENING STRUCTURES	TO MATCH #20



SCALED FROM ORIGINAL DP DRAWINGS - NTS

SCREENING ELEVATIONS & SIGNAGE

Applicant's Submission, Renderings, and Development Permit Plans

NORR

CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



SOUTHWESTERN
INTERFACE

Applicant's Submission, Renderings, and Development Permit Plans

NORR

CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



FROM PEDESTRIAN BRIDGE -
WEST (FROM HAYSBORO)

Applicant's Submission, Renderings, and Development Permit Plans

NORR

CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



VIEW OF MAIN ENTRANCE
FROM HORTON ROAD (WEST)

Applicant's Submission, Renderings, and Development Permit Plans

NORR

CHARTwell
retirement residences

NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



FROM HULL AVENUE FACING NORTH

Applicant's Submission, Renderings, and Development Permit Plans

NORR



NCCA18-0065
CHARTWELL HAYSBORO | DP2019-0978
CALGARY PLANNING COMMISSION APPLICANT PACKAGE
2019-11-12



ELEVATOR ACCESS & SOUTHEASTERN
INTERFACE

10-15-1312	SHIL SUPPLY
------------	-------------

Page 24 of 52

Applicant's Submission, Renderings, and Development Permit Plans



1
DP-102
PERSPECTIVE OVERALL
SCALE: 1:200



3
DP-102
PERSPECTIVE MAIN ENTRY
SCALE: 1:200



2
DP-102
PERSPECTIVE FROM HULL AVENUE SW
SCALE: 1:200



4
DP-102
PEDESTRIAN VIEW FROM BRIDGE
SCALE: 1:200

DATE	ISSUED FOR	REV
20190225	DP	A
20190305	DPB 1	B
20190702	DPB 2	C

NOT FOR CONSTRUCTION

1. This drawing is for informational purposes only and is not to be used for construction. It is not a contract document and does not constitute an offer of any services. It is the property of NORR and shall remain the property of NORR. It is not to be reproduced or used in any way without the written consent of NORR.

This drawing shall not be used for construction. It is not a contract document and does not constitute an offer of any services. It is the property of NORR and shall remain the property of NORR. It is not to be reproduced or used in any way without the written consent of NORR.

Project: CHARTWELL
Sheet: 1 of 1

CHARTWELL
2500 411 St Street SE
Calgary, Alberta, Canada T2D 0T5
nor@nor.ca

NORR
NORR ARCHITECTS PRACTICE
410 1st Avenue SW, Suite 1000
Calgary, Alberta T2P 0K1
403.261.1111
www.norr.ca

Project Name	Drawn
CHARTWELL RETIREMENT RESIDENCES	C. D. A.

CHARTWELL RETIREMENT RESIDENCES
2500 411 St Street SE, Suite 1000
Calgary, Alberta T2D 0T5
Canada

Project: CHARTWELL HAYSBORO
4610 100th St SE
Calgary, AB
T2C 1P8

3D VIEWS

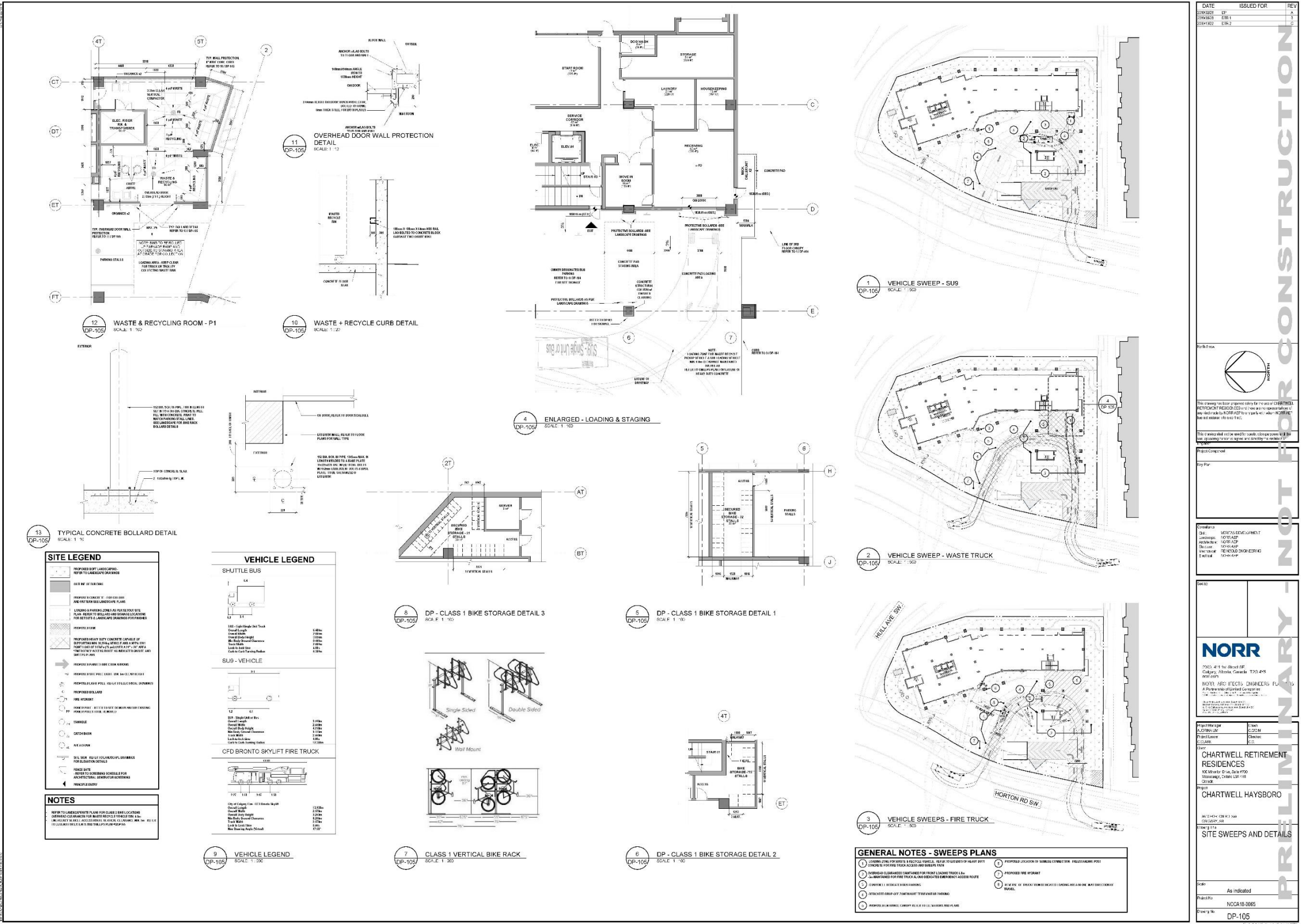
Scale: 1:200
Project No: NCA18-0005
Drawing No: DP-102

Applicant's Submission, Renderings, and Development Permit Plans

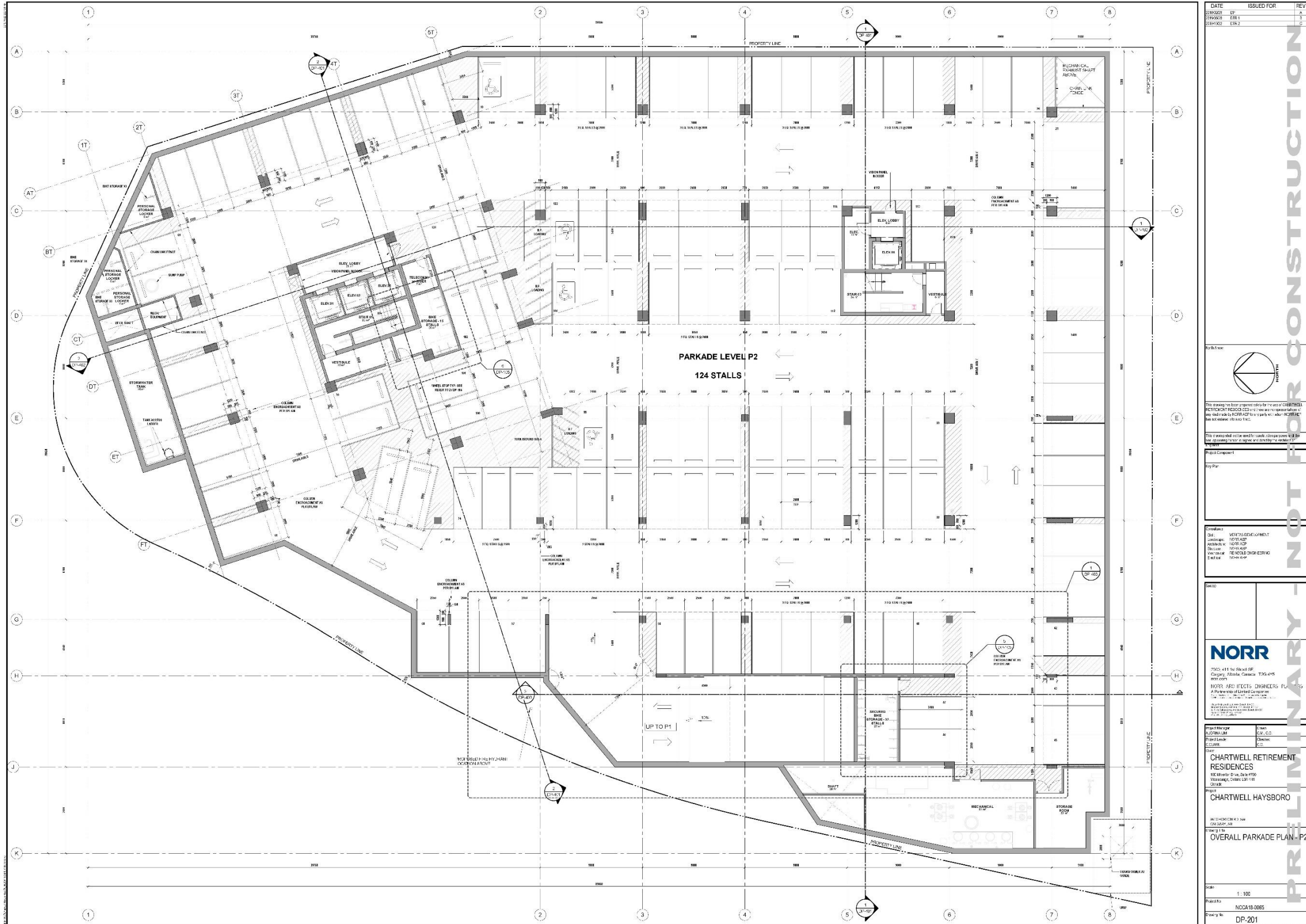


Applicant's Submission, Renderings, and Development Permit Plans

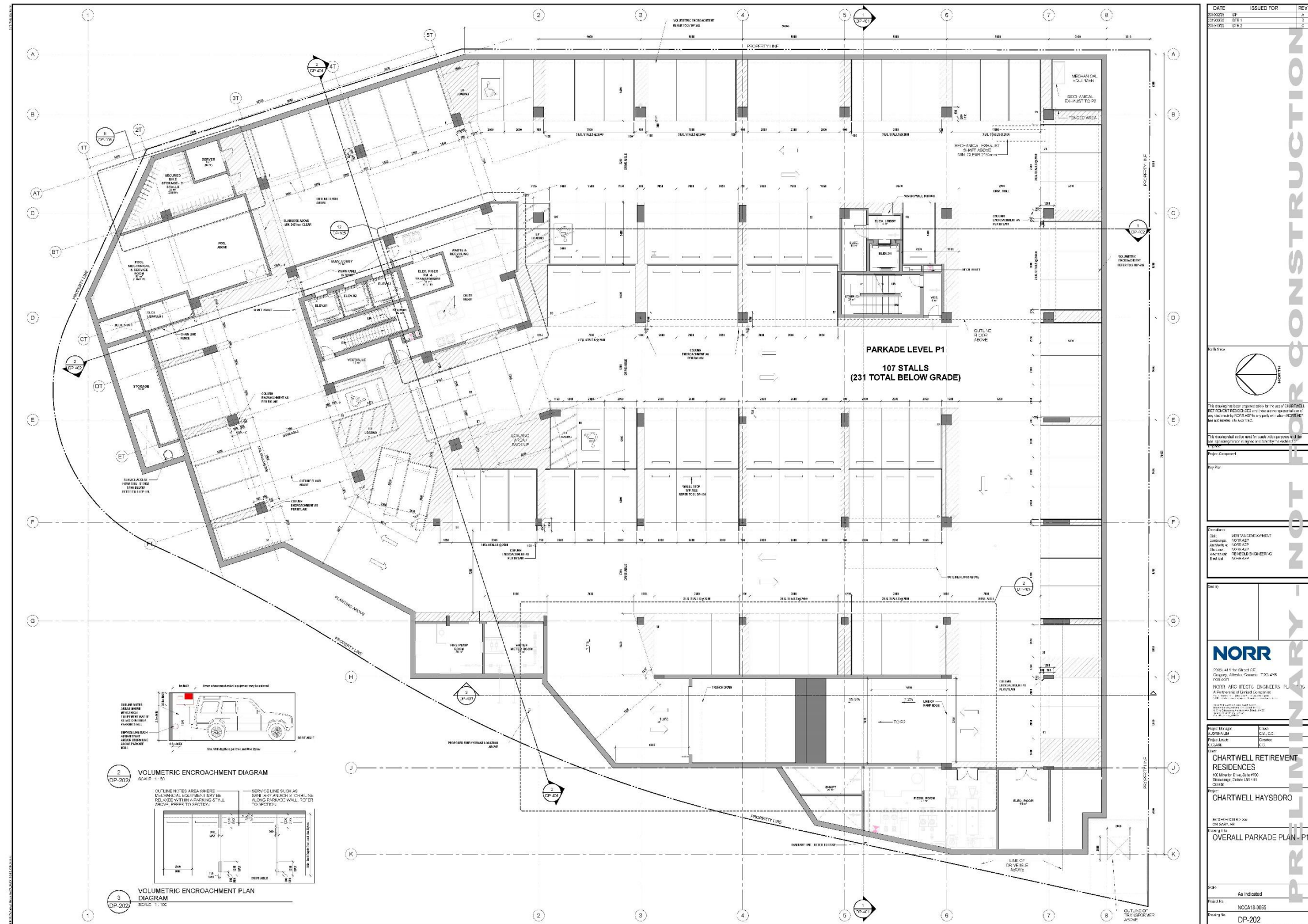
Applicant's Submission, Renderings, and Development Permit Plans



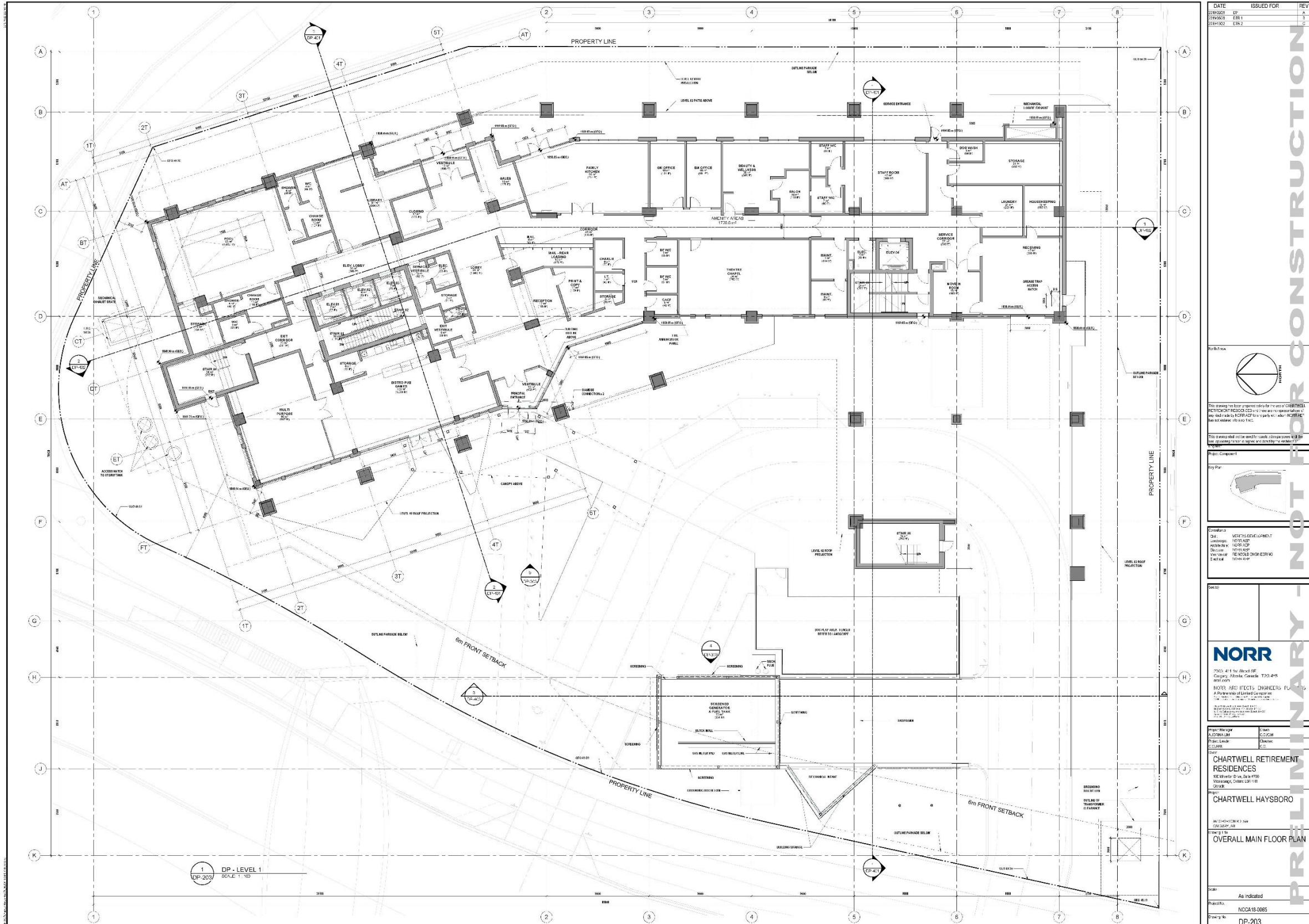
Applicant's Submission, Renderings, and Development Permit Plans



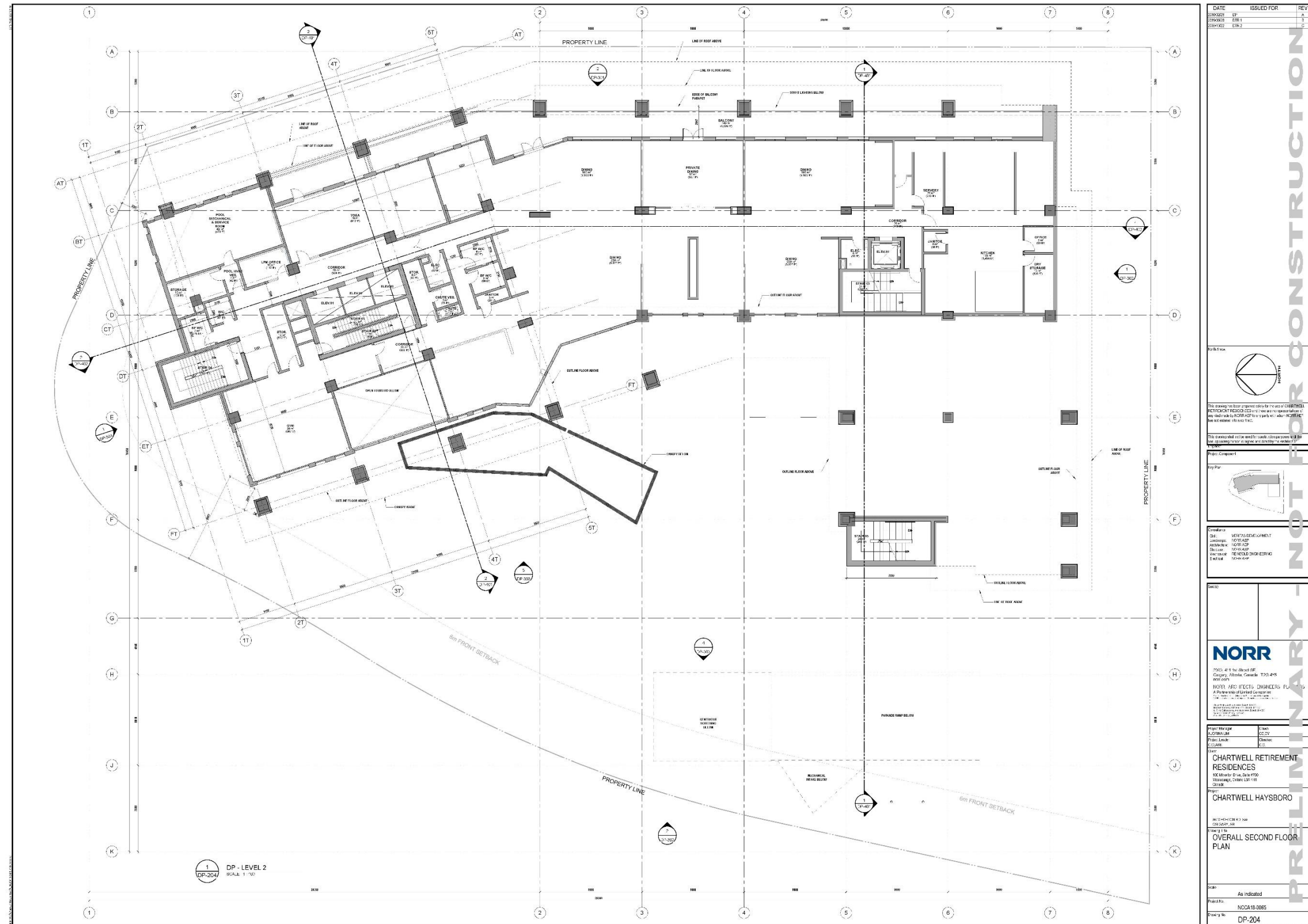
Applicant's Submission, Renderings, and Development Permit Plans



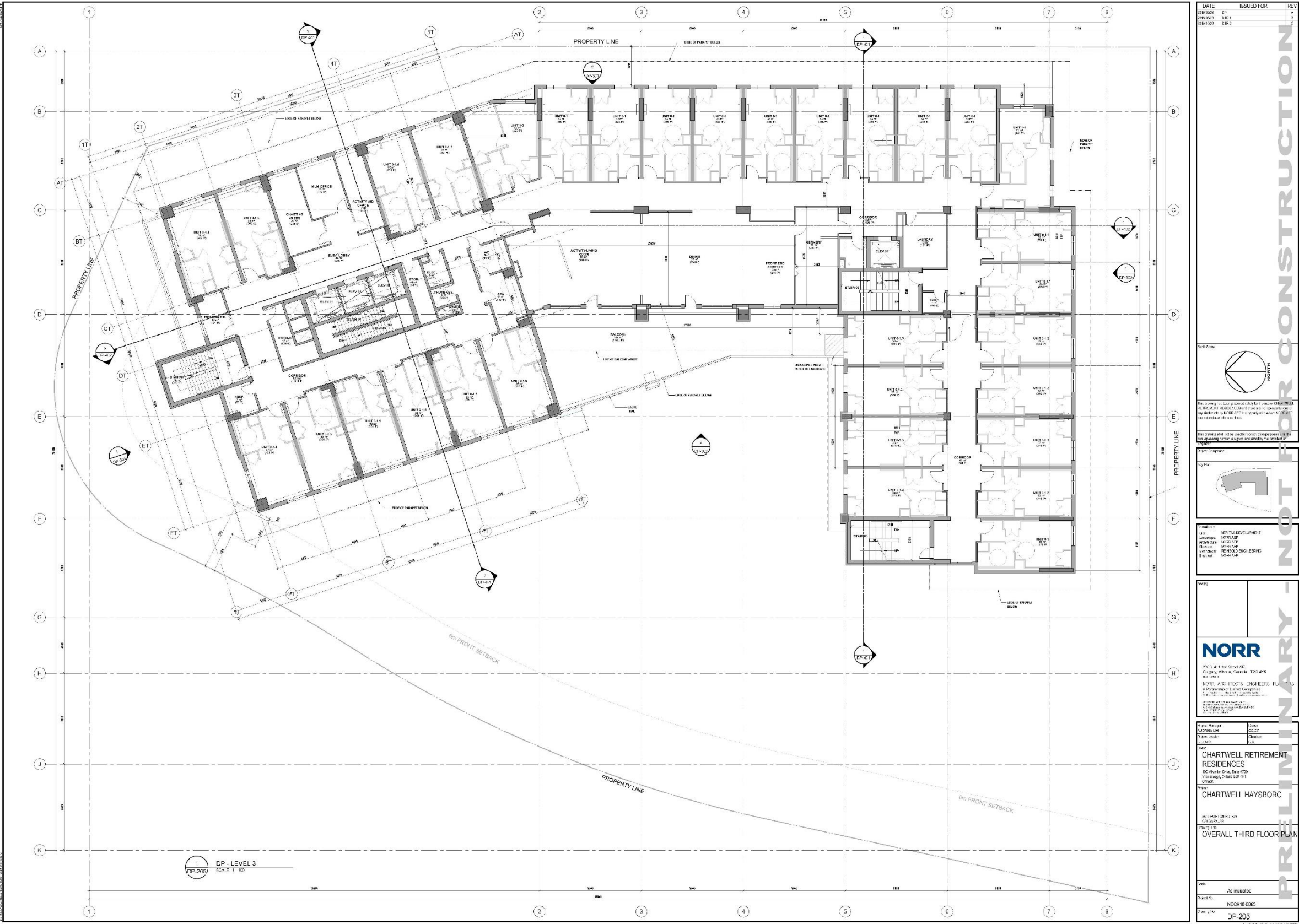
Applicant's Submission, Renderings, and Development Permit Plans



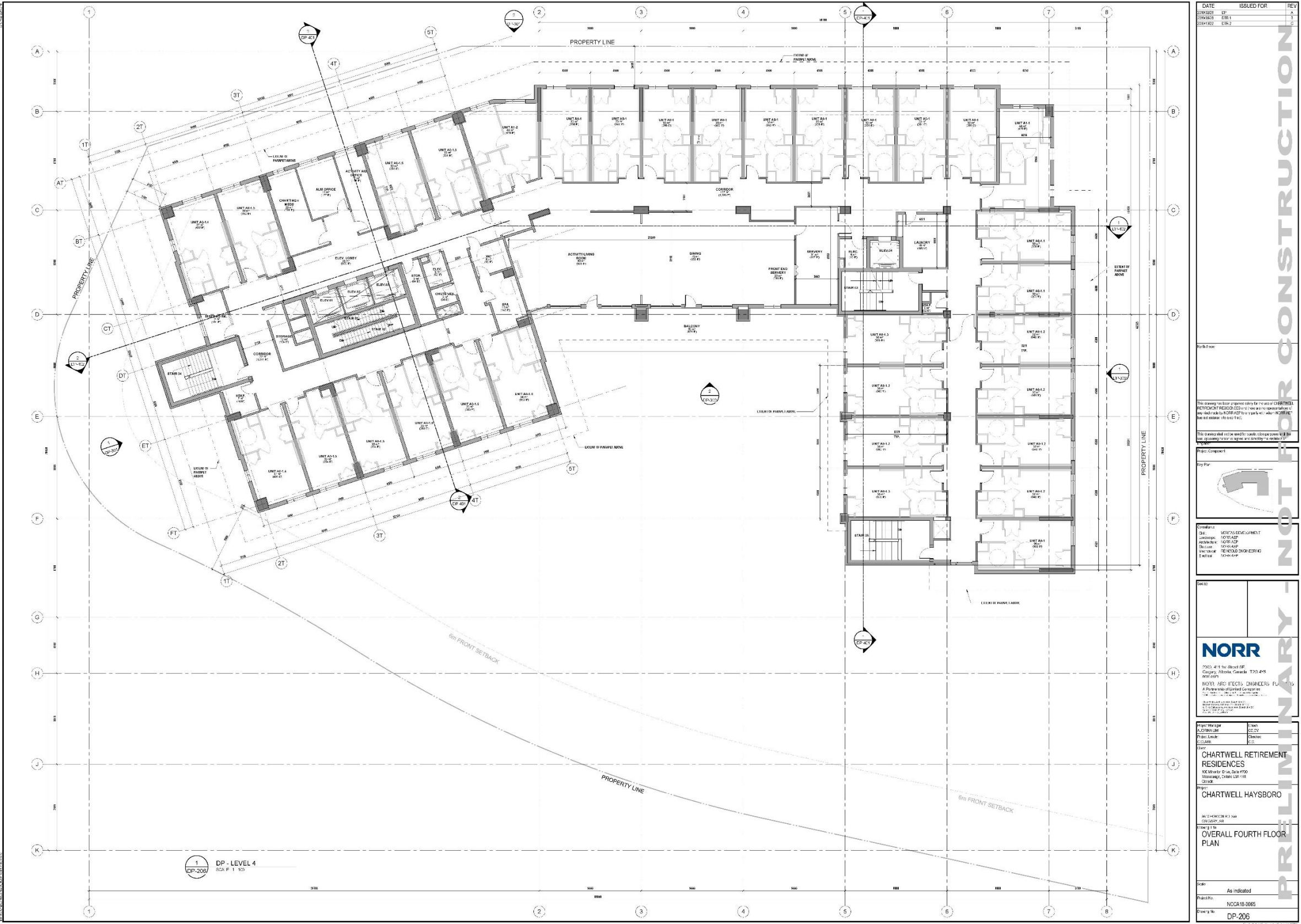
Applicant's Submission, Renderings, and Development Permit Plans



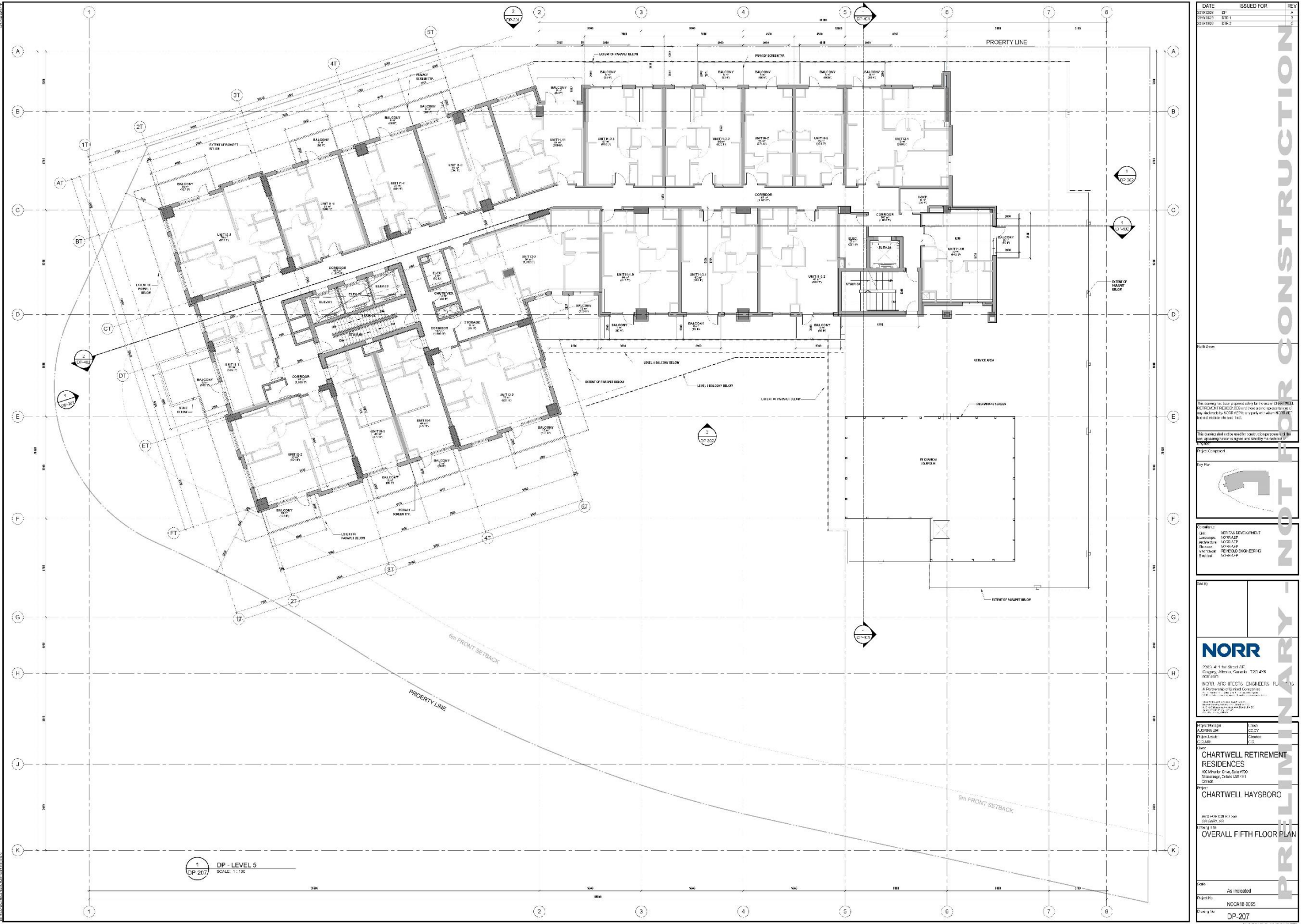
Applicant's Submission, Renderings, and Development Permit Plans



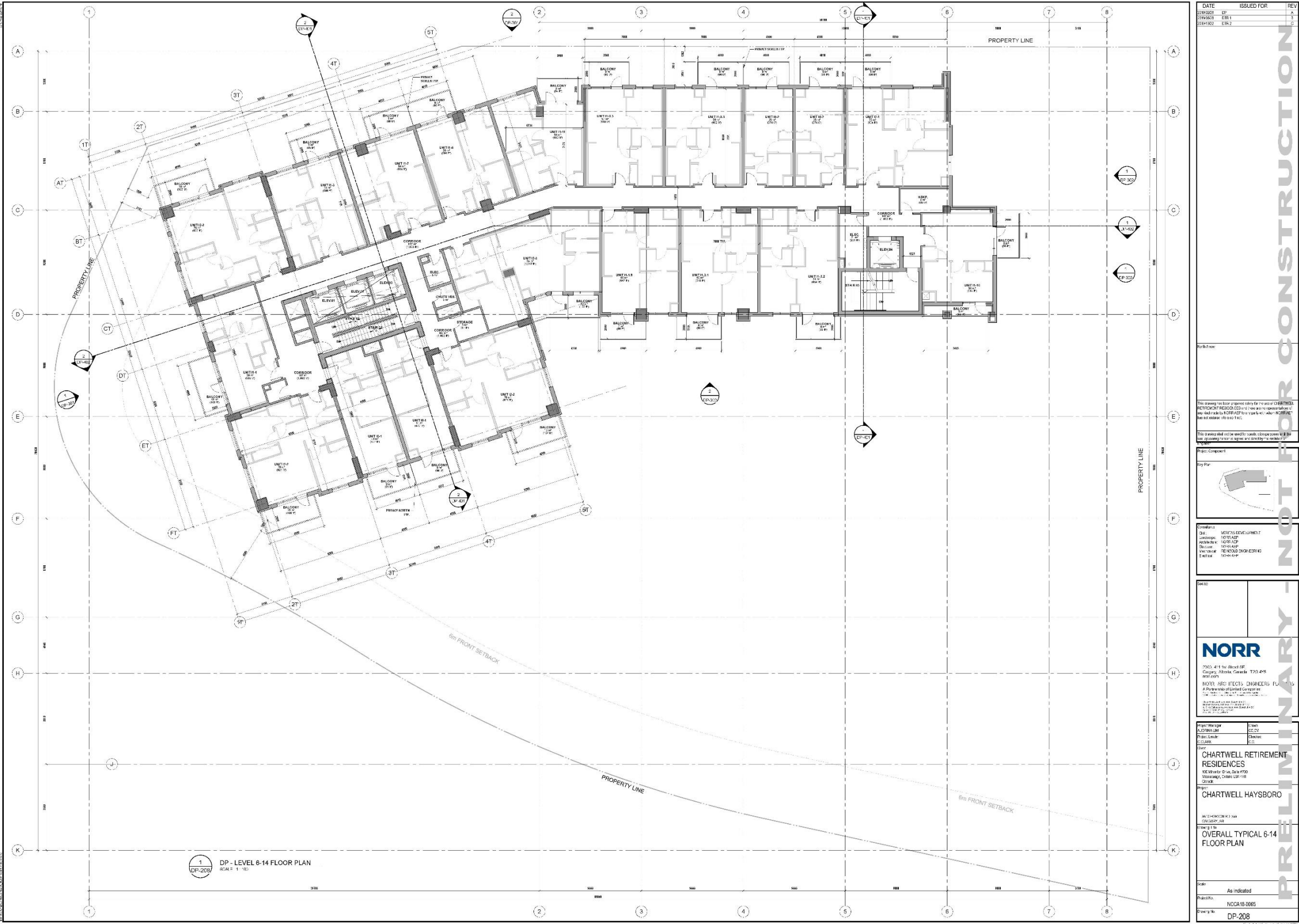
Applicant's Submission, Renderings, and Development Permit Plans



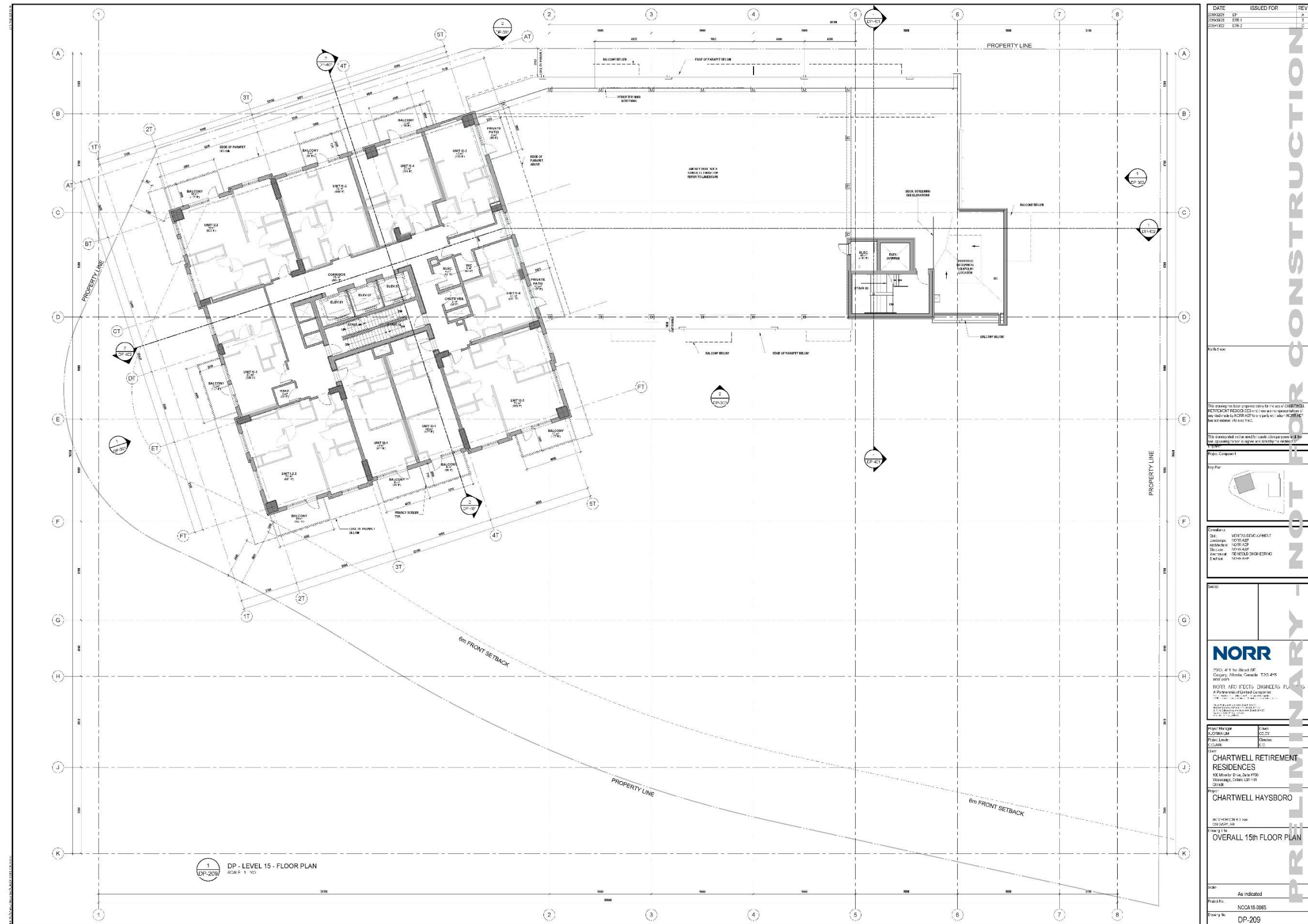
Applicant's Submission, Renderings, and Development Permit Plans



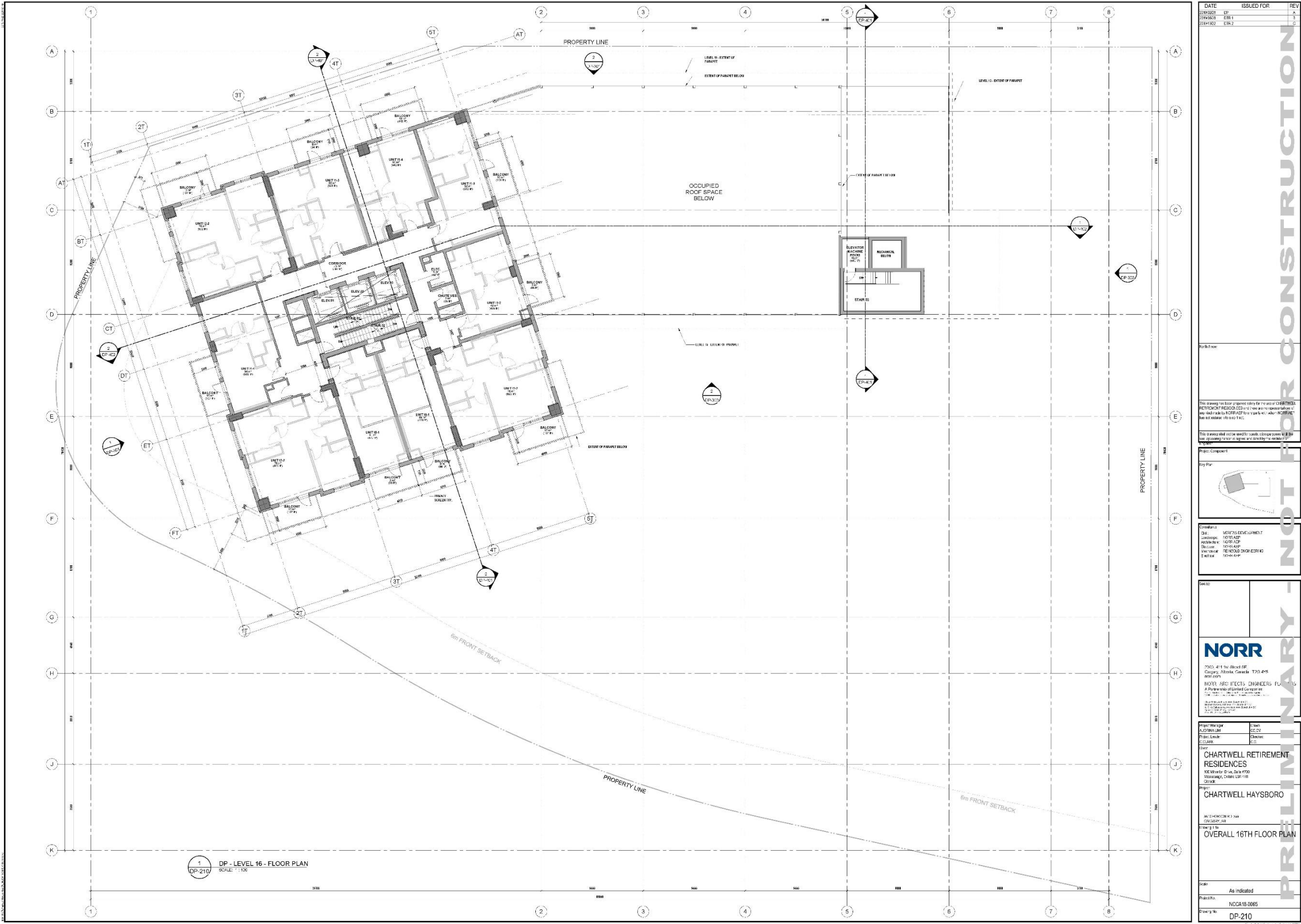
Applicant's Submission, Renderings, and Development Permit Plans



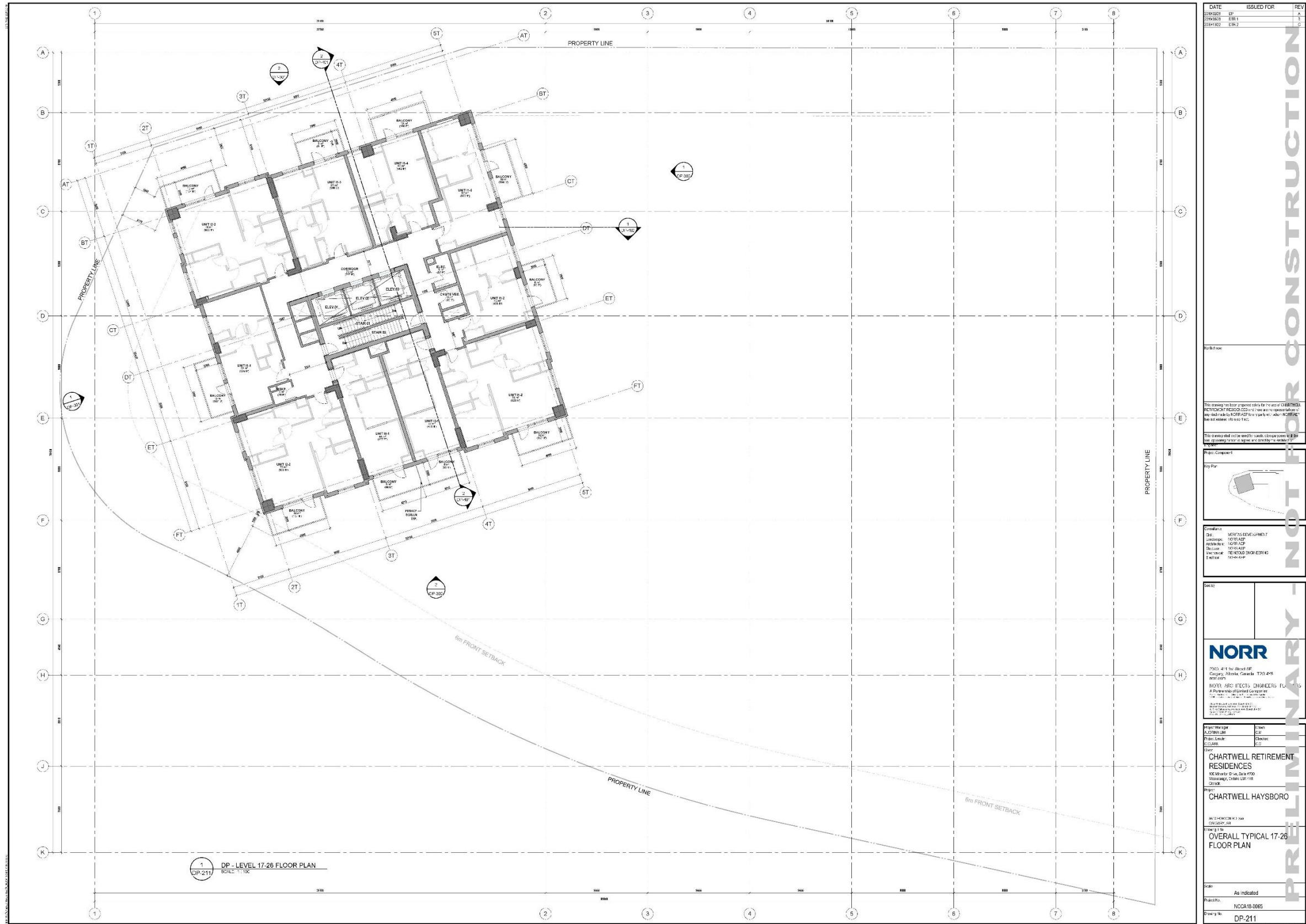
Applicant's Submission, Renderings, and Development Permit Plans



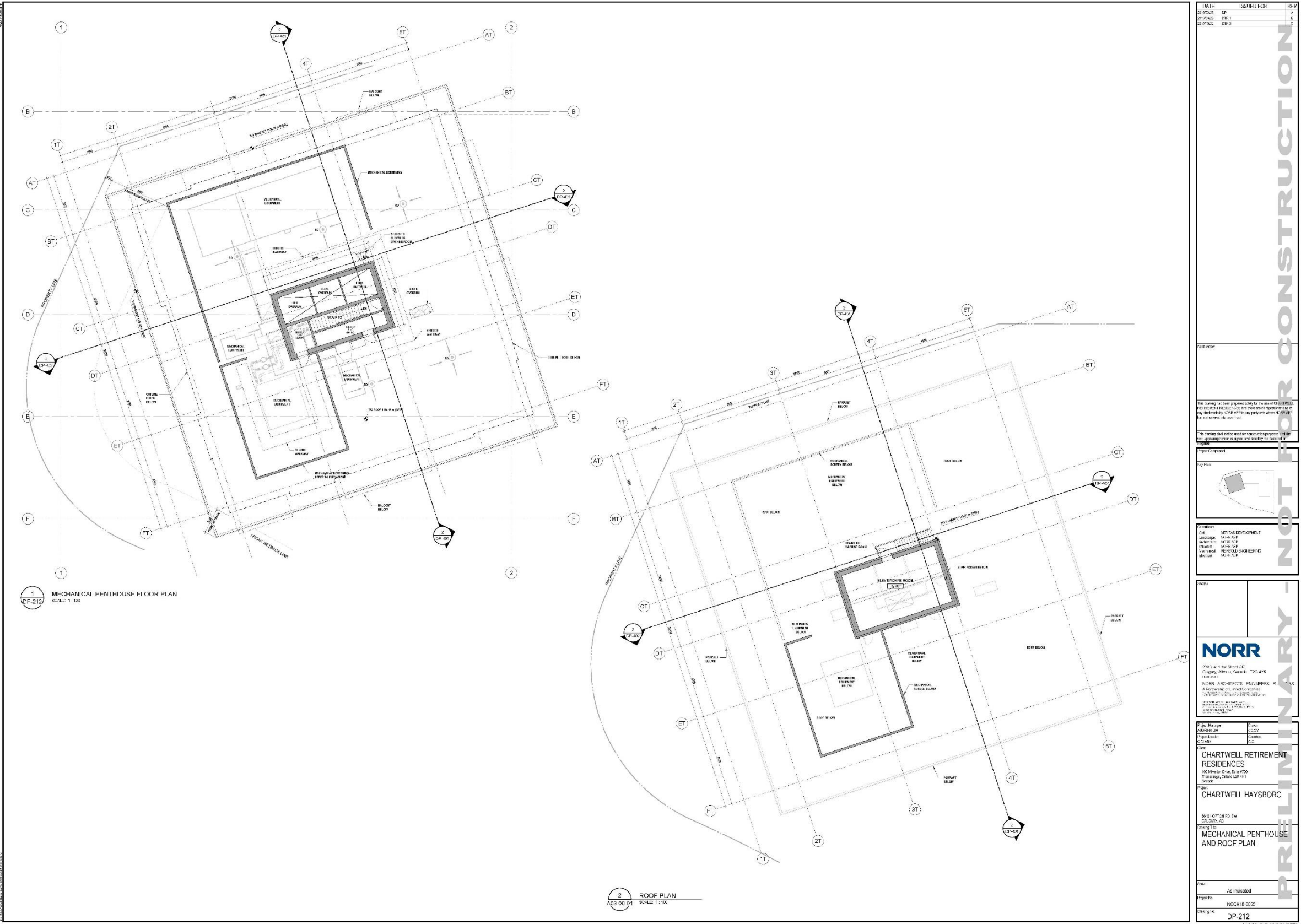
Applicant's Submission, Renderings, and Development Permit Plans



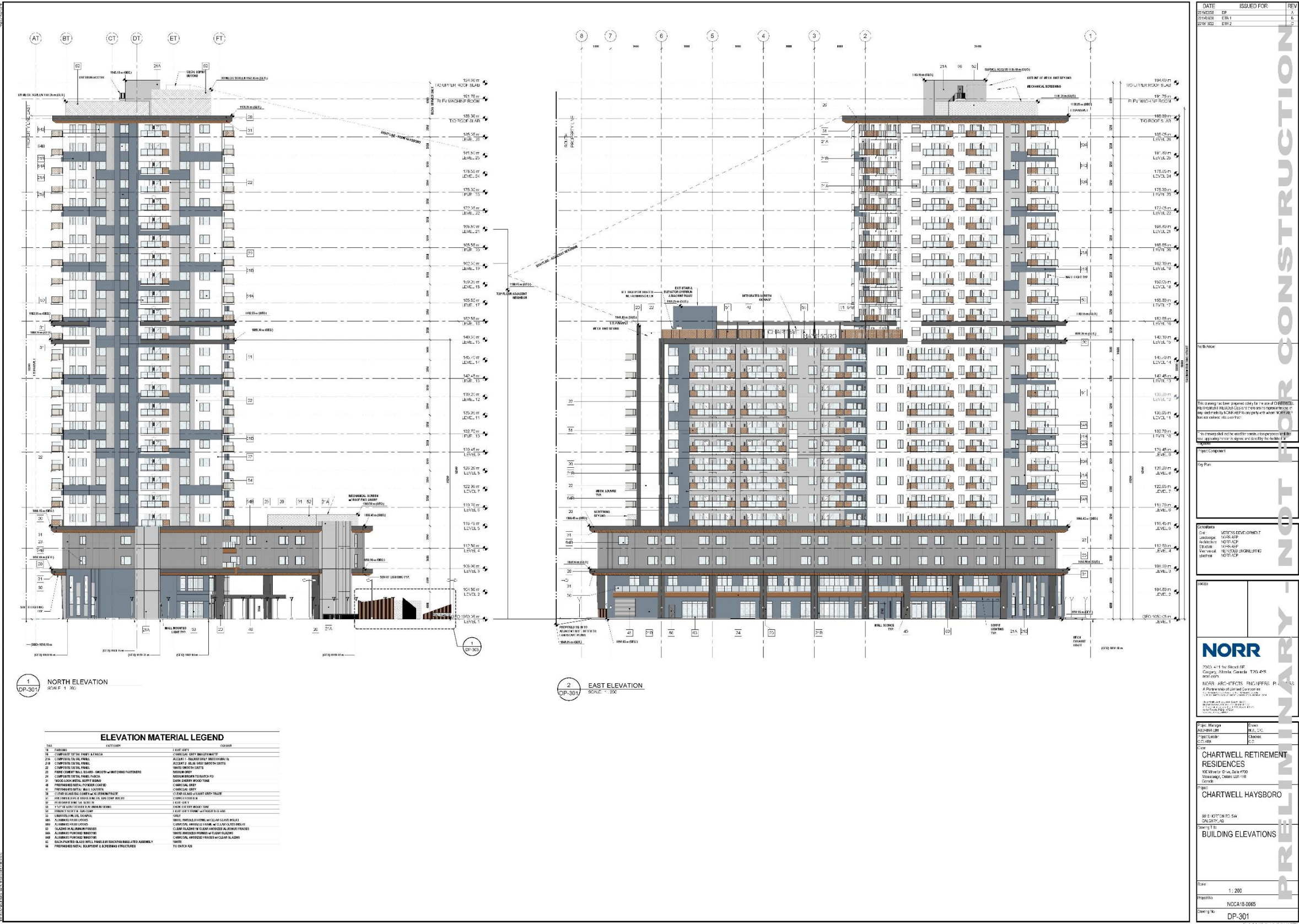
Applicant's Submission, Renderings, and Development Permit Plans



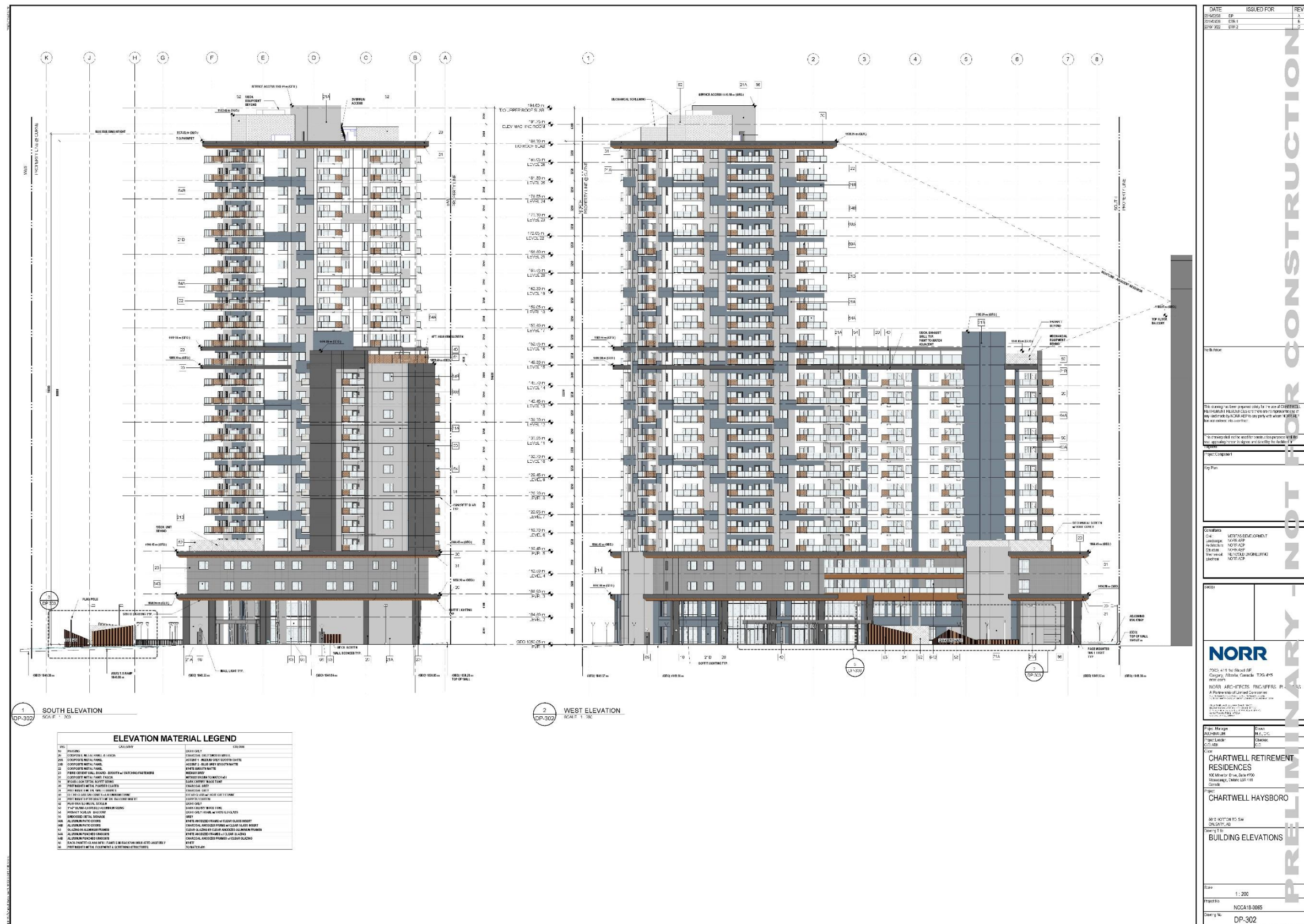
Applicant's Submission, Renderings, and Development Permit Plans



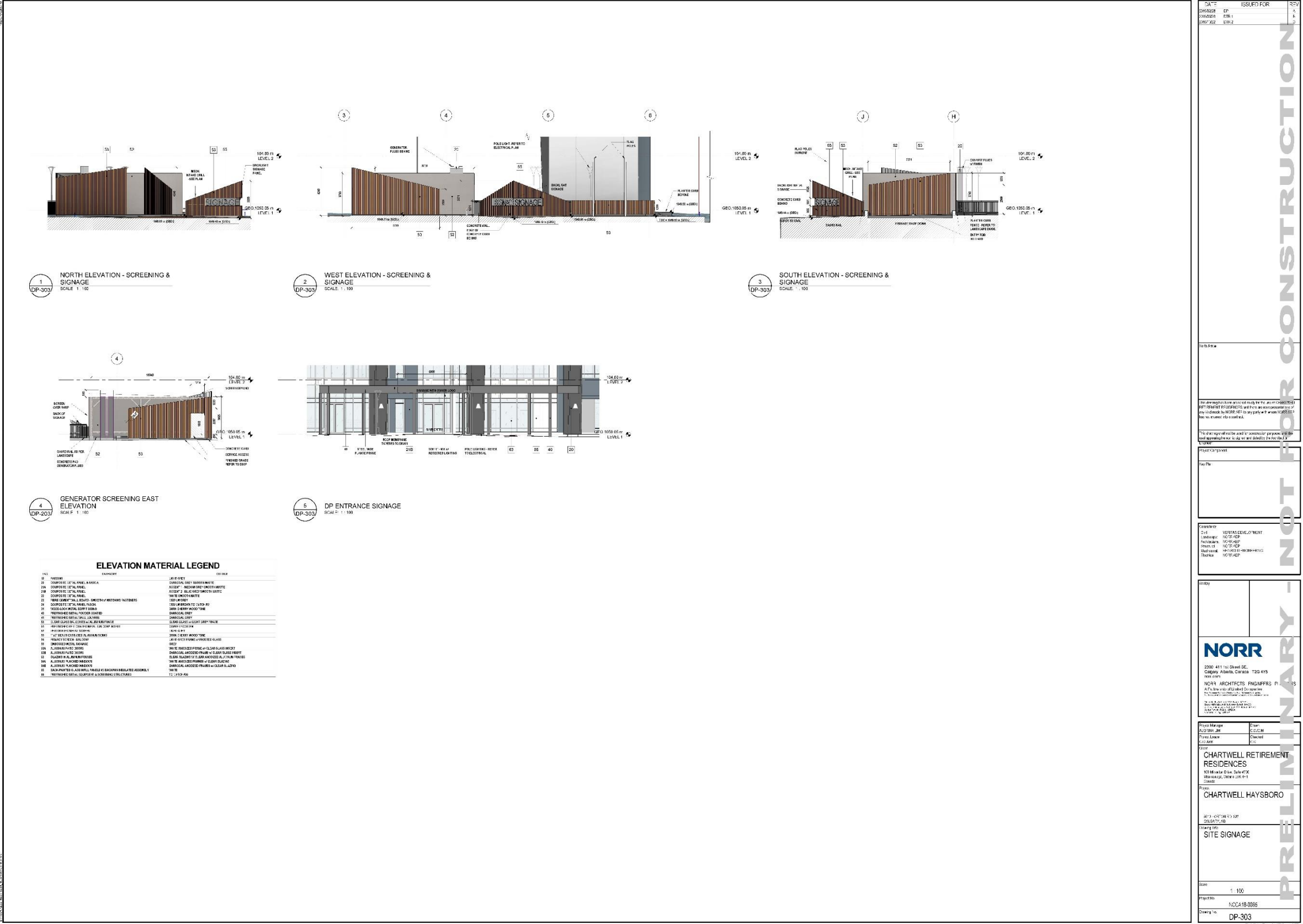
Applicant's Submission, Renderings, and Development Permit Plans



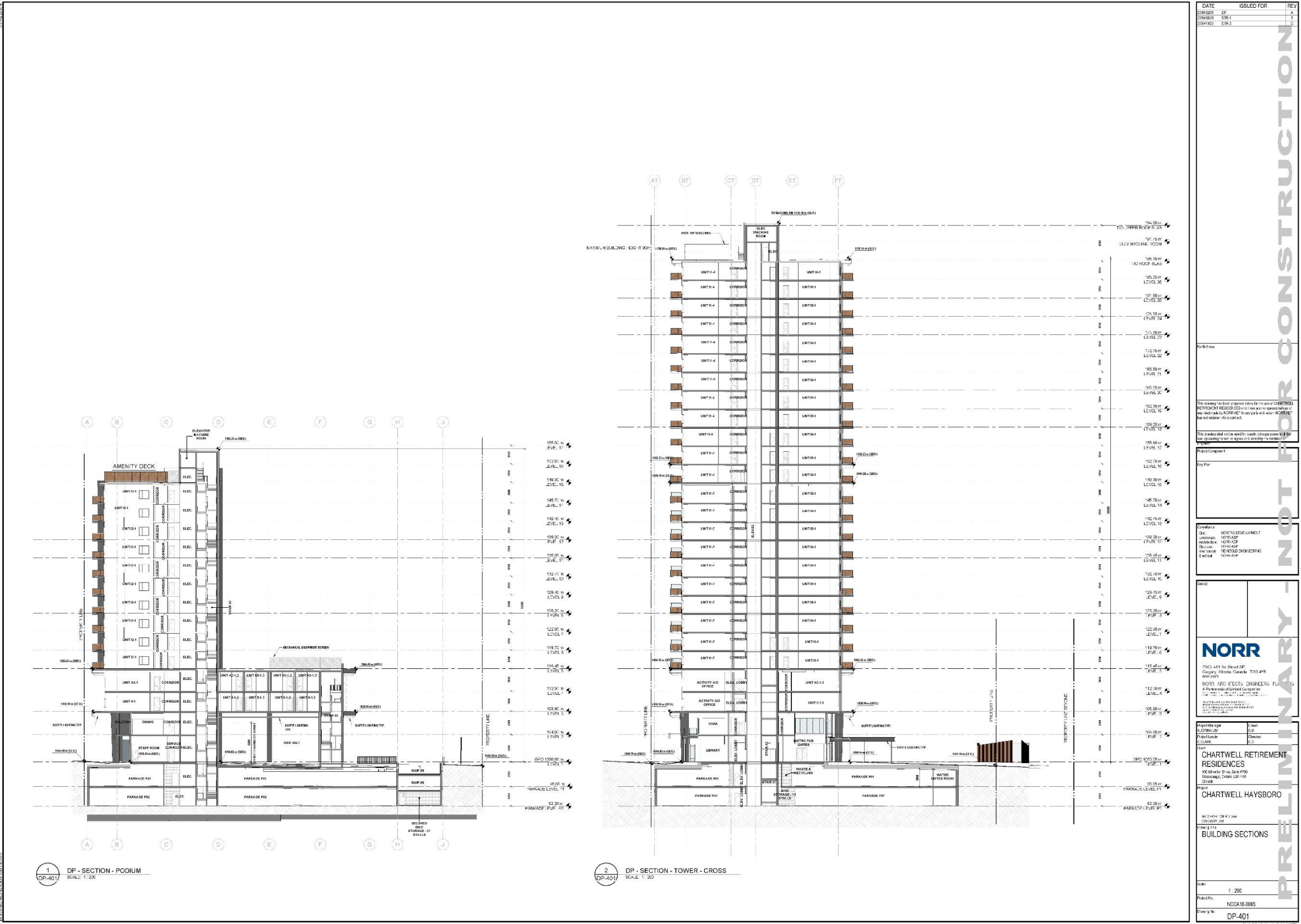
Applicant's Submission, Renderings, and Development Permit Plans

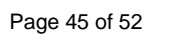


Applicant's Submission, Renderings, and Development Permit Plans

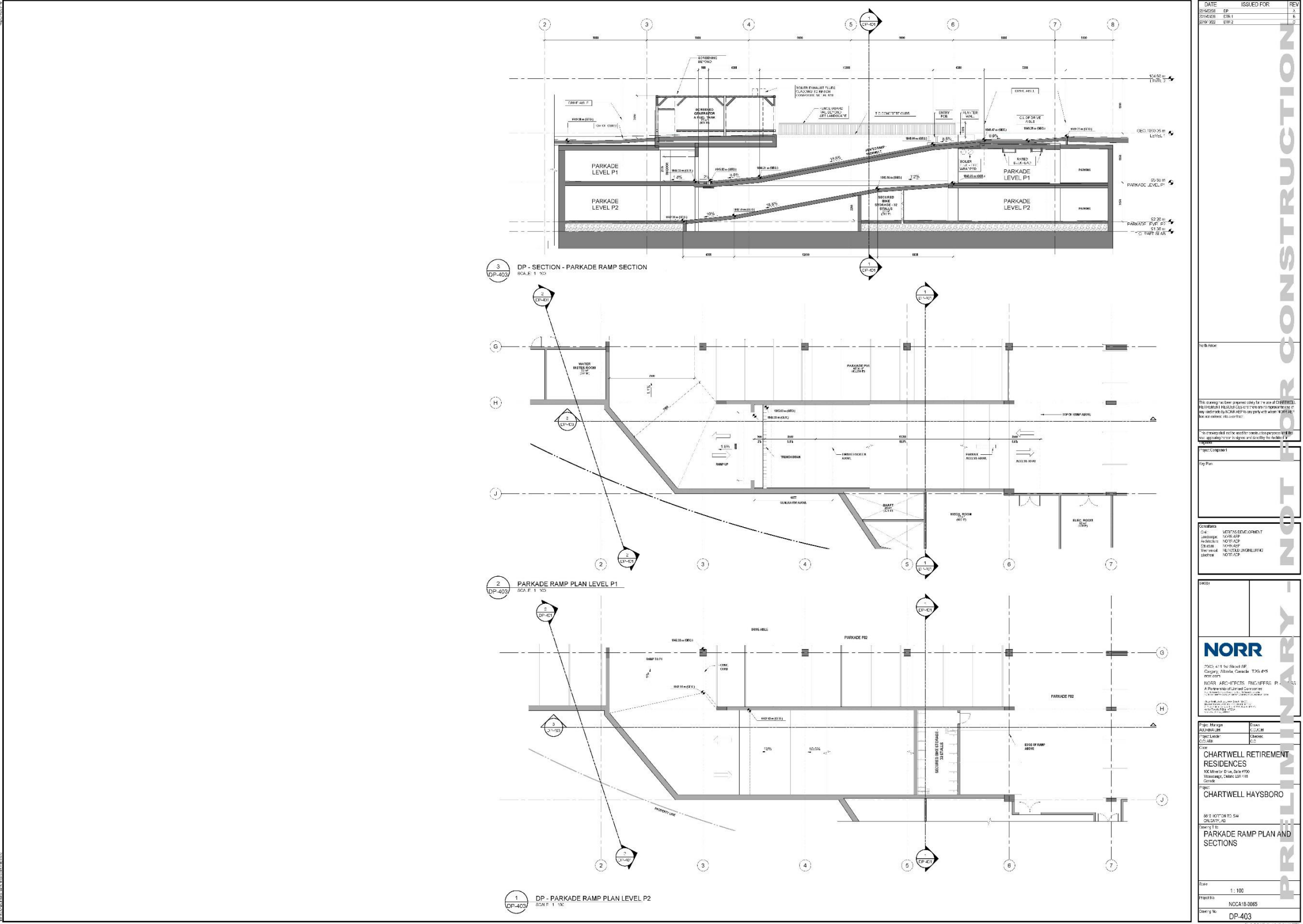


Applicant's Submission, Renderings, and Development Permit Plans





Applicant's Submission, Renderings, and Development Permit Plans



DATE	ISSUED FOR	REV
2019-02-28	DP	1
2019-02-28	DP	2
2019-02-28	DP	3

NOT FOR CONSTRUCTION

NORR
7505 41st Ave. Road 66
Calgary, Alberta, Canada T2G 4K5
norrcorp.com

CHARTWELL RETIREMENT RESIDENCES
1000 Blvd. 17th Ave. SW
Calgary, Alberta T2M 1K1

CHARTWELL HAYSBORO
880 10TH AVENUE SW
CALGARY, AB

PARKADE RAMP PLAN AND SECTIONS

Scale: 1" = 10'

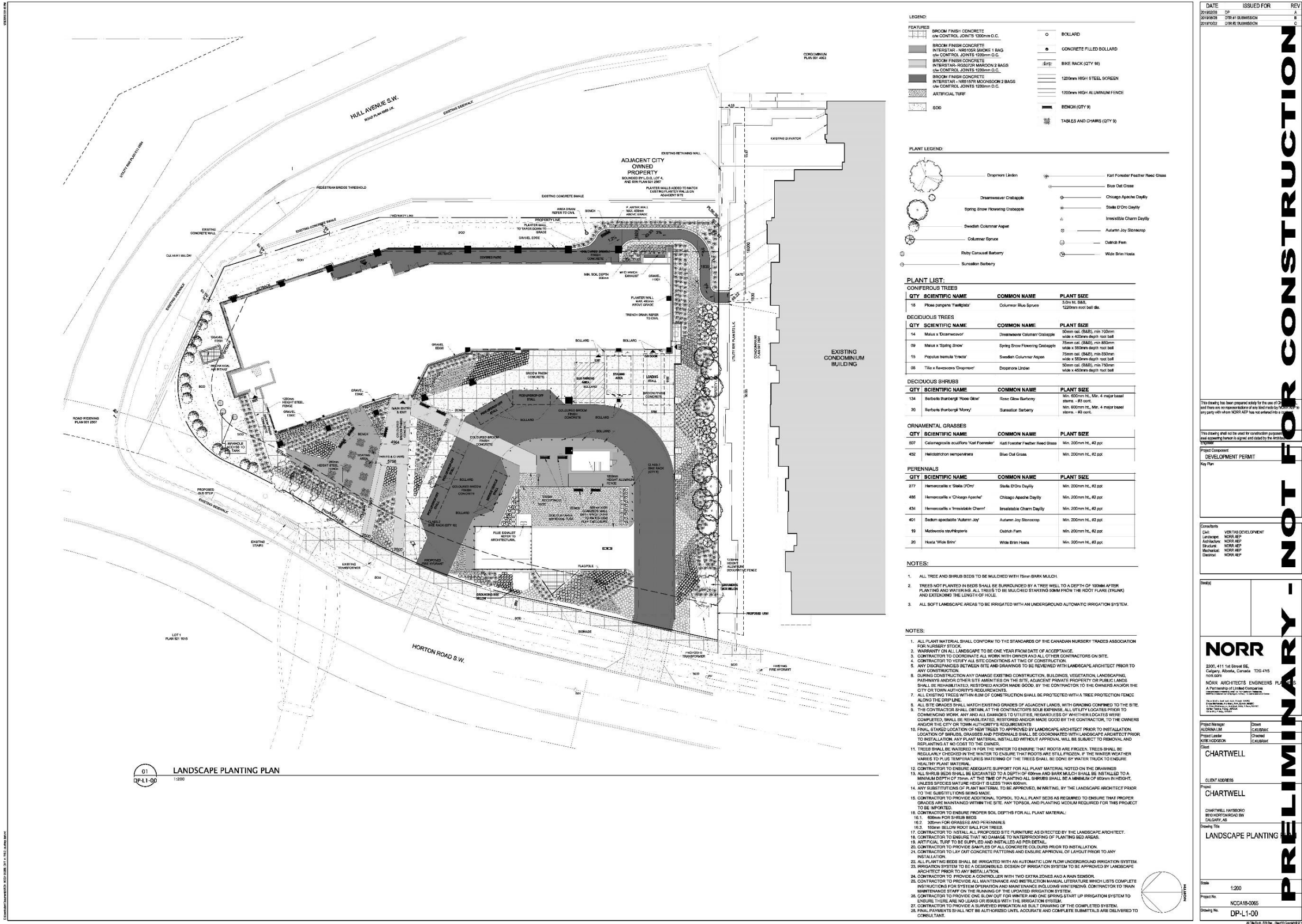
Project No: NCCA18-0085

Drawing No: DP-403

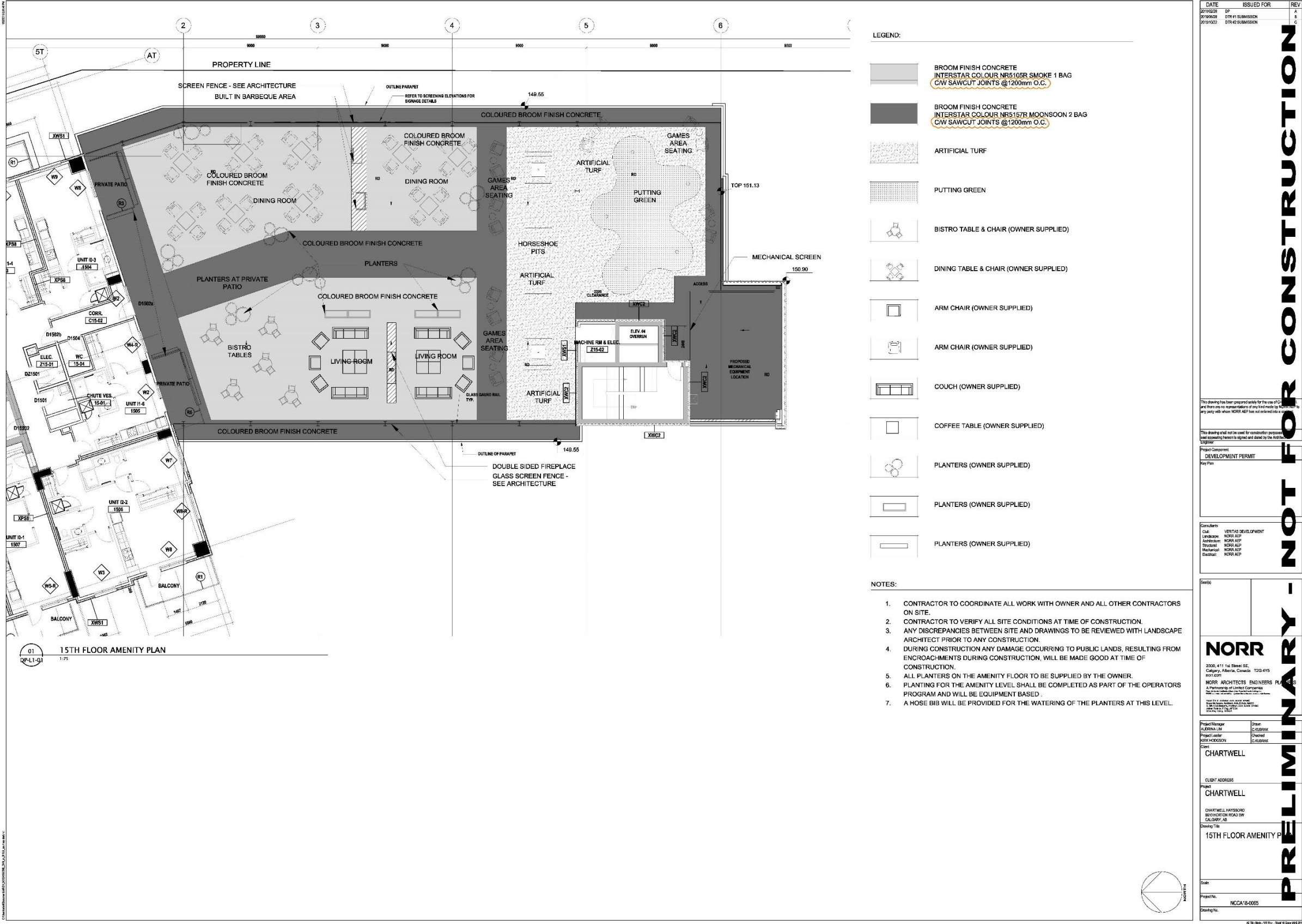
Applicant's Submission, Renderings, and Development Permit Plans



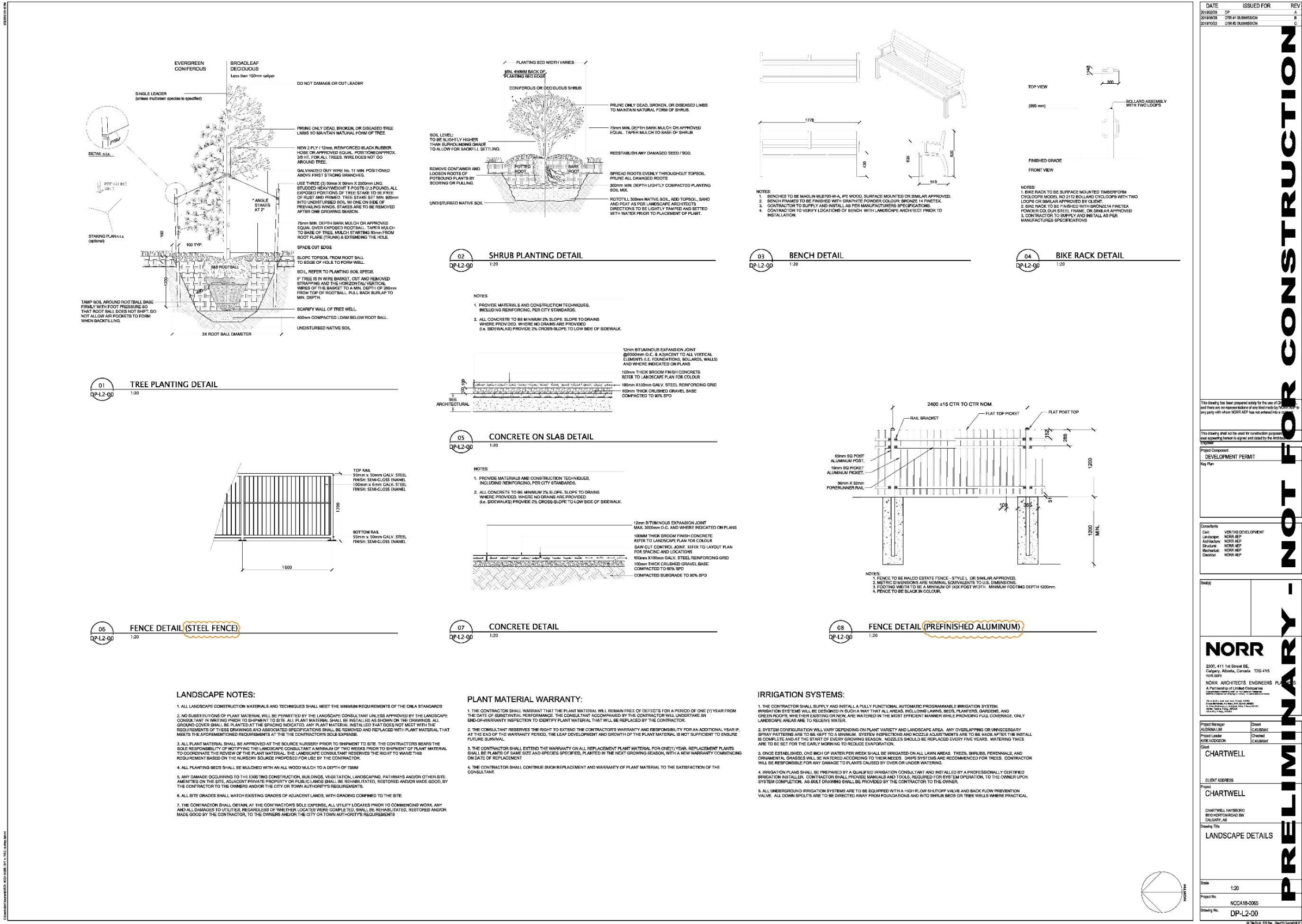
Applicant's Submission, Renderings, and Development Permit Plans



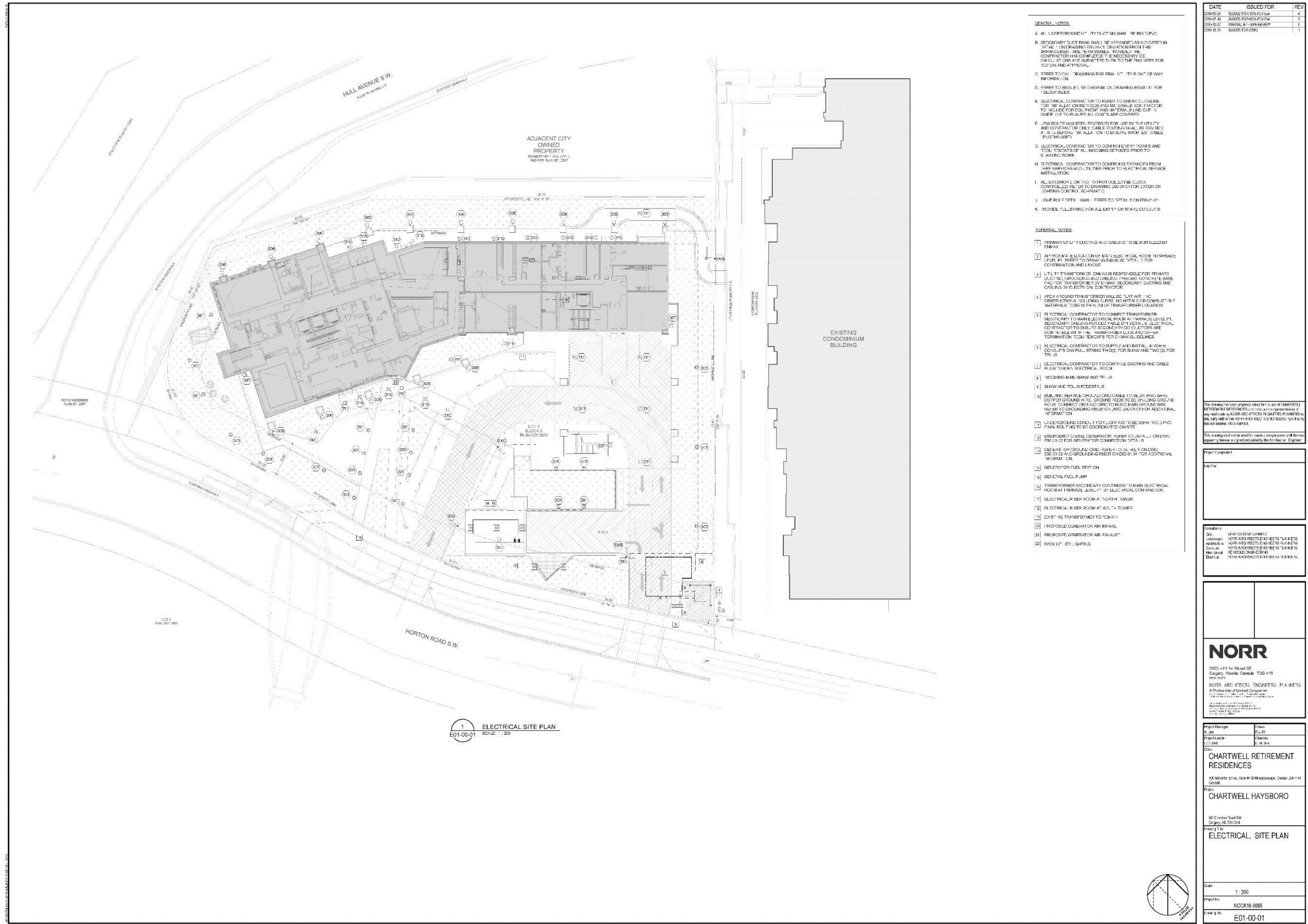
Applicant's Submission, Renderings, and Development Permit Plans



Applicant's Submission, Renderings, and Development Permit Plans



Applicant's Submission, Renderings, and Development Permit Plans



Applicant's Submission, Renderings, and Development Permit Plans

LUMINAIRE SCHEDULE									
Type	Image	Description	Lum.	Voltage	NO.	Voltage	Manufacturer	Model	Mounting
100		SUI. E. SLEEPING AREA LIGHTING	LED	18			KUDCO RECFORD LIGHTING	CAT# F10011	SURFACE
101		POT LIGHTS - COMMON WASH-ROOMS-CORRIDOR	LED	18			ADULTY	CAT# J07701 12.5M 30K 90CRI MVOLT 77 WH	SURFACE
102		VANITY LIGHTING	LED	12		120	VOQUE	CAT# VVS-111-BE	WALL
103		UNDER COUNTER LIGHTING STRIP	LED			120	KICKLEY	CAT# K11100H20WH	SURFACE
104		CAT# K11100H20WH	LED	16.5		120	WAC LIGHTING (VESSEL)	CAT# WVS-H101-32	WALL
200		2' x 2' LIGHTING	LED	26		120	LITHONIA LIGHTING	2BLT2 18.5" ADP 221 L1558 ALTAIR2 RECFOT	RECESSED
201		4' STRIP LIGHTING	LED	31		120	LITHONIA LIGHTING	21 IN 48 5200 M FST MVOLT 4 CK 90CRI WH	SURFACE
202		STAIRWELL LIGHTING	LED	45		120	KENALL	MLH48-48 FMM CP 48LMK OCC 24 LBL	WALL
203		4' GENERAL LINEAR LIGHTING	LED	30		120	AXIS LIGHTING	CLKLD 750 80 35 50 4 W UNV DP 1	RECESSED
204		2.5' GENERAL LINEAR LIGHTING	LED	20		120	AXIS LIGHTING	CJLJL2 750 80 35 50 3.5 W UNV DP 1	RECESSED
205		WALL SCONCE (TYPICAL COMMON AREAS)	LED	21.5		120	WAC LIGHTING	CAT# WVS-30607-BK	WALL
206		GENERAL POT LIGHTING	LED	30		120	INBY	CAT# I1P4-200 14.5M 30K 120-54 90CRI LIT-VIS-CD	RECESSED
207		2' x 4' LIGHTING	LED	49		120	LITHONIA	CAT# SPANU-2X4-480LM-80CRI-30K	RECESSED
208		WALL SCONCE - BEDRM	LED	11		120	BARN LIGHTING U.S.A.	CAT# BLN-1V1WH412	WALL
209		POOL LIGHTING	LED	108		120	PHILIPS LIGHTING	CAT# HSK/2L40-LV4-8WV1-KT18	RECESSED
210		POOL POT LIGHTING	LED	32		120	GOTHEM LIGHTING	CAT# G1051 8.4M 25.4 CK 90CRI 150 CCT	RECESSED
211		PARADE LIGHTING	LED	24		120	BATON VALO L.L.C.	CAT# BV11-02-LED-E-HQ4P	SURFACE
212		MEMORY CASE KITCHENETTE PENDANT LIGHTING	LED	12		120	TCC LIGHTING	CAT# T070200VPMWH-LED200	PENDANT
213		WALL SCONCE - LOBBY CORRIDOR	LED	12.5		120	TCC LIGHTING	CAT# T0005000000-LED000	WALL
214		WALL SCONCE - SALON RECEPTION AREA	LED	99		120	FEISS	CAT# F10011/275K	WALL
215		FAMILY KITCHEN/SALON PENDANT LIGHTING	LED	50		120	TRAX SOLUBLE LIGHTING	CAT# TRD-2115 H S	PENDANT
216		WALL SCONCE - SALON TREATMENT AREA	LED	21.5		120	WAC LIGHTING	CAT# WVS3007-A8	WALL
300		TRAC LIGHTING	LED	142		208	LITHONIA	DSX2-P1-30K-14M 80CRI 20A HLTAMP2-20STX	POLE
300A		TRAC LIGHTING WITH RACK LIGHT CONTROL	LED	142		208	LITHONIA	DSX2-P1-30K-14M 80CRI 20A HLTAMP2-20STX	POLE
300B		TRAC LIGHTING WITH RACK LIGHT CONTROL	LED	142		208	LITHONIA	DSX2-P1-30K-14M 80CRI 20A HLTAMP2-20STX	POLE
300C		TRAC LIGHTING WITH 2 HEAD MOUNTED BACK TO BACK	LED	142		208	LITHONIA	DSX2-P1-30K-14M 80CRI 20A HLTAMP2-20STX	POLE
301		WALL SCONCE LIGHTING	LED	49		208	BECA	LPHN-LED-300K	POLE
302		EXTERIOR LINEAR WALL MOUNT LIGHTING	LED	11.7		208	DEGA	CAT# 40623SG	WALL
303		EXTERIOR WALL MOUNT LIGHTING	LED	56		208	LITHONIA	CAT# WST LED-P3-30K-20W-PERS-008"XD	WALL
310		DOWN LIGHT FOR ENTRANCE	LED	42		208	INBY	CAT# LLPV-40LM 30K MVOLT 65 90CRI E210 HLTAMP2	RECESSED
311		DOWN LIGHT UNDER SOFFIT	LED	45		208	INBY	CAT# LLPV-40LM 30K MVOLT 65 90CRI E210 HLTAMP2	RECESSED
EX1		EXIT LIGHTING	LED	16.5		120	STANPRO	REMANOV-INDC	SEE REMARKS

DATE

ISSUED FOR

REV

2019-01-01

REVISIONS

1

2

3

4

5

6

7

8

9

10

11

12

13

14

15

16

17

18

19

20

21

22

23

24

25

26

27

28

29

30

31

32

33

34

35

36

37

38

39

40

41

42

43

44

45

46

47

48

49

50

51

52

53

54

55

56

57

58

59

60

61

62

63

64

65

66

67

68

69

70

71

72

73

74

75

76

77

78

79

80

81

82

83

84

85

86

87

88

89

90

91

92

93

94

95

96

97

98

99

100

101

102

103

104

105

106

107

108

109

110

111

112

113

114

115

116

117

118

119

120

121

122

123

124

125

126

127

128

129

130

131

132

133

134

135

136

137

138

139

140

141

142

143

144

145

146

147

148

149

150

151

152

153

154

155

156

157

158

159

160

161

162

163

164

165

166

167

168

169

170

171

172

173

174

175

176

177

178

179

180

181

182

183

184

185

186

187

188

189

190

191

192

193

194

195

196

197

198

199

200

201

202

203

204

205

206

207

208

209

210

211

212

213

214

215

216

217

218

219

220

221

222

223

224

225

226

227

228

229

230

231

232

233

234

235

236

237

238

239

240

241

242

243

244

245

246

247

248

249

250

251

252

253

254

255

256

257

258

259

260

261

262

263

264

265

266

267

268

269

270

271

272

273

274

275

276

277

278

279

280

281

282

283

284

285

286

287

288

289

290

291

292

293

294

295

296

297

298

299

300

301

302

303

304

305

306

307

308

309

310

311

312

313

314

315

316

317

318

319

320

321

322

323

324

325

326

327

328

329

330

331

332

333

334

335

336

337

338

339

340

341

342

343

344

345

346

347

348

349

350

351

352

353

354

355

356

357

358

359

360

361

362

363

364

365

366

367

368

369

370

371

372

373

374

375

376

377

378

379

380

381

382

383

384

385

386

387

388

389

390

391

392

393

394

395

396

397

398

399

400

401

402

403

404

405

406

407

408

409

410

411

412

413

414

415

416

417

418

419

420

421

422

423

424

425

426

427

428

429

430

431

432

433

434

435

436

437

438

439

440

441

442

443

444

445

446

447

448

449

450

451

452

453

454

455

456

457

458

459

460

461

462

463

464

465

466

467

468

469

470

471

472

473

474

475

476

477

478

479

480

481

482

483

484

485

486

487

488

489

490

491

492

493

494

495

496

497

498

499

500

501

502

503

504

505

506

507

508

509

510

511

512

513

514

515

516

517

518

519

520

521

522

523

524

525

526

527

528

529

530

531

532

533

534

535

536

537

538

539

540

541

542

543

544

545

546

547

548

549

550

551

552

553

554

555

556

557

558

559

560

561

562

563

564

565

566

567

568

569

570

571

572

573

574

575

576

577

578

579

580

581

582

583

584

585

586

587

588

589

590

591

592

593

594

595

596

597

598

599

600

601

602

603

604

605

606

607

608

609

610

611

612

613

614

615

616

617

618

619

620

621

622

623

624

625

626

627

628

629

630

631

632

633

634

635

636

637

638

639

640

641

642

643

644

645

646

647

648

649

650

651

652

653

654

655

656

657

658

659

660

661

662

663

664

665

666

667

668

669

670

671

672

673

674

675

676

677

678

679

680

681

682

683

684

685

686

687

688

689

690

691

692

693

694

695

696

697

698

699

700

701

702

703

704

705

706

707

708

709

710

711

712

713

714

715

716

717

718

719

720

721

722

723

724

725

726

727

728

729

730

731

732

733

734

735

736

737

738

739

740

741

742

743

744

745

746

747

748

749

750

751

752

753

754

755

756

757

758

759

760

761

762

763

764

765

766

767

768

769

770

771

772

773

774

775

776

777

778

779

780

781

782

783

784

785

786

787

788

789

790

791

792

793

794

795

796

797

798

799

800

801

802

803

804

805

806

807

808

809

810

811

812

813

814

815

816

817

818

819

820

821

822

823

824

825

826

827

828

829

830

831

832

833

834

835

836

837

838

839

840

841

842

843

844

845

846

847

848

849

850

851

852

853

854

855

856

857

858

859

860

861

862

863

864

865

866

867

868

869

870

871

872

873

874

875

876

877

878

879

880

881

882

883

884

885

886

887

888

889

890

891

892

893

894

895

896

897

898

899

900

901

902

903

904

905

906

907

908

909

910

911

912

913

914

915

916

917

918

919

920

921

922

923

924

925

926

927

928

929

930

931

932

933

934

935

936

937

938

939

940

941

942

943

944

945

946

947

948

949

950

951

952

953

954

955

956

957

958

959

960

961

962

963

964

965

966

967

968

969

970

971

972

973

974

975

976

977

978

979

980

981

982

983

984

985

986

987

988

989

990

991

992

993

994

995

996

997

998

999

1000

UDRP Comments and Applicant Response

Urban Design Review Panel Comments

Date	December 12, 2019	
Time	2:00	
Panel Members	Present Janice Liebe (Chair) Robert Leblond Terry Klassen Chris Hardwicke	Absent Chad Russill (Co-Chair) Gary Mundy Amelia Hollingshurst Yogeshwar Navagrah Jack Vanstone Ben Bailey Bruce Nelligan Glen Pardoe (recused) Eric Toker
Advisor	David Down, Chief Urban Designer	
Application number	PE2018-01104	
Municipal address	8610 Horton Rd SW	
Community	Haysboro	
Project description	Pre-Application for a Proposed Development	
Review	first	
File Manager	Ezra Wasser	
City Wide Urban Design	Lothar Wiwjorra	
Applicant	NORR Architects, Engineers, Planners	
Panel Position	Endorse / Further Review Recommended	

Note: Overall ranking is not an average of the 13 *Urban Design Elements*, but a qualitative project rating of the application in its context.

5=Excellent (exceeds expectations, clear intent to achieve high quality outcome)

4=Good (somewhat exceeds expectations, high probability of success)

3=Average (meets expectations, likely to achieve basic standard of quality outcome)

2=Fair (somewhat meets expectations, multiple weaknesses or deficiencies)

1=Poor (does not meet expectations, demonstrates some lack of understanding of requirements)

Summary

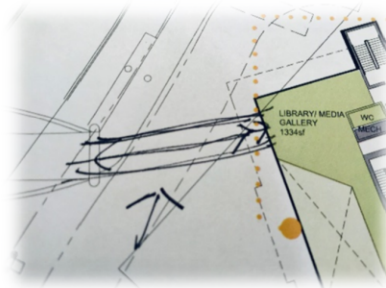
The proposal to redevelop a difficult site near the Heritage LRT Station will offer an array of senior care options. Site challenges include establishing an active street-presence on a site below the significantly-sloped Hull Avenue SW and connection to the existing pedestrian bridge that crosses the busy Horton Road SW, the freight rail and the LRT lines to the Station.

The main areas of priority are outlined below and support the rated urban design elements –

- 1) Create direct connections for active seniors to streets and Station bridge that are accessible, safe, and convenient.
 - a) The discussion between Panel, Applicant and City revealed new information about a potential shared-use-public-access from Hull Ave to an elevator/stairwell in the neighbouring London Tower.

UDRP Comments and Applicant Response

- b) The Panel recommends functional design assessment of the bridge extending directly into what is currently shown as the Library/Media Gallery on the Level 02 Amenity Floor (see sketch notation below.)



- 2) Without changing architectural massing, consider colour and explore options to break up the white masses as well as tying together the lower and upper masses (positive precedent image examples shown in the Applicant's UDRP submission).
- 3) Further design exploration should give consideration of the blank-south-facing-stepped-back-11-storey-wall facing the neighbouring London Tower, during day and dark.

The Panel supports the positive impact and demographic diversity the 383-unit density will contribute to the area.

Applicant Response

1 a) An existing elevator on the southern London Towers site provides a potential opportunity for controlled access for residents of our proposed site. A convenient and safe passageway from our site to the elevator will be explored if an applicable access agreement can be reached. This is currently being pursued by our team.

An at-grade connection was also discussed as an option east of the proposed building up to Hull Avenue. In a concurrent meeting held by the Owner and the City of Calgary's legal department, the City had advised that this connection would be required to be temporary in nature and advised to be removable on City order within a 30 day notification period. Due to grading changes and requests for temporary/removable structures on City-owned land, this is not viewed as an efficient means of negotiating the slope up to Hull Ave for access to transit or Macleod Trail.

We will continue to explore the possible use of the adjacent site's elevator.

1 b)- The request made for connection to the Pedestrian Bridge on City-owned land is not a recommended solution for the site from a programmatic or security perspective. The Pedestrian Bridge is located outside of the property line and connects for a larger number of public users to cross over Horton Road and the train line. From a security standpoint, this interface is not coordinated to the main building entrance and program, and safety is of utmost concern for the resident demographic.

Concerns also lie with the bridge's existing foundations and structure in addition to maintenance of the existing structure and repairs.

To summarize, our team is of the position there is adequate connection to the public realm along Horton Road to satisfy pedestrian connectivity for this permit application; sidewalk upgrades along Horton/Hull Ave may be required to emphasize this connection, to be further discussed with the City during the review period.

UDRP Comments and Applicant Response

2- Further studies on the design of the tower will look to utilize material screening on the building's balconies to integrate with the exterior design features on the podium. This will formulate a more seamless design, and further reflect the concept of the building as a connected, vertical neighbourhood.

3- Transparency and adjustment to material finish will help break up this building face and reduce monotony. Elevations will demonstrate design of this interface with the London Towers to the south.

Urban Design Element		
Creativity <i>Encourage innovation; model best practices</i> <ul style="list-style-type: none"> Overall project approach as it relates to original ideas or innovation 		
UDRP Commentary	The Applicant's presentation was thorough and responsive to its innovative vertical urban neighbourhood vision.	
Applicant Response	<i>The team will continue this approach and concept moving forward into detailed design.</i>	
Context <i>Optimize built form with respect to mass and spacing of buildings, placement on site, response to adjacent uses, heights and densities</i> <ul style="list-style-type: none"> Massing relationship to context, distribution on site, and orientation to street edges Shade impact on public realm and adjacent sites 		
UDRP Commentary	The massing relationships to context is positive. The articulation of the massing and optimization of access to sunlight is strategic with main entrance activation.	
Applicant Response	<i>Site lighting and solar access will be pursued alongside accessibility and safety for residents in the drop-off zone. To address comments regarding shading of the main floor feature canopy, the design of the canopy will be reviewed further with an emphasis on light optimization, providing adequate shading in summer months.</i>	
Human Scale <i>Defines street edges, ensures height and mass respect context; pay attention to scale</i> <ul style="list-style-type: none"> Massing contribution to public realm at grade 		
UDRP Commentary	Architecturally the Applicant has effectively achieved an architectural outcome that is more permeable at grade.	
Applicant Response	<i>See note above regarding context. Aim is to provide strategic 'coverage' at grade to minimize the feeling of exposure by residents enjoying outdoor spaces at grade. This will be done with select feature canopies and localized planting to create comfortable amenity spaces appropriate to the human scale.</i>	
Integration <i>The conjunction of land-use, built form, landscaping and public realm design</i> <ul style="list-style-type: none"> Parking entrances and at-grade parking areas are concealed Weather protection at entrances and solar exposure for outdoor public areas Winter city response 		
UDRP Commentary	Parking access has been designed with pedestrian-first principle. Applicant is positioning main entrance public realm in area with most sun access. Winter city low angle sun will create a significant shaded public realm – Applicant proposing heated surface, so mitigate the predominance of icy surfaces. The applicant is encouraged to relook at the location and orientation of the structures over the parking entrance. They appear to be more central to the landscape plan that would be desirable and block views to and from the lobby areas. If they could be turned and placed parallel to the southern boundary it might improve the conditions.	
Applicant Response	<i>Restriction due to site access limits the ability to place the parking ramp in a different location, and rotation of the ramp would greatly remove any underground efficiencies for parkade. This in turn would require on-grade parking. Our team feels this would deter from the integrity of the project and public realm. Reduction of the overall mass of the parkade ramp structure will help create more visual connection of pedestrians to the internal site and main entry. Signage and landscape features will address pedestrian and vehicular access to the site.</i>	

UDRP Comments and Applicant Response

Connectivity Achieve visual and functional connections between buildings and places; ensure connection to existing and future networks. <ul style="list-style-type: none"> • Pedestrian first design, walkability, pathways through site • Connections to LRT stations, regional pathways and cycle paths • Pedestrian pathway materials extend across driveways and lanes 		
UDRP Commentary	<p>Pedestrian first design, walkability and pathways through the site will be further enhanced with discussion of shared-use public access along the London Tower's north walkway to elevator/stairwell.</p> <p>Connections to LRT needs to be direct, convenient, secure, safe for active seniors walking and cycling to west of Haysboro (see sketch notation in summary). The Panel feels that a direct access to the LRT from the "front" face of the building would be more desirable than having to proceed further along the path to the back of the project – hence the suggested location.</p> <p>Pedestrian pathway materials that extend across driveways need to be dimensionally and structurally vehicle-rated.</p>	
Applicant Response	<p><i>Challenges associated with providing a direct access to the LRT from the building "front" is previously outlined in the response summary. The proposed access towards the adjacent site elevator acts as an accessible connection to Macleod Trail and the LRT. Additional transportation for the Chartwell community bus, is provided by the Owner's program which also promotes a safer means of navigation for this demographic.</i></p>	
Animation Incorporate active uses; pay attention to details; add colour, wit and fun <ul style="list-style-type: none"> • Building form contributes to an active pedestrian realm • Residential units provided at-grade • Elevations are interesting and enhance the streetscape 		
UDRP Commentary	<p>Optimizing animation extended on different levels, maximizing the outdoor potential on the site should also connect to the neighbouring mixed use environment of the London Towers and to the existing bridge connecting beyond the project site to the LRT and the community within which it will reside.</p>	
Applicant Response	<p><i>Addressed above. – Residential units provided at grade does not provide the level of security required for this targeted demographic. Activated areas paralleling the street frontage such as amenity spaces, outdoor program and bistro/lounge areas, over the main and second floors promote a semi public interface with increased visibility over the changing pedestrian elevation.</i></p>	
Accessibility Ensure clear and simple access for all types of users <ul style="list-style-type: none"> • Barrier free design • Entry definition, legibility, and natural wayfinding 		
UDRP Commentary	<p>Barrier-free, clarity and legibility in approach and entry design in this application demonstrates natural wayfinding. The missing pieces, as noted in the summary, are the off-site connections at the north and south ends of the project site (and a direct link that requires no stairs to the LRT bridge.)</p>	
Applicant Response	<p><i>Addressed above. In addition, this proposed design aims to utilize the public elevator noted within the Panel discussion, provides additional modes of community transport and enlivens the street frontage without compromising the security of the residence.</i></p>	
Diversity Promote designs accommodating a broad range of users and uses <ul style="list-style-type: none"> • Retail street variety, at-grade areas, transparency into spaces • Corner treatments and project porosity 		
UDRP Commentary	<p>The diversity in the broad range of care options will draw a diversity of seniors and caregivers to the area – people walking, cycling, driving and for residents a diversity of lifestyle options that will activate inside and outside areas.</p>	
Applicant Response	<p><i>In developing this current site from its current use as a car sales lot, it will bring an increased resident presence to the area, with provision of increased amenity, services and care options to the wider Community for aging in place. This will also boost the population of local consumers to the adjacent commercial areas and improve the local area business.</i></p>	
Flexibility Develop planning and building concepts which allow adaptation to future uses, new technologies <ul style="list-style-type: none"> • Project approach relating to market and/or context changes 		

UDRP Comments and Applicant Response

UDRP Commentary	Urban retrofitting for Transit-Oriented-Development to achieve better multimodal public realm outcomes often dovetail with road dieting, rarely road widening. The context change of a road widening should emphasize generous tree-lined walkways, raised bike paths, and reduced speed driving lanes, to complement the aspirational merits of vibrant placemaking. The Applicant's proposed public plaza lends itself to flexibility in future uses and advancing technologies.	
Applicant Response	<i>Further discussions with the City regarding intended strategies in place for the existing road widening along Horton Rd will continue through the application process with City owned property. Continuous sidewalk access will be proposed in the public realm tying into existing infrastructure adjacent our property for the City's review and comment.</i>	
Safety Achieve a sense of comfort and create places that provide security at all times <ul style="list-style-type: none"> • Safety and security • Night time design 		
UDRP Commentary	Ensuring safe, freedom of movement is well stated in the application – the opportunities to safely access off-site connections at the north and south sides of the project site along with activating both east and west facades is fundamental to safe environments during daytime and during dark – as is, a careful positioning of the parking entry structures. The activation and resident eyes on the neighbourhood are important aspects of improving the livability and safety of the Transit-Oriented neighbourhood.	
Applicant Response	<i>The site will also be monitored by the Owner's security program, which provides additional presence and safety to the area. Increased site illumination utilizing CPTED principles, and out of hours activity programs also contribute to improving safety for the area, and surrounding neighbours. More physical presence with 'at-home' residents increases passive safety during daylight hours as well.</i>	
Orientation Provide clear and consistent directional clues for urban navigation <ul style="list-style-type: none"> • Enhance natural views and vistas 		
UDRP Commentary	Site context and creativity in the Applicant's submission narrates orientation in response to site constraints and opportunities. The vertical urban neighbourhood and its tiered outdoor spaces, and relief from the neighbor is generally positive, however, UDRP feels form, finish and overall articulation in a contemporary response should be applied to the south-facing-11-story-blank wall.	
Applicant Response	<i>Addressed above – The southern façade has also been advanced in our design development to articulate a more residential interface with Balconies and fenestration. This can be seen in the DP drawings.</i>	
Sustainability Be aware of lifecycle costs; incorporate sustainable practices and materials <ul style="list-style-type: none"> • Site/solar orientation and passive heating/cooling • Material selection and sustainable products 		
UDRP Commentary	Long term operation and sustainability aspiration expressed by the Applicant.	
Applicant Response	<i>This building will incorporate durable materials, high quality infrastructure systems and quality construction to ensure a sustainable operation for the life cycle of this building and to ensure that future changes in need can be accommodated within this design.</i>	
Durability Incorporate long-lasting materials and details that will provide a legacy rather than a liability <ul style="list-style-type: none"> • Use of low maintenance materials and/or sustainable products • Project detailed to avoid maintenance issues 		
UDRP Commentary	Robust, durable and long-lasting material details are generally expressed in the architectural response of the submission. Without changing architectural massing, consider colour and explore options to break up the white masses as well as tying together the lower and upper masses (positive precedent image examples shown in the Applicant's UDRP submission). Further design exploration should give consideration of the blank-south-facing-stepped-back-11-storey-wall facing the neighbouring London Tower, during day and dark.	
Applicant Response	<i>Addressed above.</i>	

Engagement Summary



RECORD OF COMMUNICATION from Haysboro Community Association engagement meeting.

From: Audrina Lim

Sent: March 13, 2019 5:10 PM

To: Kathy Kakish (kkakish@chartwell.com) <kkakish@chartwell.com>; Courtney Clark <Courtney.Clark@norr.com>; Sonja Sahlen <play@haysboro.org>; Haysboro President <president@haysboro.org>

Cc: Marco Gerolini (mgerolini@chartwell.com) <mgerolini@chartwell.com>; Craig Abercrombie (Craig.Abercrombie@norr.com) <Craig.Abercrombie@norr.com>; Carmen Kubrak (Carmen.Kubrak@norr.com) <Carmen.Kubrak@norr.com>

Subject: NCCA18-0065 Chartwell Horton - Haysboro community association introductions.

Hi All,

Thanks for joining us last night, it was such a positive and productive meeting with such great ideas and insight into the community.

For your records, and to loop into our team, please see my meeting notes below. Please let me know if I have missed anything or incorrectly recorded the notes.

Notes from Community meeting:

Sonja Sahlen / Kourtney Branagan – Haysboro Community

Kathy Kakish - Chartwell

Audrina Lim / Courtney Clark - NORR

1. Haysboro Community Association noted that the site does fall within their community, as the boundary goes all the way to McLeod Trail.
2. Noted that the train lines are quite the impediment to community connection – they are working on a pedestrian crossing initiative Reservoir to River pathway.
3. Neighbourhood partners also include Acadia Kingsland and Fairview, share community programs and connections, especially for Seniors Resources and School programs.
4. Security can be an issue in this area – bottle depot attracts migratory residents, that traverse around the train lines. Passive supervision of this area from increased seniors demographic is definitely supported!
5. Security is also one of the reasons why the facilities won't be made fully public. Due to the care environment, there is a need to manage the amenities and the access within. There is lots of site amenities and landscaping to be shared with the community.
6. Fencing is minimized on this site to create more visual connection and passive supervision, also enhanced with the programming of this building, creating more active frontage at pedestrian elevations.

NORR Architects Engineers Planners

A Partnership of Limited Companies
Poon McKenzie Architects (AB) Inc. Poon McKenzie Holdings Inc.

Victor Smith, Architect, AAA, AIBC, OAA
Bruce G. McKenzie, Architect, AAA, AIBC
A. Silvio Baldassarra, Architect, AAA, AIBC, OAA
Adrian Todella, P.Eng., APEGA
Chris Pal, P.Eng., APEGA

2300, 411 – 1st Street SE
Calgary, AB
Canada T2G 4Y5

T 403 264 4000
norr.com

Engagement Summary



RECORD OF COMMUNICATION from Neighbour engagement meeting.

From: Audrina Lim

Sent: April 26, 2019 1:26 PM

To: Kathy Kakish <kkakish@chartwell.com>; Craig Abercrombie <Craig.Abercrombie@norr.com>; Jonathan Milroy <jmilroy@westcorp.net>; Michelle Calloway <mcalloway@westcorp.net>; Ramandeep Gill <rgill@westcorp.net>; Keenan Chow <kchow@westcorp.net>; Marco Gerolini <mgerolini@chartwellreit.ca>; Tom Donovan <tdonovan@maple.ca>; Cara Lock <caral@maple.ca>; mgerolini@chartwellreit.ca; kkakish@chartwellreit.ca; jeffrey@jeffreyclassen.com; justincb77@gmail.com; Constance Davis <cdavis@westcorp.net>; Bruce McKenzie <Bruce.McKenzie@norr.com>

Subject: FW: NCCA18-0065 Chartwell Horton Rd - Summary from Westcorp meeting.

Hi All,

Again, thank you all for your time provided to our team to discuss our proposed development at 8610 Horton Rd SW.

Please see below for my notes from our call in. Please advise I have omitted anything or captured incorrectly.

Present at the meeting:

Jonathan Milroy – Westcorp / President of Condo Board.

jmilroy@westcorp.net

Justin Barrett – Condo Board Member and resident on Dominion Towers

justincb77@gmail.com

Kathy Kakish – Development Manager – Chartwell Retirement Residences

kkakish@chartwell.com

Cara Lock – Design Manager and Estimator - Maple Reinders – Construction Managers

caral@maple.ca

Audrina Lim – Project Manager – NORR Architects Engineers Planners

Audrina.lim@norr.com

Craig Abercrombie – Principal – NORR Architects Engineers Planners

Craig.Abercrombie@norr.com

Constance Davis – Westcorp – Property Manager

cdavis@westcorp.net

INTRODUCTIONS -

1. Quick summary of London Towers – North Tower is called Dominion, south tower is Abbey, 2 proposed additional towers are currently being evaluated – Pro-forma for the currently approved DP is being reviewed – no construction is planned in the immediate future.

NORR Architects Engineers Planners

A Partnership of Limited Companies
Poon McKenzie Architects (AB) Inc. Poon McKenzie Holdings Inc.

Victor Smith, Architect, AAA, AIBC, OAA
Bruce G. McKenzie, Architect, AAA, AIBC
A. Silvio Baldassarra, Architect, AAA, AIBC, OAA
Adrian Todella, P.Eng., APEGA
Chris Pal, P.Eng., APEGA

2300, 411 – 1st Street SE
Calgary, AB
Canada T2G 4Y5

T 403 264 4000
norr.com

Engagement Summary



2. All components of the London Towers and associated site form a Condominium board with bare-land title, including the commercial entities in the commercial block. 60% owned by AIMCO and 40% by Westcorp for commercial block.
3. 5 board members represent the Condominium.
4. Chartwell – Longterm Owner Operator for Retirement Residences, strong community involvement, high quality developments. Proposed site is first new build in Calgary, with 5 current properties in Calgary. Significant property ownership and operations across Canada.

Key items of note:

1. NORR presented the proposed site plan and perspective renders of the building – takes into account maintaining views for the Dominion Towers as much as possible, alleviating the built form away from the southern edge to promote access to views and daylighting, increased rooftop amenity and an interesting interface to the south to interact with the London Towers, Provision of contemporary site planning for improved views on the current site utilization.
2. 384 suites in total proposed, 320 of independent living, 64 over 2 care floors, the Memory living floor is secured and the Assisted Living floor has its own amenity but also has more access to other areas.
3. Notably this proposed demographic of residents will be mainly entertained in-house with significant amenities provided on site including – Casual and formal dining providing meal plans, a bar / bistro, swimming pool
4. Fence to the South – is it possible to remove this fence to enable a more open landscape interface between Chartwell proposed development and the north façade of the Dominion Towers.
5. Security concerns led to the mitigation of access to the public elevator – previously transients were living in the stair well and had attacked one of the residents. Now it is fobbed access only with exception to Level 4 parkade entrance. Consideration from the Westcorp / Condo board to provide shared fob access to residents of Chartwell seniors building to enable efficient access to the commercial centre to the south, to navigate the change in grade and avoid having to walk all the way around on Hull Ave on a steep grade.
6. Chartwell has a good reputation for Seniors development amongst the residents of London Towers and has some demographic moving into the retirement category, generating some interest in the project.
7. Overall, Westcorp and the Condo board sees the development as a good fit for the Haysboro community as it is an ageing community which promotes long term residency.
8. 17th floor on the Dominion towers has a rooftop amenity, however it is super windy and underutilized.
9. Concerns about increased traffic by neighbours is noted – NORR and Chartwell advised that the seniors demographic doesn't follow the same peak hour traffic patterns to the typical multifamily demographic. With the significant amenity, pedestrian proximity to the grocery and medical provisions in the London at Heritage commercial block, there is a significant reduction in traffic impact to the area. Whilst this is notably a Transit oriented development site, Chartwell has also significantly studied their resident demographic expertly and has specific statistics for traffic and vehicle ownership and use. A group bus service is provided to residents for outings, and shuttle to the grocery store and events, which also assists in alleviating traffic impacts.

Engagement Summary



10. CL discussed the proposed site management plan during construction with the early planning of a by-pass lane to minimize disruption or closure to Horton Rd Traffic flow, and to improve the security and safety of the construction site and the neighbours.
11. CL also advised that there will be proposed Shoring to the property line and that for due diligence, piling / foundation design drawings for the Dominion Tower would be extremely helpful. We will formally request these from Westcorp and will also require permissions to go past property line.
12. JM advised that there are some good lessons learned from their development on the soils in the area, NORR will be in touch to discuss further with Maple Reinders.

Thanks,
Audrina.

Audrina Lim, Ba.Env. Des, Ba.Arch

Project Manager

NORR Architects Engineers Planners

Tel: 403 538 3396 | Fax: 403 269 7215 | Cell: 403 681 2947 | Email: Audrina.Lim@norr.com | Web: norr.com
2300, 411 - 1st Street SE, Calgary, AB T2G 4Y5

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1129

Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE, LOC2019-0099

EXECUTIVE SUMMARY

This application was submitted by Inertia on 2019 June 21 on behalf of the landowner, Huafeng Zhang. The application proposes to change the land use designation of the subject site from Residential – Contextual One / Two Dwelling (R-C2) District to Multi-Residential – Contextual Grade-Oriented (M-CGd72) District to allow for:

- multi-residential development (e.g. primarily townhouses and fourplexes with some units with direct access to grade);
- a maximum building height of 12 metres (an increase from the current maximum 10 metres);
- a maximum of 4 dwelling units (an increase from the current maximum 2 units), based on a density modifier of 72 units per hectare; and
- the uses listed in the M-CG District.

The proposal aligns with the applicable policies of the *Municipal Development Plan*. A development permit application has not been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares ± (0.15 acres ±) located at 3827 Centre A Street NE (Plan 3674S, Block 18, Lot 31) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Multi-Residential – Contextual Grade Oriented (M-CGd72) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

This land use amendment application was submitted by Inertia on 2019 June 21 on behalf of the landowner, Huafeng Zhang. The Applicant's Submission (Attachment 1) indicates the landowners' intent to redesignate the property to allow for the development of a grade oriented multi-residential building in alignment with the rules of the M-CGd72 District. This land use amendment application will prepare the subject site for future development with four grade-oriented units within approximately 200 metres walking distance of the future 40 Avenue Green Line LRT Station.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1129

**Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE,
LOC2019-0099**

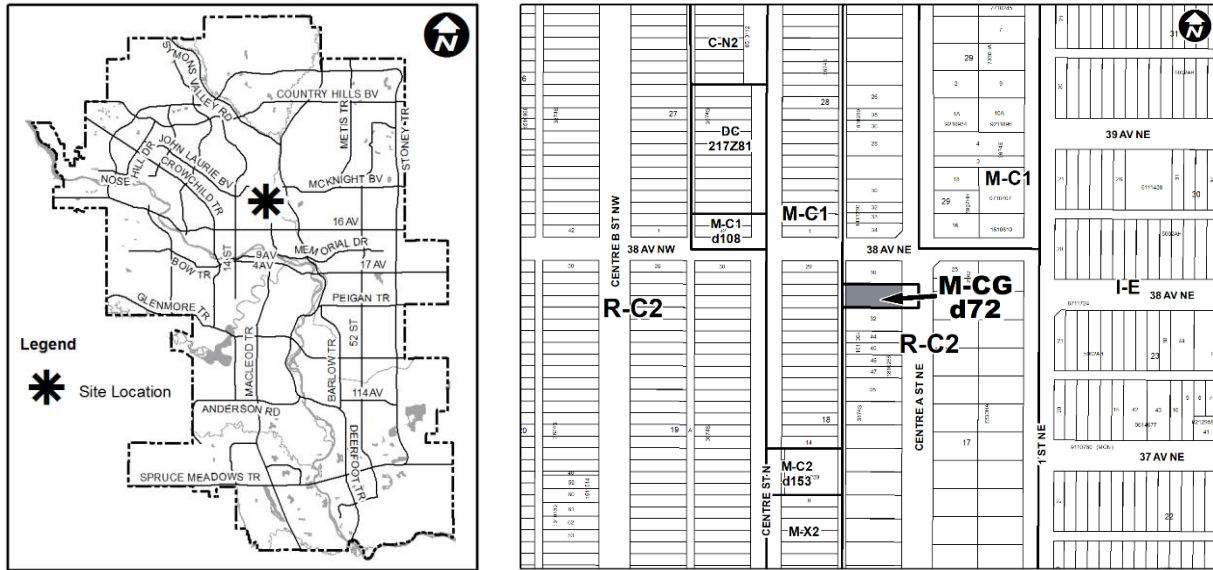
The subject site is developed with a two-unit residential building situated in Highland Park, which does not currently have a statutory approved local area plan. The community is currently part of the North Hill Communities Local Growth Planning Initiative as discussed under the Strategic Alignment section of this report.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1129

Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE,
LOC2019-0099

Location Maps



**Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE,
LOC2019-0099**

Site Context

The community of Highland Park is an inner-city residential community straddling Centre Street N and providing direct vehicular and transit access to downtown Calgary. The street block on Centre A Street N between 38 Avenue NE and 41 Avenue NE is earmarked for the location of the future 40 Avenue Green Line LRT Station. The subject site is situated approximately 200 metres to the south-east of the proposed station on Centre A Street NE.

The site is approximately 0.06 hectares (0.15 acres) large with street frontage of approximately 15 metres (50 feet), and a parcel depth of approximately 36 metres (118 feet), and has a steep gradient sloping down from the western property line with the rear lane to Centre A Street NE. The site is currently developed with a two-unit residential building with vehicular access from the rear lane to a parking pad.

Surrounding uses and development consist of the following:

- North and South – single and semi-detached dwellings with a Residential – Contextual One / Two Dwelling (R-C2) District;
- East – beyond the immediate single and semi-detached dwellings on Centre A Street NE lies Greenview Industrial Park; and
- West – two vacant properties under a Multi-Residential – Contextual Low Profile (M-C1) District across the rear lane with an elementary school across Centre Street N further to the west.

Highland Park is an established community with residential developments at various densities and stages of redevelopment. The community provides for the needs of its residents in terms of commercial uses, schools, place of worship and work opportunities in Greenview Industrial Park. The community is also well connected through the Primary Transit Network with the rest of the City. Highland Park is experiencing change and anticipated growth as a result of the future Green Line LRT, planned to be developed in the future along Centre Street N.

As identified in *Figure 1*, the community of Highland Park has seen a population decrease since its population peak in 1969.

Figure 1: Community Peak Population

Highland Park	
Peak Population Year	1969
Peak Population	4,875
Current Population (2018)	4,191
Difference in Population (Number)	-684
Difference in Population (Percent)	-14%

Source: *The City of Calgary 2018 Civic Census*

Additional demographic and socio-economic information may be obtained online through the

**Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE,
LOC2019-0099**

Highland Park community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This land use amendment proposal has the potential for the redevelopment of an underdeveloped site in proximity to a key transportation and transit corridor in the city. The subject site can contribute to transit-oriented development by providing multi-residential housing at medium residential densities within walking distance to a future LRT Station, existing and future commercial development and an existing elementary school.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

The existing Residential – Contextual One / Two Dwelling (R-C2) District allows for a low-density residential development with a maximum of two units with a building height of no more than 10 metres. This land use amendment application proposes the redesignation of the subject site to Multi-Residential – Contextual Grade Oriented (M-CGd72) District with a density modifier of 72 units per hectare, which would allow for up to 4 dwelling units on this site. The proposed M-CG District is intended to accommodate multi-residential developments in close proximity or adjacent to low density residential development.

With the proposed M-CG District, the subject site has the potential to develop with a four-plex multi-residential building at a maximum building height of 12 metres as proposed in the Applicant's Submission in Attachment 1. The current land use district along Centre Street N is Multi-Residential – Contextual Medium Profile (M-C2) District and the proposed M-CG District is an appropriate adjacent land use district; specifically, in context to the existing R-C2 land use district to the east across Centre A Street NE and the new semi-detached infill developments surrounding the subject site.

Development and Site Design

The applicant's intended four-unit residential development will be evaluated against the rules of the M-CGd72 District and will be reviewed by City Wide Urban Design to guide the built form in relation to height, massing, parking and landscaping. Other key factors that will be considered during the development permit application include the following:

- ensure higher residential densities to support existing bus ridership and the future LRT ridership at the proposed 40 Avenue Green Line LRT Station;
- ensure acceptable vehicular access from the rear lane to on-site parking;
- mitigate any potential impacts on the existing R-C2 properties to the north, south, and east of the subject site;

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1129

**Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE,
LOC2019-0099**

- provide for grade-oriented development with at least two front doors facing Centre A Street NE; and
- integrate sufficient amenity space in the site plan.

Environmental

There are no environmental concerns associated with the subject site or this proposal however, a Slope Stability report may be required by Administration at the development permit stage.

Transportation

A Transportation Impact Assessment was not required for this proposal. The subject site is situated in close proximity to the Primary Transit Network along Centre Street N, as well as 40 Avenue NE and 41 Avenue NE. The Primary Transit Network provides high transit connectivity along these north-south and east-west corridors.

Transit and bus rapid transit stops are located along Centre Street N with the closest stop within 150 metres of the subject site. Transit stops for the east-west bus line (Route 38) are situated along 40 Avenue NW and 41 Avenue NE, one block to the north of the subject site. Vehicular access to the subject site will be provided from the existing rear lane to on-site parking and will be reviewed at the development permit stage.

Green Line

The Corporate Planning Applications Group - Green Line Team reviewed the land use amendment application and is in general support of the proposal.

The subject site is within proximity to the future 40 Avenue Green Line Station, which has been identified as a future transit-oriented development (TOD) area. As such, future development and land use for this site should be transit supportive.

As discussed below under the Strategic Alignment section of this report, the *Municipal Development Plan* and the *Transit Oriented Development Policy Guidelines* support creating a transit-supportive land use framework within existing or future TOD areas.

Utilities and Servicing

Water and sanitary sewer are available to service the site, but storm sewer is not immediately available. The specific servicing arrangements and stormwater management will be reviewed and evaluated in detail as part of the development permit process.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1129

Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE, LOC2019-0099

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practises, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised on-line.

Administration circulated the application to the Highland Park Community Association. They provided a letter of support on the development proposal (Attachment 2).

Administration received two letters in opposition to the application. Identified concerns can be summarized as a shortage of on-street parking and too many units; two residential units may be acceptable, but four units are too much for the size of the site.

Following Calgary Planning Commission, notifications for the Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Planning Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSPR) which directs population growth in the region to the Cities and Towns and promotes efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed policy and land use amendment build on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The subject parcel is located within the Urban Main Street Land Use Typology identified on Map 1: Urban Structure in the *Municipal Development Plan* (MDP). Urban Main Streets are served by the Primary Transit Network and should provide for a broad mix of residential, employment and retail uses. The MDP policies of the Urban Main Street typology focus on ensuring development with a public realm that is of high quality, pedestrian-oriented and is of an intensity that will contribute to a vibrant environment.

The proposal is in keeping with relevant MDP policies as the rules of the M-CG District provide for a development form that is appropriate to the subject site and other surrounding land uses in terms of height, built form and density.

**Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE,
LOC2019-0099**

North Hill Communities Local Growth Planning Initiative

The subject site is situated in the community of Highland Park which does not currently have a statutory approved local area plan. Administration is currently undertaking the North Hill Communities Local Growth Planning initiative which includes the community of Highland Park and does not prohibit applications from being submitted. The local area plan is anticipated by Q1 2020.

Transit Oriented Development Policy Guidelines (Non-Statutory – 2004)

The *Transit Oriented Development Policy Guidelines* provide direction for the development of areas typically within 600 metres of a transit station. The subject site is located approximately 150 metres south-east of the future 40 Avenue Green Line LRT Station and is entirely within the transit-oriented development (TOD) area. The highest densities in station areas are generally located in close proximity to the station and there is an anticipated intensity threshold (within 400 metres of an LRT station) of a minimum of 100 jobs/population per hectare.

Location Criteria for Multi-Residential Infill (Non-Statutory – 2016)

The proposed land use amendment meets most of the location criteria for Multi-Residential Infill Development, such as:

- the subject site is situated within 250 metres of several transit stops;
- including transit stops on a primary transit route within 300 metres;
- the site will be adjacent to future non-residential uses situated on Centre Street N;
- the site is in close proximity to an existing corridor (Centre Street) and activity centre (40 Avenue NE); and
- the subject site gains vehicular access by means of a rear lane.

While these criteria are not used as a checklist, they do provide a framework in which Administration evaluates a parcel's appropriateness for intensification. These criteria together indicate that the subject site is an appropriate location for sensitive residential intensification.

Social, Environmental, Economic (External)

This proposal will allow for additional residential density in an established inner-city community in close proximity to a major transit corridor. The proposal will facilitate a more compact urban form that makes efficient use of land and existing infrastructure. The proposed land use district allows for a higher density housing type and as such, the proposed changes may better accommodate the housing needs of different age groups, lifestyles and demographics.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1129

**Land Use Amendment in Highland Park (Ward 4) at 3827 Centre A Street NE,
LOC2019-0099**

Current and Future Capital Budget

The proposed amendments do not trigger capital infrastructure investment and therefore there are no growth management concerns currently.

Risk Assessment

There are no significant risks associated with this proposal.

REASONS FOR RECOMMENDATIONS:

The proposal is in keeping with applicable policies of the *Municipal Development Plan*. The proposed M-CGd72 District is designed to be implemented in proximity to adjacent low-density residential development. The proposed land use district also supports the vision for future development intensification in proximity to Centre Street N and specifically for transit-oriented development near the future 40 Avenue Green Line LRT Station.

ATTACHMENTS

1. Applicant's Submission
2. Highland Park Community Association Letter

Date: June 21, 2019

Applicant's Submission

Site Address: 3827 Centre A ST NE
Land Use Redesignation from R-C2 to M-CG

Design Intent and Context:

The subject parcel is located in the community of Highland Park and consists of 0.055737ha. We are proposing a development with a four-unit multi-residential building with doors facing Centre A ST NE and vehicular access from the adjacent lane. Parking would be provided at grade in an enclosed garage.

The current land use designation is (R-C2) Residential - Contextual One/Two Dwelling which allows for single detached or semi-detached dwelling and a maximum height of 10m. To support the proposed development, we are applying to change the designation from R-C2 to M-CG Multi Residential - Contextual Grade Oriented District.

The Land Use Bylaw describes the M-CG district as follows:

The Multi-Residential - Contextual Grade Oriented District is intended to apply to the Developed Area; has Multi-Residential Development that will typically have higher numbers of Dwelling Units and traffic generation than low density residential dwellings; has Multi- Residential Development designed to provide some or all units with direct access to grade; provides for Multi-Residential Development in a variety of forms; has Multi-Residential Development of low height and low density; allows for varied building height and front setback areas in a manner that reflects the immediate context; is intended to be in close proximity or adjacent to low density residential development; provides outdoor space for social interaction; and provides landscaping to complement the design of the development and to help screen and buffer elements of the development that may have impacts on residents or nearby parcels.

The parcel is located mid-block and is situated between two low density Single-detached buildings. The properties to the West, North West and North East are all designated M-C1. While M-CG does allow slightly higher density than the current designation, the rules of the district ensure that development is compatible with low density residential development.

The proposed development will be designed so each unit will have separate and direct access to grade. The overall massing will be designed within the setbacks and maximum height rule of the Land Use Bylaw which respects the existing character of the block.

The site has specific attributes which make it ideal for an increase in intensity:

The site is only 100m to the nearest transit stop.

The site is only 5 minutes' walk to Buchanan School.

The site is only 10 minutes' walk to James Fowler High School.

The site is only 7 minutes' walk to Highland Park Community Centre.

Municipal Development Plan (MDP):

The proposed project fundamentally meets the goals of the Municipal Development Plan.

This City policy encourages more housing options in established communities, more efficient use of infrastructure, and more compact built forms in locations with direct easy access to transit, shopping, schools and other community services.

Date: June 21, 2019

Applicant's Submission

The MDP encourages modest redevelopment of the Established Area. (3.5.3 (a.)) The proposed development modestly intensifies the use of the land to a density more appropriate for a central Calgary community. The MDP requires that "ground and lower levels of developments should demonstrate a strong relationship to the human scale and contribute positively to the public realm and street." (2.4.2 (b.)).

By having all vehicular access from the adjacent lane, and unit entries facing Centre A ST NE, the pedestrian realm along Centre A ST NE is protected.

Bylaw (and Relaxations):

The proposed development is at a preliminary stage of design. At this point, relaxations are not anticipated. Should any be proposed in the future, these will be within the intent of City policy.

Engagement:

Given the small increment in density proposed, no pre-application meeting was scheduled prior to this land use redesignation application. The Highland Park Community Association will be consulted as this application and the Development Permit application progress.

Highland Park Community Association Letter



Highland Park Community Association
3716 2nd St. NW
Calgary, AB T2K 0Y4
Tel: (403)276-6969

October 22, 2019

Community Planning | Planning & Development
The City of Calgary

RE: LOC2019-0099 3827 Centre A Street NE

The Highland Park Community Association supports the application to change the land use on this parcel from R-C2 to M-CG. The surrounding area has been redeveloping over the past few years, and the scale of the proposed 4-plex will be approximately the same as any semi-detached or multi-family units nearby. This location is well within TOD for the future Green Line LRT station at 40th Avenue and Centre Street North. Moreover, the results of the North Hill Planning Group's Session 4 workshop indicate that this street is envisioned as multi-storey multi-residential.

The applicant's architect, Trent Letwiniuk, met with members of the Planning & Development Committee recently to show and discuss their concept for the site. We appreciate the courtesy shown to the Association, especially the fact that the applicant had delayed proceeding to the Calgary Planning Commission (CPC) with their application until after they had met with us. We understand that the units will have approximately 1240 sq. ft of living space with 3 bedrooms each. These units will fulfill a need for close-in family housing that is more affordable than the usual semi-detached infill development.

Several questions and concerns were raised by the Association, which were discussed at this meeting. We understand that these are more likely to be dealt with at the Development Permit stage, however, it may be advantageous to consider these at the earlier stage:

- Stormwater management – concerns were expressed by committee members regarding the potential issue of stormwater management on a parcel with significantly greater building lot coverage as well as concrete walkways. Mr. Letwiniuk explained that multi-residential structures of greater than 3 units require some form of onsite management, such as underground tanks to hold water from the downspouts, etc. In addition to onsite stormwater containment systems, we hope that the City will encourage developers of multi-residential buildings to consider utilizing other supplemental options available for managing stormwater. Examples include permeable pavers, non-concrete surfaces, planting more trees and the re-use of rainwater. Mr. Letwiniuk mentioned that a bylaw change enables a developer to plant trees are planted on city land and then the city will adopt them into their inventory. Although a landscaping plan was not provided, we encourage the developer and the city to plant trees back of the sidewalk on city owned land. This will also help maximize the usable amenity space provided for each unit if the requisite number of trees is planted out front on the city land.

Highland Park Community Association Letter

- Building position on the parcel – committee members noticed on the site plan that the rear units of the buildings have less amenity space than the street-facing units. Unit #3, in particular has amenity space that is only 2.45 m deep. The site plan shows the front setback as 3.46 m from the property line. The distance from the property line to the sidewalk is an even greater distance. We realize that the Land Use Bylaw sets limits on the front setbacks for M-CG properties. However, once the contextual front setback is known, we hope that it will be discovered that the building could be placed farther ahead a bit closer to the front property line. We believe this would help equalize the amount of amenity space provided to each unit. A secondary benefit would be to reduce the length of the stairway leading from the sidewalk to the front of the building. Some form of terracing might be considered that could help make the front amenity space more usable.
- The matter of 12 black, blue and green carts was also discussed at the meeting. We were informed that the garage units would be deep enough to store the carts when they are not out in the alley for pickup

We look forward to seeing the actual plans when they are submitted with the Development Permit application. If you have any questions, please do not hesitate to contact me at development@highlandparkcommunity.ca or on my cell at 403-390-7705.

Thank you.



D. Jeanne Kimber

On Behalf of the Highland Park Community Association

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1376

Land Use Amendment in Stoney 3 (Ward 5) at 3730 – 108 Avenue NE, LOC2019-0131

EXECUTIVE SUMMARY

This land use amendment application was submitted by CivicWorks Planning + Design on behalf of landowner 2060708 Alberta LTD (Eagle Crest Construction) on 2019 August 20. This application proposes to change the designation of the subject site from Industrial – Business f1.0 (I-B f1.0) District to a DC Direct Control District based on the Industrial – Business (I-B) District with the additional permitted use of General Industrial – Light and the additional discretionary use of Cannabis Facility to allow for:

- high-quality buildings in a campus-like setting (e.g business park);
- a maximum building height of 12 metres (the same as the current maximum of 12 metres);
- a maximum building floor area of 12,125 square metres (the same as the current maximum of 12,125 square metres), based on a building floor to parcel area ratio (FAR) of 1.0; and
- the uses listed in the Industrial – Business (I-B) District with the additional uses of Cannabis Facility and General Industrial – Light.

The proposed land use amendment to DC Direct Control District is supported by Administration as it aligns with the applicable policies of the *Northeast Industrial Area Structure Plan* and the *Municipal Development Plan*.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 1.21 hectares ± (2.99 acres ±) located at 3730 108 Avenue NE (Condominium Plan 1912139) from Industrial – Business f1.0 (I-B f1.0) District **to** DC Direct Control District to accommodate the additional uses of Cannabis Facility and General Industrial – Light, with guidelines (Attachment 2); and
2. Give three readings to the proposed Bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1376

Land Use Amendment in Stoney 3 (Ward 5) at 3730 - 108 Avenue NE, LOC2019-0131

BACKGROUND

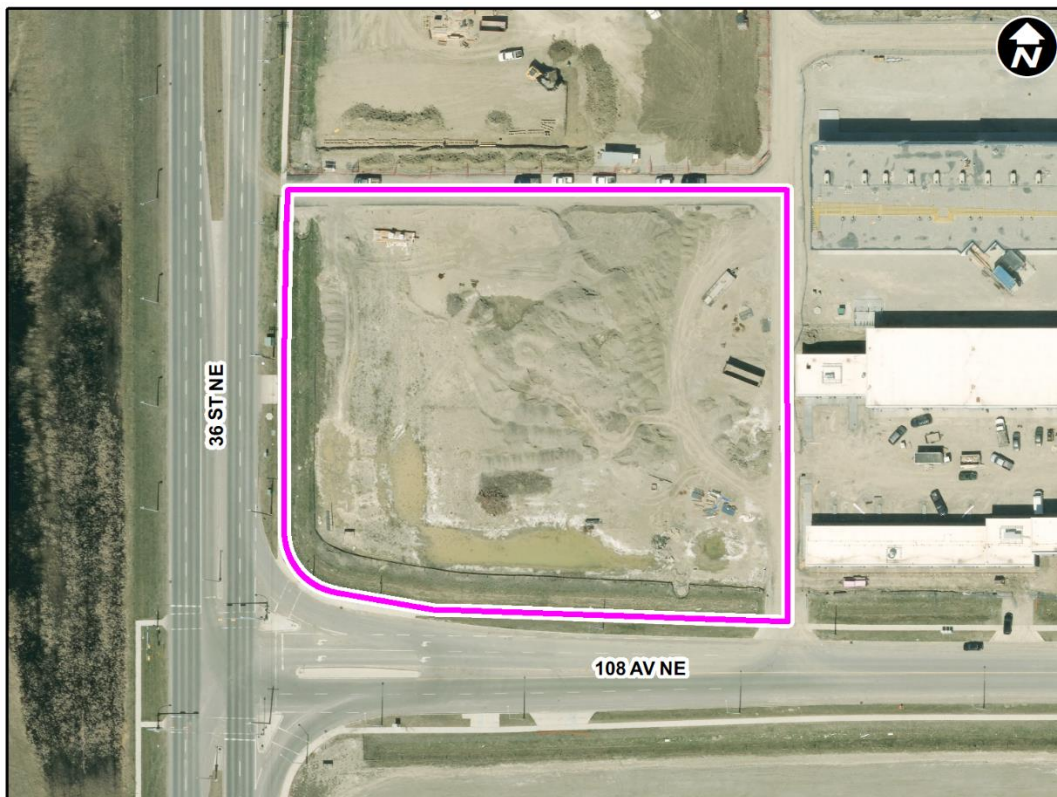
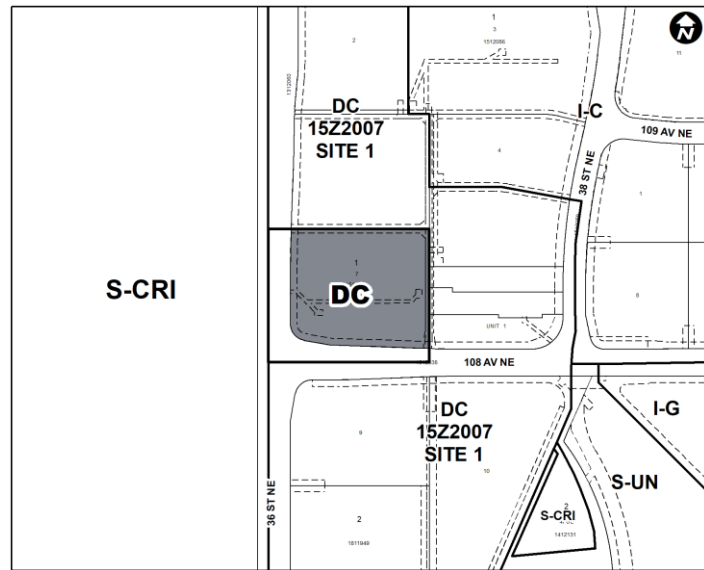
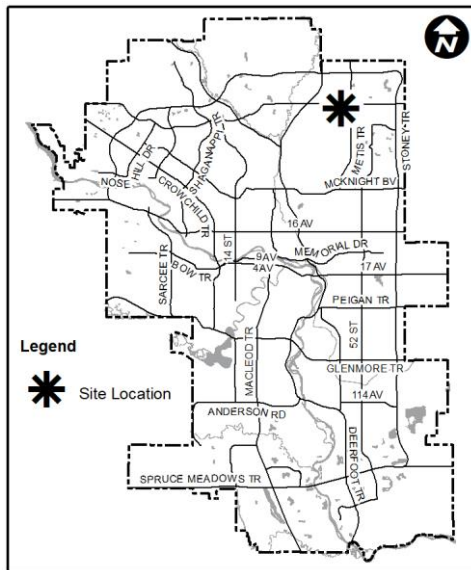
This land use amendment application was submitted by CivicWorks Planning + Design on behalf of landowner 2060708 Alberta LTD (Eagle Crest Construction) on 2019 August 20. No development permit application has been submitted at this time, although two buildings are currently approved under a previous Development Permit (DP2017-2566) and are under construction. The Applicant Submission notes that the applicant is looking to bring the approved non-conforming use of "Office and Warehouse" into conformity through the addition of General Industrial – Light to the suite of listed uses, and would also like to accommodate a new use, Cannabis Facility, as a discretionary use for a future tenant (Attachment 1).

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1376

Land Use Amendment in Stoney 3 (Ward 5) at 3730 - 108 Avenue NE, LOC2019-0131

Location Maps



Land Use Amendment in Stoney 3 (Ward 5) at 3730 - 108 Avenue NE, LOC2019-0131

Site Context

The subject site is located at the northeast corner of the intersection of 36 Street NE and 108 Avenue NE in the community of Stoney 3. It is directly east of the Calgary International Airport, and is approximately 250 metres south of Country Hills Boulevard NE. This site is approximately 120 metres by 100 metres and is 1.2 hectares (3 acres) in size. Two buildings are currently under construction and have been approved as office-warehouse spaces, but have not yet received occupancy.

The site is immediately surrounded by parcels designated as DC Direct Control District (Bylaw 15Z2007) with nearby parcels designated as Industrial – Commercial (I-C) District and Industrial – General (I-G) District. The DC District is based off the General Light Industrial (I-2) District in the 2P80 Land Use Bylaw and allows for a variety of commercial and light industrial, while providing additional development guidelines for outdoor storage along Metis Trail and next to the Calgary International Airport. Many of these parcels contain buildings that are also under construction for a variety of industrial and support commercial uses. The neighbouring I-C and I-G designated parcels are likewise developed with a mix of industrial and commercial uses, with many lots either undeveloped or under construction.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This proposed land use amendment would enable a greater range of small-scale industrial uses on the site, and also allow an approved non-conforming use to become conforming. The proposed redesignation does not contemplate changes to the height or intensity of development on the site. The proposed redesignation was reviewed against the policies of the *Northeast Industrial Area Structure Plan* and *Municipal Development Plan*. Consideration was given to the fact that development on the site is currently under construction.

Planning Considerations

As this application for a DC Direct Control District is proposing to keep the current base Industrial – Business (I-B) District and add two new uses to the suite of uses currently listed, planning considerations have been focused on how these align with the planning vision for the area and how they may interact with existing developments.

Land Use

The current land use, Industrial – Business f1.0 (I-B f1.0) District, was approved in 2019 April 16, as it aligns with the *Northeast Industrial Area Structure Plan*, allowing for a variety of industrial uses and support commercial uses. The maximum allowable height (12 metres) and the floor area ratio of 1.0 FAR match the rules within DC Direct Control District (Bylaw 15Z2007), which surrounds the subject parcel, and was the previous designation prior to 2019 April 16.

Land Use Amendment in Stoney 3 (Ward 5) at 3730 - 108 Avenue NE, LOC2019-0131

The proposed DC Direct Control District allows the current mix of commercial and business uses in the I-B District to remain and proposes to add two uses to the suite of listed uses, specifically General Industrial – Light and Cannabis Facility. Both uses are within the General Industrial Group in Schedule A of Land Use Bylaw 1P2007. Also, both uses are contained indoors, decreasing or eliminating impacts on neighbouring businesses and parcels. These uses are in alignment with the *Municipal Development Plan* and the *Northeast Industrial Area Structure Plan*, which is explored further in the Strategic Alignment Section.

On 2018 June 13, DP2017-2566 approved two buildings under the 2P80 Land Use Bylaw rules, and under the use “Warehouse and Office,” which would be called General Industrial – Light today under the current Land Use Bylaw 1P2007 uses. General Industrial – Light is a use that allows for a variety of businesses that are industrial in nature and typically used for businesses with an office portion in the front for staff and the public and a warehouse or production area in the back. Adding this use to the district allows the approved Warehouse and Office use to be conforming under the current General Industrial – Light use, and allows for the approved use to expand or be added to bays for future tenants.

Cannabis Facility is currently only a listed use in the General – Industrial (I-G) District, and is proposed as a discretionary use in this DC Direct Control District. This use allows for cannabis to be grown, processed, packaged, tested, destroyed, or stored, and would be carried out indoors. It is a requirement that the facility must have equipment designed and intended to remove odours from the air where it is discharged from the facility as per the rules in Land Use Bylaw 1P2007. A Cannabis Facility may also include cannabis retail sales as an ancillary use.

Development and Site Design

Development Permit DP2017-2566 was approved on 2018 June 13 and construction is ongoing. The approval consists of two buildings oriented towards a central parking area with rear loading doors for industrial bays located on the north building and accessed by a private road. The approval occurred under the 2P80 Land Use Bylaw use “Warehouse and Office,” which, if this application is approved, would be considered conforming under the proposed General Industrial – Light use. To approve the additional proposed use of Cannabis Facility in a specific bay for a tenant, a development permit and business licence would be required. The rules of the I-B District, which the proposed DC Direct Control District is based on, will provide basic guidance for further future site development including landscaping, parking requirements and access.

Environmental

No environmental issues have been identified through the consideration of this application. An environmental site assessment was not required for this application.

Land Use Amendment in Stoney 3 (Ward 5) at 3730 - 108 Avenue NE, LOC2019-0131

Transportation

Pedestrian and vehicle access to the site is provided off 36 Street NE and 108 Avenue NE. Vehicle accesses are shared with the adjacent north and east parcels through access right-of-ways. A Transportation Impact Assessment was not required for this application.

Utilities and Servicing

A public water main is located within the adjacent public right-of-way (108 Avenue NE) and a public sanitary main is within an existing utility right-of-way within the easterly portion of the site. A bioswale system, as constructed by the area developer, is within the outer south and west edges of the site for storm water drainage and treatment. Development servicing was resolved at both the development permit and development site servicing plan stages, to the satisfaction of Administration.

Stakeholder Engagement, Research and Communication

In keeping with Administration's practices, this application was circulated to external stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online. As Stoney 3 is an industrial and commercial area, no community association exists to comment on the application.

In response to the notifications, Administration received one letter of opposition with concerns over the potential odour of a cannabis facility operating near existing businesses. Rules within the Land Use Bylaw require Cannabis Facilities to include equipment designed to remove odours from the air where it is discharged. This requirement would be reviewed through a subsequent development permit application to accommodate a Cannabis Facility on the site. As such, Administration is comfortable including this use within the suite of uses.

Following Calgary Planning Commission, notifications for the Public Hearing of Council will be posted on-site and mailed out to adjacent land owners. In addition, Commission's recommendation, and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan (SSRP)* which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1376

Land Use Amendment in Stoney 3 (Ward 5) at 3730 - 108 Avenue NE, LOC2019-0131

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

Map 1: Urban Structure in the *Municipal Development Plan* identifies the subject parcel as being within the Standard Industrial Area Typology. This typology is intended to contain a mix of industrial uses at varying intensities while allowing for support commercial uses. This application is seeking to introduce two industrial uses back into the suite of uses available for the site, aligning with the intent of the typology and respecting the character of the neighbouring sites.

Northeast Industrial Area Structure Plan (Statutory – 2007)

The *Northeast Industrial Area Structure Plan* identifies the subject parcel as being within the Business Industrial Area. The plan notes that light industrial uses should be the predominant use of land, with various commercial uses being incorporated when appropriate. The proposed redesignation reintroduces the General Industrial – Light use back into the list of allowable uses on the site, and adds a new use Cannabis Facility, which is also an industrial use. The addition of these uses strengthens the industrial nature of the site, while the base Industrial – Business (I-B) District allows for uses that are more commercial in nature and support the industrial businesses.

Calgary International Airport Vicinity Protection Area Regulation (Statutory – 2009)

The subject site is located within the 35-40 Noise Exposure Forecast contour of the *Airport Vicinity Protection Area (AVPA)* land use regulations. The proposed General Industrial – Light and Cannabis Facility uses are generally allowable within the 35-40 NEF contour area. Future development permits would be circulated to the Airport and reviewed against the applicable regulations to ensure alignment and compliance.

Social, Environmental, Economic (External)

The proposed land use will allow for a wider range of industrial uses in addition to the industrial and commercial uses already allowed. This will help support local employment in the area and will improve the ongoing viability of the development as the owner will be able to attract a wider variety of tenants now and in the future.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1376

Land Use Amendment in Stoney 3 (Ward 5) at 3730 - 108 Avenue NE, LOC2019-0131

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is in keeping with the applicable policies of the *Municipal Development Plan* and other applicable policies. The addition of these industrial uses strengthens the industrial base for the area and provides opportunities for a variety of businesses to operate in close proximity to one another with minimal impacts. As these uses are confined to the interior building uses, and do not have external operations, there would be few impacts to existing businesses in the area if these are added to the mix of uses already approved and under construction.

ATTACHMENT(S)

1. Applicant Submission
2. Proposed DC Direct Control District

Applicant Submission



460 - 5119 Elbow Drive SW P 403 201 5305
Calgary, Alberta T2V 1H2 F 403 201 5344

19.08.2019

—

Planning & Development
The City of Calgary
PO Box 2100, Station M
800 Macleod Trail SE
Calgary, Alberta T2P 2M5

APPLICANT STATEMENT

Land Use Redesignation **from** Industrial - Business (I-B) District **to** Direct Control (DC) District
Previously Approved under LOC2018-0247 and DP2017-2566
3730 108 Avenue NE | Lot 7 Block 1, Plan 1512086 | 1.21 ha (2.99 ac)

INTRODUCTION

The subject site is located in the northeast industrial community of Stoney 3 and consists of a 3.0 ac (1.214 ha) parcel of land privately owned by 2060708 Alberta Ltd. (Eagle Crest). Under LOC2018-0247 and with file manager Peter Schryvers, the property was recently redesignated from an outdated Direct Control (DC) District 15Z2007 to Industrial - Business (I-B) District in order to reflect current development trends and to accommodate a number of interested local businesses wanting to locate at the site. The development which is well underway, includes two (2) buildings - Building 1, a two storey general industrial light / office building and Building 2, a two storey office building.

LAND USE CHANGE - PROPOSED DIRECT CONTROL DISTRICT

Eagle Crest has retained CivicWorks to undertake a new land use change process to redesignate the subject lands from Industrial - Business (I-B) District to a Direct Control (DC) District that allows craft cannabis facilities within the mix of already allowed light industrial, office and support commercial uses on the subject site. As a housekeeping item, the application also seeks to resolve the non-conforming General Industrial - Light use approved in Development Permit DP2017-2566 by including the said use within the District. The proposed DC District is based off of the current I-B District with the addition of 'Cannabis Facility' as a discretionary use and 'General Industrial - Light' as a permitted use. The proposed district will be in keeping with the current FAR of 1.0.

CANNABIS FACILITY

Cannabis Facilities are a relatively new and interesting use within the City of Calgary. Such businesses are evolving quickly, with growing interest in smaller-scale niche cannabis production. Cannabis Facilities are only permitted within the Industrial - General (I-G) District characterized by light and medium general industrial uses. Creating a DC District based on the I-B District allows the proposed craft Cannabis Facilities to co-locate within an area of appropriate and similar-scaled mix of light industrial uses, promoting business diversification and improving market absorption.

There are two different prospective tenants that have expressed interest in operating two individual Cannabis Facilities on the site within Building 1. A change of use Development Permit application will be submitted to demonstrate the project intent. At the Development Permit stage a Public Utility and Waste Management Plan will be required to manage nuisance factors such as odours.

Applicant Submission



460 - 5119 Elbow Drive SW P 403 201 5305
Calgary, Alberta T2V 1H2 F 403 201 5344

POLICY ALIGNMENT

The proposed change of use is consistent with the Northeast Industrial Area Structure Plan (ASP) and with the city-wide goals and policies of the Municipal Development Plan, which encourage the retention of a broad range of industrial uses. It should be noted that the development vision is also in alignment with the Airport Vicinity Protection Area (AVPA) policies which limit certain uses at this location.

CONCLUSION

The proposed Land Use Redesignation facilitates a development vision that enables a fresh and innovative use within the City of Calgary. For the reasons outlined above, we respectfully request that Administration support this application.

Should you have any questions, comments or concerns, please contact me at 403 201 5305 or darlene@civicworks.ca.

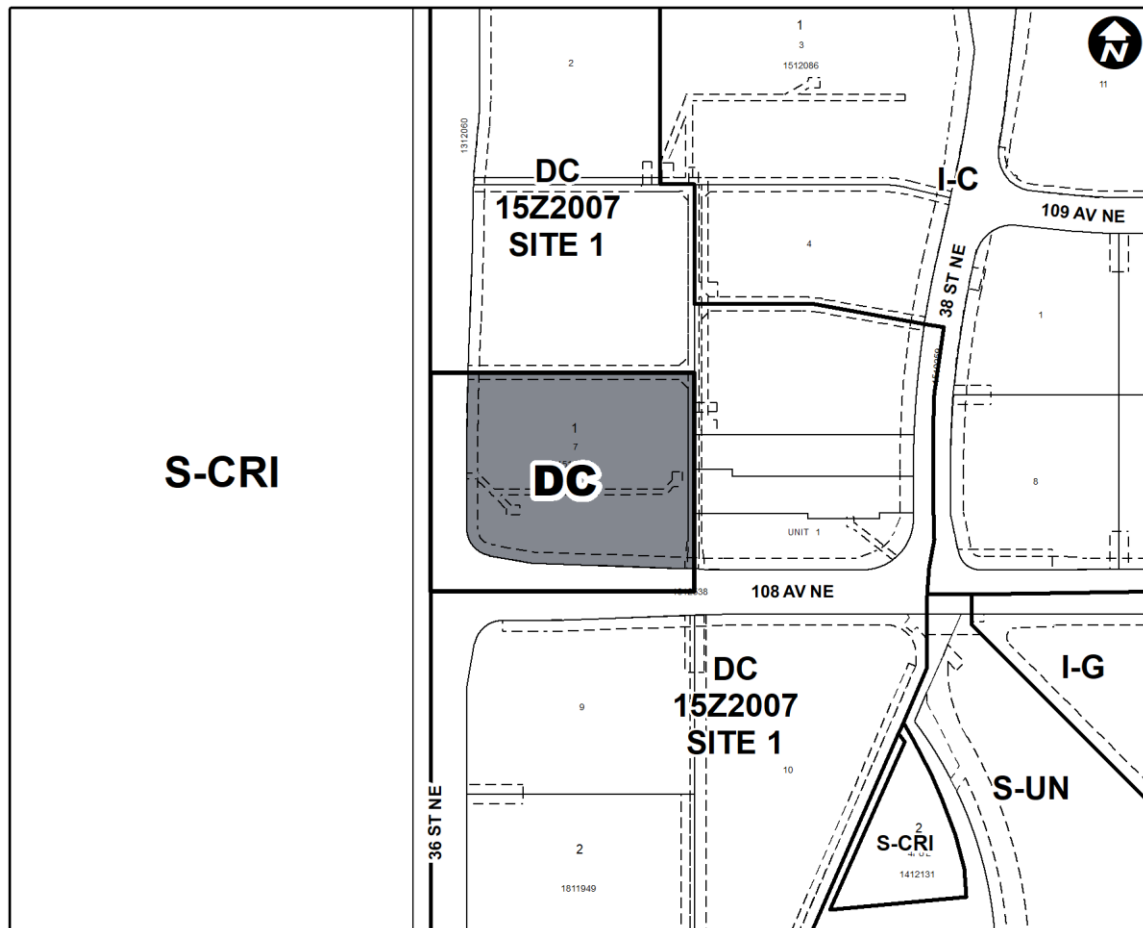
Sincerely,

Darlene Paranaque | Urban Planner
BES, MSc, RPP, MCIP, PMP

Proposed DC Direct Control District

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by amending that portion of the Land Use District Map shown as shaded on Schedule “A” to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule “A”.

SCHEDULE A



DIRECT CONTROL DISTRICT

Purpose

- 1 This Direct Control District Bylaw is intended to allow for the additional uses of cannabis facility and general industrial – light.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District Bylaw.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District Bylaw, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Proposed DC Direct Control District

Permitted Uses

- 4 (1) The **permitted uses** of the Industrial – Business (I-B) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.
- (2) The following **use** is an additional **permitted use** in this Direct Control District if it is located within existing approved **buildings**:
- (a) **General Industrial – Light.**

Discretionary Uses

- 5 (1) The **discretionary uses** of the Industrial – Business (I-B) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District with the addition of:
- (a) **Cannabis Facility.**
- (2) The **use** listed in subsection 4(2) is an additional **discretionary use** in this Direct Control District if it is located in proposed **buildings** or proposed additions to existing **buildings**.

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Industrial – Business (I-B) District of Bylaw 1P2007 apply in this Direct Control District.

Floor Area Ratio

- 7 The maximum **floor area ratio** is 1.0.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1436

Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE, LOC2019-0105

EXECUTIVE SUMMARY

This application was submitted by Zeidler Architecture on 2019 July 03 on behalf of landowner The Western Canadian District of The Christian and Missionary Alliance. The application proposes to redesignate an eastern 0.5 hectare (1.23 acre) portion of 6927 Rundlehorn Drive NE from Special Purpose – Community Institution (S-CI) District to Commercial – Neighbourhood 2 (C-N2) District to allow for:

- small-scale commercial developments with buildings that are in keeping with the scale of nearby residential areas;
- a maximum building height of 10 metres (a decrease from the current maximum of 12 metres);
- a maximum building floor area of approximately 5,000 square metres, based on a parcel to floor area ratio (FAR) of 1.0; and
- the uses listed in the C-N2 District.

The proposal is in conformance with applicable policies of the *Municipal Development Plan*.

No development permit application has been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed land use redesignation of 0.50 hectares \pm (1.23 acres \pm) located at 6927 Rundlehorn Drive NE (Portion of Plan 1280AJ, Block 1B) from Special Purpose – Community Institution (S-CI) District **to** Commercial – Neighbourhood 2 (C-N2) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

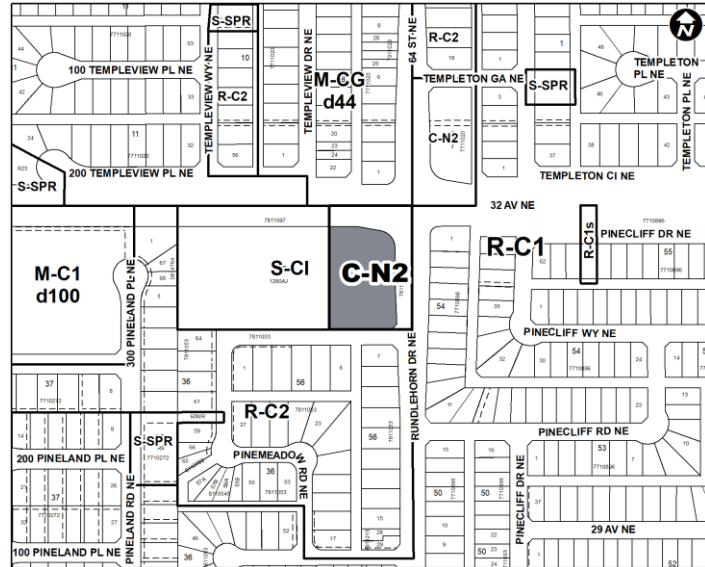
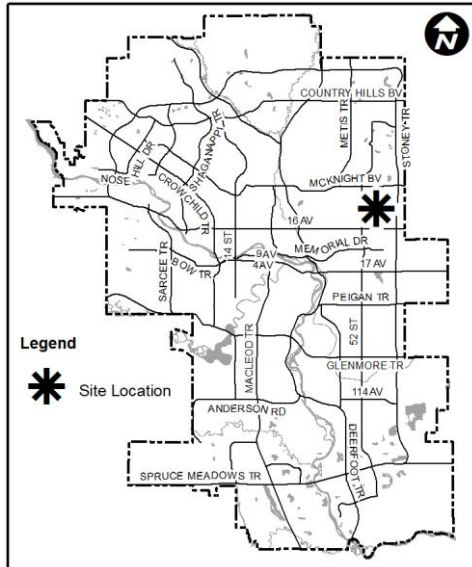
This application was submitted by Zeidler Architecture on 2019 July 03 on behalf of landowner The Western Canadian District of The Christian and Missionary Alliance. No development permit application has been submitted at this time; however, as noted in the Applicant Submission (Attachment 1), the applicant intends to subdivide the subject portion of the parcel and develop it with a small-scale commercial building (see Attachment 2 for the conceptual site plan).

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1436

Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE,
LOC2019-0105

Location Maps



**Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE,
LOC2019-0105**

Site Context

The subject parcel is located in the northeast community of Pineridge on the southwest side of the intersection of 32 Avenue NE and Rundlehorn Drive NE. This approximate 1.67 hectare (4.1 acre) parcel is currently developed with a place of worship in the middle and a single detached dwelling toward the southeast corner.

The eastern portion of the parcel, subject to this land use redesignation application, is approximately 0.5 hectares (1.23 acres) in size with dimensions of approximately 89 metres by 50 metres. This portion contains the single detached dwelling and surface parking for the place of worship. The western portion of this parcel is currently vacant. Although not included in this application, the applicant wishes to develop the western portion of the parcel with residential uses in future.

The parcel currently has driveway access from Rundlehorn Drive NE to the east, two driveway accesses from Pinemeadow Road NE to the south and a pedestrian-only access from 32 Avenue NE to the north. The parcel is easily accessible by foot, bicycle, transit or vehicle. The nearest transit stops are located approximately 350 metres south at 28 Avenue NE and Rundlehorn Drive NE (5-minute walk).

Surrounding development consists of low-density dwellings with the exception of the northeast corner of the intersection of 32 Avenue NE and Rundlehorn Drive NE which is also developed with a small-scale commercial building and has the C-N2 District. Other C-N2-designated parcels are available within 500 metres in both west and east directions along 32 Avenue NE; however, no recent commercial redevelopment is evident in the nearby area.

As identified in *Figure 1*, the community of Pineridge reached its peak population in 1991 and since then it has lost population.

Figure 1: Community Peak Population

Pineridge	
Peak Population Year	1991
Peak Population	10,501
2018 Current Population	9,822
Difference in Population (Number)	-679
Difference in Population (Percent)	-6%

Source: *The City of Calgary 2018 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Pineridge](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for small-scale commercial buildings that are in keeping with the scale of nearby residential areas. As the Pineridge community does not have a local area plan, this

Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE, LOC2019-0105

development is primarily guided by the policies within the *Municipal Development Plan*. The proposal complies with the applicable policies as discussed in the Strategic Alignment section of this report.

At the pre-application and formal application review stages, Administration recommended Commercial – Neighbourhood 1 (C-N1) District or a DC Direct Control District based on C-N1 District to ensure a building and site design that provides street-oriented buildings with high quality interface with the public realm. The C-N1 District has a maximum front setback requirement of three metres which encourages buildings to be located closer to the property line and discourages the provision of parking in the front. However, the applicant decided to continue with C-N2 as it allows them to have more flexibility with building and site design at the development permit stage. While the alternative designations would have been more appropriate for this site, similar development outcomes may also be achieved with the C-N2 District. Administration's concerns associated with the proposed concept plan are discussed in the Development and Site Design section in this report.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

The current S-CI District is a special purpose designation to provide for large scale culture, worship, education, health and treatment facilities. This District allows for a wide variety of building forms which are sensitive to context when located within residential areas. At this location, the S-CI District allows for a maximum building height of 12 metres. The S-CI District does not have any maximum floor area ratio (FAR) requirements.

The proposed C-N2 District is a commercial designation. The C-N2 District provides for small-scale commercial developments with buildings that are in keeping with the scale of nearby residential areas. The C-N2 District also allows for dwelling units. The proposed district would allow for approximately 5,000 square metres of building floor area (based on a maximum FAR of 1.0) to be developed under 10-metre height limit. The C-N2 District provides for setbacks and landscaping that buffer residential districts from commercial developments. The C-N2 District has a minimum front setback requirement of three metres and allows motor vehicle access to commercial uses. Typically, this results in a one to two-storey commercial building with parking provided in front of buildings.

For comparison purposes, the purpose of the C-N1 District is broadly similar to C-N2 District for providing small-scale commercial developments except that C-N1 District limits direct motor vehicle access to commercial uses. The C-N1 District is very similar in its list of allowed uses as C-N2 with the exception of vehicle-oriented uses. The C-N1 District brings the commercial building closer to the sidewalk as it provides for a maximum front setback requirement of three metres. The C-N1 District envisions lanes for motor vehicle access to parking areas.

**Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE,
LOC2019-0105**

Development and Site Design

The applicable land use policies, the rules of the proposed C-N2 District, and feedback from the City-Wide Urban Design team will provide guidance for future site development including building height, massing, landscaping, parking and the building's relationship with adjacent streets.

The applicant has provided a concept plan showing potential site development (Attachment 2). The concept plan shows a conventional strip mall with two rows of parking in front of the building and highlights the relocation of the existing parking stalls located on the subject portion of the site to the west of the existing place of worship building. At the development permit stage, it will be important to ensure that the place of worship has adequate parking available for their continued use. Also, noteworthy – is that the subject parcel does not have a lane which puts certain constraints to the future site design.

Administration has concerns about this conceptual site design. Notably, the setting of the subject site at the corner of 32 Avenue NE and Rundlehorn Drive NE offers an opportunity for strengthening an already existing commercial/retail node with a street-oriented building with high quality interface with the pedestrian realm. Administration has reviewed this conceptual design and determined that it does not demonstrate a high quality interface with the pedestrian realm. This direction is supported by the *Municipal Development Plan* urban design objectives in established neighborhoods which envision developments to “demonstrate a strong relationship to human scale and contribute positively to the public realm and street.” Site design will continue to be reviewed at the development permit stage to potentially bring the commercial building closer to the sidewalk by moving the parking behind the building to provide for a pedestrian-friendly interface with the street and this corner.

While the proposed C-N2 District provides the necessary land use framework to redevelop this site with small-scale commercial uses to serve the nearby residential area, additional items to be considered through the development permit process include, but are not limited to:

- ensuring building and site design that prioritizes pedestrians and provides for high quality interface with the streets;
- ensuring building and site design that addresses the unique setting of this corner site;
- explore if parking could be shared with the existing place of worship to achieve efficiency;
- considering relaxations to required parking for shared parking scenarios and finding ways to integrate this site with the place of worship; and
- require 10 percent Municipal Reserve (MR) dedication at the time of subdivision and explore options of cash-in-lieu payment.

A new commercial building on this parcel will require a discretionary use development permit. Site design, building placement and orientation will be negotiated with the applicant at the development permit stage relying on the *Municipal Development Plan* Urban Design objectives.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1436

Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE, LOC2019-0105

Environmental

An Environmental Site Assessment was not required. There are no environmental concerns associated with the site or this proposal.

Transportation

Vehicular access to the site will be determined at the development permit stage, but will be generally available from Rundlehorn Drive NE and Pinemeadow Road NE. Direct vehicular access to or from 32 Avenue NE is prohibited. Pedestrian access is available from 32 Avenue NE, Pinemeadow Road NE, and Rundlehorn Drive NE. Regarding transit service, Route 48 (Rundle) and Route 34 (Pineridge) have stops adjacent to the intersection of 28 Avenue NE and Rundlehorn Drive NE, approximately 350 metres south from the site (5-minute walk).

At present time, there are no sidewalks along the north boulevard of Pinemeadow Road NE. At the development permit stage, the developer will be required to address this deficiency.

A Transportation Impact Assessment (TIA) was reviewed as part of this application. The TIA findings have been accepted by Administration.

Utilities and Servicing

Water, sanitary and storm sewer mains are available and can accommodate the proposed land use redesignation without the need for network upgrades at this time. A sanitary servicing study was submitted for review in support of the proposed land use and deemed acceptable. Specific details of site servicing and stormwater management will be reviewed in detail at the development permit stage.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online.

Administration received two public responses in opposition to this application. The concerns focused on the following areas:

- no need for additional commercial establishments in the area; and
- potential to increase traffic.

The applicant-led engagement included two meetings with Pineridge Community Association representatives. A letter was received from the Pineridge Community Association in support of the application (Attachment 3).

Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, Commission's

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1436

**Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE,
LOC2019-0105**

recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy directions of the *South Saskatchewan Regional Plan* which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2019)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The proposed land use conforms to multiple policies in the *Municipal Development Plan* (MDP). The subject parcel is located within Residential – Developed – Established designation as identified on Map 1: Urban Structure in the MDP.

Section 3.5.1 outlines Land Use Policies for Developed Residential Area and encourages “moderate intensification in a form and nature that respects the scale and character of the neighborhood” and “revitalization of local communities by ... adding a mix of commercial and service uses.” Section 3.5.3 outlines Land Use Policies for Established Area, within Developed Residential Area. It encourages “modest redevelopment of Established Areas.” Section 2.3 Creating great communities provides a City-wide key direction for providing “more choice within complete communities.”

The proposed C-N2 District complies with these policies as it allows for small-scale commercial development that would serve the nearby residential area. This corner site is an appropriate location for infill neighborhood-oriented commercial uses because it is currently developed with a parking lot and has other similar commercial uses across the intersection.

There is no local area plan for the subject area.

Social, Environmental, Economic (External)

The recommended land use allows for additional neighborhood-oriented commercial uses and services for the nearby residential area.

Financial Capacity

Current and Future Operating Budget

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1436

**Land Use Amendment in Pineridge (Ward 10) at 6927 Rundlehorn Drive NE,
LOC2019-0105**

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

While a small-scale commercial development has potential to activate the street and to strengthen the developing commercial node, the proposed C-N2 District has a minimum front setback requirement of three metres and allows “motor vehicle access to commercial uses.” The C-N2 District poses the risk of the parcel being developed with a commercial building that is not oriented to the street and has a parking lot in the front – which may not provide a pedestrian-friendly interface with the public realm. However, as identified in the Development and Site Design section of this report, Administration has notified the applicant about the *Municipal Development Plan* urban design objectives and will continue to work toward improvement of the site design through the development permit stages.

REASON(S) FOR RECOMMENDATION(S):

The proposal conforms to the applicable policies of the *Municipal Development Plan*. The Commercial – Neighbourhood 2 (C-N2) District is structured to allow for small-scale commercial developments with buildings that are in keeping with the scale of nearby residential areas. The proposed district will allow for moderate intensification and add a mix of additional neighborhood-oriented commercial uses in an established community where commercial uses are already existing.

ATTACHMENT(S)

1. Applicant Submission
2. Concept Site Plan
3. Community Association Letter

Applicant Submission

July 3, 2019

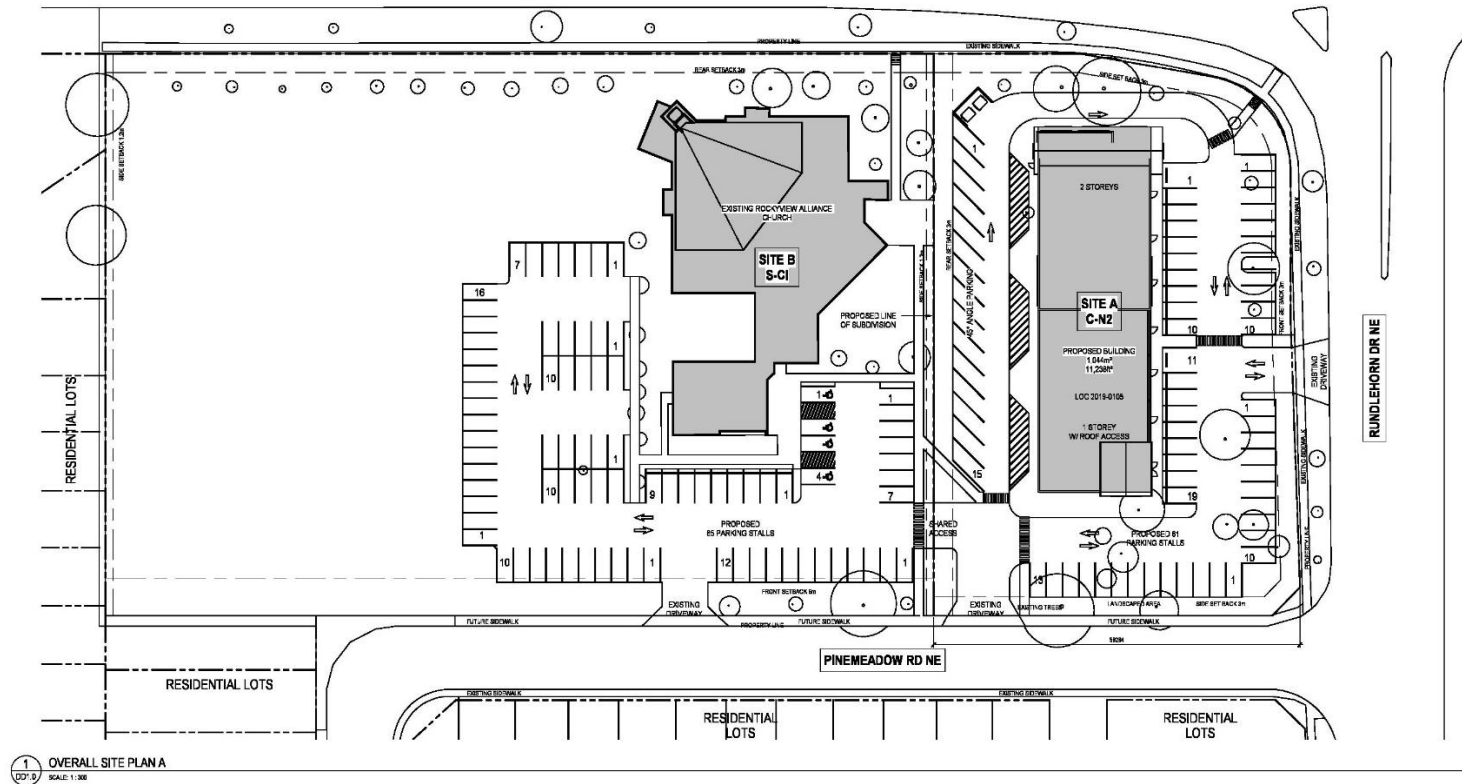
This proposed Land Use Re-designation has been motivated by the appearance of greater demand for services and the space required to deliver them in the neighbourhoods of Pineridge and Temple. This application is supported by the following considerations:

- .1 All of Calgary's neighbourhoods are expressing interest in improved walkability which must include increased proximity to commercial and community services. This is even more true in the neighbourhoods experiencing and aging of their residents for whom walkability and proximity are even more important.
- .2 Commercial and community service nodes activate neighbourhoods and contribute to their vibrancy. This is not something limited to so-called inner-city neighbourhoods proximate to the downtown core but to all neighbourhoods.
- .3 Densification should be a city-wide initiative that speaks not just to residential density but also increased proximity to community and commercial services.
- .4 32nd Ave. NE constitutes a barrier to pedestrian movement in the north/south direction. In the long term we can imagine increased traffic, and perhaps public transit where none currently exists, which will only strengthen this status. On the positive side an expansion of transit service on this artery will serve to increase the need for commercial nodes which can become transit stop opportunities. This site would capture both N/S and E/W increases in traffic.
- .5 By word of mouth alone we have been made aware of demand for medical clinic space and child care space in the Pineridge/Temple neighbourhoods as the aging original homeowners are replaced with young families. Opportunities for space to deliver these services in a walkable environment are very limited to non-existent. This site offers just such an opportunity.
- .6 Existing commercial sites in the neighbourhood are operating at 100% capacity and have been for a long time. They are small sites with little to no room for expansion thus opening up the opportunities for initiatives such as the one before you to fill the demand gaps.
- .7 The Community Association is supportive of this initiative and, in fact, their input contributed and gave credence to the reasoning presented above.

Concept Site Plan



300, 640 - 8 Avenue S.W.
Calgary, AB T2P 1G7
T +1 403 233 2525



DD1.0

SITE PLAN

RUNDELHORN LAND USE REDESIGNATION - LOC 2019-0105 6927 RUNDELHORN DRIVE NE

scale: 1:300
project no: 215-006
date: 09-09-2019

Community Association Letter



Pineridge Community Association

May 13, 2019

Zeidler Architecture
Calgary, Alberta

Attention: Martin Veenhoven

RE: Proposed Development on Rockyview Alliance Church Property
32nd Avenue/Rundlehorn Drive NE, Calgary, AB

Good day, Martin:

Based on our conversation and a subsequent meeting of our Board of Directors in April, we are in favour of the exploration by your company for the re-zoning to add retail and multi-family residential to the Rockyview Alliance Church property located on the southwest corner of 32nd Avenue and Rundlehorn Drive NE.

We believe that both of these draft proposals will have a positive impact on the Pineridge community and we look forward to further opportunities to discuss once more details are known.

Let us know if there is anything further you need.

Kind regards,

Marjorie Roy
President

cc Councillor Ray Jones, Ward 10

6024 Rundlehorn Drive NE, Calgary, Alberta T1Y 2X1 Telephone: 403-280-2774
Email: info@pineridgecommunity.ca Website: www.pineridgecommunity.ca

**Planning & Development Report to
Calgary Planning Commission
2019 November 21**

**ISC: UNRESTRICTED
CPC2019-1375**

**Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249**

EXECUTIVE SUMMARY

This application was submitted by B&A Planning Group on behalf of Mattamy (Northpoint) Limited and the City of Calgary, the landowners, on 2018 November 15. This application proposes to close a portion of Cityscape Drive NE, and to redesignate approximately 71.83 hectares (177.50 acres) of land in the Northeast community of Cityscape. The land use amendment seeks to redesignate a range of land use districts that were previously designated under LOC2006-0156 (CPC2007-093). Specifically, the application provides for:

- a number of land uses to accommodate a low density residential development with an open space network that provides for a logical extension and completes the Cityscape neighbourhood;
- approximately 36.3 hectares (89.70 acres) for an anticipated mix of single detached and semi-detached dwellings (R-G);
- approximately 3.47 hectares (8.57 acres) for anticipated rowhouse dwellings (DC (R-Gm)) which will allow for Live Work units;
- approximately 2.30 hectares (5.68 acres) of mid-rise multi-residential development (DC(M-2)) which will provide for approximately 285 dwelling units in a multi-residential built form of up to 21 metres in height;
- maintaining a large central wetland complex through Environmental Reserve (ER) dedication as a focal point of the community (S-UN);
- dedicating approximately 0.42 hectares (1.04 acres) as Public Utility Lot (PUL) for pipeline and stormwater infrastructure within the plan area (S-CRI); and
- approximately 0.61 hectares (1.51 acres) of open space land dedicated Municipal Reserve (MR) (S-SPR); and
- a joint use school site between the Calgary Catholic School District and the Calgary Board of Education for an elementary/junior high school and an elementary school respectively totaling 7.54 hectares (18.63 acres) of Municipal School Reserve (MSR) dedication (S-SPR).

This application has been applied for with the support of a related outline plan application (CPC2019-1391) to provide the future subdivision layout for the site's development. Conditions have been incorporated into the outline plan to effectively address the site's development. The proposed land use amendment has been developed in accordance with the policies of the Northeast Community 'A' Area Structure Plan.

**Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249**

ADMINISTRATION RECOMMENDATION:

1. Direct this report (CPC2019-1375) to the 2019 December 16 Combined Meeting of Council to the Public Hearing portion of the Agenda;
2. Recommend that Council hold a Public Hearing; and
 - a) **ADOPT**, by bylaw, the road closure to close a 0.017 hectare \pm (0.042 acre \pm) adjacent to 5555 Country Hills Boulevard NE (Plan 1912254, Area 'A') , with conditions (Attachment 1); and
 - b) Give three readings to proposed closure bylaw.
 - c) **ADOPT**, by bylaw, the proposed redesignation of 64.76 hectares \pm (160.03 acres \pm) located at 10010 and 10011 – 52 Street NE, 5299 and 5555 Country Hills Boulevard NE and the closed road (Portion of SW1/4 Section 23-25-29-4; Portion of SE1/4 Section 22-25-29-4; Portion of Plan 0712654, Lot B; Portion of NW1/4 Section 23-25-29-4; Plan 1912254, Area 'A') from Residential – Narrow Parcel One Dwelling (R-1N) District, Residential – One Dwelling (R-1) District, Residential – One / Two Dwelling (R-2) District, Residential – Low Density Multiple Dwelling (R-2M) District, Multi-Residential – Low Profile (M-1) District, Multi-Residential – Medium Profile (M-2) District, Multi-Residential – High Density Medium Rise (M-H2) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – Urban Nature (S-UN) District, Special Purpose – City and Regional Infrastructure (S-CRI) District, Special Purpose – Future Urban Development (S-FUD) District, DC Direct Control District and Undesignated Road Right-of-Way **to** Residential – Low Density Mixed Housing (R-G) District, Special Purpose – City and Regional Infrastructure (S-CRI) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – Urban Nature (S-UN) District; and
 - d) Give three readings to proposed bylaw.
 - e) **ADOPT**, by bylaw, the proposed redesignation of 3.42 hectares \pm (8.45 acres \pm) located at 10010 – 52 Street NE and 5555 Country Hills Boulevard NE (Portion of SW1/4 Section 23-25-29-4; Portion of NW1/4 Section 23-25-29-4) from Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – Future Urban Development (S-FUD) District, **to** DC Direct Control District to accommodate multi residential development, with guidelines (Attachment 2); and
 - e) Give three readings to proposed bylaw.

Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple addresses, LOC2018-0249

- f) **ADOPT**, by bylaw, the proposed redesignation of 6.50 hectares \pm (16.06 acres \pm) located at 10011 – 52 Street NE, 5299 and 5555 Country Hills Boulevard NE (Portion of SE1/4 Section 22-25-29-4; Portion of NW1/4 Section 23-25-29-4; Portion of Plan 0712654, Lot B) from Residential – Narrow Parcel One Dwelling (R-1N) District, Residential – Low Density Multiple Dwelling (R-2M) District, Multi-Residential – Medium Profile (M-2) District, Multi-Residential – High Density Medium Rise (M-H2) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, and DC Direct Control District to DC Direct Control District to accommodate low density development with Live Work, with guidelines (Attachment 3); and
- g) Give three readings to proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

B&A Planning Group submitted the application to The City of Calgary on 2018 November 15 on behalf of the landowners Mattamy (Northpoint) Limited and the City of Calgary and has provided a summary of their proposal in the Applicant's Submission (Attachment 4).

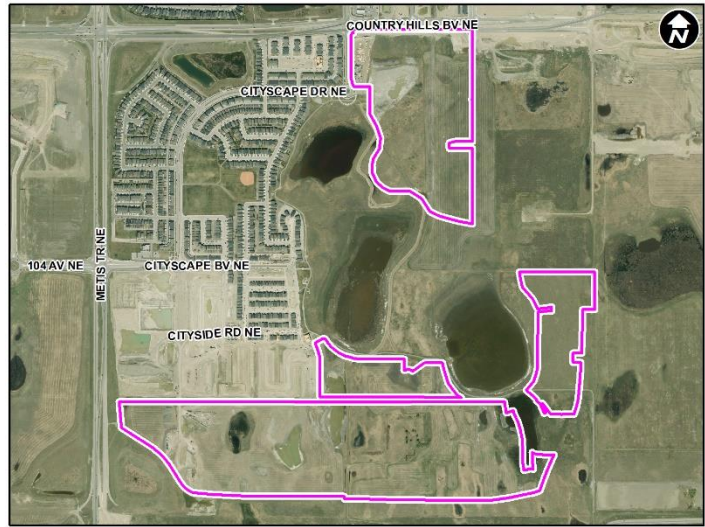
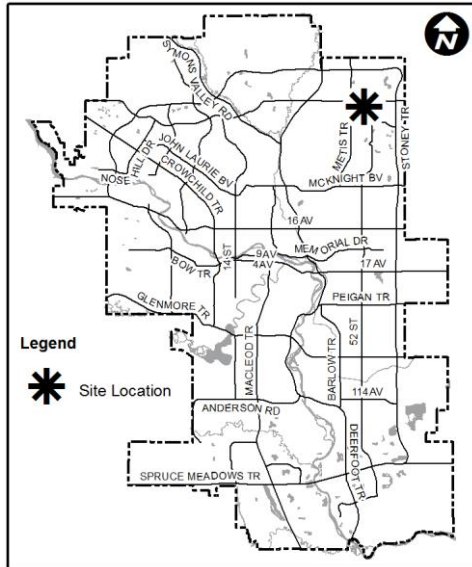
In 2007 July, the *Northeast Community 'A' Area Structure Plan (ASP)* was adopted by Council. This plan contained detailed land use policies for the communities of Redstone, Skyview Ranch and Cityscape. Concurrent with the adoption of the ASP, an outline plan and land use amendment application (LOC2006-0156) for lands owned by Walton International Group was approved for the subject site contained within this application. The application comprised over 460 hectares (1,100 acres) of land and included the majority of the Cityscape community. The lands remained undeveloped and were eventually purchased by Mattamy Homes from the Walton International Group in late 2011. Mattamy Homes had a different vision for development of the lands than that which was originally proposed, including unique housing forms and different street standards. As such, new land use districts and revisions to the previously approved outline plan design are required. Mattamy Homes received outline plan and land use amendment approvals for Cityscape Stage 1 (February 2013) and Cityscape Stage 2 (July 2014) to redesign the area in better alignment with Mattamy Homes' vision for the area.

This proposed land use amendment application and corresponding outline plan (CPC2019-1391) represents the third and final stage of Mattamy Homes' Cityscape lands, thereby completing the remaining portion of the community. The proposed land use amendment, and corresponding outline plan application, integrate the site with the existing approved Cityscape Stage 1 and 2 to the west, and the future land use and outline plan to the east (Skypointe South, LOC2014-0165 brought forward for approval at the November 07, 2019 CPC.

Planning & Development Report to
Calgary Planning Commission
2019 November 21
Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249

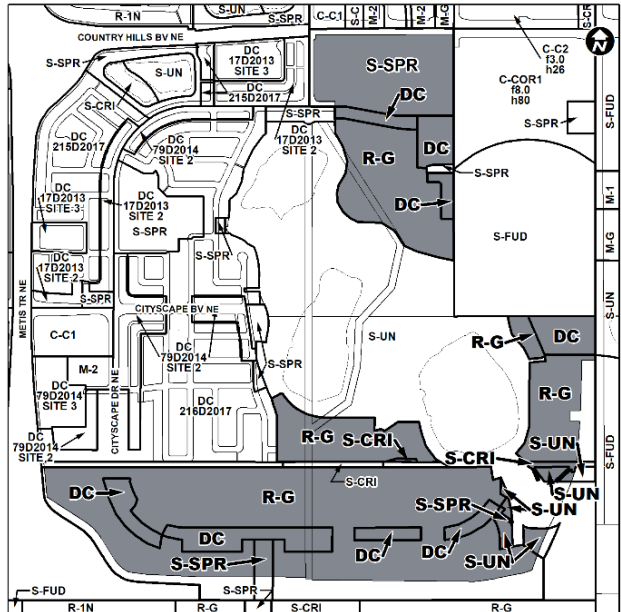
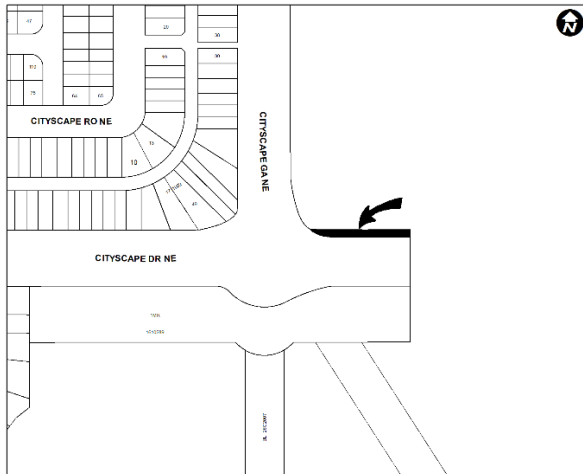
ISC: UNRESTRICTED
CPC2019-1375

Location Maps



Road Closure Map

Proposed Land Use Map



**Planning & Development Report to
Calgary Planning Commission
2019 November 21**

**ISC: UNRESTRICTED
CPC2019-1375**

**Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249**

Site Context

The plan area, is located in Calgary's northeast, approximately 12 kilometers northeast of Calgary's city centre and approximately three kilometers east of the Calgary International Airport. The subject site, which comprises of an area totaling approximately 74.68 hectares (184.53 acres) of land, is located between Country Hills Boulevard NE and Airport Trail NE. The land has historically been natural grassland and wetland, and was later used for farming and ranching operations. A wetland exists within the central portion of the community, the area of which is not included in this application, and was previously designated as Special Purpose – Urban Nature as part of LOC2006-0156 (CPC2007-093).

Directly north of the subject site, across Country Hills Boulevard NE, is the developing community of Skyview Ranch. Commercial and higher density residential uses are planned near the future LRT station at the intersection of Country Hills Boulevard NE and 60 Street NE. Directly to the south of the subject site. Across the future Airport Trail NE alignment, is the developing largely low-density community of Savanna.

The first two stages of Cityscape have already largely undergone development with a total of 2,811 people living within the currently constructed portions of the community. *Figure 1* provides an overview of the population statistics for the community of Cityscape.

Figure 1: Community Peak Population

Cityscape	
Peak Population Year	2018
Peak Population	2,811
2018 Population	2,811
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [Cityscape](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This road closure and land use amendment, along with the corresponding outline plan, will facilitate the development of a range of residential and recreational developments that will contribute to the growth of the Cityscape community as envisioned by the policies of the *Municipal Development Plan* (MDP) and the *Northeast Community 'A' Area Structure Plan* (ASP).

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

**Planning & Development Report to
Calgary Planning Commission
2019 November 21**

**ISC: UNRESTRICTED
CPC2019-1375**

**Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249**

Road Closure

A 0.017-hectare portion of the Cityscape Drive NE right-of-way is proposed to be closed with this application (Attachment 1) with conditions found in Attachment 5. The to-be-closed road right-of-way is proposed to be redesignated to Special Purpose – School, Park and Community Reserve (S-SPR) District, and consolidated with the adjacent school site.

Land Use

The subject lands were previously designated under application LOC2006-0156. Due to this previous redesignation, the land areas of this land use redesignation and the associated outline plan (CPC2019-1391) are not aligned. Approximately 2.83 hectares (6.99 acres) of land is not being redesignated in this application because the designation from the previous application (LOC2006-0156 (CPC2007-093)) is being retained. The existing land use districts on the subject site are:

- Residential – Narrow Parcel One Dwelling (R-1N) District;
- Residential – One Dwelling (R-1) District;
- Residential – One / Two Dwelling (R-2) District;
- Residential – Low Density Multiple Dwelling (R-2M) District;
- Multi-Residential – Low Profile (M-1) District;
- Multi-Residential – Medium Profile (M-2) District;
- Multi-Residential – High Density Medium Rise (M-H2) District;
- Special Purpose – School, Park and Community Reserve (S-SPR) District;
- Special Purpose – Urban Nature (S-UN) District;
- Special Purpose – City and Regional Infrastructure (S-CRI) District;
- Special Purpose – Future Urban Development (S-FUD) District; and
- DC Direct Control District (Bylaw 83Z2007 based on RM-4 Residential Medium Density Multi-Dwelling District of Land Use Bylaw 2P80.

The proposed land use amendment proposes to redesignate the lands from the list of districts above, to:

- Residential – Low Density Mixed Housing (R-G) District;
- DC Direct Control District based on Residential – Low Density Mixed Housing (R-Gm) District;
- DC Direct Control District based on Multi-Residential – Medium Profile (M-2) District;
- Special Purpose – City and Regional Infrastructure (S-CRI) District;
- Special Purpose – School, Park and Community Reserve (S-SPR) District; and
- Special Purpose – Urban Nature (S-UN) District.

The proposed districts are located strategically throughout the plan area, with districts that allow for higher densities located in proximity to those that can provide for commercial, recreational and community service uses. Transit routes were also considered in the placement of districts. The proposed land use maps are provided in Attachment 6.

**Planning & Development Report to
Calgary Planning Commission
2019 November 21**

**ISC: UNRESTRICTED
CPC2019-1375**

**Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249**

Low density housing forms will be provided by both the R-G and DC Direct Control District based on the R-Gm District. The R-G District and the DC Direct Control District based on the R-Gm District will accommodate a wide range of low density residential development in the form of cottage housing cluster, single detached, semi-detached, duplex dwellings and rowhouse buildings. Secondary suites are also permitted within all of these housing forms.

Two DC Direct Control District designations are proposed within the plan area, in order to accommodate low density residential uses on shallow lots with the live work use as well as mid-rise multi-residential on two multi-residential parcels.

DC Direct Control District – Mid-Rise Multi-Residential

The second proposed DC District (Attachment 2) is based on the M-2 District and is intended to allow for a mid-rise multi-residential built form with special height rules and increasing the maximum height from 16 metres to 21 metres.

DC Direct Control District – Shallow Lots with Live Work

The proposed DC District (Attachment 3) is based on the R-Gm District and is intended to allow for a variety of low density residential development on lots that are served by rear lanes and are shallower than a standard parcel in Land Use Bylaw 1P2007, as well as the introduction of live work units to the low density residential housing type.

Throughout the planned neighbourhood, non-residential land uses include the S-UN District that will allow for the dedication of reconstructed wetlands as Environmental Reserve (ER), as well as other natural areas bordering the large central wetland area. The S-SPR District is proposed to accommodate a 7.54-hectare (18.63 acre) joint use site (school site) which will be dedicated as Municipal School Reserve (MSR). The S-SPR District will also accommodate neighbourhood parks and open space—which will be dedicated as Municipal Reserve (MR). The S-CRI District will accommodate the storm water maintenance access easements that are required to service the subject plan area, as well as a portion of a utility right-of-way and will be dedicated as a Public Utility Lot (PUL).

Subdivision Design

The proposed subdivision design (Attachment 7), which comprises approximately 74.68 hectares (184.53 acres), embraces a comprehensive open space system which is central to the plan area and creates a continuous user experience by incorporating passive and active, formal and informal areas for learning, playing, and living.

The proposed design generally follows a modified grid street pattern that allows for a variety of housing forms with a combination of laned and non-laned product types. Where a lane exists, front garages have been generally limited to improve the residential street interface and access conditions. A restrictive covenant will be registered against certain laned parcels through an outline plan condition. The block layout also supports the intensification of the neighbourhood over time by accommodating a variety of built forms. Anticipated lot widths vary from 5.0 to 7.0

**Planning & Development Report to
Calgary Planning Commission
2019 November 21**

**ISC: UNRESTRICTED
CPC2019-1375**

**Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249**

metres for rowhouse buildings in the Residential – Low Density Mixed Housing (R-G) District to an average lot width of 12.2 metres for single detached dwellings in the R-G District.

The modified grid pattern has been adopted due to the open space corridor that is centrally located within the plan area. The central wetland is the iconic place making feature of the community and as such, several large publicly accessible access points to the wetland exist within the plan area. This will allow for everyone in the community to view the wetland and interact with it in a number of different passive and active ways.

Overall, the grid-based subdivision design provides a framework to accommodate a diversity of housing types and densities, open spaces, an integral naturalized storm pond, neighbourhood focal points, and community amenities. The subdivision design is tied together by an efficient system of multi-modal pathways and road networks. The proposed subdivision anticipates 1,645 units, with a maximum of 2,982 units possible.

There is a total of 7.54 hectares (18.63 acres) of Municipal School Reserve (MSR) dedicated to the required Joint Use Site (one elementary school and one elementary/junior high school), and 0.61 hectares (1.51 acres) of Municipal Reserve (MR) dedicated to open space. In total, 7.54 hectares (18.63 acres) of land is dedicated to MSR and MR, which is 11.4 percent of the gross developable area. Additional land has been dedicated above the required 10 percent through a deferred reserve caveat (DRC 111 100 837) transferred from Skyview Ranch and registered on title of the West ½ of the NW ¼ Sec. 23, Twp. 25, Rge. 29 W4M of the subject lands.

Density

Policy 7.2 of the *Northeast Community 'A' Area Structure Plan (ASP)* identifies that the minimum residential density of 17.5 units per hectare (7 units per acre) is required in each neighbourhood outside of the identified transit areas. The proposed mix of land uses allows for a maximum of 2,982 dwelling units. The corresponding outline plan proposes an anticipated total of 1,645 residential units. These unit counts equate to an anticipated density of 23.1 units per hectare (9.3 units per acre) with a maximum density yield of 41.9 units per hectare (16.9 units per acre), exceeding the requirements of the ASP.

The ASP also identifies a Neighbourhood Node in the south portion of the plan area that requires a minimum density of 50 units per hectare (20 units per acre). The proposal provides an anticipated density of 52.08 units per hectare (20.83 units per acre) and therefore exceeding the ASP requirement within this Neighbourhood Node area.

During the review of the application, it was determined that the broader community was over saturated with commercial development and that a commercial component would not be feasible within the Neighbourhood Node. For this reason, the live work component of the proposed DC Direct Control District found within the Neighbourhood Node is intended to satisfy the demand for local services provided within the node. The proposed outline plan and land use designations provide for development that exceeds both the MDP and the ASP minimum density and intensity targets within the identified Neighbourhood Node.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1375

Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple addresses, LOC2018-0249

Environmental

A Biophysical Impact Assessment was completed in conjunction with the original outline plan approval (LOC2006-0156) and Environmental Reserve (ER) boundaries were determined at that time. Two reconstructed stormwater wetlands were identified in the original approval to satisfy the wetland compensation agreement, one of which is in this stage 3 area. The reconstructed stormwater wetland in the subject plan area has been modified from the original design but the size of the associated ER has not been reduced. Updates to the original BIA findings were provided in a Preliminary Natural Site Assessment in support of this application and no additional recommendations or actions were required.

Transportation

Street and Mobility Network

The subject site is within the section of land bound by Country Hills Boulevard NE to the north, 60 Street NE to the east, Airport Trail to the south, and Metis Trail to the west. The internal road system will complete Cityscape Drive, the main circulatory collector within the subject area, and contains a modified grid network that connects with this collector road network.

Cityscape is partially built-out with existing community entrance roads (Cityscape Boulevard and Cityscape Gate) that connect Cityscape drive to Metis Trail NE and Country Hills Boulevard NE, respectively. New community entrances to Country Hills Boulevard and to 60 Street NE are planned as a component of this application.

The pathway within the west boulevard of 60 Street NE is part of the Rotary Mattamy Greenway system. The Greenway is planned to traverse the community, entering via the newly created community entrance road at 100 Avenue NE. The greenway, in combination with the planned regional pathway and local pathways, provide a comprehensive pathway system around the wetlands in the community. The Greenway itself continues to the north into the adjacent community of Skyview Ranch. A multi-use pathway is also planned for Cityscape Drive, providing connectivity to the future LRT Station located at Country Hills Boulevard NE and 60 Street NE.

The plan area is well-connected to the regional transportation network and is in close proximity to both Stoney Trail NE and Airport Trail NE. In support of ongoing development in the area, the City of Calgary is currently constructing the extension of Airport Trail between Metis Trail NE and 60 Street NE. This roadway is anticipated to open by end 2019. The connection of 60 Street between Country Hills Boulevard and Airport Trail by area developers is required to tie into the Airport Trail extension and is currently being designed with an anticipated opening of end-2021. These connections represent the only remaining missing links in the network for northeast Calgary. Once each of these projects are complete, residents of this and surrounding communities will benefit from having a comprehensive regional roadway network.

**Planning & Development Report to
Calgary Planning Commission
2019 November 21**

**ISC: UNRESTRICTED
CPC2019-1375**

**Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249**

Transit

The Blue Line LRT extension from Saddletowne Station to Country Hills Boulevard is currently unfunded.

Skyview Ranch and Cityscape are currently serviced by Route 145, which provides connection to the Saddletowne LRT Station. The subject lands planned under this application could potentially be serviced by a slight modification to this route. The adjacent communities of Cornerstone and Redstone are currently serviced by Route 128.

The existing routes currently use Metis Trail as the north-south connection to Saddletowne LRT Station via 80 Avenue. The construction of 60 Street NE between Country Hills Boulevard NE and Airport Trail NE by area developers will provide significant benefit to the subject lands and neighbouring communities in terms of travel time and convenience for transit service, as it will reduce the distance required to connect to the existing Blue Line LRT.

In addition to the local transit service and the future Blue Line LRT extension, *RouteAhead* (Calgary Transit long-term strategic plan) identifies Primary Transit service along 52 Street NE between the Saddletowne LRT Station and the future (Green Line LRT) Seton Station. This service would connect to other cross-town Primary Transit services including MAX Orange (Rundle), MAX Purple (17 Avenue), and MAX Teal (future – 114 Avenue).

Utilities and Servicing

Water servicing for the plan area will be provided via extensions of the water distribution systems installed within Cityscape Stage 1 and Stage 2, as well as ties to adjacent developments.

Sanitary servicing for the north portion of the plan area will be provided via extensions of the Sanitary sewer installed as part of Cityscape Stage 1. Sanitary servicing for the south portion of the plan area will be provided via construction of a sanitary main in 60 Street NE which will flow north to the existing sanitary trunk in Country Hills Boulevard.

Overall stormwater servicing for the Cityscape development area will be provided via connections to two reconstructed wetlands located in the northwest (existing Pond F) and southeast corners of the development. Stormwater servicing for the north portion of the plan area will be provided via extensions of the storm sewer installed as part of Cityscape Stage 1 and will drain towards the northwest Pond F. Stormwater servicing for the south portion of the plan area will be provided via construction of the proposed southeast reconstructed wetland.

Further water, sanitary, and storm servicing details for the plan area will be resolved at the tentative plan, construction drawing, and development permit stages.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1375

Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple addresses, LOC2018-0249

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Administration received no comments in relation to the application. Currently, there is no community association for the area.

No public meetings were conducted by the applicant or Administration in relation to this application. Applicant-led public engagement was not deemed to be necessary given the greenfield nature of the plan area.

Following Calgary Planning Commission, notifications for Public Hearing of Council, in relation to the land use amendment and road closure, will be posted in developed areas near the subject site and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy directions of the *South Saskatchewan Regional Plan* which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed application builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Calgary International Airport Vicinity Protection Area Regulation (Statutory – 2009)

A portion of the subject site is located within the 25-30 Noise Exposure Forecast (NEF) contour of the Airport Vicinity Protection Area (AVPA) land use regulations. The proposed list of uses within the proposed land use districts are allowable within the 25-30 NEF contour area. Future development permits will be circulated to NAV Canada and reviewed against the applicable regulations to ensure alignment and compliance.

Municipal Development Plan (Statutory – 2009)

The *Municipal Development Plan* (MDP) identifies the plan area as Planned Greenfield with Area Structure Plan, referring to the *Northeast Community 'A' ASP*. Planned greenfield areas have generally been planned since 1990 and are low density residential in nature, with pockets of commercial land uses. It is noted that the site is located in proximity to a Major Activity Centre

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1375

Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple addresses, LOC2018-0249

as identified in the MDP. As it was approved prior to adoption of the MDP, the *Northeast Community 'A' ASP* is recognized as appropriate policy to direct growth in the area.

The Northeast Community 'A' Area Structure Plan

The *Northeast Community 'A' ASP* designates the application area for future residential development and includes an LRT alignment and associated stations. The proposed application meets the requirements of this Plan.

The proposed *Northeast Community 'A' ASP* identifies the plan area as Predominantly Residential Area with the following major components:

- two Joint Use Sites (JUS);
- one Neighbourhood Node;
- a portion of a Transit Station Planning Area (TSPA);
- one pedestrian overpass located over Airport Trail NE;
- a Wetland Conservation Area; and
- a regional Pathway system.

The subject application contains the required land uses and components as required by the ASP.

Social, Environmental, Economic (External)

The proposed land use designations and associated outline plan will enable the continuation of development in the community of Cityscape and provide a future framework for a mix of housing types to support a broad cross section of the population. The development of these lands will enable a more efficient use of land and infrastructure, including the future Blue Line LRT Station, supporting surrounding uses and amenities while introducing additional amenities to the area.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

There are no known impacts to the current and future operating budgets at this time.

Risk Assessment

There are no risks associated with this proposal.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1375

**Road Closure and Land Use Amendment in Cityscape (Ward 05) at multiple
addresses, LOC2018-0249**

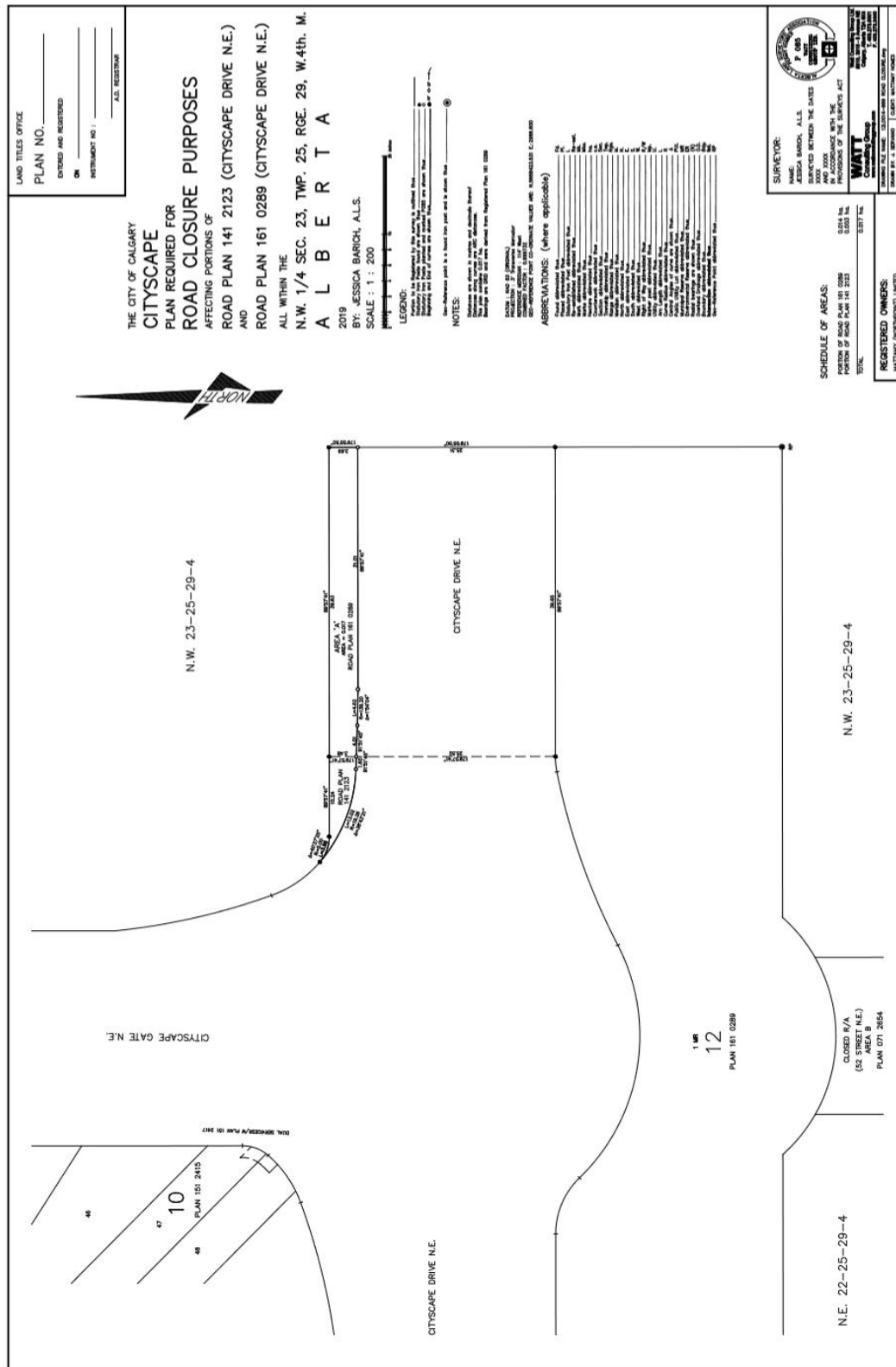
REASON(S) FOR RECOMMENDATION(S):

The proposed road closure and land use redesignation is aligned with the policies of the *Northeast Community 'A' Area Structure Plan* and the *Municipal Development Plan*, creating the foundation for a complete community. The proposed land uses will accommodate residential, support commercial, educational and recreational development throughout the plan area.

ATTACHMENTS

1. Road Closure Plan
2. Proposed DC Direct Control District (M-2)
3. Proposed DC Direct Control District (R-Gm)
4. Applicant's Submission
5. Proposed Road Closure Conditions
6. Proposed Land Use District Map
7. Proposed Outline Plan

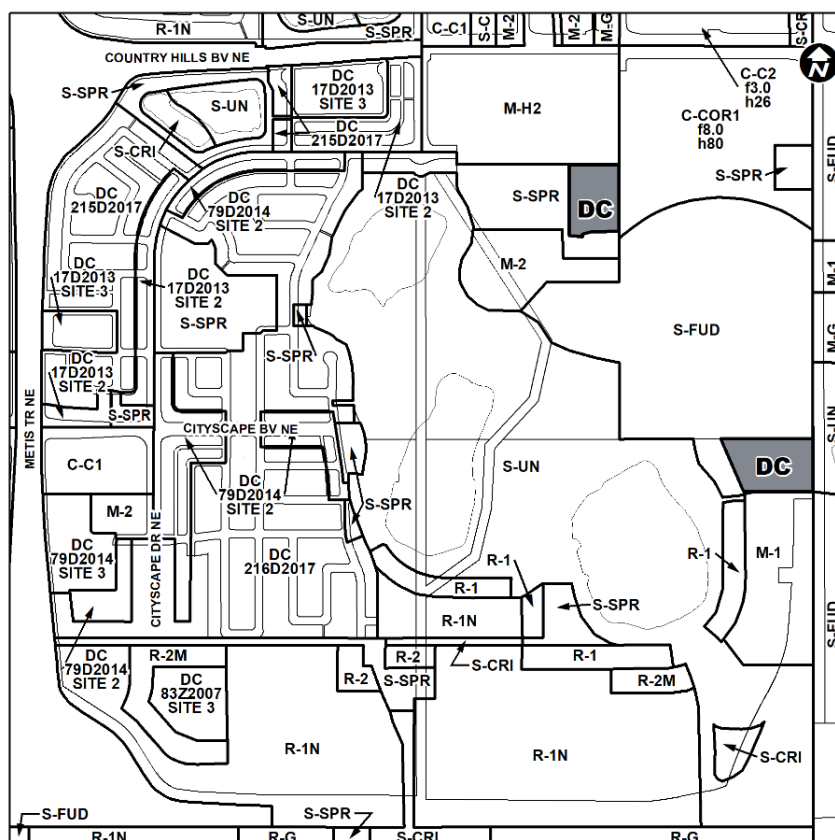
Road Closure Plan



Proposed DC Direct Control District (M-2)

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by amending that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "A".

SCHEDULE A



DIRECT CONTROL DISTRICT

Purpose

- 1 This Direct Control District Bylaw is intended to allow for additional building height to accommodate a six-storey residential development.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District Bylaw.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District Bylaw, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The **permitted uses** of the Multi-Residential – Medium Profile (M-2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Proposed DC Direct Control District (M-2)

Discretionary Uses

- 5 The **discretionary uses** of the Multi-Residential – Medium Profile (M-2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District.

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Multi-Residential – Medium Profile (M-2) District of Bylaw 1P2007 apply in this Direct Control District.

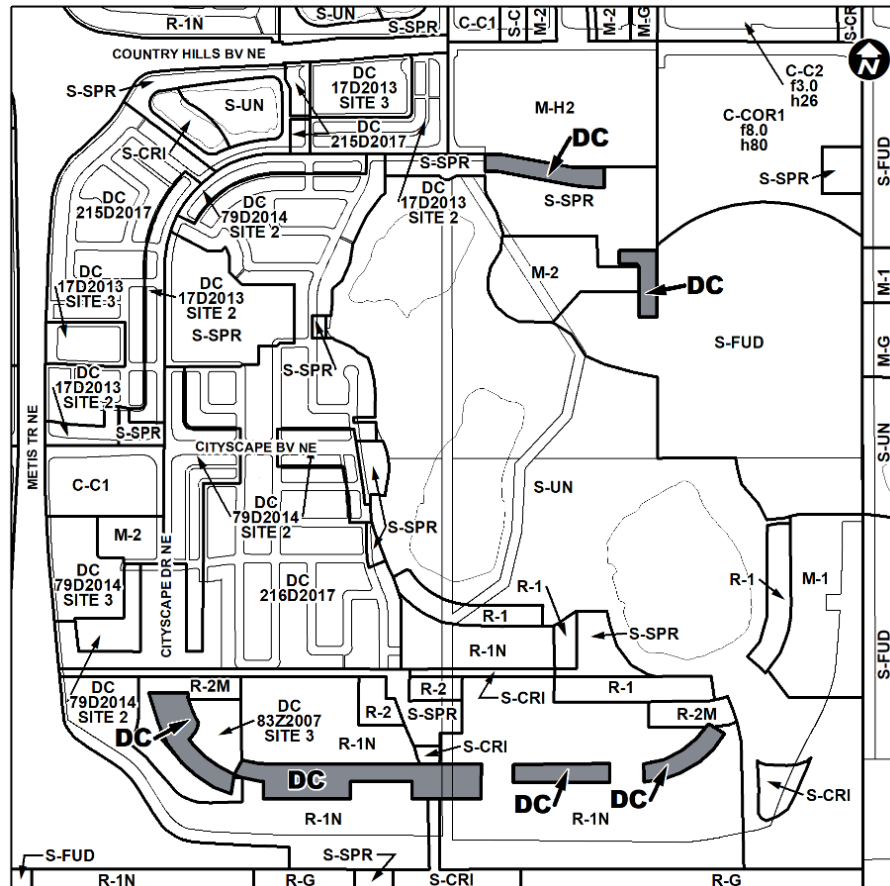
Building Height

- 7 (1) Unless otherwise referenced in subsections (2) and (3), the maximum **building height** is 21.0 metres.
- (2) Where a **parcel** shares a **property line** with a **parcel** designated with a **low density residential district** or M-G District, the maximum **building height**:
- (a) is 11.0 metres measured from **grade** at the shared **property line**; and
 - (b) increases proportionately to a maximum of 21.0 metres measured from **grade** at a distance of 5.0 metres from the shared **property line**.
- (3) Where a **parcel** shares a **property line** with a **street**, the maximum **building height** is 10.0 metres measured from **grade** within 3.0 metres of that shared **property line**.

Proposed DC Direct Control District (R-Gm)

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by amending that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "A".

SCHEDULE A



DIRECT CONTROL DISTRICT

Purpose

- 1 This Direct Control District Bylaw is intended to accommodate street-oriented residential development in the form of semi-detached dwellings and rowhouses on small-scale lots with minimal rear yard setbacks and rear lane access in the developing area.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District Bylaw.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District Bylaw, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Proposed DC Direct Control District (R-Gm)

Defined Uses

4 In this Direct Control District Bylaw:

- (a) “**Live Work**” means a *use*:
 - (i) where a business is operated from a **Dwelling Unit**, by the resident of the **Dwelling Unit**, but does not include a **Home Occupation – Class 1** or **Home Occupation – Class 2**;
 - (ii) that may incorporate only the following *uses* in a **Dwelling Unit** to create a **Live Work**:
 - (A) **Artist’s Studio**;
 - (B) **Counselling Service**;
 - (D) **Office**; and
 - (E) **Retail and Consumer Service**, provided any products sold are also made on the premises or directly related to the service provided.

Permitted Uses

5 The *permitted uses* of the Residential – Low Density Mixed Housing (R-Gm) District of Bylaw 1P2007 are the *permitted uses* in this Direct Control District.

Discretionary Uses

6 The *discretionary uses* of the Residential – Low Density Mixed Housing (R-Gm) District of Bylaw 1P2007 are the *discretionary uses* in this Direct Control District, with the addition of:

- (a) **Live Work**.

Bylaw 1P2007 District Rules

7 Unless otherwise specified, the rules of the Residential – Low Density Mixed Housing (R-Gm) District of Bylaw 1P2007 apply in this Direct Control District.

Vehicle Access

- 8
- (1) All vehicle access to a *parcel* must be from a *lane*.
 - (2) A *driveway* must not have direct access to a *street*.
 - (3) A *private garage* may only be allowed at the rear of a *unit*.

Parcel Width

- 9
- (1) The minimum *parcel width* for each *parcel* containing one *unit* of a **Semi-detached Dwelling** is:
 - (a) 7.2 metres for a *corner parcel*; and
 - (b) 6.0 metres in all other cases.

Proposed DC Direct Control District (R-Gm)

- (2) The minimum **parcel width** for a **parcel** containing one **unit** of a **Rowhouse Building** is:

- (a) 4.5 metres for a **corner parcel**; and
- (b) 3.3 metres in all other cases.

Parcel Depth

- 10 The minimum **parcel depth** is 18.5 metres.

Parcel Area

- 11 The minimum area of a **parcel** is:

- (a) 111.0 square metres for each **parcel** containing one **unit** of a **Semi-detached Dwelling**; and
- (b) 62.0 square metres for each **parcel** containing one **unit** of **Rowhouse Building**.

Parcel Coverage

- 12 (1) The maximum **parcel coverage** is 90.0 per cent of the area of the **parcel**.
- (2) The maximum **parcel coverage** referenced in subsection (1) must be reduced by 21.0 square metres for each required **motor vehicle parking stall** that is not located in a **private garage**.

Building Setback from Front Property Line

- 13 (1) Unless otherwise referenced in subsection (2), the minimum **building setback** from a **front property line** is 2.0 metres.
- (2) The minimum **building setback** from a **front property line** is 1.5 metres for a:
- (a) **porch**;
 - (b) **balcony**;
 - (c) column; or
 - (d) post.

Building Setback from Rear Property Line

- 14 (1) The minimum **building setback** from a **rear property line** is 0.6 metres.
- (2) Notwithstanding section 338(4) of Bylaw 1P2007, when an attached **private garage** has a **balcony** or **deck**, the minimum **building setback** from a **rear property line** for a **balcony** or **deck** is 0.6 metres.

Building Setback from Side Property Line

- 15 (1) Unless otherwise reference in subsection (2), (3) or (4), the minimum **building setback** from any **side property line** is 1.2 metres.
- (2) For a **corner parcel**, the minimum **building setback** from a **side property line** shared with a **street** is 1.2 metres, with the exception of subsection (3), provided there is no portion of a **building** located within 3.0 metres of:

Proposed DC Direct Control District (R-Gm)

- (a) the back of a public sidewalk; or
 - (b) the curb where there is no public sidewalk.
- (3) Eaves may project a maximum of 0.6 metres into any **side setback area**.
- (4) For **parcels** of less than 13.0 metres in width, the **building setback** required may be reduced to 0.6 metres where:
- (a) the owner of the **parcel** proposed for **development** and the owner of the adjacent **parcel** register, against both titles, a 1.8 metre exclusive private access easement;
 - (b) all roof drainage from the **building** is discharged through eavestroughs and downspouts onto the **parcel** on which the **building** is located; and
 - (c) Notwithstanding subsection (3), eaves may project a maximum of 0.45 metres into the **side setback area**.

Outdoor Private Amenity Space

- 16 (1) Each **Dwelling Unit** must have a **private amenity space**:
- (a) provided for the private use of the occupants of the **Dwelling Unit**;
 - (b) that has a minimum area of 7.0 square metres with no dimension less than 2.0 metres; and
 - (c) in the form of a **porch, balcony, deck** or **patio**.
- (2) Where a **patio** is located within 4.0 metres of a side **property line** of a **parcel** containing a **Dwelling Unit**, it must be **screened**.

Development in Setback Area

- 17 (1) Unless otherwise referenced in subsection (2), a **porch, balcony, deck** or **patio** must not be located in any **setback area**.
- (2) A **patio** may be located in the **front setback area** and in a **setback area** on the street side of a **corner parcel**.
- (3) Section 340(1) and (2) of Bylaw 1P2007 do not apply to **balconies**.

Building Height

- 18 The maximum **building height** is 13.0 metres.

Rules for Live Work

- 19 (1) **Live Work** must not exceed 50.0 per cent of the **gross floor area** of the **Dwelling Unit**.
- (2) **Live Work** may have two persons, other than a resident of the **Live Work**, working at the residence where the **use** is located.

Proposed DC Direct Control District (R-Gm)

- (3) **Live Work** requires 1.0 *motor vehicle parking stalls*.
- (4) **Live Work** requires 1.0 *bicycle parking stall – class 1*, or *bicycle parking stall – class 2*.

Relaxations

- 20** The ***Development Authority*** may relax the rules contained in this Direct Control District Bylaw in accordance with sections 31 and 36 of Bylaw 1P2007.

TEXT FOR DISCUSSION
ONLY

Applicant's Submission

22 October 2019

Cityscape Stage 3 - Land Use Redesignation

APPLICANT'S SUBMISSION

Mattamy is submitting an Outline Plan & Land Use amendment application for the development of a new neighbourhood located in the northeast sector of the City of Calgary. The subject site is 184 acres in size and comprises the remaining lands owned by Mattamy in the community of Cityscape. Cityscape a thriving and connected residential community with abundant open space amenities and a unique approach to housing and streetscapes.

The proposed land use redesignation includes R-G for low density residential uses, and S-UN, S-CRI and S-SPR for environmental reserve, public utility lot and municipal reserve respectively. Two Direct Control Districts are proposed which is based on the existing DC District approved in Cityscape Stages 1 & 2, which allows for Mattamy's unique Street-Oriented Laned Row Housing as well as another for multi-residential development.

At the heart of the community is an existing scenic 115 acre wetland. This wetland is integrated into the community through the connected open space and pathway network. Stage 3 will connect to and improve upon the existing pathway network including the Rotary/Mattamy Greenway which is a 138 km multi-use pathway around the city of Calgary. Stage 3 of Cityscape will build upon and expand the existing range of amenities in the community, providing open space in excess of the required 10% municipal reserve. A Joint- Joint Use School Site (JJUS) is located in the north portion of the plan near a future transit station. This site includes two school buildings with shared playfields and a cricket pitch. There are also three sub-neighbourhood parks spread throughout the plan area, as well as a reconstructed wetland and stormwater management pond in the southeast portion of the plan.

The plan area employs a rectilinear road network comprised of interconnected streets, supported by walkways and linear green spaces to provide connectivity through the community. In keeping with Cityscape Stages 1 & 2, the plan will include architectural and design elements that enhance the streetscape. Laned housing types are placed along collector roads, with uniform and minimal housing setbacks to create a consistent streetscape. Park spaces are centrally located adjacent to major routes and garden lots front onto two of them to highlight them as public gathering spaces.

Mattamy offers a range of unique housing products to the Calgary market and a distinctive development philosophy. Mattamy recognizes that density and efficient use of land are important, but also that the market demand for multi-residential is lower in the northeast sector. By providing a range of single detached, semi-detached and townhome products on smaller and more efficient lots, Cityscape caters to consumers who want fee-simple ownership with greater affordability and reduced maintenance. Multi-residential housing is also proposed, strategically located close to open space within the plan area and to future commercial, recreation and transit in the anticipated Major Activity Centre to the northeast.

The proposed concept plan requires an amendment to the Northeast Community 'A' ASP due to the relocation of the two school sites within the plan area to a single Joint-Joint Use Site. This JJUS is located in close proximity to the future MAC and transit planning area to the east of the plan area, and is surrounded on all sides by regional pathways. The ASP shows one of these two school sites in the south portion of the plan area, but Administration expressed concerns about its proximity to the operating natural gas pipeline corridor; relocating this school to the north portion of the plan will alleviate these concerns. In addition, relocation of the schools to this proposed JJUS location allows for more efficient sharing of playfields including the proposed cricket pitch, which allows for other Municipal Reserve to be distributed more equitably about the plan area as other types of park space. The proposed relocation of the JJUS was supported by Site Planning Team in June 2019.

Overall, the proposal aligns with overarching policies in the Calgary Municipal Development Plan and New Community Design Guidebook and specific policies of the Northeast Community 'A' Area Structure Plan. At a projected density of 9.3 units per acre, the proposal exceeds density thresholds. Additionally, the plan area aligns with the existing vision for a unique and attractive community as set out in Cityscape Stages 1 & 2.

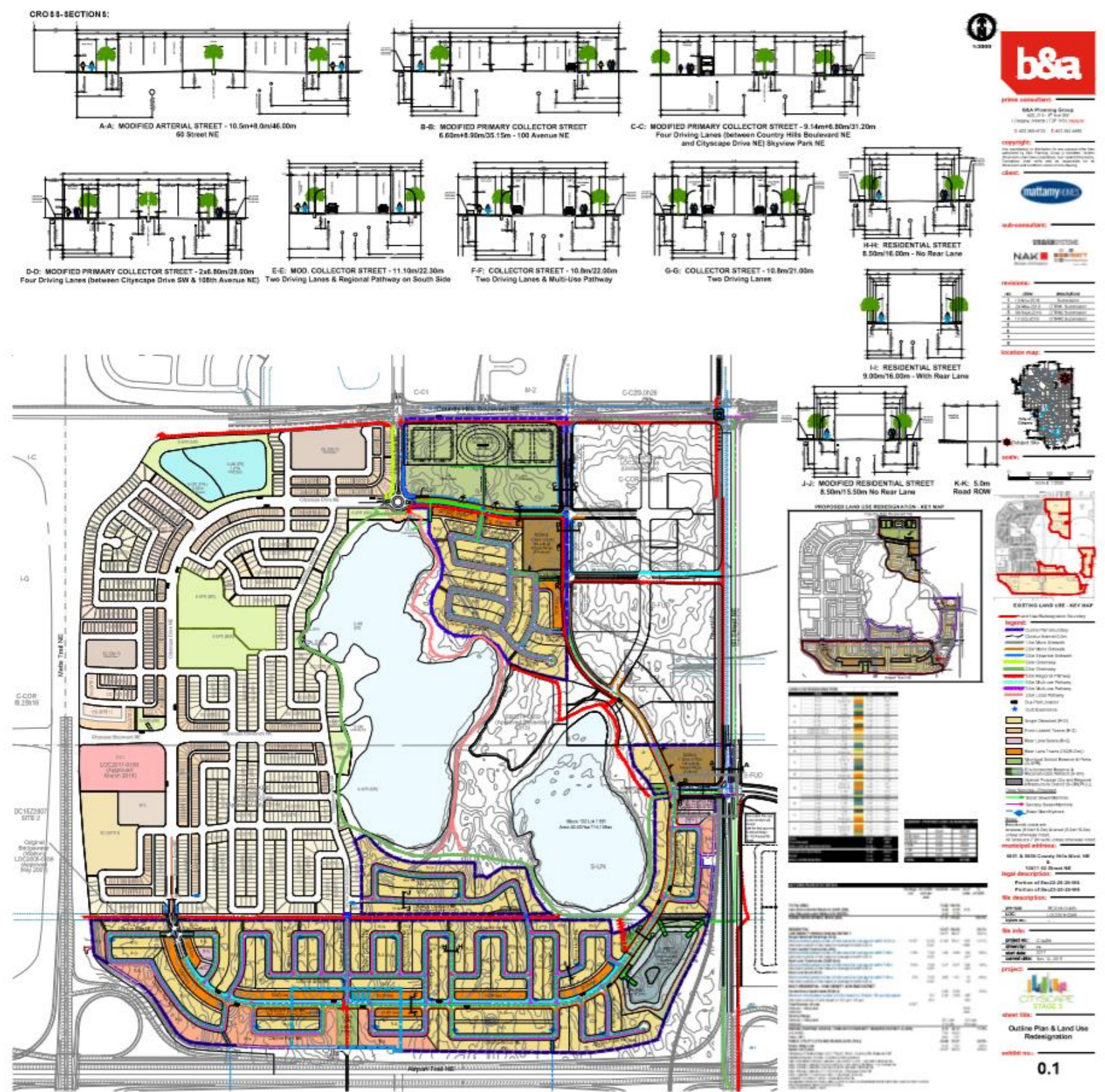
Proposed Road Closure Conditions

The following Conditions of Approval shall apply:

Planning:

1. The developer is responsible for all costs associated with the closure including all necessary physical construction, removal, rehabilitation, utility relocation, etc.
2. All costs associated with the road closure shall be borne by the developer.
3. That protection and/or relocation of any utilities required for the road closure will be at the applicant's expense and to the appropriate standards.
4. The closed road right-of-way is to be consolidated with the adjacent lands.

Proposed Outline Plan



Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1391

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

EXECUTIVE SUMMARY

This application was submitted by B&A Planning Group on behalf of Mattamy (Northpoint) Limited and the City of Calgary, the landowners, on 2018 November 15. This application proposes to close a portion of Cityscape Drive NE, and to redesignate approximately 74.66 hectares (184.49 acres) of land in the community of Cityscape. The outline plan seeks to establish a framework for the future subdivision that will complete the Cityscape community. Specifically, the application provides for:

- a low density residential development with an open space network that provides for a logical extension of and completes the Cityscape neighbourhood;
- approximately 36.3 hectares (89.70 acres) for an anticipated mix of single detached and semi-detached dwellings (R-G);
- approximately 3.47 hectares (8.57 acres) for anticipated rowhouse dwellings (DC(R-Gm)) which will allow for Live Work units;
- approximately 2.30 hectares (5.68 acres) of mid-rise multi-residential development (DC(M-2)) which will provide for approximately 285 dwelling units in a multi-residential built form of up to 21 metres in height;
- maintaining a large central wetland complex through Environmental Reserve (ER) dedication as a focal point of the community (S-UN);
- dedicating approximately 0.42 hectares (1.04 acres) as Public Utility Lot (PUL) for pipeline and stormwater infrastructure within the plan area (S-CRI); and
- approximately 0.61 hectares (1.51 acres) of open space land dedicated Municipal Reserve (MR) (S-SPR); and
- a joint use school site between the Calgary Catholic School District and the Calgary Board of Education for an elementary/junior high school and an elementary school respectively totaling 7.54 hectares (18.63 acres) of Municipal School Reserve (MSR) dedication (S-SPR); and
- the location of future local and major roadways, utilities and services.

The proposed outline plan serves to implement the objectives of the *Municipal Development Plan* and the *Northeast Community 'A' Area Structure Plan* by providing a subdivision plan for the future development of the site, in collaboration with the road closure, and land use amendment (CPC2019-1375), to be heard on the same Calgary Planning Commission agenda.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1391

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission **APPROVE** the proposed outline plan located at 10010 and 10011 – 52 Street NE, 5299 and 5555 Country Hills Boulevard NE (Portion of SW1/4 Section 23-25-29-4; Portion of SE1/4 Section 22-25-29-4; Portion of Plan 0712654, Lot B; Portion of NW1/4 Section 23-25-29-4) to subdivide 74.68 hectares \pm (184.53 acres \pm) with conditions (Attachment 1).

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

B&A Planning Group submitted the application to The City of Calgary on 2018, 15 November on behalf of the landowner Mattamy (Northpoint) Limited and the City of Calgary, and has provided a summary of their proposal in the Applicant's Submission (Attachment 2).

In 2007 July, the *Northeast Community 'A' Area Structure Plan (ASP)* was adopted by Council. This plan contained detailed land use policies for the communities of Redstone, Skyview Ranch and Cityscape. Concurrent with the adoption of the ASP, an Outline Plan, Road Closure and Land Use Amendment application (LOC2006-0156) for lands owned by Walton International Group was approved for the subject site contained within this application. The application comprised over 460 hectares (1,100 acres) of land, and included the majority of the Cityscape community. The City of Calgary is currently under process to transfer ownership of the closed roadways to Mattamy Homes per LOC2006-0156.

The lands remained undeveloped, and were eventually purchased by Mattamy Homes from the Walton International Group in late 2011. Mattamy Homes had a different vision for development of the lands than that which was originally proposed, including unique housing forms and different street standards. As such, a revision to the outline plan design, together with new land use districts, was required for development of the site. Mattamy Homes received outline plan and land use amendment approvals for Cityscape Stage 1 (February 2013) and Cityscape Stage 2 (July 2014) to redesign the area in better alignment with Mattamy Homes' vision for the area.

This proposed outline plan and corresponding land use amendment (CPC2019-1375) represents the third and final stage of Mattamy Homes' Cityscape lands, thereby completing the remaining portion of the community. The proposed land use amendment, and corresponding outline plan application, integrate the site with the existing approved Cityscape Stage 1 and 2 to the west, and the future land use and outline plan to the east (Skypointe South, LOC2014-0165 brought forward for approval at the November 07, 2019 CPC).

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

Site Context

The plan area, is located in Calgary's northeast, approximately 12 kilometers northeast of Calgary's city centre and approximately three kilometers east of the Calgary International Airport. The subject site, which is comprised of three different areas totaling approximately 74.68 hectares (184.53 acres) of land, is located between Country Hills Boulevard NE and Airport Trail NE. The land has historically been natural grassland and wetland, and was later used for farming and ranching operations. A wetland exists within the central portion of the community, the area of which is not included in this application, and was previously designated as Special Purpose – Urban Nature as part of LOC2006-0156 (CPC2007-093).

Directly to the north of the subject site, across Country Hills Boulevard NE, is the developing community of Skyview Ranch. Commercial and higher density residential uses are planned near the future LRT station at the intersection of Country Hills Boulevard NE and 60 Street NE. Directly to the south of the subject site. Across the future Airport Trail NE alignment, is the developing largely low-density community of Savanna.

The first two stages of Cityscape have already largely undergone development with a total of 2,811 people living within the currently constructed portions of the community. *Figure 1* provides an overview of the population statistics for the community of Cityscape.

Figure 1: Community Peak Population

Cityscape	
Peak Population Year	2018
Peak Population	2,811
2018 Population	2,811
Difference in Population (Number)	0
Difference in Population (Percent)	0%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [Cityscape](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This outline plan (Attachment 3), along with the associated road closure and land use amendment application, will facilitate the development of a range of residential and recreational developments that will contribute to the growth of the Cityscape community as envisioned by the policies of the *Municipal Development Plan* (MDP) and the *Northeast Community 'A' Area Structure Plan* (ASP).

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

Land Use

As previously mentioned in this report, the subject lands were previously designated under application LOC2006-0156. Due to this previous redesignation, the land areas of this outline plan and the associated land use redesignation application (CPC2019-1375) are not aligned. Approximately 2.83 hectares (6.99 acres) of land is not being redesignated because the designation from the previous application (LOC2006-0156 (CPC2007-093)) is being retained. The existing land use districts on the subject site are:

- Residential – Narrow Parcel One Dwelling (R-1N) District;
- Residential – One Dwelling (R-1) District;
- Residential – One / Two Dwelling (R-2) District;
- Residential – Low Density Multiple Dwelling (R-2M) District;
- Multi-Residential – Low Profile (M-1) District;
- Multi-Residential – Medium Profile (M-2) District;
- Multi-Residential – High Density Medium Rise (M-H2) District;
- Special Purpose – School, Park and Community Reserve (S-SPR) District;
- Special Purpose – Urban Nature (S-UN) District;
- Special Purpose – City and Regional Infrastructure (S-CRI) District;
- Special Purpose – Future Urban Development (S-FUD) District; and
- DC Direct Control District (Bylaw 87Z2007) based on RM-4 Residential Medium Density Multi-Dwelling District of Land Use Bylaw 2P80.

The proposed land use amendment proposes to redesignate the lands from the list of districts above, to:

- Residential – Low Density Mixed Housing (R-G) District;
- DC Direct Control District based on Residential – Low Density Mixed Housing (R-Gm) District;
- DC Direct Control District based on Multi-Residential – Medium Profile (M-2) District;
- Special Purpose – City and Regional Infrastructure (S-CRI) District;
- Special Purpose – School, Park and Community Reserve (S-SPR) District; and
- Special Purpose – Urban Nature (S-UN) District.

The proposed districts are located strategically throughout the plan area, with districts that allow for higher densities located in proximity to those that can provide for commercial, recreational and community service uses. Transit routes were also considered in the placement of districts. The proposed Land Use Map can be found in Attachment 4.

Subdivision Design

The proposed subdivision design (Attachment 3), which comprises approximately 74.68 hectares (184.53 acres), embraces a comprehensive open space system which is central to the plan area and creates a continuous user experience by incorporating passive and active, formal and informal areas for learning, playing, and living.

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

The proposed outline plan design generally follows a modified grid street pattern that allows for a variety of housing forms with a combination of laned and non-laned product types. Where a lane exists, front garages have been generally limited to improve the residential street interface and access conditions. A restrictive covenant will be registered against certain laned parcels through an outline plan condition. The block layout also supports the intensification of the neighbourhood over time by accommodating a variety of built forms. Anticipated lot widths vary from 5.0 metres for rowhouse buildings in the Residential – Low Density Mixed Housing (R-G) District to an average lot width of 12.2 metres for single detached dwellings in the R-G District.

The modified grid pattern has been adopted due to the open space corridor that is centrally located within the plan area. The central wetland is the iconic place making feature of the community and as such, several large publicly accessible access points to the wetland exist within the plan area. This will allow for everyone in the community to view the wetland and interact with it in a number of different passive and active ways.

Overall, the grid-based subdivision design provides a framework to accommodate a diversity of housing types and densities, open spaces, an integral naturalized storm pond, neighbourhood focal points, and community amenities. The subdivision design is tied together by an efficient system of multi-modal pathways and road networks. The proposed subdivision anticipates 1,645 units, with a maximum of 2,982 units possible.

There is a total of 7.54 hectares (18.63 acres) of Municipal School Reserve (MSR) dedicated to the required Joint Use Site (one elementary school and one elementary/junior high school), and 0.61 hectares (1.51 acres) of Municipal Reserve (MR) dedicated to open space. In total, 7.54 hectares (18.63 acres) of land is dedicated to MSR and MR, which is 11.4 percent of the gross developable area. Additional land has been dedicated above the required 10 percent through a deferred reserve caveat (DRC 111 100 837) transferred from Skyview Ranch and registered on title of the West 1/2 of the NW1/4 Sec. 23, Twp. 25, Rge. 29 W4M of the subject lands.

Density

Policy 7.2 of the *Northeast Community 'A' Area Structure Plan (ASP)* identifies that the minimum residential density of 17.5 units per hectare (7 units per acre) is required in each neighbourhood outside of the identified transit areas. The proposed outline plan proposes an anticipated total of 1,645 residential units with a maximum yield of 2,982 units. These unit counts equate to an anticipated density of 23.1 units per hectare (9.3 units per acre) with a maximum density yield of 41.9 units per hectare (16.9 units per acre), exceeding the requirements of the ASP.

The ASP also identifies a Neighbourhood Node in the south portion of the plan area that requires a minimum density of 50 units per hectare (20 units per acre). The proposal provides an anticipated density of 52.08 units per hectare (20.83 units per acre) and therefore exceeding the ASP requirement within this Neighbourhood Node area.

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

During the review of the application, it was determined that the broader community was over saturated with commercial development and that a commercial component would not be feasible within the Neighbourhood Node. For this reason, the live work component of the proposed DC Direct Control District found within the Neighbourhood Node is intended to satisfy the demand for local services provided within the node. The proposed outline plan and land use designations provide for development that exceeds both the MDP and the ASP minimum density and intensity targets within the identified Neighbourhood Node (Attachment 5).

Environmental

A Biophysical Impact Assessment was completed in conjunction with the original outline plan approval (LOC2006-0156) and Environmental Reserve (ER) boundaries were determined at that time. Two reconstructed stormwater wetlands were identified in the original approval to satisfy the wetland compensation agreement, one of which is in this stage 3 area. The reconstructed stormwater wetland in the subject plan area has been modified from the original design but the size of the associated ER has not been reduced. Updates to the original BIA findings were provided in a Preliminary Natural Site Assessment in support of this application and no additional recommendations or actions were required.

Transportation

Street and Mobility Network

The subject site is within the section of land bound by Country Hills Boulevard NE to the north, 60 Street NE to the east, Airport Trail to the south, and Metis Trail to the west. The internal road system will complete Cityscape Drive, the main circulatory collector within the subject area, and contains a modified grid network that connects with this collector road network.

Cityscape is partially built-out with existing community entrance roads (Cityscape Boulevard and Cityscape Gate) that connect Cityscape drive to Metis Trail NE and Country Hills Boulevard NE, respectively. New community entrances to Country Hills Boulevard and to 60 Street NE are planned as a component of this application.

The pathway within the west boulevard of 60 Street NE is part of the Rotary Mattamy Greenway system. The Greenway is planned to traverse the community, entering via the newly created community entrance road at 100 Avenue NE. The greenway, in combination with the planned regional pathway and local pathways, provide a comprehensive system around the wetlands in the community. The Greenway itself continues to the north into the adjacent community of Skyview Ranch. A multi-use pathway is also planned for Cityscape Drive, providing connectivity to the future LRT Station located at Country Hills Boulevard NE and 60 Street NE.

The plan area is well-connected to the regional transportation network and is in close proximity to both Stoney Trail NE and Airport Trail NE. In support of ongoing development in the area, the City of Calgary is currently constructing the extension of Airport Trail between Metis Trail NE and 60 Street NE. This roadway is anticipated to open by end 2019. The connection of 60 Street NE between Country Hills Boulevard and Airport Trail by area developers is required to tie into

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

the Airport Trail extension and is currently being designed with an anticipated opening of end-2021. These connections represent the only remaining missing links in the network for northeast Calgary, and once the projects are complete, residents of this and surrounding communities will benefit from having a comprehensive regional roadway network.

Transit

The Blue Line LRT extension from Saddletowne Station to Country Hills Boulevard is currently unfunded.

Skyview Ranch and Cityscape are currently serviced by Route 145, which provides connection to the Saddletowne LRT Station. The subject lands planned under this application could potentially be serviced by a slight modification to this route. The adjacent communities of Cornerstone and Redstone are currently serviced by Route 128.

The existing routes currently use Metis Trail as the north-south connection to Saddletowne LRT Station via 80 Avenue. The construction of 60 Street NE between Country Hills Boulevard NE and Airport Trail NE by area developers will provide significant benefit to the subject lands and neighbouring communities in terms of travel time and convenience for transit service, as it will reduce the distance required to connect to the existing Blue Line LRT.

In addition to the local transit service and the future Blue Line LRT extension, *RouteAhead* (Calgary Transit long-term strategic plan) identifies Primary Transit service along 52 Street NE between the Saddletowne LRT Station and the future (Green Line LRT) Seton Station. This service would connect to other cross-town Primary Transit services including MAX Orange (Rundle), MAX Purple (17 Avenue), and MAX Teal (future – 114 Avenue).

Utilities and Servicing

Water servicing for the plan area will be provided via extensions of the water distribution systems installed within Cityscape Stage 1 and Stage 2 as well as ties to adjacent developments.

Sanitary servicing for the north portion of the plan area will be provided via extensions of the Sanitary sewer installed as part of Cityscape Stage 1. Sanitary servicing for the south portion of the plan area will be provided via construction of a sanitary main in 60 Street NE which will flow north to the existing sanitary trunk in Country Hills Boulevard.

Overall stormwater servicing for the Cityscape development area will be provided via connections to two reconstructed wetlands located in the northwest (existing Pond F) and southeast corners of the development. Stormwater servicing for the north portion of the plan area will be provided via extensions of the storm sewer installed as part of Cityscape Stage 1 and will drain towards the northwest Pond F. Stormwater servicing for the south portion of the plan area will be provided via construction of the proposed southeast reconstructed wetland.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1391

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

Further water, sanitary, and storm servicing details for the plan area will be resolved at the tentative plan, construction drawing, and development permit stages.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online.

Administration received no comments in relation to the application. Currently, there is no community association for the area.

No public meetings were conducted by the applicant or Administration in relation to this application. Applicant-led public engagement was not deemed to be necessary given the greenfield nature of the plan area.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy directions of the *South Saskatchewan Regional Plan* which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the Interim Growth Plan. The proposed application builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Calgary International Airport Vicinity Protection Area Regulation (Statutory – 2009)

A portion of the subject site is located within the 25-30 Noise Exposure Forecast (NEF) contour of the Airport Vicinity Protection Area (AVPA) land use regulations. The proposed list of uses within the proposed land use districts are allowable within the 25-30 NEF contour area. Future development permits will be circulated to NAV Canada and reviewed against the applicable regulations to ensure alignment and compliance.

Municipal Development Plan (Statutory – 2009)

The *Municipal Development Plan* (MDP) identifies the plan area as Planned Greenfield with Area Structure Plan, referring to the *Northeast Community 'A' ASP*. Planned greenfield areas have generally been planned since 1990 and are low density residential in nature, with pockets of commercial land uses. It is noted that the site is located in proximity to a Major Activity Centre as identified in the MDP. As it was approved prior to adoption of the MDP, the *Northeast Community 'A' ASP* is recognized as appropriate policy to direct growth in the area.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1391

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

The Northeast Community 'A' Area Structure Plan

The *Northeast Community 'A' ASP* designates the application area for future residential development and includes an LRT alignment and associated stations. The proposed application meets the requirements of this Plan.

The proposed *Northeast Community 'A' ASP* identifies the plan area as Predominantly Residential Area with the following major components:

- two Joint Use Sites (JUS);
- one Neighbourhood Node;
- a portion of a Transit Station Planning Area (TSPA);
- one pedestrian overpass located over Airport Trail NE;
- a Wetland Conservation Area; and
- a regional Pathway system.

The subject application contains the required land uses and components as required by the ASP.

Social, Environmental, Economic (External)

The proposed outline plan will enable the continuation of development in the community of Cityscape and provide a future framework for a mix of housing types to support a broad cross section of the population. The development of these lands will enable a more efficient use of land and infrastructure, including the future Blue Line LRT Station, supporting surrounding uses and amenities while introducing additional amenities to the area.

Financial Capacity

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

There are no known impacts to the current and future operating budgets at this time.

Risk Assessment

There are no risks associated with this proposal.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1391

Outline Plan in Cityscape (Ward 05) at multiple addresses, LOC2018-0249(OP)

REASON(S) FOR RECOMMENDATION(S):

The outline plan is aligned with the policies of the *Northeast Community 'A' Area Structure Plan* and the *Municipal Development Plan*, creating the foundation for a complete community. The proposed land uses will accommodate residential, commercial, educational and recreational development throughout the plan area.

The conditions of approval (Attachment 1) effectively implement the technical aspects of the plan area's future development.

ATTACHMENT(S)

1. Conditions of Approval
2. Applicant's Submission
3. Proposed Outline Plan
4. Proposed Land Use District Map
5. Subdivision Data Sheet

Conditions of Approval

The following Conditions of Approval shall apply:

Planning:

1. All residential development within the 25-30 Noise Exposure Forecast (NEF) contour zones of the Calgary International Airport shall conform to the special soundproofing requirements of the Alberta Building Code and the standard City of Calgary and a NEF Restrictive Covenant shall be registered on all parcels to that effect;
2. **With each Tentative Plan**, the developer shall submit a density phasing plan indicating the intended phasing of Subdivision within the Outline Plan area and the projected number of dwelling units within each phase, demonstrating compliance with minimum required densities in accordance with the Area Structure Plan.
3. **In conjunction with the applicable tentative plan**, the Developer is responsible to ensure all infrastructure can be constructed in accordance with the applicable standards and regulations at the time of development.
4. Compensation for dedication of reserves in excess of 10% is deemed to be \$1.00.
5. If the total area for Roads and PUL dedication is over 30%, note that compensation will not be provided by The City for this over-dedication.
6. If a Residents Association is formed, it shall be comprised of all residents in the neighbourhood and all impacted titles within the plan area shall have a restrictive covenant registered on title identifying the financial and maintenance responsibility of said parcels to the Residents Association, all to the satisfaction of the Subdivision Authority and the Director, Parks.
7. The standard City of Calgary Party Wall Agreement regarding the creation of the separate parcels for semi-detached dwellings and rowhouse buildings shall be executed and registered against the titles concurrently with the registration of the final instrument.
8. Gated communities are not permitted in low density residential or multi-residential land use districts, in accordance with the General Rules for those districts in the Land Use Bylaw 1P2007.
9. Reserve owing from the phasing of the plan area shall be deferred by caveat to the residual lands.

Conditions of Approval

Development Engineering:

10. The parcels shall be developed in accordance with the development restriction recommendations outlined in the following report(s):
 - Geotechnical Evaluation Report, prepared by McIntosh-Lalani (File No ML2940.2), dated December, 2006, updated March 22, 2019.
11. **Concurrent with the registration of the final instrument**, execute and register on all parcels with double frontage lots that are adjacent to a collector road, a neighbourhood boulevard, an urban boulevard, an arterial road, a skeletal road, or a Transportation Utility Corridor, a Screening Fence Access Easement Agreement with the City of Calgary. The agreement and registerable access right of way plan shall be approved by the Manager, Infrastructure Planning and the City Solicitor **prior to endorsement of the final instrument**. A standard template for the agreement will be provided by the Development Engineering Generalist. Prepare and submit three (3) copies of the agreement for the City's signature.
12. This subject plan area is within the boundary of the Nose Creek drainage catchment and subject to stormwater volume control measures. Based on the Watershed Management Plan stormwater discharge is limited to 1.257 L/s/ha and average annual runoff volume is limited to 90mm. Low Impact Development and stormwater source control is recommended.
13. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.
14. **Prior to endorsement of any Tentative Plan/prior to release of a Development Permit**, execute a Development Agreement. Contact the Public Infrastructure Coordinator, Calgary Approvals Coordination for further information at 403-268-5706 or email galina.karadakova-mineva@calgary.ca.
15. Off-site levies, charges and fees are applicable. Contact the Public Infrastructure Coordinator, Calgary Approvals Coordination for further information at 403-268-5706 or email galina.karadakova-mineva@calgary.ca.
16. Make satisfactory cost sharing arrangements with LaCaille Group Inc. for part cost of the existing underground utilities and surface improvement installed/constructed in Country Hills Boulevard NE that was paid for and/or constructed by LaCaille Group Inc. under Skyview Ranch, Phase 01, DA2015-0018.

Conditions of Approval

17. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:

a) Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.

b) Construct the underground utilities and surface improvements in 60 Street NE, along the east boundary of the plan area.

Construct the interim at-grade intersection at Airport Trail and 60 Street NE.

c) Construct the underground utilities and surface improvements in Cityscape Drive NE, along the boundaries of the plan area.

d) Construct the underground utilities and surface improvements in Skyview Parkway NE, along the east boundary of the plan area.

e) Construct the onsite and offsite storm water management facilities (wet pond, wetlands, etc) to service the plan area according to the most current City of Calgary Standard Specifications Sewer Construction, Stormwater Management and Design Manual and Design Guidelines for Subdivision Servicing.

f) Construct a wood screening fence, chain link fence, sound attenuation fence, whichever may be required, along the boundary of the plan area.

g) Construct the MSR/MR within the plan area.

h) Construct the multiuse pathway and greenway within and along the boundaries of the plan area, to the satisfaction of the Director of Parks.

i) Street lighting.

Transportation:

18. **In conjunction with the initial Tentative Plan**, the Developer shall register a road plan to the satisfaction of the Director, Transportation Planning for the boundary half of 60 Street NE (four lanes Arterial street + Blue Line LRT right-of-way) from 100 Avenue NE to the north boundary of the Outline Plan, inclusive.
19. **In conjunction with the initial Tentative Plan**, the Developer shall construct the boundary half of 60 Street NE (southbound two lanes to ultimate stage plan) from

Conditions of Approval

the north boundary of the Outline Plan (north of 100 Avenue NE) to Airport Trail NE, inclusive. 60 Street NE and ancillary works (boulevard, noise attenuation, etc) necessary to support the plan shall be designed and constructed at the Developer's sole expense, subject to normal oversize, endeavours to assist, and boundary cost recoveries.

20. **In conjunction with the applicable Tentative Plan or Development Permit**, the Applicant shall submit construction drawings with cross sections and grading profiles to verify the ultimate grading requirements for the lands adjacent to 60 Street NE, Country Hills Boulevard NE, Metis Trail NE, and Airport Trail NE. **Prior to Approval of the Tentative Plan(s)**, the grading requirements adjacent to these streets shall be designed to the satisfaction of the Director, Transportation Planning.
21. **In conjunction with the applicable Tentative Plan**, the Applicant shall submit construction drawings with to verify the ultimate geometric design, bike lane treatments and associated right-of-way requirements for the primary collector to collector transition on Skyview Parkway, south of 108 Avenue NE. **Prior to Approval of the Tentative Plan(s)**, the geometric design and associated right-of-way requirements will be determined the satisfaction of Transportation.
22. **In conjunction with the applicable Tentative Plan**, detailed construction drawings with cross sections and grading profiles shall be submitted to verify the ultimate road right-of-way requirements for 100 Avenue, between 60 Street and Cityscape Drive NE. **Prior to Approval of the Tentative Plan**, the design for the abovementioned roadway and associated right-of-way requirements will be finalized to the Satisfaction of Transportation.
23. **In conjunction with the applicable Tentative Plan**, all residential street carriageway widths shall adhere to the requirements for emergency vehicles. For emergency vehicle access purposes, the 8.5m Residential M street carriageway width cannot be applied adjacent to lots with access to residential lanes; or adjacent to lot flankages.
24. **In conjunction with the applicable Tentative Plan**, the Developer shall enter into a Development Agreement for offsite improvements necessary to service the proposed development to the satisfaction of the Approving Authority.
25. **In conjunction with the applicable Tentative Plan**, the Developer shall register road plans for collector and residential standard streets within the subject lands to the satisfaction of the Director, Transportation Planning that provides continuous active modes and vehicle routing through the community with at least two points of public access around the Tentative Plan boundary connecting to the arterial street network.

Conditions of Approval

26. **In conjunction with the applicable Tentative Plan**, and prior to approval of construction drawings and permission to construct surface improvements, the Developer shall provide signed copies of backsloping agreements where private lands (including City of Calgary lands) are proposed to be impacted by the proposed development. Grading into City of Calgary road right-of-way to support proposed development is not permitted.
27. **In conjunction with the applicable Tentative Plan or Development Permit**, the Developer shall enter into a Construction Access Roads Agreement with Roads Maintenance.
28. **In conjunction with the applicable Tentative Plan**, bus bays shall be provided along Country Hills Boulevard NE and 60 Street NE to the satisfaction of the Director, Transportation Planning.
29. **In conjunction with the applicable Tentative Plan or Development Permit** for the staged construction of the road network, transit stops shall be provided to the satisfaction of the Director, Transportation Planning. All bus zones shall be located:
 - a) Where commercial areas are concentrated;
 - b) Where the grades and site lines are compatible to install bus zones; and
 - c) Where pedestrian walkways, pathways, and roadway crossing opportunities are provided.
30. **In conjunction with the applicable Tentative Plan or Development Permit**, temporary oil and gravel bus turnaround / cul-de-sac with a minimum radius of 15.25 meters is required at the terminus of each construction phase. Where the Developer intends to fence the turnaround, the minimum radius shall be increased to 16.25 meters. If road construction or construction phasing affects the operations of transit service, the Developer is required to provide an interim transit route replacement, to the satisfaction of the Director, Transportation Planning. Post and cable fence is required where the temporary turnaround is anticipated to be required for a period greater than one year.
31. A restrictive covenant shall be registered against the specific lot(s) identified by the Director, Transportation Planning concurrent with the final instrument prohibiting the construction of front driveways over the bus loading area(s).
32. No direct vehicular access shall be permitted to or from the sides of Cityscape Drive NE that contain a regional or multi-use pathway within the boulevard. A restrictive covenant shall be registered on all applicable titles **concurrent with the registration of the final instrument** to that effect at the applicable Tentative Plan stage.

Conditions of Approval

33. No direct vehicular access is permitted along 60 Street NE from the subject lands. A restrictive covenant shall be registered concurrent with the registration of the final instrument to that effect at the tentative plan stage.
34. No direct vehicular access is permitted along 96 Avenue (Airport Trail) NE from the subject lands. A restrictive covenant shall be registered concurrent with the registration of the final instrument to that effect at the tentative plan stage.
35. No direct vehicular access is permitted along Metis Trail NE from the subject lands. A restrictive covenant shall be registered concurrent with the registration of the final instrument to that effect at the tentative plan stage.
36. No direct vehicular access shall be permitted along the west side of Skyview Parkway for the DC(R-G) and R-G lots located between 108 Avenue and Skyview Road NE. A restrictive covenant shall be registered on all applicable titles **concurrent with the registration of the final instrument** to that effect at the applicable Tentative Plan stage.
37. No direct vehicular access shall be permitted to or from the south side of 100 Avenue between 60 Street and Cityscape Drive NE. Vehicular access shall be provided from rear lanes only. A restrictive covenant shall be registered on all applicable titles **concurrent with the registration of the final instrument** to that effect at the applicable Tentative Plan stage.
38. **In conjunction with the applicable Tentative Plan or Development Permit**, access for all multi-family, mixed-use, and commercial sites shall be designed and located to the satisfaction of the Director, Transportation Planning.
39. **In conjunction with the applicable Tentative Plan or Development Permit**, as well as in conjunction with any Land Use or Outline Plan amendments, further transportation analysis is required at the discretion and to the satisfaction of the Director, Transportation Planning.
40. **In conjunction with the applicable Tentative Plan** when the daily volumes reach 8,000 vehicles per day on Cityscape Boulevard NE, the Developer shall register a road plan to the satisfaction of the Director, Transportation Planning for the entirety (or remaining portions thereof) of Cityscape Drive NE from Cityscape Boulevard NE to 100 Avenue NE, inclusive. The Developer shall also construct the entirety of Cityscape Drive NE, from Cityscape Boulevard NE to 100 Avenue NE, inclusive. Cityscape Drive NE shall be designed and constructed at the Developer's sole expense.
41. **In conjunction with the applicable Tentative Plan or Development Permit**, the Developer shall provide a Letter of Credit or payment for pedestrian-actuated crossing signals that are agreed upon by the Developer and the Director,

Conditions of Approval

Transportation Planning. Pedestrian-actuated crossing signals shall be considered to the satisfaction of the Director, Transportation Planning as follows:

- a) where regional pathways or multi-use pathways intersect with a street;
- b) at mid-block crossings; and
- c) at intersections or pedestrian crossings adjacent to joint use sites.

The Developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit or payment, and is required to submit payment in support of the Tentative Plan or Development Permit.

- 42. **In conjunction with the applicable Tentative Plan**, the Developer shall ensure that all pathways from MR lands have direct pedestrian connection by sidewalk or pathway to crossing locations at intersections to the satisfaction of the Director, Transportation Planning.
- 43. **In conjunction with the applicable Tentative Plan**, sidewalks along a school site frontage shall be designed and constructed at the Developer's sole expense as mono-walks with a minimum width of 2.0 meters or mono-pathways with a minimum width of 3.5m. Note: the existing pathway along Country Hills Boulevard NE is exempt from this requirement.
- 44. For intersections adjacent to school sites and high-volume pedestrian crossing locations, curb extensions shall be provided to the satisfaction of the Director, Transportation Planning.
- 45. **In conjunction with the applicable Tentative Plan or Development Permit**, a noise attenuation study is required for residential development adjacent to all Arterial standard streets, certified by a Professional Engineer with expertise in the subject of acoustics related to land use planning, shall be submitted to Transportation Development Services for approval. Note that where sound attenuation is not required adjacent to Arterial streets, a uniform screening fence shall be provided to the satisfaction of the Director, Transportation Planning.
- 46. **In conjunction with the applicable Tentative Plan or Development Permit**, all noise attenuation features (noise walls, berms, etc) and ancillary facilities required in support of the development shall be constructed entirely within the development boundary (location of noise walls, berms, etc and associated ancillary works shall not infringe into the road right-of-way).
- 47. **In conjunction with the applicable Tentative Plan or Development Permit**, all community entrance features shall be located outside the public right-of-way.

Conditions of Approval

Parks:

48. **Prior to endorsement of the affected tentative plan**, under separate cover, the developer shall submit Landscape Construction drawings, for all reserve lands (ER, MR, MSR) within the Outline Plan area to Parks for review and approval. Landscape Construction Drawings shall follow the submission requirements outlined in the Parks' Development Standard Specifications: Landscape Construction (current version).

With the submission of Landscape Construction Drawings, the developer shall include a detailed Habitat Restoration Plan, including a maintenance schedule for all Environmental Reserve lands proposed to be affected by construction activity. The plan should indicate how it will be rehabilitated and restored. The restored area(s) shall be maintained by the developer until it is established and approved by Parks prior to Final Acceptance Certificate.

49. **Prior to endorsement of the first tentative plan or prior to release of the stripping and grading permit (whichever comes first)**, protection fencing must be installed completely within the subject site along the shared property line with adjacent Environmental Reserve (ER) lands. An onsite meeting shall be arranged to confirm that the fencing has been installed to the satisfaction of Parks Development Inspector. The protection fencing shall be maintained along shared property line until construction activity of the subject site has been completed.
50. **Prior to approval of the first tentative plan or stripping and grading permit (whichever comes first)**, an onsite meeting shall be arranged to confirm that the surveyed boundaries of the Environmental Reserve lands to Parks satisfaction. A plan illustrating the surveyed environmental reserve boundaries must be provided to Parks in advance of the onsite meeting.
51. Any damage to Municipal Reserve (MR/MSR) lands as a result of this development shall be restored at the developer's expense, to the satisfaction of Parks.
52. Any damage to Environmental Reserve lands as a result of this development shall be restored to natural state at the developer's expense, to the satisfaction of Parks.
- If disturbance occurs to Environmental Reserve lands, a Restoration Plan shall be submitted to Parks to the satisfaction of the Urban Conservation Lead.
53. Any damage to Environmental Reserve lands as a result of drainage or storm water infrastructure shall be restored to natural state at the developer's expense, to the satisfaction of Parks.

Conditions of Approval

54. Any damage to boulevards or public trees as a result of this development shall be restored at the developer's expense, to the satisfaction of Parks.
55. Any damage to the existing regional pathways within and along the boundaries of the plan area must be repaired at the developer's expense, to the satisfaction of Parks.
56. All impacts to pathways including Regional Pathways required for project execution shall adhere to the Pathway Closure and Detour Guidelines. Coordinate with Parks Pathways Lead, Ian Tucker at ian.tucker@calgary.ca or 403-573-7504, prior to the start of construction that impacts to the existing pathway system.
57. All landscape rehabilitation on Parks land and assets shall be performed and inspected in accordance with Parks' Development Guidelines and Standard Specifications: Landscape Construction (current edition). The disturbed area shall be maintained until establishment and approved by the Parks Development Inspector.
58. All landscape construction shall be in accordance with Parks' Development Guidelines and Standards Specifications: Landscape Construction (current version).
59. Construct all Regional Pathway routes within and along the boundaries of the plan area according to Parks' Development Guidelines and Standard Specifications: Landscape Construction (current version), including applicable setback requirements, to the satisfaction of Parks.
60. Development activities must ensure that suitable erosion and sedimentation controls are being implemented to protect our environment and drainage systems. Please refer to The City of Calgary's Guidelines for Erosion and Sediment Control for more information.
61. The Streets Bylaw (20M88) and the Tree Protection Bylaw (23M2002) contain clauses intended to protect trees growing on Public Land. No person shall remove, move, cut, or prune a Public Tree or cause a Public Tree to be removed, moved, cut or pruned without prior written authorization from the Director, Parks.
62. The applicant shall provide compensation to the City of Calgary for any Public Trees that are removed or damaged as a result of development activity.
63. Plant all public trees in compliance with the approved Public Landscaping Plan.

Conditions of Approval

64. The developer shall submit under separate cover, Utility Line Assignment Construction Drawings for trees installed within City of Calgary boulevards and/or right of ways to Utility Line Assignment and Parks for review and approval.

No person shall plant trees or shrubbery on City Lands without prior written authorization from the Director, Calgary Parks and in the case of walkways, medians, boulevards, and road rights of way, without additional prior written authorization from the Director, Development Engineering

65. Any tree planting in the City boulevard shall be performed and inspected in accordance with Parks' Development Guidelines and Standard Specifications: Landscape Construction (current version).
66. Low Impact Development (LID) drainage components shall not conflict with pathways.
67. All proposed site fencing adjacent to or abutting reserve lands (ER, MR, MSR), including footings and other components, shall be installed completely within private property.
68. A 1.2 m fence (or Parks approved alternative) shall be maintained along the boundary of the adjacent reserve lands (ER, MR, MSR), where applicable, for the duration of the development.
69. Drainage from the development site into adjacent reserve lands (ER, MR, MSR) is not permitted, unless otherwise authorized by Parks in writing.
70. Construction access through Environmental Reserve lands is not permitted, unless otherwise authorized by Parks in writing.
71. Stockpiling or dumping of construction materials on Environmental Reserve lands is not permitted, unless otherwise authorized by Parks in writing.
72. Retaining walls within reserve lands (ER, MR, MSR) are not permitted, unless otherwise authorized by Parks in writing.
73. Site grading of the development site shall match the grades of adjacent Environmental Reserve lands with all grading confined to private property, unless otherwise authorized by Parks in writing.
74. Backsloping from the development site into adjacent reserve lands (ER, MR, MSR) is not permitted, unless otherwise authorized by Parks in writing.
75. Surface or subterranean encroachment from the development site into the adjacent Environmental Reserve lands is not permitted. Any unauthorized

Conditions of Approval

encroachment will require removal and restoration at the developer's expense, to the satisfaction of Parks.

76. The alteration of any water body (including Class 1 – 6 wetlands) is subject to a *Water Act* approval from the Province. Until receipt of the *Water Act* approval by the applicant from Alberta Environment, the wetland(s) shall not be developed or disturbed in anyway and shall be protected in place.
77. Municipal Reserve lands in proximity to or abutting Environmental Reserve lands may require the use of native planting species only. Subject to further review and sole discretion of Parks at the Landscape Construction drawing stage.
78. Prior to any disturbance or construction activity within the Outline Plan area, a nesting and breeding bird survey may be required, should the removal of trees or other vegetation take place during the nesting period of migratory bird species (April 15 to August 20). Sweeps for active nests or other sensitive wildlife features should be conducted at least 6 days prior to start of construction, and appropriate mitigation measures taken as per direction from Alberta Environment wildlife division, where applicable, to be in compliance with the Wildlife Act and the Migratory Birds Convention Act.

Applicant's Submission

22 October 2019

Cityscape Stage 3 - Land Use Redesignation

APPLICANT'S SUBMISSION

Mattamy is submitting an Outline Plan & Land Use amendment application for the development of a new neighbourhood located in the northeast sector of the City of Calgary. The subject site is 184 acres in size and comprises the remaining lands owned by Mattamy in the community of Cityscape. Cityscape a thriving and connected residential community with abundant open space amenities and a unique approach to housing and streetscapes.

The proposed land use redesignation includes R-G for low density residential uses, and S-UN, S-CRI and S-SPR for environmental reserve, public utility lot and municipal reserve respectively. Two Direct Control Districts are proposed which is based on the existing DC District approved in Cityscape Stages 1 & 2, which allows for Mattamy's unique Street-Oriented Laned Row Housing as well as another for multi-residential development.

At the heart of the community is an existing scenic 115 acre wetland. This wetland is integrated into the community through the connected open space and pathway network. Stage 3 will connect to and improve upon the existing pathway network including the Rotary/Mattamy Greenway which is a 138 km multi-use pathway around the city of Calgary. Stage 3 of Cityscape will build upon and expand the existing range of amenities in the community, providing open space in excess of the required 10% municipal reserve. A Joint- Joint Use School Site (JJUS) is located in the north portion of the plan near a future transit station. This site includes two school buildings with shared playfields and a cricket pitch. There are also three sub-neighbourhood parks spread throughout the plan area, as well as a reconstructed wetland and stormwater management pond in the southeast portion of the plan.

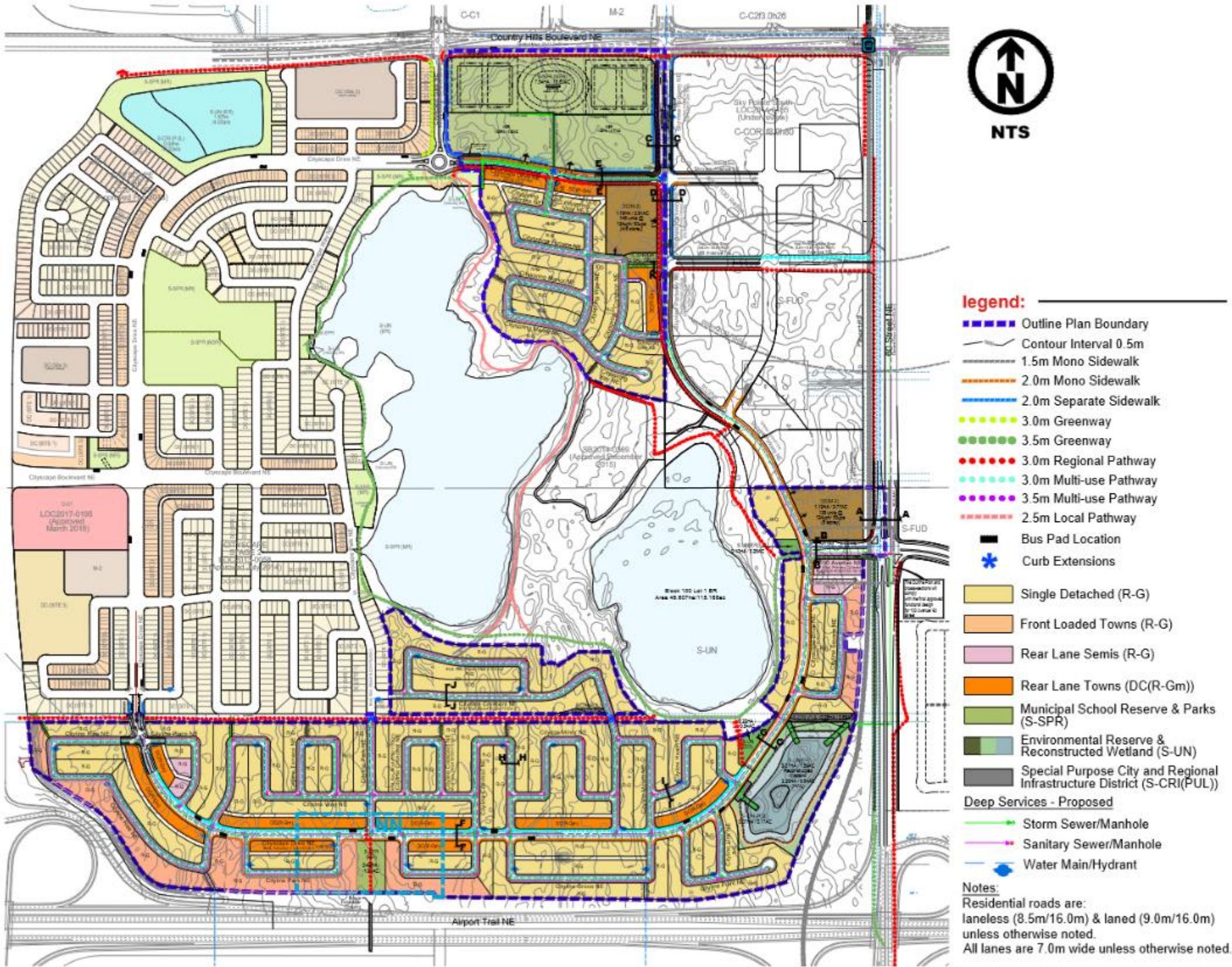
The plan area employs a rectilinear road network comprised of interconnected streets, supported by walkways and linear green spaces to provide connectivity through the community. In keeping with Cityscape Stages 1 & 2, the plan will include architectural and design elements that enhance the streetscape. Laned housing types are placed along collector roads, with uniform and minimal housing setbacks to create a consistent streetscape. Park spaces are centrally located adjacent to major routes and garden lots front onto two of them to highlight them as public gathering spaces.

Mattamy offers a range of unique housing products to the Calgary market and a distinctive development philosophy. Mattamy recognizes that density and efficient use of land are important, but also that the market demand for multi-residential is lower in the northeast sector. By providing a range of single detached, semi-detached and townhome products on smaller and more efficient lots, Cityscape caters to consumers who want fee-simple ownership with greater affordability and reduced maintenance. Multi-residential housing is also proposed, strategically located close to open space within the plan area and to future commercial, recreation and transit in the anticipated Major Activity Centre to the northeast.

The proposed concept plan requires an amendment to the Northeast Community 'A' ASP due to the relocation of the two school sites within the plan area to a single Joint-Joint Use Site. This JJUS is located in close proximity to the future MAC and transit planning area to the east of the plan area, and is surrounded on all sides by regional pathways. The ASP shows one of these two school sites in the south portion of the plan area, but Administration expressed concerns about its proximity to the operating natural gas pipeline corridor; relocating this school to the north portion of the plan will alleviate these concerns. In addition, relocation of the schools to this proposed JJUS location allows for more efficient sharing of playfields including the proposed cricket pitch, which allows for other Municipal Reserve to be distributed more equitably about the plan area as other types of park space. The proposed relocation of the JJUS was supported by Site Planning Team in June 2019.

Overall, the proposal aligns with overarching policies in the Calgary Municipal Development Plan and New Community Design Guidebook and specific policies of the Northeast Community 'A' Area Structure Plan. At a projected density of 9.3 units per acre, the proposal exceeds density thresholds. Additionally, the plan area aligns with the existing vision for a unique and attractive community as set out in Cityscape Stages 1 & 2.

Proposed Outline Plan



client:

sub-consultant:

URBANSYSTEMS
NAK **WATT**
design strategies Consulting Group Since 1988

revisions:

no:	date:	description:
1	13-Nov-2018	Submission
2	29-May-2019	DTR#1 Submission
3	09-Sept-2019	DTR#2 Submission
4	17-Oct-2019	DTR#3 Submission

location map:

*** Subject Site**
municipal address:
5051 & 5555 County Hills Blvd. NE
& 10011 52 Street NE

legal description:
Portion of Sec 22& Sec 23-25-29-W4

file description:

pre-app:	PE2018-01483
LOC:	LOC2018-0249
bylaw no.:	--

file info:

project no.:	C1448d
drawn by:	es
start date:	2017
current date:	Nov 08, 2019

project:

Proposed Outline Plan

OUTLINE PLAN STATISTICS

OUTLINE PLAN STATISTICS					
	frontage (m)	lot width/ unit per acre	hectares	acres	no. of units of GDA
TOTAL AREA			74.66	184.49	
Less Environmental Reserve (S-UN (ER))			0.32	0.79	8.53
Less Reconstructed Wetland (S-UN(ER))			3.13	7.73	
GROSS DEVELOPABLE AREA (GDA)			71.21	175.96	100.0%
RESIDENTIAL			42.07	103.96	59.1%
LOW DENSITY MIXED HOUSING DISTRICT			39.77	98.27	58.2%
Single Detached Dwellings (R-G)					
Minimum/Anticipated number of lots based on average lot width 12.20 m	11437	12.20	31.85	78.21	937
Maximum number of lots based on average lot width 6.00 m		6.00			1906
Front Loaded Townhomes (R-G)					
Minimum/Anticipated number of lots based on average lot width 7.00m	1136	7.00	4.00	9.88	162
Maximum number of lots based on average lot width 5.00 m		5.00			227
Rear Lane Townhomes (DC(R-Gm))					
Minimum/Anticipated number of lots based on average lot width 7.00m	1616	7.00	3.47	8.57	230
Maximum number of lots based on average lot width 5.00 m		5.00			323
Rear Lane Semis (R-G)					
Minimum/Anticipated number of lots based on average lot width 7.00 m	218	7.00	0.65	1.61	31
Maximum number of lots based on average lot width 5.00 m		5.00			43
MULTI-RESIDENTIAL - HIGH DENSITY LOW RISE DISTRICT					
Condominium Apartments DC(M-2)			2.30	5.68	3.2%
Minimum / Anticipated number of units based on 124uph / 50 upa (by bylaw)			124	2.30	5.68
Maximum number of units based on 210 uph / 85 upa					483
Total Number of Lots	14407	210			
Minimum / Anticipated					1645
Maximum					2982
Density Range					
Minimum / Anticipated			23.1 uph		9.3 upa
Maximum			41.9 uph		16.9 upa
SPECIAL PURPOSE SCHOOL, PARK AND COMMUNITY RESERVE DISTRICT (S-SPR)			8.15	20.14	11.4%
JUS (MSR)			7.54	18.63	
Parks (MR)			0.61	1.51	
PUBLIC UTILITY LOTS AND ROADS (S-CRI (PUL))			20.99	51.87	29.5%
Public Utility Lots			0.42	1.04	0.6%
Roads and Lanes			20.57	50.83	28.9%
Widening of Divided Major (2x11.70m/51.00m) - Country Hills Boulevard NE Modified Arterial (10.50m + 8.00m/46.00m (portion) Half of Modified Primary Collector (2x6.80m/31.24m) - Skyview Parkway NE Mod. Primary Collector (6.60+8.90m/35.15m) with Greenway - 100 Avenue NE Mod. Primary Collector (2x6.80/28.00m) Skyview Parkway NE Mod. Primary Collector (11.10/22.30m) - Cityscape Drive NE Mod. Collector (10.80m/22.00m) - Cityscape Drive NE Collector (10.80m/21.00m) - Cityscape Drive NE Residential Street No Rear Lane (8.50m/16.00m) & Residential Street with Rear Lane (9.00m/16.00m) Mod Residential Street (8.50m/15.50m) 5.0m ROW & Lanes 7.00m					

PROPOSED LAND USE REDESIGNATION - KEY MAP



SUMMARY - PROPOSED LAND USE REDESIGNATION

	ha	ac
R-G	50.87	125.70
DC(R-Gm)	6.50	16.06
DC(M-2)	3.42	8.44
S-SPR	9.38	23.18
S-UN	1.15	2.84
S-CRI	0.51	1.26
TOTAL	71.83	177.50

EXISTING LAND USE - KEY MAP

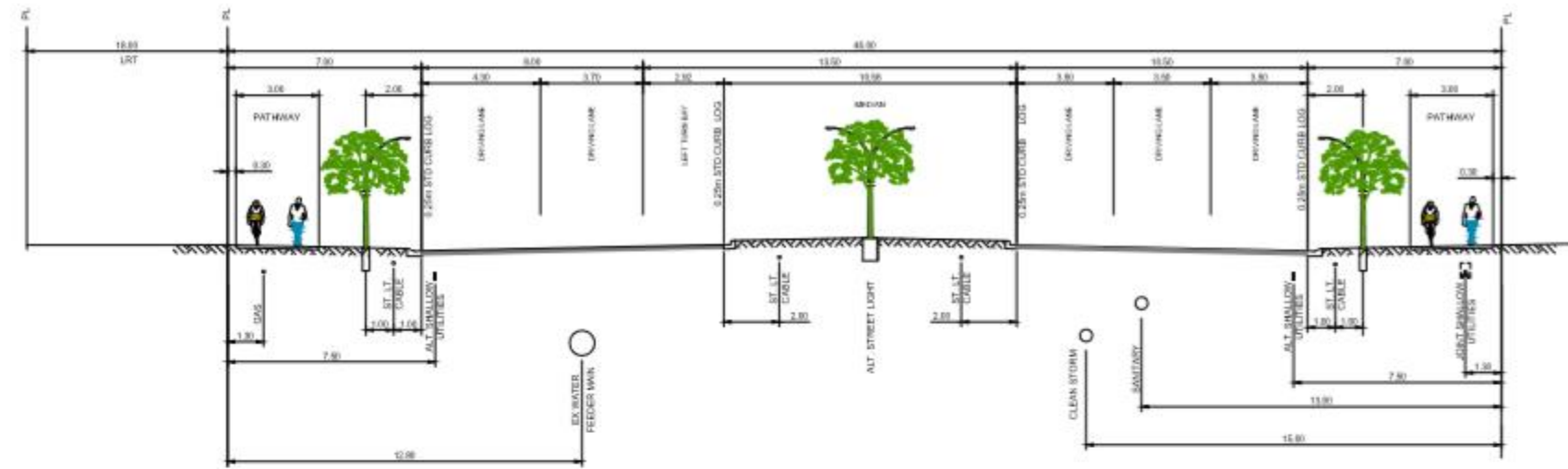


LAND USE REDESIGNATION

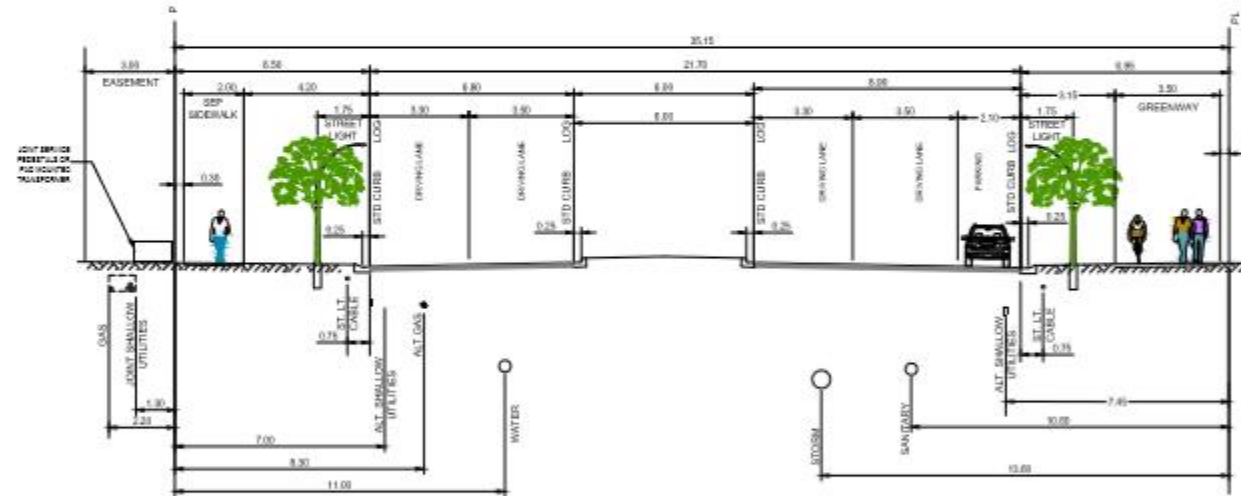
	FROM	TO	HA	AC
1	R-1N	R-G	26.52	65.53
	R-1N	DC(R-Gm)	3.73	9.22
	R-1N	S-CRI	0.12	0.30
	R-1N	S-SPR	0.55	1.36
	R-1N	S-UN	0.39	0.96
2	R-2M	DC(R-Gm)	0.26	0.64
	R-2M	R-G	2.95	7.29
	R-2M	S-UN	0.06	0.15
3	DC8322007 site 3	DC(R-Gm)	0.91	2.25
	DC8322007 site 3	R-G	1.2	2.97
4	R-2	R-G	1.14	2.82
5	R-1	R-G	4.57	11.29
	R-1	S-CRI	0.02	0.05
6	M-H2	S-SPR	8.71	21.52
	M-H2	DC(R-Gm)	0.25	0.62
7	M-1	R-G	2.95	7.29
	M-1	DC(M-2)	0.08	0.20
	M-1	S-CRI	0.26	0.64
	M-1	S-UN	0.35	0.86
	M-2	R-G	3.38	8.35
8	M-2	DC(R-Gm)	0.19	0.47
	S-SPR	R-G	4.45	11.00
	S-SPR	DC(M-2)	1.50	3.71
	S-SPR	DC(R-Gm)	0.78	1.93
	S-SPR	S-CRI	0.06	0.15
9	S-SPR	S-SPR	0.24	0.59
	S-UN	S-UN	2.43	6.00
	S-UN	S-CRI	0.05	0.12
	S-UN	R-G	0.34	0.84
	S-CRI	S-UN	0.35	0.86
11	S-CRI	R-G	0.47	1.16
	S-CRI	DC(R-Gm)	0.19	0.47
	S-FUD	R-G	2.9	7.17
12	S-FUD	DC(R-Gm)	0.19	0.47
	S-FUD	DC(M-2)	1.84	4.55
	S-FUD	S-SPR	0.12	0.30
TOTAL			74.50	184.09
LU (unchanged)			2.67	6.60
TOTAL LAND USE REDESIGNATION			71.83	177.50
OUTLINE PLAN BOUNDARY			74.66	184.49
ROADS			0.16	0.40
TOTAL LU REDESIGNATION			71.83	177.50



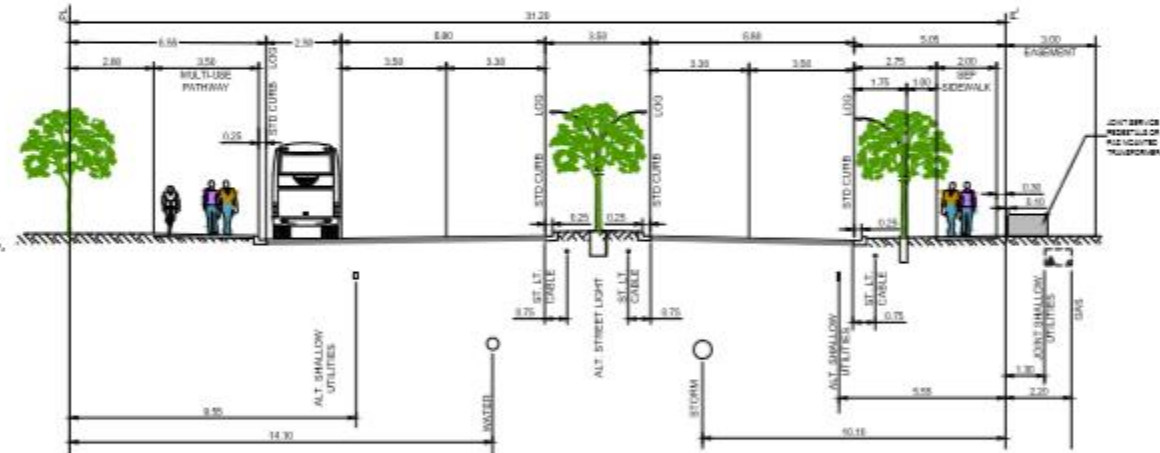
Proposed Outline Plan



A-A: MODIFIED ARTERIAL STREET - 10.5m+8.0m/46.00m
60 Street NE

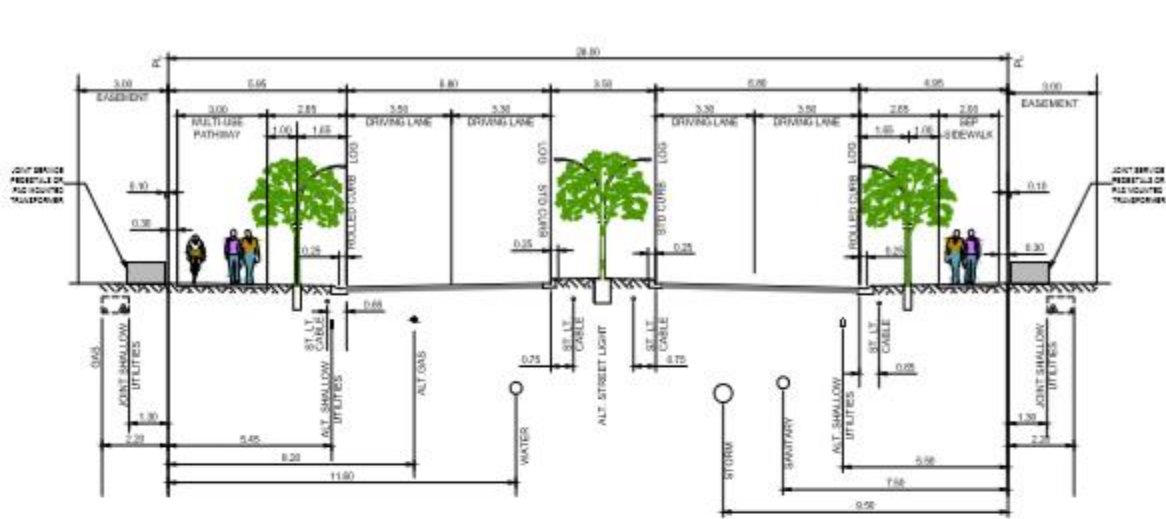


B-B: MODIFIED PRIMARY COLLECTOR STREET
6.60m+8.90m/35.15m - 100 Avenue NE

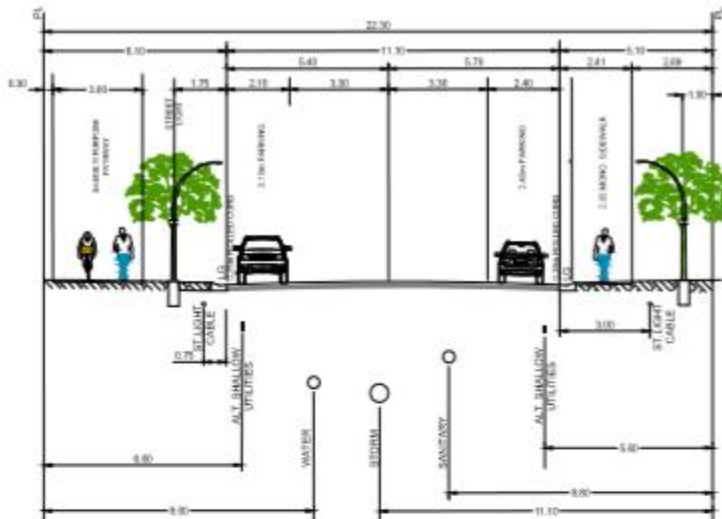


C-C: MODIFIED PRIMARY COLLECTOR STREET - 9.14m+6.80m/31.20m
Four Driving Lanes (between Country Hills Boulevard NE
and Cityscape Drive NE) Skyview Park NE

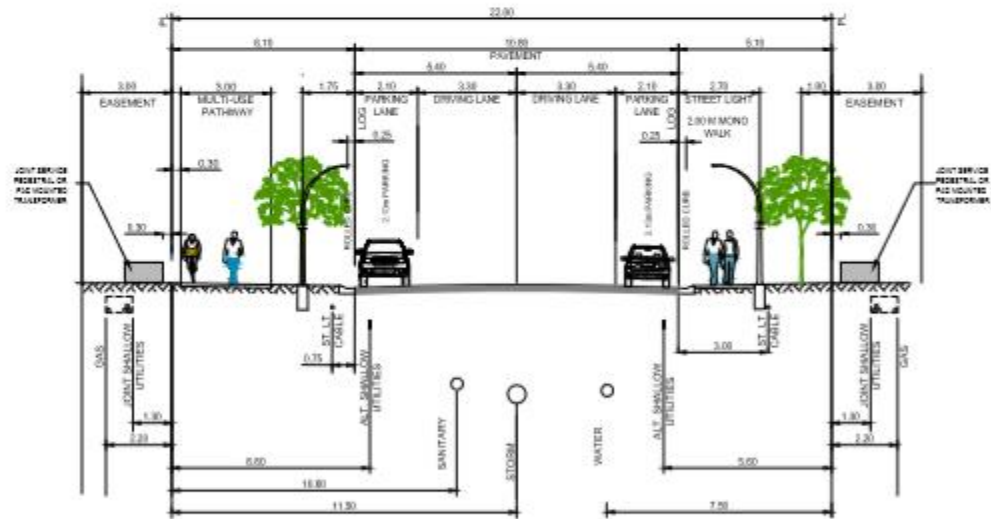
Proposed Outline Plan



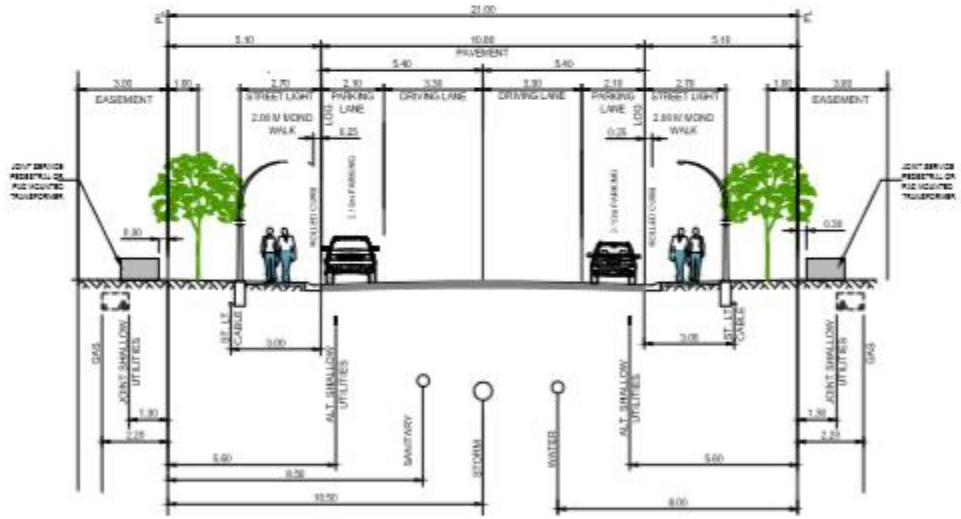
D-D: MODIFIED PRIMARY COLLECTOR STREET - 2x6.80m/28.00m
Four Driving Lanes (between Cityscape Drive SW & 108th Avenue NE)



E-E: MOD. COLLECTOR STREET - 11.10m/22.30m
Two Driving Lanes & Regional Pathway on South Side



F-F: COLLECTOR STREET - 10.8m/22.00m
Two Driving Lanes & Multi-Use Pathway

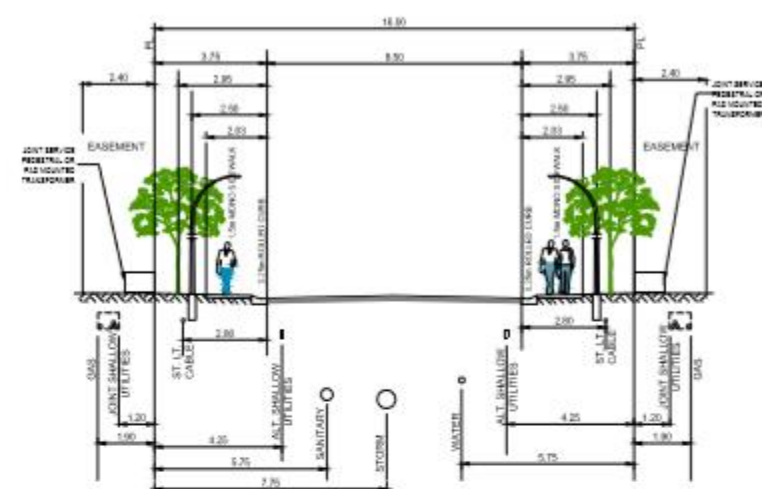


G-G: COLLECTOR STREET - 10.8m/21.00m
Two Driving Lanes

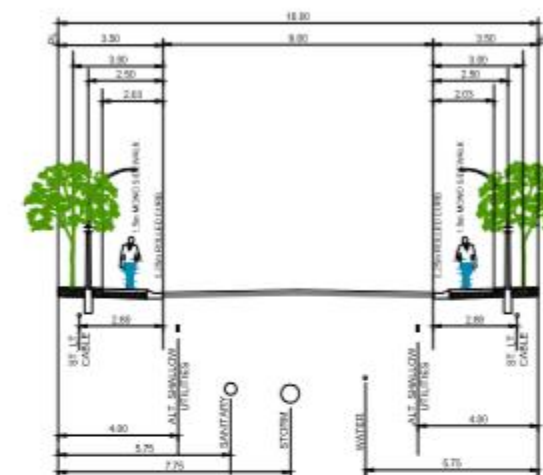


LOC2018-0249
Cross Sections 2-3

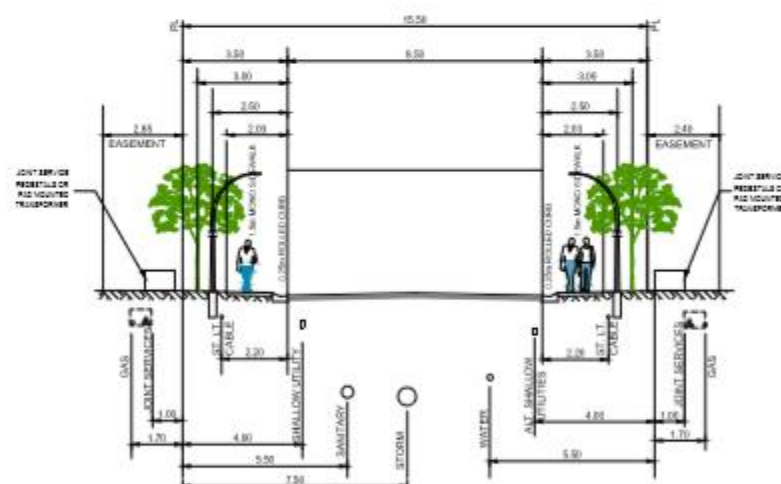
Proposed Outline Plan



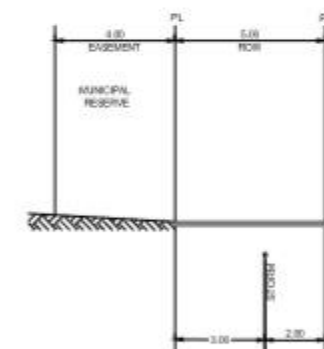
H-H: RESIDENTIAL STREET
8.50m/16.00m - No Rear Lane



I-I: RESIDENTIAL STREET
9.00m/16.00m - With Rear Lane

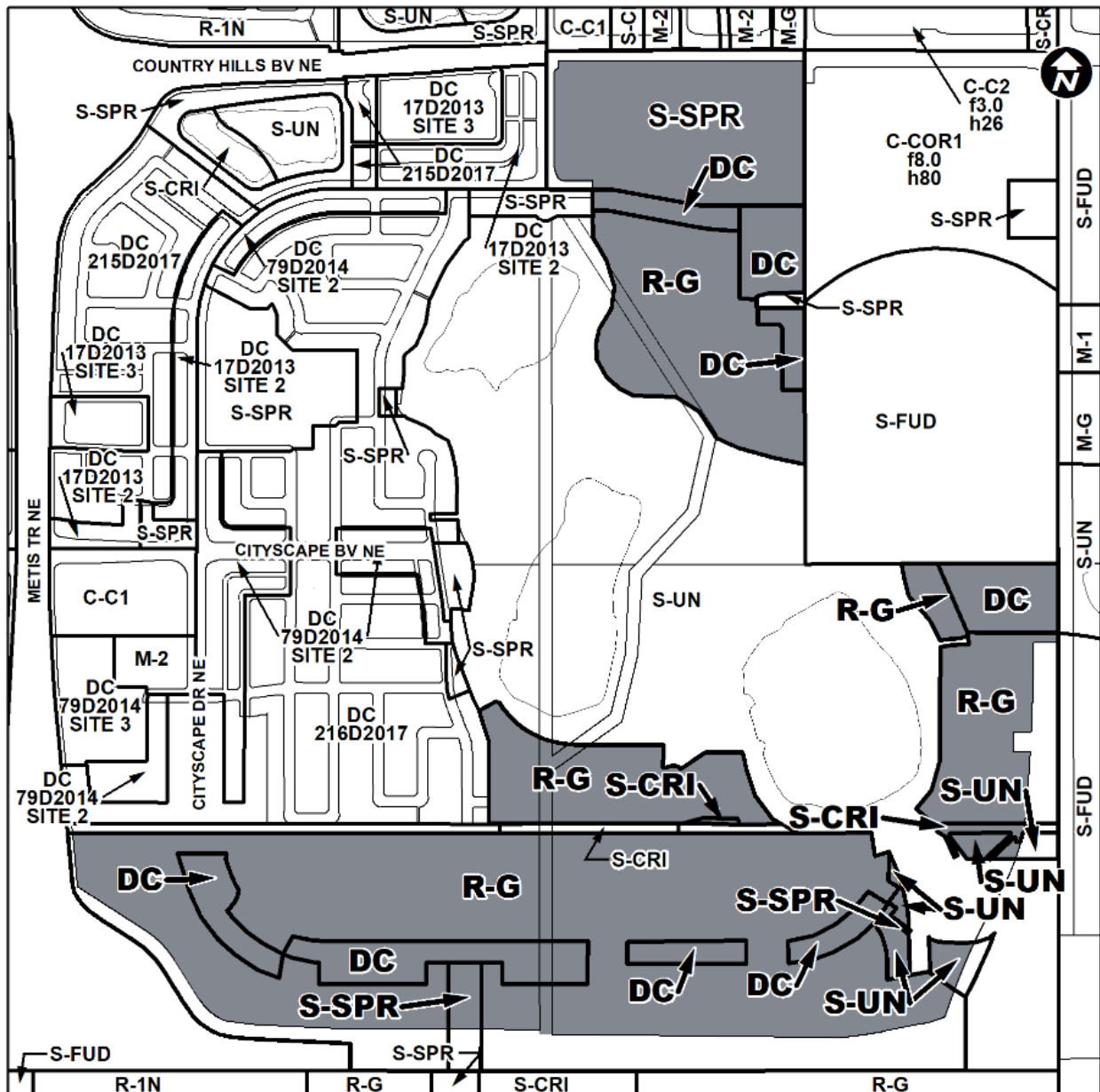


J-J: MODIFIED RESIDENTIAL STREET
8.50m/15.50m No Rear Lane



K-K: 5.0m
Road ROW

Proposed Land Use District Map



Subdivision Data Sheet

	HECTARES	ACRES
GROSS AREA OF PLAN	74.66	184.49
LESS: ENVIRONMENTAL RESERVE	3.45	8.53
NET DEVELOPABLE AREA	71.21	175.96

LAND USE (Residential / Mixed Use)	HECTARES	ACRES	# OF LOTS	# OF UNITS (FOR MULTI RESIDENTIAL)
R-G	36.3	89.70	1130	1130
DC (R-Gm)	3.47	8.57	230	230
DC (M-2)	2.30	5.68	2	285
Total Residential / Mixed Use	42.07	103.95	1362	1645

	HECTARES	ACRES	% OF NET AREA
ROADS	20.57	50.83	28.9%
PUBLIC UTILITY LOT	0.42	1.04	0.6%

RESERVES	HECTARES	ACRES	% OF NET AREA
MR	0.61	1.51	0.9%
MSR	7.54	18.63	10.5%

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1398

Land Use Amendment in Crestmont (Ward 1) at multiple addresses,
LOC2019-0061

EXECUTIVE SUMMARY

This application was submitted by Stantec Consulting on behalf of the landowners, Crestmont Developments Inc, on 2019 May 01. The application proposes to change the designation of 22 properties from DC Direct Control District (Bylaw 17D2008) to Residential – One Dwelling (R-1) District to allow for:

- single detached dwellings that may contain secondary suites (no change from the current district);
- a maximum building height of 12 metres (an increase from the current maximum of 11 metres);
- maximum of one dwelling unit per parcel (no change from the current maximum of one dwelling unit); and
- the uses listed in the R-1 District.

The applicant originally applied to amend the current DC Direct Control District in order to re-align the boundary between Sites 1, 2, and 3 to reflect an approved Subdivision; however, Administration recommended that redesignating to a standard Land Use Bylaw district that achieves the same goal would be more appropriate.

The proposal is in keeping with the applicable policies of the *Municipal Development Plan*, the *Rocky View / Calgary Intermunicipal Development Plan*, and the *Calgary West Area Structure Plan*. No development permits have been submitted at this time.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommends that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed redesignation of 9.88 hectares \pm (24.41 acres \pm) located at various addresses (Attachment 1) from DC Direct Control District to Residential – One Dwelling (R-1) District; and
2. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

As a result of the 2007 City-wide annexation of lands from Rocky View County, the subject parcels were redesignated from DC Direct Control 103 within Rocky View County Land Use Bylaw (C-4841-97) to a DC Direct Control District (Bylaw 17D2008) within City of Calgary Land Use Bylaw 1P2007 in May 2008 (LOC2008-0006 / CPC2008-040).

Approval(s): **S. Lockwood** concurs with this report. Author: **L. Ganczar**

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1398

**Land Use Amendment in Crestmont (Ward 1) at multiple addresses,
LOC2019-0061**

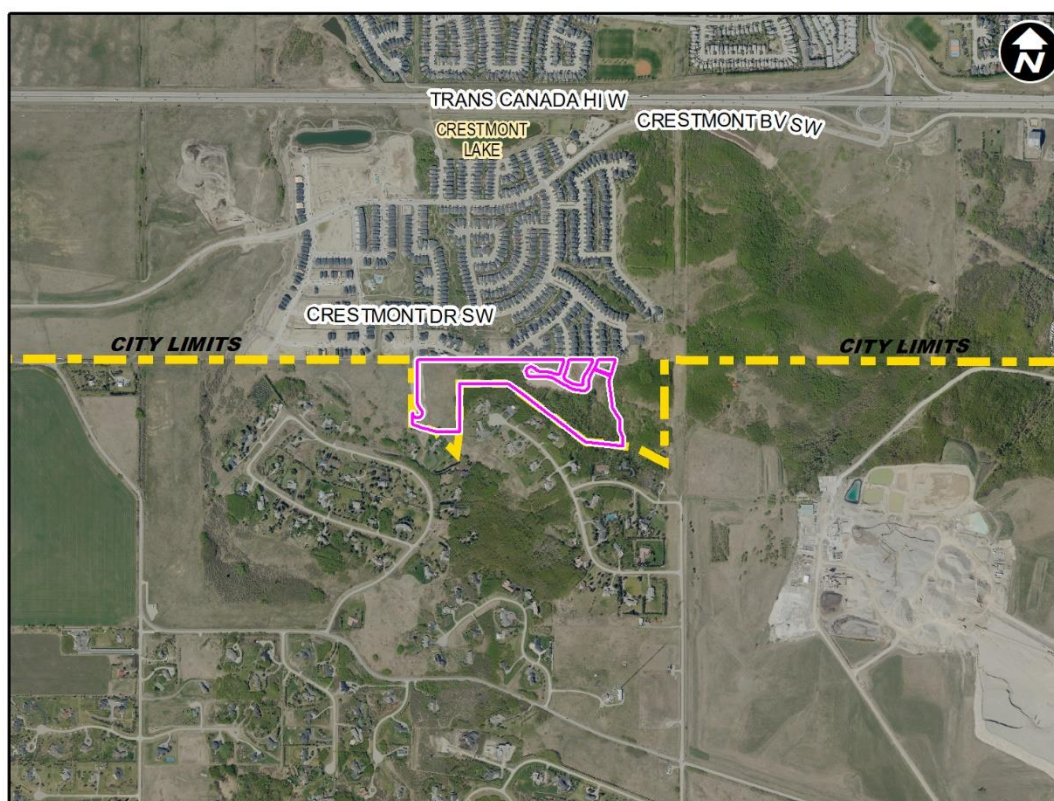
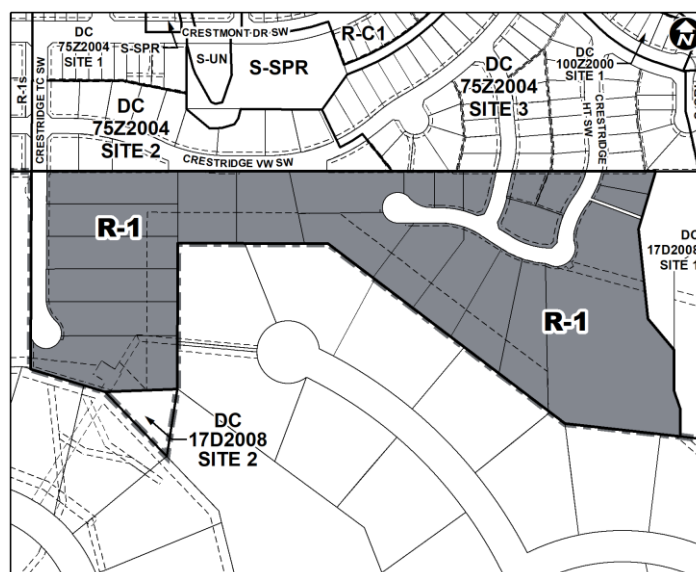
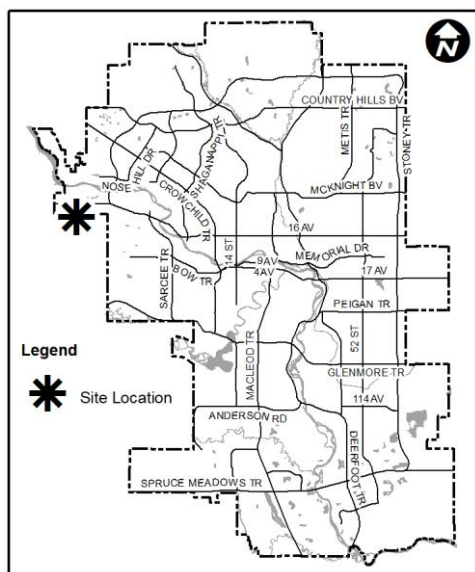
The current DC Direct Control District was adopted to fulfill The City of Calgary / M.D. of Rocky View Annexation Agreement. The lands were governed by the Crestmont Conceptual Scheme, a policy document approved by Rocky View County and inherited by The City of Calgary. They are now governed by the *Calgary West Area Structure Plan* (ASP).

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1398

Land Use Amendment in Crestmont (Ward 1) at multiple addresses,
LOC2019-0061

Location Maps



**Land Use Amendment in Crestmont (Ward 1) at multiple addresses,
LOC2019-0061**

Site Context

The subject site is located in the southwest community of Crestmont, immediately north of the Artists View neighbourhood in Rocky View County. The existing DC Direct Control District allows for a maximum of one dwelling unit on the site with one secondary suite. The subject site is approximately 9.88 hectares in size consisting of 22 subdivided parcels. The site is currently undeveloped.

Surrounding development consists of low density residential dwellings to the north in the community of Crestmont, country residential development to the south in Rocky View County, and undeveloped, rural land to the west and east in Rocky View County.

As identified in *Figure 1*, the community of Crestmont has seen the population generally increase since development first began in 2001. The population has remained relatively stable in the last seven years.

Figure 1: Community Peak Population

Crestmont	
Peak Population Year	2015
Peak Population	1.542
2018 Current Population	1.514
Difference in Population (Number)	-28
Difference in Population (Percent)	-1.8%

Source: *The City of Calgary 2018 Civic Census*

Additional demographic and socio-economic information may be obtained online through the [Crestmont](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal allows for the development of single detached dwellings on large parcels that maintain an appropriate transition from country residential to urban residential within a standard district of the Land Use Bylaw. The proposal meets the objectives of applicable policies as discussed in the Strategic Alignment section of this report.

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration.

**Land Use Amendment in Crestmont (Ward 1) at multiple addresses,
LOC2019-0061**

Land Use

The existing DC Direct Control District ([Bylaw 17D2008](#)) is intended to accommodate residential development in the form of single detached dwellings on parcels of varying size in order to establish an interface between country and urban residential development. Single detached dwellings may include a secondary suite. The DC Direct Control District allows for a maximum building height of 11 metres and a maximum of one dwelling unit per parcel.

The proposed Residential – One Dwelling (R-1) District allows for a maximum of one dwelling unit that may include a secondary suite, and a maximum building height of 12 metres. The proposed R-1 District also allows for additional home-based uses that complies with the intent of the Calgary West Area Structure Plan.

Development and Site Design

The rules of the proposed R-1 District will provide guidance for future site development including appropriate uses, building massing, height, landscaping and parking.

Environmental

An Environmental Site Assessment was not required as part of this application. There are no environmental concerns associated with the site or this proposal.

Transportation

The Transportation department was circulated as part of the initial review of this application. There are no transportation concerns associated with the proposal.

Utilities and Servicing

Water, sanitary, and storm sewer mains are available and can accommodate potential redevelopment of the subject site without the need for off-site improvements at this time. Individual servicing connections as well as appropriate stormwater management will be considered and reviewed as part of a development permit.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to external stakeholders, including the Community Association, and notice posted on-site. Notification letters were sent to adjacent landowners and the application was advertised online. The applicant did not pursue any neighbourhood engagement for the proposed land use amendment as per their Applicant Submission (Attachment 2).

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1398

**Land Use Amendment in Crestmont (Ward 1) at multiple addresses,
LOC2019-0061**

Administration received 12 letters in objection to the proposal. The one concern heard specific to the proposed redesignation is the one metre increase in building height. Other concerns were about development in general in this area of the City, including disturbance to wildlife corridors, potential hillside erosion, removal of trees, and impacts on the water table. Upon consideration, a land use redesignation from the current DC Direct Control District to the R-1 District will not impact these concerns.

Administration did not receive any correspondence from the Crestmont Community Association. Following Calgary Planning Commission, notifications for Public Hearing of Council will be posted onsite and mailed out to adjacent land owners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSRP) which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

The proposed land use amendment complies with the MDP policies applicable to the Developing Residential Areas (Section 3.6), and also meets several broader city-wide objectives such as providing greater housing choices, and facilitating the availability of competitively priced, easily serviceable and developable land for residential purposes.

Rocky View / Calgary Intermunicipal Development Plan (Statutory – 2012)

These lands are subject to the *Rocky View / Calgary Intermunicipal Development Plan* (IDP) and, as such, the application was circulated to Rocky View County for comment in accordance with the requirements of the IDP. A response was received indicating they had no concerns with the application as the proposed redesignation is similar in intent to the original DC district.

The subject site falls within the Highway 1 West Corridor Key Focus Area of the IDP. This focus area recognizes the importance of the country / urban residential interface between the community of Crestmont and the community of Artists View in Rocky View County.

**Land Use Amendment in Crestmont (Ward 1) at multiple addresses,
LOC2019-0061**

Section 6.0 of the IDP provides policies for facilitating compatible land use transitions across the municipal boundary. These include:

- Applications for redesignation, subdivision, or development should be evaluated with respect to adjacent existing and planned uses across the municipal boundary.
- Respect existing and planned land uses across the municipal boundary and mitigate nuisance factors.
- Transition tools that may be used to mitigate impacts and provide an appropriate transition are listed below. This list is not exhaustive and other methods may be appropriate. These transition tools are intended to be used in combination.

Transition tools include density, land use, phasing, open space, landscaping, fencing, and site / building design, such as:

- building orientation;
- site coverage;
- building height;
- setbacks;
- form and massing; and
- lighting.

Calgary West Area Structure Plan (Statutory – 2014)

The subject lands fall within the Neighbourhood Area of the ASP. Section 2.3 provides policy for appropriate development within the designated area. Applicable policies include:

- provide a mix of built forms and housing types that support different income levels and age groups; and
- include opportunities for residential-based commercial uses such as live-work units, home-based businesses, and child care.

While the overall density of the approved subdivision is below what is anticipated in the ASP, a redesignation to the R-1 District will actually allow for further subdivision in the future as the minimum lot area in the current DC District (4000 square metres) is significantly larger than the minimum lot area in R-1 (330 square metres).

Social, Environmental, Economic (External)

The proposed R-1 District allows for a rural / urban interface to be maintained, while also providing the opportunity for further densification in the future when appropriate through further subdivision.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1398

**Land Use Amendment in Crestmont (Ward 1) at multiple addresses,
LOC2019-0061**

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current or future operating budget at this time.

Current and Future Capital Budget

The proposed land use amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no significant risks associated with this proposal.

REASON(S) FOR RECOMMENDATION(S):

The proposal is generally in keeping with the applicable policies in the *Municipal Development Plan*, the *Rocky View / Calgary Intermunicipal Development Plan*, and the *Calgary West Area Structure Plan*. The proposed land use redesignation to the Residential – One Dwelling (R-1) District is an appropriate transition from the existing Direct Control District to a standard Land Use Bylaw district that will allow for the aforementioned transitional interface to be maintained.

ATTACHMENT(S)

1. List of Affected Addresses
2. Applicant Submission

List of Affected Addresses

The following is a list of the 22 properties included in the redesignation application:

- 36 Crestridge Heights SW (Plan 1911141, Block 21, Lot 14)
- 40 Crestridge Heights SW (Plan 1911141, Block 21, Lot 13)
- 41 Crestridge Heights SW (Plan 1911141, Block 22, Lot 2)
- 78 Crestridge Heights SW (Plan 1911141, Block 21, Lot 9)
- 86 Crestridge Heights SW (Plan 1911141, Block 21, Lot 8)
- 94 Crestridge Heights SW (Plan 1911141, Block 21, Lot 7)
- 95 Crestridge Heights SW (Plan 1911141, Block 21, Lot 1)
- 102 Crestridge Heights SW (Plan 1911141, Block 21, Lot 6)
- 103 Crestridge Heights SW (Plan 1911141, Block 21, Lot 2)
- 110 Crestridge Heights SW (Plan 1911141, Block 21, Lot 5)
- 111 Crestridge Heights SW (Plan 1911141, Block 21, Lot 3)
- 119 Crestridge Heights SW (Plan 1911141, Block 21, Lot 4)
- 28 Crestridge Rise SW (Plan 1911141, Block 22, Lot 1)
- 112 Crestridge Terrace SW (Plan 1911141, Block 20, Lot 6)
- 120 Crestridge Terrace SW (Plan 1911141, Block 20, Lot 5)
- 128 Crestridge Terrace SW (Plan 1911141, Block 20, Lot 4)
- 136 Crestridge Terrace SW (Plan 1911141, Block 20, Lot 3)
- 144 Crestridge Terrace SW (Plan 1911141, Block 20, Lot 2)
- 152 Crestridge Terrace SW (Plan 1911141, Block 20, Lot 1)
- 66 Crestridge View SW (Plan 1911141, Block 20, Lot 8)
- 72 Crestridge View SW (Plan 1911141, Block 20, Lot 7)
- 12111 Trans Canada Highway SW (Portion of Plan 7510024, Block 2)

Applicant Submission



Stantec Consulting Ltd.
200-325 25 Street SE, Calgary AB T2A 7H8

October 4, 2019
File: 116500605.231

Attention: Lindsey Ganczar
800 Macleod Trail SE,
PO Box 2100 Station M (#8073)
Calgary, AB T2P 2M5

Dear Ms. Ganczar,

Reference: DTR Response LOC2019-0061

The following are written responses to the DTR comments received regarding LOC2016-0161 on September 12, 2019. Our response to each comment is indicated in [blue](#).

Planning

1. Amend the proposed site plan to show all of the subject legal parcels. The current site plan shows only the area to be redesignated. The DC site boundaries are not required to be shown on the site plan as the proposal is no longer requesting DC boundary changes.

[The site plan has been amended as requested and three \(3\) copies have been attached.](#)

2. The proponent must address the comments generated from the circulation and the notice postings on Crestridge RI SW and Crestridge HT SW. Administration received 12 letters in opposition to the application.

Provide a written response, as well as a summary of the public engagement undertaken for this application prior to the application being included on a future CPC agenda.

[The application for land use redesignation to R-1 was selected based on a recommendation from the City since the preferred land use designation of Direct Control was not supported. We felt that the land use change was insignificant, as described below, and did not warrant additional outreach. As such, we indicated our expectation for the City to coordinate public outreach. The City responded to all of the objection letters that were received and provided rationale for the height increase which is the only concern that arose from the change in land use. All other concerns relate to development in general.](#)

[We are not proposing a new residential area. This area has already been approved as part of the Crestmont Conceptual Scheme \(Bylaw C-6083-2005\) and the current Direct Control District \(Bylaw 17D2008\). The purpose of this application is only to adjust the boundaries to fully encompass all subdivided lots and to increase the maximum number of parcels by one \(1\) parcel. Further, the surrounding lands were approved for development \(City file: SB2016-0299\) and have been registered with Land Titles as Subdivision Plan 1911141. This proposed land use amendment is being requested as a housekeeping item to facilitate the development of the remaining lots in the remnant portions of Block 2, Plan 7510024.](#)

[Specific responses to each concern identified by the public can be found below:](#)

[Design with community in mind](#)

Applicant Submission

October 4, 2019

Lindsey Ganczar

Page 2 of 2

Reference: DTR Response LOC2019-0061

- *Disturbance to wildlife corridor;* The wildlife corridor is not impacted by this land use redesignation.
- *Maximum building height too high (no three or four storey houses should be allowed);* This is a result of the City's request to use the R-1 District. It is our understanding that this comment has been responded to through L. Ganczar. It would be appreciated if the City shares the rationale that was provided for the increase in height.
- *Potential erosion / hill instability in the subject area due to disturbance;* There will be no negative impact to erosion/hill instability as a result of this land use redesignation.
- *Removal of a significant amount of trees;* and The land use redesignation will not change the potential for tree removal.
- *Impact on the existing water table.* The water table is not impacted by this land use redesignation.

We trust that this provides the City of Calgary with the changes, clarification, and rationale necessary to proceed with LOC2019-0061 to Calgary Planning Commission, and Calgary City Council. Should you have any questions or require electronic copies of the attached, please do not hesitate to contact the undersigned.

Regards,

Stantec Consulting Ltd.



Claire Woodside M.P.L., RPP, MCIP
Team Lead – Planning, Senior Planner
Phone: 403 716 8229
Fax: 403 716 8099
Claire.Woodside@stantec.com

ta v:\1165\active\116500605\200_planning\330_land_use_redesignation\05_assessment_dtr\let_dtr_response_loc2019-0061_20191001.docx

Design with community in mind

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1079

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

EXECUTIVE SUMMARY

This application was submitted on 2018 April 19 by O2 Designs on behalf of Aspen Springs GP Lt. (Slokker Canada West). This application proposes a land use redesignation of approximately 10.65 hectares (26.32 acres) on five contiguous residential acreage parcels in the southwest community of Springbank Hill. The proposed land use redesignation and associated outline plan application allow for the development and subdivision of the subject lands including:

- residential and mixed-use developments of up to 50 metres in height (13 to 15 storeys) where the existing land use district limits this site to country residential development;
- the accommodation of residential and commercial uses and built forms that complement its proximity to a Neighbourhood Activity Centre ;
- an anticipated 1,235 dwelling units in a mix of medium and high density residential building forms comprising of townhouse and apartment buildings (MU-1, M-1, DC – base M-2);
- approximately 1.01 hectares (2.51 acres) of Municipal Reserve (MR) in the form of neighbourhood parks and open spaces (S-SPR);
- approximately 0.62 hectares (1.53 acres) of Environmental Reserve (ER) in the form of natural areas to be conserved (S-UN); and
- future dedication of road right-of-way to accommodate the extension of 19 Avenue SW to 85 Street SW, bisecting the subject site;

The proposed redesignation implements the policies of the *Municipal Development Plan*. While the proposal is generally in keeping with the objectives of the *Springbank Hill Area Structure Plan* (ASP), a number of amendments to the ASP are required to provide further direction for future development of the subject lands and align the ASP with the proposal. The associated outline plan application (CPC2019-1333), provides technical rationale to support the proposed ASP amendments and land use redesignation.

Two development permit applications have been submitted and are currently under review

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1079

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

ADMINISTRATION RECOMMENDATION:

The Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **ADOPT**, by bylaw, the proposed amendments to the Springbank Hill Area Structure Plan (Attachment 5); and
2. Give three readings to the proposed bylaw.
3. **ADOPT**, by bylaw the proposed redesignation of 10.65 hectares \pm (26.32 acres \pm) located at 1880, 2188 and 2220 – 85 Street SW, 2027 – 81 Street SW and 8361 and 8473 – 17 Avenue SW (Plan 3056AC, Lots 26 and 27; Plan 2747HB; Blocks 28, 30, 32 and 33) from DC Direct Control District **to** Mixed-Use – General (MU-1f5.0h50) District, Mixed-Use – General (MU-1f3.0h20) District, Multi-Residential – Low Profile (M-1) District, Special Purpose – School, Park and Community Reserve (S-SPR) District, Special Purpose – City and Regional Infrastructure (S-CRI) District, Special Purpose – Urban Nature (S-UN) District and DC Direct Control District to accommodate a moderate increase in residential density with guidelines (Attachment 3); and
4. Give three readings to the proposed bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

O2 Designs, on behalf of Slokker Canada West, submitted the subject application along with the associated outline plan (CPC2019-1333) on 2018 April 19 and have provided a summary of their proposal in the Applicant's Submission (Attachment 1). The application was submitted a year after the *Springbank Hill Area Structure Plan (ASP)* was approved by Council in June 2017.

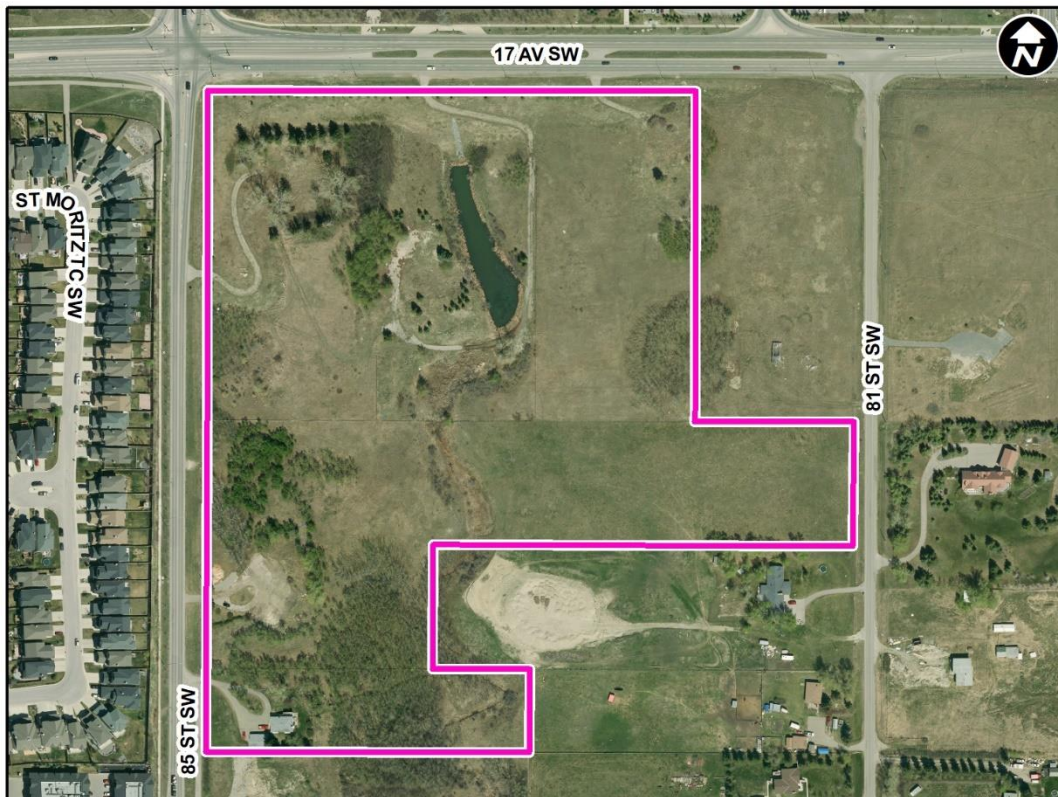
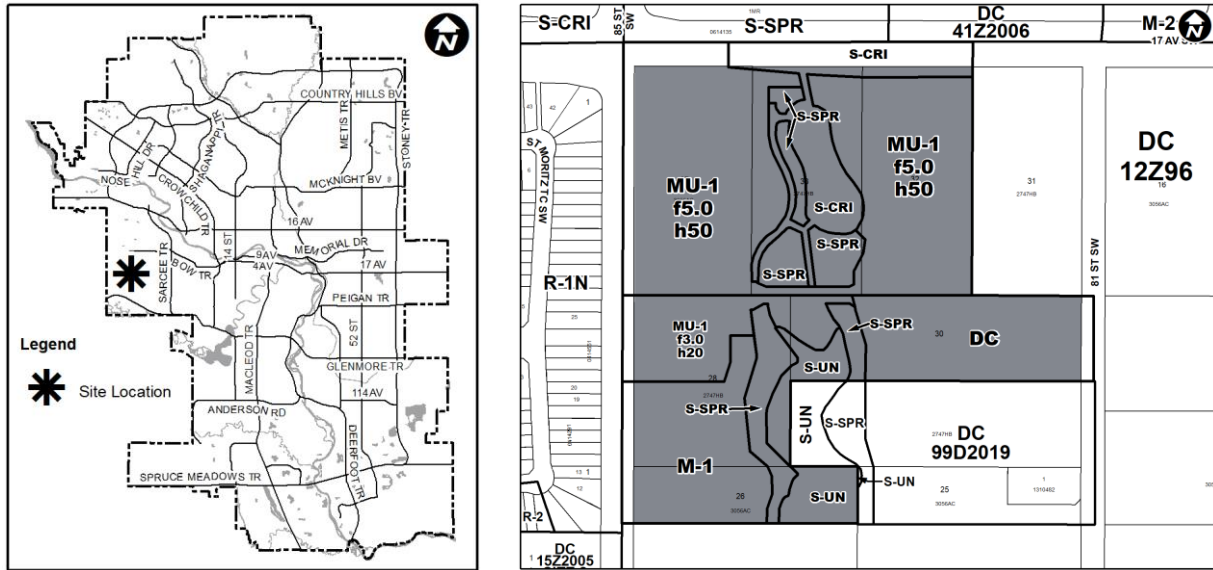
Two development permit applications have recently been submitted. The first is for a mixed-use building with 48 units and at-grade retail uses, located on the south-east corner of 19 Avenue SW and 85 Street SW. The second is a multi-residential development comprised of 97 townhomes (DP2019-4785 & DP2019-4791 respectively). Both development permit applications are currently under review by Administration.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1079

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at
multiple addresses, LOC2018-0085

Location Maps



Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

Site Context

The subject site is situated in the southwest quadrant of the city. This site is located adjacent and south of 17 Avenue SW and along the eastern edge of 85 Street SW. The 69 Street SW LRT Station is located approximately 1.5 kilometres to the east of the site along 17 Avenue SW. All parcels forming the site are located within the *Springbank Hill Area Structure Plan*.

The subject site consists of six contiguous residential acreages, located at 1880, 2188 and 2220 – 85 Street SW, 2027 – 81 Street SW, 8361 and 8473 – 17 Avenue SW. The site, totalling approximately 8.67 hectares (21.42 acres), spans approximately 385 metres on its east-west axis and is roughly 345 metres running north-south. A single detached dwelling exists on one of the parcels, while the others remain undeveloped and vacant. The rolling topography of the subject site sits highest along 17 Avenue SW and 85 Street SW. A watercourse running north-south bisects the site beginning at the culvert on the north end of the site, to an impoundment (created to hold water for agricultural purposes) to an impoundment, transitioning into an ephemeral drainage which turns into an intermittent watercourse with its associated ravine system. There are three tree stands that span various parcels throughout the site. The impoundment located on the north half of the site (south of 17 Avenue SW and mid-block to the site) runs north-south through a portion of the site and will be redesigned to serve as a stormwater pond servicing the entire plan area, upon development.

Figure 1 provides peak population statistics for the community of Springbank Hill. As identified in *Figure 1*, the community of Springbank Hill reached its peak population in 2018 with 10,052 residents.

Figure 1: Community Peak Population

Springbank Hill	
Peak Population Year	2018
Peak Population	10,052
2018 Current Population	10,052
Difference in Population (Number)	0%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the Springbank Hill community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

This land use amendment application and associated outline plan application will accommodate medium and high density residential and mixed use development including the dedication of

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

open space and natural areas. The resulting land uses will contribute to the growth of the Springbank Hill community in alignment with the policies of the *Municipal Development Plan* and *Springbank Hill ASP*. Council approved a new vision for the *Springbank Hill ASP* in 2017 to enable distinct neighbourhoods that foster a wide variety of lifestyles, from existing country residential to urban living near amenities such as retail, open spaces and transit, while preserving natural areas. The subject application achieves Council's vision.

Planning Considerations

As part of the review of this application, several key factors were considered by Administration, including the alignment with relevant policies, consideration of amendments to the *Springbank Hill ASP* and the appropriateness of the proposed land use districts. A series of proposed upgrades to local infrastructure (new storm pond, water and sanitary connections, and extension of a liveable street - 19 Avenue SW) propel Administration to consider accommodating moderate increases in density south of 19 Avenue SW, compared to the existing policy limits for these areas found within the *Springbank Hill ASP*, when it was adopted in 2017. The following sections highlight the scope of technical planning analysis conducted by Administration.

Land Use

The subject site is currently designated DC Direct Control District ([Bylaw 12Z96](#)) that accommodates country residential development. Its purpose is to maintain the character of the existing rural residential neighbourhood.

The subject site is proposed to be redesignated to various land uses that accommodate urban development typologies and building forms. The proposed moderate increases in density south of the proposed 19 Avenue SW extension will accommodate land use patterns that are consistent to those existing east of the ravine; providing gradual transitions in density and built forms between mixed use areas along 17 Avenue SW, towards low density residential areas further south of 19 Avenue SW.

The proposed redesignation is supported by the proposed amendments to the *Springbank Hill ASP* to primarily recognize upgrades in local infrastructure and amenities; appropriately accommodating moderate increases of densities in specific areas within the subject site. The *Springbank Hill ASP* envisions this area to be a vibrant community with a mix of commercial and residential uses and a broad range of housing types and densities. The proposed redesignation will accommodate development through the subject site that is complementary to the adjoining Neighbourhood Activity Centre (NAC) that is within 400 metres, east of the subject site (Attachment 8); and residential land use patterns to the east, across the ravine.

This land use amendment application (Attachment 2) proposes to redesignate the subject lands from DC Direct Control District (Bylaw 12Z96) to the following districts:

- Mixed Use - General (MU-1f5.0h50) District;

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

- Mixed Use – General (MU-1f3.0h20) District;
- Multi-Residential – Low Profile (M-1) District;
- Direct Control (DC) District;
- Special Purpose – School, Park and Community Reserve (S-SPR) District;
- Special Purpose – City and Regional Infrastructure (S-CRI) District; and
- Special Purpose – Urban Nature (S-UN) District.

Mixed Use - General (MU-1f5.0h50) and (MU-1f3.0h20) Districts

The northern half of the subject site adjoining 17 Avenue SW, an approximate area of 4.43 hectares (9.74 acres) is to be redesignated to the Mixed Use - General (MU-1f5.0h50) District. This area represents the densest area within the subject site and adjoins the future Neighbourhood Activity Centre (NAC) to the east. The purpose of these districts are to provide residential densities that support the primarily commercial uses to the east.

Located at the southern half of the subject site, in the southeast corner of 19 Avenue SW and 85 Street SW is an approximate 0.49 hectare (1.22 acre) portion to be redesignated to the Mixed Use - General (MU-1f3.0h20) District. The purpose of this portion of the site area is to provide complementary mixed-used development on the south side of 19 Avenue SW that is proposed to be designated a liveable street. Additionally, this district is to provide for a gradual transition of density and building forms from the mixed-use areas to the north, to the medium and low density areas further south of 19 Avenue SW. The reduction of density from 5.0 to 3.0 FAR, and building height from 50 (approximately 15 storeys) to 20 metres (approximately 6 storeys) is intended to help achieve this transition.

Multi-Residential – Low Profile (M-1) District

An approximate 1.31 hectare (3.24 acre) portion south of the mixed use district is to be redesignated to the Multi-Residential – Low Profile (M-1) District. The purpose of this district is to accommodate multi-residential development in a variety of forms of low height and medium density. Densities are to range between 50 and 148 units per hectare in these districts, while accommodating buildings up to 14 metres in height (approximately 3 to 4 storeys). This area of the subject site is intended to provide for the gradual transition from the mixed-use areas to the north, towards the low densities area to the south.

DC Direct Control District

A portion of the site (south west corner of 19 Avenue SW and 81 Street SW), an approximate area of 1.17 hectares (2.90 acres) is to be redesignated to a DC Direct Control District (Attachment 3) with a base of Multi-Residential – Medium Profile (M-2) District. The rules of the M-2 District apply in the proposed DC District, with additional rules that provide transition in density and built forms between mixed-use districts to the north and medium density residential districts to the south. The maximum density of the DC District is 210 units per hectare (uph), compared to 148 uph in the M-2 District. The maximum height of the DC District is 20.0 metres, compared to 16.0 metres in the M-2 District.

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

Special Purpose – City and Regional Infrastructure (S-CRI) District

An approximate total of 0.85 hectares (2.11 acres) within the plan area is to be designated Special Purpose – City and Regional Infrastructure (S-CRI) District. These areas are intended to provide for infrastructure, utility facilities, and systems for public transportation. A 0.28 hectare (0.68 acres) portion that spans 17 Avenue SW is to be dedicated for the purpose of accommodating future extension of the Blue Line LRT right-of way. The remaining 0.57 hectares (1.43 acres) is to be dedicated to accommodate municipal infrastructure and utility facilities associated with the proposed storm pond and drainage lines.

Special Purpose – School, Park and Community Reserve (S-SPR) & Special Purpose – Urban Nature (S-UN) District

An environmentally significant natural area (0.62 hectares or 1.53 acres) is proposed as S-UN District, to be dedicated as ER. This area will preserve the existing north-south ephemeral drainage/intermittent watercourse and ravine system.

Open space is provided to preserve the existing north-south ephemeral drainage, intermittent watercourse and associated ravine. Recent land use and outline plan applications for lands to the south and east have already preserved these features within the same network. This application will contribute to a continuous open space system in accordance with the *Springbank Hill ASP*.

A portion of the subject site (1.01 hectares or 2.51 acres) is proposed to be designated as S-SPR District with a Municipal Reserve (MR) designation, and will meet the 10 percent MR requirement for the subject area, anticipated for future subdivision. The park area north of 19 Avenue SW will accommodate the proposed stormwater pond flanked by MR to the west and a privately owned, publicly accessible open space to the east. A 4.0 metre maintenance access road will double as a park pathway and in concert with local pathways will create a loop around the stormwater pond. Part of this area is anticipated to be a naturalized park, with the area south of the pond proposed as an open lawn for active and passive recreational uses containing a re-aligned piped drainage within a culvert. The park area south of 19 Avenue SW will function as a naturalized park containing a Regional Pathway, and a local pathway and will act as a buffer between Environmental Reserve (ER) lands to be protected and developed areas. The Regional Pathway is proposed along the east side of 85 Street SW to the south side of 19 Avenue SW, where it continues to the east along this road until it enters the west side of the ravine where it will continue south and will link with future connections generally as per the *Springbank Hill ASP*.

The associated outline plan (CPC2019-1333) provides the framework for community and infrastructure build out, to be implemented largely at the subdivision stage. The land uses are complimentary to this plan and provide more certainty on densities and built form.

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

Density

The associated outline plan (Attachment 4) will accommodate development achieving both the *Municipal Development Plan (MDP)* and *Springbank Hill ASP* minimum density and intensity targets (population and jobs). This will support future transit and community amenities within the great community of Springbank Hill.

Overall, this proposal accommodates 1,235 dwelling units and 150 jobs within the total plan area; resulting in a residential density of 178 units per hectare (68 units per acre) and intensity of 285 people and 22 jobs per hectare, across the subject site.

More specifically, the associated outline plan meets the minimum intensities for the land typologies set out in the *Springbank Hill ASP*, outlined in Table 1 below.

Table 1: Minimum and Anticipated Intensities

Land Typology	Minimum Required Density per ASP (uph)	Anticipated Density (uph)	Minimum Required Intensity per ASP (population and jobs per hectare)	Anticipated Intensity (population and jobs per hectare)
Medium Density Residential	75	136	-	-
Mixed-Use Commercial/Residential	100	203	125	321 people and 22 jobs

Environmental

A Phase I Environmental Site Assessment (ESA) was submitted and was reviewed by Administration. No concerns were noted and further investigation deemed unnecessary.

A Biophysical Impact Assessment (BIA) was submitted and provides an assessment of existing site conditions. Key findings of the BIA include:

- The existing drainage/coulee system that qualifies as ER has been designated accordingly.
- The impoundment (created to hold water for agricultural purposes) and area surrounding the impoundment (within northern half of site) is disturbed and not considered a natural area;
- The same impoundment contains little riparian vegetation and is significantly sloped; and
- Rehabilitation of the impoundment into a naturalized pond including shallower margins and native vegetation may improve wildlife habitat and connectivity for certain species.

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

A Hydrogeological Assessment was submitted and investigated the source water emanating from the impoundment. Key findings of this assessment include:

- Test results indicate the impoundment having a strong stormwater component; similar to the upstream stormwater pond on the west side of 85 Street SW;
- Source of water can be partially derived from adjacent stormwater ponds or a combination of localized shallow ground and stormwater runoff from 17 Avenue SW; and
- No evidence the impoundment having any significant source of natural springwater.

A Geotechnical Investigation was submitted and evaluated the subsurface soil and groundwater conditions within the site. No significant concerns were identified by Administration. A Deep Fills Report will be required to be submitted and approved at either tentative (subdivision) or development permit stages.

Transportation

As part of the associated outline plan (CPC2019-1333), the extension of 19 Avenue SW (modified collector) is proposed from 81 Street SW; terminating at 85 Street SW, the western extent of the subject site. The subject site is bisected by the proposed 19 Avenue SW right of way, completing the east-west connection that primarily serves the mixed-use and medium density land use areas within the community. This extension begins as a 23.2 metre multi-modal Right-of-Way (ROW) from the east end of the site, and narrows down to a 15 metre ROW where it crosses the ravine. Narrowing the cross-section through this area results in removing the on-street parking lanes through this section. The benefit to the narrowing of this ROW across the ravine is the reduction of impacts to these open space and natural areas. Additionally, the narrowing will encourage slower vehicle speeds and minimizes the distance animals would have to navigate over 19 Avenue SW. This ROW returns to a 23.2 metre multi-modal ROW and terminates at 85 Street SW.

A Transportation Impact Assessment (TIA) was submitted as part of the application and it demonstrates that the proposed development, along with the Transportation Network outlined in the ASP, functions within acceptable levels of service.

The exact street ROW requirements and street cross sections to accommodate development in the area are under discussion and will form part of future tentative plan applications. This approach ensures alignment amongst the area developers, and allows the developer and Administration to ensure roadways are designed to accommodate all modes of travel and accommodate future development within the entire ASP area. Localized impacts from the future LRT extension will also be further evaluated with the appropriate tentative plan applications.

Extension of the West leg of the LRT (Blue Line) is anticipated along 17 Avenue SW; from 69 Street SW Station (approximately 1.5 km east of the site). Dedication of LRT right-of-way is required prior to approval of the first tentative plan. A Functional Study for the LRT was submitted by Hatch Ltd. on behalf of the applicant that studied three possibilities for an LRT

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1079

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

alignment (below, at and above grade) along the southern edge of 17 Avenue SW; anticipated by the adoption of the *Springbank Hill ASP* in 2017.

Future Transit connections (bus routes) are to be accommodated along 85 Street SW, serving the western half of the community, and along 81 Street SW that is to tie into an existing bus route serving the eastern half the community that loops back to the 69 Street LRT Station.

Regional pathways will be located along the southern edge or 19 Avenue SW, connecting into the open space network through the southern half of the outline plan area, swinging up towards 17 Avenue SW, from the western edge of 85 Street SW.

The active modes crossing along 19 Avenue SW was projected by the ASP. However, Administration reviewed a Technical Memo that analyzed pedestrian travel times using above-grade crossing versus a grade-level mid-block crossing. The memo found that travel times are substantially longer when pedestrians cross roadways using an above grade bridge, requiring pedestrians to navigate through four lengthy switchbacks to cross a 23.5 metre roadway. Since the maximum speed limit along the stretch of roadway leading up to the grade-level mid-block crossing would be 30 kilometres per hour (same as school zones), Administration accepts the findings of the technical memo and will not require an active-modes crossing in this location.

Utilities and Servicing

Water Servicing will be provided through an existing connection to the water feeder main along 17 Avenue SW, and a new main along 19 Avenue SW, to be installed under the extension of 19 Avenue SW. The proposed Strathcona Flow Control Station, located at the intersection of 17 Avenue SW and 85 Street SW, must be in place before any utilities are operational or any construction permissions are granted. The Strathcona Flow Control Station is City funded infrastructure and is scheduled to complete construction by Q2 2019.

A Regional Sanitary Servicing Study was provided by Pasquini & Associates and Urban Systems that includes all areas subject to the *Springbank Hill ASP*. This study identified downstream constraints. There are two regional options available to remove these constraints. The first option requires a developer/applicant funded extension, connecting to existing servicing along 85 Street SW and 81 Street SW. The second option is for the City to front-end a pilot project to upgrade the existing downstream sanitary mains to increase their capacity to accommodate the full build-out of the Springbank Hill study area. Some parcels may be allowed to proceed without upgrades, but will be required to provide securities, proportional to the parcel's associated flow rate, and developable area.

The subject site is within the approved Springbank Master Drainage Plan (MDP) completed by Stantec in 2017. Stormwater from the proposed outline plan area is to be directed through the adjoining ravine network and existing impoundment that is to be enhanced and rehabilitated to be the stormwater pond. Ultimately, stormwater is to be managed in concert with the Springbank Hill Staged Master Drainage Plan.

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners, the Springbank Hill Community Association (SHCA) and the application was advertised online.

No letters from adjacent landowners were received.

The applicant engaged external stakeholders, holding a public open house on 2019 May 28. According to the applicant, more than 25 people attended this community information session. The open house provided residents information about the proposed development and to speak directly with the applicants. Information panels provided attendees details of the proposed land use and outline plan. An opportunity to provide feedback directly to the applicants was also accommodated. Following the public open house, the applicant submitted a summary of the feedback received on the project through a 'What We Heard' report (Attachment 6). The most noteworthy comments received are summarized below:

- Maximum height for all development should be 6-storeys;
- 10-storey buildings should not be allowed along 17 Avenue SW;
- Traffic resulting from additional density, particularly southbound along 85 Street SW; and
- There is already too much commercial activity in the area;

Subsequent to the comments provided, the applicant addressed the concerns received from the public open house. While buildings that are higher than 10 storeys are anticipated along 17 Avenue SW, the associated impact to the public realm would be limited based on the future LRT alignment of the blue line that spans the site along its northern extent. Furthermore, this edge slopes steeply down away from 17 Avenue SW, further reducing visual impacts of a build that may sit below the future alignment of the LRT right-of-way. Administration finds the accommodation of the densest developments within the Mixed Use – General (MU-1f5.0h50) District is appropriate. According to the proposed outline plan, 95 percent of the density is to be allocated for residential uses with 5 percent for commercial uses (Attachment 4). Six-storey residential buildings are anticipated for sites fronting 19 Avenue SW (south side) within the Mixed Use- General (MU-1f3.0h20) District and Direct Control District. Dwellings units within the Multi-Residential – Low Profile (M-1) District would be two to three storeys.

The SHCA provided a letter (Attachment 7) that does not support the proposed land use amendment and amendments to the Springbank Hill ASP. The primary concern of the SHCA was the increase in residential densities in portions of the outline plan south of 19 Avenue SW. Administration finds that the proposed moderate increase in densities (accommodated by the proposed amendments to the Springbank Hill ASP, explained in associated report CPC2019-1333) provides appropriate transition of densities and built forms into low density areas to the south. Furthermore, the allocation of additional density within a 400 metre walking distance to a Neighbourhood Activity Centre (NAC) to the east, along a liveable street and adjoining publicly accessible natural open spaces, reinforces the policies of the MDP to accommodate additional density in future greenfield areas. This is expanded upon in the proceeding sections.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1079

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

Administration anticipates more detailed transportation analysis to be conducted through the review of subsequent development permit applications, addressing traffic concerns perceived with the emerging increase of the local population and built form of this developing community.

Following Calgary Planning Commission, notifications for Public Hearing of Council, in relation to this land use amendment, will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (Statutory – 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSPR) which directs population growth in the region to the Cities and Towns and promotes efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment build on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

Map1: Urban Structure of the Municipal Development Plan (MDP) identifies the subject lands as Residential – Developing – Future Greenfield. The MDP provides guidance for the development of new communities through the policies of the ASP. The proposal meets the following MDP objectives (Section 3.6.2):

- providing a diversity of housing types;
- providing parks located throughout the community in walkable proximity to all residences; and
- creating an inter-connected, multi-modal street network.

According to Section 3.6.2 of the MDP (Future Greenfield Area), new communities should be organized to include a number of distinct neighbourhoods that are defined by a 400 metre or five-minute walking distance from a Neighbourhood Activity Centre (NAC) or Neighbourhood *Main Street*. Furthermore concentration of residential density is encouraged by this policy in areas adjacent to open space, parks, wetlands and sports fields, especially where the area is served by transit, services and other community amenities. All areas south of the proposed extension of 19 Avenue SW are located within an adjoining open space network. These areas are also 400 metres (5 minute walking distance) from the future NAC (Attachment 8); to be established as part of Land Use and Outline Plan application LOC2017-0386. Connectivity from the subject site to the future NAC is to be accommodate by way of a liveable street; an enhanced pedestrian connection established along 19 Avenue SW. For such reasons the

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

following proposed ASP amendments are found to support policies of the MDP, accommodating a moderate increase in density:

- redesignation of a 0.49 hectare (1.22 acre) site at southeast corner of 19 Avenue SW and 85 Street SW from Medium Density to Mixed - Use;
- redesignation of a 1.31 hectare (3.23 acre) site south of 19 Avenue SW between 85 Street SW and open space network from low density to medium density; and
- additional ASP Policy under section 3.1.6 (Mixed-Use) that accommodates moderate increase in density through land use amendments for areas within close proximity to a liveable street.

Springbank Hill Area Structure Plan (Statutory – 2017)

The *Springbank Hill Area Structure Plan (ASP)* provides direction with detailed policies for future development of this community. The subject site comprises of areas subject to mixed-use and medium density land use policies. The ASP also identifies portions of the site subject to an Environmental Open Space (EOS) Study Area. While the proposal is generally in keeping with the objectives of the ASP, a number of amendments to the ASP are required to provide direction for future development of the subject lands and align the ASP with the proposal.

Amendments to the Springbank Hill ASP

A series of proposed upgrades to local infrastructure and amenities (new storm pond, water and sanitary connections, and extension of a liveable street - 19 Avenue SW) propel Administration to consider accommodating moderate increases in density south of 19 Avenue SW; compared to the existing policy limits for these areas found within the *Springbank Hill ASP* when it was adopted in 2017. Amendments to the ASP (Attachment 5) are proposed to provide further direction to consider moderate increase of density for sites adjoining a liveable street, as defined in the ASP. The proposed amendments are supported by the aforementioned policies of the MDP, supporting moderate increase in density in areas that are within 400 metres of a Neighbourhood Activity Centre (NAC) and well served by local pedestrian and cycling infrastructure. The following factors reinforce the appropriateness of accommodating moderate increase in density in specific areas south of 19 Avenue SW:

- proximity to an open space and regional pathway network, connected to a liveable street;
- proximity to a Neighbourhood Activity Centre (NAC) that is within 400 metres walking distance to community amenities;
- accommodate gradual transitions in density and building forms between mixed use areas along 17 Avenue SW, medium density residential areas south of 19 Avenue SW and low density residential areas further south; and
- provide for land use patterns that are consistent with existing residential areas east of adjoining ravine; areas south of 19 Avenue SW.

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

The following maps within the ASP are proposed to be amended; primarily to recognize the extension of 19 Avenue SW between 81 Street and 85 Street; also to extend its designation as a liveable street from the eastern half of the plan area. The proposed amendments to the ASP are outlined as follows:

Map 2: Land Use Concept

Remove Active Modes Crossing along 19 Avenue SW;

- Extend 19 Avenue SW from 81 Street to 85 Street SW;
- Extend 'Liveable Street' typology through the extension of 19 Ave SW as described above;
- Redesignate SE corner of 19 Avenue SW and 85 Street SW from 'Medium Density' to 'Mixed Use' (follow associated Outline Plan for extent of area); and
- Redesignate Low Density Areas (along 85 Street SW, south of 19 Avenue SW) to Medium Density – mirroring policy areas east of ravine (follow associated Outline Plan for extent of area).

Map 4: Open Space

- Remove Active Modes Crossing along 19 Avenue SW; and
- Extend 19 Avenue SW from 81 Street SW to 85 Street SW.

Map 5: Transit Network

- Extend 19 Avenue SW from 81 Street SW to 85 Street SW.

Map 6: Street Network

- Extend 19 Avenue SW from 81 Street SW to 85 Street SW and designate as "Collector" street

Map 7-9 (Water, Sanitary and Storm)

- Extend 19 Avenue SW from 81 Street to 85 Street SW.

Section 3.1.6 (Medium Density) Under Policies

- Under section 3.1.6, delete policy 1 and replace with the following:
"Densities shall range between 38 to 148 units per gross developable hectare. However, moderate increases in residential density may be considered for residential areas in close proximity to 19 Avenue SW west of 81 Street SW."
- Under section 3.1.6, delete policy 3 and replace with the following:

"The maximum height of a building should be six storeys. Increased height may be considered in residential areas in close proximity to 19 Avenue SW and west of 81 Street SW. Any increase in building height should be based on topographic conditions and where impacts and compatibility with surrounding built form can be addressed to the satisfaction of the Approving Authority (e.g. shadow impact, privacy concerns).

Policy Amendment and Land Use Amendment in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085

Social, Environmental, Economic (External)

The proposal helps achieve a greater mix of housing types and mixed use development that complement its close proximity to a Neighbourhood Activity Centre, in the community of Springbank Hill.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no known risks associated with this application.

REASON(S) FOR RECOMMENDATION(S):

The proposed policy amendments and land use redesignation accommodates future development as envisioned in the *Springbank Hill Area Structure Plan* and as encouraged by policies of the *Municipal Development Plan*. Further, the proposal helps achieve more efficient use of land and infrastructure by accommodating mixed use areas and moderate increase of density in residential districts adjoining an enhanced pedestrian realm (19 Avenue SW).

The proposal accommodates for a mix of housing forms and commercial opportunities, found to be appropriate along arterial and collector streets. The extension of 19 Avenue SW to 85 Street SW provides for a logical extension of a roadway designated to be a 'liveable street', and appropriately accommodating land uses adjoining the future extension of the West LRT corridor.

ATTACHMENTS

1. Applicant Submission
2. Proposed Land Use District Map
3. Proposed DC Direct Control District
4. Proposed Outline Plan
5. Proposed Amendments to the Springbank Hill Area Structure Plan
6. What We Heard Report
7. Community Association Letter
8. Location Context Map – Future Neighbourhood Activity Centre (NAC)

Applicant Submission

October 22, 2019

On behalf of Slokker Homes, O2 Planning + Design submitted this outline plan and land use amendment application regarding the subject site, comprised of six (6) parcels at the southeast intersection of 85 ST SW and 17 AV SW, to enable the development of a mixed-use neighbourhood that responds to the evolving context of the area. With low density residential uses to the south, anticipated commercial and multi-residential uses to the east, and 17 avenue SW and the future 85 avenue SW LRT Station to the north, the proposed outline plan and land use amendment enables the development of a new community that will seamlessly integrate into the existing fabric of the area.

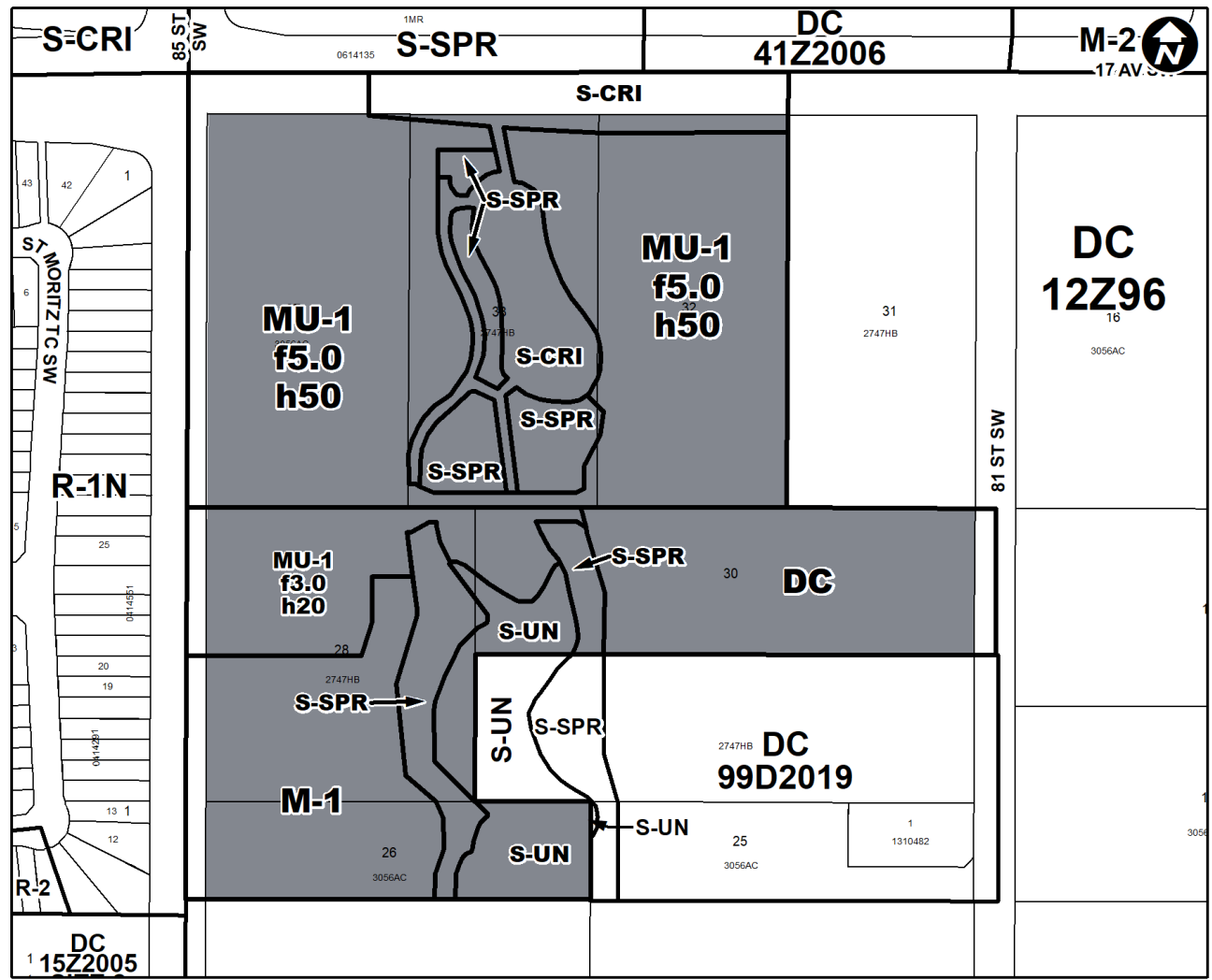
Two organizing features have guided the design of the community. First, 19 avenue will be a new east-west liveable street that will connect 85 street to 77 street. Although not contemplated in the Springbank Hill ASP, it was determined during the outline plan design that this connection was feasible and results in improved urban design by providing the opportunity for better connectivity through the site to adjacent developments and a better public realm along the street. The second organizing feature is a linear open space network that runs north south through the site which provides opportunities for passive and active recreation for residents of the community and wider neighbourhood. The green network follows the natural ravine system with municipal reserve connecting the north and south areas of the community and includes a naturalized storm pond at the north which will be an additional community amenity.

The application seeks to redesignate the northern portion of the site to Mixed Use – General (MU-1 f5.0h50). This will enable a built form and a mix of uses that responds to the site's adjacency to 17 avenue and proximity to existing and proposed transportation infrastructure. The intention is that this area will be predominantly (95%) residential with retail uses facing 19 avenue on the western edge and integrated with and facing the neighbouring development on the east. The southwestern portion of the site will be designated Mixed Use – General (MU-1 f3.0h20) and Multi-Residential – Low Profile (M-1). These designations will allow for mixed use development fronting onto 19 avenue transitioning to lower density townhouses towards the south. The southeastern area will be designated Direct Control based on Multi-Residential – Medium Profile (M-2). This designation will allow low rise multi-family development that adapts to the slope of the site. This allocation of land uses, along with the use of density and height modifiers, effectively distributes density and intensity throughout the site in a manner that responds to the local context on all four edges.

The site falls within the Springbank Hill Area Structure Plan (ASP) and is designated Mixed-Use in the north and both Medium Density and Low Density in the south. This application proposes an amendment to the ASP to enable the 19 AV SW connection and to extend the Mixed-Use designation to the south. Doing so will create an active mixed-use corner at 19 avenue SW and 85 street SW, increase connectivity throughout the area, relieve pressure from 17 avenue SW, and enable a transition between higher and lower density uses.

Throughout the application process, O2 and Slokker worked collaboratively with administration and consulted with the Springbank Hill Community Association and local residents on an ongoing basis. An open house was held on May 28 and was attended by approximately 25 people. A summary of the engagement undertaken for this project is provided in a What We Heard Report.

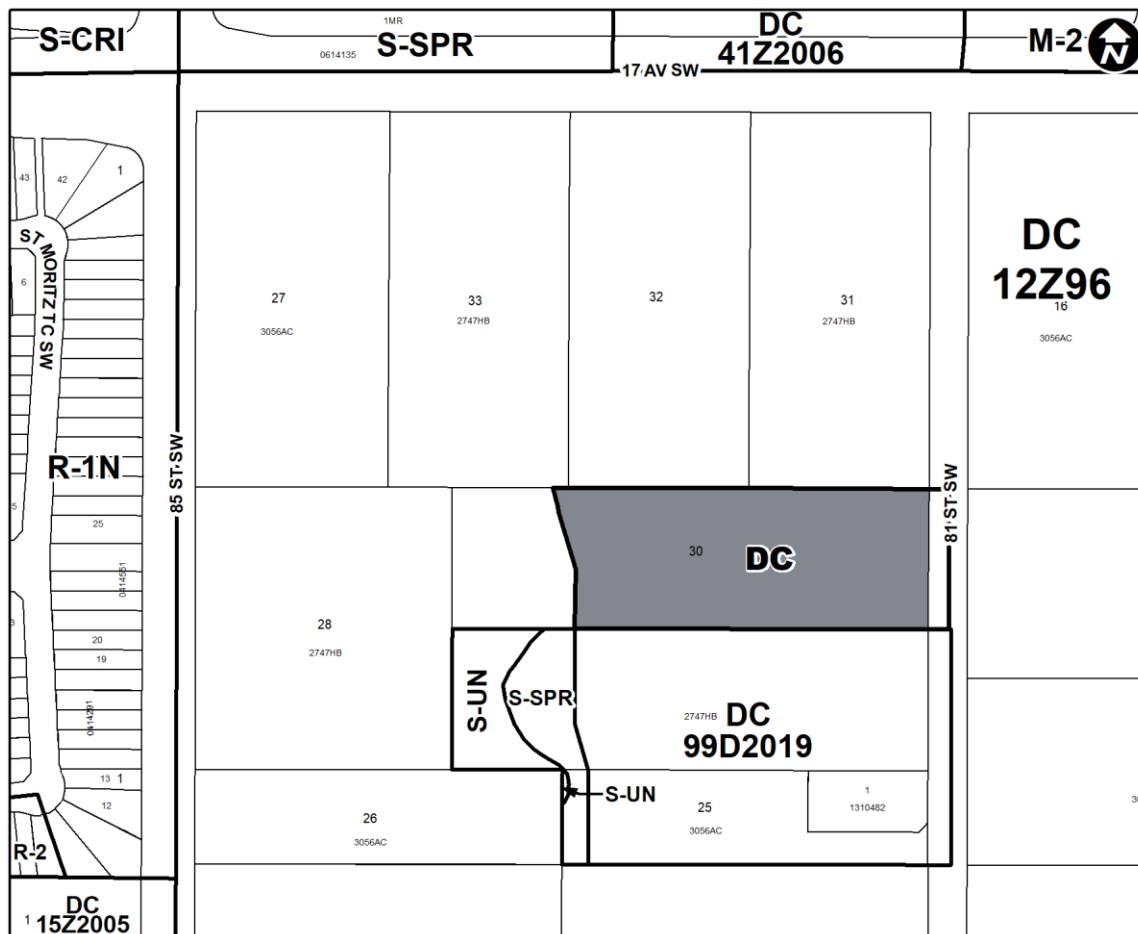
Proposed Land Use District Map



Proposed DC Direct Control District

- The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by amending that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "A".

SCHEDULE A



DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control District Bylaw is intended to:

- accommodate medium density, mid-rise multi-residential development in accordance with the policies of the Springbank Hill Area Structure Plan;
- accommodate additional height to accommodate buildings up to 6 storeys; and
- provide a transition in density and built forms between mixed-use districts to the north and medium density residential districts to the south.

Proposed DC Direct Control District

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District Bylaw.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District Bylaw, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The **permitted uses** of the Multi-Residential – Medium Profile (M-2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

Discretionary Uses

- 5 The **discretionary uses** of the Multi-Residential – Medium Profile (M-2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District.

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Multi-Residential – Medium Profile (M-2) District of Bylaw 1P2007 apply in this Direct Control District.

Density

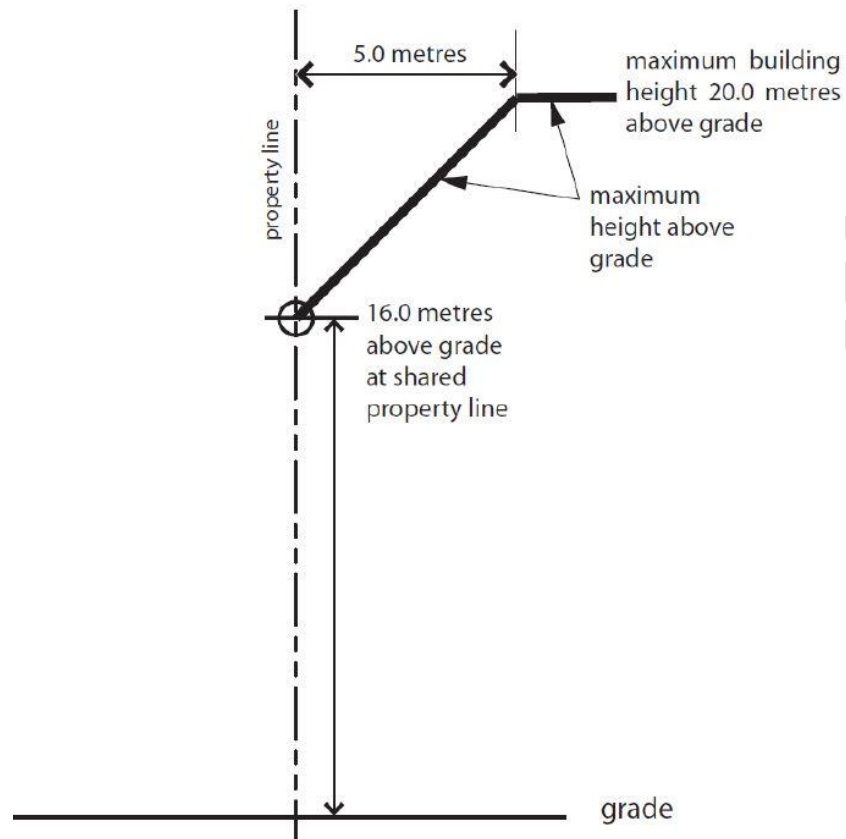
- 7 (1) The minimum **density** is 60 **units** per hectare.
- (2) The maximum **density** is 210 **units** per hectare.

Building Height

- 8 (1) Unless otherwise referenced in subsection (2), the maximum **building height** is 20.0 metres.
- (2) The maximum **building height**:
- (a) is 16.0 metres measured from **grade** at the **property line**; and
- (b) increases proportionately to a maximum of 20.0 metres measured from **grade** at a distance of 5.0 metres from the **property line**.
- (3) The following diagram illustrates the rules of subsection (2):

Proposed DC Direct Control District

Illustration 1:
Building Height and Cross Section

**Relaxations**

- 9 The **Development Authority** may relax the rules contained in Sections 7 and 8 of this Direct Control District Bylaw in accordance with Sections 31 and 36 of Bylaw 1P2007.

Proposed Outline Plan

OUTLINE PLAN



LAND USE LEGEND

- Multi-Residential District (MU-1 (f5 h50))
- Multi-Residential District (MU-1 (f3h20))
- Multi-Residential District (DC (M-2))
- Multi-Residential District (M-1)
- Special Purpose District (S-CRI)
- Environmental Reserve (ER)
- Municipal Reserve (MR)

LEGEND

- Outline Plan Boundary
- Proposed Parcels Boundaries
- Proposed PUL Boundaries
- Existing Grades Contours (0.50m Intervals)
- Existing Grades Contours (1.00m Intervals)
- Existing Ravine Drainage (from Stantec BI)
- 6m Offset From Existing Drainage
- Coulee Break / Top of Slope
- Top of Slope Buffer
- Toe of Slope / Graded Break Line
- 3.0 Regional Path Connection
- Pedestrian / Bike Link from MR along Pond to Regional Pathway Up Slope along 17 Avenue SW
- Mid-Block Crossing
- Potential Primary Access Location
- Potential Emergency Access Location
- Utility Right of Way / Easement
- Existing Sanitary Line
- Proposed Sanitary Line
- Existing Water Line
- Proposed Water Line
- Existing Storm Line
- Proposed Storm Line (Pipe Size in mm)
- Proposed Storm Line Inlet / Outlet + Pipe Invert Elevation
- Proposed Storm Manhole + Pipe Invert Elevation
- Proposed Pond and Pond Storm System Maintenance Access (4m wide)
- Emergency Spillway Storm Water Pond
- Emergency Spillway Flow

SLOKKER SPRINGBANK HILL DEVELOPMENT

Slokker Homes

O2
810 256 17th Ave SW Calgary Alberta Canada
T 403.228.1228 F 403.228.1229 www.o2.ca

22 October 2019

Proposed Amendments to the Springbank Hill Area Structure Plan

1. The Springbank Hill Area Structure Plan attached to and forming part of Bylaw 28P2017, as amended, is hereby further amended as follows:
 - (a) Delete the existing Map 2 entitled 'Land Use Concept' and replace with the revised Map 2 entitled 'Land Use Concept', attached as Schedule A;
 - (b) Delete the existing Map 3 entitled 'Attributes and Constraints' and replace with the revised Map 3 entitled 'Attributes and Constraints', attached as Schedule B;
 - (c) Delete the existing Map 4 entitled 'Open Space' and replace with the revised Map 4 entitled 'Open Space', attached as Schedule C;
 - (d) Delete the existing Map 5 entitled 'Transit Network' and replace with the revised Map 5 entitled 'Transit Network', attached as Schedule D;
 - (e) Delete the existing Map 6 entitled 'Street Network' and replace with the revised Map 6 entitled 'Street Network', attached as Schedule E;
 - (f) Delete the existing Map 7 entitled 'Water Services' and replace with the revised Map 7 entitled 'Water Services', attached as Schedule F;
 - (g) Delete the existing Map 8 entitled 'Sanitary Servicing' and replace with the revised Map 8 entitled 'Sanitary Servicing', attached as Schedule G;
 - (h) Delete the existing Map 9 entitled 'Stormwater Servicing' and replace with the revised Map 2 entitled 'Stormwater Servicing', attached as Schedule H;
 - (i) Under section 3.1.6, delete policy 1 and replace with the following:

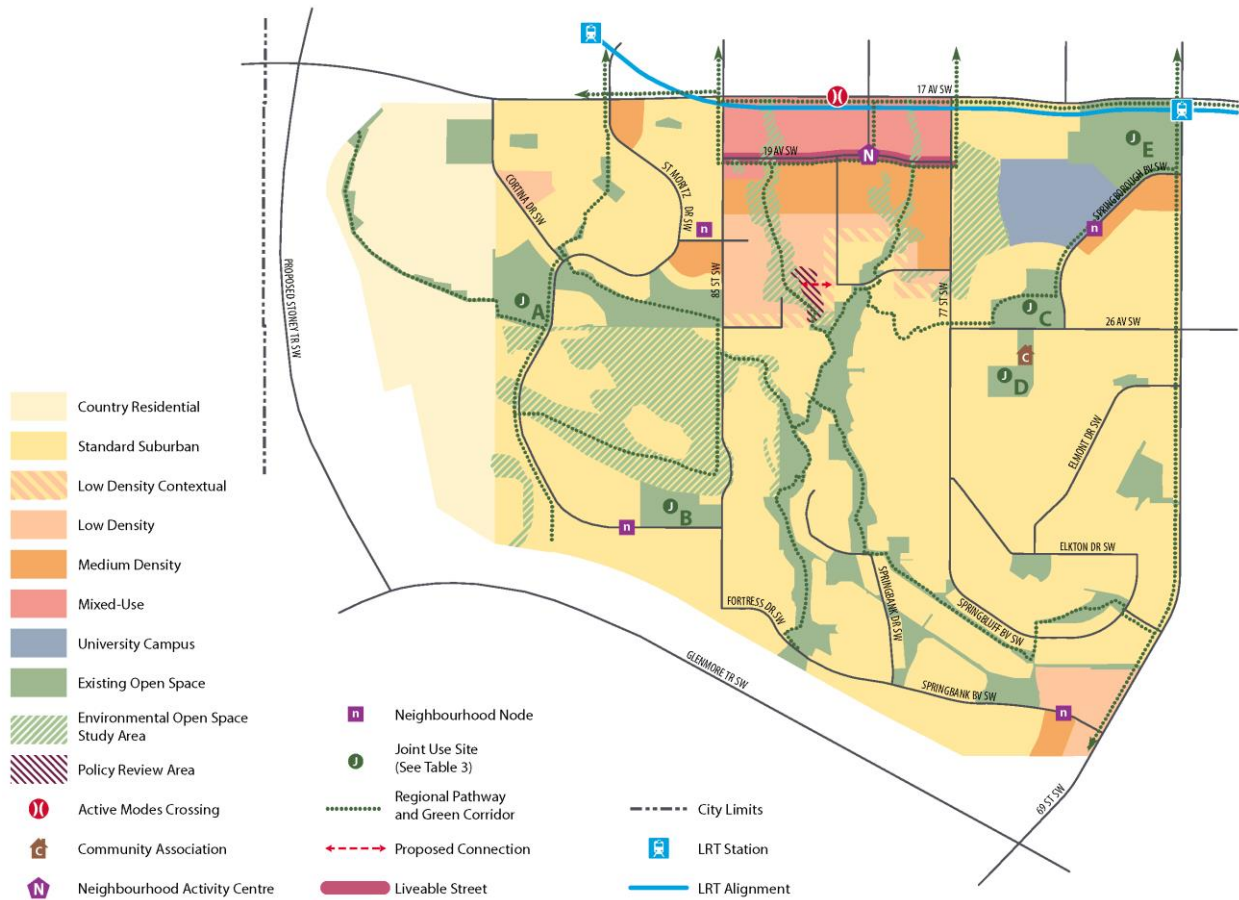
"Densities should range between 38 to 148 units per gross developable hectare. However, moderate increases in residential density may be considered for residential areas in close proximity to 19 Avenue SW west of 81 Street SW."
 - (j) Under section 3.1.6, delete policy 3 and replace with the following:

"The maximum height of a building should be six storeys. Increased height may be considered in residential areas in close proximity to 19 Avenue SW and west of 81 Street SW. Any increase in building height should be based on topographic conditions and where impacts and compatibility with surrounding built form can be addressed to the satisfaction of the Approving Authority (e.g. shadow impact, privacy concerns).

Proposed Amendments to the Springbank Hill Area Structure Plan

SCHEDULE A

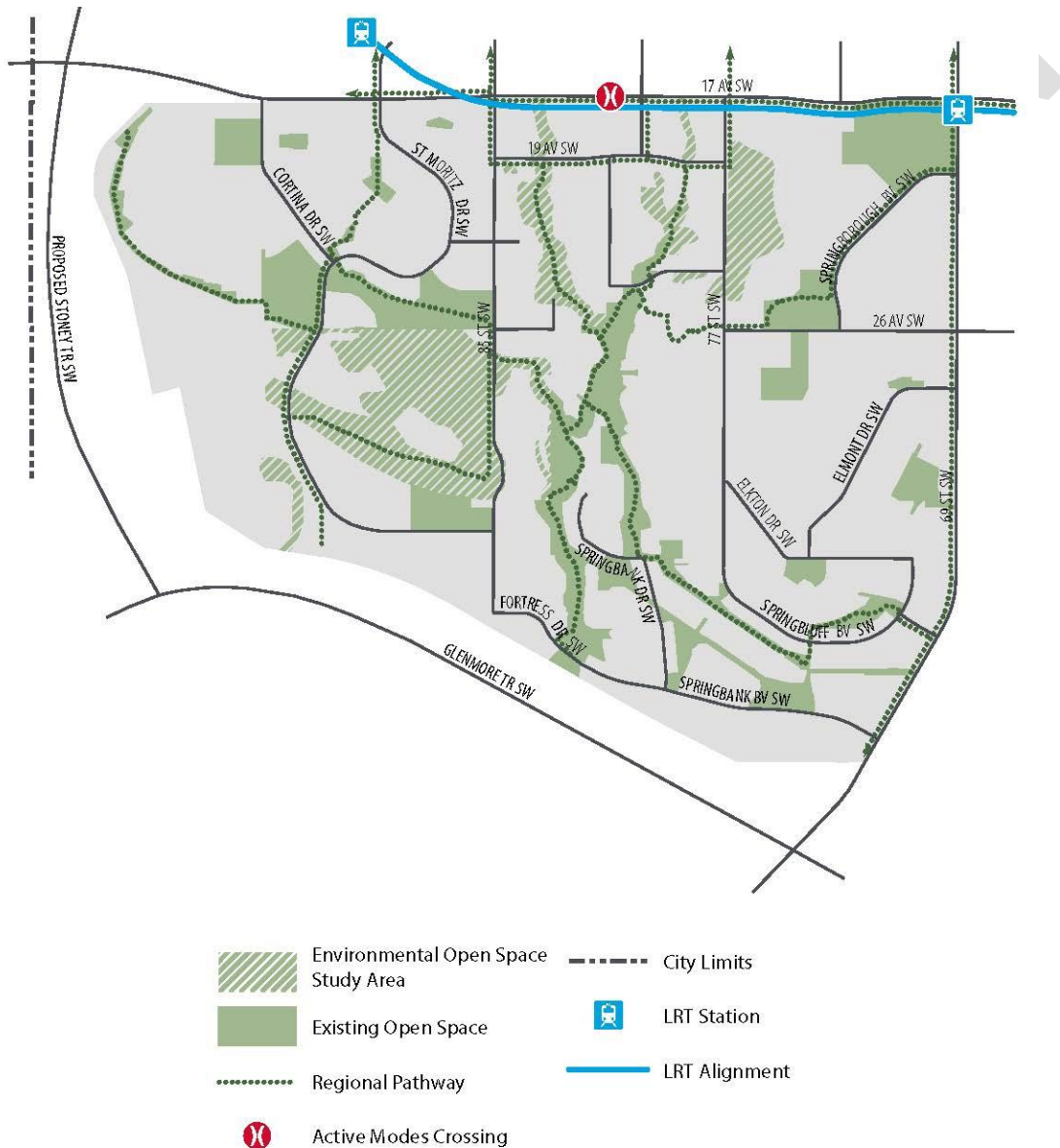
Map 2: Land Use Concept



Proposed Amendments to the Springbank Hill Area Structure Plan

SCHEDULE C

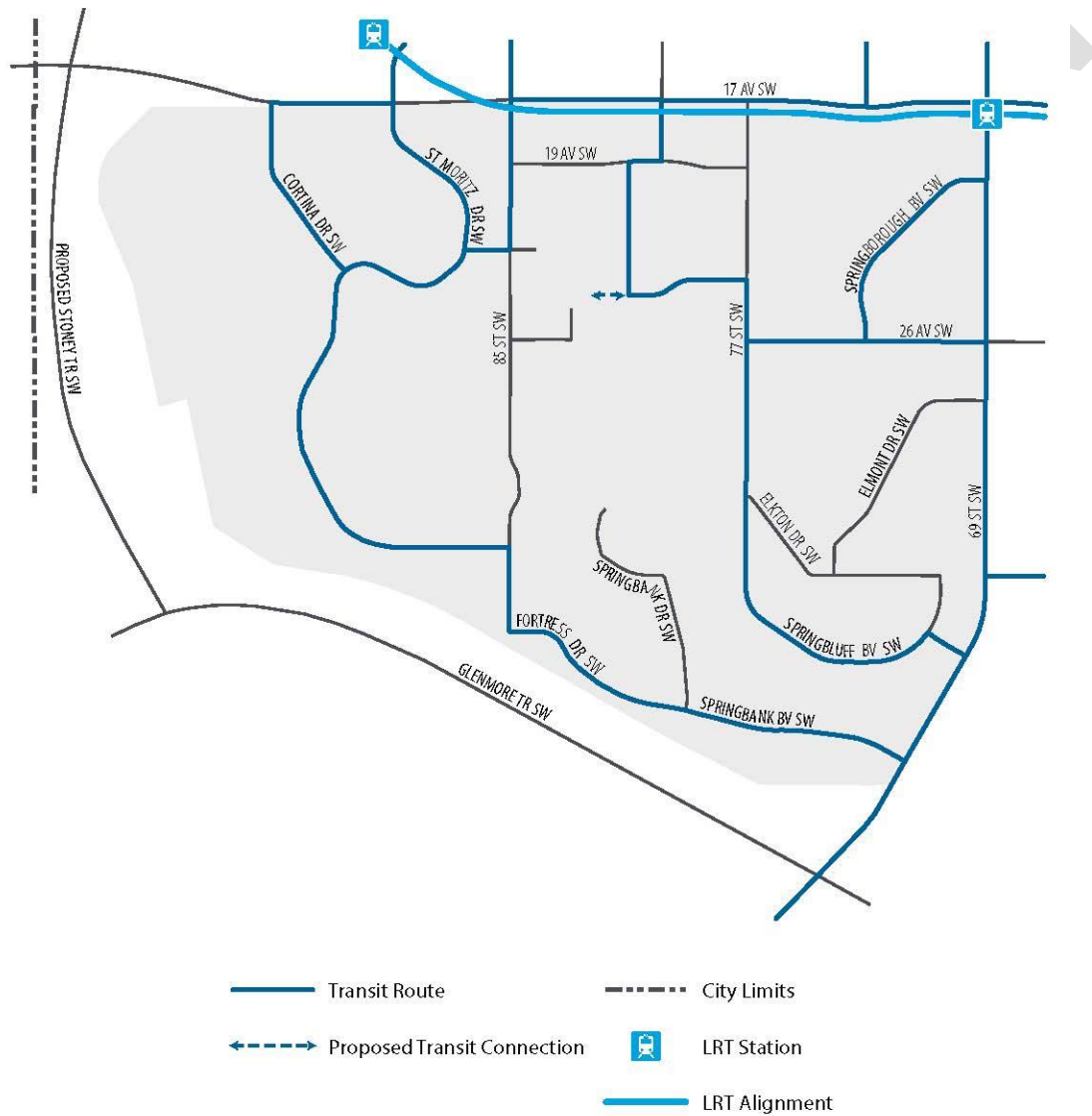
Map 4: Open Space



Proposed Amendments to the Springbank Hill Area Structure Plan

SCHEDULE D

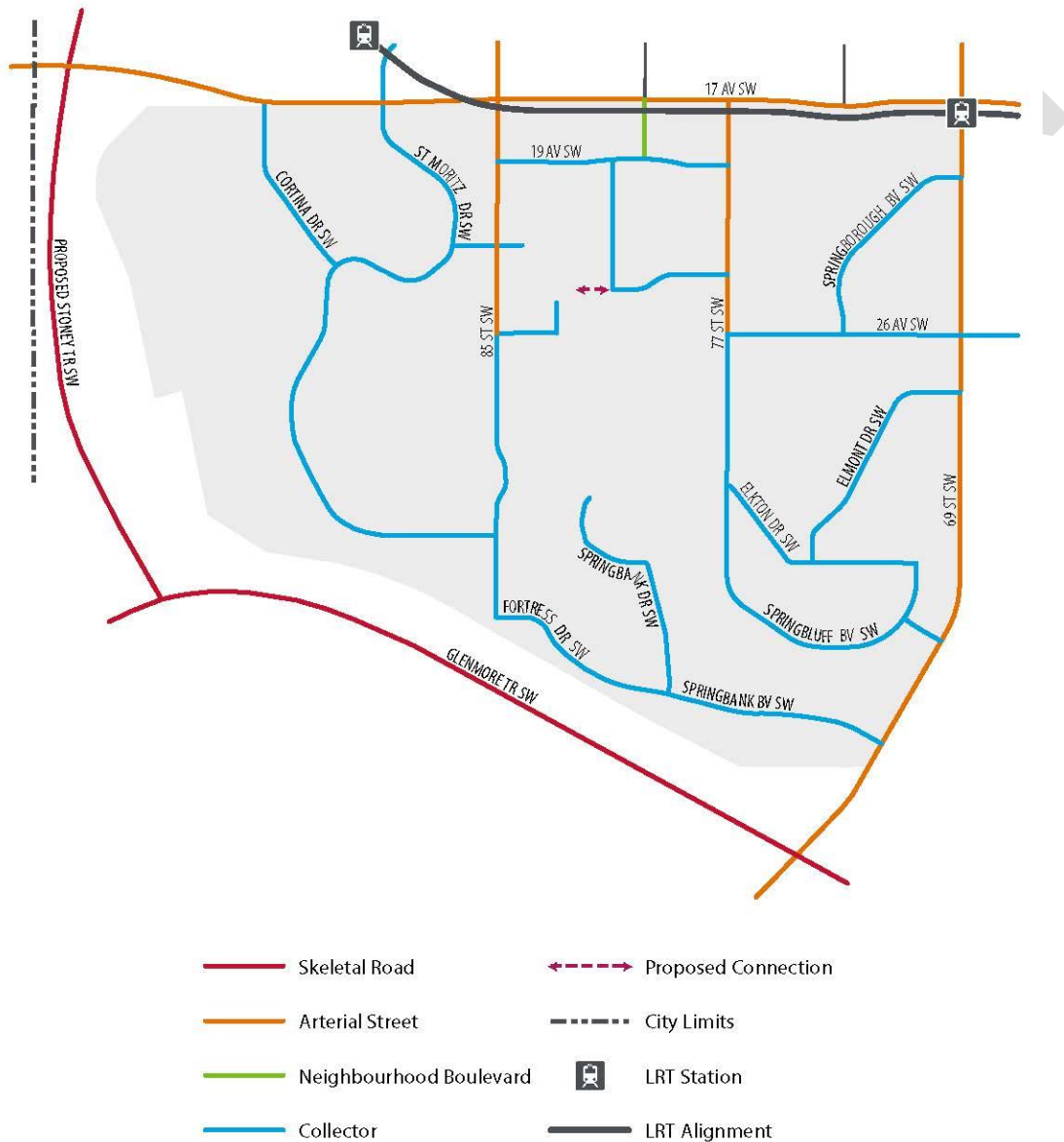
Map 5: Transit Network



Proposed Amendments to the Springbank Hill Area Structure Plan

SCHEDULE E

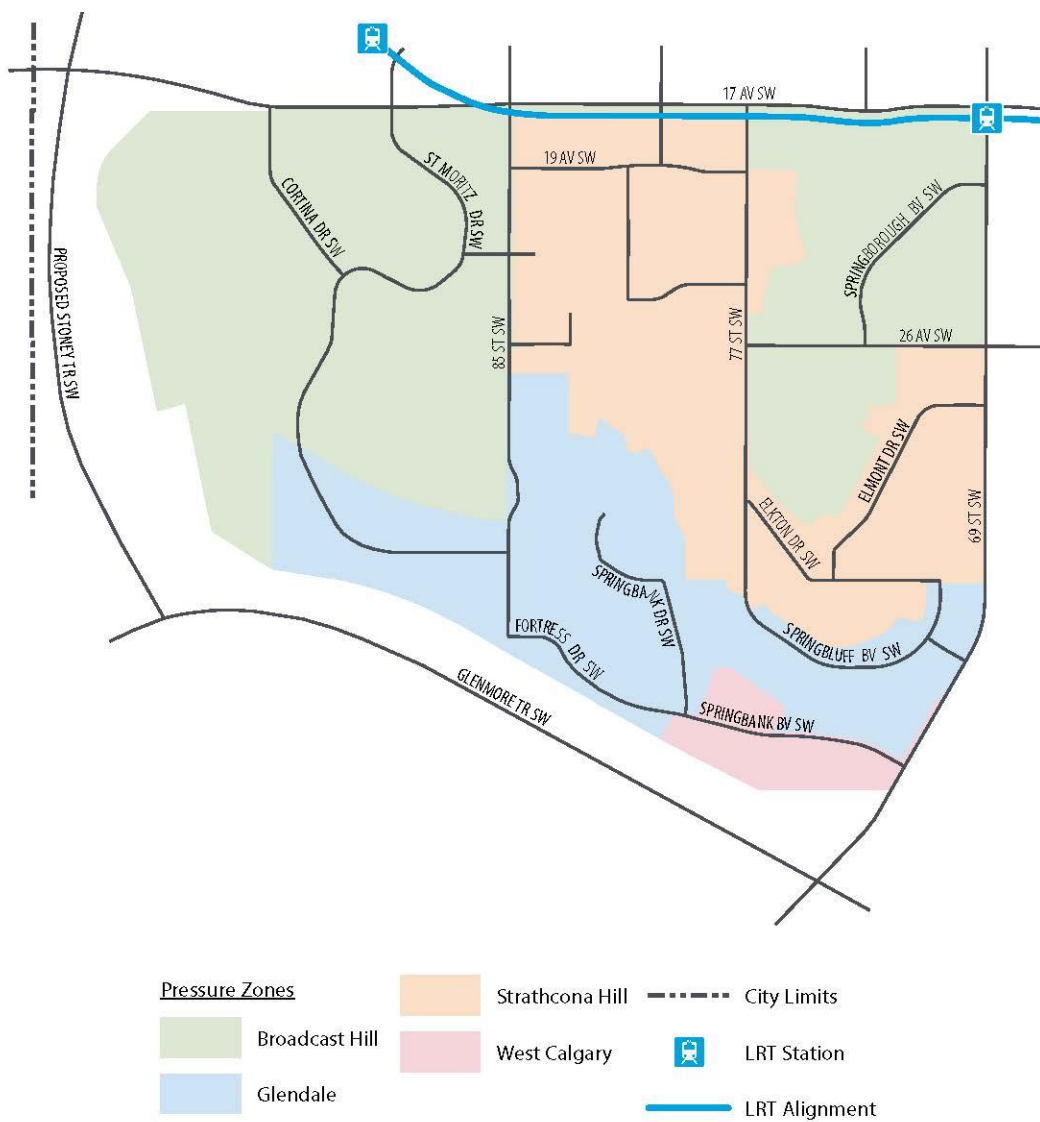
Map 6: Street Network



Proposed Amendments to the Springbank Hill Area Structure Plan

SCHEDULE F

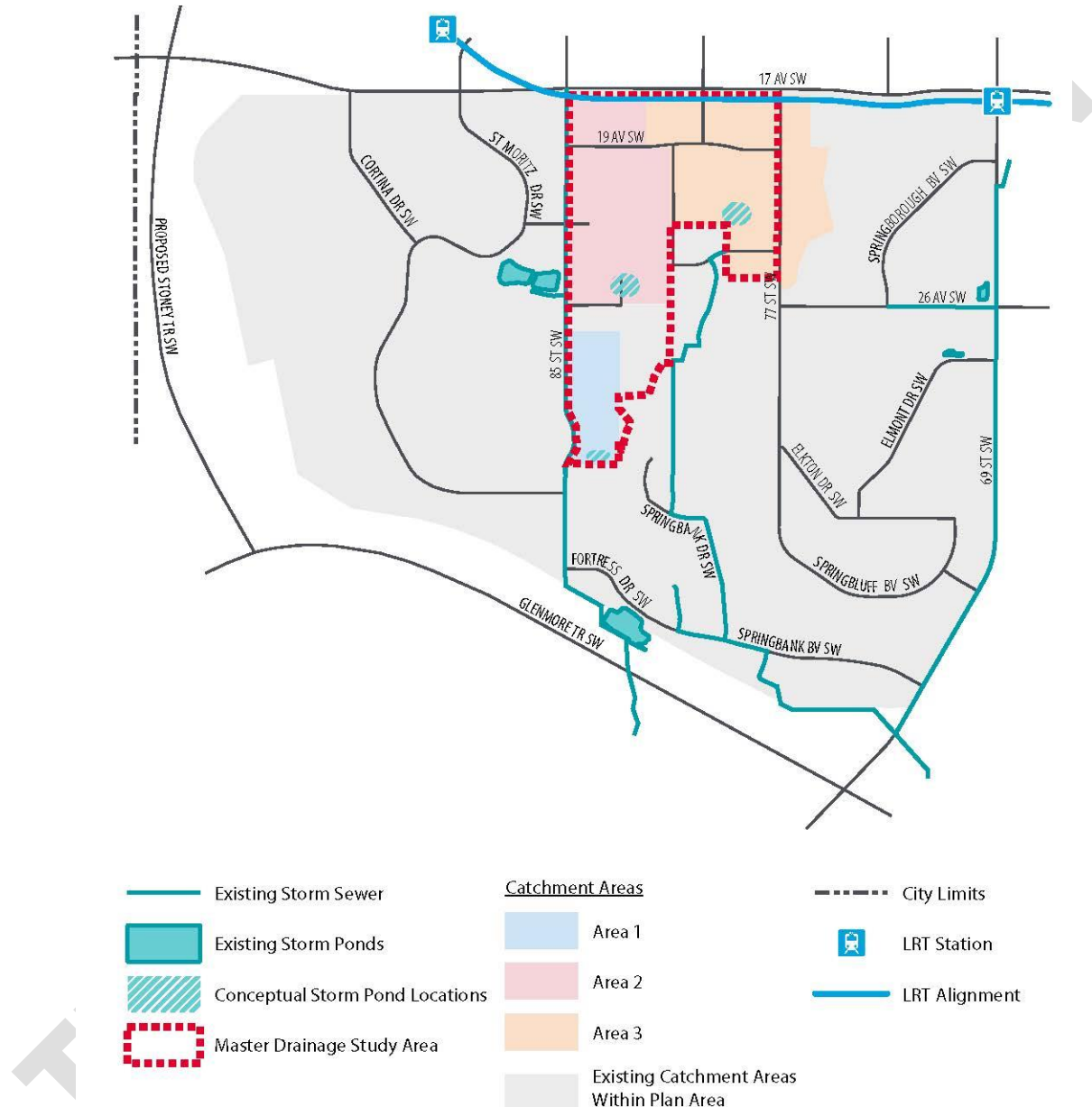
Map 7: Water Services



Proposed Amendments to the Springbank Hill Area Structure Plan

SCHEDULE H

Map 9: Stormwater Servicing



What We Heard Report

June 2019: (Excerpt from Page 6 – What We Heard Report)

WHAT WE HEARD

Participants at the open house were encouraged to leave comments on the various information boards. The following is a summary of the comments that were received.

COMMENTS	APPLICATION RESPONSE
The C-Train on 69 St should go to Mount Royal University first, to move traffic flow.	This is a transportation decision. This proposal is in response to the City's plans to extend the Blue Line to 85 St SW. It strives to place new density closest to the LRT, in order to mitigate traffic impacts and support transit ridership. Additionally, the proposed 19 Ave SW will also alleviate traffic impacts by providing a new east-west connection through the community.
The maximum height of all development should only be 6 stories.	Much of the development will be 6-storeys and below. To place the highest concentration of future residents near the LRT, the proposal includes taller buildings in the north. This is in alignment with the ASP's mixed-use area.
Density is a main concern. Particularly for traffic going south on 85 St SW.	A transportation impact assessment has been submitted in support of the proposed development. As development occurs, including parcels to the south and east of the subject site, the City will need to respond with appropriate road network improvements.
There is already too much commercial activity in the area.	The commercial activity envisioned for the proposed development is in response to the anticipated population increase for the area. New commercial developments will be rationalized through appropriate market analyses.
No 10 storey buildings. 17 Ave SW is very busy and unsafe.	The proposed taller buildings in the north of the development are in alignment with the ASP. Taller buildings do not automatically increase traffic. Traffic impacts are a result of unit counts and the inclusion of Transportation Demand Management measures, or lack thereof. The community will have the opportunity to respond to proposed unit counts and detailed design at the Development Permit stage.
Need to consider emergency vehicle access.	The transportation impact assessment prepared for this project suggests that the surrounding road network will continue to function appropriately. Emergency vehicles will be able to access the site and circulate through the community safely and efficiently.

Community Association Letter

2019 October 28 – *via email*

To Joseph, Morgan and Melanie,

On behalf of the planning committee for the Springbank Hill Community Association I am providing you with our updated comments regarding the land use amendment plan for the Slokker proposed development at 85th Street & 17th Avenue SW.

Based on our interpretation of this land use application we are unable to provide our support at this time.

The Springbank Hill ASP was passed and approved by Council in the Summer of 2017. One of the primary objectives of the ASP was to create a cohesive plan for the 190 acre study area. By creating 5 neighborhood area density zones, placement of neighborhood nodes, and a neighborhood activity centre, the intended goal was to avoid the potentially inconsistent development approach that might otherwise occur as a result of having over 15 different land owners and developers in the study area.

At this time only 5 applications have been reviewed by CPC and council, within the 190 acre study area. Four were within the bounds of the ASP in terms of density, and build form. The 5th application requested a density increase of 150% without an ASP amendment, and we understand is currently under review. These 5 applications account for less than 20% of the 190 acre study area.

The subject of this letter, LOC2018-0085, represents the 6th application which will soon go to CPC for review.

As a result of our discussions with city planning, and with the applicant, we are very concerned that there appears to be a willingness to allow substantial increases in density and modifications to build form, and an increase in the amount of retail/commercial development versus limits originally established in the ASP. We are also concerned about the apparent lack of a formal mechanism or process to address the cumulative impacts of individual development changes on the overall 190 area study area, in terms of infrastructure needs, additional traffic impacts and other impacts to the existing community. We are also very disappointed with the city in that after many years of work and engagement with the community to establish an approved ASP in 2017 you appear to be prepared to dismiss the requirements and limits in the ASP document that were established by you and accepted by the community.

In reviewing this land use application there are several issues we wish to raise:

1. Request to change the ASP 'medium density' zone to 'mixed-use zone' in the area south of 19th Ave

The applicant has requested an ASP amendment to change the medium density zone to mixed use south of 19th ave along 85th street. Our understanding after discussing this with the planning department is that the applicant believes that the community will support additional retail offerings.

We are concerned that pushing retail development further south is outside the scope of the ASP and seems to be excessive in our opinion. We note that there is 260,000 sq ft of retail planned less than 100 metres from this location on the north side of 19th ave as per the ASP. There is

Community Association Letter

also an existing retail node less than 400 metres south of this location on the west side of 85th street. In addition, the existing Aspen Landing shopping mall is less than 400m north of this location. We had previously expressed concerns about potential traffic issues in our community and it is our believe that the additional retail will only make the problem worse.

The also wish to note that the ASP identifies a goal to achieve a pedestrian friendly 'livable street' on 19th Ave, incorporating a blend of retail and residential. We are very concerned that the proposals to date are not incorporating the desired design features for this mixed use area as originally defined in the ASP. We supported the following design elements when the ASP was approved but we have yet to see these elements included in any of the proposed developments:

- All development sites shall provide short, direct and convenient multi-modal connections to the community
- Active Modes Crossings shall be integrated within the site
- At-grade units should be oriented to the street and provide a seamless at-grade transition to the public sidewalk
- Sites shall incorporate a pedestrian-scaled lighting and street furniture treatment that contributes to a high quality urban environment
- Parking should be located underground where possible
- Where surface parking areas are considered, they should: a) be located at the rear or side of buildings and screened with landscaping; and b) incorporate Low Impact Development (LID) treatments to reduce environmental impacts

As a specific example, we are concerned with the proposed design of the condo building on the southeast corner of 85th St and 19th Ave SW. As shown in the attached illustration provided by the applicant, a retail mall is incorporated into the first floor of the a condo building with no street level access from 85th or 19th ave, a large surface level parking lot. This is not a pedestrian friendly design! We can't expect to achieve the goals stated in the ASP if we continue to allow the same type of automobile friendly designs!



Community Association Letter

We also request that the planning department provide the community with a sound basis for their recommendation to amend the ASP, only two years after establishing the original ASP guidelines.

2. Request to change the ASP 'Low Density' zone to 'Medium Density', south of the new proposed mixed use zone on the south side of 19th Ave

We understand that the applicant is requesting to change the 'low density' zone in their original application to 'medium density'. This request would increase the ASP planned density by 135%. We also note that the ASP requires a transition and variety of build forms which may not be addressed in this zone.

We also understand that the planning department would consider the amendment from low density to medium density to create a better transition to the proposed new mixed use modification. We believe the critical discussion will be the decision for the mixed use area, and if there is no support for that decision then our view is that the low density area should remain unchanged.

3. Request to change the ASP 'Medium Density' zone south of 19th ave, east of the ravine, to DC

The applicant is effectively requesting a density increase of 40% within the proposed DC area, over the original ASP medium density zone. The planning department criteria for approving this appears to be based on the fact that this location is adjacent 19th ave which is planned to be a 'livable street'. The designation of 19th Avenue as a 'livable street' was the original intention of the ASP so we question why the original zoning in the ASP is no longer relevant.

4. Insufficient development plans have been submitted

In reviewing the land use application and submitted development plans to date, preliminary development plans have been submitted for only 25% of the land use area. We are questioning how this land use application can proceed without additional information regarding development plans and an understanding of the full impact on densities versus the limits placed in the original ASP.

5. Traffic Study Requirements

We have made numerous requests to the city traffic department and planning department, as well as the applicant, to review the traffic impact analysis of this and other developments. To date, we have not received a satisfactory response. The community had been given information on the traffic analysis based on the initial ASP plans but we are unaware of any updated traffic impact studies. As we believe the study area has a very limited capacity to accept increased traffic we request further analysis and discussion on this matter.

6. Pathways and Connected Communities

Given the current patchwork of development along 85th street the ASP vision of a continuous pathway through the community seems to have been lost, leading to disconnected paths, streets designated as pathways where traffic densities are unknown, a pathway now suggested on the westside of 85th (the original plan was to run paths on the east side), but without clear

Community Association Letter

development plans we are concerned about the city's ability to deliver on it's vision. In reviewing current development plans, placement of residential buildings are being positioned to limit multi-modal traffic in an east west fashion, in essence creating a gated community, purposely making it difficult for the community at large to traverse the new developments. This is at odds with the city's planning criteria.

In summary, given the above 6 concerns, we cannot support this application. We are planning to meet with the applicant to discuss their plans in more details and to present our concerns. In addition, our community association requests that the city require the applicant to submit concurrent development plans for the entire land use application area, so that the planning department, CPC, council and the community can fully understand the impacts of the revised proposal to the overall 190 area study area development.

Finally, we question why the city would even consider deviating significantly from the original ASP plans for this area. The community has accepted the intentions of the original ASP, any amendment should require new community engagement sessions otherwise the entire ASP process loses credibility with the community.

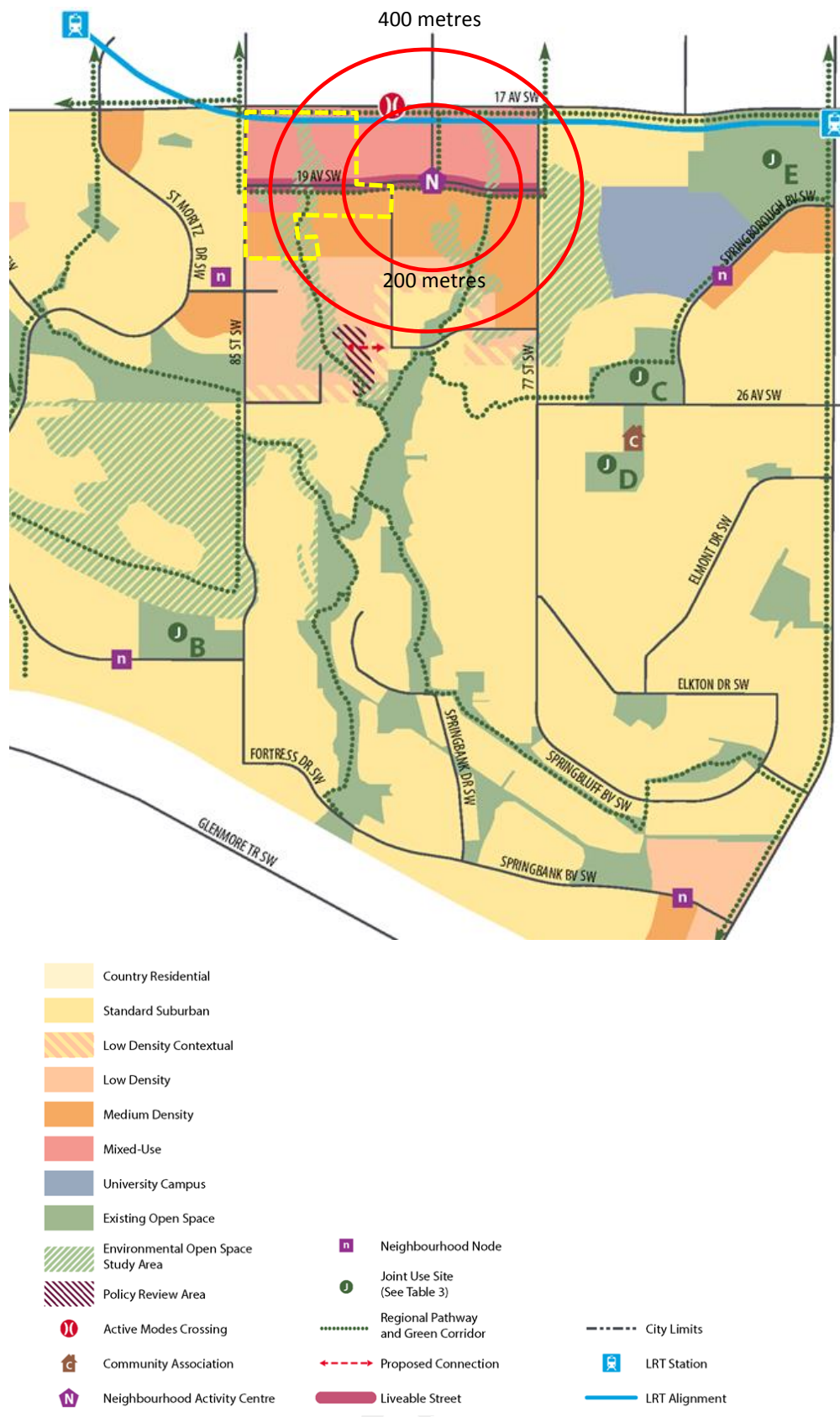
Sincerely,

Elio Cozzi

President, Springbank Hill Community Association

website: springbankhill.org

Location Context Map – Future Neighbourhood Activity Centre (NAC)



Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1333

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

EXECUTIVE SUMMARY

This application was submitted on 2018 April 19 by O2 Designs on behalf of Aspen Springs GP Ltd (Slokker Canada West). The application proposes a framework for the future subdivision and development of 10.65 hectares \pm (26.32 ac \pm) on five contiguous parcels currently comprised of residential acreages in the southwest community of Springbank Hill. The subject area is located adjacent and to the south of 17 Avenue SW and along the eastern portion of 85 Street SW. The extension of 19 Avenue SW from 80 Street SW is also being considered through the subject lands. The application provides for:

- residential and mixed-use developments of up to 50 metres in height (13 – 15 storeys) where the existing land use district limits this site to country residential development;
- the accommodation of residential and commercial uses and built forms that complement its proximity to a Neighbourhood Activity Centre that is within walking distance;
- An anticipated 1,235 dwelling units in a mix of low, medium and high density residential building forms comprising of townhouse and apartment buildings (MU-1, DC, M-1);
- Approximately 1.01 hectares (2.51 acres) of Municipal Reserve (MR) in the form of neighbourhood parks and open spaces (S-SPR);
- Approximately 0.62 hectares (1.53 acres) of Environmental Reserve (ER) in the form of natural areas to be conserved (S-UN);
- The location of future local and collector roadways, utilities and services; and
- Future dedication of road right-of-way to accommodate the extension of 19 Avenue SW to 85 Street SW, bisecting the subject site.

Two Development Permit applications (DP2019-4785 & DP2019-4791) associated with sites within the outline plan area have been recently submitted and are currently under review by Administration. These applications are for a mixed-use 48-unit residential and commercial development (at-grade restaurant and retail uses) on the site proposed to be redesignated to Mixed Use – General (MU-1f3.0h20 District); and a 16 phase (97 units total) multi-residential development for townhomes, on the Multi-Residential – Low Profile (M-1) District.

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission **APPROVE** the proposed outline plan, located at 1880, 2188 and 2220 – 85 Street SW, 2027 – 81 Street SW, 8361 and 8473 – 17 Avenue SW (Plan3056AC, Lots 26 and 27; Plan 2747HB; Blocks 28, 30, 32 and 33) to subdivide the 10.65 hectares \pm (26.32 acres \pm), with conditions (Attachment 1).

The proposed outline plan implements policies of the *Municipal Development Plan (MDP)* and the *Springbank Hill Area Structure Plan (ASP)*. One of the key elements of the application is a greater mix of housing types being accommodated in a developing greenfield community,

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1333

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

including opportunities for high rise mixed use residential and commercial development within walking distance to future LRT stations, anticipated with the extension of the Blue Line. The associated land use amendment application (CPC2019-1079) provides policy and technical rationale to support the proposed outline plan application.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

O2 Designs, on behalf of Aspen Springs Ltd (Slokker Canada West), submitted the subject application to the City with the associated land use amendment application on 2018 April 19 and have provided a summary of their proposal in the Applicant's Submission (Attachment 2). The application was submitted a year after the *Springbank Hill Area Structure Plan (ASP)* was approved by Council in June 2017.

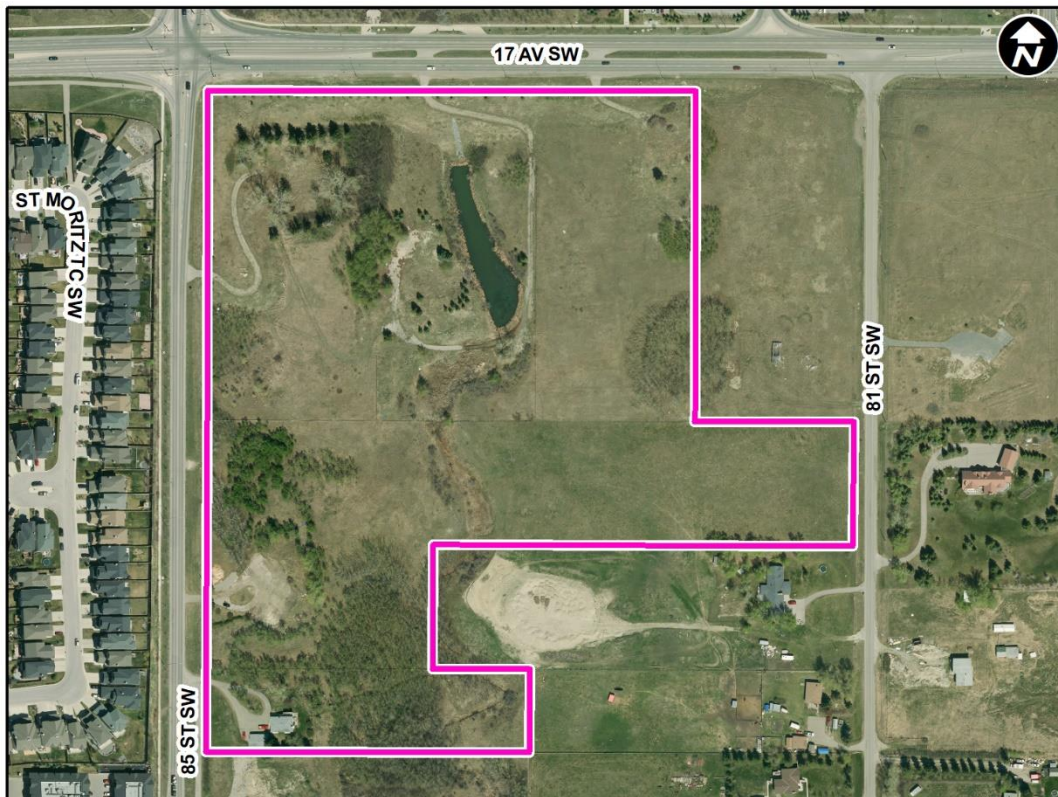
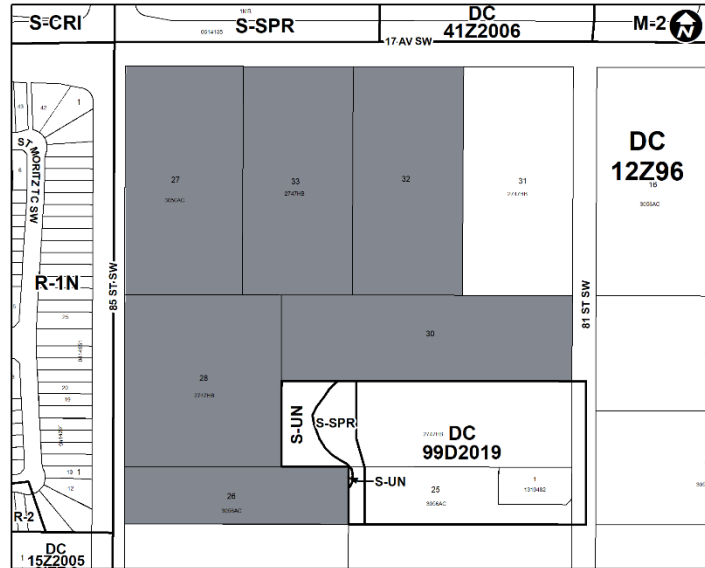
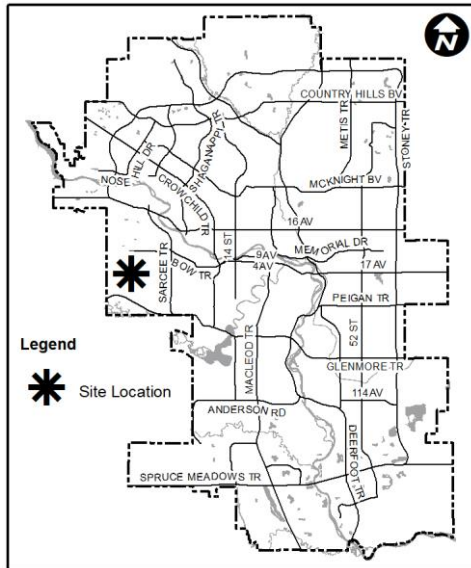
Two development permit applications have recently been submitted. The first is for a mixed-use building with 48 units and at-grade retail uses, located on the south-east corner of 19 Avenue SW and 85 Street SW; the second is a multi-residential development comprised of 97 townhomes (DP2019-4785 & DP2019-4791 respectively). Both development permit applications are currently under review by Administration.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1333

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085
(OP)

Location Maps



Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

Site Context

The subject site is situated in the southwest quadrant of the city. This site is located adjacent and south of 17 Avenue SW and along the eastern edge of 85 Street SW. The 69 Street SW LRT Station is located approximately 1.5 kilometres to the east of the site along 17 Avenue SW. All parcels forming the site are located within the *Springbank Hill Area Structure Plan*.

The subject site currently exists as six contiguous residential acreages, located at 1880, 2188 and 2220 – 85 Street SW, 2027 – 81 Street SW, 8361 and 8473 – 17 Avenue SW. The site, totalling approximately 8.67 hectares (21.42 acres), spans approximately 385 metres on its east-west axis and is roughly 345 metres running north-south. A single detached dwelling exists on one of the parcels, while the others remain undeveloped and vacant. The rolling topography of the subject site sits highest along 17 Avenue SW and 85 Street SW. A watercourse running north-south bisects the site beginning at the culvert on the north end of the site, to an impoundment (created to hold water for agricultural purposes), transitioning into an ephemeral drainage which turns into an intermittent watercourse with its associated ravine system. There are three aspen tree stands that span various parcels throughout the site. The impoundment located on the north half of the site (south of 17 Avenue SW and mid-block to the site) runs north-south through a portion of the site and will be redesigned to serve as a stormwater pond servicing the entire outline plan area upon development.

Figure 1 provides Peak Population statistics for the community of Springbank Hill. As identified in Figure 1, the community of Springbank Hill reached its peak population in 2018 with 10,052 residents.

Figure 1: Community Peak Population

Springbank Hill	
Peak Population Year	2018
Peak Population	10,052
2018 Current Population	10,052
Difference in Population (Number)	0%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [Springbank Hill](#) community profile.

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposed outline plan (Attachment 3) along with the associated policy amendments and land use amendment application will accommodate medium and high density residential and mixed use development including the dedication of open space and natural areas. The resulting land uses will contribute to the growth of the Springbank Hill community in alignment with the policies of the *Municipal Development Plan* and *Springbank Hill ASP*, supported through the proposed policy amendments. Council approved a new vision for the *Springbank Hill ASP* in 2017 which is to see distinct neighbourhoods that foster a wide variety of lifestyles, from existing country residential to urban living near amenities such as retail, open spaces and transit, while preserving natural areas. The subject application achieves Council's vision.

Planning Considerations

As part of the review of this application, several key factors were considered by Administration, including the alignment with relevant policies, consideration of amendments to the *Springbank Hill ASP* and the appropriateness of the proposed land use districts. A series of proposed upgrades in local infrastructure (new stormwater pond, water and sanitary connections, and extension of 19 Avenue SW) propel Administration to consider accommodating moderate increases in density south of 19 Avenue SW, compared to the existing policy limits for these areas found within the *Springbank Hill ASP*, upon its adoption in 2017. The following sections highlight the scope of technical planning analysis conducted by Administration that support the proposed Outline Plan.

Subdivision Design

The proposed application is approximately 10.65 hectares (26.32 acres) and responds to its proximity to the existing natural areas. These areas are to be preserved as Municipal Reserve (MR) and Environmental Reserve (ER); areas providing a logical concentration of mixed use development areas along two major arterial streets (85 Street SW and 17 Avenue SW) and the extension of a modified collector (19 Avenue SW) between 81 Street SW and 85 Street SW. This extension of 19 Avenue SW, designated a liveable street is to be dedicated at the subdivision stage and will connect developments within the subject site to future mixed use areas and the Neighbourhood Activity Centre (NAC) to the east, subject to a separate Land Use and Outline Plan Application, currently under review by Administration (LOC2017-0386).

The proposed outline plan area features portions of areas to be redesignated to the Special Purpose – City and Regional Infrastructure (S-CRI) District. A 0.28 hectares (0.68 acres) portion of the site along 17 Avenue SW is to be dedicated as LRT right-of-way, to be designated as an S-CRI District. This dedication is to occur at the subdivision stage. In addition, an area of 0.57 hectares (1.43 acres) is intended to form part of the future storm pond and associated utilities servicing the outline plan area, thus also to be redesignated to the S-CRI District (Attachment 4).

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

Overall, the design of the subject site (Attachment 3) is based on the larger existing grid network with privately maintained internal road network to be established through comprehensively designed developments at the development permit stages, and future subdivision of the various development parcels. The street network, bike lanes, regional and local Pathways through public and private open spaces provide connections within the proposed community and to the existing and future surrounding communities.

There are two residential and four mixed use land use areas within the plan. The northern half of the site between 17 Avenue SW and 19 Avenue SW comprises of three of the largest mixed use areas of greatest density and intensity within the outline plan. The proposed outline plan (Attachment 3) anticipates parcels of areas that are 1.39 hectares, 1.96 hectares and 1.47 hectares each (3.44 acres, 4.83 acres and 1.47 acres respectively). All three parcels are proposed to be designated as Mixed-Use – General (MU-1f5.0h50) Districts that is intended for a mix of residential and commercial uses in street-oriented apartment buildings (Attachment 4). The anticipated density of this northern half of the outline plan is 215 units per hectare. According to the applicant, these parcels are to be predominantly residential where 95 percent of the projected density and floor area is to be allocated for residential uses. This is evident in the outline plan (Attachment 3) that suggests a total of 898 dwellings units anticipated and 52 home-based jobs are being assumed. The fourth mixed use area is proposed at the southeast corner of 19 Avenue SW and 85 Street SW, proposed to be designated Mixed-Use – General (MU-1 f3.0h20) District that is 0.59 hectares (1.47 acres). Density in this mixed-use area is anticipated to be 97 units per hectare (uph), anticipated to create 93 retail jobs and 3 home-based jobs being assumed.

Two residential areas are proposed on the southern half of the site, where 19 Avenue SW, designated a liveable street functions as the transition zone between the two land use areas. The first parcel is proposed to be located along 85 Street SW, adjoining the smaller mixed-used parcel to the north that also fronts onto 19 Avenue SW. This parcel is proposed to be redesignated to the Multi-Residential – Low Profile (M-1) District, intended to accommodate multi-residential development of low height and medium density. Additionally, this district provides transitions in building heights, density and massing between mixed use areas, and low density residential areas south of the subject site. A development permit application was submitted on 2019 September 16, proposing a 97 unit townhouse development, aggregated into 16 phases (buildings). Administration is currently undertaking its technical review of this development permit application. The second residential parcel is proposed to be redesignated to a Direct Control (DC) District. The proposed Direct Control District is based on the rules provided in the Multi-Residential – Medium Profile (M-2d210) District. This DC District provides an increase in maximum building heights from 16 metres in the M-2 District to 20 metres, accommodating buildings up to 6-storeys, as envisioned in medium density areas by the *Springbank Hill ASP*. Additionally, a moderate increase in density (from 148 uph to 210 uph) is also being proposed in this DC District. Based on the proposed parcel area (1.17 ha), the maximum number of units is increased from 173 units to 246 units, representing a net increase of 73 units, feasible on the subject site. The outline plan anticipates a total of 226 units on this site. A development permit application has not been submitted for this site.

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

Open Space and Natural Areas

Open space is provided to preserve the existing north-south ephemeral drainage/ intermittent watercourse and associated ravine. Recent land use and outline plan applications for lands to the south and east have already preserved these features within the same network. This application will contribute to a continuous open space system in accordance with the *Springbank Hill ASP*.

A portion of the subject site (1.01 hectares or 2.51 acres) is proposed to be designated as S-SPR District with an MR designation, and will meet the 10 percent MR requirement for the subject area, anticipated for future subdivision. The park area north of 19 Avenue SW will accommodate the proposed stormwater pond flanked by MR to the west and a privately owned, publicly accessible open space to the east. A 4.0 metre maintenance access road will double as a park pathway and in concert with Local Pathways will create a loop around the stormwater pond. Part of this area is anticipated to be a naturalized park, with the area south of the pond proposed as an open lawn for active and passive recreational uses containing a re-aligned piped drainage within a culvert. The park area south of 19 Avenue SW will function as a naturalized park containing a Regional Pathway, and a Local Pathway and will act as a buffer between ER lands to be protected and developed areas. The Regional Pathway is proposed along the east side of 85 Street SW to the south side of 19 Avenue SW, where it continues to the east along this road until it enters the west side of the ravine where it will continue south and will link with future connections generally as per the *Springbank Hill ASP*.

An environmentally significant natural area (0.62 hectares or 1.53 acres) is proposed as S-UN District, to be dedicated as ER. This area will preserve the existing north-south ephemeral drainage/intermittent watercourse and ravine system.

Land Use

The subject site is currently designated DC Direct Control District ([Bylaw 12Z96](#)) that accommodates country residential development. Its purpose is to maintain the character of the existing rural residential neighbourhood. The *Springbank Hill ASP* envisions this area to be a vibrant community with a mix of commercial and residential uses and a broad range of housing types and densities. The proposed redesignation is supported by the proposed amendments to the *Springbank Hill ASP* to primarily recognize upgrades in local infrastructure; appropriately accommodating moderate increases of densities in specific areas within the subject site. The *Springbank Hill ASP* envisions this area to be a vibrant community with a mix of commercial and residential uses and a broad range of housing types and densities that are complementary to the adjoining Neighbourhood Activity Centre (NAC) that is within 400 metres, east of the subject site. Amendments to the *Springbank Hill ASP* and the proposed redesignations are supported by local upgrades to infrastructure and community amenities that were not originally contemplated in Council's initial adoption of the *Springbank Hill ASP* in 2017. More specifically, the proposed moderate increases in density south of the proposed 19 Avenue SW extension will accommodate land use patterns that are consistent to those existing east of the ravine;

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

providing for gradual transitions in density between mixed use areas along 17 Avenue SW, towards low density residential areas further south of 19 Avenue SW.

Concurrent with the outline plan, is a land use amendment application (CPC2019-1079) proposing to redesignate the subject lands from DC Direct Control District (Bylaw 12Z96) to the following districts:

- Mixed Use - General (MU-1f5.0h50) District;
- Mixed Use – General (MU-1f3.0h20) District;
- Multi-Residential – Low Profile (M-1) District;
- Direct Control (DC) District;
- Special Purpose – School, Park and Community Reserve (S-SPR) District;
- Special Purpose – City and Regional Infrastructure (S-CRI) District; and
- Special Purpose – Urban Nature (S-UN) District.

The outline plan provides the framework for community and infrastructure build out, to be implemented largely at the subdivision stage. The land uses (Attachment 5) are complimentary to this plan and provide more certainty on densities, unit types and forms.

Density

The associated outline plan will accommodate development achieving both the *Municipal Development Plan (MDP)* and *Springbank Hill ASP* minimum density and intensity targets (population and jobs). This will support future transit and community amenities within the great community of Springbank Hill.

Overall, this proposal accommodates 1,235 dwelling units (Attachment 4); resulting in a residential density of 178 units per hectare (68 units per acre) and minimum intensity of 22 people and jobs per hectare across the subject site.

More specifically, the associated outline plan meets the minimum intensities for the land typologies set out in the *Springbank Hill ASP*, outlined in Table 1 below.

Table 1: Minimum and Anticipated Intensities

Land Typology	Minimum Required Density per ASP (uph)	Anticipated Density (uph)	Minimum Required Intensity per ASP (population and jobs per hectare)	Anticipated Intensity (population and jobs per hectare)
Medium Density Residential	75	136	-	-
Mixed-Use Commercial/Residential	100	203	125	321 people and 22 jobs

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

Environmental

A Phase I Environmental Site Assessment (ESA) was conducted by Pinchin West for the plan area. The Phase I ESA report was reviewed by Administration; no concerns were noted and further investigation deemed unnecessary.

A Biophysical Impact Assessment (BIA) was prepared by Ghostpine Environmental, providing an assessment of existing site conditions. Key findings of the BIA include:

- The existing drainage/coulee system that qualifies as ER has been designated accordingly.
- The impoundment (created to hold water for agricultural purposes) and area surrounding the impoundment (within northern half of site) is disturbed and not considered a natural area;
- The same impoundment contains little riparian vegetation and is significantly sloped; and
- Rehabilitation of the impoundment into a naturalized pond including shallower margins and native vegetation may improve wildlife habitat and connectivity for certain species.

A Hydrogeological Assessment was conducted by MPE Engineering, investigating the source water emanating from the impoundment. Key findings of this assessment include:

- Test results indicate the impoundment having a strong stormwater component; similar to the upstream stormwater pond on the west side of 85 Street SW;
- Source of water can be partially derived from adjacent stormwater ponds or a combination of localized shallow ground and stormwater runoff from 17 Avenue SW; and
- No evidence the impoundment having any significant source of natural springwater.

A Geotechnical Investigation was conducted by Almor Testing Services Ltd., evaluating the subsurface soil and groundwater conditions within the site. No significant concerns were identified by Administration. A Deep Fills Report will be required to be submitted and approved at either tentative (subdivision) or development permit stages.

Transportation

As part of the associated outline plan (CPC2019-1333), the extension of 19 Avenue SW (modified collector) is proposed from 81 Street SW; terminating at 85 Street SW, the western extent of the subject site. 19 Avenue SW is proposed to bisect the subject site, completing the east-west connection that primarily serves the mixed-use and medium density land use areas within the community. This extension begins as a 23.2 metre multi-modal Right-of-Way (ROW) from the east end of the site, and narrows down to a 15 metre ROW where it crosses the ravine. Narrowing the cross-section through this area results in removing the on-street parking lanes through this section. The benefit to the narrowing of this ROW across the ravine is the reduction of impacts to these open space and natural areas. Additionally, the narrowing will encourage slower vehicle speeds and minimizes the distance animals would have to navigate over 19

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

Avenue SW. This ROW returns to a 23.2 metre multi-modal ROW and terminates at 85 Street SW.

A Transportation Impact Assessment (TIA) was submitted as part of the application and it demonstrates that the proposed development, along with the Transportation Network outlined in the ASP, functions within acceptable levels of service.

The exact street Right-of-Way (ROW) requirements and street cross sections to accommodate development in the area are under discussion and will form part of future tentative plan applications. This approach ensures alignment amongst the area developers, and allows the developer and Administration to ensure roadways are designed to accommodate all modes of travel and accommodate future development within the entire ASP area. Localized impacts from the future LRT extension will also be further evaluated with the appropriate tentative plan applications.

Extension of the West leg of the LRT (Blue Line) is anticipated along 17 Avenue SW; from 69 Street SW station (approximately 1.5 km east of the site). Dedication of LRT right-of-way is required prior to approval of the first tentative plan.

Future Transit connections (bus routes) are to be accommodated along 85 Street SW, serving the western half of the community, and 81 Street SW that is to tie into an existing bus route serving the eastern half the community that loops back to the 69 Street LRT station.

Regional pathways will be located along the southern edge or 19 Avenue SW, connecting into the open space network through the southern half of the outline plan area, swinging up towards 17 Avenue SW, from the western edge of 85 Street.

The active modes crossing spanning the ravine (located in the southwest portion of the site) was projected by the ASP to provide pedestrian and cycling connections, in lieu of a roadway. However, Administration reviewed a proposal by the applicant to extend 19 Avenue SW across the ravine between 81 Street SW and 85 Street SW. As this proposed road extension has been accepted by Administration, it now also serves the purpose of an active modes crossing. For such reasons Map 2 (Land Use Concept) in the *Springbank Hill ASP* is proposed to be amended accordingly.

Utilities and Servicing

Water Servicing will be provided through an existing connection to the water feeder main along 17 Avenue SW, and a new main along 19 Avenue SW, to be installed under the extension of 19 Avenue SW. The proposed Strathcona Flow Control Station, located at intersection of 17 Avenue SW and 85 Street SW, must be in place before any utilities are operational or any construction permissions are granted. The Strathcona Flow Control Station is City funded infrastructure and is scheduled to complete construction by Q2 2019.

A Regional Sanitary Servicing Study was provided by Pasquini & Associates and Urban Systems that includes all areas subject to the *Springbank Hill ASP*. This study identified

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

downstream constraints. There are two regional options available to remove these constraints. The first option requires a developer/applicant funded extension, connecting to existing servicing along 85 Street SW and 81 Street SW. The second option is for the City (Water Resources) to front-end a pilot project to upgrade the existing downstream sanitary mains to increase their capacity to accommodate the full build-out of the Springbank Hill study area. Some parcels may be allowed to proceed without upgrades, but will be required to provide securities, proportional to the parcel's associated flow rate, and developable area.

The subject site is within the approved Springbank Master Drainage Plan (MDP) completed by Stantec in 2017. Stormwater from the proposed outline plan area is to be directed through the adjoining ravine network and existing man-made impoundment that is to be enhanced and rehabilitated to be the stormwater pond. Ultimately, stormwater is to be managed in concert with the Springbank Hill Staged Master Drainage Plan.

Stakeholder Engagement, Research and Communication

In keeping with Administration's standard practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent landowners, the Springbank Hill Community Association (SHCA) and the application was advertised online.

No letters from adjacent landowners were received.

The applicant engaged external stakeholders, holding a public open house on 2019 May 28. According to the applicant, more than 25 people attended this community information session. The open house provided residents information about the proposed development and to speak directly with the applicants. Information panels provided attendees details of the proposed land use and outline plan. An opportunity to provide feedback directly to the applicants was also accommodated. Following the public open house, the applicant submitted a summary of the feedback received on the project through a 'What We Heard' report (Attachment 6). The most noteworthy comments received are summarized below:

- Maximum height for all development should be 6-storeys;
- 10-storey buildings should not be allowed along 17 Avenue SW;
- Traffic resulting from additional density, particularly southbound along 85 Street SW; and
- There is already too much commercial activity in the area;

Subsequent to the comments provided, the applicant addressed the concerns received from the public open house. While buildings that are higher than 10 storeys are anticipated along 17 Avenue SW, the associated impact to the public realm would be limited based on the future LRT alignment of the blue line that spans the site along its northern extent. Furthermore, this edge slopes steeply down away from 17 Avenue SW, further reducing visual impacts of a build that may sit below the future alignment of the LRT right-of-way. Administration finds the accommodation of the densest developments within the Mixed Use – General (MU-1f5.0h50) District is appropriate. According to the proposed outline plan (Attachment 3) 95 percent of the density is to be allocated for residential uses with 5 percent for commercial uses. Six-storey

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

residential buildings are anticipated for sites fronting 19 Avenue SW (south side) within the Mixed Use- General (MU-1f3.0h20) District and Direct Control District. Dwellings units within the Multi-Residential – Low Profile (M-1) District would be two to three storeys.

Two Development Permit applications (DP2019-4785 and DP2019-4791) associated with sites within the outline plan area have been recently submitted and are currently under review by Administration. These applications are for a mixed-use 48-unit residential and commercial development (at-grade restaurant and retail uses) on the site proposed to be redesignated to the Mixed Use – General (MU-1f3.0h20 District); and a 16 phase (97 units total) townhouse buildings development for the Multi-Residential – Low Profile (M-1) District.

Both applications have been circulated to the Springbank Hill Community Association (SHCA) for feedback. The SHCA provided a letter (Attachment 7) that does not support the proposed land use amendment and outline plan. The primary concern of the SHCA was the increase in residential densities in portions of the outline plan south of 19 Avenue SW. Administration finds that the proposed moderate increase in densities (accommodated by the proposed amendments to the Springbank Hill ASP, explained in associated report CPC2019-1079) provides appropriate transition of densities and built forms into low density areas to the south. Furthermore, the allocation of additional density within a 400 metre walking distance to a Neighbourhood Activity Centre (NAC) to the east, along a liveable street and adjoining publicly accessible natural open spaces, reinforces the policies of the MDP to accommodate additional density in future greenfield areas. This is expanded upon in the proceeding sections. Administration anticipates more detailed transportation analysis to be conducted through the review of subsequent development permit applications, addressing traffic concerns perceived with the emerging increase of the local population and built form of this developing community.

Following Calgary Planning Commission, notifications for Public Hearing of Council, in relation to this land use amendment, will be posted on-site and mailed out to adjacent landowners. In addition, Commission's recommendation and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the *South Saskatchewan Regional Plan*, which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

Interim Growth Plan (2018)

The recommendation aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment build on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

Municipal Development Plan (Statutory – 2009)

Map1: Urban Structure of the MDP identifies the subject lands as Residential – Developing – Future Greenfield. The MDP provides guidance for the development of new communities through the policies of the ASP. The proposal meets the following MDP objectives (Section 3.6.2):

- providing a diversity of housing types;
- providing parks located throughout the community in walkable proximity to all residences; and
- creating an inter-connected, multi-modal street network.

The MDP directs Future Greenfield Areas to achieve a minimum intensity threshold of 60 people and jobs per gross developable hectare. Further, ASPs are to demonstrate how a target density of 70 people and jobs per gross developable hectare can be achieved over the life of the plan. The proposed land use amendments and associated outline plan demonstrate these minimum intensity targets will be sufficiently achieved (as shown in Table 1) in subsequent subdivision and development permit stages.

According to Section 3.6.2 of the MDP (Future Greenfield Area), new communities should be organized to include a number of distinct neighbourhoods that are defined by a 400 metre or five-minute walking distance from a NAC or Neighbourhood *Main Street*. Furthermore concentration of residential density is encouraged by this policy in areas adjacent to open space, parks, wetlands and sports fields, especially where the area is served by transit, services and other community amenities. All areas south of the proposed extension of 19 Avenue SW are located within an adjoining open space network. These areas are also 400 metres (5 minute walking distance) from the future NAC to be established as part of Land Use and Outline Plan application LOC2017-0386. Connectivity from the subject site to the future NAC is to be accommodate by way of a liveable street; an enhanced pedestrian connection established along 19 Avenue SW. For such reasons the following proposed ASP amendments are found to support policies of the MDP to accommodate a moderate increase in density in the following areas south of 19 Avenue SW:

- redesignation of a 0.49 hectare (1.22 acre) site at southeast corner of 19 Avenue SW and 85 Street SW from Medium Density to Mixed - Use;
- redesignation of a 1.31 hectare (3.23 acre) site south of 19 Avenue SW between 85 Street SW and open space network from low density to medium density; and
- additional ASP Policy under section 3.1.6 (Mixed-Use) that accommodates moderate increase in density through land use amendments for areas within close proximity to a liveable street.

Springbank Hill Area Structure Plan (Statutory – 2017)

The *Springbank Hill Area Structure Plan (ASP)* provides further direction with detailed policies for future development of this community. The subject site comprises of areas subject to mixed-

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-1333

Outline Plan in Springbank Hill (Ward 6) at multiple addresses, LOC2018-0085 (OP)

use and medium density land use policies. The ASP also identifies portions of the site subject to an Environmental Open Space (EOS) Study Area. The proposed redesignations and associated outline plan is generally consistent with the applicable policies of this plan.

Social, Environmental, Economic (External)

The proposal helps achieve a greater mix of housing types in the community of Springbank Hill.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

Risk Assessment

There are no known risks associated with this application.

REASON(S) FOR RECOMMENDATION(S):

The proposed outline plan serves to implement the objectives of the *Municipal Development Plan* which supports the efficient use of land and infrastructure by providing an outline for the future subdivision of new multi-residential districts and park spaces recommended in the associated land use application. The proposal is in keeping with applicable policies of the *Municipal Development Plan* and the *Springbank Hill Area Structure Plan*, with amendments proposed through the associated Land Use Amendment application.

ATTACHMENT(S)

1. Conditions of Approval
2. Applicant's Submission
3. Proposed Outline Plan
4. Subdivision Data Sheet
5. Proposed Land Use District Map
6. What We Heard Report
7. Community Association Letter

Conditions of Approval

The following Conditions of Approval shall apply:

Planning and Subdivision Services:

1. Existing buildings shall be removed prior to endorsement of the final instrument.
2. With each Tentative Plan, the developer shall submit a density phasing plan indicating the intended phasing of Subdivision within the Outline Plan area and the projected number of dwelling units within each phase and demonstrating compliance with minimum required densities in accordance with the Area Structure Plan.
3. A Mutual/Pathways/Easement Agreement and right of way plan shall be executed and registered on title concurrent with the registration of the final instrument at the Tentative Plan stage.
4. Relocation of utilities shall be at the developer's expense and to the appropriate standards.
5. Prior to approval of any affected Tentative Plan and/or submission of construction drawings, the proposed street names shall be submitted and approved, to the satisfaction of the Subdivision Authority. The new street name will need Council approval prior to endorsement of the legal plan.
6. Decorative street lighting (known as the McKenzie Towne Style or the 5 metre Traditional 17 Avenue SW Style) shall be installed at the developer's expense.
7. If the total area for Roads & Public Utility Lot dedication is over 30%, note that compensation in the order of \$1 for over dedication is deemed to be provided.
8. If the total area for Municipal Reserve dedication is over 10%, note that this is considered a voluntary Municipal Reserve contribution, and compensation in the order of \$1 for over dedication is deemed to be provided.
9. The standard City of Calgary Party Wall Agreement regarding the creation of separate parcels for semi-detached / townhouses / rowhouse units shall be executed and registered against the titles concurrently with the registration of the final instrument.

Development Engineering:

10. The parcels shall be developed in accordance with the development restriction recommendations outlined in the following report(s):
 - Updated Slope Stability Assessment, prepared by Geo-Slope Stability Services (File No. 19-103) and dated February 5, 2019.
 - Geotechnical Report, prepared by Almor Testing Services Ltd. (File No 030-01-17), dated May, 2017.
11. **Prior to endorsement of affected tentative plans or approval of development permits** Execute and register on all affected title(s) a geotechnical covenant by way of caveat prohibiting the development of the lands, except in strict accordance with the accepted Updated Slope Stability Assessment, prepared by Geo-Slope Stability

Conditions of Approval

Services (File No.19-103) dated February 5, 2019. A copy of the above noted report should be attached to the covenant as Schedule A. Prior to endorsement of the linen, contact the Development Engineering Generalist to initiate work on the covenant.

12. **Prior to approval of affected tentative plans/development permits**, submit a post-development slope stability assessment (analyzing the plan under anticipated post-development conditions [anticipated grading and surcharge loads from building and road infrastructure] incorporated into analysis as per Geotechnical Report Guidelines for Land Development Applications). Alternatively, provide a letter signed and stamped by a professional geotechnical engineer for the development noting that no slopes in excess of 15% are proposed.
13. **Prior to approval of affected tentative plans/development permits**, a deep fills report will be required (as per Geotechnical Report Guidelines for Land Development Applications).

Note: It was noted in the DTR response that a Deep Fills report was submitted with the latest amended plans. A Deep Fills report has not been received by The City.

14. Multi-residential sites within the plan area may require additional access points depending on the final building layout and/or total number of units to meet City of Calgary Fire Access Standards. Sites with over 100 dwelling units require a secondary public access. A third access is required for sites in excess of 600 dwelling units two of which must be public.

Note: Restrictions on the location of public accesses to multi-family sites may be enforced at tentative plan/development permit by Transportation due to classification of adjacent roads, proximity of nearby intersections, and grade/access restrictions. As a result, permitted unit counts for multifamily sites may need be restricted at **tentative plan/development permit** to meet Fire Access Standards.

15. **Prior to approval of the first tentative plan or development permit**, the Staged Master Drainage Plan (SMDP) must be approved to the satisfaction of Water Resources.
16. **Prior to approval of any tentative plan or development permit**, all **offsite** utility rights of way must be registered for deep utilities (Sanitary, Storm and Water) or the ultimate road right of way (if applicable) must be dedicated to protect the offsite deep utilities.
Note – The proposed 300 sanitary south of 19 Avenue SW runs within private multifamily parcels and land not included in this outline plan and will require URW to be registered.

Register on all affected titles, a utility easement on the adjacent parcels for the proposed public underground utilities (sanitary, storm, water) within the subject site **concurrent with the registration of the final instrument**. A draft of the easement area and agreement shall be submitted to the satisfaction of the Manager, Infrastructure Planning, **prior to the endorsement of the final linen**.

Conditions of Approval

Contact the Development Approvals Engineer, Water Resources at 268-5697 for more information regarding the width and alignment of the required easements.

17. **Prior to approval of affected tentative plan or development permit approval**, ensure that permission for downstream storm drainage through existing private culvert has been secured if ultimate storm drainage solution has not been constructed.
18. Servicing for this site is not yet fully resolved. As a result, **prior to approval of the first tentative plan/development permit**, water, sanitary, and storm servicing for the Outline Plan area is to be fully resolved to the satisfaction of Water Resources.
19. **Prior to endorsement of the tentative plan**, update the regional sanitary servicing study (as needed). Provide proposed sanitary flow from the subject site for comparison with the existing sanitary servicing study for the area to Water Resources, Development Approvals for review and approval.

An update of the overall Sanitary Servicing Study for Springbank Hill (Springbank Hills Seniors Village Pre-Application Number: PE2017-00756 Sanitary Servicing Study) prepared by Pasquini & Associates, dated January 8, 2018) will be required if the proposed flow is considerably greater than the projected flow for this site in the above report.

20. Water Servicing: The site is within Strathcona pressure zone. The proposed Strathcona Flow Control Station, located at intersection of 17 Avenue SW and 85 Street SW, must be in place before any utilities are operational or any construction permissions (including construction drawings permission & building permits) are granted. The Strathcona Flow Control Station is City funded infrastructure and is scheduled to complete construction by summer of 2019.

Note – Temporary fire coverage solutions can be explored (and required to be approved) with Water Resources & Development Engineering in advance of the FCS being operational for show homes permits (if requested).

21. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.
22. **Prior to endorsement of any Tentative Plan/prior to release of a Development Permit**, execute a Development Agreement. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
23. Off-site levies, charges and fees are applicable. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.

NOTE: for cost sharing conditions below - Cost sharing obligations to be refined/resolved at the first tentative plan/development permit from this outline plan area – The details of the construction and timing of the future servicing options is still being resolved (Sanitary & Storm).

Conditions of Approval

24. **Prior to endorsement of all affected tentative plans or release of development permits**, the developer shall provide security as determined by the City and enter into an agreement with the City to pay the developer's proportionate share of the cost of the downstream sanitary upgrade required to service the Springbank Hill Study Area.
- NOTE:** Further details will be provided as details of the sanitary upgrade pilot become available.
25. **Prior to release of the Development Permit or Prior to Endorsement of the final instrument**, make satisfactory cost sharing arrangements with Springbank Lands Company Ltd. for part cost of the existing storm sewer, sanitary sewer and watermain in 17 Avenue SW that was paid for and/or constructed by Springbank Lands Company Ltd. under Aspen Woods, Phase 01 (2004-025).
26. **Prior to release of the Development Permit or Prior to Endorsement of the final instrument**, make satisfactory cost sharing arrangements with Springbank Lands Company Ltd. for part cost, on a per hectare basis, less the payment for oversize, for the existing storm connection at 85 Street SW. Private Storm Trunk, by Springbank Lands Company Ltd. under Aspen Woods, Phase 01 (2004-025).
27. **Prior to release of the Development Permit or Prior to Endorsement of the final instrument**, make satisfactory cost sharing arrangements with Springbank Lands Company Ltd. for part cost of the existing storm water wet pond constructed in Lot 2PUL in Block 1 that was paid for and/or constructed by Springbank Lands Company Ltd. under Aspen Woods, Phase 01 (2004-025).
28. **Prior to release of the Development Permit or Prior to Endorsement of the final instrument**, make satisfactory cost sharing arrangements with Springbank Lands Company Ltd. for part cost of the private utilities in 17 Avenue SW, constructed by Springbank Lands Company Ltd. under the 17 Avenue SW Upgrading – Springbank Land Company Ltd. Special Development Agreement.
29. **Prior to release of the Development Permit or Prior to Endorsement of the final instrument**, make satisfactory cost sharing arrangements with West Gallant Developments Ltd. for part cost of the existing storm sewer and watermain in 85 Street SW, that was paid for and/or constructed by West Gallant Developments Ltd. under Aspen Woods, Phase 01 (2004-038).
30. **Prior to release of the Development Permit or Prior to Endorsement of the final instrument**, make satisfactory cost sharing arrangements with Montreux Development Inc. and 966150 Alberta Inc. for part cost of the existing storm sewer and watermain in 85 Street SW, that was paid for and/or constructed by Montreux Development Inc. and 966150 Alberta Inc. under Montreux, Phase 01 (2004-031).
31. **Prior to release of the Development Permit or Prior to Endorsement of the final instrument**, make satisfactory cost sharing arrangements with Montreux Development Inc. and 966150 Alberta Inc. for part cost of the existing storm water wet pond constructed in Lot 18PUL and 19PUL in Block 3, that was paid for and/or constructed by Montreux Development Inc. and 966150 Alberta Inc. under Aspen Woods, Phase 01 (2004-031).

Conditions of Approval

32. **Prior to endorsement of any Tentative Plan/prior to release of a Development Permit**, make repayment arrangements with the City of Calgary for part cost of the existing, paving (5.75 metres), curbs and gutters and street lighting installed along the south side in 17 Avenue SW that were constructed by Springbank Lands Company Ltd. under Aspen Woods, Phase 01 (2004-025) subdivision (and financed through Roads Program 204 (432695).

Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.

33. **Prior to endorsement of any Tentative Plan/prior to release of a Development Permit** the Developer shall make payment to the City for their share of the East Springbank Servicing Study.

34. The developer, at its expense, but subject to normal oversize, endeavors to assist and boundary cost recoveries shall be required to enter into an agreement to:

a) Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.

b) Construct the underground utilities and surface improvements within the full width of 77 Street SW, along the east boundary of the plan area.

c) Construct the underground utilities and surface improvements within the east two lanes of the four (4) lane divided arterial within 85 Street SW, along the west boundary of the plan area.

d) Construct a wood screening fence, chain link fence, sound attenuation fence, whichever may be required along the boundaries of the plan area.

e) Construct the onsite and offsite storm water management facilities (wet pond, wetlands, etc.) to service the plan area according to the most current City of Calgary Standard Specifications Sewer Construction, Stormwater Management and Design Manual and Design Guidelines for Subdivision Servicing.

f) Construct the MR within the plan area.

g) Construct the regional pathway within and along the boundaries of the plan area, to the satisfaction of the Director of Parks Development.

Transportation:

35. **In conjunction with the applicable Tentative Plan** the Utility right-of-way along 17 Avenue SW shall be dedicated to the satisfaction of the Director, Transportation Planning. The Utility right-of-way shall include lands to accommodate the future West LRT extension, including any ancillary features (substations, retaining wall structures, operations and maintenance access easement, and grading) necessary to accommodate Utility requirements.

Conditions of Approval

36. **In conjunction with the applicable Tentative Plan** that includes the parcels bounding 17 Avenue SW and 85 Street SW, transportation analysis and functional planning for the intersection of 17 Avenue SW and 85 Street SW shall be completed to the satisfaction of the Director, Transportation Planning. The transportation analysis shall include review and assessment of access to the subject lands for all travel modes and shall include widening necessary to accommodate vehicular movements as well as pedestrian and cycling connections within and adjacent to the plan area.
37. **In conjunction with each Tentative Plan**, two accesses are to be constructed and open to the public for each tentative plan. It is understood that construction sequencing between all land owners and developers in the area is not certain and the connections may change at tentative plan.

Each tentative plan shall demonstrate that it provides a contiguous extension of development within the outline plan area and connects to the Regional Transportation Network. The intent is to ensure transportation connectivity for all modes (walking, cycling, transit, vehicles) and facilitate interim transit routing.

Design and Construction Requirements

38. **In conjunction with the applicable Tentative Plan**, the Developer shall register a road plan for road widening required for 17 Avenue SW, to the satisfaction of the Director of Transportation Planning.
39. **In conjunction with the applicable Tentative Plan**, the Developer shall construct 17 Avenue SW to accommodate upgrades necessary to service the proposed development to the satisfaction of the Director, Transportation Planning. The upgrades shall accommodate all modes of travel, including pathway(s) that connect to the regional system north of 17 Avenue SW, and any road widening necessary to service the ASP lands.
40. **In conjunction with the initial Tentative Plan**, the Developer shall register a road plan to the satisfaction of the Director of Transportation Planning for the boundary half of 85 Street SW from 17 Avenue SW to the south plan boundary.
41. **In conjunction with the initial Tentative Plan**, the Developer shall construct 85 Street SW from the north boundary of the plan area to the south boundary of the Plan area to the satisfaction of the Director, Transportation Planning. 85 Street SW and ancillary works to support the roadway shall be designed and constructed at the Developer's expense subject to normal oversize, endeavors to assist and boundary cost recoveries.
42. **In conjunction with the initial Tentative Plan**, the Developer shall register a road plan to the satisfaction of the Director, Transportation Planning for 19 Avenue SW throughout the plan area from the west plan boundary to the east plan boundary.
43. **In conjunction with the applicable Tentative Plan**, the Developer shall construct 19 Avenue SW from 85 Street SW to the east boundary of the plan area to the satisfaction of the Director, Transportation Planning. 19 Avenue SW and ancillary works to support the roadway shall be designed and constructed at the Developer's sole expense.

Conditions of Approval

44. **In conjunction with the applicable Tentative Plan** All roads, intersections, driveways and intersection spacing shall be designed and constructed at the Developer's sole expense and to the satisfaction of the Director, Transportation Planning.
45. **In conjunction with the applicable Tentative Plan, Construction Drawings (detailed design) and Permissions to Construct Surface Improvements:**
 - a. The developer shall provide signed copies of backsloping agreements for any backsloping that is to take place on adjacent lands (whether owned privately or owned by the City).
 - b. Adjustments to the tentative plan boundary may be required to accommodate all necessary Right-Of-Way requirements for appropriate roadway and intersection design at the discretion of the Director, Transportation Planning.
46. **In conjunction with the applicable Tentative Plan or Development Permit**, the Developer shall enter into a Construction Access Roads Agreement with Roads Maintenance.
47. **In conjunction with the applicable Tentative Plan or Development Permit** for the staged construction of the road network, transit stops shall be provided to the satisfaction of the Director, Transportation Planning. All bus zones shall be located:
 - a. Where commercial areas are concentrated;
 - b. Where the grades and site lines are compatible to install bus zones; and
 - c. Where pedestrian walkways, pathways, and roadway crossing opportunities are provided.
48. Where lots exist adjacent to bus zones, caveats must be registered on the subject lots that restrict driveways or other vehicle accesses from encroaching upon the bus zone
Concurrent with Registration of the affected Tentative Plan.
49. **In conjunction with the applicable Tentative Plan**, the developer shall ensure that all pathways from MR lands have a direct pedestrian connection by sidewalk or pathway, to the desired crossing locations at intersections to the satisfaction of the Director, Transportation Planning.
50. **In conjunction with the applicable Tentative Plan or Development Permit**, all community entrance features must be located outside the public right-of-way.
51. **In conjunction with the applicable Tentative Plan**, the Developer shall provide a Letter of Credit for installing traffic signals and that are agreed upon by the Developer and the Director, Transportation Planning. Note that the Developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit, and is required to submit payment in support of the proposed Tentative Plan applications.
52. **In conjunction with the applicable Tentative Plan**, the developer shall provide a Letter of Credit for pedestrian-actuated crossing signals that are required by the Director, Transportation Planning. Pedestrian-actuated crossing signals shall be considered to the satisfaction of the Director, Transportation Planning:
 - a. where regional pathways or multi-use pathways intersect with the street;

Conditions of Approval

- b. at mid-block crossings; and
- c. at intersections or pedestrian crossings adjacent to Joint Use sites

The Developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit, and is required to submit payment in support of the proposed Tentative Plan applications.

53. **In conjunction with the applicable Tentative Plan** that propose mid-block crossings, the Developer shall design and construct infrastructure at its sole expense, as follows:
- a. Curb bump outs shall be built, at a minimum, at all proposed mid-block crossing locations to the satisfaction of the Director, Transportation Planning.
 - b. Approach grades for all proposed mid-block crossing locations shall be no more than 4%.
54. **In conjunction with the applicable Tentative Plan or Development Permit**, a noise attenuation study is required for the residential adjacent to 17 Avenue SW and the LRT, certified by a Professional Engineer with expertise in the subject of acoustics related to land use planning, shall be submitted to Transportation Planning for approval.
- Note that where sound attenuation is not required adjacent to Arterial roadways or Utility ROW containing LRT, a uniform screening fence shall be provided, in accordance with the latest version of the Design Guidelines for Subdivision Servicing.
55. **In conjunction with the applicable Tentative Plan** all noise attenuation features (noise walls, berms, etc.) and ancillary facilities required in support of the development shall be constructed entirely within the development boundary (location of noise walls, berms, etc. and associated ancillary works shall not infringe onto the road right-of-ways).
56. **In conjunction with the applicable Tentative Plan**, temporary oil and gravel bus turnaround or a cul-de-sac with a minimum radius of 15.25 metres is required at the terminus of each construction phase. Where the developer intends to fence the turnaround, the minimum radius shall be increased to 16.25 metres. If road construction and/or construction phasing affects the operations of transit service, the Developer is required to provide an interim transit route replacement, to the satisfaction of the Director, Transportation Planning. Post and cable fence is required where the temporary turnaround is anticipated to be required for a period greater than 1 year.
57. **In conjunction with the applicable Tentative Plan**, a bus zone is required at northbound 85 Street SW at 19 Avenue SW. Identify the bus zone on all plans and construct as part of the roadway upgrades in this area.
58. **In conjunction with the applicable Tentative Plan or Development Permit**, Transit shelter(s) shall be provided as stipulated by the Director, Transportation Planning and shall be supplied and installed at the Developer's sole expense. The shelter(s) shall be installed by Calgary Transit upon receipt of satisfactory payment.

Conditions of Approval

Access, Easements and Covenants

59. No direct vehicular access is permitted to or from 17 Avenue SW; restrictive covenants shall be registered against the subject lots **concurrent with registration** of the affected Tentative Plan. Exceptions may apply for accesses to pre-existing sites on an interim basis, if necessary and at the discretion of the Director, Transportation Planning.
60. No direct vehicular access is permitted to or from 85 Street SW; restrictive covenants shall be registered against the subject lots **concurrent with registration** of the affected Tentative Plan. Exceptions may apply for accesses to pre-existing sites on an interim basis, if necessary and at the discretion of the Director, Transportation Planning.
61. No direct vehicular access shall be permitted to crosswalk/wheelchair ramp locations for any proposed T intersections. Restrictive covenant shall be registered on all applicable titles **concurrent with the registration of the final instrument** to that effect at the Tentative Plan stage.
62. Public Access Easements for all proposed Public Access Easement Areas and the private pathway within the 4.5m Public Access Easement along the sanitary servicing easement, as indicated on the Approved Outline Plan, shall be executed and registered on title **concurrent with the registration of the final instrument** for the affected Tentative Plan.
63. Where lots exist adjacent to bus zones, caveats must be registered on the subject lots that restrict driveways or other vehicle accesses from encroaching upon the bus zone apron **Concurrent with Registration of the affected Tentative Plan**.

Miscellaneous

64. **In conjunction with the applicable Tentative Plan**, future revisions, addendums, or submissions within the development area are subject to further Transportation review and analysis, at the discretion and satisfaction of the Director, Transportation Planning and in accordance with the Springbank Hill Area Structure Plan policy.
65. **In conjunction with the applicable Tentative Plan**, the road and pedestrian network for future development in the area is subject to further review and analysis in conjunction with future outline plan revisions and Tentative Plan submissions.
66. **In conjunction with the applicable Tentative Plan or Development Permit**, phasing plans shall be submitted as a component of the Tentative Plan or Development Permit submission package to the satisfaction of Transportation Planning and Roads, for the staged development of Collector standard and above roadways, inclusive of the staged development of intersections to the satisfaction of the Director, Transportation Planning and Roads.

Parks:

67. **Prior to approval of the tentative plan**, any outstanding comments within the conditionally approved BIA – Biophysical Impact Assessment are to be addressed and the BIA is to be finalized.

Conditions of Approval

68. Regional and Local Pathways are to be “field fit” with Calgary Parks – Pathways and Urban Conservation in order to reduce the environmental impacts. Arrange a meeting through the Parks CPAG Generalist to field fit the Pathways.
69. There shall be no backsloping within MR/ER extents except what is currently shown on LOC2018-0085.
70. During Engineering Construction Design, ensure that the street light cable has a 1.0 metre minimum offset from the proposed boulevard trees.
71. Parks maintained infrastructure located on proposed MR (i.e. lighting, benches, etc.) shall meet the same minimum FOS – Factor of Safety requirement of 1.5 unless otherwise approved by the Director, Calgary Parks.
72. **Prior to the approval of the affected tentative plan**, finalized Landscape Concept plans for all MR and MSR sites shall be submitted for Parks’ review and approval. These plans are to contain:
 - A site plan showing general conformance to Outline Plan landscape concepts, intended park program, site layout, and preliminary planting.
 - Grading plans that are coordinated with engineering to show updated perimeter grades to confirm slope percentage and details of any other features, including (but not limited to) retaining structures, utility rights-of way, green infrastructure, trap lows, drainage from private lots, etc.
 - Storm-related infrastructure details above and below ground, including (but not limited to) access roads with required vehicle turning radii, inlets, outlets, retaining walls, control structures, oil grit separators, etc.
 - A note that tree stands will be retained within ER – Environmental Reserve extents.
 - Cross-sections showing the interface between residential lots and the proposed MR/ER including how stormwater will be addressed.
 - Clarification on Regional/Local Pathways.
 - Information on how soils and wetland vegetation material (including Narrow-leaf willow) on the west side of the ravine will be mitigated and salvaged.

A meeting is to be arranged with the Parks Landscape Construction Coordinator Group through the Parks CPAG Generalist prior to submitting the final Landscape Concepts (at Tentative Plan) and Landscape Construction Drawings to discuss the proposed MR design and function, grades, etc.

73. No disturbance of Environmental Reserve lands is permitted without written permission from the Parks Generalist for this area. The Parks Generalist (listed above) can be reached at 403-268-5635.
74. The developer shall minimize stripping and grading within the Environmental Reserve. Any proposed disturbance within the ER, including that for roadways, utilities, and storm water management infrastructure, shall be approved by Calgary Parks **prior to stripping and grading**.
75. With the submission of Landscape Construction Drawings, the developer shall include a detailed Habitat Restoration Plan including a maintenance schedule for each

Conditions of Approval

Environmental Reserve proposed to be affected by any construction. The Plan should indicate how it will be rehabilitated and restored. The restored area(s) shall be maintained by the developer until it is established and approved by Parks prior to Final Acceptance Certificate.

76. The developer shall install and maintain a temporary construction fence on the private property line with the adjacent Environmental Reserve to protect public lands **prior to the commencement of any stripping and grading related to the site** and during all phases of construction. Contact the Parks Development Inspector Jacqueline Swartz - Jacqueline.Swartz@calgary.ca or (403) 476-8317 or (403) 620-3216 for an inspection to approve the location of the fencing prior to its installation.
77. **Prior to the approval of a stripping and grading permit, a Development Agreement or a subject area Tentative Plan**, Parks requires details pertaining to the total limit of disturbance resulting from the proposed development in its entirety.
78. **Prior to approval of the related Stripping and Grading Permit or Engineering Construction Drawings**, whichever submitted first, the developer shall install Environmental Reserve (ER) protection measures around the ravines/Environmentally Significant Areas to be retained in order to prevent excessive overland drainage and siltation onto said areas during all phases of construction, in accordance with The City's "Guidelines for Erosion and Sediment Control", to the satisfaction of the Manager of Urban Development and the Director of Calgary Parks. Contact the Parks Development Inspector Jacqueline Swartz - Jacqueline.Swartz@calgary.ca or (403) 476-8317 or (403) 620-3216 to approve the location prior to commencement of Stripping and Grading activities.
79. The developer shall restore, to a natural state, any portions of the environmental reserve lands along the boundaries of the plan area that are damaged in any way as a result of this development. The restored area is to be maintained until established and approved by the Park Development Inspector.
80. The developer shall submit detailed Engineering Construction Drawings and Landscape Construction Drawings for the proposed storm pond to both Water Resources and Parks for review.
81. Construct all regional/local pathway routes within and along the boundaries of the plan area according to Parks' **Development Guidelines and Standard Specifications – Landscape Construction** (current version), including setback requirements, to the satisfaction of the Director, Parks.
82. **Concurrent with registration of the affected tentative plan**, public access easements shall be registered on any publicly accessible local pathways that are within privately owned open space.
83. Plant all public trees in compliance with the approved Public Landscaping Plan.
84. **Prior to the approval of the affected tentative plan**, the developer shall confirm fencing requirements adjacent to MR, MSR and ER parcels to the satisfaction of the Director, Calgary Parks.

Conditions of Approval

85. **Prior to approval of the first tentative plan or stripping and grading permit** (whichever comes first), it shall be confirmed that grading of the development site will match the existing grades of adjacent parks and open space (MR and/or ER), with all grading confined to the private property, unless otherwise approved by Parks.
86. **Prior to approval of the affected tentative plan** submit all applications, report and responses related to historic resources subject to Section 31 of the Historical Resources Act.
87. **Prior to approval of the tentative plan or stripping and grading permit** (whichever comes first), an onsite meeting shall be arranged to confirm that the surveyed boundaries of the environmental reserve area meet Parks' approval. A plan illustrating the surveyed ER boundaries must be provided to Parks in advance of the onsite meeting.
88. **Prior to Endorsement of the tentative plan**, Landscape Construction Drawings and/or a Habitat Restoration Plan that are reflective of the subject Tentative Plan for the proposed Private Open Space (east of the stormwater pond) is to be submitted to the Parks Development Coordinator for review and approval prior to construction.
89. **Prior to Endorsement of the tentative plan**, provide the documentation showing the discharge of URW 021 2634. Parks cannot accept ER or MR encumbered by URW's.
90. **Prior to Endorsement of the tentative plan**, the developer may require a Landscape Maintenance Agreement or Optional Amenities Agreement for the landscaping of any proposed park elements above the standard (i.e. Parkour Station, overlook, etc.). Contact the Parks CPAG Generalist for details.
91. Pursuant to Part 4 of the *Water Act* (Alberta), the applicant shall promptly provide Parks with a copy of the *Water Act* approval, issued by Alberta Environment and Parks, for the proposed drainage disturbance and proposed stormwater outfall/discharge into the ravine.
92. Until receipt of the *Water Act* approval by the applicant from Alberta Environment and Parks, the drainage/ravine affected by the development boundaries shall not be developed or disturbed in anyway and shall be protected in place.

Applicant's Submission

October 22, 2019

On behalf of Slokker Homes, O2 Planning + Design submitted this outline plan and land use amendment application regarding the subject site, comprised of six (6) parcels at the southeast intersection of 85 ST SW and 17 AV SW, to enable the development of a mixed-use neighbourhood that responds to the evolving context of the area. With low density residential uses to the south, anticipated commercial and multi-residential uses to the east, and 17 avenue SW and the future 85 avenue SW LRT Station to the north, the proposed outline plan and land use amendment enables the development of a new community that will seamlessly integrate into the existing fabric of the area.

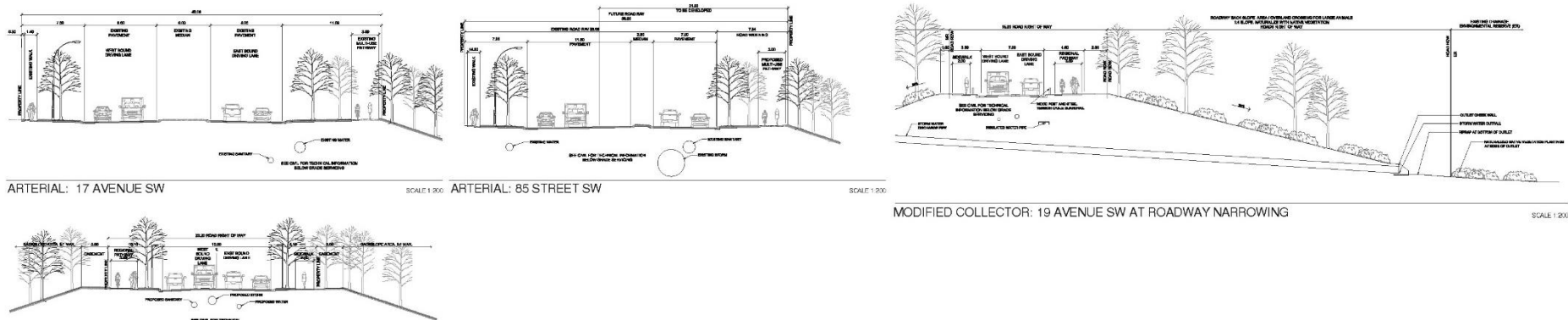
Two organizing features have guided the design of the community. First, 19 avenue will be a new east-west liveable street that will connect 85 street to 77 street. Although not contemplated in the Springbank Hill ASP, it was determined during the outline plan design that this connection was feasible and results in improved urban design by providing the opportunity for better connectivity through the site to adjacent developments and a better public realm along the street. The second organizing feature is a linear open space network that runs north south through the site which provides opportunities for passive and active recreation for residents of the community and wider neighbourhood. The green network follows the natural ravine system with municipal reserve connecting the north and south areas of the community and includes a naturalized storm pond at the north which will be an additional community amenity.

The application seeks to redesignate the northern portion of the site to Mixed Use – General (MU-1 f5.0h50). This will enable a built form and a mix of uses that responds to the site's adjacency to 17 avenue and proximity to existing and proposed transportation infrastructure. The intention is that this area will be predominantly (95%) residential with retail uses facing 19 avenue on the western edge and integrated with and facing the neighbouring development on the east. The southwestern portion of the site will be designated Mixed Use – General (MU-1 f3.0h20) and Multi-Residential – Low Profile (M-1). These designations will allow for mixed use development fronting onto 19 avenue transitioning to lower density townhouses towards the south. The southeastern area will be designated Direct Control based on Multi-Residential – Medium Profile (M-2). This designation will allow low rise multi-family development that adapts to the slope of the site. This allocation of land uses, along with the use of density and height modifiers, effectively distributes density and intensity throughout the site in a manner that responds to the local context on all four edges.

The site falls within the Springbank Hill Area Structure Plan (ASP) and is designated Mixed-Use in the north and both Medium Density and Low Density in the south. This application proposes an amendment to the ASP to enable the 19 AV SW connection and to extend the Mixed-Use designation to the south. Doing so will create an active mixed-use corner at 19 avenue SW and 85 street SW, increase connectivity throughout the area, relieve pressure from 17 avenue SW, and enable a transition between higher and lower density uses.

Throughout the application process, O2 and Slokker worked collaboratively with administration and consulted with the Springbank Hill Community Association and local residents on an ongoing basis. An open house was held on May 28 and was attended by approximately 25 people. A summary of the engagement undertaken for this project is provided in a What We Heard Report.

Proposed Outline Plan

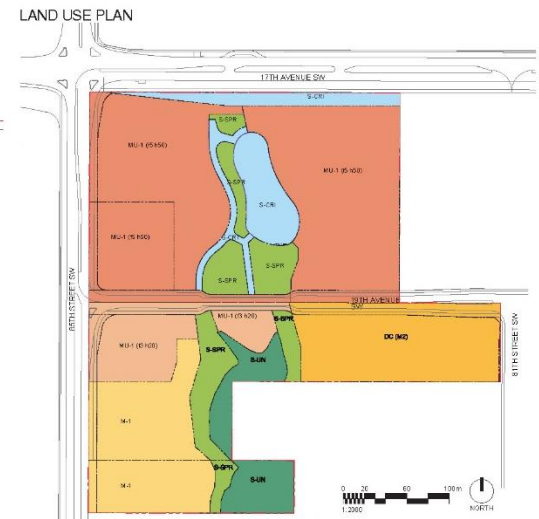
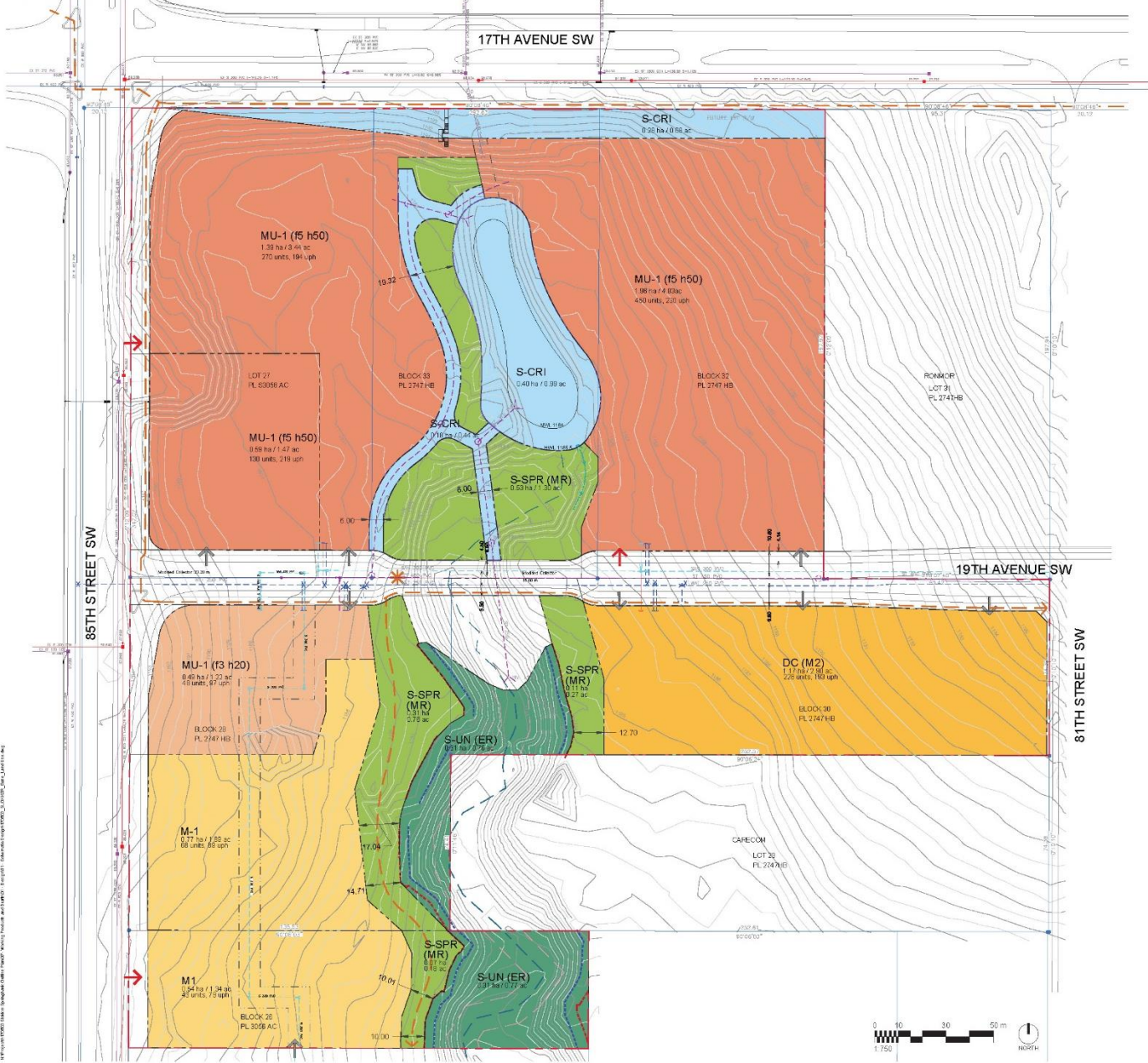


ARTERIAL: 17 AVENUE SW
SCALE 1:200

ARTERIAL: 85 STREET SW
SCALE 1:200

MODIFIED COLLECTOR: 19 AVENUE SW AT ROADWAY NARROWING
SCALE 1:200

MODIFIED COLLECTOR: 19 AVENUE SW
SCALE 1:200



LAND USE LEGEND

[Symbol]	Multi-Residential District (M-1) (R-100)
[Symbol]	Multi-Residential District (M-1) (R-200)
[Symbol]	Multi-Residential District (M-1) (R-300)
[Symbol]	Multi-Residential District (M-1) (R-400)
[Symbol]	Multi-Residential District (M-1) (R-500)
[Symbol]	Multi-Residential District (M-1) (R-600)
[Symbol]	Multi-Residential District (M-1) (R-700)
[Symbol]	Multi-Residential District (M-1) (R-800)
[Symbol]	Multi-Residential District (M-1) (R-900)
[Symbol]	Multi-Residential District (M-1) (R-1000)
[Symbol]	Multi-Residential District (M-1) (R-1100)
[Symbol]	Multi-Residential District (M-1) (R-1200)
[Symbol]	Multi-Residential District (M-1) (R-1300)
[Symbol]	Multi-Residential District (M-1) (R-1400)
[Symbol]	Multi-Residential District (M-1) (R-1500)
[Symbol]	Multi-Residential District (M-1) (R-1600)
[Symbol]	Multi-Residential District (M-1) (R-1700)
[Symbol]	Multi-Residential District (M-1) (R-1800)
[Symbol]	Multi-Residential District (M-1) (R-1900)
[Symbol]	Multi-Residential District (M-1) (R-2000)
[Symbol]	Multi-Residential District (M-1) (R-2100)
[Symbol]	Multi-Residential District (M-1) (R-2200)
[Symbol]	Multi-Residential District (M-1) (R-2300)
[Symbol]	Multi-Residential District (M-1) (R-2400)
[Symbol]	Multi-Residential District (M-1) (R-2500)
[Symbol]	Multi-Residential District (M-1) (R-2600)
[Symbol]	Multi-Residential District (M-1) (R-2700)
[Symbol]	Multi-Residential District (M-1) (R-2800)
[Symbol]	Multi-Residential District (M-1) (R-2900)
[Symbol]	Multi-Residential District (M-1) (R-3000)
[Symbol]	Multi-Residential District (M-1) (R-3100)
[Symbol]	Multi-Residential District (M-1) (R-3200)
[Symbol]	Multi-Residential District (M-1) (R-3300)
[Symbol]	Multi-Residential District (M-1) (R-3400)
[Symbol]	Multi-Residential District (M-1) (R-3500)
[Symbol]	Multi-Residential District (M-1) (R-3600)
[Symbol]	Multi-Residential District (M-1) (R-3700)
[Symbol]	Multi-Residential District (M-1) (R-3800)
[Symbol]	Multi-Residential District (M-1) (R-3900)
[Symbol]	Multi-Residential District (M-1) (R-4000)
[Symbol]	Multi-Residential District (M-1) (R-4100)
[Symbol]	Multi-Residential District (M-1) (R-4200)
[Symbol]	Multi-Residential District (M-1) (R-4300)
[Symbol]	Multi-Residential District (M-1) (R-4400)
[Symbol]	Multi-Residential District (M-1) (R-4500)
[Symbol]	Multi-Residential District (M-1) (R-4600)
[Symbol]	Multi-Residential District (M-1) (R-4700)
[Symbol]	Multi-Residential District (M-1) (R-4800)
[Symbol]	Multi-Residential District (M-1) (R-4900)
[Symbol]	Multi-Residential District (M-1) (R-5000)
[Symbol]	Multi-Residential District (M-1) (R-5100)
[Symbol]	Multi-Residential District (M-1) (R-5200)
[Symbol]	Multi-Residential District (M-1) (R-5300)
[Symbol]	Multi-Residential District (M-1) (R-5400)
[Symbol]	Multi-Residential District (M-1) (R-5500)
[Symbol]	Multi-Residential District (M-1) (R-5600)
[Symbol]	Multi-Residential District (M-1) (R-5700)
[Symbol]	Multi-Residential District (M-1) (R-5800)
[Symbol]	Multi-Residential District (M-1) (R-5900)
[Symbol]	Multi-Residential District (M-1) (R-6000)
[Symbol]	Multi-Residential District (M-1) (R-6100)
[Symbol]	Multi-Residential District (M-1) (R-6200)
[Symbol]	Multi-Residential District (M-1) (R-6300)
[Symbol]	Multi-Residential District (M-1) (R-6400)
[Symbol]	Multi-Residential District (M-1) (R-6500)
[Symbol]	Multi-Residential District (M-1) (R-6600)
[Symbol]	Multi-Residential District (M-1) (R-6700)
[Symbol]	Multi-Residential District (M-1) (R-6800)
[Symbol]	Multi-Residential District (M-1) (R-6900)
[Symbol]	Multi-Residential District (M-1) (R-7000)
[Symbol]	Multi-Residential District (M-1) (R-7100)
[Symbol]	Multi-Residential District (M-1) (R-7200)
[Symbol]	Multi-Residential District (M-1) (R-7300)
[Symbol]	Multi-Residential District (M-1) (R-7400)
[Symbol]	Multi-Residential District (M-1) (R-7500)
[Symbol]	Multi-Residential District (M-1) (R-7600)
[Symbol]	Multi-Residential District (M-1) (R-7700)
[Symbol]	Multi-Residential District (M-1) (R-7800)
[Symbol]	Multi-Residential District (M-1) (R-7900)
[Symbol]	Multi-Residential District (M-1) (R-8000)
[Symbol]	Multi-Residential District (M-1) (R-8100)
[Symbol]	Multi-Residential District (M-1) (R-8200)
[Symbol]	Multi-Residential District (M-1) (R-8300)
[Symbol]	Multi-Residential District (M-1) (R-8400)
[Symbol]	Multi-Residential District (M-1) (R-8500)
[Symbol]	Multi-Residential District (M-1) (R-8600)
[Symbol]	Multi-Residential District (M-1) (R-8700)
[Symbol]	Multi-Residential District (M-1) (R-8800)
[Symbol]	Multi-Residential District (M-1) (R-8900)
[Symbol]	Multi-Residential District (M-1) (R-9000)
[Symbol]	Multi-Residential District (M-1) (R-9100)
[Symbol]	Multi-Residential District (M-1) (R-9200)
[Symbol]	Multi-Residential District (M-1) (R-9300)
[Symbol]	Multi-Residential District (M-1) (R-9400)
[Symbol]	Multi-Residential District (M-1) (R-9500)
[Symbol]	Multi-Residential District (M-1) (R-9600)
[Symbol]	Multi-Residential District (M-1) (R-9700)
[Symbol]	Multi-Residential District (M-1) (R-9800)
[Symbol]	Multi-Residential District (M-1) (R-9900)
[Symbol]	Multi-Residential District (M-1) (R-10000)

LAND USE STATISTICS

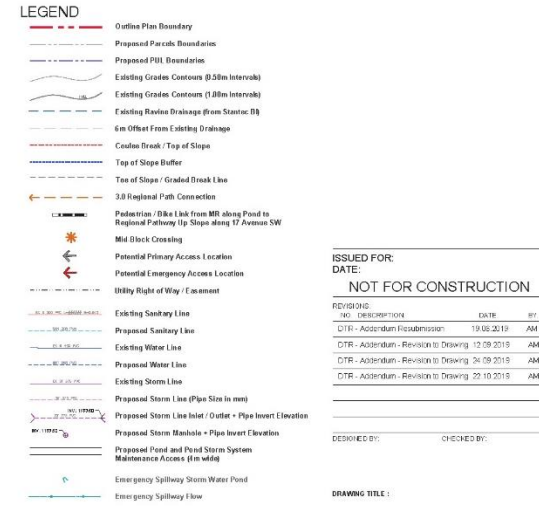
Land Use	Acres	Hectares
DC (M2)	9.75	3.94
DC (M2) to MU-1 (R-100)	1.22	0.48
DC (M2) to MU-1 (R-200)	2.00	0.81
DC (M2) to MU-1 (R-300)	3.24	1.31
DC (M2) to MU-1 (R-400)	2.11	0.85
DC (M2) to MU-1 (R-500)	1.53	0.62
DC (M2) to MU-1 (R-600)	2.81	1.13

OUTLINE PLAN STATISTICS

Land Use	Acres	Hectares	% of GDA
Total Area	28.32	11.55	100.00%
Land Use Area	1.62	0.66	5.72%
Gross Developable Area (GDA)	26.70	10.89	94.28%
MU-1 (R-100)	8.00	3.24	28.21%
MU-1 (R-200)	4.00	1.62	14.10%
MU-1 (R-300)	2.00	0.81	7.05%
MU-1 (R-400)	1.11	0.45	3.95%
MU-1 (R-500)	2.11	0.85	7.05%
MU-1 (R-600)	1.53	0.62	5.39%
MU-1 (R-700)	2.81	1.13	9.78%
MU-1 (R-800)	2.11	0.85	7.05%
MU-1 (R-900)	1.53	0.62	5.39%
MU-1 (R-1000)	2.81	1.13	9.78%
MU-1 (R-1100)	2.11	0.85	7.05%
MU-1 (R-1200)	1.53	0.62	5.39%
MU-1 (R-1300)	2.81	1.13	9.78%
MU-1 (R-1400)	2.11	0.85	7.05%
MU-1 (R-1500)	1.53	0.62	5.39%
MU-1 (R-1600)	2.81	1.13	9.78%
MU-1 (R-1700)	2.11	0.85	7.05%
MU-1 (R-1800)	1.53	0.62	5.39%
MU-1 (R-1900)	2.81	1.13	9.78%
MU-1 (R-2000)	2.11	0.85	7.05%
MU-1 (R-2100)	1.53	0.62	5.39%
MU-1 (R-2200)	2.81	1.13	9.78%
MU-1 (R-2300)	2.11	0.85	7.05%
MU-1 (R-2400)	1.53	0.62	5.39%
MU-1 (R-2500)	2.81	1.13	9.78%
MU-1 (R-2600)	2.11	0.85	7.05%
MU-1 (R-2700)	1.53	0.62	5.39%
MU-1 (R-2800)	2.81	1.13	9.78%
MU-1 (R-2900)	2.11	0.85	7.05%
MU-1 (R-3000)	1.53	0.62	5.39%
MU-1 (R-3100)	2.81	1.13	9.78%
MU-1 (R-3200)	2.11	0.85	7.05%
MU-1 (R-3300)	1.53	0.62	5.39%
MU-1 (R-3400)	2.81	1.13	9.78%
MU-1 (R-3500)	2.11	0.85	7.05%
MU-1 (R-3600)	1.53	0.62	5.39%
MU-1 (R-3700)	2.81	1.13	9.78%
MU-1 (R-3800)	2.11	0.85	7.05%
MU-1 (R-3900)	1.53	0.62	5.39%
MU-1 (R-4000)	2.81	1.13	9.78%
MU-1 (R-4100)	2.11	0.85	7.05%
MU-1 (R-4200)	1.53	0.62	5.39%
MU-1 (R-4300)	2.81	1.13	9.78%
MU-1 (R-4400)	2.11	0.85	7.05%
MU-1 (R-4500)	1.53	0.62	5.39%
MU-1 (R-4600)	2.81	1.13	9.78%
MU-1 (R-4700)	2.11	0.85	7.05%
MU-1 (R-4800)	1.53	0.62	5.39%
MU-1 (R-4900)	2.81	1.13	9.78%
MU-1 (R-5000)	2.11	0.85	7.05%
MU-1 (R-5100)	1.53	0.62	5.39%
MU-1 (R-5200)	2.81	1.13	9.78%
MU-1 (R-5300)	2.11	0.85	7.05%
MU-1 (R-5400)	1.53	0.62	5.39%
MU-1 (R-5500)	2.81	1.13	9.78%
MU-1 (R-5600)	2.11	0.85	7.05%
MU-1 (R-5700)	1.53	0.62	5.39%
MU-1 (R-5800)	2.81	1.13	9.78%
MU-1 (R-5900)	2.11	0.85	7.05%
MU-1 (R-6000)	1.53	0.62	5.39%
MU-1 (R-6100)	2.81	1.13	9.78%
MU-1 (R-6200)	2.11	0.85	7.05%
MU-1 (R-6300)	1.53	0.62	5.39%
MU-1 (R-6400)	2.81	1.13	9.78%
MU-1 (R-6500)	2.11	0.85	7.05%
MU-1 (R-6600)	1.53	0.62	5.39%
MU-1 (R-6700)	2.81	1.13	9.78%
MU-1 (R-6800)	2.11	0.85	7.05%
MU-1 (R-6900)	1.53	0.62	5.39%
MU-1 (R-7000)	2.81	1.13	9.78%
MU-1 (R-7100)	2.11	0.85	7.05%
MU-1 (R-7200)	1.53	0.62	5.39%
MU-1 (R-7300)	2.81	1.13	9.78%
MU-1 (R-7400)	2.11	0.85	7.05%
MU-1 (R-7500)	1.53	0.62	5.39%
MU-1 (R-7600)	2.81	1.13	9.78%
MU-1 (R-7700)	2.11	0.85	7.05%
MU-1 (R-7800)	1.53	0.62	5.39%
MU-1 (R-7900)	2.81	1.13	9.78%
MU-1 (R-8000)	2.11	0.85	7.05%
MU-1 (R-8100)	1.53	0.62	5.39%
MU-1 (R-8200)	2.81	1.13	9.78%
MU-1 (R-8300)	2.11	0.85	7.05%
MU-1 (R-8400)	1.53	0.62	5.39%
MU-1 (R-8500)	2.81	1.13	9.78%
MU-1 (R-8600)	2.11	0.85	7.05%
MU-1 (R-8700)	1.53	0.62	5.39%
MU-1 (R-8800)	2.81	1.13	9.78%
MU-1 (R-8900)	2.11	0.85	7.05%
MU-1 (R-9000)	1.53	0.62	5.39%
MU-1 (R-9100)	2.81	1.13	9.78%
MU-1 (R-9200)	2.11	0.85	7.05%
MU-1 (R-9300)	1.53	0.62	5.39%
MU-1 (R-9400)	2.81	1.13	9.78%
MU-1 (R-9500)	2.11	0.85	7.05%
MU-1 (R-9600)	1.53	0.62	5.39%
MU-1 (R-9700)	2.81	1.13	9.78%
MU-1 (R-9800)	2.11	0.85	7.05%
MU-1 (R-9900)	1.53	0.62	5.39%
MU-1 (R-10000)	2.81	1.13	9.78%

INTENSITY

Land Use	Gross Hectares	Anticipated Units	SPR	People/Unit	PRN	Hotel Units	Home Based Units
MU-1 (R-100)	0.01	600	215	1303	345	73	62
MU-1 (R-200)	0.01	48	97	61	155	22	3
DC (M2)	1.17	226	183	262	300		
M-1	1.26	111	95	170	138		
Total Project	6.32	1285	178	1976	285	95	65



ISSUED FOR:
DATE:

NOT FOR CONSTRUCTION

REVISIONS			
NO.	DESCRIPTION	DATE	BY
DTR - Addendum Resubmission		19.06.2019	AM
DTR - Addendum - Revision to Drawing		12.09.2019	AM
DTR - Addendum - Revision to Drawing		24.09.2019	AM
DTR - Addendum - Revision to Drawing		22.10.2019	AM

Proposed Outline Plan

OUTLINE PLAN



LAND USE LEGEND

- Multi-Residential District (MU-1 (f5 h50))
- Multi-Residential District (MU-1 (f3h20))
- Multi-Residential District (DC (M-2))
- Multi-Residential District (M-1)
- Special Purpose District (S-CRI)
- Environmental Reserve (ER)
- Municipal Reserve (MR)

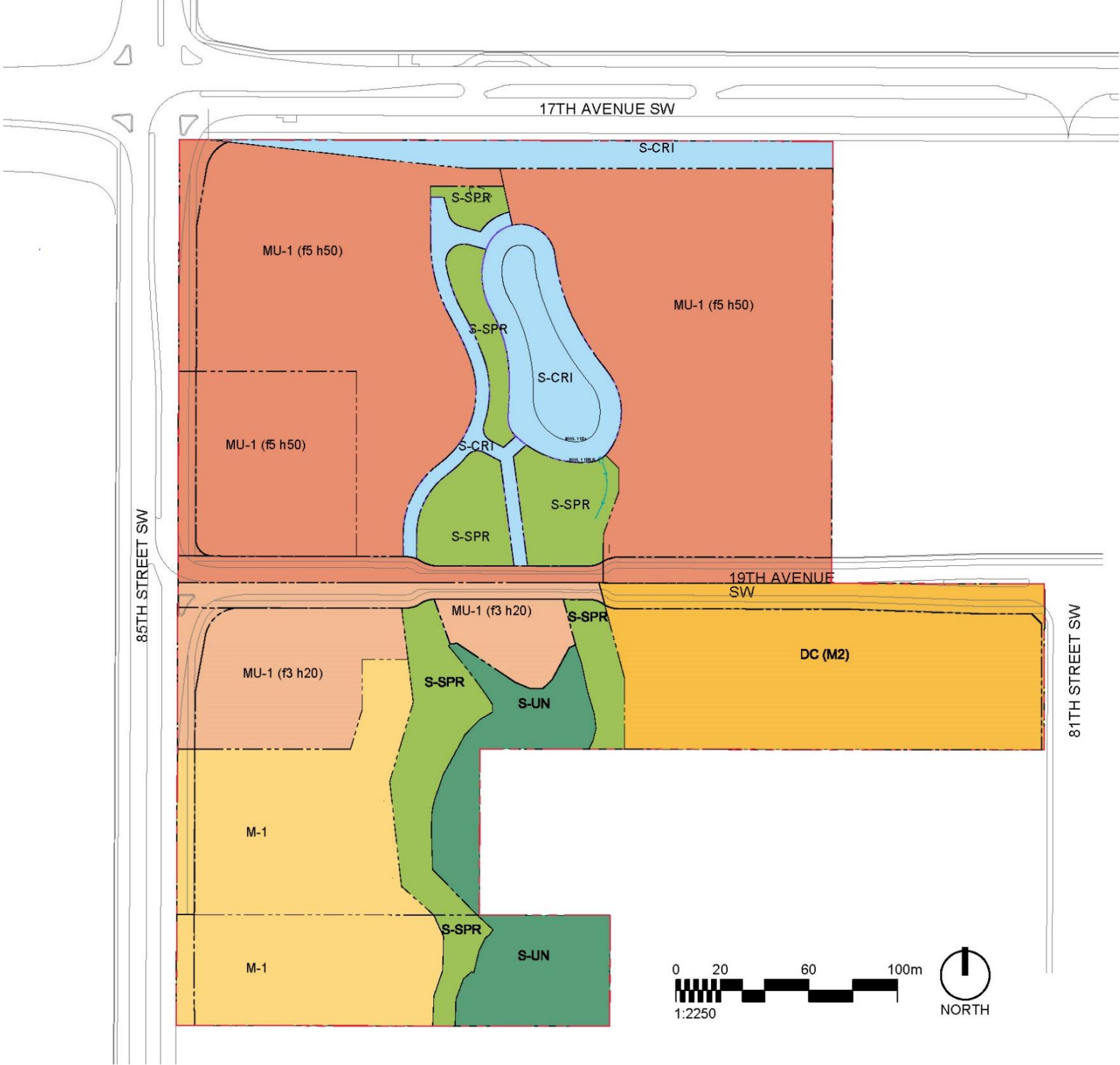
LEGEND

- Outline Plan Boundary
- Proposed Parcels Boundaries
- Proposed PUL Boundaries
- Existing Grades Contours (0.50m Intervals)
- Existing Grades Contours (1.00m Intervals)
- Existing Ravine Drainage (from Stantec BI)
- 6m Offset From Existing Drainage
- Coulee Break / Top of Slope
- Top of Slope Buffer
- Toe of Slope / Graded Break Line
- 3.0 Regional Path Connection
- Pedestrian / Bike Link from MR along Pond to Regional Pathway Up Slope along 17 Avenue SW
- Mid-Block Crossing
- Potential Primary Access Location
- Potential Emergency Access Location
- Utility Right of Way / Easement
- Existing Sanitary Line
- Proposed Sanitary Line
- Existing Water Line
- Proposed Water Line
- Existing Storm Line
- Proposed Storm Line (Pipe Size in mm)
- Proposed Storm Line Inlet / Outlet + Pipe Invert Elevation
- Proposed Storm Manhole + Pipe Invert Elevation
- Proposed Pond and Pond Storm System Maintenance Access (4m wide)
- Emergency Spillway Storm Water Pond
- Emergency Spillway Flow

SLOKKER SPRINGBANK HILL DEVELOPMENT

Proposed Outline Plan

LAND USE PLAN



LAND USE LEGEND

- Multi-Residential District (MU-1 (f5 h50))
- Multi-Residential District (MU-1 (f3h20))
- Multi-Residential District (DC (M-2))
- Multi-Residential District (M-1)
- Special Purpose District (S-CRI)
- Environmental Reserve (ER)
- Municipal Reserve (MR)

SLOKKER SPRINGBANK HILL DEVELOPMENT

Proposed Outline Plan

LAND USE STATISTICS

		Acres	Hectares
<div></div>	DC 12Z96 To MU-1 (f5 h50)	9.75	3.94
<div></div>	DC 12Z96 To MU-1 (f3h20)	1.22	0.49
<div></div>	DC 12Z96 To DC (M-2)	2.90	1.17
<div></div>	DC 12Z96 To M-1	3.24	1.31
<div></div>	DC 12Z96 To (S-CRI)	2.11	0.85
<div></div>	DC 12Z96 To S-UN (ER)	1.53	0.62
<div></div>	DC 12Z96 To S-SPR (MR)	2.51	1.01

OUTLINE PLAN STATISTICS

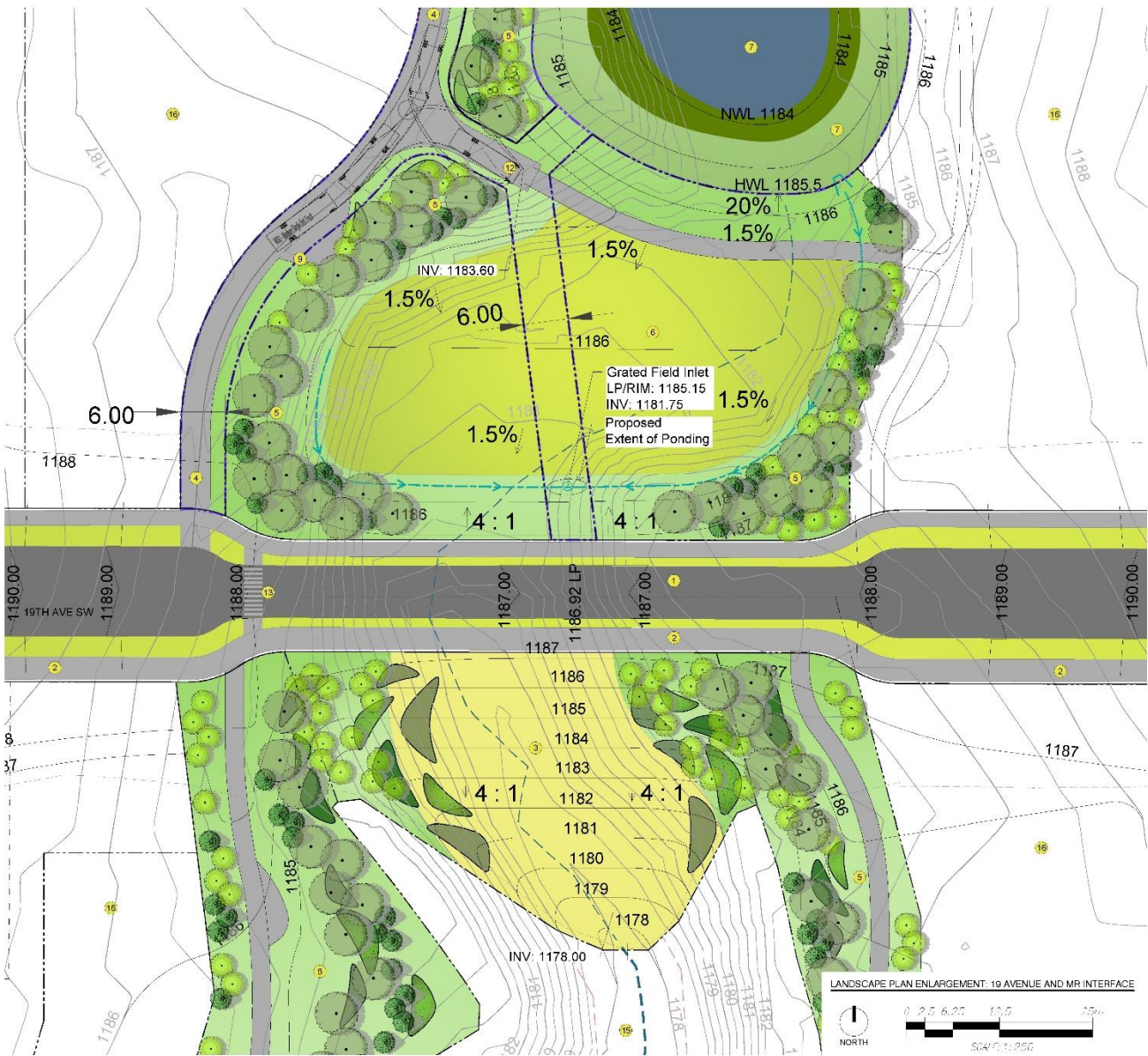
	Units	Acres	Hectares	% of GDA
<div></div>	Total Area	26.32	10.65	
<div></div>	Less S-UN / ER	1.53	0.62	
<div></div>	Gross Developable Area (GDA)	24.79	10.03	100.00%
<div></div>	MU-1 (f5 h50)	850	9.74	39.31%
	<i>Mixed Use - General District</i>			
<div></div>	MU-1 (f3h20)	48	1.22	4.93%
	<i>Mixed Use - General District</i>			
<div></div>	DC (M-2)	226	2.90	11.70%
	<i>Multi-Residential - Medium Profile District</i>			
<div></div>	M-1	111	3.24	13.05%
	<i>Multi-Residential - Low Profile District</i>			
<div></div>	S-CRI	2.11	0.85	8.49%
	<i>Special Purpose - City and Regional Infrastructure District</i>			
<div></div>	Municipal Reserve			
	Required 10% of GDA	2.48	1.00	
	(S-SPR) Provided	2.51	1.01	10.10%
<div></div>	Public Dedication			
	Roadways	1.83	0.74	7.39%
	Roadway Widening	0.81	0.33	3.28%
	Back Slope Ravine to Roads	0.43	0.17	1.73%

INTENSITY

Land Use	Gross Hectares	Anticipated Units	UPH	People(1.6/Unit)	PPH	Retail Jobs	Home-Based Jobs
MU-1 (f5 h50)	3.94	850	215	1360	345	73	52
MU-1 (f3h20)	0.49	48	97	61	155	22	3
DC (M-2)	1.17	226	193	362	308		
M-1	1.31	111	85	178	136		
Total Project	6.92	1235	178	1976	285	95	55

SLOKKER SPRINGBANK HILL DEVELOPMENT

Proposed Outline Plan



PROJECT
**SPRINGBANK HILL
DEVELOPMENT**
FOR
Slokker Homes

MUNICIPAL ADDRESS
1188 17TH AVE SW, CALGARY, ALBERTA, CANADA
T2C 1P5

LEGAL DESCRIPTION
1188 17TH AVE SW, CALGARY, ALBERTA, CANADA
T2C 1P5

PERMIT
1188 17TH AVE SW, CALGARY, ALBERTA, CANADA
T2C 1P5

CONSULTANTS
O2 Design
510 7th Ave SW, Calgary, Alberta, Canada
T2C 1P5
403.225.1336
www.o2designs.ca

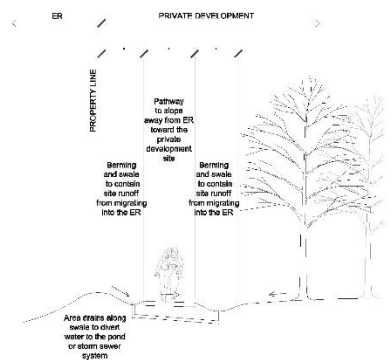
DEALS
1188 17TH AVE SW, CALGARY, ALBERTA, CANADA
T2C 1P5



PLANT SCHEDULE	
TYPE	CODE
Tree (Small)	10
Tree (Medium)	11
Tree (Large)	12
Shrub (Small)	13
Shrub (Medium)	14
Shrub (Large)	15
Grass (Small)	16
Grass (Medium)	17
Grass (Large)	18
Grass (Small)	19
Grass (Medium)	20
Grass (Large)	21
Grass (Small)	22
Grass (Medium)	23
Grass (Large)	24
Grass (Small)	25
Grass (Medium)	26
Grass (Large)	27
Grass (Small)	28
Grass (Medium)	29
Grass (Large)	30

- SITE AMENITIES**
1. PROPOSED 17TH AVE SW REGIONAL PATHWAY
 2. PROPOSED 19TH AVE SW
 3. PROPOSED 10TH AVE SW
 4. PROPOSED 11TH AVE SW
 5. PROPOSED 12TH AVE SW
 6. PROPOSED 13TH AVE SW
 7. PROPOSED 14TH AVE SW
 8. PROPOSED 15TH AVE SW
 9. PROPOSED 16TH AVE SW
 10. PROPOSED 17TH AVE SW
 11. PROPOSED 18TH AVE SW
 12. PROPOSED 19TH AVE SW
 13. PROPOSED 20TH AVE SW
 14. PROPOSED 21TH AVE SW
 15. PROPOSED 22TH AVE SW
 16. PROPOSED 23TH AVE SW
 17. PROPOSED 24TH AVE SW
 18. PROPOSED 25TH AVE SW
 19. PROPOSED 26TH AVE SW
 20. PROPOSED 27TH AVE SW
 21. PROPOSED 28TH AVE SW
 22. PROPOSED 29TH AVE SW
 23. PROPOSED 30TH AVE SW
 24. PROPOSED 31TH AVE SW
 25. PROPOSED 32TH AVE SW
 26. PROPOSED 33TH AVE SW
 27. PROPOSED 34TH AVE SW
 28. PROPOSED 35TH AVE SW
 29. PROPOSED 36TH AVE SW
 30. PROPOSED 37TH AVE SW
 31. PROPOSED 38TH AVE SW
 32. PROPOSED 39TH AVE SW
 33. PROPOSED 40TH AVE SW
 34. PROPOSED 41TH AVE SW
 35. PROPOSED 42TH AVE SW
 36. PROPOSED 43TH AVE SW
 37. PROPOSED 44TH AVE SW
 38. PROPOSED 45TH AVE SW
 39. PROPOSED 46TH AVE SW
 40. PROPOSED 47TH AVE SW
 41. PROPOSED 48TH AVE SW
 42. PROPOSED 49TH AVE SW
 43. PROPOSED 50TH AVE SW
 44. PROPOSED 51TH AVE SW
 45. PROPOSED 52TH AVE SW
 46. PROPOSED 53TH AVE SW
 47. PROPOSED 54TH AVE SW
 48. PROPOSED 55TH AVE SW
 49. PROPOSED 56TH AVE SW
 50. PROPOSED 57TH AVE SW
 51. PROPOSED 58TH AVE SW
 52. PROPOSED 59TH AVE SW
 53. PROPOSED 60TH AVE SW
 54. PROPOSED 61TH AVE SW
 55. PROPOSED 62TH AVE SW
 56. PROPOSED 63TH AVE SW
 57. PROPOSED 64TH AVE SW
 58. PROPOSED 65TH AVE SW
 59. PROPOSED 66TH AVE SW
 60. PROPOSED 67TH AVE SW
 61. PROPOSED 68TH AVE SW
 62. PROPOSED 69TH AVE SW
 63. PROPOSED 70TH AVE SW
 64. PROPOSED 71TH AVE SW
 65. PROPOSED 72TH AVE SW
 66. PROPOSED 73TH AVE SW
 67. PROPOSED 74TH AVE SW
 68. PROPOSED 75TH AVE SW
 69. PROPOSED 76TH AVE SW
 70. PROPOSED 77TH AVE SW
 71. PROPOSED 78TH AVE SW
 72. PROPOSED 79TH AVE SW
 73. PROPOSED 80TH AVE SW
 74. PROPOSED 81TH AVE SW
 75. PROPOSED 82TH AVE SW
 76. PROPOSED 83TH AVE SW
 77. PROPOSED 84TH AVE SW
 78. PROPOSED 85TH AVE SW
 79. PROPOSED 86TH AVE SW
 80. PROPOSED 87TH AVE SW
 81. PROPOSED 88TH AVE SW
 82. PROPOSED 89TH AVE SW
 83. PROPOSED 90TH AVE SW
 84. PROPOSED 91TH AVE SW
 85. PROPOSED 92TH AVE SW
 86. PROPOSED 93TH AVE SW
 87. PROPOSED 94TH AVE SW
 88. PROPOSED 95TH AVE SW
 89. PROPOSED 96TH AVE SW
 90. PROPOSED 97TH AVE SW
 91. PROPOSED 98TH AVE SW
 92. PROPOSED 99TH AVE SW
 93. PROPOSED 100TH AVE SW

- GRAPHIC LEGEND**
- Outline Parcel Boundary
 - Proposed PUL Boundaries
 - Existing Grades Contours (0.5m Intervals)
 - Existing Grades Contours (1.0m Intervals)
 - Proposed Grading Contours and Elevation (1.0m Intervals)
 - Existing Runoff Drainage (from Station 8)
 - On Off Road From Existing Drainage
 - Grading Break / Top of Slope
 - Top of Slope Buffer
 - Top of Slope / Graded Break Line
 - 3.0 Regional Path Connection
 - Proposed Storm Line (Pipe Size 18 in)
 - Proposed Storm Line (Pipe Size 24 in)
 - Proposed Storm Line (Pipe Size 30 in)
 - Proposed Storm Line (Pipe Size 36 in)
 - Proposed Storm Line (Pipe Size 42 in)
 - Proposed Storm Line (Pipe Size 48 in)
 - Proposed Storm Line (Pipe Size 54 in)
 - Proposed Storm Line (Pipe Size 60 in)
 - Proposed Storm Line (Pipe Size 66 in)
 - Proposed Storm Line (Pipe Size 72 in)
 - Proposed Storm Line (Pipe Size 78 in)
 - Proposed Storm Line (Pipe Size 84 in)
 - Proposed Storm Line (Pipe Size 90 in)
 - Proposed Storm Line (Pipe Size 96 in)
 - Proposed Storm Line (Pipe Size 102 in)
 - Proposed Storm Line (Pipe Size 108 in)
 - Proposed Storm Line (Pipe Size 114 in)
 - Proposed Storm Line (Pipe Size 120 in)
 - Proposed Storm Line (Pipe Size 126 in)
 - Proposed Storm Line (Pipe Size 132 in)
 - Proposed Storm Line (Pipe Size 138 in)
 - Proposed Storm Line (Pipe Size 144 in)
 - Proposed Storm Line (Pipe Size 150 in)
 - Proposed Storm Line (Pipe Size 156 in)
 - Proposed Storm Line (Pipe Size 162 in)
 - Proposed Storm Line (Pipe Size 168 in)
 - Proposed Storm Line (Pipe Size 174 in)
 - Proposed Storm Line (Pipe Size 180 in)
 - Proposed Storm Line (Pipe Size 186 in)
 - Proposed Storm Line (Pipe Size 192 in)
 - Proposed Storm Line (Pipe Size 198 in)
 - Proposed Storm Line (Pipe Size 204 in)
 - Proposed Storm Line (Pipe Size 210 in)
 - Proposed Storm Line (Pipe Size 216 in)
 - Proposed Storm Line (Pipe Size 222 in)
 - Proposed Storm Line (Pipe Size 228 in)
 - Proposed Storm Line (Pipe Size 234 in)
 - Proposed Storm Line (Pipe Size 240 in)
 - Proposed Storm Line (Pipe Size 246 in)
 - Proposed Storm Line (Pipe Size 252 in)
 - Proposed Storm Line (Pipe Size 258 in)
 - Proposed Storm Line (Pipe Size 264 in)
 - Proposed Storm Line (Pipe Size 270 in)
 - Proposed Storm Line (Pipe Size 276 in)
 - Proposed Storm Line (Pipe Size 282 in)
 - Proposed Storm Line (Pipe Size 288 in)
 - Proposed Storm Line (Pipe Size 294 in)
 - Proposed Storm Line (Pipe Size 300 in)

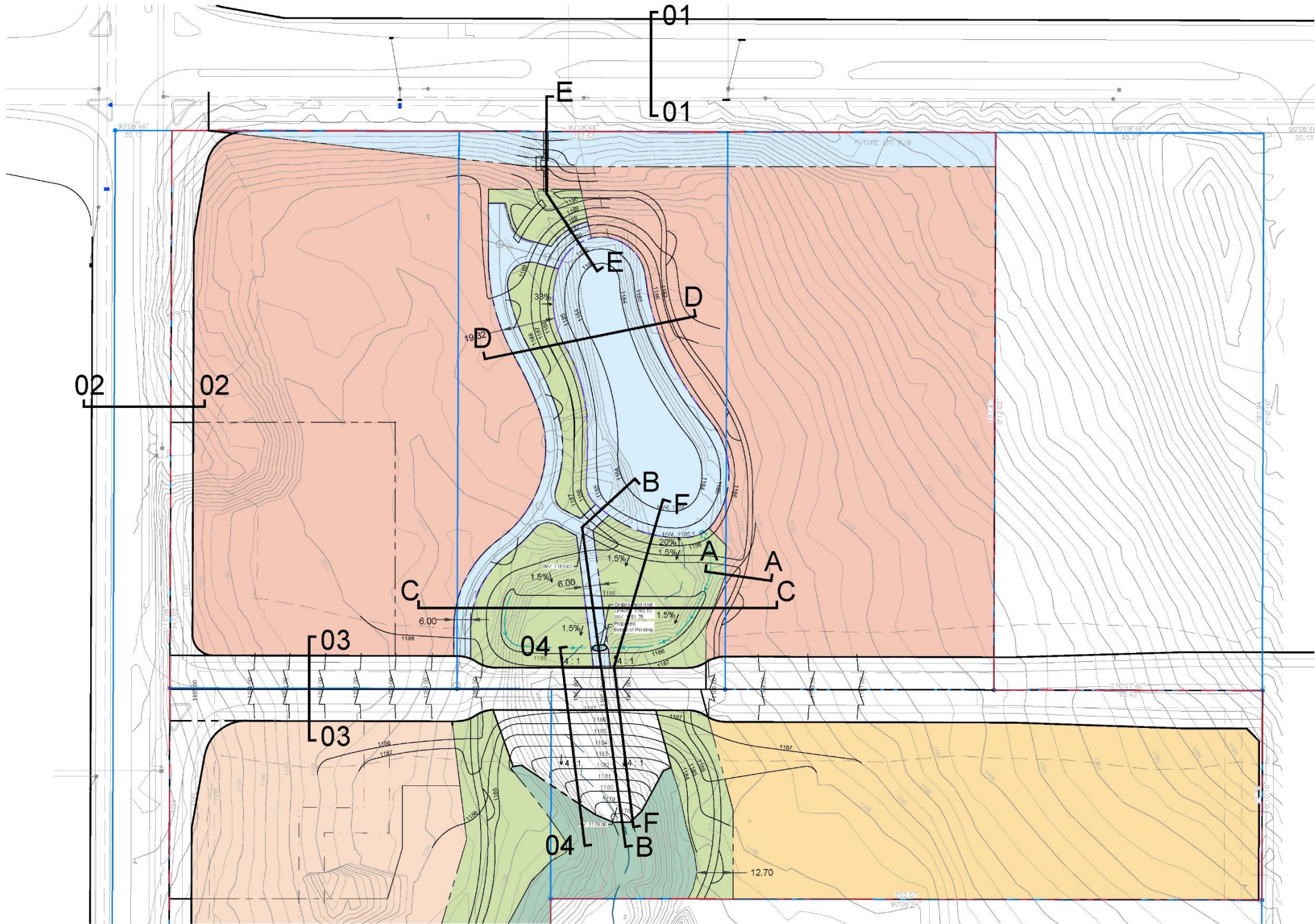


ISSUED FOR:
DATE:
NOT FOR CONSTRUCTION

REVISIONS
NO. DESCRIPTION DATE BY
01 Addendum - Revision to Drawing 12/03/2019 AM
02 Addendum - Revision to Drawing 24/05/2019 AM

DRAWING TITLE:
**CONCEPTUAL
DESIGN MR
LANDSCAPE PLAN**

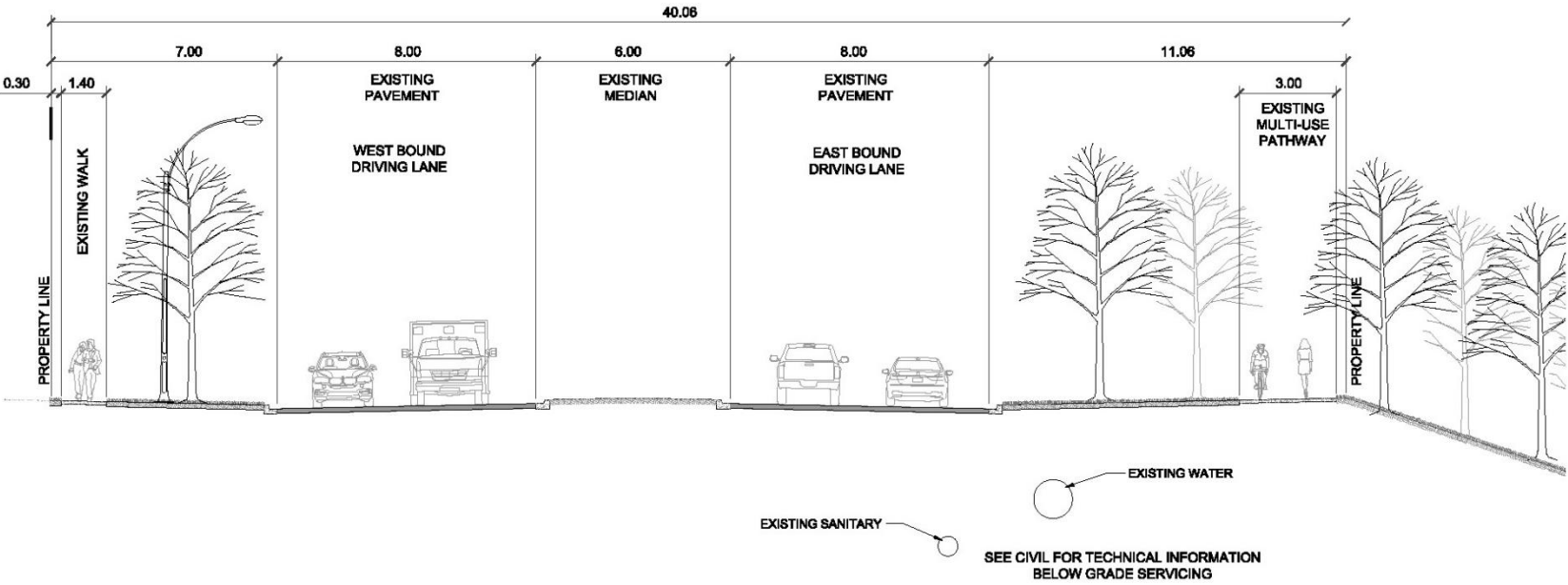
Proposed Outline Plan



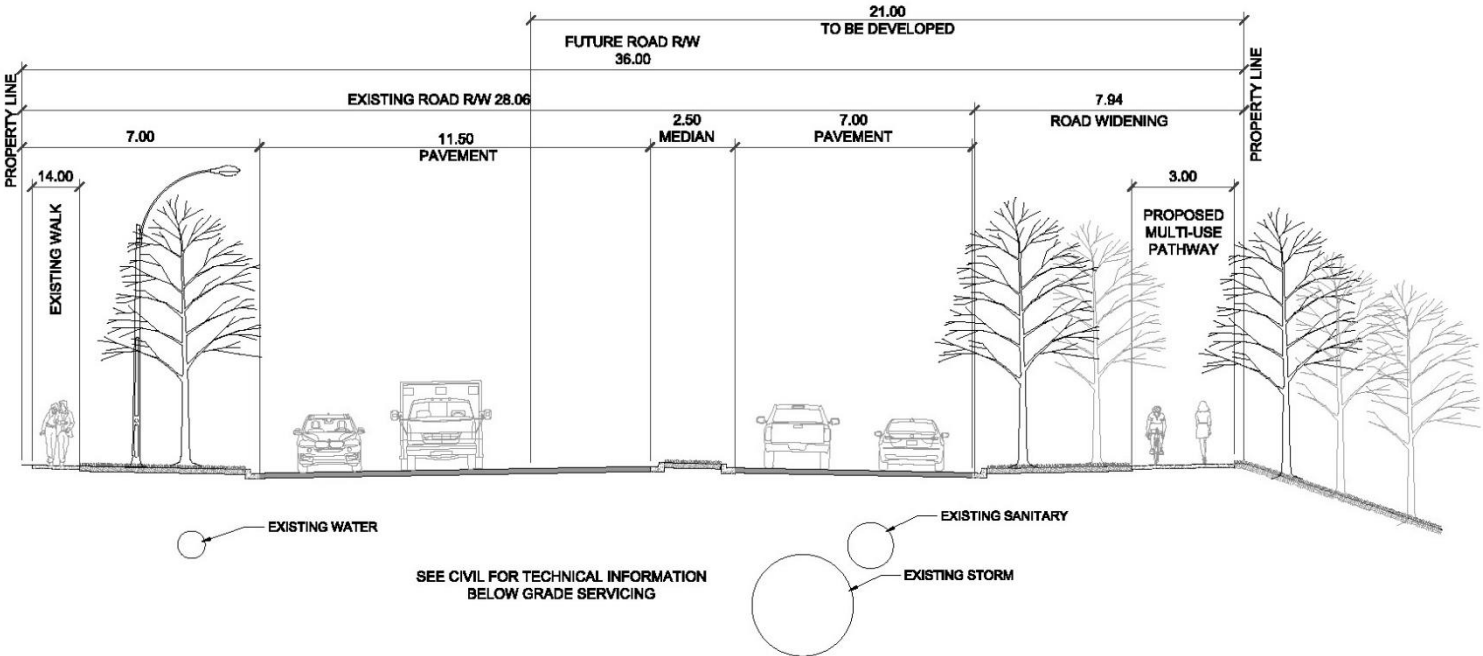
PLAN LEGEND: SECTIONS LOCATIONS

SLOKKER SPRINGBANK HILL DEVELOPMENT

Proposed Outline Plan

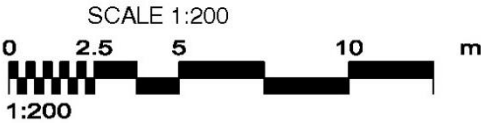


Secion 1-1: ARTERIAL - 17 AVENUE SW SCALE 1:200



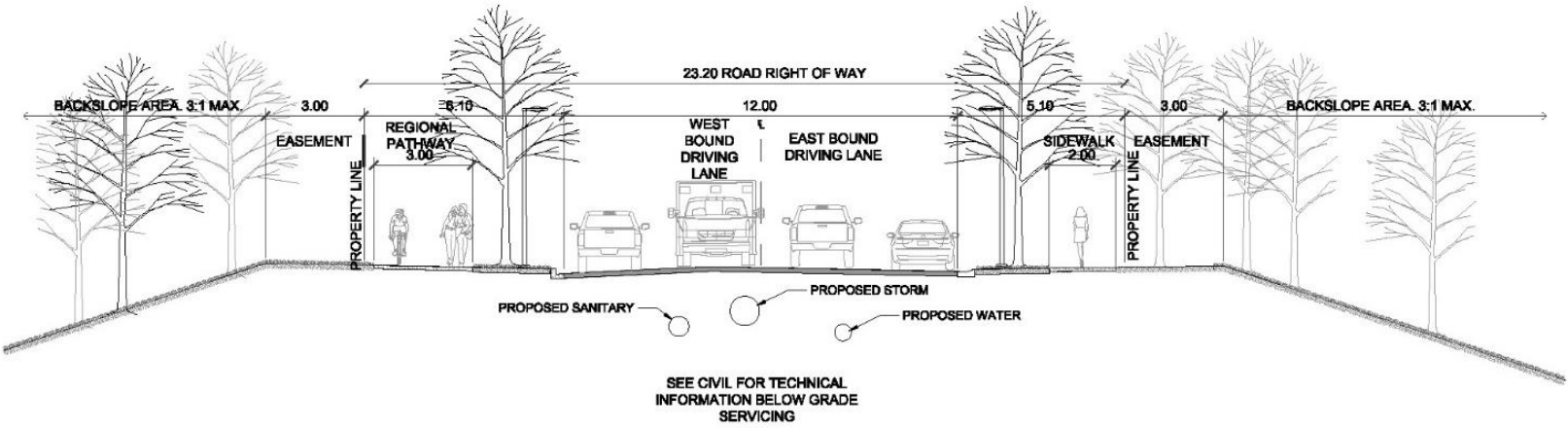
Section 2-2: ARTERIAL - 85 STREET SW

SLOKKER SPRINGBANK HILL DEVELOPMENT



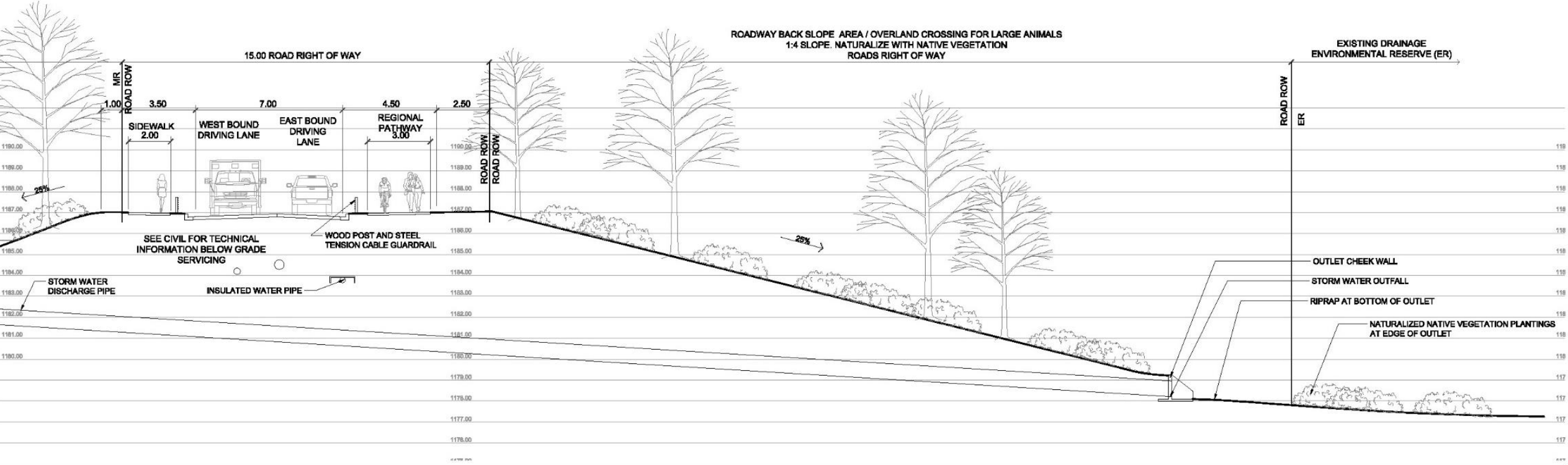
22 October 2019

Proposed Outline Plan



Section 3-3: MODIFIED COLLECTOR - 19 AVENUE SW

SCALE 1:200



Section 4-4: MODIFIED COLLECTOR 19 AVENUE SW AT ROADWAY NARROWING

SCALE 1:200

SLOKKER SPRINGBANK HILL DEVELOPMENT

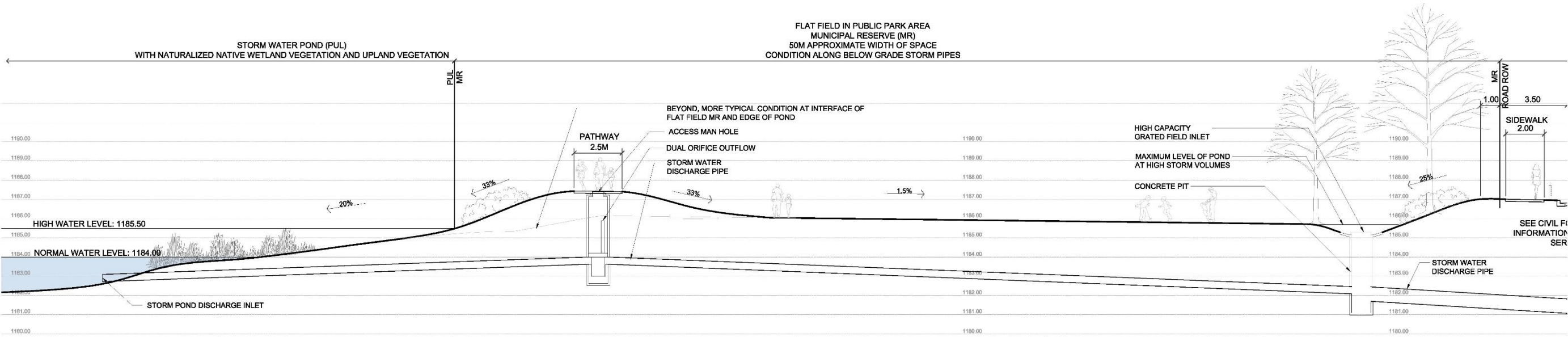


Slokker Homes

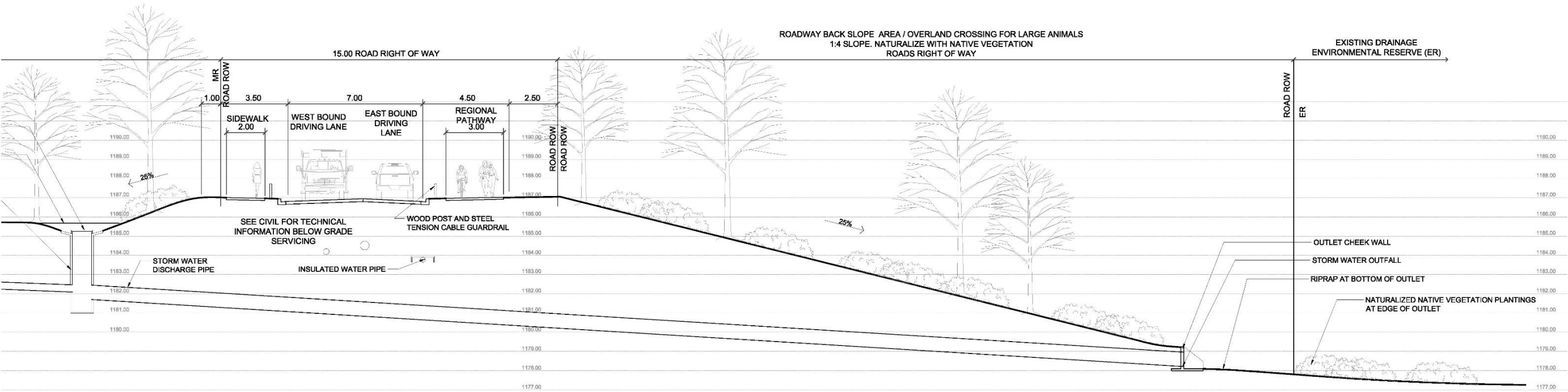
22 October 2019

O2
810 208 1715 AVE SW CALGARY ALBERTA CANADA
403 226 1238 / 403 226 1239 WWW.O2DESIGN.COM

Proposed Outline Plan



SECTION B-B 1: Cross Section through proposed flat Public Park (MR) area and 19 Avenue SW



SECTION B-B 2: Cross Section through proposed flat Public Park (MR) area and 19 Avenue SW

LANDSCAPE SECTIONS PUBLIC PARK (MR) - SHEET 2
SLOKKER SPRINGBANK HILL DEVELOPMENT

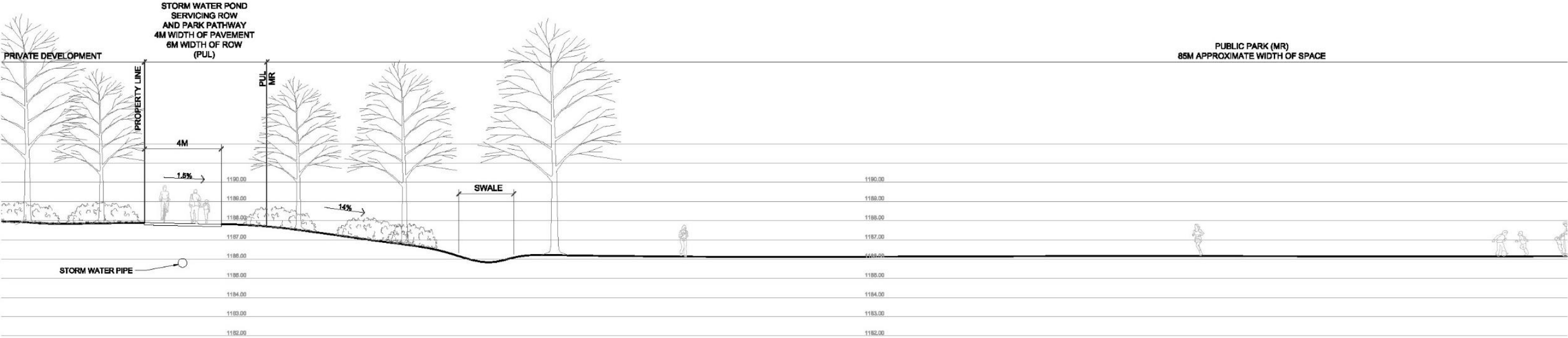


Slokker Homes

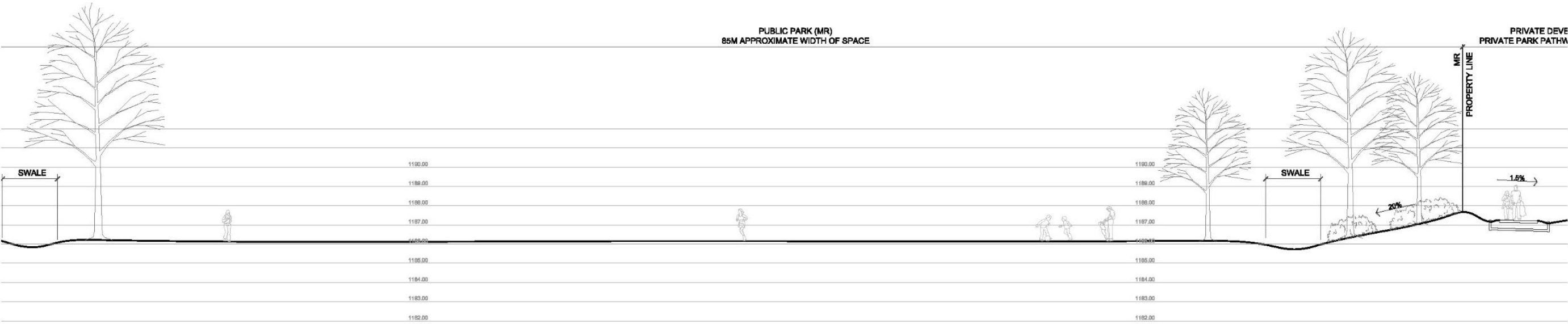
24 September 2019

O2
510 255 17th AVE SW CALGARY ALBERTA CANADA
T 403.228.1339 F 403.228.1320 www.o2design.com

Proposed Outline Plan



SECTION C-C 1: Longitudinal Section through proposed flat Public Park (MR) area between 19 Avenue and the Storm Pond

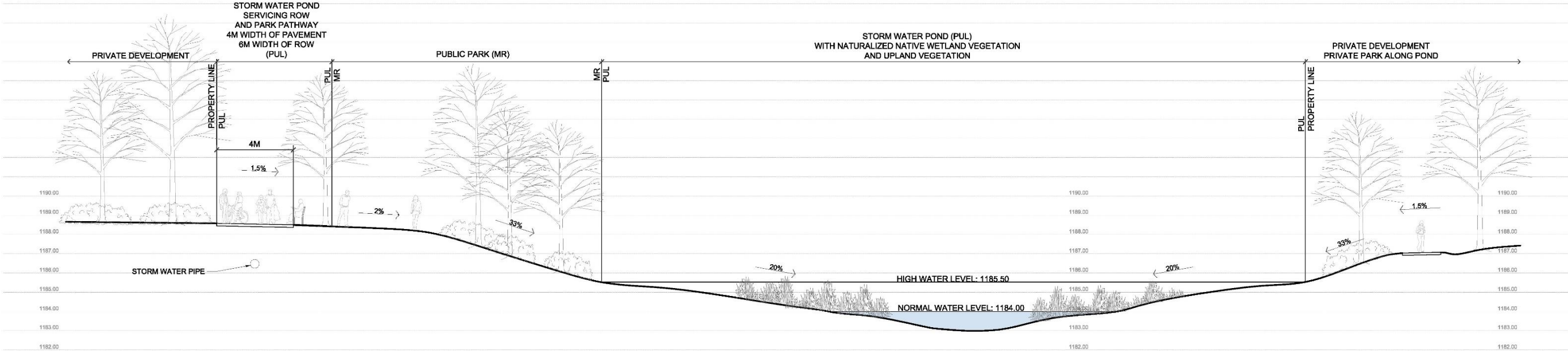


SECTION C-C 2: Longitudinal Section through proposed flat Public Park (MR) area between 19 Avenue and the Storm Pond

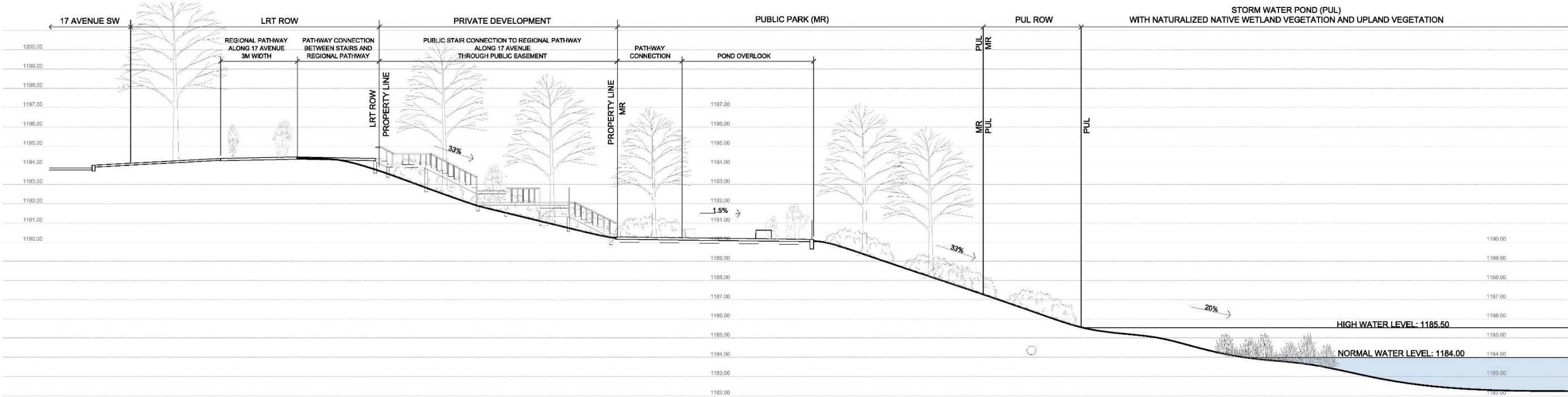
LANDSCAPE SECTIONS PUBLIC PARK (MR) - SHEET 3
SLOKKER SPRINGBANK HILL DEVELOPMENT



Proposed Outline Plan



CROSS SECTION D-D: Cross Section through width of Pond showing Public Park (MR) and Private Park along Pond perimeter



CROSS SECTION E-E: Stair Connection up to 17 Avenue and Overlook Amenity in Public Park (MR) at interface with Storm Pond

LANDSCAPE SECTIONS PUBLIC PARK (MR) - SHEET 4
SLOKKER SPRINGBANK HILL DEVELOPMENT



 Slokker Homes

24 September 2019

 O2
510 255 17th Ave SW Calgary Alberta Canada
T 403.228.1338 F 403.228.1320 www.o2esign.com

Subdivision Data Sheet

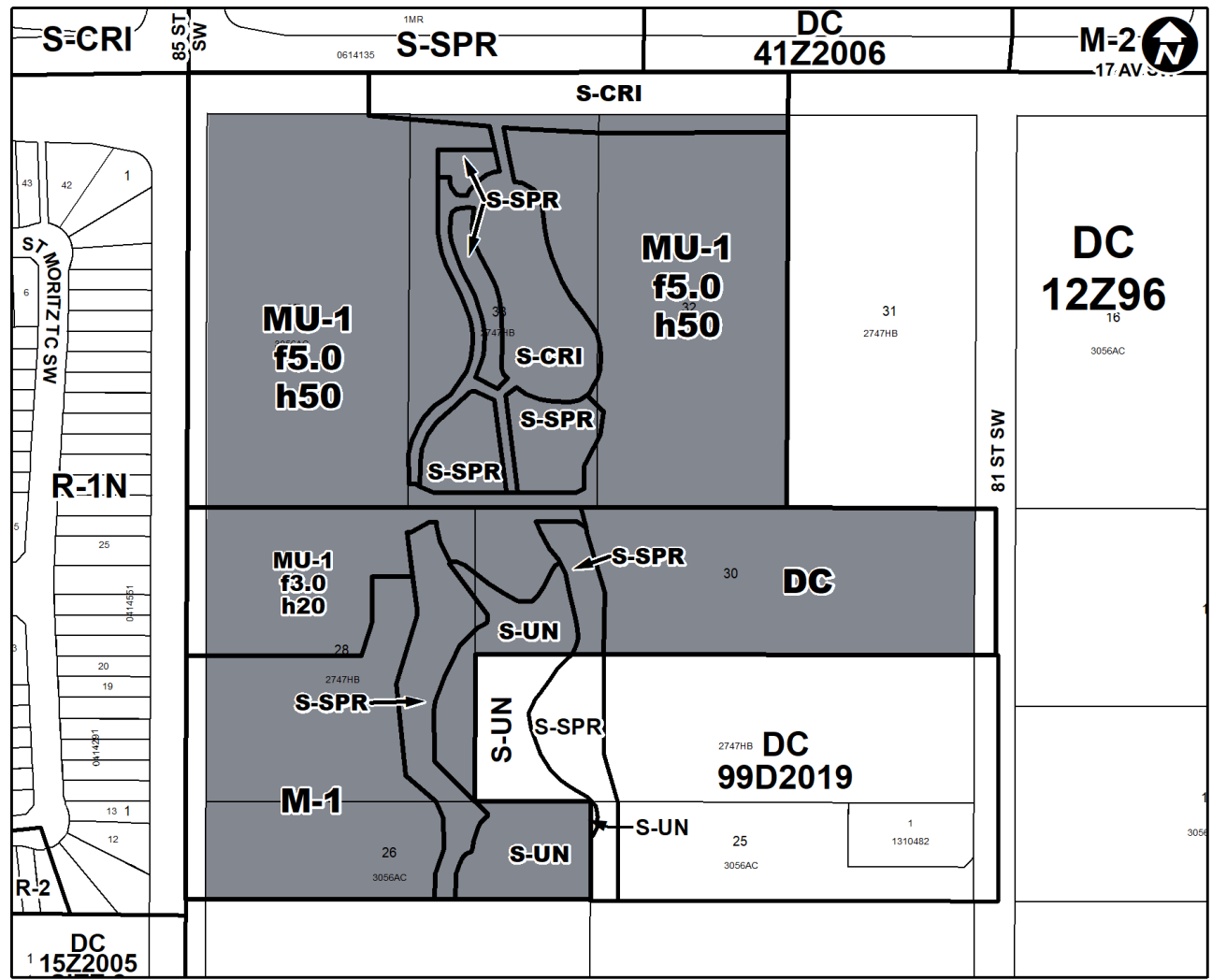
	HECTARES	ACRES
GROSS AREA OF PLAN	10.65	26.32
LESS: ENVIRONMENTAL RESERVE	0.62	1.53
NET DEVELOPABLE AREA	10.03	24.79

LAND USE (Residential / Mixed Use)	HECTARES	ACRES	# OF LOTS	# OF UNITS
MU-1 (f5.0h50)	3.94	9.74	3	850
MU-1 (f3.0h20)	0.49	1.22	1	48
DC (M-2)	1.17	2.90	1	226
M-1	1.31	3.24	2	111
Total Residential	6.91	17.10		1235

	HECTARES	ACRES	% OF NET AREA
ROADS (CREDIT)	1.24	3.07	12.40
Public Utility Lot	0.58	2.11	8.49

RESERVES	HECTARES	ACRES	% OF NET AREA
MR	1.01	2.51	10.10

Proposed Land Use District Map



What We Heard Report

June 2019: (Excerpt from Page 6 – What We Heard Report)

WHAT WE HEARD

Participants at the open house were encouraged to leave comments on the various information boards. The following is a summary of the comments that were received.

COMMENTS	APPLICATION RESPONSE
The C-Train on 69 St should go to Mount Royal University first, to move traffic flow.	This is a transportation decision. This proposal is in response to the City's plans to extend the Blue Line to 85 St SW. It strives to place new density closest to the LRT, in order to mitigate traffic impacts and support transit ridership. Additionally, the proposed 19 Ave SW will also alleviate traffic impacts by providing a new east-west connection through the community.
The maximum height of all development should only be 6 stories.	Much of the development will be 6-storeys and below. To place the highest concentration of future residents near the LRT, the proposal includes taller buildings in the north. This is in alignment with the ASP's mixed-use area.
Density is a main concern. Particularly for traffic going south on 85 St SW.	A transportation impact assessment has been submitted in support of the proposed development. As development occurs, including parcels to the south and east of the subject site, the City will need to respond with appropriate road network improvements.
There is already too much commercial activity in the area.	The commercial activity envisioned for the proposed development is in response to the anticipated population increase for the area. New commercial developments will be rationalized through appropriate market analyses.
No 10 storey buildings. 17 Ave SW is very busy and unsafe.	The proposed taller buildings in the north of the development are in alignment with the ASP. Taller buildings do not automatically increase traffic. Traffic impacts are a result of unit counts and the inclusion of Transportation Demand Management measures, or lack thereof. The community will have the opportunity to respond to proposed unit counts and detailed design at the Development Permit stage.
Need to consider emergency vehicle access.	The transportation impact assessment prepared for this project suggests that the surrounding road network will continue to function appropriately. Emergency vehicles will be able to access the site and circulate through the community safely and efficiently.

Community Association Letter

2019 October 28 – *via email*

To Joseph, Morgan and Melanie,

On behalf of the planning committee for the Springbank Hill Community Association I am providing you with our updated comments regarding the land use amendment plan for the Slokker proposed development at 85th Street & 17th Avenue SW.

Based on our interpretation of this land use application we are unable to provide our support at this time.

The Springbank Hill ASP was passed and approved by Council in the Summer of 2017. One of the primary objectives of the ASP was to create a cohesive plan for the 190 acre study area. By creating 5 neighborhood area density zones, placement of neighborhood nodes, and a neighborhood activity centre, the intended goal was to avoid the potentially inconsistent development approach that might otherwise occur as a result of having over 15 different land owners and developers in the study area.

At this time only 5 applications have been reviewed by CPC and council, within the 190 acre study area. Four were within the bounds of the ASP in terms of density, and build form. The 5th application requested a density increase of 150% without an ASP amendment, and we understand is currently under review. These 5 applications account for less than 20% of the 190 acre study area.

The subject of this letter, LOC2018-0085, represents the 6th application which will soon go to CPC for review.

As a result of our discussions with city planning, and with the applicant, we are very concerned that there appears to be a willingness to allow substantial increases in density and modifications to build form, and an increase in the amount of retail/commercial development versus limits originally established in the ASP. We are also concerned about the apparent lack of a formal mechanism or process to address the cumulative impacts of individual development changes on the overall 190 area study area, in terms of infrastructure needs, additional traffic impacts and other impacts to the existing community. We are also very disappointed with the city in that after many years of work and engagement with the community to establish an approved ASP in 2017 you appear to be prepared to dismiss the requirements and limits in the ASP document that were established by you and accepted by the community.

In reviewing this land use application there are several issues we wish to raise:

1. Request to change the ASP 'medium density' zone to 'mixed-use zone' in the area south of 19th Ave

The applicant has requested an ASP amendment to change the medium density zone to mixed use south of 19th ave along 85th street. Our understanding after discussing this with the planning department is that the applicant believes that the community will support additional retail offerings.

We are concerned that pushing retail development further south is outside the scope of the ASP and seems to be excessive in our opinion. We note that there is 260,000 sq ft of retail planned less than 100 metres from this location on the north side of 19th ave as per the ASP. There is

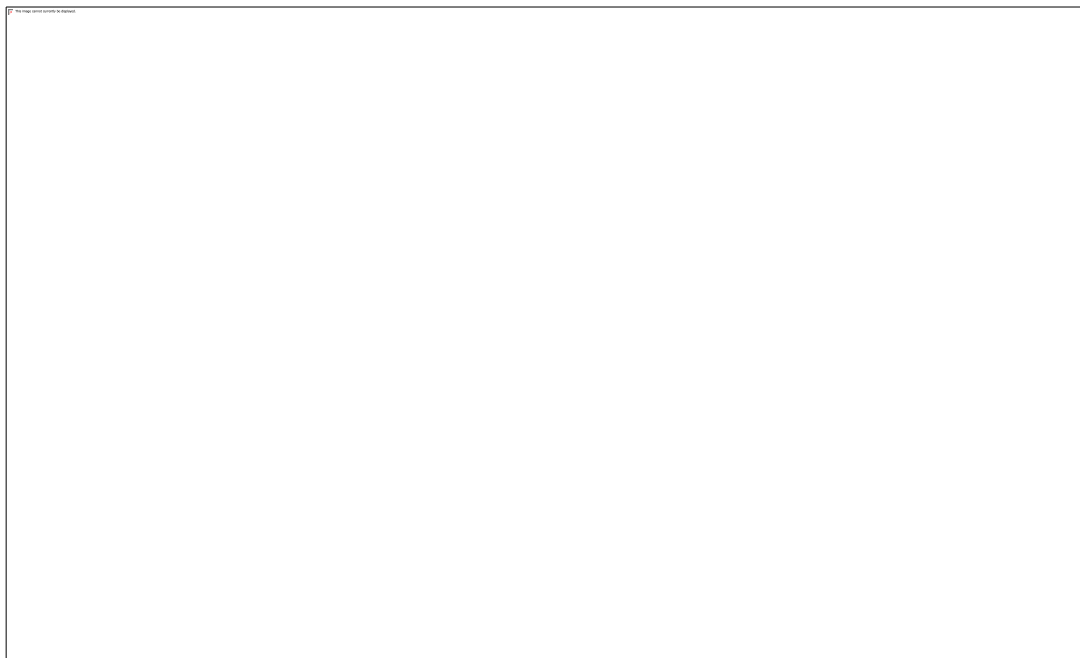
Community Association Letter

also an existing retail node less than 400 metres south of this location on the west side of 85th street. In addition, the existing Aspen Landing shopping mall is less than 400m north of this location. We had previously expressed concerns about potential traffic issues in our community and it is our believe that the additional retail will only make the problem worse.

The also wish to note that the ASP identifies a goal to achieve a pedestrian friendly 'livable street' on 19th Ave, incorporating a blend of retail and residential. We are very concerned that the proposals to date are not incorporating the desired design features for this mixed use area as originally defined in the ASP. We supported the following design elements when the ASP was approved but we have yet to see these elements included in any of the proposed developments:

- All development sites shall provide short, direct and convenient multi-modal connections to the community
- Active Modes Crossings shall be integrated within the site
- At-grade units should be oriented to the street and provide a seamless at-grade transition to the public sidewalk
- Sites shall incorporate a pedestrian-scaled lighting and street furniture treatment that contributes to a high quality urban environment
- Parking should be located underground where possible
- Where surface parking areas are considered, they should: a) be located at the rear or side of buildings and screened with landscaping; and b) incorporate Low Impact Development (LID) treatments to reduce environmental impacts

As a specific example, we are concerned with the proposed design of the condo building on the southeast corner of 85th St and 19th Ave SW. As shown in the attached illustration provided by the applicant, a retail mall is incorporated into the first floor of the a condo building with no street level access from 85th or 19th ave, a large surface level parking lot. This is not a pedestrian friendly design! We can't expect to achieve the goals stated in the ASP if we continue to allow the same type of automobile friendly designs!



Community Association Letter

We also request that the planning department provide the community with a sound basis for their recommendation to amend the ASP, only two years after establishing the original ASP guidelines.

2. Request to change the ASP 'Low Density' zone to 'Medium Density', south of the new proposed mixed use zone on the south side of 19th Ave

We understand that the applicant is requesting to change the 'low density' zone in their original application to 'medium density'. This request would increase the ASP planned density by 135%. We also note that the ASP requires a transition and variety of build forms which may not be addressed in this zone.

We also understand that the planning department would consider the amendment from low density to medium density to create a better transition to the proposed new mixed use modification. We believe the critical discussion will be the decision for the mixed use area, and if there is no support for that decision then our view is that the low density area should remain unchanged.

3. Request to change the ASP 'Medium Density' zone south of 19th ave, east of the ravine, to DC

The applicant is effectively requesting a density increase of 40% within the proposed DC area, over the original ASP medium density zone. The planning department criteria for approving this appears to be based on the fact that this location is adjacent 19th ave which is planned to be a 'livable street'. The designation of 19th Avenue as a 'livable street' was the original intention of the ASP so we question why the original zoning in the ASP is no longer relevant.

4. Insufficient development plans have been submitted

In reviewing the land use application and submitted development plans to date, preliminary development plans have been submitted for only 25% of the land use area. We are questioning how this land use application can proceed without additional information regarding development plans and an understanding of the full impact on densities versus the limits placed in the original ASP.

5. Traffic Study Requirements

We have made numerous requests to the city traffic department and planning department, as well as the applicant, to review the traffic impact analysis of this and other developments. To date, we have not received a satisfactory response. The community had been given information on the traffic analysis based on the initial ASP plans but we are unaware of any updated traffic impact studies. As we believe the study area has a very limited capacity to accept increased traffic we request further analysis and discussion on this matter.

6. Pathways and Connected Communities

Given the current patchwork of development along 85th street the ASP vision of a continuous pathway through the community seems to have been lost, leading to disconnected paths, streets designated as pathways where traffic densities are unknown, a pathway now suggested on the westside of 85th (the original plan was to run paths on the east side), but without clear

Community Association Letter

development plans we are concerned about the city's ability to deliver on it's vision. In reviewing current development plans, placement of residential buildings are being positioned to limit multi-modal traffic in an east west fashion, in essence creating a gated community, purposely making it difficult for the community at large to traverse the new developments. This is at odds with the city's planning criteria.

In summary, given the above 6 concerns, we cannot support this application. We are planning to meet with the applicant to discuss their plans in more details and to present our concerns. In addition, our community association requests that the city require the applicant to submit concurrent development plans for the entire land use application area, so that the planning department, CPC, council and the community can fully understand the impacts of the revised proposal to the overall 190 area study area development.

Finally, we question why the city would even consider deviating significantly from the original ASP plans for this area. The community has accepted the intentions of the original ASP, any amendment should require new community engagement sessions otherwise the entire ASP process loses credibility with the community.

Sincerely,

Elio Cozzi
President, Springbank Hill Community Association
website: springbankhill.org

Planning & Development
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-0695

Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses, LOC2016-0088

EXECUTIVE SUMMARY

This land use amendment application was submitted on 2016 April 18 by Mediated Solutions, representing 26 landowners and 24 parcels in the community of Ramsay. The application proposes to redesignate the subject site, including a number of parcels containing historic properties, from Residential Contextual One / Two Dwelling (R-C2) District to Direct Control (DC) District based on the Multi-Residential – Contextual Low Profile (M-C1) District to allow for:

- a broad range of residential development including multi-residential buildings (e.g. townhouses);
- a maximum building height of 14 metres, approximately three to four storeys (an increase from the current maximum of 10 metres, approximately two to three storeys);
- a maximum of 155 units per hectare, which results in approximately 147 units for the entire site of 0.95 hectares \pm (an increase from the current maximum of two units per parcel or about 48 units for the entire site);
- the uses listed in the M-C1 District with the addition of *Character Home Retention Development* and Contextual Single Detached Dwelling;
- regulations to allow for a larger building height cross section than the standard M-C1 district for multi-residential development; and
- requirement for at-grade entrances for all ground floor units and the entrance for at least one unit to front onto Maggie Street SE for multi-residential development of three units or more.

Administration recommends refusal of the proposed land use redesignation and the associated amendment to the existing *Ramsay Area Redevelopment Plan* (ARP). The proposal does not conform to the existing ARP and is not in keeping with heritage policies of the *Municipal Development Plan* (MDP) which directs The City to identify and help protect Calgary's historic resources. It is Administration's position that the proposal may incentivize the destruction of a high concentration of properties that contribute to the heritage character of Ramsay without any mechanism to offset the loss.

No development permit application has been submitted at this time.

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

ADMINISTRATION RECOMMENDATION:

That Calgary Planning Commission recommend that Council hold a Public Hearing; and

1. **REFUSE** the proposed amendments to the Ramsay Area Redevelopment Plan (Attachment 1); and
2. **REFUSE** the proposed redesignation of 0.95 hectares \pm (2.35 acres \pm) located at 1105, 1107, 1109, 1111, 1113, 1115, 1117, 1121, 1123, 1125, 1129, 1131, 1133, 1135, 1137, 1139, 1141, 1143, 1145 and 1147 – 8 Street SE, 803 and 805 - 11 Avenue SE and 1110 and 1120 Maggie Street SE (Plan A2, Block 17, Lots 4 to 13, 15 to 22, and 25 to 27; Plan 8310686, Block 17, Lots 28 to 30) from Residential Contextual One / Two Dwelling (R-C2) District **to** DC Direct Control District to accommodate a broad range of residential development including multi-residential buildings, with guidelines (Attachment 2) and abandon the proposed Bylaw.

PREVIOUS COUNCIL DIRECTION / POLICY

None.

BACKGROUND

The application was submitted on 2016 April 18 by Mediated Solutions on behalf of 26 landowners and 24 parcels in the community of Ramsay. The subject site comprises nearly an entire city block. The remaining parcels on the block, not included in the proposal, include the commercial development at 1101 - 8 Street SE, and the residential parcels 801 - 11 Avenue SE, 1127 - 8 Street SE and 1140 Maggie Street SE. The owners of these parcels elected to not partake in the proposal. As indicated in the Applicant Submission (Attachment 3), the applicant is seeking approval of the proposed DC District which does not include a mechanism to offset the potential loss of heritage properties on the subject site. Despite not supporting the proposal, Administration has put in significant effort to work with the applicant to ensure the proposed DC District will enable development that responds to the unique characteristics of 8 Street SE and Maggie Street SE.

Due to this, the length of time to bring the application forward to Calgary Planning Commission is longer than Administration's typical performance commitments. An overview of key milestones and dates of the process are provided in Attachment 4. Contributing factors to the complexity of the application and resulting extended review timeline include:

- a high number of landowners with various interests and readiness for redevelopment;
- a concurrent and ongoing ARP process and community-wide discussions around appropriate intensity of development throughout the communities of Inglewood and Ramsay, and approaches and tools for heritage conservation; and

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-0695

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

- change of ownership and addition/removal of parcels from the application throughout the process.

Three properties within the site are identified on Map 3 Historic Interest Sites of the existing ARP. These include: Site 22, 1129 - 8 Street SE, and Site 42, 1105 and 1107 - 8 Street SE. Additionally, in a survey conducted by The City's Heritage Planning team the following properties within the subject site were identified as contributing to the heritage character of Ramsay: 1105, 1107, 1113, 1115, 1129, 1135, 1137, 1141, 1145, and 1147 - 8 Street SE. These properties in combination with those identified in the ARP represent 42 percent (10 out of 24 parcels) of the existing properties within the site, which is a high concentration of properties contributing to a community's heritage character within a single block.

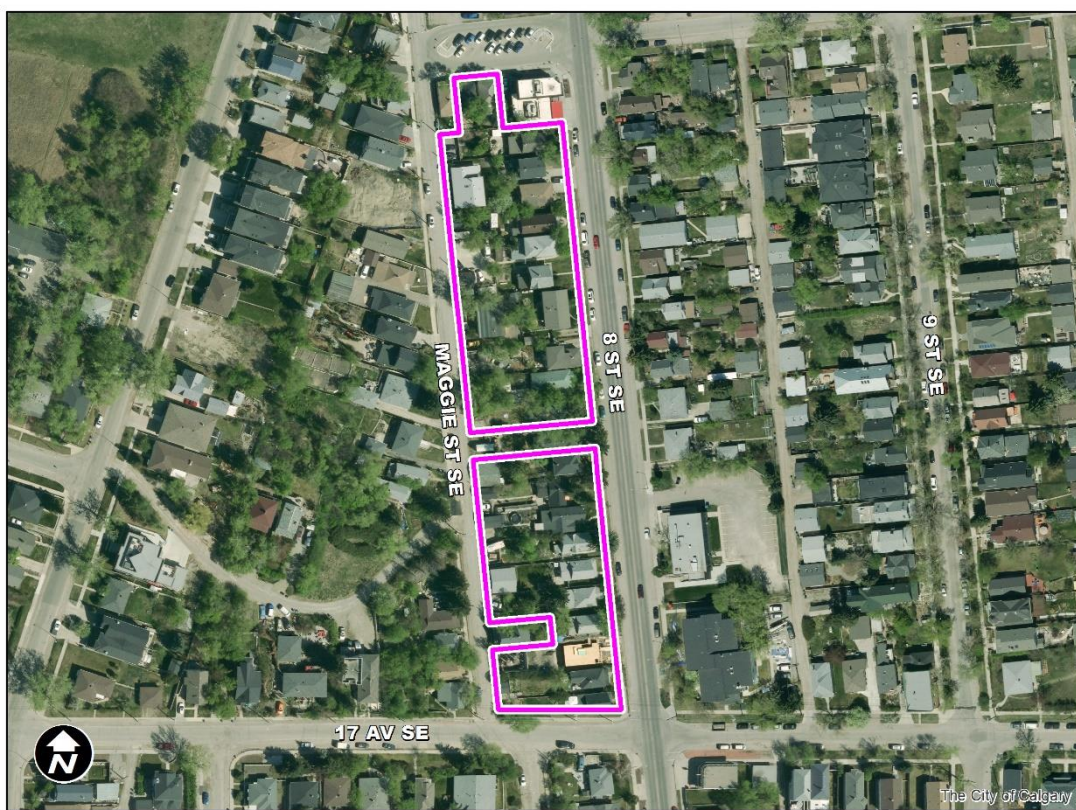
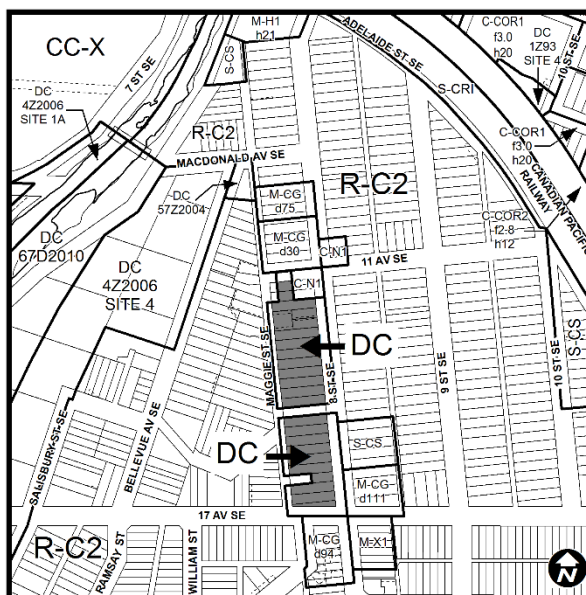
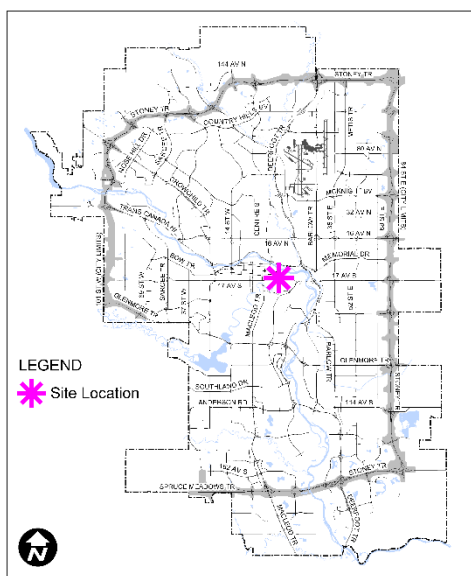
There are many properties throughout Calgary with heritage value that are not currently listed on The City's Inventory of Evaluated Historic Resources (the Inventory). While none of the properties within the subject site are currently listed on the Inventory, they have been identified to have heritage value and may merit inclusion but have yet to be listed.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-0695

Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695

Location Maps



**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

Site Context

The subject site includes 24 parcels along 8 Street SE between 17 Avenue SE and 11 Avenue SE in the community of Ramsay. The Ramsay Community Association building and a multi-residential parcel are located across 8 Street SE from the southeast corner of the site. Other surrounding development is designated low density residential on the east side of 8 Street SE and on the west side of Maggie Street SE. Multi-residential development is located south of the site on 17 Avenue SE.

The subject site is developed with low density residential development consisting of single detached and semi-detached dwellings. The northern portion of the site is approximately 130 metres in length and 45 metres wide, and the southern portion of the site is approximately 94 metres in length and 45 metres wide, the entire site encompasses an area of 0.95 hectares \pm (2.35 acres \pm). The subject site is located within 400 metres of the future Green Line LRT station with easy access to the downtown core. Two parcels within the site have been subdivided to create two individual parcels with one fronting onto Maggie Street SE and the other onto 8 Street SE. Four parcels have existing vehicle access from 8 Street SE, and the remaining twenty have access from Maggie Street SE.

As identified in *Figure 1*, Ramsay's peak population was in 1969, reaching 3,005 residents.

Figure 1: Community Peak Population

Ramsay	
Peak Population Year	1969
Peak Population	3,005
2018 Current Population	2,187
Difference in Population (Number)	-818
Difference in Population (Percent)	-27%

Source: The City of Calgary 2018 Civic Census

Additional demographic and socio-economic information may be obtained online through the [Ramsay](#) community profile.

INVESTIGATION: ALTERNATIVES AND ANALYSIS

The proposal does not align with the existing ARP or MDP policies related to heritage and is not supported by Administration for the reasons outlined in the following sections. Alternative land use options that were considered through the application review process are identified as part of this report's analysis.

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

Planning Considerations

The following sections highlight the scope of technical planning analysis conducted by Administration and alternative land use options that were considered.

Land Use

Existing Land Use

The existing Residential – Contextual One / Two Dwelling (R-C2) District is a residential designation in developed areas that is primarily for single-detached, semi-detached and duplex homes. Single-detached homes may include a secondary or backyard suite. The R-C2 District allows for a maximum building height of 10 metres and a maximum of two dwelling units per parcel, or a maximum of 48 units for the entire application site.

Proposed Land Use

The proposed DC District is based on the Multi-Residential – Contextual Low Profile (M-C1) District and allows for:

- a broad range of residential development including multi-residential buildings (e.g. townhouses);
- multi-residential development with entrances at-grade for all ground floor units;
- a maximum building height of 14 metres;
- a maximum of 155 units per hectare; and
- the uses listed in the M-C1 District with the addition of Character Home Retention Development and Contextual Single Detached Dwelling.

Throughout the application review, and in response to comments received from community members and the Ramsay Community Association, the applicant and Administration explored ways to address the unique characteristics of Maggie Street SE. One of the major themes in community comments was the desire for Maggie Street SE to retain its feel and function as a street and not become a de-facto lane. In response to this comment, the proposed DC District requires at least one entrance and amenity space to face onto Maggie Street SE for multi-residential development of three units or more.

Challenges facing redevelopment of the subject site were raised throughout the application process. These challenges include significant variation in grades, lack of flexibility within the standard M-C1 District regulations regarding building massing and site design, and different timelines and readiness for redevelopment between the various landowners along with a desire for a broad range of built forms. The applicant and Administration collaborated to explore options to address these challenges and agreed upon the specific regulations included in the proposed DC District regarding:

- at-grade entrances – required for units located on the floor closest to grade;

**Planning & Development Report to
Calgary Planning Commission
2019 November 21**

**ISC: UNRESTRICTED
CPC2019-0695**

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

- building setbacks – 1.5 metre front yard setback, and potential for 0 metre side yard setback for property lines shared with commercial parcels or parcels that are part of the DC District;
- landscaping – potential for landscaping to be provided above grade;
- building height – larger horizontal cross section as regulated by Land Use Bylaw P12007;
- a broad range of built-forms – Contextual Single Detached Dwelling included as a permitted use; and
- parking – no parking stalls required for units less than 45 square metres.

Character Home Retention Development has been included as a discretionary use in the proposed DC District. The use is not required but is included as an option to enable and accommodate future development that retains existing heritage properties while achieving increased intensity through additional units or buildings. This use was first introduced in Calgary in the community of Bridgeland-Riverside in 2017 July and is increasingly common in established communities in cities such as Vancouver. Rules outlining what retention entails is included in the proposed DC District and an overview of the components of character homes is provided in Attachment 5.

While Administration worked with the applicant to address both community comments and the expressed challenges to redevelopment of the site, Administration does not support the proposed DC District as the proposal incentivizes the demolition of heritage properties without a mechanism to offset the loss. An overview of alternative land use options that were explored is provided in the following section.

Alternative Land Use Options

Given the concentration and number of Historic Interest Sites and properties contributing to the heritage character of Ramsay within the site, and the direction in the MDP and existing ARP to encourage the conservation of heritage resources, Administration and the applicant explored mechanisms to do so. Throughout the review of the application, Administration provided the option to amend the application and discussed alternative approaches to a refusal recommendation, including:

1. hold the application until the forthcoming *Historic East Calgary Local Growth Plan* (draft title), a new ARP for the communities of Inglewood and Ramsay, has been adopted by Council, and align the proposal to community-wide strategies for heritage included in the ARP;
2. a major amendment to the existing ARP that incorporates a community-wide strategy for heritage;
3. amending the application to a smaller site with a concurrent development permit for parcels that are ready for redevelopment;
4. moving forward with the Administration supportable DC District (Attachment 6); or

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

5. an alternative DC District that provides a mechanism for offsetting the impacts from increased intensity on the heritage character of Ramsay other than the density bonus provisions included in the Administration supportable DC District noted above.

Alternative Option 1 – hold until new ARP

The applicant was not interested in holding the application until the forthcoming *Historic East Calgary Local Growth Plan* (draft title) has been adopted due to uncertainty around timing.

Alternative Option 2 – major amendment to the existing ARP

A major amendment to incorporate a community-wide strategy for heritage conservation within the existing ARP was considered but given ongoing efforts to update the ARP as part of the *Historic East Calgary Local Growth Plan* (draft title) process, this approach was determined to be ill-timed and inappropriate given the proposal is applicant-initiated.

Alternative Option 3 – amending application site

Despite the landowners having various levels of readiness and interest in redevelopment, an amendment to the application site to a smaller site along with a concurrent development permit application was not pursued. It was identified by the applicant that there is benefit in pursuing redesignation for the block comprehensively, aside from those individual landowners who elected to not participate in the application.

Alternative Option 4 - Administration supportable DC District

A compromised solution that allows for increased intensity balanced with offsetting the impacts from that intensity was provided to the applicant and landowners by Administration. The solution put forward is a DC District based on the Multi-residential – Contextual Grade-Oriented (M-CG) District that allows increased intensity on the site of 111 units per hectare along with opportunity for further density up to a maximum of 155 units per hectare through density bonus options which offset the increased intensity with community contributions in the form of heritage preservation. The specific regulations regarding at-grade entrances, entrances facing Maggie Street SE, building setbacks, landscaping, building height and motor vehicle parking stalls included in the proposed DC District have also been incorporated into the Administration supportable DC District.

The Administration supportable DC District is intended to allow for:

- a maximum building height of 12 metres;
- a maximum of 111 units per hectare with opportunity for additional density up to 155 units per a hectare for Character Home Retention Developments and development that utilizes the two density bonus options; and
- the uses listed in the M-CG District with the addition of Character Home Retention Development and Contextual Single Detached Dwelling.

The applicant was not interested in moving forward with this option for the reasons provided in Attachment 3.

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

Alternative Option 5 – alternative DC with a different mechanism to offset impacts.

Administration did discuss alternative mechanisms or options for offsetting the impacts from increased intensity on the heritage character of Ramsay with the applicant. However, no alternative mechanisms to those included in the Administration supportable DC District were brought forward by the applicant.

The applicant has elected to proceed with the proposed DC District and a recommendation of refusal, which allows for increased intensity on the site up to 155 units per hectare without a mechanism to offset the impact on the heritage character of Ramsay.

Development and Site Design

A development permit application has not been submitted for the site, and varied interest and timelines for redevelopment are anticipated across the 24 parcels; thus, the interface between new and existing development will be important as redevelopment occurs. IN the review of future development permit applications on the site, attention should be paid to materials and buffers at the interface between new development and existing properties, particularly in cases where rowhouse style development is desired to be built overtime across adjacent parcels. Setbacks and buffers through design elements such as landscaping should also be utilized between new development and the three residential parcels that are not part of the proposed land use amendment.

The importance of the interface with Maggie Street SE requires design considerations to ensure the street does not become a de-facto lane, including solutions such as entrances and amenities that front onto Maggie Street SE, as well as landscaping. If the entire subject site is redeveloped through a single development consideration for the interface with Maggie Street SE will be particularly important. Limiting vehicular access off of 8 Street SE is important for ensuring pedestrian safety along the street. As indicated in the accompanying amendments to the existing ARP, vehicular access should be from Maggie Street SE. Where access from Maggie Street SE is a challenge, access from 8 Street SE may be considered where land assembly and comprehensive redevelopment is proposed and impacts to pedestrian, cyclist, and vehicular circulation are mitigated.

Lastly, where Character Home Retention Developments are proposed, flexibility in building and site design should be exercised to enable the conservation of heritage properties through the support of relaxations. However, any additions or extensions to existing character homes should be subordinate when viewed from 8 Street SE so as to not detract from the heritage aspects of the property.

Environmental

No environmental issues have been identified at this time.

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

Transportation

A Transportation Impact Assessment and parking study were not required for this land use amendment, however these may be required as part of any future development permit applications.

There are two bus routes that run along 8 Street SE, Route 17 and Route 24. The bus stops are located northbound at the Community Association building and southbound at 11 Avenue SE. The future Inglewood Ramsay LRT Station will be located within approximately a 400 metre walk from the subject site.

Eighth Street SE is protected for pedestrians and cyclists, therefore vehicular access should be taken from Maggie Street SE to prevent conflict between vehicles and pedestrians and cyclist through the creation of new driveways. When land assembly and comprehensive redevelopment is proposed, limited access from 8 Street SE may be considered. The 1.5 metre front yard setback combined with the existing 1.0 metre right-of-way on the west side of Maggie Street SE may be used to develop a 1.2 metre sidewalk as redevelopment occurs.

Utilities and Servicing

Water, sanitary, and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time. At the development permit stage, overland stormwater drainage is not permitted to cross property lines as per the Drainage Bylaw, unless an overland drainage agreement and private easement between property owners is executed. A comprehensive design to consolidate some or all of the lots (and possibly creation of a condominium) may provide a practical solution to manage the drainage requirements and minimize the number of service connections that will be required for future redevelopment.

Stakeholder Engagement, Research and Communication

In keeping with Administration's practices, this application was circulated to relevant stakeholders and notice posted on-site. Notification letters were sent to adjacent land owners and the application was advertised online through the Development Map.

In 2017 April, the application site was expanded to include two additional sites, 803 and 805 11 Avenue SE which prompted an additional circulation to stakeholders and notice posting. One public meeting was held by the applicant in 2016 August, which Administration attended.

Administration received two letters from the Ramsay Community Association. Comments from the Ramsay Community Association were received on 2016 July 21 and a second letter was received on 2018 August 23 in response to the proposed land use and updates made to the application through the review process (Attachment 7).

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-0695

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

The Ramsay Community Association has indicated support of the application but have also shared concerns regarding the interface with Maggie Street SE, impacts on parking and pedestrian safety if access is from 8 Street SE, the availability of infrastructure to support redevelopment, and addressing heritage in an individual application versus community-wide ARP policy. Administration has reached out numerous times to meet and discuss the proposal and the relationship between the application and ongoing ARP work with the Ramsay Community Association. It was clarified through a phone conversation with the Ramsay Community Association in 2019 May that they felt a meeting was not required.

In response to the notice postings and circulation of the land use amendment, 10 letters were received. Eight of the letters indicated support of redevelopment in Ramsay as long as the history and unique character of the community are respected. Two letters indicated objection to the application. Reasons stated for opposition include:

- desire for conservation of heritage properties;
- concern about Maggie Street SE being turned into a lane through redevelopment on 8 Street SE;
- lack of appropriateness of three to four storey development given scale of existing properties;
- negative impacts on views and solar access; and
- concerns regarding access and pedestrian safety along 8 Street SE.

Administration is in the process of drafting a new ARP for Ramsay and Inglewood, the *Historic East Calgary Local Growth Plan* (draft title). Draft policies and land uses are currently being developed and reviewed. Consideration for the stakeholder feedback collected and draft direction of the forthcoming ARP were incorporated into Administration's review of this proposal. A major discussion through engagement for the forthcoming ARP has been regarding ways to balance increased intensity with community benefits from redevelopment, and potential approaches to heritage conservation in two of Calgary's oldest neighbourhoods.

Following Calgary Planning Commission (CPC), notifications for a Public Hearing of Council will be posted on-site and mailed out to adjacent landowners. In addition, CPC's recommendations and the date of the Public Hearing will be advertised.

Strategic Alignment

South Saskatchewan Regional Plan (2014)

The proposed land use in this report is aligned with the policy direction of the *South Saskatchewan Regional Plan* (SSRP), which directs population growth in the region to Cities and Towns and promotes the efficient use of land.

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

Interim Growth Plan (2018)

The proposed land use aligns with the policy direction of the *Interim Growth Plan*. The proposed land use amendment builds on the principles of the *Interim Growth Plan* by means of promoting efficient use of land, regional infrastructure, and establishing strong, sustainable communities.

Municipal Development Plan (Statutory – 2009)

In accordance with the Urban Structure Maps (Map 1) of the *Municipal Development Plan* (MDP), the subject site is identified as being located within the Developed Residential Area – Inner City Area. The land use policies in Section 3.5.2 encourage intensification of inner-city communities through redevelopment that is consistent and compatible with the existing character of the neighbourhood.

Heritage is identified in the MDP as being an integral part of good city building. Heritage policies in Section 2.3.3 provide direction for the identification of properties of special historic quality and character and that land owners are encouraged to conserve and/or enhance heritage resources.

While the proposal would enable intensification of an inner-city community, the scale of the site, almost an entire block, and the proposed maximum density of 155 units per hectare, approximately 147 units across the entire site may result in a significant change from the existing character of 8 Street SE and Maggie Street SE. Furthermore, the proposed land use amendment would incentivize the loss of a high concentration of buildings that contribute to the heritage character of Ramsay with no mechanism included in the proposed DC District to compensate for or offset their loss.

Ramsay Area Redevelopment Plan (Statutory – 1994)

Administration is currently drafting the *Historic East Calgary Local Growth Plan* (draft title) for the communities of Ramsay and Inglewood as part of the Green Line land use planning work, which would replace the current ARPs for both communities.

The existing *Ramsay Area Redevelopment Plan (ARP)* was adopted by Council in 1994 and updated in 2015. The subject site is located within the Low Density Residential area as identified on Map 1 of the ARP. The applicable policies for this area are intended to maintain stability and encourage new development to reflect the character of the existing streetscape. Low density conservation policies in Section 3.1 of the ARP directs for the conservation of existing structures in good repair and the rehabilitation or redevelopment of properties in structurally poor condition.

Additionally, three properties within the site are identified on Map 3 – Historic Interests Sites of the ARP (Site 22, 1129 - 8 Street SE and Site 42, 1105 and 1107 - 8 Street SE). Section 2.0 of the ARP encourages the conservation of heritage resources in Ramsay, and for additions and renovations to identified potential heritage or historically significant structures to respect the existing character of the site. The addition of Character Home Retention Developments as a

**Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695**

discretionary use in the proposed DC District aligns with this approach to conservation. However, the proposed increase in density will incentivize the loss of heritage resources.

An amendment to the ARP is required to accommodate the increase in density proposed with this application. The proposed density and building forms do not align with policies in the existing ARP for the site, which limit development to the existing R-C2 designation or similar (Attachment 1). The amendment identifies the subject site as a Medium Density Residential area on Map 1 – Action Plan - Executive Summary. Proposed new policy in Section 3.1 allows for a broader range of building forms than existing ARP policy, including low-rise multi-residential developments with direct access to grade for ground floor units, character home retention developments, rowhouses, and town houses. Additional policy in Section 3.1 provides direction that access to the site should be from Maggie Street SE. Access from 8 Street SE may be considered where land assembly and comprehensive redevelopment is proposed and impacts to pedestrian, cyclist, and vehicular circulation are mitigated. The proposed amendment also requires an update to Map 4 – Residential Sub-Area Sites Under Policy Review in order to create a new sub-area with associated development guidelines for the subject site.

Social, Environmental, Economic (External)

The proposed land use amendment may result in the loss of existing properties that contribute to the heritage character of Ramsay.

Financial Capacity

Current and Future Operating Budget

There are no known impacts to the current and future operating budgets at this time.

Current and Future Capital Budget

The proposed land use amendment does not trigger capital infrastructure investment and therefore, there are no growth management concerns at this time.

Risk Assessment

The proposed land use amendment may result in the loss of existing properties that contribute to the heritage character of Ramsay by incentivizing their demolition through allowing for an increase in maximum density from two units per a parcel to a maximum of 155 units per a hectare. Administration provided a mechanism to mitigate this risk through the density bonus provisions incorporated into the Administration supportable DC District, however, the applicant has elected to proceed with the proposed DC District which does not include the density bonus provisions or an alternative mechanism.

Planning & Development Report to
Calgary Planning Commission
2019 November 21

ISC: UNRESTRICTED
CPC2019-0695

Policy Amendment and Land Use Amendment in Ramsay (Ward 9) at multiple addresses,
LOC2016-0088, CPC2019-0695

REASON(S) FOR RECOMMENDATION(S):

Administration does not support the proposal and recommends refusal as the proposed land use amendment does not align with the heritage policies of the *Municipal Development Plan* and will incentivize the loss of a high concentration of buildings that contribute to the heritage character of Ramsay without any mechanism to offset the loss. Furthermore, the proposal does not align with the existing *Ramsay Area Redevelopment Plan* which identifies the site as low density residential, and could result in significant change to the existing context along 8 Street SE and Maggie Street SE.

ATTACHMENT(S)

1. Proposed Amendments to the Ramsay Area Redevelopment Plan
2. Proposed DC Direct Control District
3. Applicant Submission and List of Landowners
4. Application Timeline
5. Character Home Components
6. Administration Supportable Proposed DC Direct Control District
7. Community Association Letters

Proposed Amendments to Ramsay Area Redevelopment Plan

1. The Ramsay Area Redevelopment Plan attached to and forming part of Bylaw 1P94, as amended, is hereby further amended as follows:

- (a) Delete the existing Map 1 entitled 'Action Plan – Executive Summary' and replace with the revised Map 1 entitled 'Action Plan – Executive Summary', attached as Schedule A.

- (b) In Section 3.0 Land Use And Development, section 3.1.2.3 Medium Density Residential, after the second paragraph insert the following paragraph:

"In addition, the site comprising of 1105 -1147 8 Street SE (except for 1127 8 Street SE), 1110 and 1120 Maggie Street SE, and 803 and 805 -11 Avenue SE was redesignated to accommodate medium density residential development. Redevelopment may occur on individual parcels or comprehensively across a number of parcels. Considering the site's proximity to downtown and a proposed LRT station, a broad range of building forms including low-rise multi-residential developments (with direct access to grade for ground floor units), character home retention developments, rowhouses, and townhouses are appropriate. Generally, vehicular access should be from Maggie Street SE. Where land assembly and comprehensive redevelopment is proposed, limited vehicular access from 8 Street SE may be considered, where impacts to pedestrian, cyclist, and vehicular circulation are mitigated, at the discretion of the Development Authority."

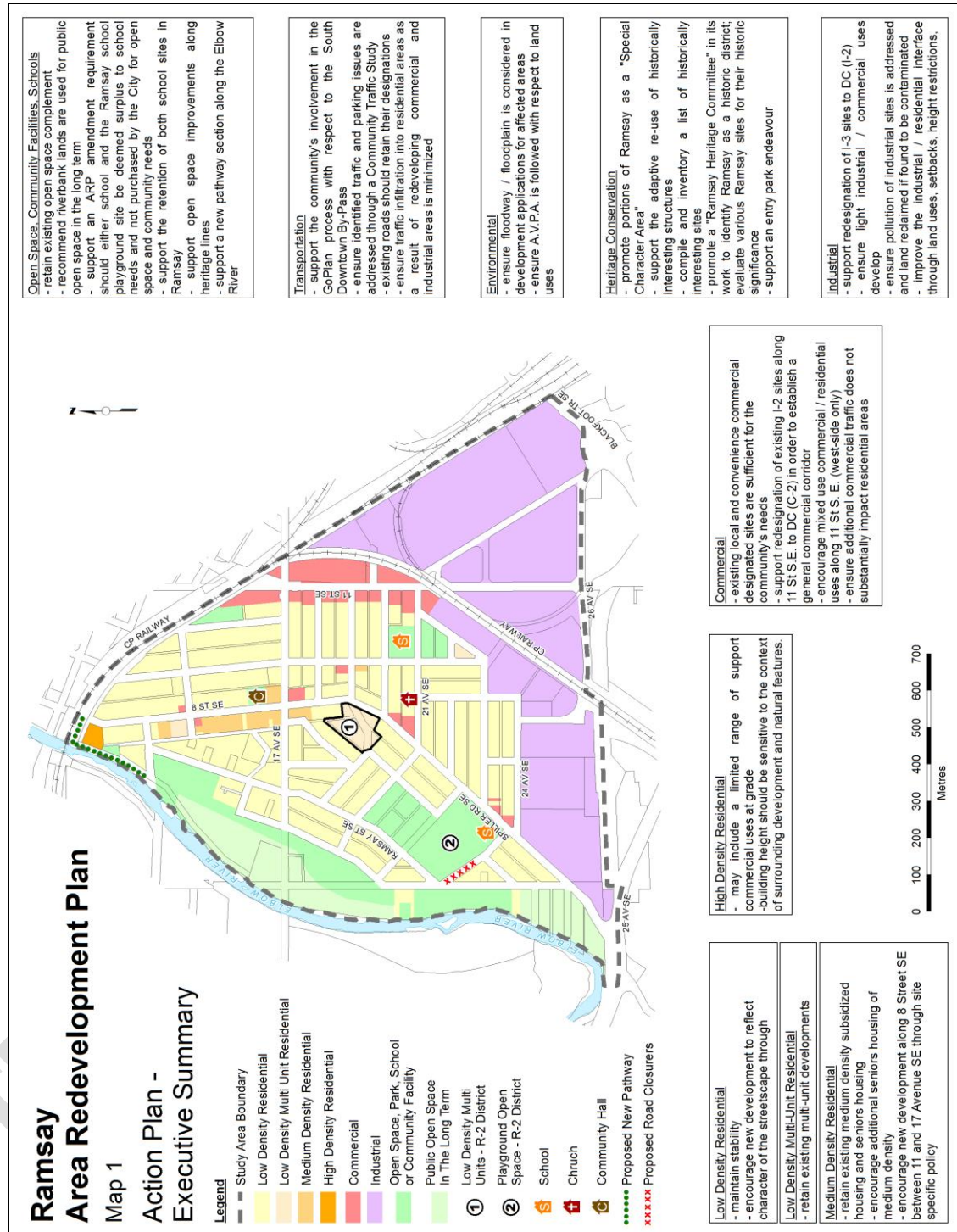
- (c) Delete the existing Map 4 entitled 'Residential Sub-Area and Sites Under Policy Review' and replace with the revised Map 4 entitled 'Residential Sub-Area and Sites Under Policy Review', attached as Schedule B.

- (d) In Section 3.0 Land Use and Development, Implementation, after subsection 3.1.4.2 Sub-Areas 3, 4 and 5 Development Guidelines; insert the following new subsection: 3.1.4.3 'Sub-Area 6 Development Guidelines':

"Sub-area 6 is intended for a broad range of building forms that allow for an increase in density that is compatible with the existing community and street character. The range of appropriate building forms include low-density forms, low-rise multi-residential developments with direct access to grade for ground floor units, character home retention developments, rowhouses, and townhouses. Development in this sub-area must consider the interface with Maggie Street SE and incorporate building and design elements that contribute to maintaining a residential street experience."

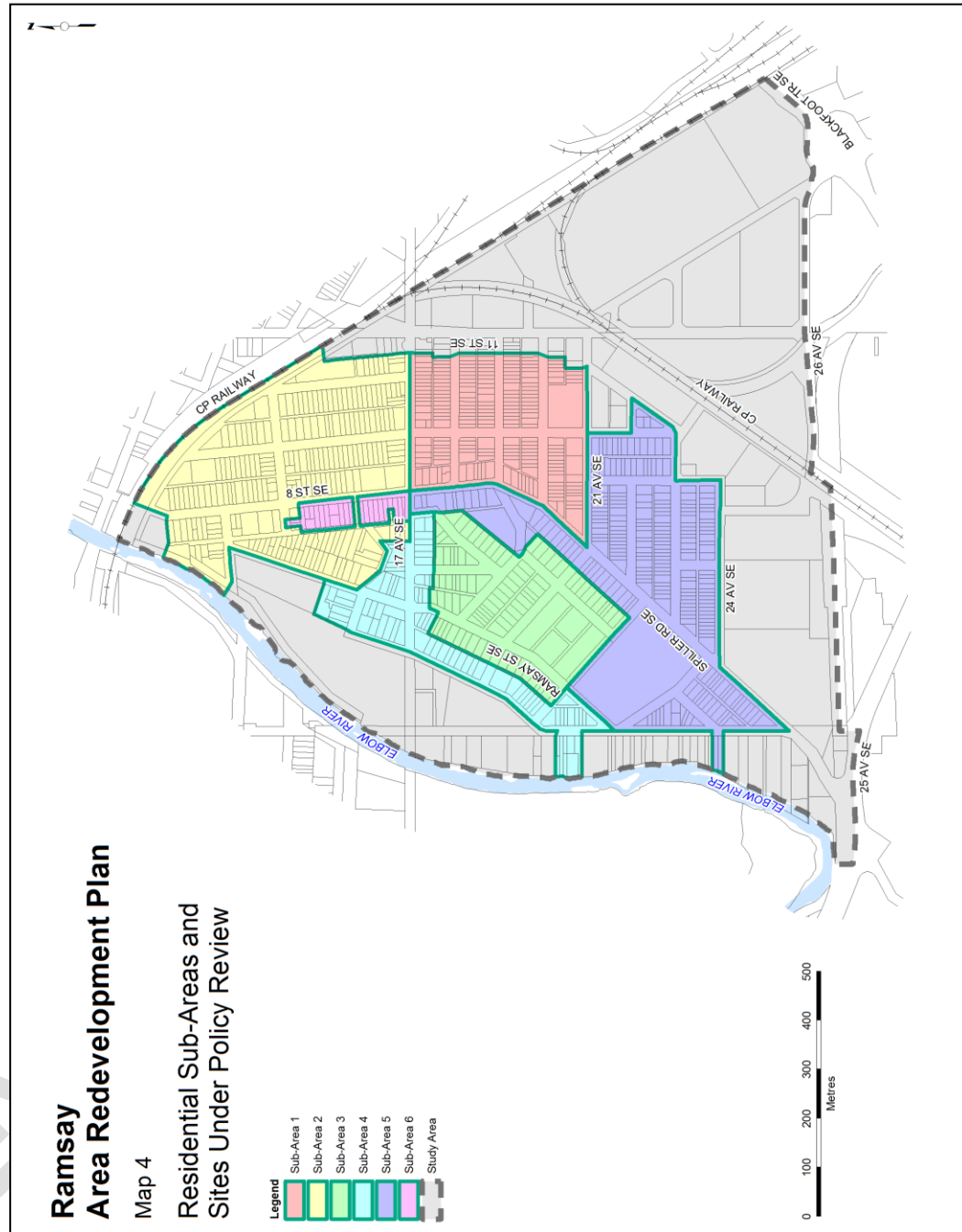
Proposed Amendments to Ramsay Area Redevelopment Plan

SCHEDULE A



Proposed Amendments to Ramsay Area Redevelopment Plan

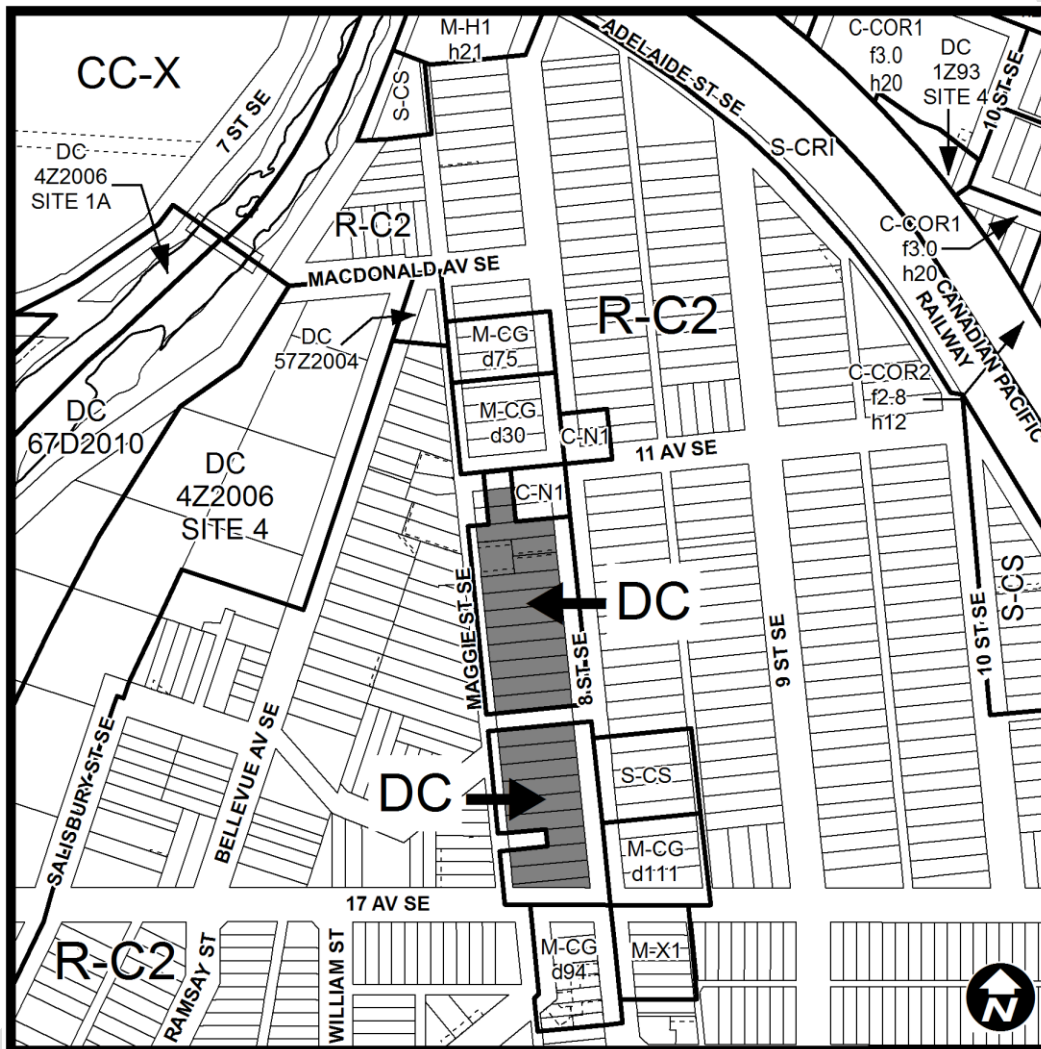
SCHEDULE B



Proposed DC Direct Control District

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by amending that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "A".

SCHEDULE A



DIRECT CONTROL DISTRICT

Purpose

- 1 This Direct Control District Bylaw is intended to:

- (a) allow for transit oriented development in proximity to an LRT station;

Proposed DC Direct Control District

- (b) allow for a broad range of residential development including low-rise grade-oriented multi-residential development that responds to the interface with 8 Street SE and Maggie Street SE and the unique character of the block and neighbourhood; and
- (c) include custom site and building design rules to allow for both the retention and restoration of character homes combined with new grade-oriented multi-residential development, and the redevelopment of multi-residential development.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District Bylaw.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District Bylaw, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

General Definitions

- 4 In this Direct Control District Bylaw:

- (a) “**character home**” means a **Duplex Dwelling**, **Semi-detached Dwelling**, or **Single Detached Dwelling** existing on the date of passage of this Direct Control District Bylaw that:
 - (i) is listed on the **City’s** Inventory of Evaluated Historic Resources;
 - (ii) is designated as a Municipal Historic Resource pursuant to the *Historical Resources Act* by a bylaw approved by **Council**; or
 - (iii) is included in the portion of the map shown as shaded on Schedule B; and
 - (iv) meets the criteria for a “**character home**” as set out in Schedule C;
- (b) “**retained**” means maintaining, conserving, or restoring the “**character home**” with an emphasis on the **street** facing façade elements of a “**character home**”, and may include any one or more of the following:
 - (i) conversion into multiple **Dwelling Units**;
 - (ii) raising of the floor closest to **grade** to allow for more usable **basement** space;
 - (iii) additions and extensions to the “**character home**”;
 - (iv) replacement of original materials with in-kind substitutes; and

Proposed DC Direct Control District

- (v) relocation of the “**character home**” on the **parcel** included in the **development**.

Defined Uses

5 In this Direct Control District Bylaw;

- (a) “**Character Home Retention Development**” means a **use**:
 - (i) that consists of one or more **buildings**, each containing one or more **units**; and
 - (ii) where all “**character home**” on the **parcel** included in the **development** are “**retained**” within the **development**.

Permitted Uses

6 The **permitted uses** of the Multi-Residential – Contextual Low Profile (M-C1) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District, with the addition of:

- (a) **Contextual Single Detached Dwelling**.

Discretionary Uses

7 The **discretionary uses** of the Multi-Residential – Contextual Low Profile (M-C1) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District, with the addition of:

- (a) **Character Home Retention Development**.

Bylaw 1P2007 District Rules

- 8 (1) Unless otherwise specified, the rules of the Multi-Residential – Contextual Low Profile (M-C1) District of Bylaw 1P2007 apply in this Direct Control District.
- (2) For **Contextual Single Detached Dwellings** the rules of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 apply in this Direct Control District.

Density

9 The maximum **density** is 155 **units** per hectare.

At Grade Orientation of Units

10 All **units** located on the floor closest to **grade** in a **Multi-Residential Development** or **Character Home Retention Development** must have:

- (a) an individual, separate, direct access to **grade**;
- (b) an entrance that is visible from the **street** that the **unit** faces; and
- (c) on **parcels** that share a **property line** with Maggie Street SE;

Proposed DC Direct Control District

- (i) a minimum of one **unit** with an entrance visible from Maggie Street SE; and
- (ii) a minimum of one **amenity space** facing Maggie Street SE.

Building Height and Cross Section

- 11 (1) Unless otherwise referenced in subsection (2), the maximum **building height** is 14.0 metres.
- (2) The maximum area of a horizontal cross section through a **building** at 12.0 metres above **average grade** must not be greater than 65.0 per cent of the maximum area of a horizontal cross section through the **building** between **average grade** and 10.0 metres.

Landscaping

- 12 (1) Unless otherwise referenced in subsection (2), all of the required **landscaped area** must be provided at **grade**.
- (2) The **Development Authority** may consider **landscaped area** to be provided above **grade** where:
- (a) the **landscaped area** provides a visual buffer to **adjacent development**;
 - (b) the minimum number of trees and shrubs are provided in accordance with section 552 of Bylaw 1P2007; and
 - (c) where the **landscaped area** is not located in a **setback area adjacent** to 8 Street SE.

Setback Areas

- 13 The depth of all **setback areas** must be equal to the minimum **building setbacks** required in section 14.

Building Setbacks

- 14 (1) The minimum **building setback** from a **property line** shared with a **street** for a **Multi-Residential Development** is 1.5 metres.
- (2) Unless otherwise referenced in subsection (3), for a **Multi-Residential Development** the minimum **building setback** from a **property line** shared with another **parcel** is 1.2 metres.
- (3) For a **Multi-Residential Development** the minimum **building setback** from a **property line** shared with another **parcel** may be reduced to zero metres where the **adjacent parcel** is subject to this Direct Control District or is designated as a **commercial district**.

Proposed DC Direct Control District

- (4) In all other cases the **building setbacks** of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 are the **building setbacks** in this Direct Control District.

Required Motor Vehicle Parking Stalls

- 15 (1) **Units** less than 45.0 square metres do not require **motor vehicle parking stalls**.
- (2) For **Multi-Residential Development** or **Character Home Retention Development** the number of **motor vehicle parking stalls** is as required in Part 6 of Bylaw 1P2007.
- (3) In all other cases the number of **motor vehicle parking stalls** is as required in Bylaw 1P2007.

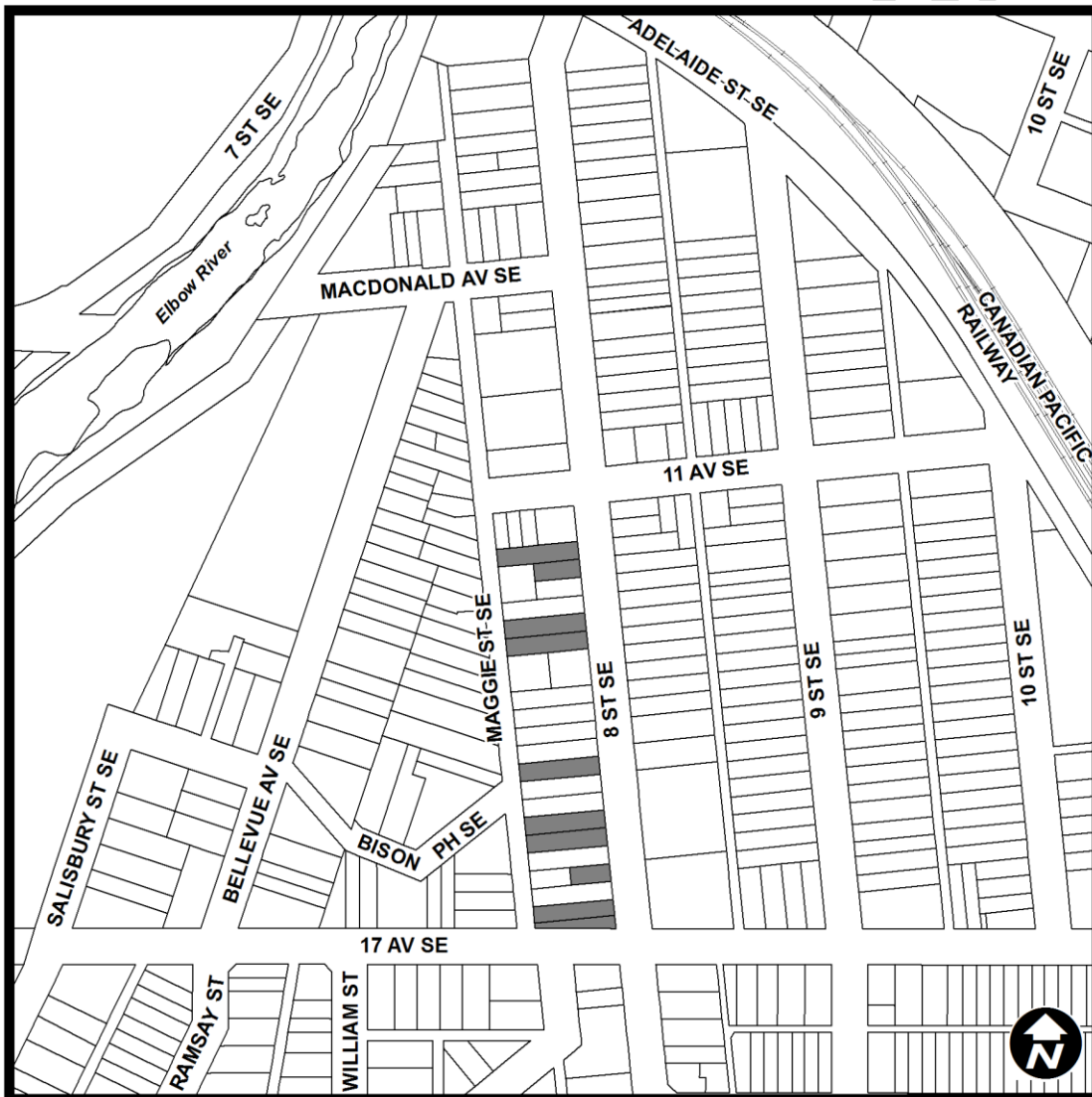
Relaxations

- 16 The **Development Authority** may relax the rules of this Direct Control District Bylaw in accordance with sections 31 and 36 of Bylaw 1P2007 in order for a “**character home**” to be “**retained**” and restored.

Proposed DC Direct Control District

SCHEDULE B

1105 – 8 Street SE
1107 – 8 Street SE
1113 – 8 Street SE
1115 – 8 Street SE
1129 – 8 Street SE
1135 – 8 Street SE
1137 – 8 Street SE
1141 – 8 Street SE
1145 – 8 Street SE
1147 – 8 Street SE



Proposed DC Direct Control District

SCHEDULE C

The following criteria are required for a “**character home**”:

- (a) A “**character home**” will:
 - (i) have been constructed prior to 1945;
 - (ii) maintain both:
 - (A) the original form, scale, massing and roof profile of the **building** and may include additions and extensions if they are compatible with the original form, scale, massing and roof profile of the **building**; and
 - (B) at least 50.0 per cent of the original window pattern and openings on the portions of the front and side façades visible from a **street** excluding **porch** glazing; and
 - (iii) have one of the following on the portions of the front façade visible from a **street**:
 - (A) original cladding; or
 - (B) a majority of original exterior finishes, comprising two or more of the following items:
 - (i) at least 50.0 per cent of window sashes;
 - (ii) soffits; exposed rafters, or beams;
 - (iii) front door or doorway assembly; and/or
 - (iv) if present, a front **porch** with majority of original finishes including piers, columns, balustrades, entablature mouldings, and may include enclosure of the **porch** with glazing where the majority of other original finishes remains in place.

Applicant Submission and List of Landowners

Rezoning of 25 lots on 8 St./Maggie St. SE in Ramsay. R-C2 to DC (M-C1) 11/4/2019

Members of Council and CPC,

This is a very unique and exciting application. The 28 owners of 24 lots in Ramsay have applied for a rezoning of their properties which comprise almost a full block along 8 St/ Maggie St. SE. We applied for the rezoning in April 2016, over three years ago, and it has been a very challenging path to finally reach CPC and Council.

The land is currently zoned RC-2 and with the Planning Dept. we have negotiated a **DC** (based on M-C1) which would basically allow all low density residential built forms up to stacked townhousing. To our understanding all heights and densities are within the Developed Areas Guidebook/draft Ramsay ARP/MDP/ and TOD area guidelines that Council has approved or will be presented with (the draft ARP).

This site is perfect for increased density and multi-unit building forms as it is:

- Within 400 m of future LRT
- Walking distance to downtown
- One of the most unattractive yet highest visibility blocks in Ramsay, and
- It contains a number of seriously deteriorated structures which cannot be restored.

Because it is a narrow block with significant grades and without a lane specific D.C. guidelines are proposed which allow more flexibility in (multi-unit) built form.

Ramsay has a very low number of multi-family units and the second highest proportion of seniors in the city resulting in a pressing need for more townhousing.

The Community and neighbours have been involved in this application over the years it has taken to get before you, and we have been pleased and surprised at the almost unanimous support.

Reason for Administration Refusal – Fee for Density

The one major disagreement which has led to a recommendation for refusal is the desire on the part of the Administration to impose a 'heritage preservation fee' on new development over certain densities in Ramsay. This is basically a heritage or density tax and represents a very significant fee for new development. The landowners, with agreement from the Community Association, oppose this fee for the following reasons:

Applicant Submission and List of Landowners

- The large size of the fee - (up to \$11,900 /unit or \$95,000 for an 8 unit townhouse) which we argue will discourage new development (and this is within a TOD area)
- The terrible economy which makes redevelopment very problematical even without new City fees
- The unclear nature of the program. It is our understanding that the density tax funds will be payable to other community residential property owners who want to upgrade their properties.
- Basically, the current landowners (applicants) will be paying money (through reduction of selling prices for their property) to other landowners in the community (who will have home renovations undertaken with the funds). The benefitting landowners will have to own houses that have a registered heritage status. There are no such properties in Ramsay.

We strongly argue that if this fee is going to be levied it should be part of the future Historic East Calgary ARP approval and should be discussed at Council and a decision made for the whole community. The ARP is scheduled to be considered by Council within the next year. Imposing a fee of this magnitude (which we argue will push TOD area development to other communities with NO LRT and NO fee) is a major decision and should be decided by Council through a full community-wide discussion and review.

We have been told that should Council approve this DC rezoning with the 'heritage housing fee' included and then not approve the 'heritage housing fee' in the proposed ARP then we, the applicant landowners, will have to apply at our cost to re-rezone the properties back to remove this fee. This would be absurd.

The landowners/applicants, although believing the fee would discourage new development are willing, of course, to accept the fee if it is approved by Council in the ARP. We have been told that the Law Dept. has said that once this DC is approved the owners on this block will be exempt from a 'heritage housing fee' even if it is approved in the upcoming ARP and therefore the staff is demanding that it be included in the DC. To repeat the Owners/Applicants are fully willing to pay any Council required fees – we don't want to be exempt however we don't want to pay the fees until Council has approved them in the new ARP.

Somewhat similar fees have been included in the Beltline and Brentwood ARPs and would presumably apply to all development in those communities. We suggest that a similar approach be applied in Ramsay.

Applicant Submission and List of Landowners

It is our request that should Council not support this proposed DC as requested by the applicant that Council simply refuse the rezoning rather than amending it to include the density fees.

List of Landowners

NAME
Campbell, David
Ledene, Debbie
Hardman, Alexander James
Zacher, Bonny
Cordner, Richard
Ilgert, Jonathan
Ilgert, Kristen
M1 Group INC.
M1 Group INC.
Zacher, Darren Anthony
Colborne, Joshua
Gehl, Mark
Blais, Brian
Blais, Brian
Garrow, Jason
Garrow, Jayne
Vanderknaap, Theodorus Johannes
MccCrohan, Barry
McCrohan, Barry
Davis, Michael Norman
Davis, Susan Marie
Dormer, Christopher
Piechotta, Katrina
Jensen, David P
Francis, Desiree
Francis, Steve
Clarey, Shauna
Gallant, Michael
Campbell, David
Ledene, Debbie
Ledene, Jodie

Application Timeline

DATE	APPLICATION MILESTONE
April 14, 2016	Application submitted.
April 26, 2016	ITR issued, City initiated hold complex application.
May 12, 2016	Meeting with applicant to discuss ITR and options for moving forward.
May 25, 2016	Amended ITR sent to applicant with three options for moving forward.
June 6, 2016	<p>Response to ITR received, Applicant elected to move forward with option (C) as outlined in the ITR:</p> <p>(A) Request Cancellation of the application at this time and wait for the outcome of the City initiated Local Area Planning/ Greenline LRT Planning policy work. Any applicable fee refund will be determined at the time of cancellation. If this option is selected, an approximate 90% application fee refund may be provided at this time as a full review has not commenced.</p> <p>(B) Pursue the comprehensive review of the application, as proposed, with a potential recommendation of refusal if the application does not adequately address the key considerations and technical development review issues identified above and further comments resulting from a full evaluation. If this option is pursued, only an approximate 10% application fee refund may be obtained if the proposal is cancelled and abandoned at a later time, prior to Calgary Planning Commission.</p> <p>(C) Pursue the application acknowledging the City led planning process currently underway. The application would be brought to CPC and Council after the current local area planning/ Green Line LRT policy work, and Administration will work with the Applicant to ensure the application is aligned with the proposed policy.</p> <p>As part of Option (C) Administration recommends:</p> <ul style="list-style-type: none"> • Application specific community engagement to clarify relationship of this application to the Green Line planning process. Administration is happy to work with you on this. Applicants for similar applications have entered into a Memorandum of Understanding outlining the community engagement plan and expectations from both the City and the applicant. An example memorandum is attached for your reference. • Amending the applicant submission to focus more on the high level aspirations for the application rather than the specific planning mechanisms. A higher level submission will avoid creating expectations with the adjacent neighbours and community association that may or may not be met as the application moves through the review process. An example applicant submission is attached for your reference.
August 26, 2016	DTR 1 sent to applicant.
October 18, 2016	Response to DTR 1 received.

Application Timeline

October 31, 2016	Meeting with applicant to discuss DTR 1 and options for moving forward.
December 7, 2016	DTR 2 sent to applicant.
January 19, 2017	Meeting with applicant to discuss DTR 2 and options for moving forward.
March 15, 2017	Direction from Community Planning and Calgary Growth Strategies management to work with the applicant to find a mechanism to balance increased intensity with provision of community benefit in alignment with ARP discussions.
March 22, 2017	Meeting with Community Planning – South Manager, File Manager and applicant to discuss finding a mechanism to balance increased intensity with provision of community benefit.
April 12, 2017	Request from applicant to amend application site to include 2 additional parcels.
April 28, 2017	Decision that use of a Direct Control District most appropriate tool to incorporate a mechanism to balance increased intensity with provision of community.
May 4, 2017	First draft of Direct Control District with a mechanism to balance increased intensity with provision of community sent to applicant for feedback and input.
June 6, 2017	Request from applicant to delay CPC date to allow more time to discuss Direct Control District.
October 11, 2017	Email to applicant requesting update.
November 9, 2017	Confirmation received from applicant that landowners would like to move forward with application, but no details on which of the three options is preferred.
December 8, 2017	Meeting with applicant to discuss how landowners would like to move forward.
February 7, 2018	Submission of new Direct Control District with no mechanism to balance increased intensity with provision of community benefit submitted by applicant.
February 16, 2018	Administration comments on Applicant Direct Control District sent to applicant.

Application Timeline

February - May 2018	Refining of Applicant Direct Control District by applicant and Administration
July 5, 2018	Request to delay CPC by applicant.
August 10, 2018	Communication from applicant that application maybe cancelled.
August 16, 2018	Response provided to applicant with options for moving forward, final decision on preferred option requested by September 10, 2018: <ol style="list-style-type: none"> 1. Amended application to incorporate a mechanism to balance increased intensity with some provision of community benefit, or amend application to a reduced site with a concurrent DP; 2. Put application on hold or cancel application until ARP is finalized; or, 3. Move forward with a Direct Control District without a mechanism to balance increased intensity with some provision of community benefit and a recommendation of refusal from Administration.
September 18, 2018	No response received, October 17, 2018 deadline provided to applicant to confirm preferred option for moving forward to complete application before end of 2018.
November 20, 2018	Request for confirmation from applicant on preferred option for moving forward and suggestion that application be put on a Customized Timeline.
December 19, 2018	Confirmation from applicant that preferred option is to move forward with a recommendation for refusal and proposed Applicant Direct Control District with no mechanism to balance increased intensity with provision of community benefit. Administration communicated that updated letters of authorization from new landowners, confirmation of which parcels will participate in application, and updated Applicant Submission required prior to CPC.
February 21, 2019	Updated application site submitted by applicant.
April 4, 2019	Updated letters of authorization submitted by Applicant, application added to June 6 agenda in conversation with Applicant.
May 1, 2019	Inquiry from applicant about increasing maximum density in proposed Direct Control District from 148-178 uph.
May 21, 2019	Update from applicant that one landowner may no longer be interested in participating in application, confirmation of request to change maximum density to 155 uph in proposed Direct Control District, and request to delay CPC to June 20.

Application Timeline

June 3, 2019	Application removed from June 20 CPC due to lack of response from applicant to confirm which parcels will participate in the application.
June 6, 2019	Confirmation that one landowner will participate in application. Application added to July 18, 2019 CPC agenda.
July 10, 2019	Application pulled from July 18, 2019 CPC agenda by Community Planning Management. Moved to September 5, 2019 CPC agenda to provide time to improve CPC report and more clearly reflect where Administration and the applicant agree and where they differ.
August 23, 2019	Request from applicant to delay CPC in order to adjust DC District to include Contextual Single Detached Dwelling as a permitted use and pursue having an additional landowner join the application.
October 24, 2019	Update from applicant that additional landowner is not interested in joining the application and confirmation to move forward to CPC. Application added to November 21, 2019 CPC agenda.

Character Home Components



Administration Supportable Proposed DC Direct Control District

Purpose

- 1 This Direct Control District Bylaw is intended to:
- (a) allow for transit oriented development in proximity to an LRT station;
 - (b) allow for a broad range of residential development including low-rise grade-oriented multi-residential development that responds to the interface with 8 Street SE and Maggie Street SE and the unique character of the block and neighbourhood;
 - (c) provide incentive for the retention of “**character homes**” through increased options for multi-residential and grade-oriented residential development where “**character homes**” are being “**retained**”; and
 - (d) implement a density bonus provision to balance the increased intensity with contribution to heritage preservation and enhancement in the community of Ramsay.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District Bylaw.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District Bylaw, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

General Definitions

- 4 In this Direct Control District Bylaw:
- (a) “**character home**” means a **Duplex Dwelling, Semi-detached Dwelling, or Single Detached Dwelling** existing on the date of passage of this Direct Control District Bylaw that:
 - (i) is listed on the **City’s** Inventory of Evaluated Historic Resources;
 - (ii) is designated as a Municipal Historic Resource pursuant to the *Historical Resources Act* by a bylaw approved by **Council**; or
 - (iii) is included in the area shown in Schedule B; and
 - (iv) meets the criteria for a “**character home**” as set out in Schedule C;
 - (b) “**retained**” means maintaining, conserving, or restoring the “**character home**” with an emphasis on the **street** facing façade elements of a “**character home**”, and may include any one or more of the following:
 - (i) conversion into multiple **Dwelling Units**;

Administration Supportable Proposed DC Direct Control District

- (ii) raising of the floor closest to **grade** to allow for more usable **basement** space;
- (iii) additions and extensions to the “**character home**”;
- (iv) replacement of original materials with in-kind substitutes; and
- (v) relocation of the “**character home**” on the **parcel** included in the **development**.

Defined Uses

5 In this Direct Control District;

- (a) “**Character Home Retention Development**” means a **use**:
 - (i) that consists of one or more **buildings**, each containing one or more **units**; and
 - (ii) where all “**character homes**” on the **parcel** included in the **development** are “**retained**”.

Permitted Uses

6 The **permitted uses** of the Multi-Residential – Contextual Grade-Oriented (M-CG) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District, with the addition of:

- (a) **Contextual Single Detached Dwelling.**

Discretionary Uses

7 The **discretionary uses** of the Multi-Residential – Contextual Grade-Oriented (M-CG) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District, with the addition of:

- (a) **Character Home Retention Development.**

Bylaw 1P2007 District Rules

- 8**
 - (1) Unless otherwise specified, the rules of the Multi-Residential – Contextual Low Profile (M-C1) District of Bylaw 1P2007 apply in this Direct Control District.
 - (2) For **Contextual Single Detached Dwellings** the rules of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 apply in this Direct Control District.

Administration Supportable Proposed DC Direct Control District

Density

- 9 (1) Unless otherwise referenced in subsection (2) or (3) the maximum **density** is 111 **units** per hectare.
- (2) The maximum **density** for a **Character Home Retention Development** is 155 **units** per hectare.
- (3) The maximum **density** referenced in subsection (1) may be increased to 155 **units** per hectare where one or more of the following is provided:
- (a) additional **density** is transferred from another site provided that:
 - (i) the historic resource on the donor site has been designated as a municipal historic resource under the *Historical Resources Act*;
 - (ii) the historic resource (donor site) is located within the community of Ramsay; and
 - (iii) prior to the release of the **development completion permit** for the receiving site, the historic resource (donor site) is designated to a Direct Control District to enable the transfer of **density** and clarify any applicable land use restrictions and any remaining allowable **density**; and
 - (b) A contribution to the City of Calgary's Heritage Incentive Reserve Fund at the time of the **Development Completion Permit** application, such that:

$$(\text{Average land value per square metre} \times 50.0 \text{ per cent}) \times \text{Proposed amount of increased floor area in square metres above the floor area ratio of 1.11} = \text{Cash Contribution Rate}$$

For the purposes of this subsection: "Cash Contribution Rate" will be based on average land value per additional **density**. "Average land value" means the average land value per a square metre in the community of Ramsay at the time of **Development Permit** application as approved by **Council** and reviewed annually. Expert analysis submitted by an applicant acceptable to the **Development Authority** may be used to determine average land value if **Council** has not approved a value.
- (4) For the purpose of this subsection to calculate **floor area ratio** for **density** measured in **units** per hectare, one **unit** is considered to be equal to 100 square metres.

At Grade Orientation of Units

- 10 All **units** located on the floor closest to **grade** in a **Multi-Residential Development** or **Character Home Retention Development** must have:
- (a) an individual, separate, direct access to **grade**;

Administration Supportable Proposed DC Direct Control District

- (b) an entrance that is visible from the **street** that the **unit** faces; and
- (c) on **parcels** that share a **property line** with Maggie Street SE;
 - (i) a minimum of one **unit** with an entrance visible from Maggie Street SE; and
 - (ii) a minimum of one **amenity space** facing Maggie Street SE.

Building Height and Cross Section

- 11 (1) Unless otherwise referenced in subsection (3), the maximum **building height** is 12.0 metres.
- (2) Unless otherwise referenced in subsection (3), the maximum area of a horizontal cross section through a **building** at 11.0 metres above **average grade** must not be greater than 65.0 per cent of the maximum area of a horizontal cross section through the **building** between **average grade** and 9.0 metres.
- (3) For a **Character Home Retention Development** or a **development** that uses any of the **density** bonus provisions contained in Section 10 of this Direct Control District Bylaw:
- (a) the maximum **building height** is 14.0 metres; and
 - (b) the maximum area of a horizontal cross section through a **building** at 12.0 metres above **average grade** must not be greater than 65.0 per cent of the maximum area of a horizontal cross section through the **building** between **average grade** and 10.0 metres.

Landscaping

- 12 (1) Unless otherwise referenced in subsection (2) all of the required **landscaped area** must be provided at **grade**.
- (2) The **Development Authority** may consider **landscaped area** to be provided above **grade** where:
- (a) the **landscaped area** provides a screen to **adjacent development**;
 - (b) the minimum number of trees and shrubs are provided in accordance with section 552 of Bylaw 1P2007; and
 - (c) where the **landscaped area** is not located in a **setback area adjacent** to 8 Street SE.

Setback Areas

- 13 The depth of all **setback areas** must be equal to the minimum **building setbacks required** in section 14.

Administration Supportable Proposed DC Direct Control District

Building Setbacks

- 14 (1) The minimum **building setback** from a **property line** shared with a **street** for a **Multi-Residential Development** is 1.5 metres.
- (2) Unless otherwise referenced in subsection (3), for a **Multi-Residential Development** or a **Character Home Retention Development** the minimum **building setback** from a **property line** shared with another **parcel** is 1.2 metres.
- (3) For a **Multi-Residential Development** or a **Character Home Retention Development** the minimum **building setback** from a **property line** shared with another **parcel** may be reduced to zero metres where the adjacent **parcel** is subject to this Direct Control District or is designated as a **commercial district**.
- (4) In all other cases the **building setbacks** of the Residential – Grade-Oriented Infill (R-CG) District of Bylaw 1P2007 are the **building setbacks** in this Direct Control District.

Required Motor Vehicle Parking Stalls

- 15 (1) **Units** less than 45.0 square metres do not require **motor vehicle parking stalls**.
- (2) For **Multi-Residential Development** or **Character Home Retention Development** the number of **motor vehicle parking stalls** is as required in Part 6 of Bylaw 1P2007.
- (3) In all other cases the number of **motor vehicle parking stalls** is as required in Bylaw 1P2007.

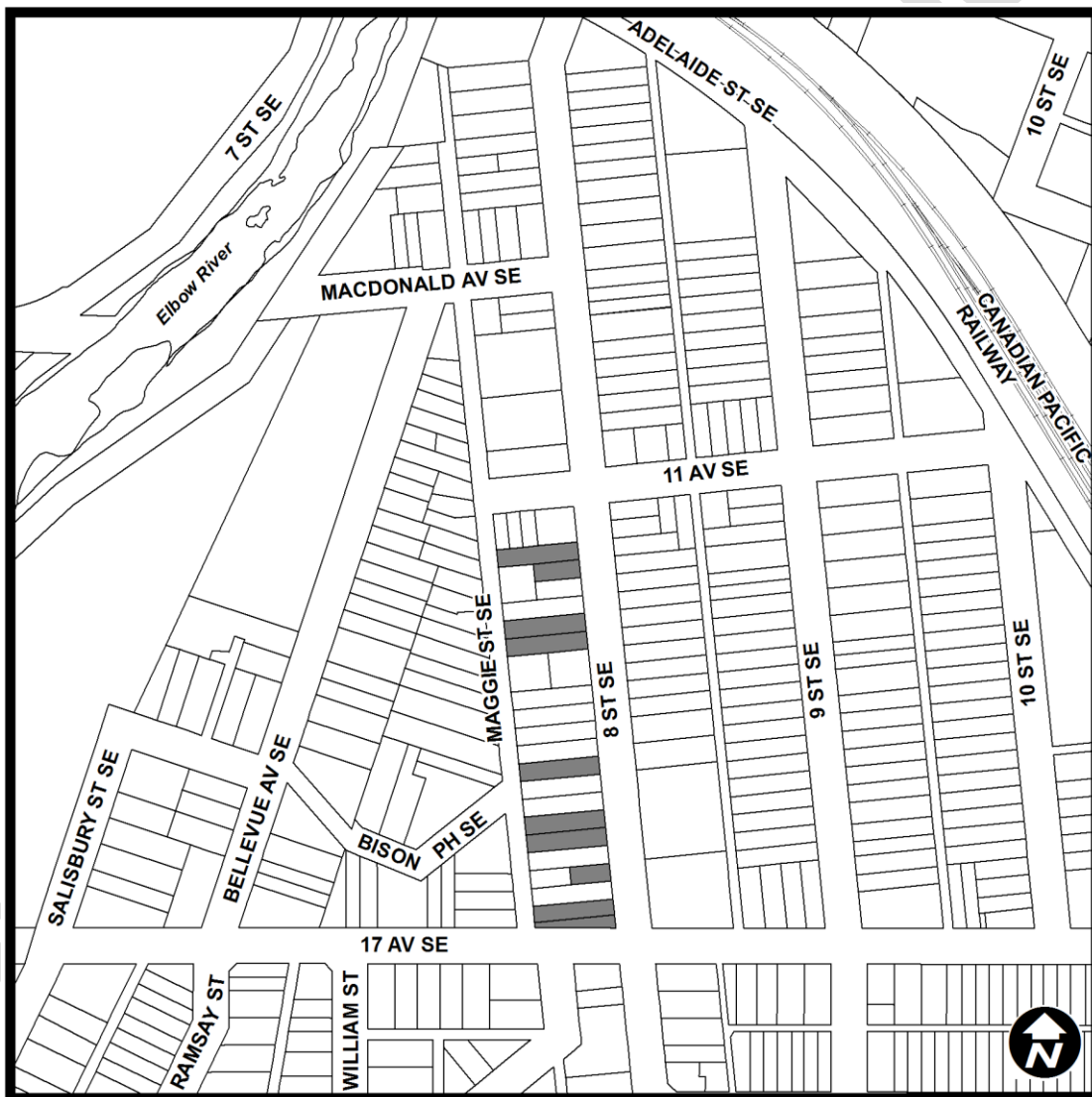
Relaxations

- 16 The **Development Authority** may relax the rules of this Direct Control District Bylaw in accordance with sections 31 and 36 of Bylaw 1P2007 in order for a "**character home**" to be "**retained**" and restored.

Administration Supportable Proposed DC Direct Control District

SCHEDULE B

1105 – 8 Street SE
1107 – 8 Street SE
1113 – 8 Street SE
1115 – 8 Street SE
1129 – 8 Street SE
1135 – 8 Street SE
1137 – 8 Street SE
1141 – 8 Street SE
1145 – 8 Street SE
1147 – 8 Street SE



Administration Supportable Proposed DC Direct Control District

SCHEDULE C

The following criteria are required for a “**character home**”:

- (a) A “**character home**” will:
 - (i) have been constructed prior to 1945;
 - (ii) maintain both:
 - (A) the original form, scale, massing and roof profile of the **building** and may include additions and extensions if they are compatible with the original form, scale, massing and roof profile of the **building**; and
 - (B) at least 50.0 per cent of the original window pattern and openings on the portions of the front and side façades visible from a **street** excluding **porch** glazing; and
 - (iii) have one of the following on the portions of the front façade visible from a **street**:
 - (A) original cladding; or
 - (B) a majority of original exterior finishes, comprising two or more of the following items:
 - (i) at least 50.0 per cent of window sashes;
 - (ii) soffits; exposed rafters, or beams;
 - (iii) front door or doorway assembly; and/or
 - (iv) if present, a front **porch** with majority of original finishes including piers, columns, balustrades, entablature mouldings, and may include enclosure of the **porch** with glazing where the majority of other original finishes remains in place.

Community Association Letters



July 21, 2016

Kate Van Fraassen
Planning, Development and Assessment #8703
P.O. Box 2100, Station M
Calgary AB
T2P 2M5

Community Response

RE: LOC2016-0088; 1113 8th Street SE

The Civic Affairs Committee has reviewed the available information for this application.

The information provided to us to review was very minimal, making it difficult to fully comment on the impact of this zoning change.

Based on the information provided, as well as the additional information provided by the applicant at a general meeting, these are our comments:

1. **Context** - The proposed DC must take into account the surrounding context. The biggest concern is the interface with Maggie STREET. This is a Street, and there are concerns over how development might change this street. What is being done to ensure this remains a Street, and that there is frontage onto Maggie, not simply access into larger developments. Maggie Street also slopes to up with a higher elevation on the South End that could impact development opportunities.
2. **Zoning** - Without having a context for the actual bylaws surrounding this DC, we do not have the opportunity to comment on set backs, heights etc. There isn't even a related Land Use that is being referenced.

Community Association Letters

3. **Parking** - Concerns over access to 8th Street, and how that might impact our main road. Access is either off of Maggie, or off of the main road in Ramsay, 8th Street. Depending on how this properties develop will have impact on both parking and parking access. This concern stems from a similar development (DP2015-3271; 1709 8 Street SE) where the original plan was a multi-unit, multi-property development that had vehicle access from 17th Ave. When the developer was unable to acquire two additional lots, the parking access was move to come off 8th Street. The community was not happy with this change in the development, and had concerns over pedestrian safety as well as traffic access onto our main Road.
4. **Infrastructure** - Can the current infrastructure support this level of density on that street?

Without knowing the specifics of this DC Zoning, it is difficult to evaluate if the proposed change falls in line with the discussions coming out of the Green Line Charrettes. We are in general support of the concept, but wish to further understand the details, and access the impact.

The Ramsay Community Association welcomes sustainable new development that respects the history and emerging character of the neighborhood, complies with the Area Redevelopment Plan and Bylaws, and will be a benefit to the people who live here.

Respectfully,
Erin Joslin

Name of Planning Representative/s who wrote the letter:

Erin Joslin, M.Arch,
Vice President External
Chair Civic Affairs Committee.

Community Association: Ramsay Community Association

Date: July 21, 2016

Community Association Letters



August 23, 2018

Kate Van Fraassen
Planning, Development and Assessment #8703
P.O. Box 2100, Station M
Calgary AB
T2P 2M5

Community Response

RE: LOC2016-0088; 1113 8th Street SE

An update has been provided to the community. Our previous comments provided July 21, 2016 should remain part of the ongoing conversation.

Based on the updated information provided, these are our comments:

1. **Context** - The proposed DC must take into account the unique context. The biggest concern remains the lack of consideration for the interface with Maggie STREET. Maggie is a very unique heritage street, with houses that front onto it. Concerns remain over how development might change this street especially when the street is being considered more like a lane. Maggie Street should not simply become a lane allowing access into larger developments. We need to see that the Maggie is respected as a street and maintains its current feel. We understand that the trade off is potential developments having vehicle access off 8th Street. This would be desired over creating multiple access points off of Maggie. Please keep in mind that the character of 8th Street, in terms of traffic, will also change as the end of the street is closed off and cul-de-saced and a potential new underpass is added on 5th Street. It is assumed that less traffic will come through the neighbourhood as a result. This might make access off 8th more appealing and possible. We need to keep this opportunity open to change with the changing conditions of Ramsay. Also be aware that Maggie Street slopes up with a higher elevation on the South End that could impact development

Community Association Letters

opportunities in terms of Maggie Street Access. The development of these properties should encourage designs that respect the character, frontage and heritage of Maggie Street. We can't be having conversations about character homes and completely disregard a significant heritage streetscape.

2. **ARP** – The proposed zoning falls along the lines of the density being discussed for our New Ramsay Area Redevelopment Plan. The community supports the density as outlined in our draft ARP. The DC for these properties should take into account the new ARP being proposed and should be held to the same rules and regulations. A clause somehow indicating that the DC is to follow the final approved ARP should be part of the discussion.
3. **Character Homes** – This is a larger discussion that should be in the ARP and NOT in the DC. We would like the opportunity, as a community, to have input into the character home discussion. This discussion would happen with the ARP development and should not be specific to this DC. The DC should say that rules surrounding heritage and character homes apply as per the ARP. The proposed fees should also be part of an ARP document and greater community conversation. This is not the place to have this information solidified. We are currently having all of these conversations as part of our ARP development.
4. **Process** – The community has been involved in this process as it has moved along. There have been meetings, newsletter information as well as open houses. Information has been communicated to the community from the applicant's side.

The Ramsay Community Association welcomes sustainable new development that respects the history and emerging character of the neighborhood, complies with the Area Redevelopment Plan and Bylaws, and will be a benefit to the people who live here.

Respectfully,
Erin Joslin

Name of Planning Representative/s who wrote the letter:
Erin Joslin, M.Arch,
Vice President External
Chair Civic Affairs Committee.